



# HURRICANE CITY UTAH

## Planning Commission

*Mark Sampson, Chair  
Shelley Goodfellow, Alternate Chair  
Ralph Ballard  
Paul Farthing  
Rebecca Broneman  
Michelle Cloud  
Brad Winder  
Kelby Iverson*

**Mayor**

**City Manager**

Nanette Billings    Kaden DeMille

### Hurricane Planning Commission Meeting Agenda

June 8, 2023

5:00 PM

Hurricane City Offices 147 N 870 W, Hurricane

Notice is hereby given that the Hurricane City Planning Commission will hold a Regular Meeting commencing at 5:00 p.m. at the Hurricane City Offices 147 N 870 W, Hurricane, UT.

Meeting link:

<https://cityofhurricane.webex.com/cityofhurricane/j.php?MTID=me42b4eb65609e35eb0e0664c925c9dbc>

Meeting number:2632 882 4836

Password:HCplanning

Host key:730111

Join by phone+1-415-655-0001 US Toll

Access code: 2632 882 4836

Host PIN: 9461

Details on these applications are available in the Planning Department at the City Office, 147 N. 870 West.

#### **5:00 p.m. - Planning Commission Business:**

1. City Council Recap
2. Discussion on development standards in park model developments

#### **6:00 p.m. - Call to Order**

Roll Call

Pledge of Allegiance

Prayer and/or thought by invitation

Declaration of any conflicts of interest

#### **Public Hearings**

1. A Zone Change Amendment request located 191 W 600 N from MH/RV, mobile home/RV, to RM-1, multifamily 6 units per acre, to build a duplex. Parcel number H-281-A-4-A.
2. A General Plan Amendment to adopt a Downtown Master Plan.

#### **OLD BUSINESS**

## **NEW BUSINESS**

1. ZC23-13: Discussion and consideration of a recommendation to the City Council on a Zone Change Amendment request located 191 W 600 N from MH/RV, mobile home/RV, to RM-1, multifamily 6 units per acre, to build a duplex. Parcel number H-281-A-4-A. Silo One Investments LLC Applicant. Chris Potter Agent.
2. Discussion and consideration of a recommendation to the City Council on a General Plan Amendment to adopt a Downtown Master Plan.
3. 2023-APA-01: Discussion and consideration of a recommendation to the City Council on an agricultural protection overlay for Kenneth Heaton located at 1200 S 1150 W. Kenneth Heaton Applicant.
4. WCUP23-01: Discussion and consideration of a possible approval of a conditional use permit for a wall of greater height located at 2802 S 3440 W. Gary Apparcel Applicant.
5. PSP23-15: Discussion and consideration of a possible approval of a preliminary site plan for the American Conservation Experience, a hostel located at 73 S Main St. American Conservation Experience Applicant. James Derby Agent.
6. 2022-PP-08: Discussion and consideration of an extension of the preliminary plat approval for Strawberry Fields Estates, a 163 lot single family subdivision located at approximately 1400 S 3700 W. Western Mortgage and Realty Co Applicant. Karl Rasmussen Agent.

### **Approval of Minutes:**

1. May 17, 2023
2. May 25, 2023

### **Adjournment**



## Planning Commission June 8th, 2023 Work Meeting

At the June 1st City Council meeting during the discussion on the application for modified zoning standards for the Canyon Villas development the City Council approved the larger park model units within the PDO with the added direction that staff start the process to amend the park model ordinance with regards to parking, size of units, and additions to buildings.

The standards for a park model development are located in [Title 10 Chapter 43](#) and are, for the most part, identical to the RV park standards. These are some of the highlights of the park model ordinance:

- Park model developments cannot be subdivided\* (Table 10-43-1)
- Minimum lot size: 1,000 sqft \* (Table 10-43-1)
- Setbacks \* (Table 10-43-1)
  - Front: 5ft
  - Rear: 5ft
  - Side: 7ft
- Maximum unit size: 400 sq ft (HCC 10-3-4) however room additions are allowed as long as setbacks are met and 50% of the lot remains open (HCC 10-43-6 D7)
- A park model cannot be used as a short-term rental unless it is part of an approved RV park (HCC 10-43-6 (D2))
- Parking: one space per unit\* (HCC 10-43-6 (C7))
- \*indicates this standard is shared with RV parks in the code

The Council's general direction was that they have concerns about subdivisions not being allowed and the land-lease program that often leads to. They thought the 400 sq ft maximum size is too small for even one person to live in but any increase in the allowed size should come with an increase in the minimum lot size and parking requirements. They also did not like the idea of allowing additions onto park models due to the haphazard look and nature of those.

Staff believes there are several options the Planning Commission can consider with regards to park models:

1. Amend Table 10-43-1 and 10-43-6 (D) to have park model developments have separate development standards from RV parks.



*Fred Resch III, Assistant City Planner-Code Enforcement*

*Brienna Spencer, Planning Technician*

2. Eliminate park models in their entirety from the code and have park model units considered under the same standards as other types of manufactured homes. There are currently standards for manufactured home parks and manufactured home subdivisions in HCC 10-43.
3. Rewrite the entire MH/RV zoning standards to make it more user friendly and easier to understand as well as make needed changes to the park model code to avoid this confusion in the future.
  - a. Earlier this year the owner of the Roberts Roost development filed for an expansion but it was not approved because it couldn't meet current ordinances, a broader overall update could allow them to proceed.

**Staff Recommendation:**

Staff recommends the Planning Commission review Title 10 Chapter 43 with regards to the park model standards as well as this report and come ready to discuss this item on June 8th.

Staff will do some research on the history of this ordinance and what was intended with some of the standards and come up with some possible code language.

## CHAPTER 43. MANUFACTURED/MOBILE HOME AND RECREATIONAL VEHICLE REGULATIONS

### Sec. 10-43-1. Purpose.

The purpose of this chapter is to establish regulations for manufactured/mobile home parks and subdivisions, and recreational vehicle parks in order to achieve the following objectives:

- A. To assure that development of such parks and subdivisions promote the purpose and objectives of the City general plan and this title;
- B. To permit variety, flexibility, and affordability in land development for residential purposes in designated areas of the City;
- C. To protect the integrity and characteristics of zones contiguous to zones where manufactured/mobile home parks and subdivisions, and recreational vehicle parks are located; and
- D. To protect the integrity of land use values contiguous to and within such parks and subdivisions.

(Ord. 2004-15, 6-17-2004)

### Sec. 10-43-2. Scope.

No manufactured/mobile home or recreational vehicle shall be used or occupied except within an approved manufactured/mobile home park or subdivision, or recreational vehicle park, or as specifically provided by a provision of this chapter or this title. Such requirements shall not be construed to prohibit or limit other applicable provisions of this title, this Code, or other laws.

(Ord. 2009-13, 10-1-2009)

### Sec. 10-43-3. Uses allowed.

- A. *Permitted and conditional use.* A manufactured/mobile home park or subdivision, and recreational vehicle park may be established in an MH/RV zone as set forth in chapter 13, residential zones, of this title, subject to the provisions of this chapter, or as otherwise specifically provided in this chapter or this title. In the event a provision of this chapter conflicts with a provision in another chapter of this title, the provision of this chapter shall apply.
- B. *Location of an occupied manufactured/mobile home.* No occupied manufactured/mobile home shall be located anywhere within the City except as follows:
  1. A manufactured home, when attached to a permanent foundation in accordance with plans providing for vertical loads, uplift, and lateral forces and frost protection in compliance with applicable building code, may be located within any single-family residential zone subject to the requirements of that zone; or
  2. Within an approved manufactured/mobile home park or subdivision; or
  3. As a dwelling unit on a bona fide farm or ranch when allowed by the zone where the ranch or farm is located; or

- 
4. Any of the following temporary uses, subject to the issuance of a temporary use permit pursuant to the requirements of section 10-7-16 and chapter 48 of this title:
    - a. Construction field office for use by contractors or as a temporary place of business for an owner or lessee while a permanent building is under construction on the same site;
    - b. Dwelling for members of an immediate family (parents, children, brothers, or sisters), subject to the requirements of section 10-43-4 of this chapter;
    - c. Sales office for manufactured/mobile homes or recreational vehicles, when allowed by the zone where the office is located;
    - d. Sales office for the sale of dwellings in a subdivision or other residential project.
  - C. *Location of an occupied recreational vehicle.* No occupied recreational vehicle shall be located anywhere within the City except as follows:
    1. Within a:
      - a. Recreational vehicle park;
      - b. Manufactured/mobile home park, subject to the requirements of subsection 10-43-6A2b of this chapter; or
      - c. Designated camping area; and
    2. As a dwelling for members of an immediate family (parents, children, brothers, or sisters), subject to the requirements of section 10-43-4 of this chapter.
    3. This section shall not be construed to prohibit the occupation of a recreational vehicle by visitors to occupants of a single-family residential lot improved with one single-family residence for a time not exceeding two weeks (14 days) in any 90-day period.
  - D. *Location of an unoccupied manufactured/mobile home or recreational vehicle.* An unoccupied manufactured/mobile home or recreational vehicle may be located as follows:
    1. On a sales lot, when allowed by the zone where the lot is located; and
    2. Long term storage of a recreational vehicle or manufactured/mobile home for maintenance operations, reconstruction, or construction is permitted only within an enclosed area and only in a zone allowing such use.
    3. This subsection shall not be construed to prohibit the storage of an unoccupied recreational vehicle on the property of the vehicle's owner or in a commercial lot approved for the storage of recreational vehicles.
  - E. *Emergency or temporary parking.* Emergency or temporary parking of an unoccupied manufactured/mobile home or recreational vehicle outside an approved manufactured/mobile home park or subdivision, or recreational vehicle park, or other authorized place may be permitted for a period not exceeding 48 hours. This limitation does not apply to an unoccupied manufactured/mobile home or recreational vehicle located in an authorized sales area.

(Ord. 2004-15, 6-17-2004; Ord. 2009-13, 10-1-2009; Ord. 2011-11, 12-15-2011; Ord. 2012-2, 4-19-2012)

#### **Sec. 10-43-4. Temporary dwelling.**

- A. *Conditions for issuance.* A manufactured/mobile home or recreational vehicle may be used for a temporary dwelling on a lot not located in a recreational vehicle park or manufactured/mobile home park as a dwelling

---

for members of an immediate family subject to the issuance of a temporary use permit as provided in section 10-7-16 and chapter 48 of this title, and the following requirements:

1. No temporary use permit shall be issued except in connection with the construction of a new dwelling or the temporary placement of a recreational vehicle on a residential lot subject to the standards in subsection B of this section.
  2. A permit for a temporary dwelling used by members of an immediate family during construction of a new dwelling shall be issued only for the property where the new dwelling is to be constructed.
  3. A valid building permit for construction of the new dwelling shall be issued and fully paid for prior to the issuance of a temporary use permit for a temporary dwelling used by members of an immediate family during construction of a new dwelling.
  4. All applicable fees shall be paid prior to placement of the manufactured/mobile home or recreational vehicle to be used as a temporary dwelling used by members of an immediate family during construction of a new dwelling on the subject property.
  5. All utility connections shall conform to City standards.
  6. No temporary use permit for a temporary dwelling used by members of an immediate family during construction of a new dwelling shall be approved for longer than six months unless an extension is approved prior to expiration of the original permit. Such extension shall not exceed six months.
- B. *Recreational vehicle.* Standards for temporary placement of a single recreational vehicle on a residential lot for use as a temporary dwelling by an immediate family member (parents, children, brothers, or sisters) in case of financial hardship.
1. Recreational vehicle must be located on a lot containing a single-family dwelling and must be occupied by an immediate family member of the family occupying the dwelling.
  2. No rent may be paid for the temporary placement.
  3. A temporary use permit must be issued for a period not exceeding six months. One six-month extension may be granted by the City Council. Such extension must be in writing.
  4. Holding tanks must be emptied into an approved facility. Power connections must conform to prevailing safety standards. Water must be connected to occupied dwelling unit's culinary water system only.
  5. Failure to comply with standards listed above is grounds for immediate revocation of the temporary use permit.
- C. *Removal upon expiration.* Upon expiration, or revocation, of a temporary use permit, or any extension thereof, a manufactured/mobile home or recreational vehicle shall be removed from the premises within ten days.
- D. *Agreement required.* No temporary use permit shall be issued unless the permittee has executed an agreement with the City which contains the terms and conditions set forth in subsections A and B of this section, and such other terms as may be reasonably required by the City to assure compliance with this section.

(Ord. 2009-13, 10-1-2009)

### **Sec. 10-43-5. Basis for approval.**

- A. *Manufactured home or recreational vehicle park or park model park.*

- 
1. A manufactured home or recreational vehicle park or park model park may be approved by the City Council in locations where such use is allowed as a permitted or conditional use by the applicable zone, pursuant to applicable requirements of this chapter. Prior to use or occupancy of any such park:
    - a. A site plan shall be approved as provided in section 10-7-10 of this title; and
    - b. A conditional use permit shall be approved as provided in section 10-7-9 of this title where the applicable zone allows such parks only as a conditional use.
  2. Before final approval is granted for any manufactured home or recreational vehicle park or park model park, a report to the City Council by the Planning Commission shall find the proposed development will:
    - a. Be in keeping with the general character of the zone in which the park is to be located;
    - b. Have an approved financing plan for construction and phase completion, together with an approved security to assure compliance and completion; and
    - c. Meet applicable development standards of section 10-43-6 of this chapter and all requirements of other applicable ordinances, except where such requirements are modified by a planned development approved pursuant to chapter 23 of this title.
- B. *Manufactured home subdivision.*
1. A manufactured home subdivision may be approved by the City Council in locations where such use is allowed as a permitted or conditional use by the applicable zone, pursuant to applicable requirements of this chapter. Prior to construction, use, or occupancy of any such subdivision:
    - a. A preliminary subdivision plan shall be approved as provided in chapter 39 of this title; and
    - b. A conditional use permit shall be approved as provided in section 10-7-9 of this title where the applicable zone allows such subdivisions only as a conditional use.
  2. Before final plat approval is granted for any such subdivision, a report to the City Council by the Planning Commission shall find the proposed development will:
    - a. Be in keeping with the general character of the zone in which the subdivision is to be located;
    - b. Have lot sizes that conform to the applicable zone where the subdivision is located;
    - c. Meet applicable requirements of chapter 39 of this title;
    - d. Have an approved financing plan for construction and phase completion, together with an approved security to assure compliance;
    - e. Meet applicable development standards of section 10-43-6 of this chapter and all requirements of other applicable ordinances, except where such requirements are modified by a planned development approved pursuant to chapter 23 of this title.
- C. *Disapproval.* The City Council may disapprove an application for a manufactured home park or subdivision, or recreational vehicle park or park model park if the council finds the proposed development cannot:
1. Connect to the City water system and the Ash Creek special service district wastewater disposal system;
  2. Meet one or more applicable development standards set forth in this chapter; or
  3. Provide adequate assurances that the development will be completed within two years after approval.

(Ord. 2014-10, 11-6-2014)

---

## Sec. 10-43-6. Development standards.

The development of a manufactured home park or subdivision, or recreational vehicle park or park model development shall conform to applicable standards and requirements of this section and as set forth in table 10-43-1 of this section unless modified by a planned development approved pursuant to chapter 23 of this title.

A. *General requirements.*

1. *Layout.* Land not contained within individual lots or spaces, roads or parking shall be set aside and developed as parks, playgrounds, trails, and service areas for the common use and enjoyment of occupants of the development, and the visitors thereto.
2. *Location.*
  - a. A manufactured home subdivision may be located on any lot as allowed by the zone where the lot is located;
  - b. A recreational vehicle park should generally be located:
    - (1) Adjacent to or in close proximity to a major collector or arterial road as shown in the City's transportation master plan;
    - (2) Near adequate shopping facilities; and
  - c. A park model development shall be located on property zoned MH/RV.
3. *Plan preparation.* Plans for a manufactured home park or subdivision, or recreational vehicle park or park model development shall be prepared by a licensed architect, licensed engineer and/or licensed land surveyor.

B. *Building standards.* All standards shown in table 10-43-1 of this section must be met.

C. *Site improvements.*

1. *Access to lots and spaces.* Sufficient access shall be provided to each manufactured home, or recreational vehicle lot or space or park model to allow maneuvering of homes or vehicles into position.
  - a. The accessway shall be kept free from trees and other obstructions.
  - b. Paving under a manufactured home is not required if adequate support is provided as required by applicable building codes. Use of planks, steel mats or other means to support the manufactured home during placement is allowed so long as the same are removed upon completion of placement.
2. *Common area.* Except for a manufactured home subdivision, one or more common areas equal to at least ten percent of the land area of the development shall be set aside for the joint use and enjoyment of occupants. Land covered by vehicular roadways, sidewalks and off street parking shall not be included in calculating this ten percent common area requirement; provided, however, that in initial phases of development, the minimum area shall be not less than one-half acre or ten percent of the land area under development, whichever is greater.
3. *Connection to City services.* Each manufactured home, recreational vehicle, or park model shall be connected to the City water system and the Ash Creek special service district wastewater disposal system, except as otherwise allowed by the regulations for such systems.
4. *Landscaping.* Any area not covered by a manufactured home or recreational vehicle or park model, hard surfacing, or a building shall be landscaped per an approved site plan.

- 
5. *Laundry.* A laundry may be provided for the convenience of residents within a manufactured home park or subdivision, or recreational vehicle park or park model development, but not for the general public.
  6. *Lighting.* Shall be provided to meet safety standards and the lighting guidelines in section 10-33-5 of this title and shall be shown on the site plan.
  7. *Parking.* Off street parking requirements for a manufactured home park or subdivision, or recreational vehicle park or park model development shall be provided on each lot or space as follows:
    - a. Manufactured home park or subdivision: two parking spaces per lot or space.
    - b. Recreational vehicle park: one parking space per RV space.
    - c. Park model development: one parking space per park model space.
    - d. Each parking space shall have a minimum width of nine feet and minimum depth of 18 feet.
    - e. Before a lot or space may be occupied, all off street parking areas and driveways shall be surfaced with a material acceptable to the City.
  8. *Roadway design.* Accessways within a manufactured home park or recreational vehicle park or park model park shall conform to construction design standards and specifications adopted by the City, with a minimum width of 30 feet, unless modified by a planned development approved pursuant to chapter 23 of this title.
  9. *Roads within a manufactured home subdivision.* Roads within a manufactured home subdivision shall conform to construction design standards and specifications adopted by the City unless modified by a planned development approved pursuant to chapter 23 of this title.
  10. *Skirting.* Each manufactured home shall be skirted.
  11. *Storage, waste receptacles, and additions.*
    - a. All storage and solid waste receptacles other than individual homeowner trash receptacles shall be contained within an enclosure at least six feet high.
    - b. All patios, carports, garages and other additions to a manufactured home shall be compatible in design and construction with the associated home. Such facilities shall be constructed in accordance with applicable building codes and kept in good repair.
  12. *Storm drainage facilities.* Storm drainage facilities shall be constructed to protect residents of the development as well as adjacent property owners in accordance with applicable provisions of this Code and the adopted storm drainage plan as reasonably determined by the City Engineer.
- D. *Standards specific to recreational vehicle parks and park model parks.*
1. No manufactured homes or site built dwelling units shall be permitted, except for that of the owner/manager and permanent maintenance personnel. However, a site built dwelling unit up to 900 square feet inclusive of covered porches shall be permitted on a deeded RV lot included in an RV subdivision recorded in the office of the county recorder before January 2020 that is served by an individual water meter and individual power meter and that does not result in coverage of more than 50 percent of the lot in structures.
  2. Recreational vehicle parks shall not be used as permanent residences, except for that of the owner/manager and permanent maintenance personnel. All recreational vehicles within a recreational vehicle park shall display current license plates/tags.

3. No "park model" units shall be permitted within a recreational vehicle park except a number of park models may be approved in the park to be used as short term vacation rentals. However, a park model development may be approved to allow recreational vehicles but not campers or tents in the development.
4. Each park must provide an adequate and easily identifiable office or registration area. The location of the office shall not interfere with the normal flow of traffic into and out of the recreational vehicle park.
5. Each recreational vehicle unit shall be equipped with wheels, which remain on the unit; however, the wheels may be blocked for stability.
6. No permanent room addition shall be attached to recreational vehicle nor shall any permanent structure be constructed on a recreational vehicle lot except shade structures open on three sides that conform to current NFPA standards for recreational vehicle parks and campgrounds.
7. Room additions may be permitted on park models if all setbacks are met and pursuant to the issuance of a building permit before construction.
8. A minimum of one toilet, one sink, and one hot shower, each designed for complete privacy, for each 40 spaces, or fraction thereof.
9. Conversion of an established residential park or park model development to another residential use, shall be subject to review and approval based on codes and zones in place at the time of conversion. A zone change will be required.

E. *Table 10-43-1*

TABLE 10-43-1  
DEVELOPMENT STANDARDS MANUFACTURED HOME AND RECREATIONAL VEHICLE PARKS AND  
SUBDIVISIONS

Development standard	Manufactured home subdivision	Manufactured home park	Recreational vehicle park/ park model development
General standards:			
Location	See subsection A2 of this section		
Minimum development size	10 acres	10 acres	5 acres
Ownership	Individual lots	Park must remain single parcel	Park must remain single parcel
Plan preparation	Licensed architect, licensed engineer and/or licensed land surveyor required; see subsection A3 of this section		
Required zone	MH/RV; see chapter 13 of this title		
Building standards:			
Maximum height, service building	35 ft.	35 ft.	35 ft.

Created: 2023-04-06 13:53:08 [EST]

Maximum height, accessory building	12 ft.	12 ft.	12 ft.
Lot/space standards:			
Minimum lot/space area	5,700 sq. ft.	4,000 sq. ft.	1,000 sq. ft.
Minimum lot/space width	60 ft.	50 ft.	As allowed under the NFPA 1194 standard for RV parks and campgrounds as it currently exists and as it may be amended in the future
Minimum lot/space depth	95 ft.	70 ft.	40 ft.
Setback standards:			
Front yard	15 ft.	15 ft.	5 ft.
Rear yard	10 ft.	10 ft.	5 ft.
Side yard, interior	10 ft.	8 ft.	7 ft.
Side yard, street	20 ft.	15 ft.	7 ft.
Accessory building	5 ft.; if adjacent to exterior boundary or utility easement, then 10 ft.	3 ft.; if adjacent to exterior boundary or utility easement, then 7.5 ft.	5 ft.
Separation between structures	20 ft.	20 ft.	14 ft.
Site improvements:			
Access to lots and spaces	Sufficient area to maneuver homes or vehicles required; see subsection C1 of this section		
Common area	Not required	10% of land area; see subsection C2 of this section	
Connection to City services	Must connect to City water system and Ash Creek special service district wastewater disposal		

	system; see subsection C3 of this section
Landscaping	Open and unpaved areas must be landscaped; see subsection C4 of this section
Laundry	Laundry may be provided for residents, but not general public; see subsection C5 of this section
Roadway and accessway design	See subsections C8 and C9 of this section

(Ord. 2014-10, 11-6-2014; Ord. 2019-04, 5-2-2019; Ord. 2020-05, 2-6-2020)

**Sec. 10-43-7. Maintenance of premises and common areas.**

- A. *Nuisance.* The premises on which any manufactured home or recreational vehicle or park model is located, used, or occupied shall be maintained in a clean, orderly, and sanitary condition. The accumulation of rubbish, waste, weeds, or other unsightly material thereon shall constitute a nuisance and a violation of this title.
- B. *Remedies.* In addition to the remedies set forth in section 10-9-6 of this title, the City may require removal of a manufactured home or recreational vehicle, or park model from any premises which do not conform to the requirements of this chapter.
- C. *Guarantee of performance.* The City may require the owner of a manufactured home park or subdivision, or recreational vehicle park or park model park to provide financial guarantees to assure that common areas and facilities, roadways, storage areas, service facilities and landscaping are adequately maintained.
- D. *Manager.* The owner of a manufactured home park or subdivision or recreational vehicle park or park model park shall appoint a property manager. The manager shall be:
  - 1. Locally available;
  - 2. Authorized to represent the owner with respect to all aspects of the management and maintenance of the park; and
  - 3. Authorized to receive official notices, including service of process.

(Ord. 2014-10, 11-6-2014)

**Sec. 10-43-8. Protection of common areas.**

If common areas are provided within a manufactured home park or subdivision, or recreational vehicle park or park model park adequate guarantees shall be provided to protect such common areas from future development. No certificate of occupancy shall be issued for any structure in such park or subdivision until all required guarantees have been submitted to and approved by the City. The developer of a manufactured home park or subdivision, or recreational vehicle park or park model park, may elect any of the methods set forth in subsection 10-23-10A of this title to preserve common areas.

(Ord. 2014-10, 11-6-2014)

---

### **Sec. 10-43-9. Compliance with other regulations.**

The use and occupancy of a manufactured home or recreational vehicle or park model shall comply with applicable provisions of this title and any other applicable code, including, but not limited to, building and health codes.

(Ord. 2014-10, 11-6-2014)

### **Sec. 10-43-10. Certificate of compliance.**

A manufactured home or recreational vehicle or park model used or intended to be used either immediately or in the future for human habitation, regardless of its location or its conforming or nonconforming status, shall be subject to the following rules, regulations and requirements:

- A. *Relocation of used manufactured/mobile homes.* No used mobile homes shall be moved into the City limits. No used manufactured home shall be moved into the City limits without written approval from the Building Official. Such approval shall be given upon written certification, accompanied by color photos of the manufactured home from a qualified inspector approved by the Building Official that such manufactured home meets the following criteria:
1. Must have original HUD sticker.
  2. Original HUD sticker and data plate must be permanently attached, intact and legible. All serial numbers must match.
  3. Roof must be undamaged and leakproof.
  4. Exterior walls shall be free from cracks, breaks, holes, nail pops, etc.
  5. Bottom board must be intact. There shall be no rot or deterioration of siding, and no new openings, such as added windows or doors, shall be permitted.
  6. Interior walls must be attached at base and top plate.
  7. There shall be no water damage to ceilings, floors, or interior walls.
  8. Floors under water heater and plumbing fixtures must be intact.
  9. Windows must be intact and meet HUD/ANSI code for time of manufacture.
  10. Exterior doors must be intact and close completely with no damage to door or frame.
  11. Must have underfloor insulation properly installed and complete.
  12. Plumbing system must meet all applicable federal and state codes.
  13. Gas system must pass pressure test.
  14. Electrical system must meet all applicable federal and state codes.
  15. Heating and cooling systems must meet all applicable federal and state codes.
  16. All units must be free of mold.
- B. *Relocation of used park model.* No used park model shall be moved into the City limits without written approval from the Building Official. Such approval shall be given upon written certification, accompanied by color photos of the park model, from a qualified inspector approved by the Building Official that such park model meets the following criteria:

- 
1. Roof must be undamaged and leakproof.
  2. Exterior walls shall be free from cracks, breaks, holes, nail pops, etc.
  3. Bottom board must be intact. There shall be no rot or deterioration of siding, and no new openings, such as added windows or doors, shall be permitted.
  4. Interior walls must be attached at base and top plate.
  5. There shall be no water damage to ceilings, floors, or interior walls.
  6. Floors under water heater and plumbing fixtures must be intact.
  7. Windows must be intact and meet ANSI code for time of manufacture.
  8. Exterior doors must be intact and close completely with no damage to door or frame.
  9. Must have underfloor insulation properly installed and complete.
  10. Plumbing system must meet all applicable federal and state codes.
  11. Gas system must pass pressure test.
  12. Electrical system must meet all applicable federal and state codes.
  13. Heating and cooling systems must meet all applicable federal and state codes.
  14. Unit must be free of mold.

C. *Setup permit.* All manufactured homes and park models located within the City shall be required to obtain a separate setup permit from the Building Department.

(Ord. 2014-10, 11-6-2014)

### **Sec. 10-43-11. License.**

No manufactured/mobile home or recreational vehicle park shall be operated unless and until a valid City business license has been obtained.

(Ord. 2004-15, 6-17-2004)



Application

Application Accepted Date: 05/25/2023
Type of Improvement: Zone Change Application
Description: Looking to rezone from MH/RV to RM-1 so that I can
Tenant / Project Name: 191 W 600 N
Bldg. Address: 191 W 600 N
City: Hurricane State: UT Zip: 84737
Subdivision: N/A Phase: N/A
Block: Lot #: Parcel #: H-281-A-4-A
Zone: MH/RV
Property Owner: Silo One Investments LLC
Permit Contact: A. Chris Potter P:(801) 860-5053
Email: potterrealty@gmail.com

Table with 2 columns: Description, Amount. Includes Valuation (\$0.00), PERMIT FEES, Planning Fee (\$500.00), Sub Total (\$500.00), Permit Total (\$500.00), Amount Paid (\$500.00), Remaining Due (\$0.00).



CONTACT INFORMATION

General Contractor: 191 W 600 N / A. Chris Potter
License #: P: (801) 860-5053
Address: 2537 vineyard dr
City: santa clara State: ut Zip: 84737
Email: potterrealty@gmail.com

Table with 4 columns: APPROVALS, DATE, INFO. Row: Plan Review Finalized: Yes, 05/26/23, Brienna Spencer

Electrical Contractor:
License #: P:
City: State: Zip:

Plumbing Contractor:
License #: P:
City: State: Zip:
Email:

Mechanical Contractor:
License #: P:
City: State: Zip:
Email:

Table with 5 columns: Setbacks, Front, Rear, Left, Right. Rows: Min., Actual:

APPLICATION DETAILS

# of Units: 2 custom text:

APPLICATION NUMBER: PENDZC23-13

This permit becomes null and void if work or construction authorized is not commenced within 180 days, or if construction or work is suspended or abandoned for a period of 180 days at any time after work is commenced. I here-by certify that I have read and examined this application and know the same to be true and correct. All provisions of laws and ordinances governing this type of work will be complied with whether specified herein or not the granting of a permit does not presume to give authority to violate or cancel the provisions of any other state or local law regulating construction or the performance of construction and that I make this statement under penalty of perjury. Maintenance Deposit is refundable after final inspection if: 1) site is kept clean 2) building is NOT occupied prior to final inspection and issuance of Certificate of Occupancy.

Applicant Name: A. Chris Potter

Signature of Applicant/Authorized Agent or Owner: Date:

Application Approved By: Date: 05/26/2023

Application Issued By: Date: 05/26/2023

Receipt #: 8.169132-05/26/23



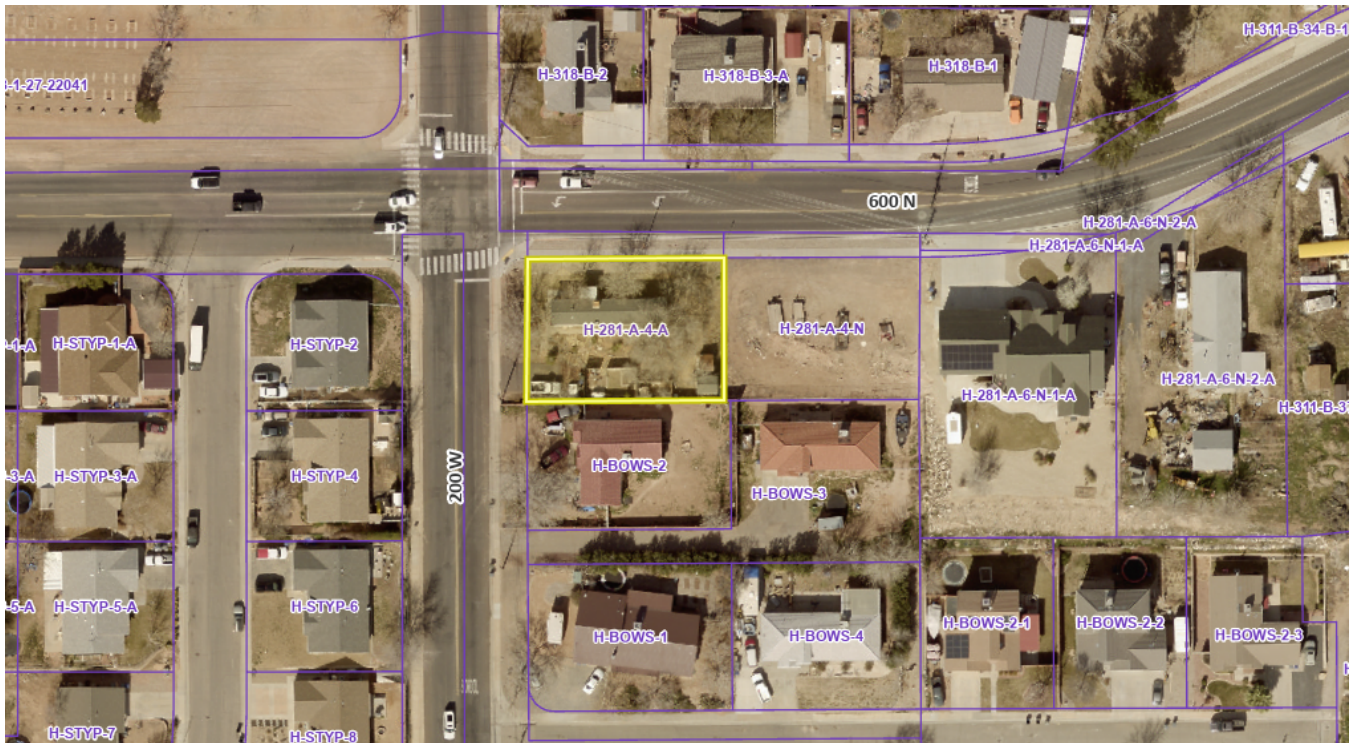
**STAFF COMMENTS**

<b>Agenda Date:</b>	<b>06/08/2023</b>
<b>Application Number:</b>	ZC 23-13
<b>Type of Application:</b>	Zone Change Application
<b>Action Type:</b>	Legislative
<b>Applicant:</b>	Silo One Investments LLC
<b>Agent:</b>	Chris Potter
<b>Request:</b>	A Zone Change from MH/RV to RM-1
<b>Location:</b>	191 W 600 N
<b>Zoning:</b>	MH/RV
<b>General Plan Map:</b>	Planned Community

**Discussion:**

The applicant is seeking a zone change from Mobile Home/Recreational Vehicle to RM-1 (multifamily) for their .22 acres. RM-1 zoning normally allows for up to 6 units an acre which would normally mean they could only build one unit on their property. However, under RM-1 zoning, “*Duplex only permitted on first 10,000 square feet. Any additional units must meet density per acre standards*”. There is currently a manufactured home on the property and the applicant would like to tear that down and put in a duplex. This property is on the corner of 200 W and 600 N. The City Council approved a similar zone change in 2021 for the property immediately to the east of this one, 161 W 600 N.

	<b>Zoning</b>	<b>Adjacent Land Use</b>
<b>North</b>	R1-10	Single Family Homes, Mesa View.
<b>East</b>	RM-1 and MH/RV	Manufactured Homes and Single Family Homes
<b>South</b>	MH/RV and RM-1	Manufactured Homes and Single Family Homes
<b>West</b>	MH/RV and R1-6	Manufactured Homes and Single Family Homes.



To change the zoning on any parcel of land within the City of Hurricane, the following questions need to be addressed:

**10-7-7: ZONING MAP AND TEXT AMENDMENTS:**

*E. Approval Standards: A decision to amend the text of this title or the zoning map is a matter within the legislative discretion of the city council as described in subsection 10-7-5A of this chapter. In making an amendment, the following factors should be considered:*

1. *Whether the proposed amendment is consistent with goals, objectives and policies of the city's general plan;*
2. *Whether the proposed amendment is harmonious with the overall character of existing development in the vicinity of the subject property;*
3. *The extent to which the proposed amendment may adversely affect adjacent property; and*
4. *The adequacy of facilities and services intended to serve the subject property, including, but not limited to, roadways, parks and recreation facilities, police and fire protection, schools, stormwater drainage systems, water supplies, and wastewater and refuse collection.*

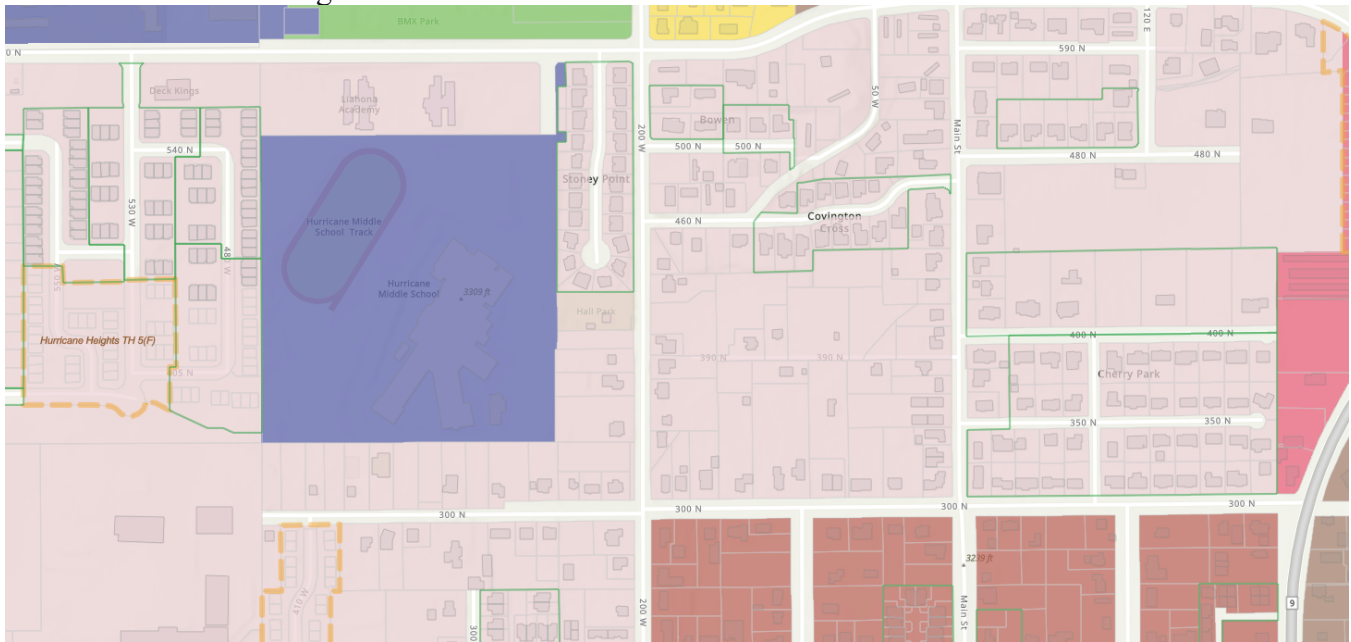
**1. Is the proposed amendment consistent with the City's General Plan's goals, objectives, and policies?**

**Response:** The General Plan Map shows this area as a Planned Community. The 2021 General Plan defines Planned Community as follows:

***Planned Community***

*Master planned communities should be complete communities that offer a mixture of housing types and supporting uses such as neighborhood and supporting commercial uses, offices, churches, schools, and parks. Development in this designation should take into account the character of existing surrounding development.*

The RM-1 zoning complies with the General Plan Map. Existing development in the area consists of a variety of housing types and other uses. In addition, this area is near schools, churches, parks and about three blocks from the edge of the Hurricane downtown.



This large area immediately north of downtown was mapped as Planned Community to encourage smaller, more dense neighborhoods. There are several townhome developments in the area, as well as older houses and several schools.

The Hurricane City General Plan recommends that the City consider adding different housing types in appropriate areas:

*In recent years, the City has recognized the need to approve zoning changes and project plans that provide a wider variety of housing options for Hurricane residents. A number of new multi-family housing units have been approved in recent years and are in various stages of construction and occupation. These multi-family homes are a mixture of rental apartments, rental and owner-occupied townhomes, and duplex housing.*

The 2022 Moderate Income Housing Plan recommends that the City, “Look at areas where “Missing Middle Housing” and mixed-use zones can be implemented within existing and future residential and commercial developments”

**2. Is the proposed amendment harmonious with the overall character of existing development in the subject property’s vicinity?**

**Response:** The area is split between single family zoning and manufactured home zoning with some multifamily zoning. One of the reasons this area is zoned MH/RV was to allow the existing manufactured homes to be able to stay as a conforming use, but over time many of the existing manufactured homes and parcels have been replaced with single family homes. While a single duplex would be out of character with the existing development, staff does not believe that a single duplex will have much of an effect on the overall character of the neighborhood. The applicant has indicated they would like to develop in some sort of cooperation with the property next door which has already been rezoned for a duplex which will help with the harmoniousness.

Despite being zoned for manufactured homes, the majority of the houses in the area are single family homes. This is because MH/RV zoning allows for single family homes.

**3. Will the proposed amendment affect the adjacent property?**

**Response:** The proposed zoning will allow the addition of one new residential unit to the property. Staff does not believe that the added residents will affect adjacent properties assuming the property is developed smartly.

**4. Are public facilities and services adequate to serve the subject property?**

**Response:** Facilities and services are adequate for this development. JUC had no major concerns about availability.

**Other Considerations**

**Missing Middle Housing:**



Duplexes are part of the missing middle housing. Over the past 60 years, most housing growth has been either single family homes in suburban areas or large apartment complexes in urban centers. “Middle”

types of housing such as duplexes, townhomes, mixed-use buildings etc. have become rare in recent years. However, with ever increasing housing prices, these housing types are increasingly popular as starter homes for young families. These housing types are also considered a good way to increase neighborhood density, especially in more walkable parts of town such as near downtown, without the same level of impact on a neighborhood that a mid-rise apartment complex would have. The Moderate Income Housing Plan passed last year encourages the City to find places for missing middle housing.

**Site Layout:**

The property is situated on the corner of 200 W and 600 N with its existing driveway being on 200 W. Per the access management policy a driveway on 600 N will not be permitted and all access will come off 200 W.

**JUC Comments:**

JUC Comments are due June 7th, 2023 this report will be updated at this time

**Power:**

**Sewer:**

**Streets:**

**Public Works:**

**Water:**

**Engineering:**

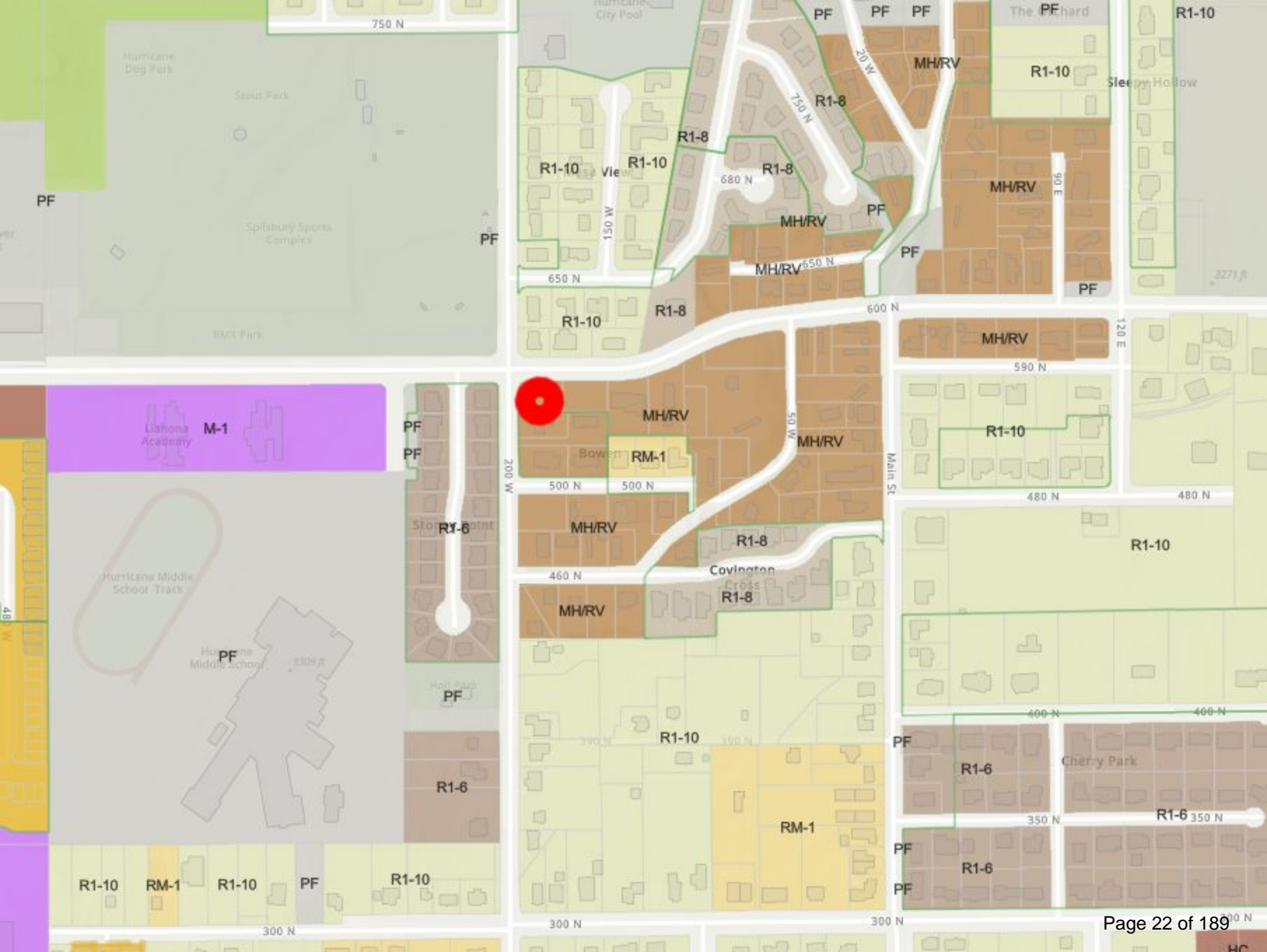
**Fire:**

**Findings:**

Staff makes the following findings:

1. The application meets the standards within the General Plan and General Plan Map.
2. Most of the surrounding development is single family homes. While a duplex would be out of character for the neighborhood staff believes a single well designed duplex would not be considered a huge change for the neighborhood
3. The proposed amendment will have an impact on the area, but the limited number of units would mitigate the impact.
4. Services are adequate to serve the area and proposed project.

**Recommendation:** Staff recommends the Planning Commission review this application and the zone change based on standards and consider residents' comments. Staff would recommend approval of the proposed zone change.



<b>Agenda Date:</b>	<b>06/08/2023</b>
<b>Application Number:</b>	
<b>Type of Application:</b>	City Master Plan
<b>Action Type:</b>	Legislative
<b>Applicant:</b>	Hurricane City Planning
<b>Agent:</b>	Ben Levenger w/ Downtown Redevelopment
<b>Request:</b>	Review and make a recommendation on an updated Downtown Master Plan

**Discussion:** Attached is the Downtown Master Plan that Planning staff is presenting for recommendation to the City Council. This draft was sent out to the Planning Commission last week for review. This newly updated draft reflects comments from UDOT and the City Engineering Departments.

Staff has presented the plan to the JUC and is expecting comments back on June 7th. Any major comments will be forwarded on to the Planning Commission and our consultant for inclusion in the plan.

There are a few major points to the plan staff would like to emphasize to the Planning Commission with regards to the recommendations, these are issues that will most likely be brought up within the public hearing

**Closing of 100 W to Vehicular Traffic:**

The 2021 General Plan states: *The City supports the transition of 100 West (from State Street to 100 South) from a vehicular road to a plaza.* The closing of 100 W from State St to 100 S has been done periodically for decades and permanently closing it has been part of City plans since the late 2000s. On May 18th, 2023 the City Council voted to temporarily close 100 W from Memorial Day through Labor Day to try it for the summer months. The existing gate on the north end was used, which provides ample turnaround space if someone accidentally turns down 100 W from State St. On the south end barriers were installed that directs traffic through the Fine Arts Center parking lot and out that parking lot to 100 S.



This provides, unlike the current system, a legal drop off location for children near the community center.

This idea saw wide support during the public outreach portion of the plan, will increase safety and provide constant space for programming in this area. The steering committee discussed other traffic calming options on 100 W such as turning it into a one-way street or adding measures such as medians but decided against it due to the effect it could have on Peach Days and the Farmer's Market.

The Downtown Master Plan proposes the City convert parallel parking along 100 S to angled parking to increase the number of available parking spaces, as well as possibly acquire more property for municipal parking lots. Except during major events the city provided parking in this area is rarely full. Staff will study the closure of 100 W's effect on parking throughout the summer.

### **Catalyst Project:**

Many cities when in the process of revitalizing their historic downtown areas will invest in some sort of "catalyst project" as a way to signal to private property owners within the downtown that the City is investing in making the downtown a place worth investing in. Many cities will invest in facade programs or municipal parking lots but where Hurricane City owns a significant piece of prime downtown real estate a larger scale project is feasible.

The commercial building on the southwest corner of 100 W and State St, most recently home to the Mexican grocery store, Su Lugar Stop And Go Market, as well the house next door, are currently in process of being torn down by the City. These buildings are in bad shape and it was not seen as feasible to renovate them. The City owns almost all of the frontage along State St on this block giving the City the opportunity to provide the catalyst for revitalization in the downtown.





Figure 62: Northwest Corner Render



Figure 63: Northeast Corner Render

The project will be a 2-3 story building, with a three story parking garage in the back (invisible to the street). There will be retail and storefronts on the bottom floor along State St and offices or residences on the upper floors.

Under this scenario the City would work with neighboring property owners to relocate the existing car lot to another location in town and the existing LDS Family History Center within the structure. The City would partner with a private developer either through a long term lease or other mechanism to develop and operate the site.

### **Downtown Overlay Zone:**

The Downtown Master Plan recommends a downtown mixed use overlay zone which would be a whole new zoning and change to the zone map. The exact details of this new zoning would have to be worked out in a separate ordinance passed by the Planning Commission and City Council however some important aspects to a downtown overlay zone would be *for new development in this area*:

- Form Based Code: regulations would focus more on the look and feel of the buildings (architectural styles, heights, building massing) rather than the uses within the buildings.
- Use Table: requiring that retail or service based industries be located on the ground levels of buildings allow for a mix of residential and commercial on second and third stories.

- Building massing: Using Floor Area Ratios, and setback requirements allowing for new buildings to be close to the street similarly to existing buildings in the downtown.
- Short-term rentals: Limiting the number and overall form of short-term rentals in the downtown area.
- Allowing for current downtown buildings to be rebuilt in largely the same footprint and spacing as currently exists

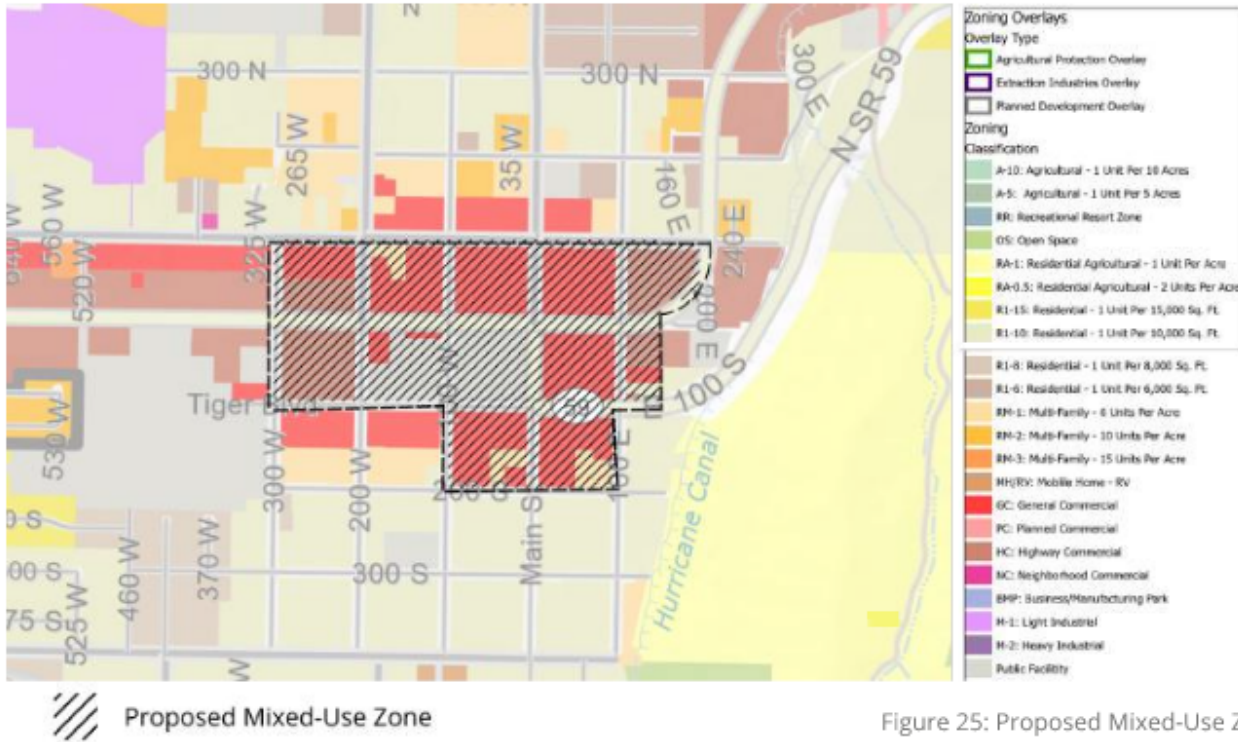


Figure 25: Proposed Mixed-Use Zone

**Recommendation:**

Staff recommends the Planning Commission read through and consider the plan again and listen to the public comments. If the Planning Commission has no major concerns staff would recommend sending this on to the City Council.



# HURRICANE DOWNTOWN

## MASTER PLAN



**Downtown Redevelopment Services**  
100 E Main St, Suite 100  
Ravenna, OH 44266



**CRSA**  
175 S Main St, Suite 300  
Salt Lake City, Utah 84101

# TABLE OF CONTENTS



## **Existing Conditions 01**

Introduction	02
Demographic Analysis	03
Consumer Expenditures	09
Built Environment	12
Transportation	26



## **Market Analysis 29**

Market Trade Areas	31
Missing Services and Amenities	31
Oversaturated Services	38



## **Public Input Synopsis 39**



## **Draft Recommendations 42**

Land Use Recommendations	44
Built Environment Recommendations	47
Urban Design Recommendations	54
Transportation Recommendations	65
Parks and Civic Space Recommendations	73
Green Infrastructure Recommendations	77
Housing Recommendations	80
Targeted Development Scenarios	86
Economic Development Recommendations	88



## **Implementation Strategies 94**

Years 0-2	95
Years 3 - 7	100
Years 8-15	102

01



# EXISTING CONDITIONS

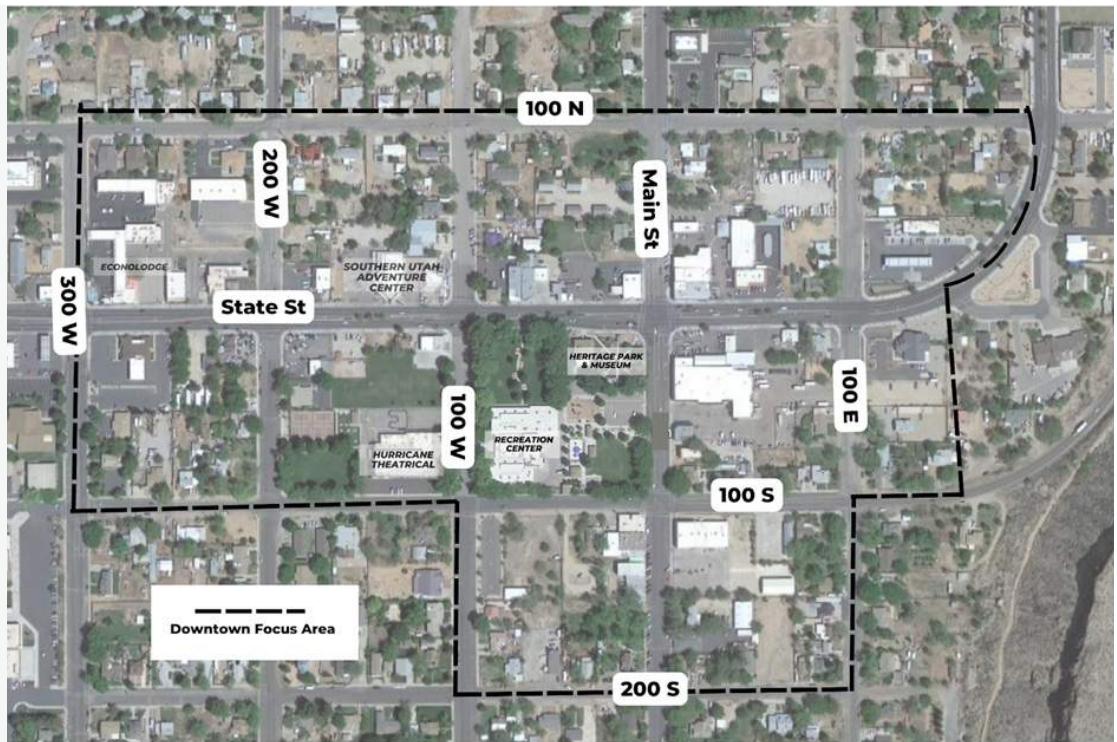




## Introduction

Hurricane City, Utah, pronounced “Her-ah-kun” by locals, is located on the eastern edge of the St. George metropolitan service area which covers south-central Washington County.

The city is a rapidly growing gateway community for the popular Zion National Park, located about 25 miles to the east on UT-9.



In addition to the National Park, other notable tourist destinations and geographic features include Sand Hollow State Park, Quail Creek State Park, and the Virgin River, public lands, trail systems and golf courses, giving Hurricane national exposure as a desirable place to live.

Interstate 15 forms the northwest border of the municipality while 300 W and 100 E along the State Street corridor are the main bounding features of downtown.

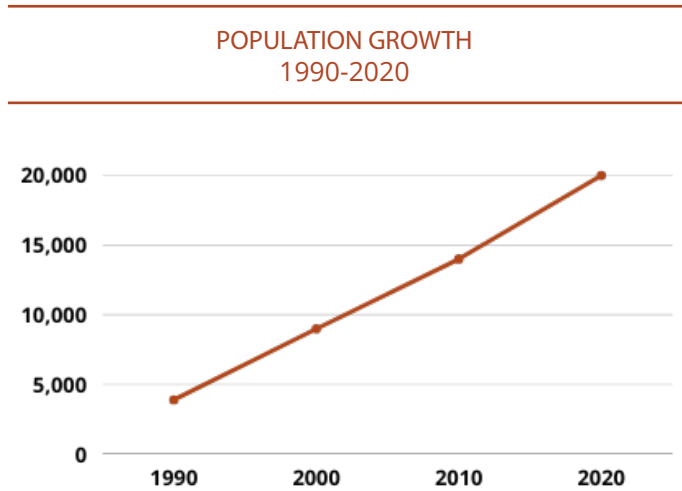




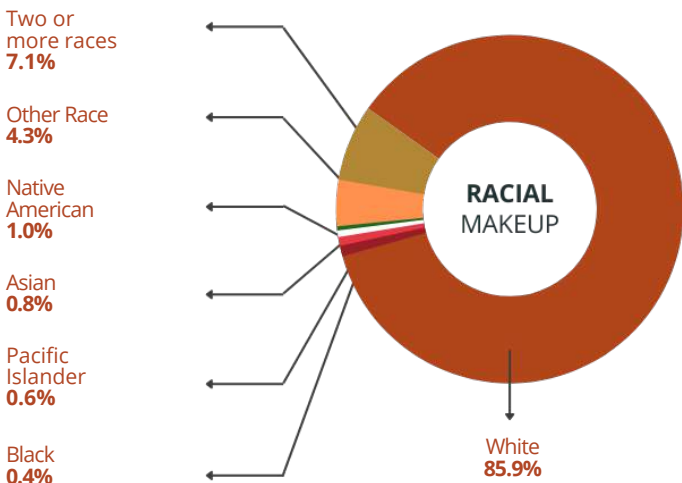
# Demographic Analysis

## Population

Hurricane’s population has grown significantly over the past four census counts. Climbing from 3,915 in 1990 to 20,036 in 2020, the population has grown by more than five-fold.



The city’s racial makeup is 85.9% White, 1.0% Native American, 0.8% Asian, 0.6% Pacific Islander, and 0.4% African American or Black. Of the remainder, 4.3% are of another race and 7.1% are of two or more races. The population is 85.7% non-Hispanic and 10.2% Hispanic, growing from 7.2% Hispanic in 2010 due to significant in-migration in recent years.



Moreover, the in-migration has resulted in the community also boasting a high multilingual percentage, specifically 9.6% who speak a non-English language at home, predominantly Spanish.



HURRICANE CITY RACIAL MAKEUP	
2020 DECENNIAL CENSUS	
White	85.9%
American Indian and Alaska Native	1.0%
Asian	0.8%
Native Hawaiian and other Pacific Islander	0.6%
African American or Black	0.4%
Other Race	4.3%
Two or More Races	7.1%

HURRICANE CITY ETHNICITY MAKEUP	
ACS 2021 5-YEAR ESTIMATE	
Non-Hispanic	85.7%
Hispanic	10.2%

Figure 1: Racial and Ethnicity Makeup



Approximately 52.3% of residents are male, and 47.7% are female, a near-even split consistent with most communities.

Female  
47.7%



Male  
52.3%

The median age is 38.4 years, significantly higher than the state median; however, the city's median is on par with that of the county and nation.

RESIDENT AGE DISTRIBUTION		
2021 ACS 5-YEAR ESTIMATE		
Age Group	# of Individuals	Percentage
Under 5 years	1,373	7.0%
5 to 9 years	1,183	6.1%
10 to 14 years	1,301	6.7%
15 to 19 years	1,286	6.6%
20 to 24 years	1,207	6.2%
25 to 29 years	812	4.2%
30 to 34 years	1,714	8.8%
35 to 39 years	1,160	5.9%
40 to 44 years	1,072	5.5%
45 to 49 years	1,095	5.6%
50 to 54 years	851	4.4%
55 to 59 years	1,273	6.5%
60 to 64 years	1,595	8.2%
65 to 69 years	1,035	5.3%
70 to 74 years	1,021	5.2%
75 to 79 years	759	3.9%
80 to 84 years	480	2.5%
85 years & over	284	1.5%

Figure 2: Resident Age Distribution

Due to the relocation of an aging population to the southern portion of Utah, the city has witnessed a rise in median age, climbing from 31.2 in 2010 to 38.4 years in 2021. A significant 33.1% of Hurricane City's population is 55 years old or older.

Hurricane's median age and trends are sustainable and are characteristic of a family-oriented community. Moreover, the city's age distribution shows promise of sustained population growth with a large percentage of minors and young adults. As those above 55 years continue to age, there will be a need for supportive services such as increased healthcare, assisted living, and disability access.

High school graduation, including equivalency, among Hurricane residents aged 25 years and over, has been trending upward over the past decade, from 91.4% in 2011 to 94.1% in 2021. The percentage of residents with bachelor's degrees or higher is also trending upward, from 20.7% in 2011 to 22.3% in 2021. The data in Figure 3 below outlines percentages and total assumed calculations of educational attainment by typology.

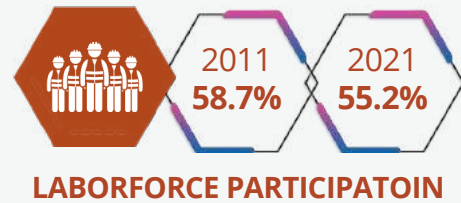
HURRICANE CITY EDUCATIONAL ATTAINMENT		
2021 ACS 5-YEAR ESTIMATE		
Less Than High School	772	5.9%
High School Graduate (Includes Equivalency)	3,498	26.6%
Some College	5,942	45.2%
Bachelor's Degree	2,057	15.6%
Master's Degree	507	507
Professional School Degree	261	2.0%
Doctorate Degree	114	0.9%
High School Graduate or More (Includes Equivalency)	12,379	94.1%
Bachelor's Degree or More	2,939	22.3%

Figure 3: Educational Attainment



## Employment

Among residents aged 16 years and over, 8,380 residents are in the civilian labor force and experience an unemployment rate of 1.5%. The unemployment rate is down from 3.7% in 2011 due to the national rebound from the Great Recession. Hurricane’s labor force participation has decreased slightly from 58.7% to 55.2% from 2011 to 2021.



For city residents, the following NAICS sectors are ranked from most common to least common for industry employers. The median earnings in Utah for the respective industry for the past 12 months are listed in the right column, including part-time and full-time employees. This data does not include individuals who work inside the city and live elsewhere, but rather only those living in the city. The three most common sectors in Hurricane are retail trade (15.13%); accommodation and food services (13.34%); and health care and social assistance (12.91%).

EMPLOYMENT BY INDUSTRY			
Industry	# of employees	%age	Utah Industry Median Earnings
Retail trade	1,249	15.13%	\$27,113.00
Accommodation and food services	1,101	13.34%	\$15,301.00
Health care and social assistance	1,066	12.91%	\$35,430.00
Transportation and warehousing	800	9.69%	\$43,970.00
Educational services	774	9.38%	\$34,301.00
Construction	748	9.06%	\$45,124.00

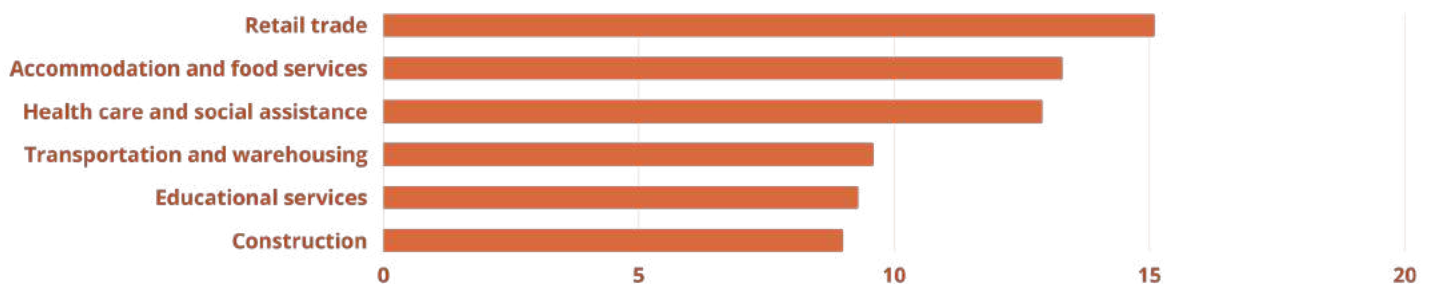


Figure 4: Employment by Industry (Percentages)



The figures below represent the number of Hurricane residents employed in each respective occupation. Utah median earnings are provided for each NAICS category on the furthestmost right column. These figures are a cumulative average of earning over 12 months, including part-time and full-time employees.

EMPLOYMENT BY OCCUPATION			
Occupation	# of employees	%age	Utah Occupational Median Earnings
Management occupations	1,023	12.39%	\$71,842.00
Sales and related occupations	985	11.93%	\$34,639.00
Office and administrative support occupations	948	11.48%	\$31,211.00
Building and grounds cleaning and maintenance occupations	579	7.01%	\$20,719.00
Food preparation and serving related occupation	545	6.60%	\$13,100.00
Production occupations	491	5.95%	\$35,869.00

The three most common occupations in Hurricane are management occupations (12.39%); sales and related occupations (11.93%); and office and administrative support occupations (11.48%).

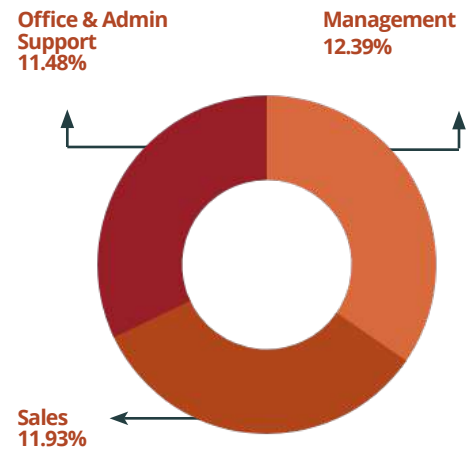


Figure 5: Employment by Occupation

## Commuting and Transportation Habits

For workers 16 years and over, 2021 commuting patterns were heavily auto dependent with a total of 85.5% of Hurricane residents commuting by driving, of which 76.2% drove alone and 9.3% carpool. A significant 10.4% of residents worked from home, while just 3.6% walked to work. Residents' average commute to work is 22.5 minutes, comparable to state and national averages but slightly higher than the county average. The data below shows the breakdown of commute times among Hurricane residents.

COMMUTE TIME BREAKDOWN			
Commute Time	%age	Commute Time	%age
Less than 10 minutes	21.6%	30 to 34 minutes	14.0%
10 to 14 minutes	12.3%	35 to 44 minutes	5.5%
15 to 19 minutes	7.1%	45 to 59 minutes	5.2%
20 to 24 minutes	17.7%	60 or more minutes	3.4%
25 to 29 minutes	13.2%		

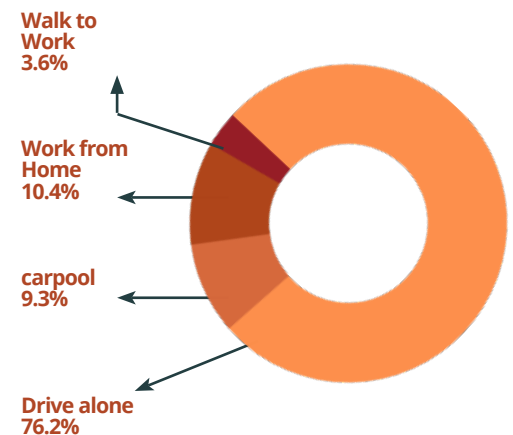


Figure 6: Commute Time Breakdown



**Hurricane City is served by road and air infrastructure. The city is landlocked and lacks navigable waterways.**

The city is served by Interstate 15 on its northwestern border, connected primarily via Utah State Route 9 at exit 16. S.R. 9 also connects with S.R. 17 in La Verkin before going on to connect with Interstate 15 north of Toquerville at exit 27. The interstate is the primary connection to nearby communities including St. George, to Salt Lake City 290 miles to the north, and Las Vegas, Nevada 138 miles to the south of Hurricane



There are three state routes that run through Hurricane: State Routes 9, 59, and 7. Utah State Route 9 begins in Hurricane, at Harrisburg Junction. It travels east into Downtown Hurricane, forming State Street, before turning north towards La Verkin. Additional connections along this road include Zion National Park and S.R. 89. Second, S.R. 59 extends outward from the intersection of S.R. 9 and Main Street. Lastly, S.R. 7 (i.e., Southern Parkway) originates due south of Downtown Hurricane, connecting the city to Washington and South St. George.

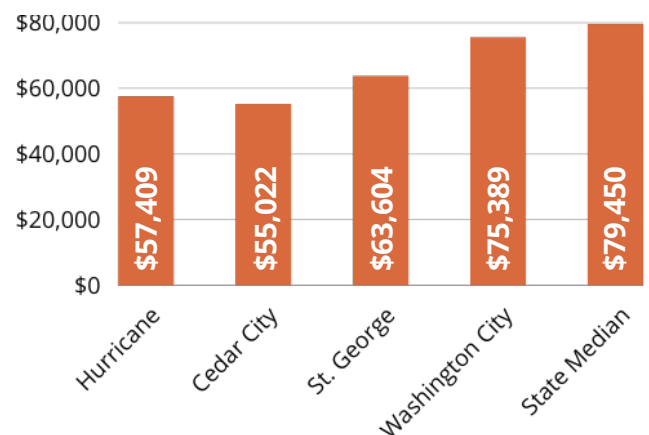


Hurricane Municipal Airport, situated in the south Hurricane, is the only public airport. The municipal airport provides very limited service, with just one runway, no taxiway, and service for only small personal aircraft. Nearby, the St. George Regional Airport provides limited commercial service to hubs such as Salt Lake City, Denver, Phoenix, Los Angeles, and Dallas-Fort Worth. Approximately 130 miles away, the recently renamed Harry Reid International Airport is the closest international airport. There is currently no intra-city public transportation, but shuttle service does exist with Salt Lake City, Las Vegas, and Southern California by way of St. George.

**Income and Poverty**

The 2021 median household income in Hurricane is \$57,409, which is comparable to Cedar City's \$55,022 and St. George's \$63,604; however, it is much lower than Washington City's \$75,389. It is also lower than the state median household income of \$79,450. Hurricane's median household income has increased slightly from \$51,146 in 2011.

**Median Household Income**





### The per capita income adjusted for inflation

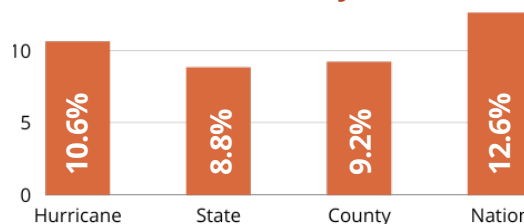
has also increased slightly from \$21,133 in 2011 to \$27,086 in 2021. The median income is \$70,513 for married-couple families and \$28,465 for non-family households.



PER CAPITA INCOME

The poverty rate in Hurricane of 10.6% is higher than the state’s 8.8% and the county’s 9.2% but lower than the nation’s 12.6%. While the City’s poverty rate is much lower than the nation, the 10.6% of residents living in poverty demonstrates a mixture of various socio-economic conditions within the community.

### Hurricane Poverty Rate



## Housing

Hurricane’s housing stock consists of 7,791 units (ACS 2021 5-Year Estimate) compared to 5,252 (2010), a 48.3% increase. The city’s housing supply is primarily occupied (87.5%,) with only 12.5% (974) of the units being vacant. Of the 6,142 occupied units, 4,616 (75.15%) are owner-occupied and 2,201 (35.84%) are renter occupied.



Housing Stock



The Zillow-reported median rent is \$2,399, up from \$355 over last year, resulting in an estimated 54.5% of Hurricane renters being considered cost-burdened, classified as more than 30% of household income in the past 12 months.



Median Rent

The median value of an owner-occupied house is \$486,000, up from \$199,600 in 2011. The selected monthly owner costs (SMOC) for owners with a mortgage are \$1,433, resulting in 27.4% of Hurricane households with a mortgage being cost-burdened. For owners without a mortgage, the SMOC is \$390, resulting in 6.2% of Hurricane households without a mortgage being cost-burdened. The average household size is 2.7 persons, which is on par with the county, state, and national averages.



Median Value



The data provided in this section highlights the estimated consumer expenditures for 2021 within Hurricane City.

(Source: EASI)

TOTAL CONSUMER EXPENDITURES					
Category	City Total	Avg. per household Expenditure	% of Total Spending	UT %	UT %
Food	\$54,753,00	\$8,914	13.0%	12.8%	12.7%
Housing	\$135,889,000	\$22,124	32.4%	32.0%	32.4%
Apparel and services	\$12,031,000	\$1,958	2.9%	2.9%	3.0%
Transportation	\$72,009,000	\$11,724	17.1%	17.0%	16.8%
Healthcare	\$35,603,000	\$5,796	8.5%	7.9%	7.9%
Entertainment	\$21,136,000	\$3,441	5.0%	5.1%	4.9%
Personal care products and services	\$5,076,000	\$826	1.2%	1.2%	1.2%
Reading	\$629,000	\$102	0.1%	0.1%	0.1%
Education	\$9,261,000	\$1,507	2.2%	2.5%	2.5%
Tobacco products and smoking supplies	\$2,097,000	\$341	0.5%	0.5%	0.5%
Cash contributions	\$13,497,000	\$2,197	3.2%	3.2%	3.2%
Personal insurance and pensions	\$48,385,000	\$7,877	11.5%	12.4%	12.3%
<b>Total Annual Expenditures</b>	<b>\$419,948,000</b>	<b>\$68,373</b>			

Consumer expenditure distribution within Hurricane is comparable to that of the state of Utah and the nation. The two most significant differences between Hurricane and the state / national percentages are for healthcare and personal insurance and pensions. Healthcare makes up 0.6% more of consumer expenditures in the city than the rest of the state or nation, and personal insurance and pensions make up 0.9% and 0.8% less of consumer expenditures in the city than the rest of the state and nation respectively.

Figure 7: Total Annual Consumer Expenditures

Food expenditure distribution within Hurricane is comparable to that of the state of Utah and the nation. Notably, food expenditures in the city are skewed more towards food at home when compared to the state and nation. In contrast, expenditures on food away from home are proportionately lower for the city than the state and nation. All other expenditures listed in the table below are comparable between the city, state, and nation.

CONSUMER EXPENDITURES ON FOOD				
	Total	% Of Total	UT %	US %
Food at home	\$31,207,000	57.00%	55.8%	55.9%
Food away from home	\$23,544,000	43.00%	44.2%	44.2%
Cereals and bakery products	\$3,957,000	7.23%	7.1%	7.0%
Dairy products	\$3,136,000	5.73%	5.6%	5.4%
Fruits and vegetables	\$5,917,000	10.81%	10.6%	10.7%
Nonalcoholic beverages	\$3,013,000	5.50%	5.3%	5.3%
Reading	\$629,000	0.1%	0.1%	0.1%
Education	\$9,261,000	2.2%	2.5%	2.5%
Food prepared by the consumer unit on out-of-town trips	\$435,000	0.79%	0.8%	0.8%
Food on out-of-town trips	\$2,530,000	4.62%	4.9%	4.8%
Alcoholic beverages	\$3,959,000	7.23%	7.3%	7.0%
<b>Total food expenditures</b>	<b>\$54,753,000</b>			

Figure 8: Consumer Expenditures on Food



Household and housing expenditure distribution within Hurricane is comparable to that of the state of Utah and the nation.

CONSUMER EXPENDITURES ON HOUSING				
	Total	% Of Total	UT %	US %
Household operations	\$10,915,000	8.0%	8.2%	7.8%
Housekeeping services	\$943,000	0.7%	0.7%	0.7%
Household furnishings and equipment	\$14,053,000	10.3%	10.5%	10.1%
Household textiles	\$873,000	0.6%	0.6%	0.6%
Furniture	\$3,437,000	2.5%	2.6%	2.5%
Floor coverings	\$172,000	0.1%	0.1%	0.1%
Major appliances	\$2,199,000	1.6%	1.6%	1.5%
Small appliances	\$300,000	0.2%	0.2%	0.2%
Miscellaneous household equipment	\$6,605,000	4.9%	4.9%	4.7%
<b>Total housing expenditures</b>	<b>\$135,889,000</b>			

Figure 9: Consumer Expenditures on Housing

Apparel expenditure distribution within Hurricane is comparable to that of the state of Utah and the nation.

CONSUMER EXPENDITURES ON APPAREL				
	Total	% Of Total	UT %	US %
Men and boys	\$2,921,000	24.3%	24.6%	23.9%
Men, 16 and over	\$2,241,000	18.6%	18.9%	18.4%
Boys, 2 to 15	\$714,000	5.9%	6.1%	5.8%
Women and girls	\$4,508,000	37.5%	37.2%	37.3%
Women 16 and over	\$3,821,000	31.8%	31.6%	31.7%
Girls, 2 to 15	\$703,000	5.8%	5.9%	5.8%
Children under 2	\$561,000	4.7%	4.7%	4.1%
Footwear	\$2,523,000	21.0%	20.8%	22.0%
Other apparel products and services	\$1,501,000	12.5%	12.8%	12.7%
<b>Total apparel products and services expenditures</b>	<b>\$12,031,000</b>			

Figure 10: Consumer Expenditures on Apparel

Transportation expenditure distribution within Hurricane is comparable to that of the state of Utah and the nation. The most significant differences are for vehicle purchases (net outlay) and public and other transportation. While 40.6% of the nation's transportation expenditures are on vehicle purchases (net outlay), a slightly higher 41.4% of Hurricane's transportation expenditures are in this category. This may demonstrate a preference for larger or custom (e.g., crossovers, trucks). On the contrary, public and other transportation makes up 1.2% less of consumer expenditures in Hurricane than the nation. The disparity is likely due to the lack of intra-city public transit.

CONSUMER EXPENDITURES ON TRANSPORTATION				
	Total	% Of Total	UT %	US %
Vehicle purchases (net outlay)	\$29,840,000	41.4%	41.5%	40.6%
Gasoline and motor oil	\$14,000,000	19.4%	19.1%	19.1%
Other vehicle expenses	\$22,896,000	31.8%	31.6%	32.1%
Public and other transportation	\$5,143,000	7.1%	7.8%	8.3%
<b>Total transportation expenditures</b>	<b>\$72,009,000</b>			

Figure 11: Consumer Expenditures on Transportation

Healthcare expenditure distribution within Hurricane is comparable to that of the state of Utah and the nation. With the influx of older residents, it is likely that healthcare expenditures will rise in the coming years.

CONSUMER EXPENDITURES ON HEALTHCARE				
	Total	% Of Total	UT %	US %
Health insurance	\$23,971,000	67.3%	67.1%	68.1%
Medical services	\$6,950,000	19.5%	20.2%	19.3%
Drugs	\$3,354,000	9.4%	9.0%	9.0%
Medical supplies	\$1,333,000	3.7%	3.7%	3.6%
<b>Total healthcare expenditures</b>	<b>\$35,603,000</b>			

Figure 12: Consumer Expenditures on Healthcare



Entertainment expenditure distribution within Hurricane is comparable to that of the state of Utah and the nation. Yet, it varies the most from state and national distributions among the various consumer expenditure categories.

CONSUMER EXPENDITURES ON ENTERTAINMENT (Thousands)				
	Total	% Of Total	UT %	US %
Fees and admissions	\$6,083,000	28.8%	31.0%	31.1%
Audio and visual equipment and services	\$6,474,000	30.6%	29.0%	30.6%
Pets, toys, hobbies, and playground equipment	\$5,909,000	28.0%	26.8%	25.6%
Other entertainment supplies, equipment, and services	\$2,690,000	12.7%	13.1%	12.9%
<b>Total entertainment expenditures</b>	<b>\$21,136,000</b>			

Figure 13: Consumer Expenditures on Entertainment (Thousands)

Retail sales by sector varies much more greatly than the tables above. This can be attributed to the lack of, or limited, retail offerings in Hurricane, resulting in expenditures on these categories outside of the municipality. While numerous amenities and service based outlets are present in the municipality, there are likely missing amenities and/or services resulting in missed tax revenue. causing expenditures on necessity purchases outside of the municipal boundaries.

While numerous amenities and service-based outlets are present in the municipality, there are likely missing services and amenities community wide that are creating a lack of expenditures, due to the small market size within the city limits. However, these retail sectors may or may not be met through the larger trade areas throughout the region. If further detail is desired, a comprehensive market analysis must be completed.

Two retail sectors are underserved within the city limits – electrical and appliances stores and sporting goods stores. These specific sectors are likely met within the regional context, and may be provided locally as the City continues to grow and diversify. Non-served sectors aside, several sectors receive a markedly different share of retail sales in Hurricane than statewide or nationwide. Sectors with notable differences include motor vehicle store sales; gasoline stations store sales; general merchandise store sales; non-store purchases sales; and food services.

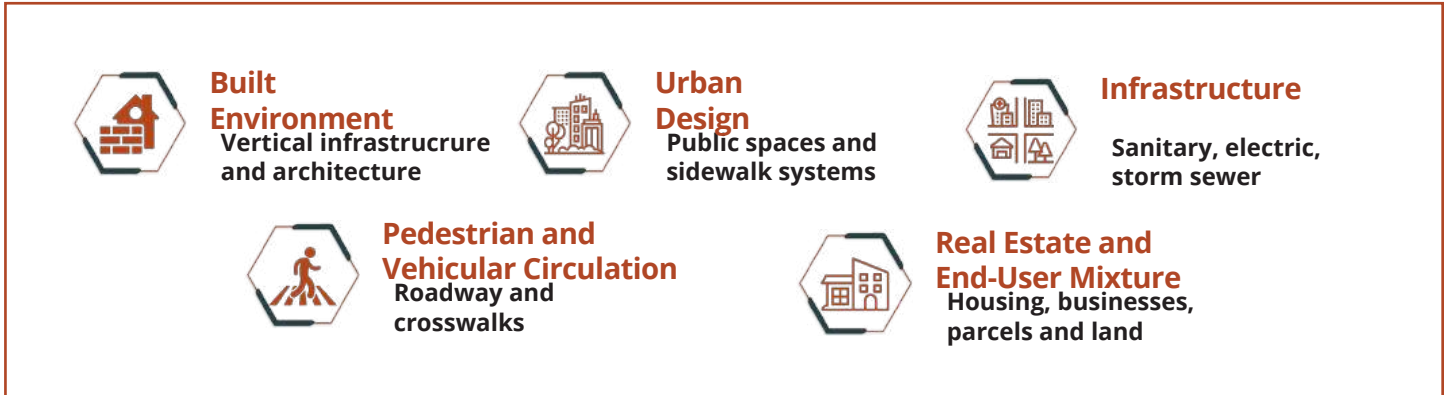
RETAIL SALES BY SECTOR				
	Total	% Of Total	UT %	US %
Motor Vehicles Store Sales	\$119,137,000	38.2%	21.5%	21.3%
Furniture and Home Furnishings Store Sales	\$1,800,000	0.6%	2.7%	1.8%
Electrical and Appliances Store Sale	\$0	0.0%	1.0%	1.2%
Building Materials and Garden Store Sales	\$11,425,000	3.7%	6.6%	6.6%
Food and Beverage Store Sales	\$41,603,000	13.3%	8.9%	13.4%
Health and Personal Care Store Sales	\$9,901,000	3.2%	2.7%	5.3%
Gasoline Stations Store Sales	\$41,137,000	13.2%	6.8%	6.5%
Clothing and Accessories Store Sales	\$195,000	0.1%	2.6%	3.1%
Sporting Goods Store Sales	\$0	0.0%	1.9%	1.4%
General Merchandise Store Sales	\$52,595,000	16.9%	13.2%	11.5%
Miscellaneous Store Sales	\$1,320,000	0.4%	2.0%	2.1%
Nonstore Purchases Sales	\$19,979,000	6.4%	22.8%	16.4%
Food Services	\$12,823,000	4.1%	7.4%	9.5%
<b>Total Retail Sales (including Food Services)</b>	<b>\$311,913,000</b>			

Figure 14: Retail Sales by Sector



## Built Environment

A detailed baseline analysis of existing conditions is a vital precursor to formulating downtown strategies and recommendations. Outlined in this section are the results of a thorough assessment, highlighting data retrieved from open-source databases and visual inspections of the downtown area depicted in the graphic above. Below are the specific finding classifications that were reviewed:



For the existing conditions analysis, Downtown Hurricane has been divided into minor study areas (i.e., groupings of blocks) to help improve the nuance of the findings.

These study areas, outlined in Figure 15 on the right, were each studied independently, allowing for a detailed analysis of the current conditions and deepened understanding of how they will impact the overall future goals of the downtown.

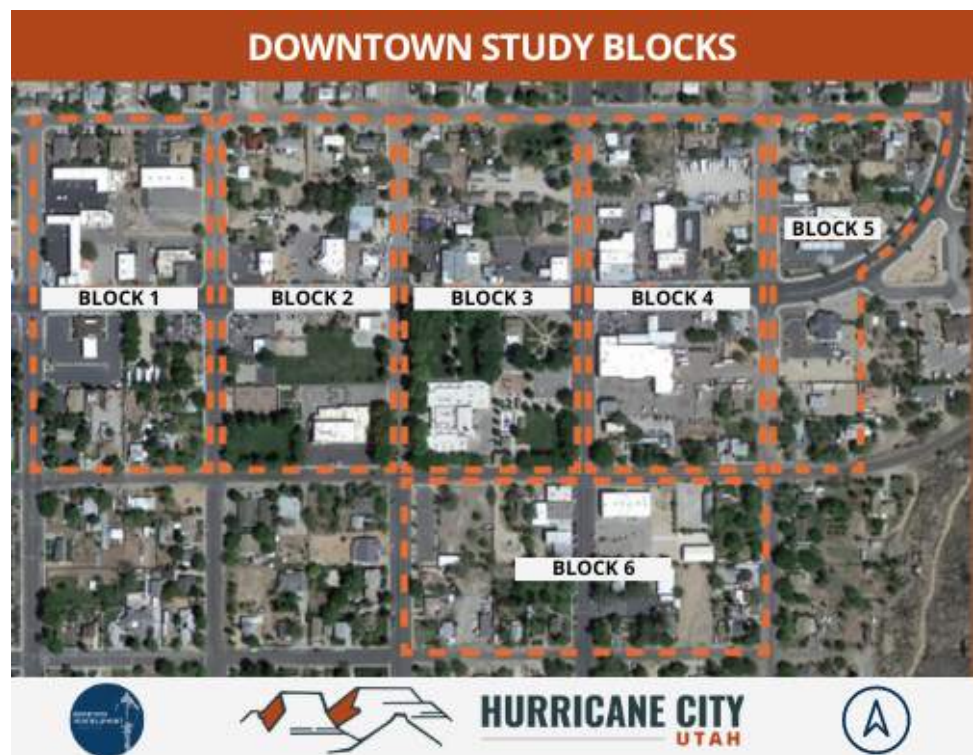


Figure 15 - Downtown Study Area

These existing conditions are the first informational chapter of the overall downtown master plan, focusing on outlining the current conditions witnessed within the downtown corridors. Physical ailments, pedestrian uses, and even visible or perceived roadblocks were identified, each providing for a deepened level of understanding to help plan for the future of Downtown Hurricane.



Outlined below are the findings from on-site investigations completed during November 2022 and open-source databases. The information provided is not meant to be a comprehensive list yet is provided to explain a clear vision of how an outsider witnesses the downtown, the interaction of residents, and the overall downtown atmosphere. The findings are prepared to outline general themes and do not propose to make assessments of individual properties.

- 1. The corridor lacks general identity and distinguishable characteristics.**
- 2. Building forming and massing is not providing a distinct look or feel.**
- 3. The downtown is currently underserved in site amenities, limiting the time individuals or groups desire to be downtown.**
- 4. A handful of asset buildings exist, including: the 1950s hotel; historic main street building; small close-to-the-street historic brick store buildings (found in blocks 2, 3, and 4); and the Heritage Park and its buildings.**

- 5. Buildings throughout the study area demonstrate a substantial variety in size and purpose yet have limited ties or unifying characteristics together.**
- 6. Gateway signage or area delineation are not properly identified within the downtown.**
- 7. There is limited architectural uniformity in defining an existing corridor.**
- 8. Vegetation and other softening features are generally lacking or misplaced, limiting their impact and resulting in an unpleasant pedestrian experience.**





## Block-by-Block Findings

### Block 1

This area is bound by 300 W and 200 W to the West and East and 100 N and 100 S on the north and south sides respectively, with State Street bisecting the area as a major thoroughfare and commercial corridor. This is the westernmost study block with a 3,200 linear feet perimeter.



#### BUILT ENVIRONMENT



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

The lack of continuous street-facing businesses does not promote slowing traffic or engagement with the businesses. In addition, the architectural styles are not distinguished nor add to Hurricane's sense of place, lacking historical ties or promotion.

The south side of area no. 1 has a used car auto dealer, laundromat, hair salon, and the local Zions Bank branch. The north side of the study block has a chiropractor's office, restaurant, sportswear store, vacant commercial space, and a small hotel. Most of these structures appear to be made of cement block and brick construction, with some facades consisting of stone or stucco.

No buildings in this block seem to exceed 30 feet in height and vary in setback distances from the road. The Econo Lodge hotel is setback just a few feet from the road with a cement block barricade around the outdoor pool that is positioned at the intersection of State Street, and N 300 W.

The Zions Bank branch is positioned on the corner of State Street and South 300 W creates an insignificant entrance to the downtown area because most of the parcel is paved from curb to curb, lacking any notable architectural elements, landscaping, and space-defining building masses.

#### URBAN DESIGN



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

In area no. 1, there are no distinct or defining characteristics aiding in creating a sense of place within the downtown. The current urban form identifies the downtown corridor as "just another small town" in Utah. Perhaps the oldest building in this study block, the Econo Lodge hotel is a 1950s mid-century motel/hotel-style building with low horizontal roof angles and vintage balcony railings.

This nostalgic architecture helps to anchor the area by giving it some historical significance and a sense of permanence. Evenly spaced traditional acorn luminaire street lighting lines both sides of the street; however, there is a significant lack of climate-appropriate landscaping to enhance the existing buildings and streetscape.



### INFRASTRUCTURE (based on known datasets)



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

All infrastructure is in good condition. There is no data for maintenance and upgrades, yet preparing a database for this would be useful in capital improvements planning.

- All underground valves and valve boxes are in the ROW, ensuring proper maintenance and repairs.
- Abandoned water lines of 8" and 14" run underneath State Street.
- One high-pressure 14" ductile iron water line following State Street before turning north onto 200W.
- One 10" polyvinyl chloride water line runs parallel to State Street.
- Below-ground electricity is the primary method of power distribution, with just one short overhead secondary line connecting to a light near the NW corner of the hotel.
- Cast iron catch basins are present in the streetscape for stormwater drainage, and they are also present mid-block on the north side of State Street in some of the larger paved areas. All manholes are in the ROW for maintenance and repair access.

### PEDESTRIAN AND VEHICULAR CIRCULATION



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

The recently updated sidewalks, curbs, and streets make for easy traveling by automobiles.

However, the lack of a setback from the street for the sidewalk decreases both perceived and actual safety for pedestrians utilizing the sidewalks. The sidewalks are an average of 6 feet wide in this block area, with intermittent trees positioned between the pedestrians and the street. ADA ramps are placed at each intersection with marked crosswalks.

In addition, crosswalks crossing State Street are only present at the westernmost corner of this study block, all other crosswalks parallel State Street. Bicycle infrastructure is not currently allocated space or consideration for bicycle amenities in block 1.

State Street or SR-9 has two travel lanes in each direction, with a third lane for left turns at cross-street intersections. 300 W and 200 W are smaller streets with one lane of travel in each direction, a turn lane at State Street, and a parking lane on each side starting one-half block from State Street. Overall, circulation is concentrated on State Street with limited access in and out of parking lots depending on the direction of travel due to curbed medians.

### REAL ESTATE AND END-USER MIX



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

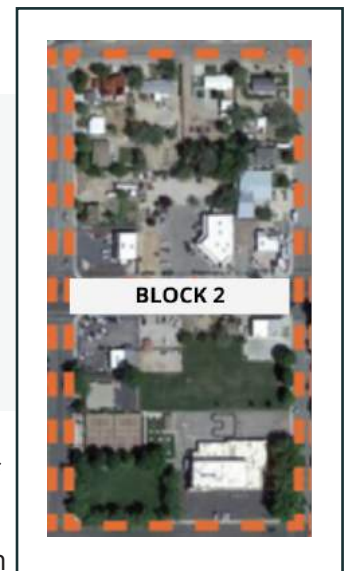
Current tenants in the area are a Pizza Hut, offices, a massage studio, a sporting goods store, a hotel, a bank, and a used car dealer.

There are currently three retail space with frontage on State Street is currently inactive, which could host service or retail-based businesses. There are no other vacant spaces in this study area.



## Block 2

This area is bound by 200 W and 100 W to the West and East and 100 N and 100 S on the North and South sides, with State Street bisecting the area as a major thoroughfare and commercial corridor. This study block has a 3,200 linear feet perimeter.



### BUILT ENVIRONMENT



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

On the southwest corner of State Street and 100 W, a small one-story building can host up to four small service or retail-based businesses. This building adds to Hurricane's character with its traditional brick stucco architecture and small setback.

A greater mix of businesses and uses occupy this study block. The southern portion of this block hosts the Hurricane Valley Heritage Park, Family History Center, a used car dealership, an archery store, a market, and a single-family home.

The north side of the study area has a home furnishings store, an ATV rental and tours center, a fast-food restaurant, and an assisted living facility. The area is sparsely built, with the dominating features being paved surfaces for parking and driving.

The outdoor furnishings store is awkwardly oriented towards 200 W. Currently, the built form has a setback of 35 feet from the road with a swath of pavement between the building and the road, detracting from the definition of the commercial corridor. All the structures in this block are one story but have different setbacks or interactions to the street, a combination of their setback orientation or angle towards State Street.

The building positioned at the northwest corner of State Street and 200 W, formerly a maverick gas station, has an appealing western-style architecture with an inviting covered front porch, harkening back to the area's history. In this study block, the corridor begins to have a landscaped median, adding much-needed greenery and interest to the thoroughfare.

### INFRASTRUCTURE

(based on known datasets)



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

### URBAN DESIGN



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

All infrastructure within block 2 is in good condition. There is no full data for maintenance or upgrades completed over the last decade.

- *Water infrastructure for the southern half of the study block is accessible only on 200 W and 100 W and not State Street, as the water line runs on the north side of the ROW providing water to the buildings on the north side of the corridor.*



## Existing Conditions

- New fire hydrants were installed in 2008 and 2009.
- All underground valves and valve boxes are in the ROW, ensuring the ability for proper maintenance and repairs.
- Underground secondary electrical lines (lateral or service feeders) supply electricity to the park and lights on the south side of the study block.
- Underground main lines supply electricity to all the businesses on the north side of the corridor in this study block, except for one residence at 44 N 200 W.
- Cast iron catch basins are in the streetscape for stormwater drainage. All manholes are in ROW for maintenance and repair access.

## PEDESTRIAN AND VEHICULAR CIRCULATION



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

Like the last study block, the central vehicular circulation occurs on State Street or SR-9; the primary difference for this section is a slightly elevated median that provides ample room for vegetation. 200 W on the west side of this block is more minor than State Street, with one lane of travel in each direction, a turn lane at State Street, and a parking lane on each side starting one-half block from state street.

100 W utilizes a stop sign instead of a traffic signal to cross the highway and has angled parking on both sides of the road, making for a narrower and calmer travel lane. Well-maintained narrow sidewalks cap both sides of the street above a six-inch curb.

ADA ramps are accessible at all marked crosswalks; however, the crosswalks are only presently paralleling State Street, and no bicycle amenities are present.

## REAL ESTATE AND END-USER MIX



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

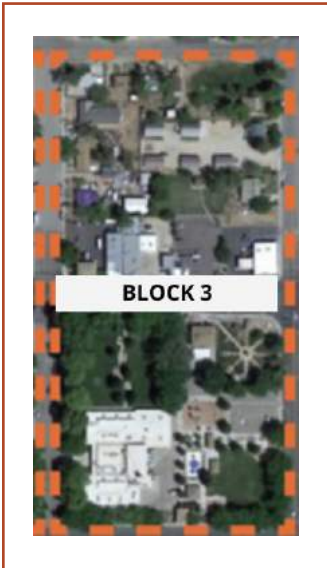
The tenant mixture in this study block is less diverse than the first study block due to the abundance of in-front parking and large swaths of space utilized by the used car dealer.

Other tenants include a small market, home goods store, fast food diner, an adventure center, an assisted living home, and a few single-family homes. Two small commercial spaces with a history of service and retail use at approximately 117 W State Street are vacant. The City owns most of the block to the south of State Street. Additionally, there are plans by the municipality to build a multi-purpose recreational hall in this area.





**Block 3**



100 W and Main Street bisects this area to the east to the west and 100 N and 100 S on the north and south sides, with State Street bisecting the area as a major thoroughfare and commercial corridor. This study block has a 3,200 linear feet perimeter.

**BUILT ENVIRONMENT**



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

**HOW DOES IT SCORE**

Hurricane Valley Heritage Park and a local history museum and tourist information center border this study area. The north side of the study area has a Wells Fargo Bank, restaurant, barber shop, craft store, cafe, pawn shop, and a real estate agency office. This area has less land dedicated to parking lots than any other study area in this report.

The buildings across the street from the park are one story with similar form and position near the sidewalk or slightly recessed behind a small parking courtyard-like area. One building on the west side of this block is two stories with a hipped roof and stucco and stone exterior.

The Wells Fargo Bank, positioned on the eastern corner, uses almost 200 feet of street frontage for its parking lot and building combined, breaking up the continuous storefronts to the west.

**URBAN DESIGN**



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

**HOW DOES IT SCORE**

Block 3 is more inviting and conducive to pedestrian mobility and community gathering. The large park on the south side of State Street provides ample grass and shade with a sidewalk behind mature street trees.

Adding to the historical character of Hurricane, the local history museum's architecture and stone and iron fence bordering the public sidewalk are reminiscent of early Mormon settler architecture. The buildings across the street from the park, excluding the bank, are positioned next to the sidewalk, helping to define the corridor while inviting people to utilize the sidewalks. Street trees on this side of the street are smaller but help to shade the sidewalk for pedestrians.





### INFRASTRUCTURE (based on known datasets)



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

All infrastructure is in fair to good condition. There is no data for maintenance except for recent fire hydrant upgrades.

- *New fire hydrants were installed in 2008 and 2009*
- *Underground secondary lines supply electricity to the park and lights on the south side of the study block.*
- *All underground valves and valve boxes are in the ROW, ensuring proper maintenance and repairs.*
- *Above-ground secondary electric lines supply electricity to the buildings on the north side of the street, equating to the most overhead lines out of the study blocks.*
- *Cast iron catch basins are in the streetscape for stormwater drainage. All manholes are in ROW for maintenance and repair access.*

### PEDESTRIAN AND VEHICULAR CIRCULATION



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

Automobile circulation is centralized around State Street with limited left turn mobility due to a landscaped median. The side streets, 100 W and Main Street, are quieter, with one lane of travel in each direction with a turn lane in the median at intersections. State Street and Main Street intersection has a traffic signal, while 100 W and State Street utilizes a stop sign.



While the intersection of State and Main has a crosswalk, the 2021 walking survey of the area called this intersection out as one of the most dangerous. Pedestrian accessibility is greatest in this study area because of the park’s network of pathways and open space away from the traffic of State Street. In addition, there is a four-direction crosswalk with ADA accessibility at Main Street and State Street, one of the few crosswalks across the Highway. Moreover, the General Plan recommends that 100 W south of State Street in this area be closed for greater pedestrian access. No bicycle amenities are present.

### REAL ESTATE AND END-USER MIX



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

Single-family detached homes exist on the north side of the study block, and the municipally owned public park exists on the southern edge. Private businesses operate along the north side of State Street. A bank, pet shelter, restaurant, craft store, and a real estate broker’s office are tenants on the north side of the highway. Two retail spaces in the middle of the block are vacant, yet in the process of renovation.



**Block 4**



This area is bound by Main Street and 100 E to the West and East and 100 N and 100 S on the North and South sides, with State Street bisecting the area as a major thoroughfare and commercial corridor. This is the easternmost study block with a 3,200 linear feet perimeter.

**BUILT ENVIRONMENT**



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

**HOW DOES IT SCORE**



This study block has a variety of businesses. The south side hosts a health foods store, auto repair, tire shop, Mexican food restaurant, and canine recreation center and spa.

The north side of the study area has a laundromat, tanning salon, antique mercantile, florist, tattoo shop, and a short-term rentals. Over 50% of the land area of this block is paved for parking and driving with inconsistent building placement and form. A mix of parking between and in front of buildings, in combination with no landscaping or uniform setbacks, creates a cumbersome area to navigate by foot, bicycle, or vehicle.

The historic building on the corner of State Street and Main Street is the most defining structure in this area because of its traditional form and placement next to the sidewalk.





### URBAN DESIGN



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

### PEDESTRIAN AND VEHICULAR CIRCULATION



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

The urban design of this specific area is non-descript except for the historic Main Street building north of the intersection with State Street. This two-story brick building has four storefronts and historic heritage apartments on the second floor. A well-maintained craftsman bungalow with large windows, brick and shingle exterior, and large roof overhangs, houses the canine recreation and spa center. Some other buildings show decay and discoloration on their stucco, detracting from the area's value and aesthetics.

Like the other study areas, the vehicular transportation is concentrated in the main corridor of State Street. However, strip mall-style development (parking in front with large building setbacks) on the north side of State Street that is oriented perpendicular to the corridor creates a terminal and narrow shaped parking lot wedged between other buildings, making vehicular navigation cumbersome—navigating as a pedestrian is similar here to the other study areas. A six-foot-wide sidewalk abutting the curbs with intermittent street trees and no bicycle amenities.

### INFRASTRUCTURE

(based on known datasets)



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

### REAL ESTATE AND END-USER MIX



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

HOW DOES IT SCORE

All infrastructure is in good condition. There is no data for maintenance except for recent fire hydrant upgrades.

- *New fire hydrants were installed in 2008 and 2009.*
- *Water of smaller caliper (6" -10") mains in ROW are present on all streets in the study block.*
- *All underground valves and valve boxes are in the ROW, ensuring proper maintenance and repairs.*
- *Below-ground electricity is the primary method of power distribution, with just one short overhead secondary line connecting to a house at 83 E State Street.*

On the south side of State Street, current tenants include a veterinary clinic, Mexican restaurant, and a car mechanic. There is a recently renovated vacant auto body shop or commercial garage space at 46 E State Street with a large parking lot in front. On the north side of the study area, there is a single-family home, a vacant lot, and a strip mall with a tanning salon and laundromat. In the historic building on the corner of Main Street and State Street, there is an antique mercantile and florist with vacant units to the north on the ground floor. The upper level of the historic building is a mixture of vacancies and overnight accommodations.



**Block 5**

This area is uniquely shaped compared to the other study areas, given the topographic features on the far eastern edge of Hurricane. Bound by 100 N and 100 S to the North and South, the east border of this area is defined partially following the northward curve of State Street (HWY 9), and then the border follows the rear property line of the parcels fronting S 100 E. mid-block.

**BUILT ENVIRONMENT**



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

**HOW DOES IT SCORE**

Block 5 has a newly built veterinary hospital and maverick gas station situated across State Street from each other. North of the gas station are eight single-family residential properties. South of the veterinary clinic is four residential properties. State Street in the block curves northward, eventually perpendicular to the rest of the corridor to the west. A twelve-foot wide median with mature desert landscaping adds beauty to the streetscape while traffic calming around the curve of the Highway.



**URBAN DESIGN**



Negatively Impacting Downtown



**HOW DOES IT SCORE**

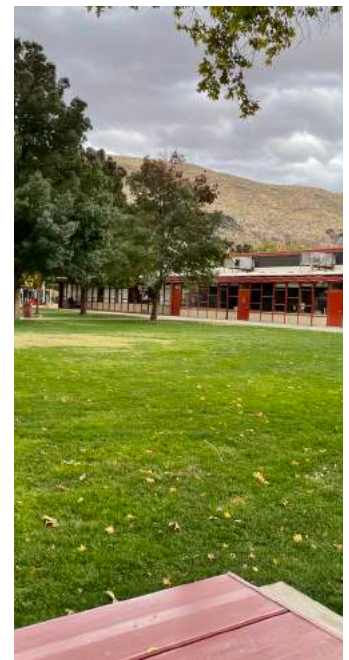


Neutral



Positively Impacting Downtown

This area has quality urban design, in part due to the well-designed maverick gas station with western styling and the veterinary clinic's craftsman styling. Both new developments have desert-appropriate landscaping with swales for sequestering stormwater runoff – an aesthetic and functional feature. Most of the homes in the study block are charming older homes with large trees and maintained yards.





### INFRASTRUCTURE (based on known datasets)



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

All infrastructure is in good condition. There is no data for maintenance except for recent fire hydrant upgrades.

- *New fire hydrants were installed in 2008 and 2009.*
- *Water of smaller caliper (6" -10") mains in the ROW are present on all streets in the study block.*
- *All underground valves and valve boxes are in the ROW, ensuring proper maintenance and repairs.*
- *Cast iron catch basins are in the streetscape and the veterinary hospital for stormwater drainage. All manholes are in the ROW for maintenance and repair access.*
- *Electrical mains run below ground to all businesses and homes except on E 100 N, where all the homes receive electrical overhead.*



### PEDESTRIAN AND VEHICULAR CIRCULATION



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

Like the other study areas, vehicular transportation is concentrated in the main corridor of State Street or SR-9. No crosswalks are available for crossing State Street in the study block, nor are there any bicycle amenities, demonstrating the auto centric design of the corridor. The space has been designed for the vehicle with little consideration for how bicyclists and pedestrians navigate the area, besides comfortable 9' wide sidewalks with ADA accessibility. Vehicle mobility through the parking lots is ample, with driveway access only present on 100 E for both lots due to the curve of the highway.

### REAL ESTATE AND END-USER MIX



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

The businesses in this block are Maverick gas station, Zion Veterinary Hospital, Ari Styles Hair Salon, and a short-term renal complex. The salon works out of a residential building tucked behind the gas station, and the area appears to have no vacant commercial spaces.



This area is south of the main corridor centered on Main Street and bound by 100 W and 100 E, and 200 S to the south. This is the southernmost study block with a 3,200 linear feet perimeter.

### BUILT ENVIRONMENT



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

This study block has many charming qualities typically found in the historic grid-style planning of early Mormon settlements. Notable features are present, including wide streets, mature trees, and historic homes worthy of historic protection. In addition, infill development in the form of ranch-style single-family homes and a Century Link facility at 53 W 200 S. A strip of small businesses on the west side of the main street working out of historic buildings help to tie the neighborhood to its historical roots.

The wide street on 200 S can feel undefined due to the large gravel parking strip on both sides of the road in the place of a curb and gutter. The gravel parking strip and lack of gutter make the neighborhood feel quaint and less urbanized, adding a rustic western motif while allowing stormwater to percolate naturally into the soil instead of being piped off-site. Blighting parcels exist, including the Century Link facility and dry vacant lots.

### URBAN DESIGN



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

The intermittent Victorian and Craftsman style historic homes are the greatest asset to this study block's urban design.





### INFRASTRUCTURE (based on known datasets)



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

All infrastructure is in fair to good condition, yet a lack of capacity increase has been notated due to limited pipe sizes. There is no data for maintenance except for fire hydrant upgrades.

- Fire hydrant ages range from 1979 to 2012.
- Most electricity is supplied from primary overhead lines located mid-block at the rear property lines of residential parcels.
- The only stormwater infrastructure is a gutter on the west side of S 100 E.
- Potable water runs through a 6" cast iron line in the ROW.



### PEDESTRIAN AND VEHICULAR CIRCULATION



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

This neighborhood area is calm and has wide streets, including ample room for 30-degree angled parking. Sidewalks are only present on 100 W and Main Street, where there is a curb and gutter. Parking is ample, with angled parking on 100 W and Main Street and gravel parking strips on 200 S. Accessibility is limited on 200 S for those using mobility devices due to the gravel parking lanes without sidewalks.

### REAL ESTATE AND END-USER MIX



Negatively Impacting Downtown



Neutral



Positively Impacting Downtown

#### HOW DOES IT SCORE

Chums headquarters, and the Main Street Café anchor the neighborhood businesses on Main Street. In addition, the Catholic thrift center, a roofing company, and Over the Edge Sports also have tenure in this study block. 132 S Main Street is a mains street property with limited interaction to the street.



# Transportation



## Traffic Volume

Annual average daily traffic (AADT) is an estimation of how many cars travel along a specific street or street segment in a day. This number is typically derived by recording traffic counts for an extended period on a specific street. After the traffic counts have concluded, the numbers are examined and determined to be representative of normal traffic behavior, this data is then used to create an annual daily average.

The highest traffic volumes in Downtown Hurricane are recorded on State Street (SR-9) where it reaches up to 36,200 vehicles per day. State Street connects I-15, west of downtown, to Toquerville Boulevard (SR-17), which is the main access to Toquerville and La Verkin, as well as the entryway to Springdale and Zion National Park. SR-59 (100 S) has the second largest traffic volume in Downtown Hurricane (5,300 cars daily) where it connects to 100 E at the eastern edge of the study area. Other roads in downtown have lower traffic volumes remaining under 5,000 vehicles per day.

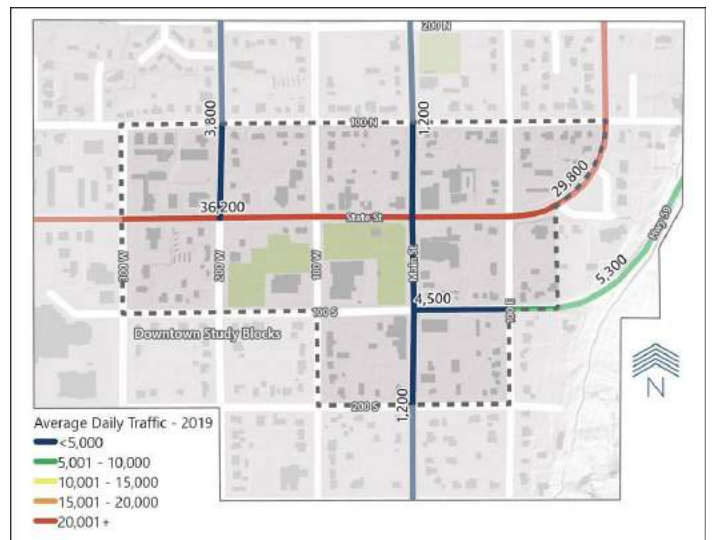


Figure 16: Average Daily Traffic Volumes (2019)



## Truck Traffic Volume

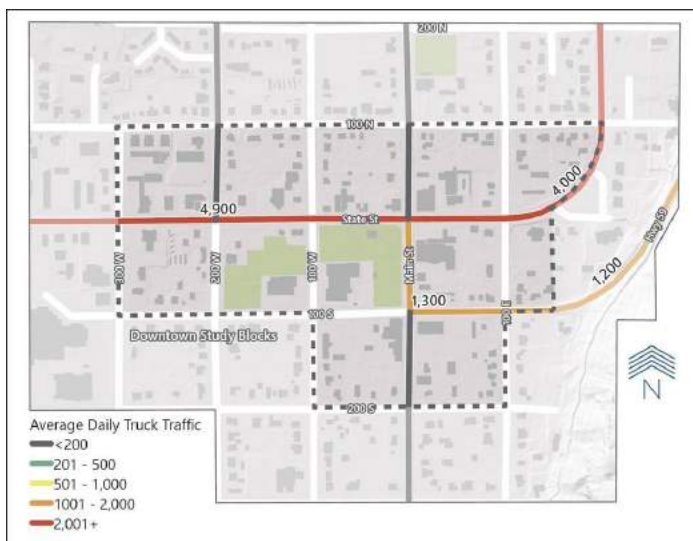


Figure 17: Average Daily Traffic Volume (2019)

State Street (SR-9) is also the main truck route in Hurricane. About 12.4% of the traffic on SR-9 is related to trucks. Trucking routes are essential to local and regional economies and the ability to move goods is a necessary component for a comprehensive transportation system. In Downtown Hurricane, accommodating large tractor-trailers as well as passenger vehicles can be a challenge. In general, tractor-trailers take up more space and require more time to come to a complete stop. Safety becomes a heightened concern not only for other vehicles but for non-motorists. High noise levels, road debris, and air pollution are also issues of concern. However, designated truck routes and urban design strategies can help mitigate these challenges.

All maps available in a larger format in the appendix



## Bicycle Usage

Strava is an app that uses GPS tracking to record a variety of outdoor activities including cycling, running, walking, and other forms of active transportation. The company provides the GPS data in the form of geospatial data, providing a general idea of high-traffic corridors for active transportation (AT) such as biking and walking. It is understood that the data is representative of only certain segments and demographics of the population, such as expert bicyclists and those with access to mobile devices and does not by any means represent all active transportation users. However, it is beneficial to see where these AT trips are currently occurring in Downtown Hurricane.

State Street is the corridor with highest bicyclist usage in Downtown Hurricane, with up to 1,390 average yearly two-way trips. This represents potential conflict with car and truck traffic on the same route since bike lanes or other dedicated bicycle facilities do not exist along SR-9. The second most used corridor is 100 S which is a lower conflict alternative to State Street for moving cyclists east-west.



## Pedestrian Usage

Strava data for pedestrians in Downtown Hurricane shows that State Street has the highest pedestrian usage as expected.

The area between 200 W and Main Street sees the highest number of people walking since it contains most of the main destinations in Downtown, including the Hurricane Valley Heritage Park Museum, the Hurricane City Leisure & Recreation building and park, Hurricane Theatrical, pickleball courts, and various commercial establishments. The second most trafficked street by pedestrians is Main Street followed by 100 W only between 200 S and 100 S. Most other streets in Downtown Hurricane are not used much by pedestrians. Pedestrian signal requests were evaluated for the existing traffic signals within the downtown area. These pedestrian requests are the number of times the crosswalk signal was called to allow pedestrians to cross

at the intersection. These requests represent the number of times pedestrians could cross the street at the signal and not the total number of pedestrians, since each request can allow for more than one pedestrians to cross.

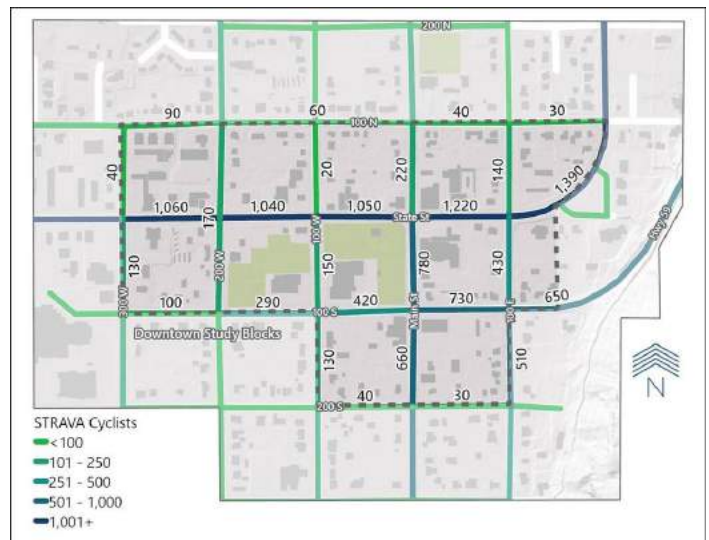


Figure 18: STRAVA Cycling Trips (2019) Yearly Two-way Trips

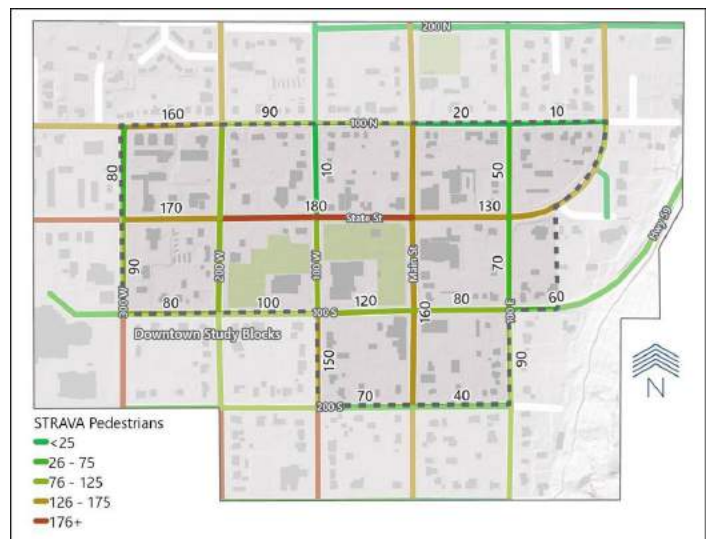


Figure 19: STRAVA Walking Trips (2019) Yearly Two-way Trips

02



# MARKET ANALYSIS

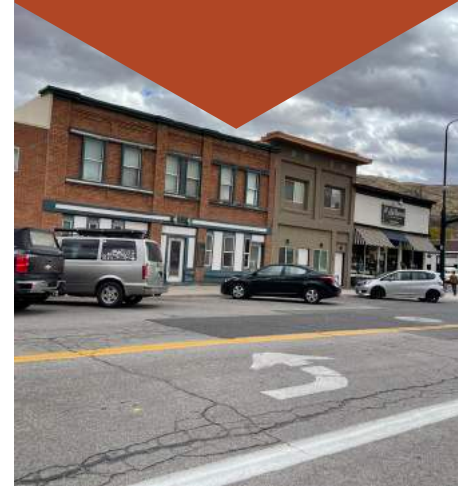




Hurricane City serves as one of several regional nodes for commerce, offering various services and amenities to the City, nearby communities, and visitors to Zion National Park. While it does meet most of the necessities and many of the desired purchases, the City is still missing several critical services and amenities that are causing residents to venture further away, towards Washington or St. George, or even further to fulfill a need.

These underserved amenities are causing residents to spend taxable dollars outside the local market, demonstrating a leakage. These missing services are called “gap” services or amenities, hindering the long-term growth of Hurricane’s economy.

The community must understand the most viable services or amenities to help the Downtown become a hub of activity and provide the highest use possible for properties. When reviewing market data and sales information, it is critical to remember there are two types of highest and best use, namely:



### ROI Focused Use

This style of highest and best-use calculation is based on the return on investment (ROI), focusing on the most information generated with a property.



### Civic-minded Use

This style of determination also includes qualitative factors, precisely elements necessary for vibrant communities and corridors. Often these elements have limited or no ROI yet are critical to the overall community health.

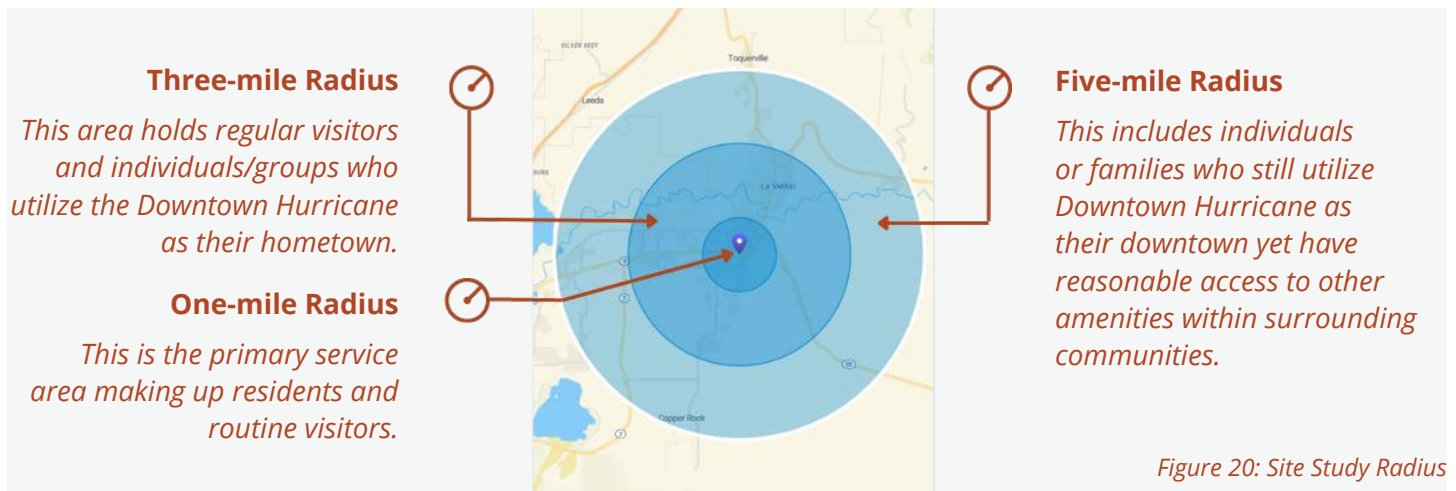
To determine opportunities for growth, the City must undergo a gap or market analysis, highlighting the potential end-users who can utilize the buildings or public spaces within Downtown, ultimately identifying the types of spaces needed. To identify these services, it is critical that the community better understand what services or amenities are being met within the community and what services residents have to travel for. By identifying these opportunity gaps, Hurricane can determine what will improve the quality of life for residents while preserving the existing businesses through limited competition.

Outlined in this section are data collected from the Claritas Market Analysis data, US Census Bureau, and other publicly available datasets. This information will be integrated within the formation from the existing conditions analysis and reviewed during recommendations preparation, ensuring the viability of critical elements or alterations.



## Market Trade Areas

When preparing a market analysis, it is critical to identify a potential market demand or target market area. In the case of Downtown Hurricane, a draw radius of up to 5 miles was identified as the primary draw area. This area is demonstrated on the map in Figure 20 below. There are multiple municipalities within this drawing area, many of which utilize the services and amenities that Downtown Hurricane offers. Due to this nature, we provide information about a series of distinct trade areas up to a five-mile radius. The areas identified are:



All information and data outlined in the remainder of this chapter represent conditions as documented for the one, three, and five-mile radius as provided in Claritas Data as pulled on 03/30/2023. Conditions change, and expenditures fluctuate regularly. Updating this report annually or bi-annually with new data is recommended to ensure it remains current.

## Missing Services & Amenities

Missing retail services within a community represent sectors in a market that are not meeting their fullest potential, specifically with the comparison between the amount of revenue captured and projected demand. For a small community to succeed, it must provide all necessary purchases and most of the desired services for residents. Without these goods and services, residents will be forced to travel for them, taking their valuable tax dollars to another location.

This section outlines the services and amenities identified as underserved within the target market areas (listed above). These identified sectors represent under-served services with either a potential for growth or start-up within Downtown Hurricane. Additionally, all missing services have been vetted as desirable or applicable to be placed within the one-mile.

Additional services have been underserved, yet they are not well situated for downtown or smaller-scale locations. Details about the style of service and required physical/socio-economic characteristics are provided for each service type. Each NAICS sector has significant variation within, both in terms of market demand and compatibility with the downtown area. Stakeholders should seek to recruit retail tenants that have gaps in supply, have positive growth outlooks, and foster pedestrian activity.

Auto-oriented businesses must be carefully designed within downtown areas, i.e., gasoline stations (NAICS 447), motor vehicle and parts dealers (NAICS 441), and non-store retailers (NAICS 454). While these three NAICS categories are not recommended as part of this Plan, the City should require that these establishments provide accommodations for pedestrians and micromobility users.



## Start-Up or Growth Opportunities

### Compounding Pharmacies and Drug Stores

This sector includes pharmacies and drug stores engaged in retailing prescription or nonprescription drugs and medicines, specifically retail pharmacies, apothecaries, drug stores, and on-site institutional pharmacies.



MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Pharmacies and drug stores (NAICS 44611)	\$4,413,189	\$14,026,681	\$4,094,296	84.35%

Pharmacies and drug stores are a necessity and are already met somewhat within all three of the trade areas. Numerous establishments are located along SR 9, but none are located within Downtown. The lack of multimodal infrastructure creates an issue with accessibility between Downtown and nearby pharmacies.

The supply gap within the five-mile trade area could nearly support an average establishment in sales, and this is worth further exploration beyond the five-mile trade area. Moreover, as the area continues to grow in population, there will be a need for an additional establishment.

### Convenience Stores

This sector includes convenience stores and food marts that sell a limited selection of necessity purchases (e.g., milk, bread, and snacks), excluding gas stations. Typical convenience stores provide necessity goods, and there is no existing supply within the one and three-mile trade areas. There is very limited supply within the five-mile trade area. The supply gap within the 5 mile trade area could nearly support two average establishments in sales. Theoretically, one could be placed on either end of Downtown. The City should promote locally-owned and moderate-income convenience stores to meet this sector.



MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Convenience stores (NAICS 44512)	\$515,150	\$1,592,102	\$2,100,316	196.16%



## Women’s Clothing Stores



This sector includes stores that sell women’s and girl’s clothing, including maternity wear. These establishments may provide basic alterations. Clothing is a desire purchase and should be designed to compete with online retail by providing a niche retail experience. There is substantial supply within all three supply trade areas, but all three still have opportunity gaps. The supply gap within the five-mile trade area could nearly support an average establishment in sales, and this is worth further exploration beyond the five-mile trade area. Moreover, as the area continues to grow in population, there will be a need for an additional store.

MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Women’s clothing stores (NAICS 44812)	\$486,382	\$1,659,544	\$563,280	63.96%

## All Other General Merchandise Stores



This sector includes stores that primarily sell general merchandise, excluding department stores, warehouse clubs, superstores, and supercenters. Merchandise may include apparel, automotive parts, dry goods, hardware, housewares or home furnishings, and other lines in limited amounts, with none of the lines predominating. Examples include general stores, home and auto supply stores, and variety stores. Stores in this sector provide necessity goods, and while there is substantial supply within all three supply trade areas, all three still have opportunity gaps.

The supply gap within the five-mile trade area could nearly support two average establishments in sales, and this is worth further exploration beyond the five-mile trade area. Moreover, as the area continues to grow in population, there will be a need for an additional store.

MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
All other general merchandise stores (NAICS 452319)	\$468,048	\$2,161,879	\$3,106,629	161.17%



## Shoe Stores

This industry comprises establishments primarily engaged in retailing all types of new footwear, excluding hosiery and specialty sports footwear, e.g., golf shoes, bowling shoes, and spiked shoes. Stores primarily selling tennis shoes or sneakers are included.



MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Shoe stores (NAICS 4482)	\$464,479	\$1,538,936	\$2,087,198	186.56%

Clothing is a desire purchase and should be designed to compete with online retail by providing a niche retail experience. There is very limited supply within the five-mile trade area. The supply gap within the 5 mile trade area could nearly support two average stores in sales. Theoretically, one could be placed on either end of Downtown.

## Jewelry Stores

This sector includes stores primarily selling new non-costume jewelry, sterling and plated silverware, and/or watches and clocks. These stores may also provide lapidary work and/or repair services. Jewelry is a desire purchase and should be designed to compete with online retail by providing a niche retail experience. While there is some supply within all three trade areas, there is still a very large opportunity gap at all distances.



MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Jewelry stores (NAICS 44831)	\$457,637	\$1,595,805	\$1,740,313	134.83%

Moreover, given the niche nature of the jewelry sector, the trade area is much larger, especially for niche services like grandfather clock repair. The supply gap within the 5 mile trade area could very well support an average store in sales, and this should be investigated further for the possibility of a larger draw area.



## Specialty Food Stores

This sector includes stores primarily selling specialized lines of food. Meat markets, fish and seafood markets, fruit and vegetable markets, other specialty food markets. Specialty food stores provide higher-end necessity goods, and there is a limited supply within the five-mile trade area. The supply gap within the 5 mile trade area could nearly support two average establishments in sales, and this will grow as the region continues to grow. Theoretically, one could be placed on either end of Downtown. The City should promote locally-owned stores to meet this sector.



MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Specialty food stores (NAICS 4452)	\$358,157	\$1,105,774	\$1,463,842	159.98%

## Snack and Non-Alcoholic Beverage Bars

This sector includes establishments primarily engaged in: preparing and/or serving a specialty snack, such as ice cream, frozen yogurt, cookies, or popcorn, or serving nonalcoholic beverages, such as coffee, juices, or sodas. These establishments may carry and sell a combination of snack, nonalcoholic beverage, and other related products (e.g., coffee beans, mugs, coffee makers) but generally sell a unique snack or nonalcoholic beverage. Examples include: beverage bars; carryout service doughnut, bagel, pretzel, and cookie shops with on-premises baking; coffee shops with on-premises brewing; and ice cream parlors.



Snack and non-alcoholic beverage bars are a mixture of niche and necessity goods, and these do not compete with online retailing. There is some supply within all three supply trade areas, but all three still have significant opportunity gaps. The supply gap within the five-mile trade area could nearly support four average establishments in sales, and this is worth further exploration beyond the five-mile trade area since Hurricane is a gateway to Zion National Park. These types of establishments are popular among travelers and families.



MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Snack and non-alcoholic beverage bars (NAICS 722515)	\$354,086	\$1,617,355	\$2,359,578	384.52%



## Electronics Stores

Electronics are an ever-changing market given the rapid evolution in technology available to the market. Moreover, recent market trends have increased niche retailing for this sector. This sector includes stores that sell:

- Products such as televisions, computers, and cameras;
- Single line of consumer-type electronic products;
- New electronic products in combination with repair and support services;
- New prepackaged computer software; and/or
- Prerecorded audio and video media, e.g., such as vinyl records.



Examples include cell phone accessories stores, consumer electronic stores, and computer stores.

Electronics are desire purchases and should be designed to compete with online retail by providing a niche retail experience. While there is significant supply within all three trade areas, there is still a very large opportunity gap at all distances. The supply gap within the 5 mile trade area could well support an average store in sales.

MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Electronics stores (NAICS 443142)	\$335,225	\$2,297,116	\$3,512,062	125.48%

## Luggage and Leather Goods Stores

This sector includes stores selling new luggage, briefcases, and trunks. These stores may also sell these in combination non-leather apparel such as belts, gloves, and handbags. Luggage and leather goods are desire purchases and should be designed to compete with online retail by providing a niche retail experience.

There is no supply within the five-mile trade area. The supply gap within the 5 mile trade area could well support two average stores in sales. Theoretically, one could be placed on either end of Downtown. National brands, whether sold by a local or national retailer, are the most popular products in this sector.



MISSING SERVICE	Supply Gap (1 mile)	Supply Gap (3 mile)	Supply Gap (5 mile)	5 Mile Gap as a % of Avg. Sales
Luggage and leather goods stores (NAICS 44832)	\$334,021	\$1,035,536	\$1,385,859	218.42%



## Missing Service Typology Recommendations

As important as identifying the potential demand is outlining the requirements for each leakage. The below chart, Figure 21, shows the critical factors for each of the specified services. These statistics are based on national standards, and further research may be required to verify their applicability to the Hurricane local economy.

MISSING SERVICE	Recommended Square Footage	Recommended Community Size	Recommended Per Capita Income
Pharmacies and drug stores (NAICS 44611)	1,500 - 2,500	5,000	\$35,000
Convenience stores (NAICS 44512)	2,500 - 4,000	1,500	\$30,000
Women's clothing stores (NAICS 44812)	1,500 - 2500	10,000	\$40,000
All other general merchandise stores (NAICS 452319)	3,500 - 7,500	1,500	\$30,000
Shoe stores (NAICS 4482)	3,00 - 5000	4,000	35,000
Jewelry stores (NAICS 44831)	1,500 - 2,500	5,000	\$40,000
Specialty food stores (NAICS 4452)	5,000 - 10,000	3,000	\$35,000
Snack and non-alcoholic beverage bars (NAICS 722515)	3,000 - 5,000	5,000	\$30,000
Electronics stores (NAICS 443142)	3,000 - 6,000	5,000	\$35,000
Luggage and leather goods stores (NAICS 44832)	2,000 - 4,000	10,000	\$45,000

Figure 21: Missing Service Recommendations

The Claritas database provides all information from the above-outlined services. Market research or the Claritas database offers details about each amenity or category interpolated or reviewed.

## Economic Impact of Missing Services or Amenities

Outlined below are calculations demonstrating the potential “leakage” of sales tax revenue to the City. The tax revenue calculations are based on a 1% City sales tax, and all estimates are provided per establishment. In addition to the 1% City sales tax, there is also a 4.85% Utah state sales tax, 0.35% Washington County sales tax, and 0.55% special district sales tax.

MISSING SERVICE	Average Revenue Per Utah Establishment (Source: 2017 Economic Census)	Potential City Sales Tax Capture (Per Establishment)
Pharmacies and drug stores (NAICS 44611)	\$4,853,671	\$48,537
Convenience stores (NAICS 44512)	\$1,070,702	\$10,707
Women's clothing stores (NAICS 44812)	\$880,723	\$8,807
All other general merchandise stores (NAICS 452319)	\$1,927,519	\$19,275
Shoe stores (NAICS 4482)	\$1,118,778	\$11,188
Jewelry stores (NAICS 44831)	\$1,290,712	\$12,907
Specialty food stores (NAICS 4452)	\$915,003 (US)	\$9,150
Snack and non-alcoholic beverage bars (NAICS 722515)	\$613,649	\$6,136
Electronics stores (NAICS 443142)	\$2,799,000	\$27,990
Luggage and leather goods stores (NAICS 44832)	\$634,500	\$63,450
<b>Potential additional City tax revenue annually</b>		<b>\$218,417</b>

Figure 22: Missing Service Economic Impact

All calculations are based on Claritas database information and US Census Bureau revenue per NAICS sector. The above data is provided to compare and demonstrate potential revenue capture for Hurricane, and all statistics will require additional investigation. The information above does not guarantee the tax capture for properties or businesses and is merely a projection based on available data.



## Oversaturated Services

While Hurricane has a documented lack of services and amenities, the community does have several over-saturated services. Oversaturated services are grossly overselling the potential demand for the Downtown. Outlined below are the services that are quoted as most oversaturated for all three identified radii.

Oversaturated Service	1 Mile Radius	3 Mile Radius	5 Mile Radius
Building material and supplies dealers (NAICS 4441)	-\$19,825,073	-\$33,986,488	-\$42,349,014
Warehouse clubs and supercenters (NAICS 452311)	-\$15,781,421	-\$21,617,311	-\$23,060,641
Other general merchandise stores (NAICS 4523)	-\$15,313,373	-\$19,455,432	-\$19,954,012
Home centers (NAICS 44411)	-\$11,235,187	-\$22,624,074	-\$32,636,821
Other building material dealers (NAICS 44419)	-\$7,006,003	-\$9,410,590	-\$8,243,352
Home furnishings stores (NAICS 4422)	-\$3,717,370	-\$6,891,539	-\$7,627,943
Furniture stores (NAICS 4421)	-\$3,499,122	-\$5,968,610	-\$6,330,576

Figure 23: Oversaturated Services



**By targeting end user clusters, the potential revenue per foot in a downtown is increased by 70%**



## Economic Opportunity Impacts

This section outlines recommendations for business or end-user types to attract into Downtown Hurricane. These recommendations are based on the market analysis and a detailed review of the currently vacant or underutilized buildings within the downtown area. The suggestions help identify tenant type, square footage, block location, and potential lease rates.

Recommended Use	Space Allocated in the building (sq. ft)	Potential Lease Rate (per foot)	Potential municipal revenue capture per foot
Pharmacies and drug stores (NAICS 44611)	6,500	\$20.00	\$7.46
Convenience stores (NAICS 44512)	5,000	\$18.00	\$2.14
Women's clothing stores (NAICS 44812)	2,000	\$15.00	\$4.40
All other general merchandise stores (NAICS 452319)	7,500	\$15.00	\$2.57
Shoe stores (NAICS 4482)	1,500	\$17.50	\$7.45
Jewelry stores (NAICS 44831)	2,000	\$20.00	\$6.45
Specialty food stores (NAICS 4452)	5,000	\$15.00	\$1.83
Snack and non-alcoholic beverage bars (NAICS 722515)	1,500	\$17.50	\$4.09
Electronics stores (NAICS 443142)	5,000	\$20.00	\$5.60
Luggage and leather goods stores (NAICS 44832)	2,000	\$15.00	\$3.17

**Note:** The above-quoted rates are outlined as regional standards. Further review is required for market condition applicability.

Figure 24: Economic Opportunity Impacts

03



# PUBLIC INPUT SYNOPSIS



# PUBLIC INPUT SYNOPSIS



Public input is the cornerstone of any civic-minded project, and the Hurricane City Downtown Plan is no different. Engagement with the public started early in the process and was a constant touchpoint throughout the entire process. Outlined below is the process that was utilized, and the results achieved.

## The Process

The process utilized for the Hurricane City Downtown Plan followed the VAK (Visual, Auditory, Kinesthetic) model. Through this model, we were able to tailor feedback models to methods for which the public was comfortable with. Additionally, a test, refine, and repeat model was utilized, ensuring that public opinion was integrated throughout, ensuring the public's vision was met at every stage. Outlined below is the process that was utilized:

1. Community values surveying
2. Stakeholder and one-on-one meetings
3. Community event participation
4. Community Visioning Workshops
5. Draft Recommendations
6. Community Feedback Survey
7. Final report presentation

## The Results

Due to the efforts of the public and stakeholders, engagement and feedback was an outstanding success. Some critical statistics include:

- **A total of 622 responses were completed for the community visioning survey.**
- **6 different stakeholder meetings were held.**
- **More than 47 individuals and families provided feedback at 3 public workshops across business owners, elected officials, and community member groups.**
- **128 members of the public participated over two meetings providing community visioning input and feedback, producing over 500 comments.**
- **A total of 220 responses were received for the community feedback survey.**

Additional data from public input events is provided in the appendix section

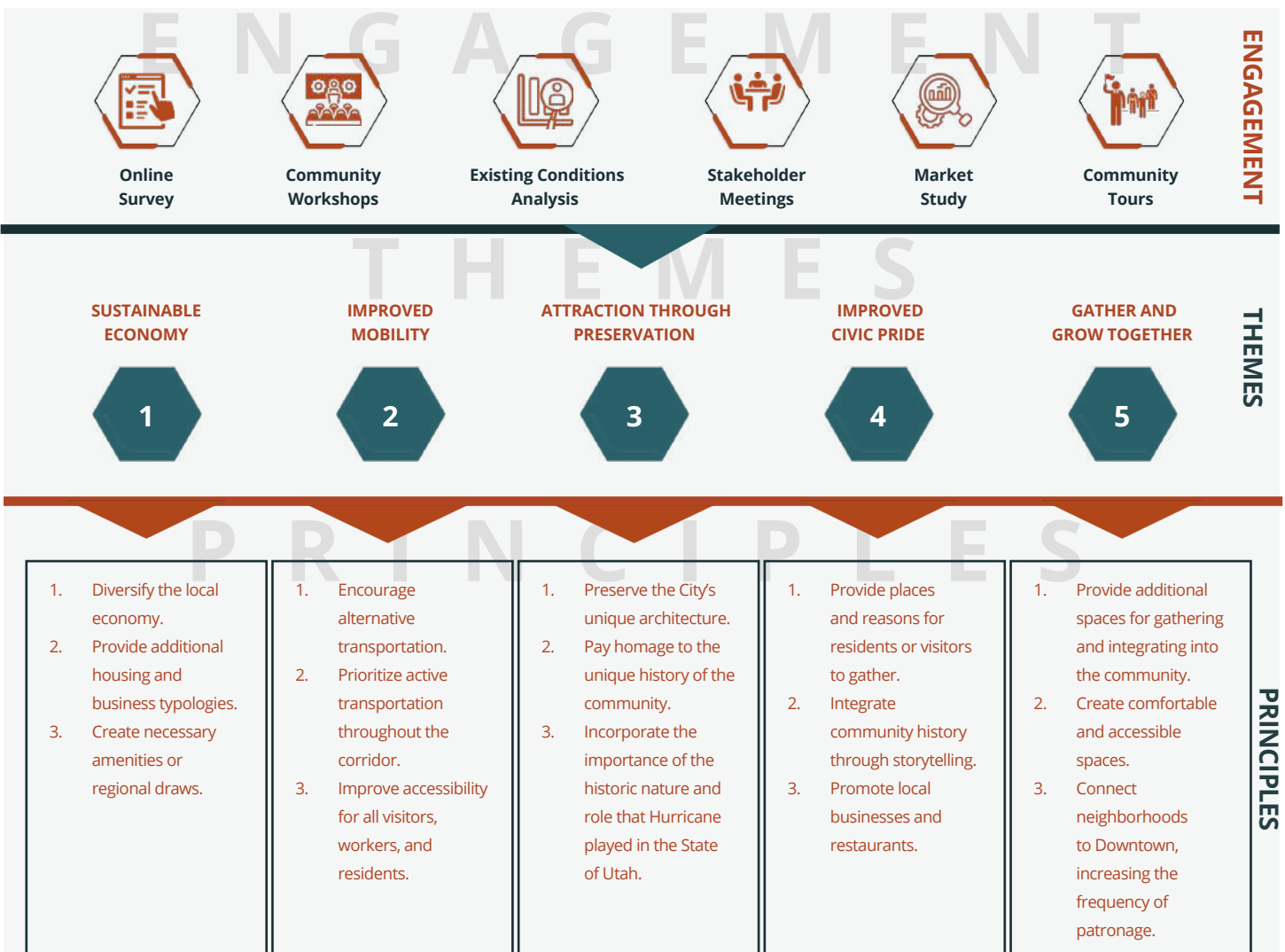


## Community Character Elements

Outlined below are some of the critical community character elements determined through public engagement process. These elements were compiled and extrapolated from input surveys, community charrette notes, online comments, and other feedback methods to demonstrate the overall vision of the community:

- Residents enjoy the small-town atmosphere of Hurricane City.
- There are concerns that revitalization efforts and development will detract from the small-town feel of the community.
- 33.8% of residents have lived in the community for one to five years.
- 37.4% of respondents visit Downtown area daily.
- 75.1% of respondents stated there are not enough service and retail options in Downtown.
- Of the second survey respondents, 72.9% indicated a desire for date night dining options as a priority.
- 89.9% of survey respondents support further downtown revitalization in Hurricane City.
- 71.8% of survey respondents indicated that improved retail and service opportunities in Hurricane City would make Downtown more vibrant.

## Plan Themes



04



# DRAFT RECOMMENDATIONS





## Revitalization Recommendations

In the previous chapters, highlighted information has been prepared to outline the current Downtown conditions and a desired future for Downtown. These two items serve as both the beginning and terminus of a path forward for the community. However critical these steps are, they are not actionable or implementable without a series of detailed and attainable recommendations that will document a concise pathway between the two identified points. Outlined in this chapter are recommendations that will help drive Downtown and the community forward, achieving their goals to make a vibrant, attractive, and inviting downtown.

All recommendations outlined in this chapter are crafted to help revitalize or amend the community based on the results of the public input received during the extensive input campaign. Recommendations are broken down into categories to offer background data and information for each recommendation. The below information is provided for each recommendation:

- Name/Topic of the recommendation**
- A detailed explanation of the recommendation**
- Justification for the recommendation**
- What issues or concerns will the recommendation address**
- Anticipated benefits from completion**



The implementation chapter will provide further details about each recommendation's timing, roles, and responsibilities, such as:

- Timing and strategies for implementation**
- Integration and cooperation necessary between community partners**





## Land Use Recommendations

This section outlines recommendations for Downtown’s current and future land use, as outlined in Figure 25. These recommendations are created to help provide a series of changes to the land-use designations within Downtown that will drive catalyst changes and ensure enforceability. A list of proposed recommendations is as follows:

1. **Implement a vacant property ordinance.**
2. **Create a Downtown mixed-use zoning district, allowing and encouraging a variety of uses on upper floors such as dwelling units, office spaces, and studios.**
3. **Create an overlay zone for Downtown (specifically related to form-based codes).**
4. **Alter local ordinances and zoning to require retail or service-based industries in Downtown.**
5. **Update zoning and ordinances to ensure a minimum Floor Area Ratio (FAR) is met.**

Detailed information about each of the above-outlined recommendations is provided below:

### 1. Implement a vacant property ordinance

A vacant property ordinance will allow the community to better regulate the use of spaces (both indoor and outdoor) within Downtown. Through this effort, the community can ensure that maximized tax revenue is collected from sales and real estate. The proposed elements of a vacant property ordinance should include the following:

#### i. Name/Topic of the recommendation

Limiting the potential uses or end-users in a building will limit the potential impact for less impactful uses. For example, storage and unused spaces should not be allowed on retail window-facing or ground-floor spaces within downtown properties.

#### ii. Requirements for building use

Thresholds (minimum and maximum) for vacancy tenure – This part of the ordinance would focus on providing a set of guidelines for properties that are choosing to be vacant, specifically:

- *Documenting the minimum requirements for marketing the properties.*
- *Outlining the maximum time frame a property can be left vacant without adequate marketing or attempts to re-lease.*

#### iii. Document what a “used” building entails

As critical as the requirements for reducing vacancies is the definition of what a used property entail. This definition should include necessary language that will clearly and concisely define characteristic traits of what a user (or non-vacant) property requires. Special emphasis should be provided on the presence or appearance of spaces, not dictating the exact use of non-visible spaces.

#### iv. Provide incentives and penalties for compliance issues

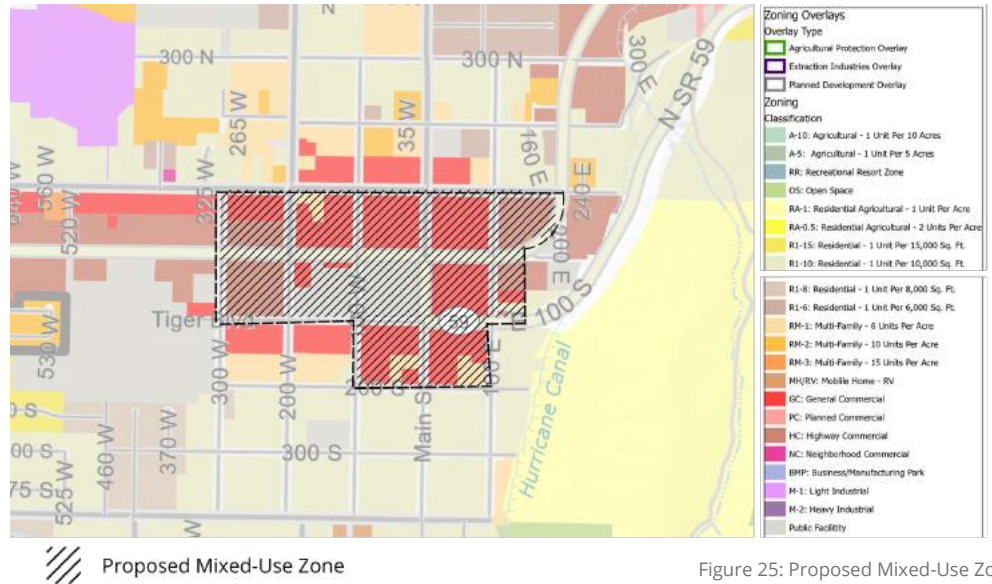
As critical as the requirements for reducing vacancies is the definition of what a used property entail. This definition should include necessary language that will clearly and concisely define characteristic traits of what a user (or non-vacant) property requires. Special emphasis should be provided on the presence or appearance of spaces, not dictating the exact use of non-visible spaces.



**2. Create a Downtown mixed-use zoning district, allowing and encouraging a variety of uses on upper floors such as dwelling units, office spaces, and studios**

A mixed-use zoning district should be established within Downtown. This will require an amendment to the current zoning code and map. Creating a mixed-use zone will encourage all floors of Downtown buildings to be utilized.

The activation of upper floors within a downtown benefits the community's overall economy but it also benefits the specific property owner by providing the owner with a more diversified source of income from the property. Upper floor uses can include professional services, office spaces, studios, housing and more, depending on the space available and other site aspects.



**3. Create an overlay zone for Downtown (specifically related to form-based codes)**

An overlay zone applies special zoning regulations over previously established zoning districts. Creating a new overlay zone will allow the City to implement the most successful parameters for Downtown while maintaining the previously determined requirements with the other zoning districts. All future development and redevelopment within Downtown will be subject to, and be able to take full advantage of, the new regulations. This is anticipated to impact newly constructed buildings, grandfathering existing uses in place. The proposed overlay zone would implement Form-Based Code regulations and the existing zoning requirements within Downtown.

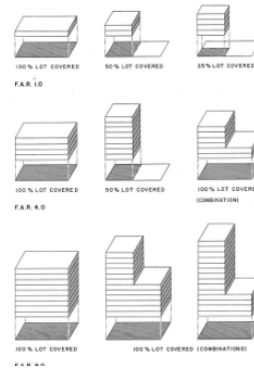
**4. Alter local ordinances and zoning to require retail or service-based industries in Downtown**

This recommendation is provided as a method for helping to improve the utilization of space within Downtown. By implementing a retail or service-based industry, first-floor requirement, the community will be able to ensure Downtown properties maximize visibility and provide the most economic impact possible. Additionally, the efforts of ensuring retail or services-based industries on the first floor will drive the appearance of full store fronts, demonstrate viability of the corridor, and improve ADA accessibility, ensuring access for all users.



## 5. Update zoning and ordinances to ensure a minimum Floor Area Ratio (FAR) is met

This recommendation will ensure that downtown development or redevelopment maximizes the economic impact on the community. This will require an increase in the floor area ratio, or land covering percentage, to allow for greater density. FARs of 1 or higher represent square footage that meets 100% lot coverage. Square footage calculations are based on occupiable space, as determined on your occupancy permit.



$$\text{Floor Area Ratio (FAR)} = \frac{L2 \times W2}{L1 \times W1}$$

Figure 26: FAR Configurations

### For example:

a. A 10,000 Sq Ft single-story building on a ½ acre lot would have the following calculation – 10,000 Sq Ft of building / 21,780 Sq Ft of lot size = FAR .459

b. A 30,000 Sq Ft building (2 floors at 15,000 Sq Ft each) on a ½ acre lot would have the following calculation – 30,000 Sq Ft of building / 21,780 Sq Ft lot size = FAR 1.37

Increasing the FAR calculations will be important to improve usable space in Downtown and even integrate housing. Outlined in Figure 27 is the proposed FAR calculation to be achieved by different tenant/building types:

While this list is not exhaustive, it should set the minimum calculations allowed for permitting during the City's business/building permitting process. Incentives for increased FAR could include reduced parking requirements or qualification. Special consideration should be given for buildings with a FAR of .75 or higher, to reduce parking. See page 83, recommendation 7 for further details.

Building Use	Floor Count	FAR
Retail	1 Floor	.75 - .90
	2 Floors	1.15 - 1.75
Commercial	1 Floor	.65 - .80
	2 Floors	1.05 - 1.3
Service/Restaurant	1 Floor	.60 - .80
	2 Floors	1.25 - 1.40
Residential (stand-alone)	1 Floor	.65 - .80
	2 Floors	.95 - 1.25

Figure 27: FAR Ratios by Building Use

Here are a few considerations to consider when calculating FARs for buildings:

- ✔ FAR's do not include parking or exterior hard surfaces in their calculations.
- ✔ ROW or other space calculations are not calculated in the FAR calculations.
- ✔ A FAR calculation should be conducted for all redevelopment or new construction.
- ✔ Outdoor dining spaces for restaurants (including decks and other above-ground spaces) should be calculated in the FAR.

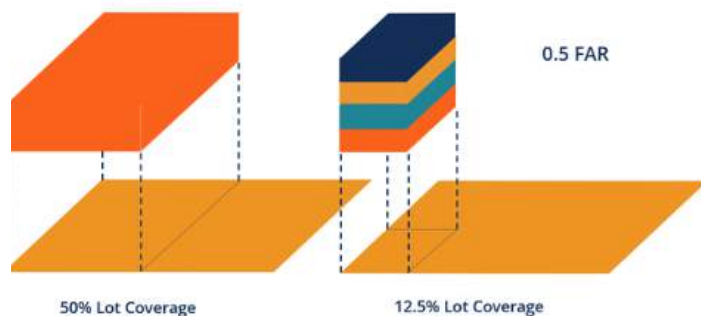


Figure 28: Lot Coverage Equivalencies



## Built Environment Recommendations

Outlined in this section are recommendations relating to the built environment of Downtown. These recommendations are crafted to help implement necessary changes to the community's vision for the overall look, feel, and atmosphere of Downtown Hurricane. A list of proposed recommendations is as follows:

1. **Update and enforce signage regulations.**
2. **Create downtown design guidelines.**
3. **Create mid-sized, hard-surface community gathering spaces in Downtown.**
4. **Incentivize the development of mixed-uses for developers acquiring underutilized downtown properties.**
5. **Work with local realtors to create a downtown properties marketing database.**
6. **Implement an annual façade program.**
7. **Preparation and implementation of form-based code, specifically in the Downtown overlay zone**

Detailed information about each of the above-outlined recommendations is provided below:

### 1. Update and enforce signage regulations

Creating unified and comprehensible signage code will drive a distinct Downtown look and feel. In addition, updating sign regulations will provide a clear and consistent review framework. These guidelines will help ensure the proposed signage accentuates Downtown, furthering the area's brand.

These downtown design guidelines will help define a sense of place and can be further personified in the proposed downtown design guidelines. Samples of appropriately integrated signage in successful downtowns are provided below:

Some regulations to consider are:

- ✓ No more than three (3) colors for small or medium-sized signage
- ✓ Limited window-mounted or painted signage
- ✓ Limited text on signage, preferring logos and graphics for storytelling and identification
- ✓ Non-internally light signs, preferring ornate and accurate hook or overcast lighting
- ✓ Informational or LED scrolling signage should be discouraged; and
- ✓ Marquee or other attention-grabbing signage should be encouraged at critical intersections





## 2. Create downtown design guidelines

Preparing this community-driven document yields a defined and implementable “look” for Downtown. These elements will help promote an attractive and inviting atmosphere, encouraging individuals to spend more time downtown. Often, a successful set of design guidelines must be specific and place-based. Elements proposed to be included are:

- ✓ District elements and typology
- ✓ Architecture typology
- ✓ Architectural styles details
- ✓ Forming and massing of architectural elements
- ✓ Incremental development
- ✓ Before and After renderings
- ✓ Façade elements
- ✓ Signage requirements
- ✓ Site amenities and vegetation guidelines

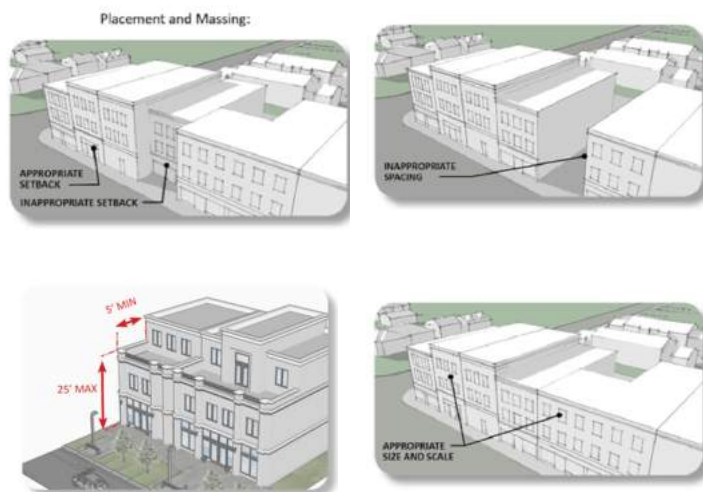


Figure 29: Design Guideline Example Graphics

Uses	Districts					
	Big Box Store	Medium Intensity Strip Mall	Medical Office	Mixed-Use Retail	Mid-Rise Residential	Village Residential
<b>Residential &amp; Lodging</b>						
Residential				●	●	●
Hotel & Inn	●	●	●	●	●	●
Residential Care				●	●	●
<b>Civic</b>						
Assembly			●	●	●	●
Transit Station	●	●	●	●	●	●
Hospital & Clinic		●	●	●	●	●
Library/Museum/Post Office (no distribution)	●	●	●	●	●	●
Police & Fire	○	○		○	○	○
School				●	●	●
<b>Retail</b>						
Neighborhood Retail	●	●		●		
General Retail	●	●		○		
Outdoor Sales Lot						
<b>Service</b>						
Neighborhood Service	●	●	●	●	●	●
General Service	●	●				
Vehicle Service				○		
<b>Office &amp; Industrial</b>						
Office	●	●	●	●	●	●
Craftsman Industrial	●	●	●	●		
<b>Infrastructure</b>						
Parking Lot	●	●	●	●	●	●
Parking Structure	●	●	●	●	●	●
Utility & Infrastructure	○	○	○	○	○	○
Open Space	●	●	●	●	●	●
<b>Accessory Uses</b>						
Home Occupation	●	●	●	●	●	●
Outdoor Storage of Goods				●		
Parking Lot	●	●	●	●	●	●
Parking Structure	●	●	●	●	●	●



### 3. Create mid-sized, hard-surface, community gathering spaces in Downtown

Public spaces are where a community gathers, integrates, and connects. For Hurricane, it is recommended that a series of public gathering spaces be created, helping to guide and encourage further downtown exploration. Depicted below are the proposed sites and amenities for each:

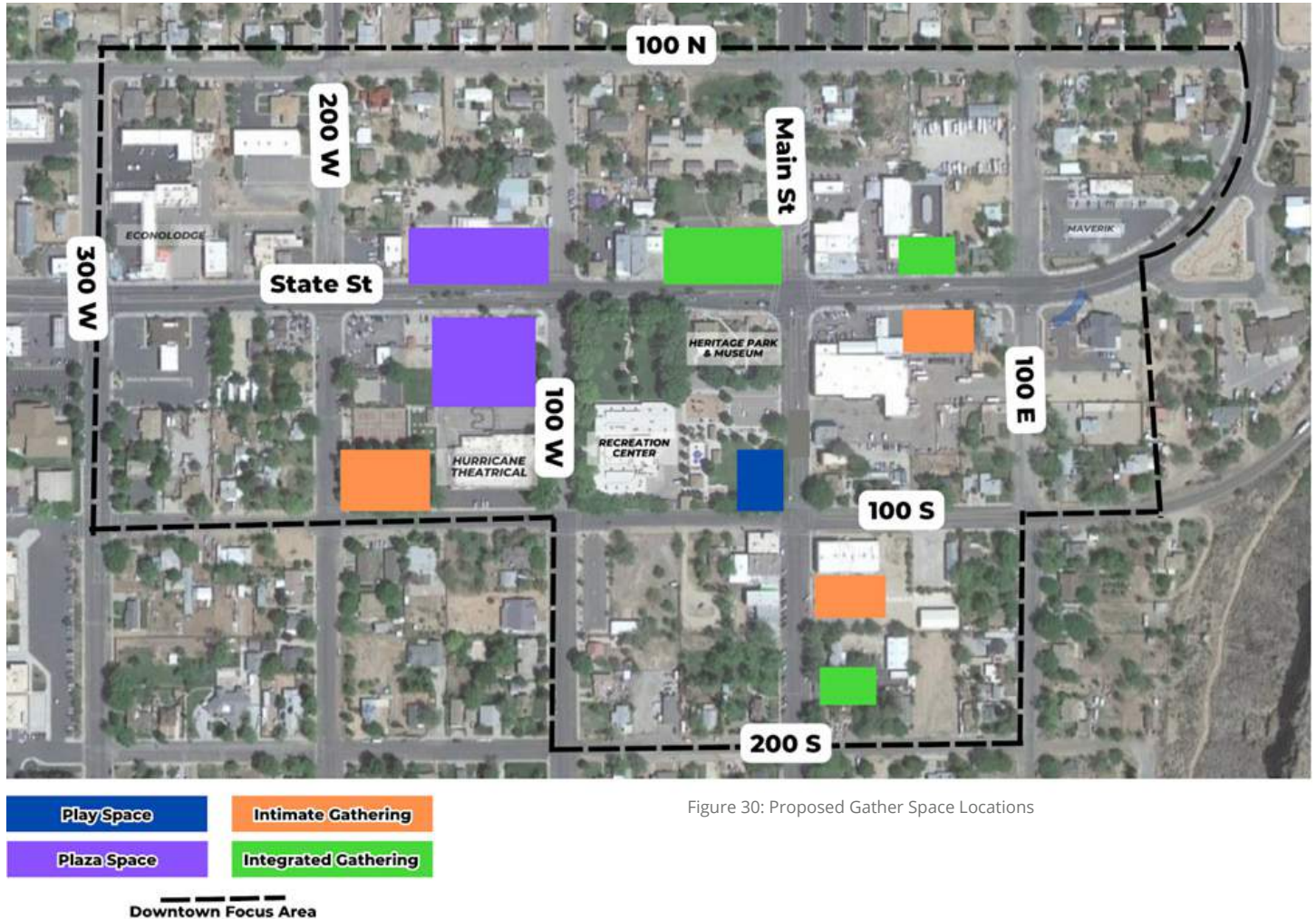


Figure 30: Proposed Gather Space Locations

#### Critical gathering spaces include:



DOWNTOWN PLAY SPACE	COMMUNITY PLAZA	INTIMATE GATHERING SPACE	INTEGRATED GATHERING SPACE
<p>This space is meant to enrich the experience for kids, providing passive play opportunities and allowing families to stay downtown for a more extended period.</p>	<p>Community plazas are multi-purpose, providing the community with a space to eat, congregate, and enjoy time together. Plazas are also equipped to accentuate areas where people can spend more time and enjoy the attributes of Downtown.</p>	<p>These gathering spaces are meant for smaller groups, highlighting the opportunity to have private or quiet conversations right in the middle of all the action happening around them.</p>	<p>Integrated gathering spaces are places where people can quickly stop and enjoy the sights, sounds, and experiences of Downtown Hurricane. These amenities should focus on unique elements to draw in users while providing comfort.</p>



#### 4. Incentivize development of mixed-uses for developers acquiring underutilized downtown properties

Incentivizing property owners and/or developers to acquire, renovate or revitalize buildings is often met with a series of roadblocks, perceived or realized. To assist this, we recommend a series of incentives that have various fiscal and non-fiscal impacts, matching the anticipated return on investment (from the municipality) to the elements offered. When offering incentives to owners and developers, non-financial opportunities must be considered first and more frequently than budgetary incentives. Providing municipal officials and developers with a sample spreadsheet of opportunities is often the first step. However, when preparing these sheets, it is important to document how many items of each category can be selected, limiting the fiscal impacts on the overall community. Some sample incentives are as follows:

		AGENCY		
Concession Type	Concession	Municipality	Property Owner	CRA/DDA/BID
Financial	Offer owner financing (purchase)			
Financial	Provide Sq. Ft. cost for rehab work (maximum)			
Financial	Offer facade or signage construction assistance			
Financial	Partner on grant applications to DEP			
Financial	Provide options for low-interest collective loans			
Financial	Offer tax abatement for necessary businesses			
<b>Select 2 from above</b>				

Figure 31: Financial Incentives

		AGENCY		
Concession Type	Concession	Municipality	Property Owner	CRA/DDA/BID
Non-Financia	Offer business marketing assistance (digital)			
Non-Financia	Allow sub-let of unused space			
Non-Financia	Market and maintain database of properties			
Non-Financia	Organize community clean up days			
Non-Financia	Coordinate downtown-wide sales or events			
Non-Financia	Create a “shop local” spreadsheet for retail			
<b>Select 2 from above</b>				

Figure 32: Non-Financial Incentives



## 5. Work with local realtors to create a downtown properties marketing database

Helping property owners market their available buildings in a user-friendly and concise manner is an essential step towards increased utilization.

It is recommended that the City partner with local realtors to prepare and market files or dedicate a municipal staff to serve as the single point of interaction, empowering downtown property owners to become more active and engaged.

Below is a series of actionable goals to achieve this recommendation:

- ✓ **Create a customized website (custom website or database)**
- ✓ **Develop an interactive map of Downtown properties.**
- ✓ **Database links for assessor and parcel data**
- ✓ **Downloadable property flyers (marketing or one-off sheets)**
- ✓ **Contacts for leasing/owner agents**

### 201 E. 7TH ST

**Community details:**  
The City of Walsenburg is a diverse community with a structurally intact downtown corridor boasting unique architectural styles. Additional socio-economic or demographic factors are outlined below:

- Average median household income of \$29,534
- 19% of residents hold a bachelors degree or higher
- Average age of 40.6 years old
- An average of \$4,498.40 spent annually on "Food away from home"
- 15.1% of residents work from home
- Underutilized services include:
  - Building materials and supplies
  - General merchandise and clothing stores
  - Furniture and home furnishings stores
- Community has a regional draw and three access points to I-25 within 3 miles of downtown.

The building is in acceptable condition and includes many of the fixtures or equipment in the purchase price. Since the building has been vacated, the structure has been maintained with utilities left turned on, preserving the structure.

- Large parcel of land available for development
- Primary structure design purpose was educational
- Property can be re-zoned to fit redevelopment or rehabilitation purposes
- 50,000+ sq ft of space for rehabilitation



#### PROPERTY HIGHLIGHTS

- 1,100 vehicles drive past the property daily
- Ample space for redevelopment or reconstruction
- Convenient access to downtown and interstate

#### BUILDING HIGHLIGHTS

- Available lots can provide ample parking opportunities
- Strong masonry shell can support many uses
- Retail and frontage windows to support a myriad of end users

— Page 2 - 415 Walsen Ave —

Partially funded by USDA Rural Development

Figure 33: Property Database Document Example

Helping prepare this type of marketing program is not cheap or quick, yet it is effective in quickly achieving more utilized buildings. To help streamline this community effort, due diligence reports should be prepared for all underutilized properties. These reports can be linked to the website and offer a downloadable flier for each. Information to be included in the reports includes:

- ✓ **Property address and imagery**
- ✓ **Allowable uses and zoning**
- ✓ **Building information (e.g., age, uses, permits)**
- ✓ **Daily traffic counts**
- ✓ **Floor plans (if available)**
- ✓ **Succinct tax and assessor information**
- ✓ **Community data or socio-economic information**

Preparing these reports will provide property searchers and developers with applicable data for lease and purchase opportunities within Hurricane. With this information, the awareness of the current community conditions and market can be shared to a broader reach.



## 6. Implement an annual facade program

Facade programs are often the most profitable investment a small to medium-sized community can implement, helping drive revitalization. The annual facade program recommendation is to identify potential funding sources, prepare a necessary framework, and implement a matching facade program.

Details about the proposed program are outlined below:

- a. Utilize a USDA or CDBG funding application to provide seed funds for the projects.
- b. 50/50 match, up to \$10,000 per addressed property
- c. Eligible elements include:



The community can begin revitalizing Downtown properties through this program. Additional consideration should be given to areas along the alleyways, encouraging property owners to create “flow-through” businesses, increasing alleyway access, and driving safe pedestrian circulation. Below is the proposed priority map for facade improvements:

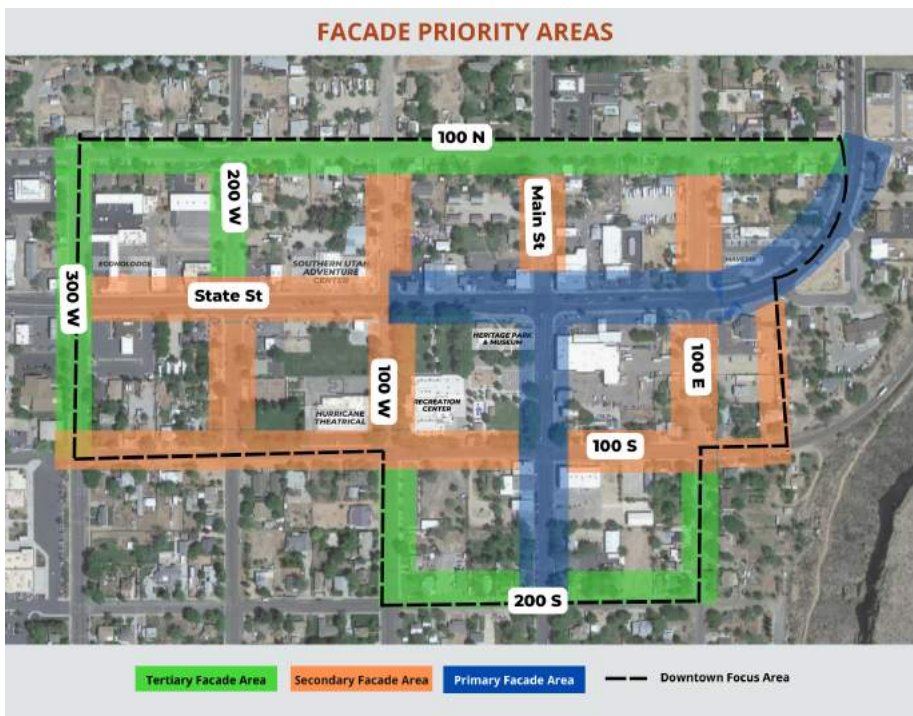


Figure 34: Property Database Document Example

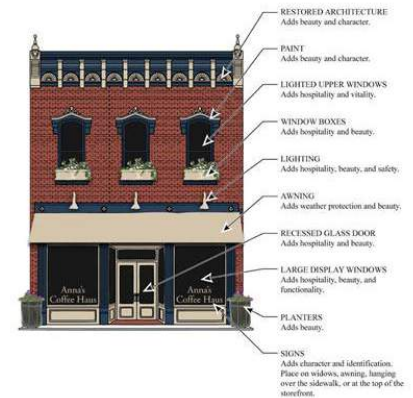
The proposed facade grant program participation process includes the following:

1. Preparation of a simple 2-page application (including cost estimates or contractor quotes)
2. Scoring and ranking of properties based on leveraged funds, project readiness, and community impact.
3. Grant agreement preparation and approval
4. Expenditure of funds and reimbursement every month

The graphics below highlight various façade elements.

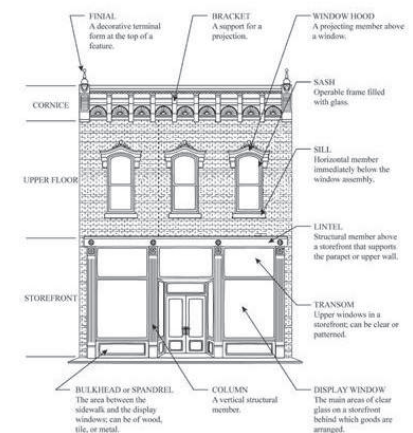
### Facade Improvements - Quick Solution Guide

Simple, low cost improvements that help make a business more attractive



### Architectural Elements - Commercial Building

Basic features of a mixed-use building within a traditional main street area



Above images provided in appendix section in larger size



## 7. Preparation and implementation of form-based code, specifically in the Downtown overlay zone

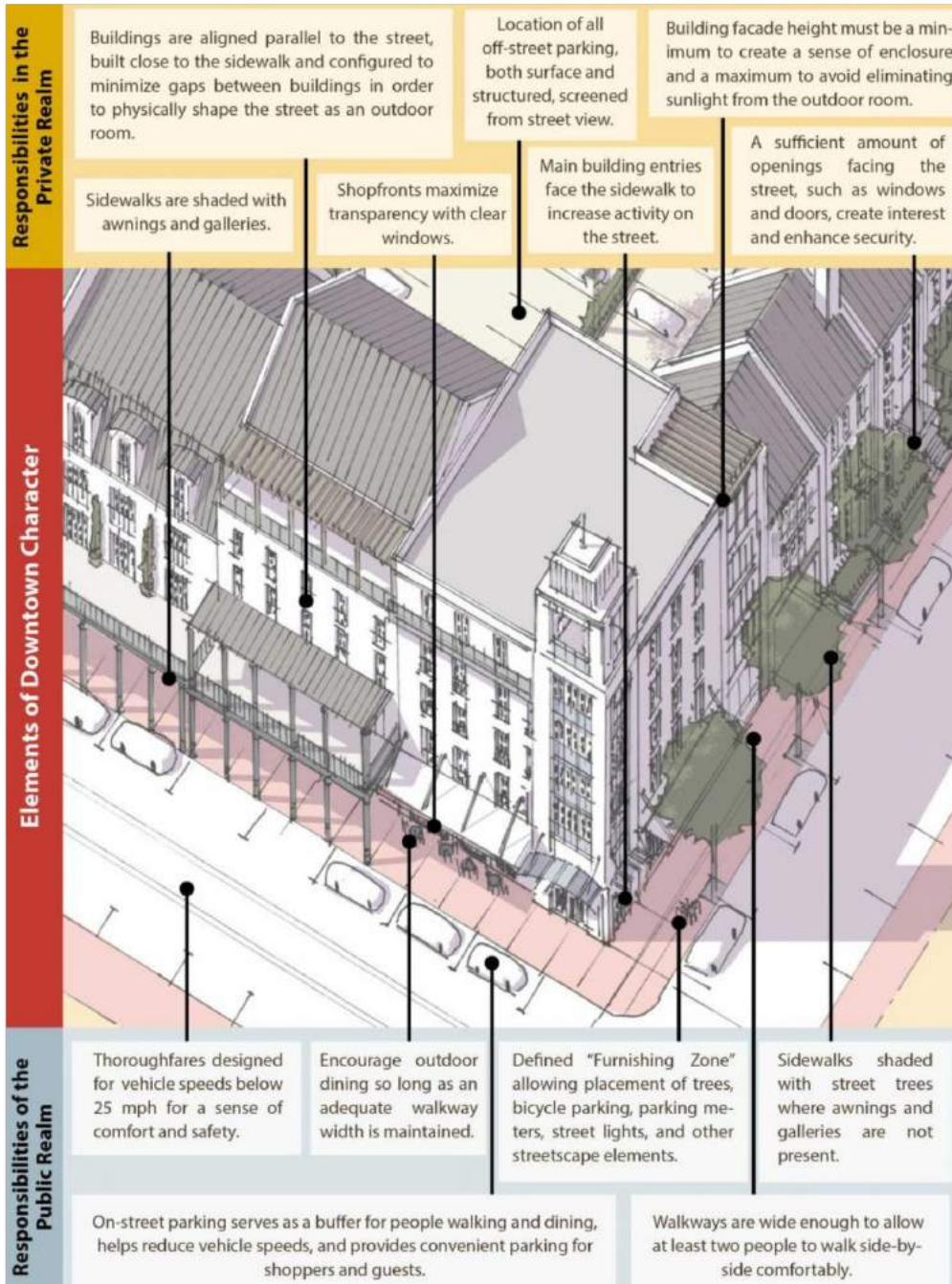


Figure 35: CNU Form Based Code Explanation

As a downtown district grows, adapts, and evolves, it will require flexibility in the types and styles of buildings provided. While the historical character is of critical importance, the forming and massing of a building can change while retaining the same character.

To support this, it is recommended that a form-based code be prepared and implemented within the proposed downtown overlay zone. The overlay zone will allow for enforcement of the form-based codes within the defined area. Implementation of a form-based code in Downtown will result in a hybrid code for the City. Samples of successful form based code can be found at the Wasatch Front Regional Council website.

Form-based code is an alternative to conventional zoning that enables a more predictable built environment. Rather than regulating land uses, form-based code governs the relationship between buildings and the public right-of-way, the form and mass of buildings in relation to each other, and the scale and types of streets and blocks.

Much of the district was developed prior to the adoption or under the conventional zoning, which regulates parking, FAR, and most notably, allowable land uses. Creating a form-based code for Downtown will allow the flexibility needed to support small businesses, promote walkability, enable revitalization, and more. As a result, revitalization will become more common and new development will be more appropriate for Downtown.



## Urban Design Recommendations

Downtown Hurricane should be seen as a destination within the City and the greater region. With the proper design and programming, it can become a true city center. This will require investing in Downtown's public infrastructure, promoting redevelopment, and leveraging its assets and location. The result will be a vibrant place with character and attractive opportunities to invest and grow.



After the initial planning assessment of the city, key opportunities and challenges emerged that needed to be mitigated with urban design solutions. Priorities that emerged include public safety, increased pedestrian space, and multimodal access. We also identified the need for universal accessibility, better visual communication of amenities in Downtown, as well as improved gathering spaces. These aspects have been considered and prioritized in our urban design recommendations and illustrated through the plans, cross-sections, and renderings included in this section.

Lastly, it is imperative that every design decision undergoes oversight by the City's staff and appointed committees, with the ultimate authority for approving or rejecting all design decisions resting with the Council.

1. **Paint highly visible crosswalks.**
2. **Permanently close or create a pedestrian and bicycle-only street on 100 W between 100 S to State Street.**
3. **Explore a HAWK signal and median at 100 W.**
4. **Explore a new traffic signal at 200 W and State Street.**
5. **Create curb bulb-outs at applicable intersections and crosswalks.**
6. **Help facilitate discussions with property owners for shared parking lots.**

7. **Strategically locate accessible parking for equitable access.**
8. **Install parking signage with a maximum of a two-hour time limit.**
9. **Consider creating additional municipal parking lots to support site density development.**
10. **Add curb, gutter, sidewalks, and striped parking along 100 N.**
11. **Install more appropriate (pedestrian scale) lighting and other amenities within the corridor.**
12. **Create additional public gathering spaces adjacent to the ROW space.**

Detailed information about each of the above-outlined recommendations is provided below:



# 01 Paint highly visible crosswalks

Adding bright colors and designs to crosswalks enhances the Downtown visual aesthetic, in addition to the added safety benefits of being highly visible and clearly delineating where pedestrians are to cross the street. These artistic crosswalk installations could be added temporarily or permanently. Temporary options would include using a non-permanent material to install an artistic design for a short period of time. These designs could be painted by local students, be centered around a specific celebration, or another idea.

If it was determined that a more permanent installation was desired, implementing attractive, long-lasting materials (i.e., thermoplastic) within crosswalks at key locations will ensure that residents and visitors can access Downtown in a safe and efficient manner for years to come. Installation of these will require coordination and maintenance discussions with UDOT.

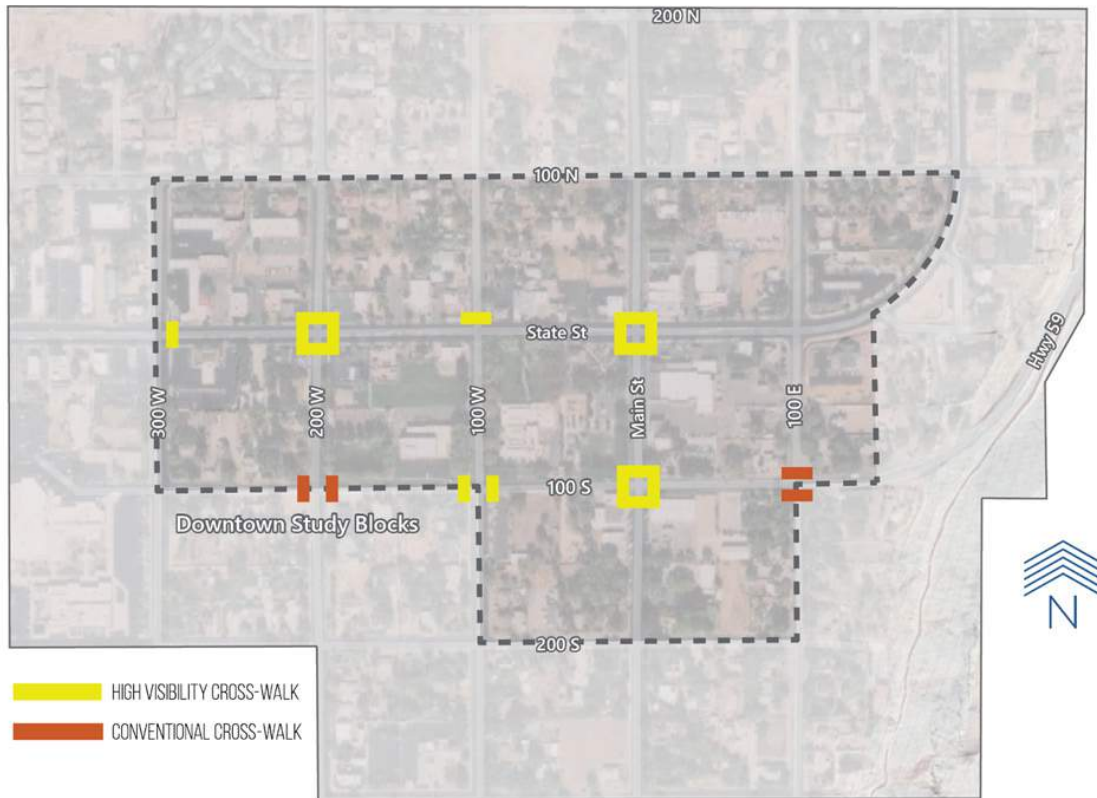


Figure 36: Suggested locations for highly visible crosswalks.



Example of thermoplastic high-visibility crosswalk (Source: Ennis Flint)



Example of thermoplastic high-visibility crosswalk (Source: Ennis Flint)



## 02 Create a pedestrian and bicycle-only street on 100 W between 100 S to State Street

### Closing this segment of 100 W to cars will bring several benefits to Downtown Hurricane:

- **Increased Safety:** *It will create a safer environment for pedestrians, as there are no cars or other vehicles on the road. As a result, there is less risk of accidents and injuries, particularly for children and the elderly.*
- **Provide space for programming:** *This area can provide a designated space for programmings such as farmers' markets, food trucks, fairs, and Peach Days.*
- **Improved Air Quality:** *Closing this street segment to traffic can improve air quality by reducing vehicle emissions.*
- **Reduced Noise Pollution:** *Pedestrian-only streets are typically much quieter than streets with vehicular traffic, which can reduce noise pollution and improve the quality of life for people who live and work in the area.*
- **Increased Foot Traffic:** *Pedestrian-only streets can attract more people to the area, benefiting businesses and the local economy.*
- **Promotes Healthy Lifestyle:** *It can encourage people to walk or bike more, creating more active lifestyles.*

The segment of 100 W between State Street and 100 S is surrounded by several popular amenities such as the Hurricane Recreation Center and Park, Hurricane Theatrical, Pickleball Courts and the Hurricane Valley Heritage Park Museum. During the public outreach for this study, residents expressed an interest in turning this area into a pedestrian-only street to make it safer and more enjoyable for all ages and abilities. The low car traffic numbers in this segment of 100 W coupled with its heavy utilization by pedestrians make it a perfect candidate for a pedestrian and bicycle-only street. Residents expressed their interest in turning this area.



Figure 37: Location of street closure: 100 W between State St and 100 S.

The traffic dislocated due to the closure of this segment of 100 W is expected to minimally impact adjacent roadways. The parking loss can be offset by creating new municipal lots and increasing the angle parking spaces along 100 S. This street segment closure can be achieved incrementally using temporary closures and tactical urbanism (see Implementation Strategies Chapter).



Peach Days Photo by Jeremy Crawford, St. George News



### 03 Explore HAWK signal and median at 100 W



Aerial view of HAWK signal

#### A HAWK (High-Intensity Activated Crosswalk) signal has several benefits, including:

- **Increased safety for pedestrians:** HAWK signals are designed to improve safety for pedestrians by providing a dedicated signal for them to cross busy roads safely.
- **Reduced risk of accidents:** HAWK signals reduce the risk of accidents involving pedestrians by up to 69%, improving visibility and reducing potential conflicts between pedestrians and vehicles.
- **Improved traffic flow:** While HAWK signals are primarily designed to improve pedestrian safety, they can also help improve traffic flow by reducing congestion and minimizing the need for pedestrians to cross the road in unsafe locations.
- **Increased pedestrian mobility:** HAWK signals can improve pedestrian mobility and encourage more people to walk or use other non-motorized modes by providing a safe and reliable pedestrian crossing point.

The location and implementation of these fixtures or amenities will require further review by UDOT, including a corridor study.

A detailed corridor study and signal warrant study on State Street is recommended (see Transportation Recommendations) to evaluate incorporating a HAWK signal and median at 100 W and State Street. A HAWK (High-Intensity Activated Crosswalk) signal is a pedestrian-activated traffic signal designed to help pedestrians safely cross busy roads. When pedestrians want to cross the road, they press a button that activates the HAWK signal. The signal will then display a flashing yellow light, indicating drivers to slow down and prepare to stop. The signal will then display a solid red light, indicating to drivers that they must come to a complete stop and allow pedestrians to cross. Once the pedestrians have safely crossed, the signal will turn off and return to its default state, allowing traffic to resume. HAWK signals are beneficial in areas with high pedestrian traffic, such as school zones or downtown areas. A HAWK signal with a refuge island/median is recommended on 100 W and State Street. The refuge island/median would restrict left turns northbound on 100 W to increase pedestrian safety. The segment of 100 W between State Street and 100 S is recommended to be closed to traffic to create a pedestrian zone (see #2 above), and as a result would not warrant a full signal. An example of a similar HAWK is found on State Street and Sunset Avenue in South Salt Lake (see below).

There have been several studies that have looked at the safety benefits of HAWK signals. One study conducted by the Federal Highway Administration (FHWA) found that HAWK signals can significantly improve safety for pedestrians at mid-block crossings.

- The study looked at pedestrian crossings in four different cities in the United States and found that HAWK signals reduced pedestrian-vehicle crashes by 29% compared to mid-block crossings without any traffic control devices.
- Another study conducted by the City of Tucson, Arizona found similar results. The study looked at pedestrian crossings at mid-block locations with and without HAWK signals and found that HAWK signals reduced pedestrian crashes by 60% and reduced overall crashes by 29%.
- Overall, these studies suggest that HAWK signals can be an effective way to improve safety for pedestrians at mid-block crossings, particularly in areas with high pedestrian traffic.



HAWK signal on State Street & Sunset Avenue in South Salt Lake, UT (Source: Google Street View).



## 04 Explore a new traffic signal at 200 W and State Street

A detailed corridor study combined with a signal warranty study on State Street is recommended (see Transportation Recommendations) to evaluate incorporating new signals along State Street in addition to the existing ones on 300 W and Main Street.



Signalized intersection at 300 W and State Street, Hurricane UT (Source: Google Street View)

A signal at 200 W is recommended due to the traffic leading up to the Hurricane Middle School north of downtown. The public expressed safety concerns in this intersection citing the desire to safely cross north-south which is not currently possible. During school pick-up and drop-off times traffic flow on 200 W is impacted due to the lack of a designated time to cross State Street. Placing a signal at 200 W would enhance traffic flow during peak school pick-up and drop-off hours as well as increase pedestrian safety at the intersection of State Street.

## 05 Strategically locate accessible parking for equitable access

Accessible parking spots are designated parking spaces that are specifically designed for use by individuals with disabilities. These parking spaces are typically located near building entrances and are marked with the international symbol of accessibility, which is a white wheelchair on a blue background. Accessible parking spots are wider than regular parking spaces to accommodate vehicles equipped with wheelchair lifts or ramps, and they are often located near curb ramps or accessible building entrances. In addition, accessible parking spots are usually positioned so that there is sufficient space for individuals with disabilities to enter and exit their vehicles safely. It's important to note that accessible parking spots are reserved for individuals with disabilities who display a valid disabled parking placard or license plate. These spots are intended to ensure that individuals with disabilities have equal access to public facilities, and parking in an accessible spot without a valid permit or placard can result in fines or penalties.



Example of accessible parking stalls at the Salt Lake City Public Library (Source: Google Street View)

People with disabilities often face significant challenges in mobility, and accessible parking spots are designed to make it easier for them to access buildings, services, and amenities. The Public Rights of Way Accessibility Guidelines (PROWAG) states that for every 25 parking spaces on the block perimeter up to 100 spaces, one parking space must be accessible. For every additional 50 parking spaces on the block perimeter between 101 and 200 spaces, an additional parking space must be accessible. Emphasis should be placed on this recommendation after the closing of 100 West.



## 06 Help facilitate discussions with property owners for shared parking lots

There is a general desire within the community for efficient and effective parking. Offering shared parking agreements may provide more efficient and effective parking for residents and visitors alike. Shared parking can be through a lease program where private lot owners can lease parking stalls to individual building tenants or could be through a lease with the municipality for the lot to be used for public parking. Shared parking agreements help business owners communicate to their customers locations where they can park.



## 08 Install parking signage with a maximum of 2-hour time limit

Time-limited parking is a type of parking regulation that limits the amount of time a vehicle can park in a particular location. It is typically used in areas where there is high demand for parking, such as in busy downtown areas, shopping districts, or near public transportation hubs.

The purpose of time-limited parking is to ensure that parking spaces are available for a larger number of people throughout the day. By limiting the amount of time a vehicle can park in a particular space, time-limited parking encourages turnover and allows more people to access the parking space throughout the day. Residents have expressed concern about parking availability in downtown, however a parking occupancy study needs to be performed to accurately evaluate the usage of current parking in the area.

Timed parking, however, can be implemented without this study in areas where parking demand is frequently seen to exceed capacity. A 2-hour time limit is often enough for downtown visitors in locations such as near the Recreation Center, Hurricane Theatrical and Pickleball courts. Parking signs should be posted every 25' of distance.

Timed parking does not necessarily need to be enforced by the city police or staff. It is shown that residents and visitors self-regulate parking time when time limits are posted.



Example of 2-hour parking limit sign (Source: cvilletomorrow.org)



## 09 Create curb bulb-outs at applicable intersections and crosswalks

**Curb bulb-outs, also known as curb extensions, refer to a traffic calming measure that involves extending the sidewalk or curb into the street. This results in a wider sidewalk and a narrower roadway, which can help slow down traffic and increase pedestrian safety.**

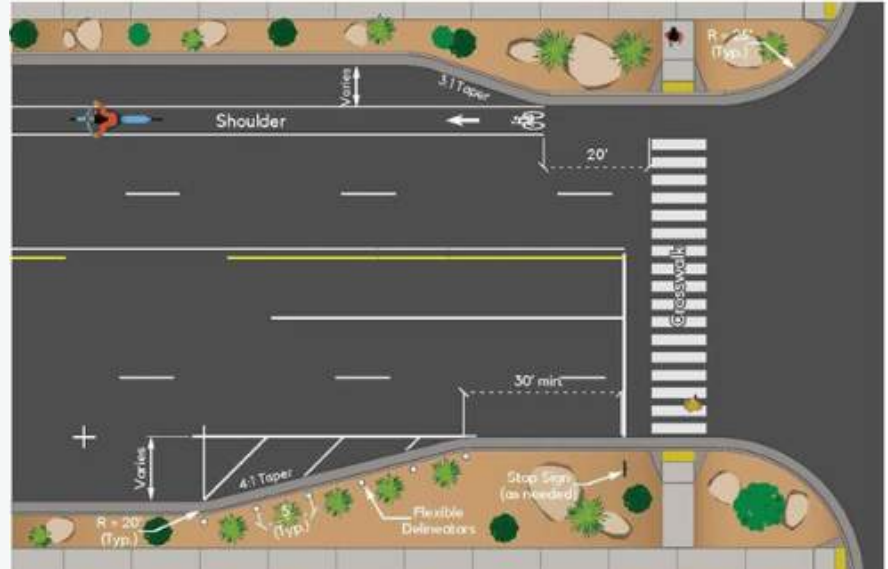


Figure 38: Sample curb bulb-outs design and specifications

Curb bulb-outs are usually created by adding a rounded or angled extension to the existing curb, creating a protrusion that narrows the roadway and shortens the crossing distance for pedestrians. They are often located at intersections or crosswalks and can include features such as benches, planters, or bicycle racks.

Concerns regarding curb bulb-outs include maintenance, vehicle turning radius and ownership. These points are discussed below:

- **Maintenance**

*In areas with year-round growth seasons, it is important for municipalities and property owners to have a clear plan for maintenance and water management. This may involve coordinating with landscape maintenance contractors that utilize specialized equipment or designating specific personnel responsible for cleaning roadways. In general, curb bulb-outs do not present an undue hardship if designed and maintained properly.*

- **Ownership**

*Curb bulb-outs in non-UDOT roads are the responsibility of the city and should be installed and maintained by Hurricane. Curb bulb-outs located within UDOT right-of-way on State Street and Main Street between State Street and 100 S are owned and maintained by UDOT.*

**The location and implementation of these amenities within the ROW will require further review by UDOT, including a corridor study.**

- **Turning Radius**

*When a curb extension is installed at an intersection, it can reduce the amount of space available for vehicles to turn. This can be particularly problematic for larger vehicles such as trucks or buses, which may have difficulty navigating the turn without crossing into adjacent lanes or hitting the curb. A well-designed curb extension has a large enough radius to accommodate larger vehicles and maintain the flow of traffic. In general, curb extensions can be designed to improve the turning radius of smaller vehicles. By reducing the size of the intersection and creating a tighter turning radius, vehicles such as cars or bicycles may be able to navigate the turn more easily and safely. This can be particularly useful in areas with high pedestrian traffic or narrow roadways where vehicles need to make sharp turns.*



Existing curb bulb-out on 300 W and State Street, Hurricane, UT (Source: Google Street View)



In Downtown Hurricane, curb bulb-outs should be considered in every intersection with a crosswalk outside of state routes. The one exception is the intersection of Main Street and State Street where the city and general public have expressed concern with the installation of curb bulb-outs, due to safety and traffic movement patterns. Below are examples of a similar width street and an after rendering.



Figure 39: Samples of curb bulb out imagery - Before or existing and after rendering



## 10 Consider the creation of additional municipal parking lots to support site density development

**Parking is crucial to the success of new developments downtown as it affects the accessibility, convenience, and safety of the area. With projected growth in Downtown, and the potential closure of 100 W to cars, it is important that the city considers additional municipal lots.**



Municipal lot installed by Hurricane City (Source: Google Street View)

Within the past 10 years, Hurricane built a municipal parking lot at the corner of 100 S and 100 W. This lot is a one-way through angle parking lot without time limits that maximizes the utilization of that parcel.

Similar underutilized parcels are present along 100 S and can be considered as potential locations for future parking lots. The design of each parking lot needs to be evaluated depending on the size and proportions of the parcel.

Adding more municipal lots would support site density development moving forward as well as assure long-term sustainability of local businesses by providing convenience, increasing foot traffic, and accessibility in downtown.



Figure 40: Municipal lot installed by Hurricane City (Source: Google Street View)



## 11 Add curb, gutter, sidewalks, and stripped parking along 100 N

Adding curb, gutter, sidewalks, and stripped parking on 100 N provide several benefits to the area. Some of the key benefits include:



### Improved drainage

Curb bulb-outs in non-UDOT roads are the responsibility of the city and should be installed and maintained by Hurricane. Curb bulb-outs located within UDOT right-of-way on State Street and Main Street between State Street and 100 S are owned and maintained by UDOT.



### Better vehicle control

Curb and gutter systems help to guide vehicles along the roadway, reducing the risk of vehicles crossing into opposing traffic lanes or leaving the roadway entirely. This can be particularly important in areas with heavy traffic or where the road is narrow.



### Reduced maintenance costs

By directing water away from the roadway and into drainage systems, curb and gutter systems can help to reduce the risk of damage to the road and other infrastructure. This can lead to lower maintenance costs over time.



### Increased pedestrian safety

Curb and gutter systems create a physical barrier between the roadway and the sidewalk, improving pedestrian safety and reducing the risk of accidents involving pedestrians.



### Enhanced aesthetics

Curb and gutter systems can help to improve the overall appearance of a street, making it look more polished and professional. This can be particularly important in commercial or residential areas where appearances are important.



### Control illegal parking

Residents have witnessed illegal parking along 100 N. Striping parking spaces on the road is a common way to control illegal parking. When parking spaces are clearly marked and designated, it becomes easier for drivers to understand where they can and cannot park. This helps to reduce the risk of illegal parking and improve overall traffic flow.

Parallel parking is recommended on 100 N due to the residential characteristic of the area and lower parking demand compared to other areas downtown. Parallel parking allows for a designated bike lane to be painted adjacent to it (see cross-section on page 68). A bike lane is proposed on 100 N by the Hurricane City Active Transportation Plan. A sidewalk minimum width of 6 feet is recommended to allow people to walk side-by-side and provide enough space for people with mobility devices. Sidewalks might be up to 10 feet wide with or without park strips. Refer to Figures 44 and 45 for recommended roadway element dimensions.



100 N between Main Street and 100 W where there is no curb and gutter, and parking stalls are not designated (Source: Google Street View).



100 N between 100 E and Main Street where there's curb and parallel parking



**12**  
**Install more appropriate (pedestrian scale) lighting and other amenities within the corridor**

Street lighting aims to create an inviting atmosphere during the night, avoiding unshielded lights that shine light horizontally into the sky.

The best street lighting conforms to dark sky standards and is focused downtown onto the surface of the street and/or sidewalk. Downward shining lights are safer for drivers and pedestrians because there is less glare.

In addition, downward facing lights do not cast dark horizontal shadows across the landscape that the eyes cannot permeate due to the glare, lessening criminal activity by creating a more visible nighttime environment.

Lighting should be installed that is appropriately scaled for pedestrians utilizing the corridor and the light features should not feel as if they are dominating the corridor.



**13**  
**Create additional public gathering spaces adjacent to the ROW space**

As mentioned above in the Built Environment Recommendation #3, public areas in Downtown that can be reclaimed for gathering and activity will ensure that Downtown is fully utilized. The integration of these spaces also provides a safe respite for pedestrians by bringing them back away from the ROW and any fast-moving traffic.

This is especially successful when the gathering space is immediately located next to the ROW space. These activities along the pedestrian walkway also encourage full use of the corridor, bringing pedestrian traffic to and from block edges. Please reference Figure 30 to see proposed gathering sites within the downtown study area.





## Transportation Recommendations

Downtowns are often seen as the heart of a city or community, and it is important that they are accessible to all individuals, regardless of their preferred mode of transportation. To achieve this, it is important to provide a range of transportation options that are safe, efficient, and convenient for all users.

This section's recommendations include infrastructure improvements, such as the installation of bike lanes, sidewalks, and parking as well as the interventions that increase safety at key intersections.

By implementing these recommendations and prioritizing equitable and inclusive transportation options, Downtown Hurricane can become more accessible and welcoming to all individuals, while also driving economic growth and sustainability. A list of proposed recommendations is as follows:

1. **New corridor and signal warrant study on State Street**
2. **Install wayfinding / parking signage**
3. **Install bicycle facilities and sidewalks throughout downtown.**

4. **Convert parallel parking to angled parking and restripe parking along 100 S to mitigate closure of 100 W.**
5. **Increase crossing time at the intersection of State Street and Main Street**
6. **Add a 4-way stop sign at the intersection of 200 W and 100 N**

Detailed information about each of the above-outlined recommendations is provided the following pages of this section:

### 01 New corridor and signal warrant study on State Street

It is recommended that Hurricane City partner with UDOT in developing a State Street Corridor Study to better define the vision of the corridor into the future. A corridor study can have several benefits for transportation planning and development of State Street and Downtown Hurricane, including:

#### Improved safety

*A corridor study can help identify safety concerns and provide recommendations for improving safety, such as adding new traffic signals, pedestrian crossings, or bike lanes.*

#### Increased efficiency

*A corridor study can help to identify bottlenecks and congestion points and provide recommendations for improving traffic flow and reducing travel times.*

#### Better coordination

*A corridor study can help to identify opportunities for coordination with other transportation modes, such as transit, bicycles, and pedestrians. This can help to create a more efficient and effective transportation network.*

#### Improved accessibility

*A corridor study can help to identify barriers to accessibility, such as lack of sidewalks or bike lanes, and provide recommendations for improving access for all users, including people with disabilities.*

#### Enhanced economic development

*A corridor study can help to identify opportunities for economic development, such as adding new commercial or residential developments, and provide recommendations for improving access and connectivity to support economic growth.*

#### Increased sustainability

*A corridor study can help to identify opportunities for sustainable transportation solutions, such as promoting walking, biking, or transit, reducing vehicle emissions and traffic congestion, and improving air quality.*



The corridor study should be prepared in conjunction with a signal warrant study which is used to determine whether a traffic signal is warranted at a particular intersection. A signal warrant study is typically conducted to evaluate the need for a new signal or to assess the effectiveness of an existing signal. The study considers a range of factors, including traffic volumes, pedestrian activity, vehicle speeds, accident history, and roadway characteristics.

If the study determines that a signal is warranted, the traffic engineering team may make recommendations for the installation of a signal, including the type of signal, signal timing, and other related considerations. If the study determines that a signal is not warranted, other traffic control devices or modifications to the intersection may be recommended to improve safety and traffic flow.



Unsignalized intersection of 200 W and State Street in Downtown Hurricane, UT (Source: Google Street View)

**02**  
**Install wayfinding / parking signage**

Wayfinding signage is necessary to help direct residents and visitors around downtown. It is recommended to incorporate integrated and branded downtown signage in addition. Efforts should be made to provide wayfinding signage for pedestrian and vehicular travel at the right locations. Optimal areas for each type are outlined below:

**a. Pedestrian**

Provided at a maximum height of 6' tall, this signage should focus on guiding or programming downtown users through a cultivated experience that downtown offers. This is referred to in the general plan as a self-guided history tour. These signs can provide interactive maps, displays, or other inviting elements to garner additional support.

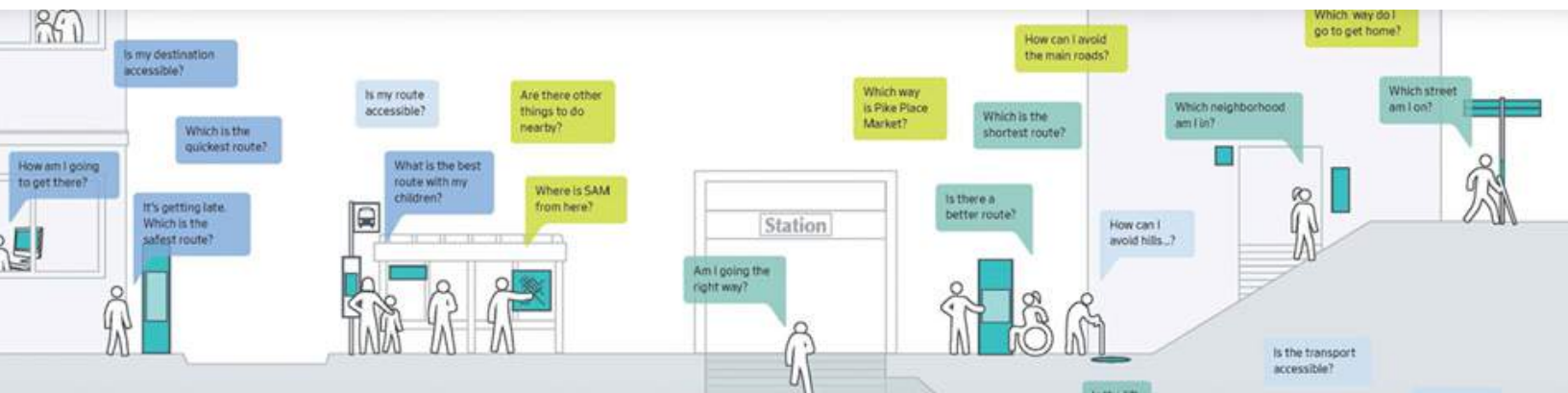


Figure 41: Wayfinding, Source: Seattle Department of Transportation



**b. Vehicular**

This style of signage should be primarily graphics with vibrant or internationally recognized colors. Emphasis should be placed on providing language at a size of no less than 9” in height and no larger than 18” in height.

**c. Business**

This kind of sign helps visitors understand what businesses or services are in Downtown and are typically placed at corners or main entry locations. They can include graphic maps or directional arrows that assist the user in understanding where they are in relation to where they want to go. These signs are good at quickly communicating what the city is known for to a visitor, and helping a resident connect with the identity of their city.



**d. Informational signage**

Information signage is excellent for providing information about events or unique features in Downtown. These signs should stand out yet should also be informational.



**e. Downtown maps**

Downtown maps are a great way to raise awareness of the extent of Downtown, driving more foot traffic from one end to another. Emphasis should be provided on keeping these maps updated with tenants and service/restaurant information.



**f. Parking signage**

Providing wayfinding to simply and easily find accessible parking will help guide downtown users to off-street parking. This signage is essential when it comes to raising awareness with locals. Additionally, State Street signage should include parking tenure signage or business/patron parking delineation.



**g. Historical markers**

These signs are used to denote historical elements or existing buildings. They often highlight the true value and prominence of the historical



events or figures that were around when they were erected. Highlighting and maintaining these feature signs or markers will prolong the character and memory of such history. Residents and visitors alike will understand and appreciate the city in a deeper way.



### 03 Install bicycle facilities and sidewalks throughout downtown

Transportation is more than just pedestrian vehicles, and it must provide equitable access for everyone. In 2021, Hurricane City adopted the Hurricane City Active Transportation Plan which proposed 41.5 miles of new bike lanes on existing roads, including in Downtown Hurricane.

There are numerous benefits of bicycle facilities in a downtown area, including:

- **Increased safety:**

*HAWK signals are designed to improve safety for pedestrians by providing a dedicated signal for them to cross busy roads safely.*

- **Improved health**

*Cycling is a low-impact form of exercise that can help individuals improve their cardiovascular health and overall fitness.*

- **Reduced congestion**

*Bicycles take up less space than cars, so encouraging more people to cycle can help reduce traffic congestion in a downtown area.*

- **Increased accessibility**

*By providing bicycle facilities, downtown areas can become more accessible to people who may not have access to a car, such as low-income individuals or those who cannot drive.*

- **Economic benefits**

*Studies have shown that investing in bicycle facilities can lead to increased economic activity in the surrounding area, as cyclists tend to spend more money at local businesses than car drivers.*

The current study's recommendations build upon the Hurricane City Active Transportation Plan. Each one provides minor few modifications to the plan to ensure bicycle safety. These include:

#### Require minimum sidewalk widths

*Requiring a minimum of 6' width for sidewalks will allow for ample room and accessibility along all ROW thoroughfares. In high-traffic areas, sidewalks wider than 6' may need to be considered, but 6' is the minimum.*

#### Adding bike lanes on 100 W from 100 S to 100 N

*Creates an alternative north-south corridor in downtown to Main Street that provides less conflicts since 100 W is proposed to be a bicycle and pedestrian-only street between 100 S and State Street. It will also connect bicyclists through the HAWK signal on 100 W and State Street which is a safer crossing option than the Main Street and State Street intersection.*

#### Adding marked shared roadways on 100 S from Main Street to 300 W

*This will help reroute bicycle traffic from Main Street into 100 W. It also offers an alternative east-west route from State Street in downtown. Angle parking is proposed along 100 S which makes a shared lane the best option for that corridor.*



Figure 42: Original recommendations from the Hurricane City Active Transportation Plan, 2021.



Figure 43: Proposed Shared Roadway

Current study's recommendations include a few modifications to the Hurricane Active Transportation Plan. To ensure safety, the following design standards are recommended for active transportation facilities in Downtown Hurricane:

*Below cross sections are from the adopted Hurricane Transportation Master Plan and are provided to illustrate the potential uses for half ROW distances, allowing for mixture of purposes on streets*

**Bike Lanes**

Minimum of 6' width, adjacent to the curb or parallel parking. It is recommended that bike lanes are not striped adjacent to angled parking to reduce conflict with cars.



Figure 44: Half street cross-section of a road with angle parking and marked shared roadway.

**Sidewalks**

A sidewalk minimum width of 6 feet is recommended to allow people to walk side-by-side and provide enough space for people with mobility devices. Sidewalks might be up to 10 feet wide with or without park strips. A sidewalk inventory and assessment should be created to prioritize replacements and installations for areas that are in the worst condition.

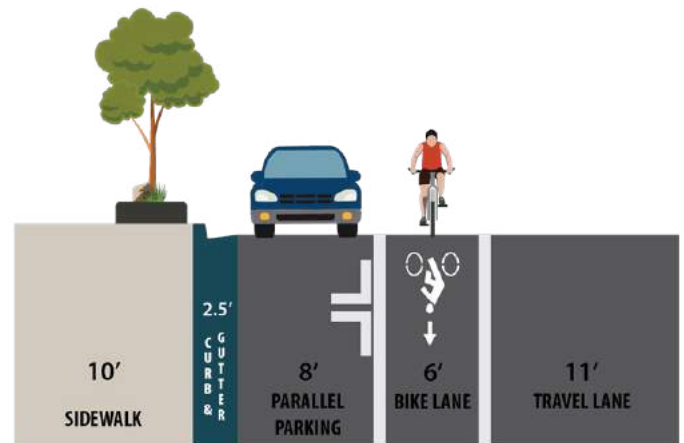


Figure 45: Half street cross-section of a road with parallel parking and bike lane.

**Marked Shared Roadway**

Include "sharrow" pavement marking on 11' minimum travel lanes. Allowing bikes to utilize the full width of the travel lanes makes it safer when adjacent to angle parking which naturally generates more conflict between users.



Bike lanes adjacent to parallel parking on 400 S in Hurricane, UT



## 04 Convert parallel parking to angled parking and restripe parking along 100 S to mitigate closure of 100 W

As parking becomes more valuable downtown, finding alternatives is a priority. Converting parallel parking to angle parking can have several benefits, including:

- **Increased parking capacity**

Angle parking can accommodate more cars in the same amount of space than parallel parking, which can be helpful in areas with limited parking.

- **Improved pedestrian safety**

Angle parking can create wider sidewalks and narrower streets, which can help improve safety for pedestrians and reduce the risk of accidents involving pedestrians.

- **Easier parking**

For drivers who find parallel parking difficult, angle parking can be easier to maneuver into and out of. This can improve traffic flow and reduce the risk of accidents or damage to parked vehicles.

- **Better accessibility**

For people with disabilities or mobility issues, angle parking can provide easier access to buildings and sidewalks, as the parking spaces are typically closer to the entrance.

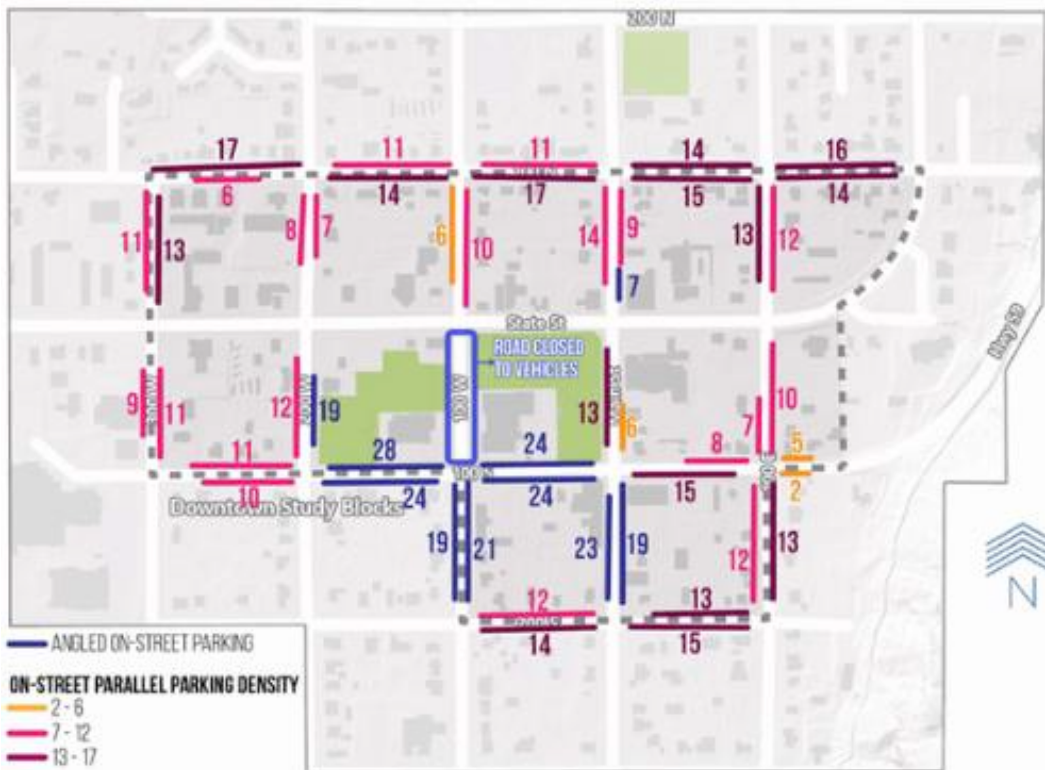


Figure 46: Parking availability in Downtown Hurricane including the creation of additional angle parking on 100 S.

Luckily, there is ample room to stripe parking stalls on 100 S. It is recommended that parking be striped on 100 S between Main Street and 200 W on one or both sides of the road depending on right-of-way widths (refer to Figure 45 above). This will make up for the lost parking due to the closure of 100 W between State Street and 100 S. The map below provides an overall idea of parking availability in Downtown Hurricane including the creation of additional angle parking on 100 S.



## 05

## Increased crossing time at the intersection of State Street and Main Street

During this study and over the years, the city has received several complaints about the crossing time at the intersection of State Street and Main Street. Residents and visitors report that it is too fast and does not allow them to comfortably cross the road before the signal starts flashing.

Increasing pedestrian crossing time at a signalized intersection can have several benefits for pedestrian safety and accessibility. Here are some reasons why it can be important to increase pedestrian crossing time:

- **Safer crossings**

*Longer crossing times give pedestrians more time to cross the street safely, especially for those who may have mobility limitations, such as the elderly or people with disabilities. Pedestrians may also feel less rushed, reducing the likelihood of accidents and increasing confidence.*

- **Reduced stress and anxiety**

*Pedestrians may feel anxious and stressed when trying to cross busy streets, particularly if they have limited time to cross. Longer crossing times can help reduce stress levels and create a more relaxing experience for pedestrians.*

- **Improved quality of life**

*Increasing pedestrian crossing times can improve the quality of life for people who walk or use public transportation. It makes walking and crossing the street easier and more pleasant, which can encourage more people to walk or use public transportation instead of driving. This can have positive effects on health, the environment, and local communities.*

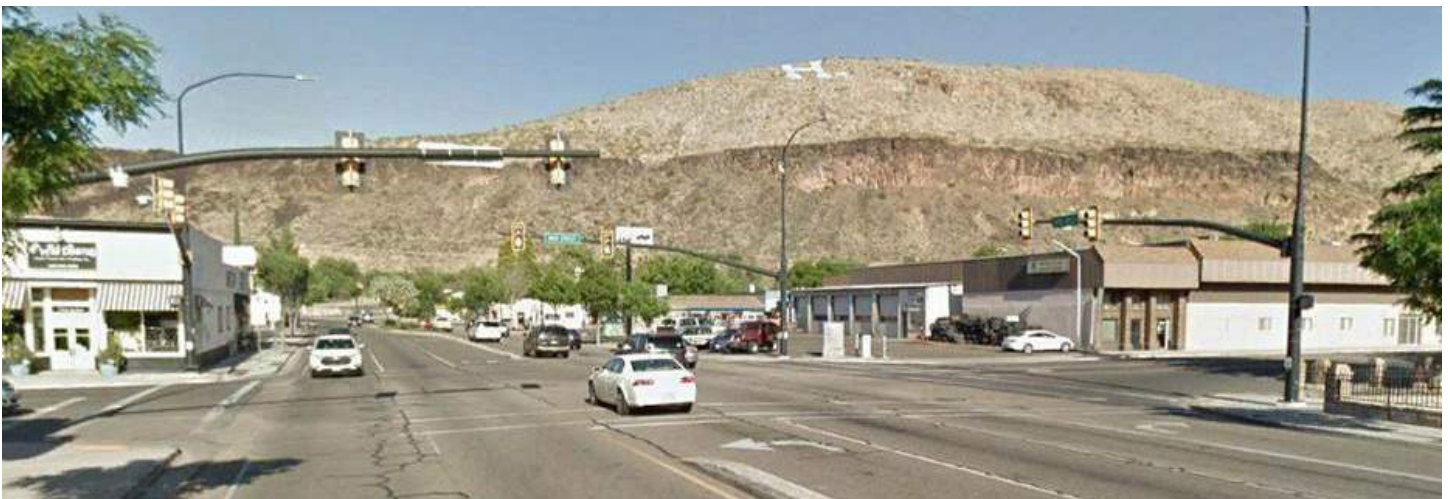
- **Better accessibility**

*Pedestrian crossing times should be increased in areas with heavy pedestrian traffic or where many people with disabilities may need to cross. This makes it easier for people to get around and access public transportation, shops, and services.*

- **Increased compliance**

*When pedestrians feel safe crossing the street, they are more likely to comply with pedestrian crossing rules and wait for signals before crossing. This can lead to fewer jaywalking incidents and reduced pedestrian accidents.*

The signal at State Street and Main Street would benefit from increased crossing time (20 seconds minimum) to accommodate all ages and abilities. The recommended corridor study and signal warrant study on State Street can address signal timing adjustment at this intersection.



Intersection of State Street and Main Street in Downtown Hurricane, UT (Source: Google Street View)



**06**  
**Add a 4-way stop sign at the intersection of 200 W and 100 N**

Currently, there is a 2-way stop sign on 200 W where it intersects 100 N which hinders traffic flow on 200 W leading to the Hurricane Middle School. Changing a 2-way stop sign to a 4-way stop sign can have several benefits, including:

***Improved traffic flow***

*A 4-way stop sign can help to improve traffic flow at busy intersections, as it ensures that all vehicles come to a complete stop before proceeding. This can help to reduce traffic congestion and delays.*

***Increased pedestrian safety***

*A 4-way stop sign can help to improve pedestrian safety at intersections by making it easier for pedestrians to cross the street. By requiring all vehicles to come to a complete stop, it can help to reduce the risk of accidents involving pedestrians.*

***Better visibility***

*A 4-way stop sign can improve visibility at intersections, making it easier for drivers to see other vehicles and pedestrians. This can help to reduce accidents and increase overall safety.*

***Increased safety***

*A 4-way stop sign can help to increase safety at intersections by reducing the likelihood of accidents. By requiring all vehicles to come to a complete stop before proceeding, it can help to prevent collisions that can occur when drivers fail to yield the right-of-way.*

***More predictable driving behavior***

*A 4-way stop sign can help to create a more predictable driving environment, as all drivers are required to stop and yield the right-of-way. This can help to reduce confusion and increase safety for all road users.*



Existing 2-way stop sign at the intersection of 200 W and 100 N in Hurricane, UT (Source: Google Street View)



## Parks & Civic Space Recommendations

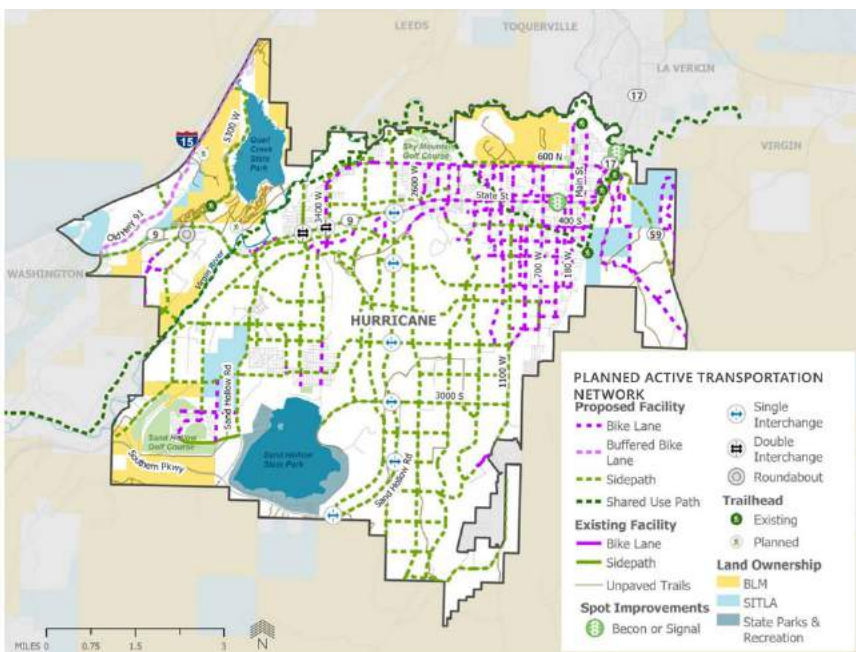
Recreational spaces are where people integrate into a downtown, imprinting on their community and creating memories. Providing access to the unique natural assets Hurricane provides will encourage more visitors and offer more purposeful destinations. Outlined below are sample parks & recreation goals that will help with downtown revitalization.

1. Integrate the Downtown recreational core into the trails network.
2. Prepare additional hard-surface recreational play spaces in Downtown.
3. Integrate a historical or cultural trail through Downtown.
4. Implement a tree & vegetation ordinance for installation and maintenance.
5. Increase the size of sidewalks to encourage longer visit durations.
6. Improve the historic amphitheater and include a new amphitheater within the Targeted Development Scenario.

Detailed information about each of the above-outlined recommendations is provided below:

### 01 Integrate the Downtown recreational core into the trails network

Recreational assets abound throughout Downtown, yet they are disconnected and not easy to access from the State Rd corridor. Making connections through shared use pathways between these locations and the existing and proposed trail network will give residents and visitors alike a more cohesive network to travel throughout the community. The map below outlines the planned active transportation network within Hurricane as identified in the Hurricane 2021 Active Transportation Plan.



Additionally, expansion of the network to incorporate linkages along 200 W will improve connectivity to city-wide and regional recreational assets including: the community pool, regional trails, and community parks. This will provide users with easy and convenient access to other regional destinations, while reducing vehicle miles traveled.

- Jem Trailhead (6 miles)
- Quail Creek State Park (9 miles)
- Sand Hollow State Park (9.5 miles)
- Green Spring Park (16.5 miles)
- St. George Dinosaur Discovery Site (17 miles)
- Zion National Park (23 miles)

Figure 47: Transportation Network



## 02

### Prepare additional hard-surface recreational play spaces in Downtown

Play spaces are a critical amenity most downtowns are missing. While an emphasis is placed on adults' purchases and spending power, there is a missing clientele, specifically the family. Families make up some of the largest expendable income households in any community, yet there are inadequate spaces to encourage lingering or regular visits. So, to encourage additional downtown tenure, it is recommended that passive or active play spaces are placed in Downtown to improve usability.

By doing passive play spaces for children, areas can be used to draw in families for an extended period, driving economic growth. A proposed location map for these types of gathering spaces can be found on the outdoor gathering space map found in the Built Environment Recommendations section. Areas used for the passive plan in downtown should:

- Be sheltered from roadways or crosswalk access.
- Be surrounded by a soft, yet imposing, barrier, such as throned or dense vegetation.
- Be ADA accessible.
- Be limited in size, ranging from 20' to 25' round, and offer a centralized play facility surrounded by safe play surfaces.
- Offer ample seating within the area and immediately adjacent to the area.
- Provide simple play options that are open to all age residents.



Sample access ramp in Hurricane

## 03

### Improve the historic amphitheater and include a new amphitheater within the Targeted Development Scenario

The existing historic amphitheater located behind the Heritage Museum is best utilized by informal meetings and gathering space for children. More formal event meetings and entertainment needs are best met with a larger amphitheater within the plaza space of the proposed State Street development between 100 W and 200 W. It introduces flexibility to Downtown gathering areas, by more easily allowing outdoor seating for restaurants, entertainment settings, event vendors and casual group meetings. With some site grading and allocation of space, this area will gain a great versatile asset. Outlined in the Targeted Development Scenario (next section) recommendations is a description of the use and location.



Bike lanes adjacent to parallel parking on 400 S in Hurricane, UT



## 04 Integrate a historic or cultural trail through Downtown

The City of Hurricane is a diverse community with a rich history. However, there is little programming documenting and celebrating the diversity and history of the community. The City should partner with stakeholders to document the historic and cultural assets that are found within it. With documentation, partners should work together to develop a comprehensive and year-round historic or cultural heritage trail through the City that highlights its culture and attracts visitors regionally. Individuals moving along the trail should have the opportunity to learn about the areas and buildings they are passing by. Historic and cultural facts and stories should be posted on buildings and at appropriate locations along the trail. The trail should also include online interactive connections where individuals can scan a QR code or visit a specific website to learn more about the historic and cultural assets they are passing along the trail.

Below are three other regional walking tour examples:

- Salt Lake City has several self-guided walking tours within the community. The tour routes can be found online, and printed materials are available at locations within the City. The information includes maps of the points of interest on each of the tours and textual information about the locations. Tour topics include:

- *North Downtown Heritage Trail*
- *Main Street Tour*
- *Time Travel on Main Street (designed for kids ages 9-12)*
- *Exchange Place and Market Street Tour*
- *Gateway and Railroad District Tour*

- Provo has a self-guided Historic Buildings Walking Tour. The tour highlights 22 pioneer era buildings that range in construction dates from 1849 - 1910. Information is available online and at locations throughout the community. This information includes detailed descriptions of the historic buildings that are identified on the route.
- Ogden provides a guided tour of Historical 25th Street every first Friday of the month. This tour lasts from 5:30 - 7 PM. Topics along the route include:

- *Pioneer history*
- *Railroad contributions*
- *Legal (and illegal) business history*
- *Architectural styles*
- *And others*



Figure 48: Salt Lake City Example of Points of Interest Map



Figure 48: Salt Lake City Example of Points of Interest Map



**05 Implement a tree & vegetation ordinance for installation and maintenance**

Vegetation is an integral part of any streetscape or public space. Its benefits include noise reduction, pollution control, limited access areas, and reducing the heat island effect. For Hurricane, it is important to provide tiered or purposeful vegetation. The types of vegetation to implement are as follows:

**Barrier vegetation**

*To be placed around children's downtown play areas or other gathering locations, reducing the opportunities for easy wandering.*



**Barrier vegetation**

*Installations are to be placed within the non-walk areas between sidewalks and curbing, promoting safe separation of uses.*



**Tree canopies**

*To be utilized throughout Downtown, encouraging extended downtown participation through heat reduction.*

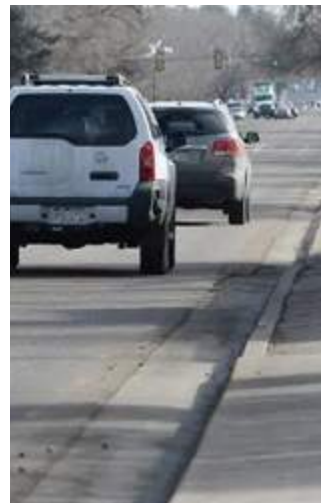


In addition to these vegetation types, special consideration needs to also be given to the inclusion of water-wise landscaping and xeriscape design. Water-wise landscaping and xeriscape design are implemented to minimize the required watering of vegetative installations and to promote water conservation with drier climate areas. More information on these specific types of landscaping can be found in the Green Infrastructure Recommendations section.

**06 Increase the size of sidewalks to encourage longer visit durations**

Larger sidewalks will encourage individuals to visit for longer durations in Downtown. The addition of sidewalk space allows for other positive downtown amenities to be added, upgraded, and included throughout Downtown. Larger sidewalks also accommodate more pedestrian traffic without resulting in congestion as individuals stop to interact with one another or view retail display windows. Additionally, a wider sidewalk increases Downtown pedestrian safety by further separating pedestrians from the road and providing more space between sidewalk users. The additional space also provides the opportunity for users to gather downtown.

Wider sidewalks should be installed, where possible, when new development is created. Sidewalks should be no less than 6 feet in width, but wider sidewalks provide additional benefits. The renovation of existing sidewalk areas would need to be considered on a case-by-case basis and it is likely that a corridor study would need to be conducted. As parking is reduced and/or relocated in Downtown, additional opportunities to expand existing sidewalk areas may arise. Any proposed changes to sidewalks within the state right-of-way would need to be coordinated with UDOT.



Narrow Sidewalk



Wide Sidewalk



## Green Infrastructure Recommendations

Creating a vibrant and attractive downtown also includes planning for the future. This requires taking proactive steps that will provide sustainability, while also providing for increased usage. Recommendations to support this in Downtown are as follows:

1. **Implement xeriscape design for new construction or rehabilitations.**
2. **Design and implement bioswales for Downtown walkways and parking areas.**
3. **Install energy efficient or solar based lighting along non-DOT roadways.**
4. **Maintain and enhance trees and sidewalk level planting areas along the pedestrian way.**
5. **Create small rain gardens within curb bulb-outs and parks downtown.**

Detailed information about each of the above-outlined recommendations is provided below:

### 01 Integrate the Downtown recreational core into the trails network

Xeriscape design is a type of landscaping that is specifically designed for areas susceptible to drought and where water conservation is important. This type of design utilizes minimal water for the success of the plantings. It also eliminates the amount of lawn area within a property, which drastically reduces the amount of water required to maintain a lush lawn area. It also reduces the time and labor needed to maintain a traditional lawn and reduces the need for fertilizers and chemical application.



A variety of plantings and/or decorative rock gardens can be included in xeriscape landscaping designs. In addition to these elements, some designs also include a mixture of cobbles, mulch, pavers, planters, and edging. When selecting plants, it is important to prioritize selecting species that are naturally accustomed to dry conditions and/or native species to the region. Prioritizing this selection criteria will both reduce water consumption and provide a large variety of plants to choose from. Some example plantings include:

- *Big Sagebrush*
- *Blackfoot Daisy*
- *Dwarf Peach*
- *Coral Bells*
- *Gaura*
- *Stonecrop*

Additional plants can be found at <https://tinyurl.com/4he5aduj>





## 02 Design and implement bioswales for Downtown walkways and parking areas

Bioswales are a type of green infrastructure tool that capture, slow and filter runoff related to high intensity rain events. These bioswales are shallow, landscaped depressions that are vegetated. Because bioswales only require limited space, they work well when installed in medians, curb bulb-outs, and other public space areas within a downtown area. These plantings also offer a visually appealing design and add to the overall aesthetic of the



Figure 50: Proposed Bioswale Locations

corridor. Native plants should be utilized in these swales as they can better accommodate the influx of water and drought.

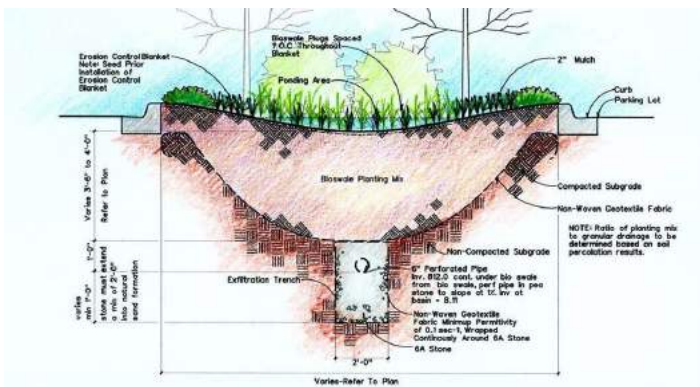


Figure 51: Bioswale Cross-Section and Style Example

## 03 Install energy efficient or solar based lighting along non-DOT roadways

Solar lighting has become more reliable and usable for downtown situations. While there are more restrictions along non-DOT roadways, it is important that the following be considered for municipally owned/operated roadways:

- ☑ Photometrics shows an overlap of no less than 50%
- ☑ Lighting metrics offer an adequate shadow cast to provide uniform lighting.
- ☑ Long-life possibilities should be considered, preferring poles and luminaires with a usable life of 25 years or more.
- ☑ Festival plugs (requiring independent circuits) should be considered.
- ☑ Light poles and luminaires with dual power sources (solar and grid power) should be prioritized for longevity purposes
- ☑ Spacing of solar lights is closer than traditional lights, averaging 100 linear feet apart

Where applicable and meeting municipal photometric standards, Outlined below are some samples of poles that could be considered:



Source - Greenshine Lighting products



#### 04 Maintain and enhance trees and sidewalk level planting areas along the pedestrian way

The City currently has a variety of street trees and other sidewalk level planting areas throughout Downtown. Continued maintenance of these areas will ensure that they continue to positively contribute to the aesthetics of Downtown. Planting additional trees and street level plantings will further enhance the existing vegetative features. When adding vegetative features, the following elements should be considered:

- ✓ **Planting distance:** all plantings should be appropriately spaced to maximize the growth and health of the vegetation.
- ✓ **Planting types:** a variety should be installed together for greater visual appeal.
- ✓ **Tree size:** Trees should be greater than 1 ½ inch diameter.
- ✓ **Planting boxes:** should be in greater than 24 inches.
- ✓ **Root Growth:** Where possible, tree filters or similar root boxes should be installed, allowing for adequate root growth
- ✓ **Overhead utilities:** only small-growing trees or sidewalk level plantings should be installed under overhead utilities.
- ✓ **Sight triangle:** maintaining the defined sight triangle at intersections to not create an obstruction

#### 05 Create small rain gardens within curb bulb-outs and parks downtown

Rain gardens common elements included in downtown. These rain gardens can provide a location for managing and temporarily retaining rainfall events, and these facilities are also attractive and an excellent opportunity for planning that require additional water. Locations for these should be in places where curb-bulb outs are:

- ✓ **More significant than 5' wide and 10' in length**
- ✓ **Where adequate fall and gravity provisions are applicable**
- ✓ **In locations away from transformers or other utility connectors**
- ✓ **In areas where additional buffering or pedestrian projection is desired.**

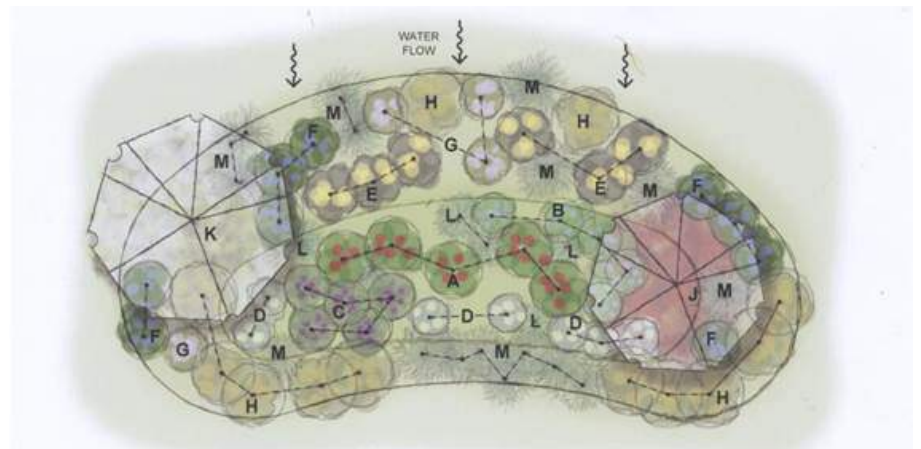


Figure 52: Rain Garden Layout Example



## Housing Recommendations

The key to a successful downtown is getting people into the corridor, and there is no better way to do that than encouraging individuals to live in Downtown. Providing a variety of accommodations within a downtown improves the base of users or taxpayers who utilize the retail establishments. For Hurricane, this would include exploring recommendations that will provide the community with multiple types of housing that would meet the demand for future residents. Recommendations include:

1. **Conduct a building inventory.**
2. **Create upper-floor residences utilizing the overlay district.**
3. **Create upper housing opportunities on State Street between 100 W and 200 W**
4. **Allow multi-family housing to be built to zero lot lines.**

5. **Provide zoning changes allowing a variety of residences in Downtown and surrounding area.**
6. **Encourage attainable housing.**
7. **Reduce parking requirements.**
8. **Craft, adopt, and enforce a short-term rental ordinance for Downtown.**

Detailed information about each of the above-outlined recommendations is provided below:

### 01 Conduct a building inventory

Preparing buildings and business inventory will help raise awareness of what facilities and services are available downtown. These inventories are focused on documenting the current conditions and availability, helping to document how to maximize the space available. The database can be a simple excel sheet or use a more specific CRM software. It is recommended that this list be prepared and regularly updated, categorizing the following:

- Age of buildings**
- Condition of buildings**
- Utilized or unused spaces**
- Owners' information**
- Business name and contact information**
- Social media handles**
- Business owner contact information**

Emphasis should be placed on documenting businesses not only in Downtown but throughout the community. Online and non-brick-and-mortar companies should also be cataloged. There are numerous ways to gather this data, yet the most user-friendly is through self-provision utilizing a survey portal. After the initiated datasets have been collected, an AmeriCorps vista can be hired or used for the year to supplement the list with door-to-door or inline database data points. Software is available to manage this information (such as maestro), but the database must first be created to have importable information. By completing the proposed building and business inventories, the community will be provided with the following benefits:

- ***Increased awareness of downtown merchants***
- ***Improved access and connection with building and business owners***
- ***Opportunities for introductions and cross-promotions***
- ***Increased reach for sales and marketing efforts***



## 02 Create upper-floor residences utilizing the overlay district

Housing in Downtown forms a symbiotic relationship with businesses. This relationship provides enterprises with “doors” or “roofs” that will use the amenities, and businesses will attract more high-quality tenants. This also creates more centrally located residences which are a great option for young professionals and senior housing. To gain these positive effects, it is recommended that upper floors be considered for housing options, within the overlay district. Property owners within the overlay district may apply for assisted funding for upper-floor residences. The City should allocate funding for these grants or loans to boost the efficiency and attractiveness of Downtown.

The ideal matrix of upper-floor housing in downtown is as follows:

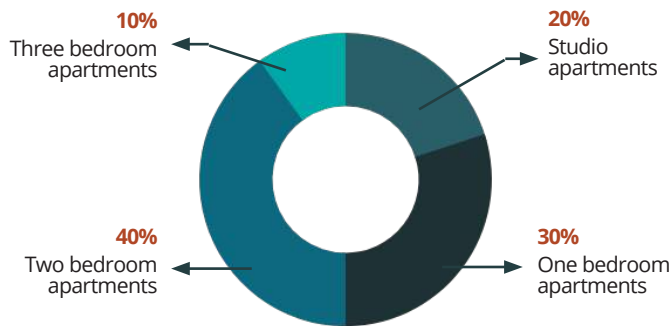


Figure 53: Quoted from the 2019 National Main Street Housing Survey

Some sample floor plans for smaller apartments are outlined below:



Figure 54: Sample Apartment Floor Plan

## 03 Create upper housing opportunities on State Street between 100 W and 200 W

As outlined in the Targeted Development Scenario (next section), there is an opportunity for additional housing on the currently underutilized parcel on State Street. This parcel is envisioned to be two full and one half stories tall and offer various housing options on the upper floors with an internal parking structure. This density will encourage better utilization of Downtown at all times of day and will provide more ‘roofs’ to attract larger businesses to Downtown. A total of 40,000 Sq Ft of residential space could be created, offering the following opportunities:

<b>40 total units</b>	<b>15</b>	<b>16</b>	<b>16</b>	<b>3</b>
	studio apartments at 550 Sq. Ft.	1 bedroom/1 bath units at 750 Sq. Ft.	2 bedroom/2 bath units at 1000 Sq. Ft.	3 bedroom/2 bath units at 1,350 Sq. Ft.





**04**  
**Allow multi-family housing to be built to zero lot lines**

Providing increased densities in Downtown core and surrounding neighborhoods will ensure that viable housing options can be provided. Yet, it will require alterations to current building practices. It is recommended that downtown properties be allowed to build up to lot lines, matching the community’s existing character. This change will require the following to be amended:

- Zero-lot line construction is a right, not by special condition.**
- Reduction of off-street parking requirements to zero**
- Allowing for increased density massing for two and 3-story buildings (capped at a set height of 34') for structures.**
- Requiring facilities to be ROW or back of sidewalk bordered**

Examples of inappropriate setbacks:



Example of uniform and appropriate setbacks:



Figure 55: Setbacks, Source: West Point, UT Design Guidelines

**05**  
**Provide zoning changes allowing a variety of residences in Downtown**

Ensuring a variety of housing is available in Downtown and surrounding zones of Hurricane will allow for more long-term activation of Downtown. Planning for housing in a changing community or demographic creates a moving target, so it is best to provide valuable developments that will adapt to a communities need, while also providing a “life cycle” housing stock within the community.



Figure 56: Life Cycle Housing

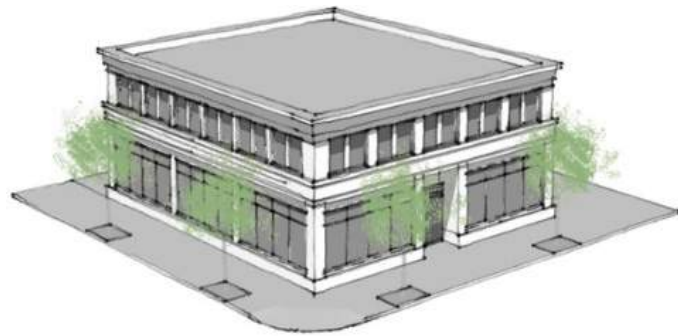
This will ensure that Hurricane can adapt to the community’s changing needs as age cohorts move through their housing cycles, encouraging individuals or families to stay in Hurricane for their entire life.



Outlined below are the proposed housing priorities for Hurricane and their applicable timeframes for implementation:

- **Senior housing and empty-nesters**

In years 1-3, it is recommended to focus on incorporating senior housing into Downtown. This is due to the aging demographic of baby boomers and their soon-approaching transition to non-single-family dwelling units. This type of development requires careful consideration of accessibility and location. All senior housing must be equipped with ADA accessible units, garages, and fixtures, as well as proximity to everyday needs such as groceries and general shopping opportunities. By providing quality amenities in a multi-family complex (or upper floors), Downtown will gain regular patrons, specifically ones that value and expect vital service.



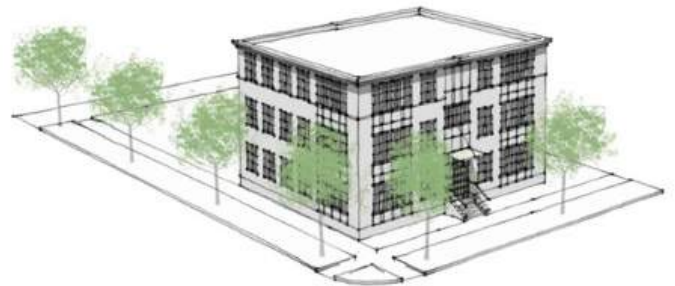
- **Empty-Nester housing**

Also within years 1-3, it is recommended that Hurricane develop the infrastructure for high quality one and 2-bedroom houses that can accommodate families whose children have recently moved out or gone to college. These houses may be new developments or renovated historic homes in the area.



- **Young professional housing**

In years 4-7, it is recommended to focus on young professional housing options. This will drive catalyst changes in Downtown’s economy, providing a need for increased variety in services, amenities, and restaurants. Additionally, this will fill a need for non-single family-based housing for entry-level young professionals. These unit types should include studio and one-bedroom options as well as rehabilitated single-family size homes.



- **Family and transitional housing**

In years 8-14, it is recommended to focus on family or transitional housing options. These housing options are often considered last for downtown housing as they tend to provide the most negligible economic impact during a revitalization process. These unit types should focus on facilities (single-family or multi-family) with 3 or more bedrooms.



**Priority should be given for these properties through incentives or reduced construction costs for municipal fees. This will ensure that equitable efforts are provided to these missing middle housing, as are given to single-family housing**



## 06 Encourage attainable housing

When more considerable site development or redevelopment is considered, it is recommended that possible accommodation be required at a minimum of 20% of occupiable units. Attainable housing is housing that can be afforded, with average household incomes, using the 30% mortgage payment rule, i.e., the mortgage of a standard home should cost no more than 30% of the take home pay for an average family household income. Possible housing varies from low-income or market-rate housing, providing a missing middle housing option for regular families to purchase in a real estate market witnessing exponential property value growth. This can be completed through a series of options, including:

- a. Grants
- b. Tax credits
- c. Reductions in property taxes

A mixture or massing of the above options may be required, yet it should be considered for developers or property owners attempting to provide attainable housing options.



Figure 57: Middle Housing, Source: Missing Middle Housing, 2020

*Supporting the recently adopted moderate-income housing plan, this style of housing is recommended to be permitted within the downtown and surrounding areas*

## 07 Reduce parking requirements

Parking requirements can make it hard to design a vibrant and diverse downtown by prioritizing parking spaces over the safety and comfortability of pedestrians, cyclists, and those taking public transport.

This practice can also impede the ability of a downtown to fully utilize both retail and residential units that maximize economic potential. Reducing parking minimums can help reduce greenhouse gas emissions, while also producing a more attractive urban environment that is used more effectively. To most accurately account for parking needs, this recommendation will require a study to be conducted on regular downtown users and their parking usage.

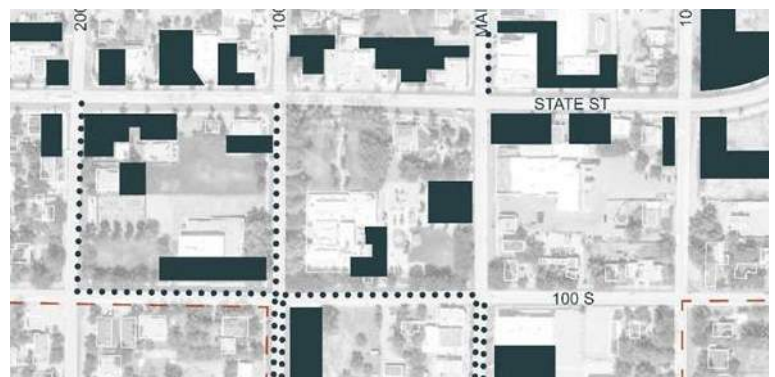


Figure 58: Existing Parking Lots and Angled Street Parking

Considering every building use, occupancy, and proximity to available parking lots, this survey would help determine the exact needs of Downtown in terms of parking stalls. In addition, some businesses may opt to reduce parking by using shared parking agreements. These are most helpful to businesses that require a consistent number of parking stalls, but operate at alternating times of day, relative to other businesses in the agreement. As an interim action step, consideration should be made to 1) reduce the parking minimums, and 2) add the requirement for parking to be located in the rear of each newly permitted structure.



## 08 Craft, adopt, and enforce a short-term rental ordinance for Downtown

Short-term rentals can be an asset to a downtown, as they stimulate economic growth and support local businesses with steady traffic of diverse tourists. However, short-term rentals can produce their own unique challenges. To effectively address the short-term rental needs and current conditions in Hurricane, a thorough assessment must be conducted to understand:

- How many short-term rentals are currently being rented?**
- How large are the short-term rentals in Hurricane?**
- When are they most utilized?**
- How much revenue are short-term rentals generating for the community?**

In addition to understanding the existing conditions, the community will need to identify what role they see short-term rental fulfilling within the downtown. These efforts should strive to provide a balance for considerations of both property owners and neighbors while also filling a regional need for overnight accommodations. Some suggestions for review and implementation are outlined below:

- Within the downtown, limit rental complexes to a spacing minimum of 100 linear feet**
- Limit the number of potential complexes on a roadway facing block**
- Require overnight accommodations have off-street parking facilities**
- Prioritize rentals in redevelopment or targeted development parcels**
- Create a registry or waiting list for high demand areas**





## Targeted Development Scenarios

Based on the feedback received from the public and the assessment of the existing conditions, the consultant team has prepared a series of recommendations for a major catalyst development site. This process involved a survey of the existing assets and challenges, meetings with the steering committee and City, and thorough analysis of public feedback gathered through survey and public input processes.



Figure 59: Proposed Catalyst Site

**The proposed scenario requires the City to acquire an underutilized lot in Downtown. This lot is located on State Street between 200 w and 100 W.**

By acquiring this lot, and envisioning its full potential, the City can pursue a public-private development agreement in which all the necessary assets are provided to Downtown, but the cost burden is distributed to a private developer.

The Catalyst site developments are outlined in the below graphic.



Figure 60: Northwest Corner Aerial



Details are outlined below:

**Catalyst Site:**

- ✓ Between 200 W and 100 W facing State Street
- ✓ Municipal land ownership through long-term lease
- ✓ Master developer agreement for Public Private partnership
- ✓ Relocation of the LDS Family Center within the structure along 200 West
- ✓ Potential for development in future phase(s)
- ✓ Two full stories, and a third half floor that is set back from the street facade (45' Tall)
- ✓ Retail storefront along State Street
- ✓ Residences and offices on upper floors
- ✓ Rooftop Terrace
- ✓ Plaza space and amphitheater for entertainment and events such as small gatherings or concerts and food truck events
- ✓ 50% lot coverage
- ✓ Frontage buildings with side parking structure
- ✓ 40,000 Sq Ft Residential
- ✓ 32,000 Sq Ft Commercial
- ✓ Incorporated 100 Car parking facility



Figure 61: Plan View Render



Figure 62: Northwest Corner Render



Figure 63: Northeast Corner Render



## Economic Development Recommendations

Creating a resilient local economy is one of the most critical components of downtown revitalization any community can undertake. For a downtown, this includes preparing a necessary framework that will bolster the existing amenities, while providing complementary businesses to ensure Downtown remains relevant while also providing flexibility for adaptation. Recommendations are as follows:

1. **Target business clusters for missing amenities.**
2. **Prepare a detailed market analysis and leakage study.**
3. **Offer façade and signage grants.**
4. **Adopt and enforce the international property maintenance code.**
5. **Facilitate a phased master development for municipal properties.**
6. **Diversify cultural amenities.**
7. **Draft and enforce a site development requirement for building permitting in Downtown.**
8. **Site Redevelopment.**
9. **Incentivize property rehabilitation.**

Detailed information about each of the above-outlined recommendations is provided below:

### 01

#### Target business clusters for missing amenities

In this Plan's market analysis phase, several end-users were identified as having a supply gap in the trade areas. These businesses are also opportunities that would fit well into the existing Downtown buildings. Attracting these businesses would provide the community with a necessary addition to the tax revenue and create a regional draw to the State Street corridor for regional attributes. This is done through clustering of development typologies, and the businesses for Downtown Hurricane are as follows, restating some of the start-up and growth opportunities from the market analysis.

#### Pharmacies and Drug Stores



#### Women's Clothing Stores



#### Convenience Stores



#### Jewelry Stores





## 02 Prepare a detailed market analysis and leakage study

One of the most important things any community can undertake is studying the expenditure of residents and visitors, specifically where, when, and what the funds are being spent on. Preparing a market analysis is not simply providing a series of statistics about expenditures within a community, it is a nuanced and detailed review of a community that illustrates deficiencies, over-expenditures, and other critical factors. The City will be able to build on this Plan's market analysis and:

- ✓ **Identify GAPS in services or amenities.**
- ✓ **Document areas where the community is over saturated.**
- ✓ **Create retail user profiles that document spending habits or characteristics.**
- ✓ **Define areas or geographic regions primary visitors are traveling from.**
- ✓ **Identify unique attributes about community expenditure habits.**
- ✓ **Prepare strategies for business expansions.**

Through this process, a NAICS-level analysis should be created to help identify beneficial targeted clusters. These clusters should be identified for locations within the community, providing a mixture of amenities or services that would benefit from a symbiotic relationship. Samples of targeted clusters are:

- **Women's clothing stores**
- **General Merchandise stores**
- **Jewelry stores**

Through these clustered developments, the community will be able to witness a catalyst style of development that is self-sufficient. As outlined in the above cluster, many of these facilities would serve a relationship with one another, driving increased uses that are consistent and not event driven. All efforts should be made to provide elements that are maximally productive. This process will require further development and revisitation in the future to build upon the market analysis provided in this document.

## 03 Offer façade and signage grants

As part of recommendation #6 from the Built Environment Recommendation section, a grant application system should be implemented to encourage upkeep of facades in Downtown. This process will require funding to be allocated from outside grants, or from the annual budget.

One great source of funding for this process is the USDA Rural Business Development Grant program. Towns and communities may apply for grants to be used for economic development, as well as planning and training, and even business incubators. Applications are evaluated by economic need, and consistency with local priorities and goals.

It is recommended that the City reach out to interested business owners in advance to get their proposed construction or rehabilitation plans. This information should be organized and ranked accordingly by need and benefit to the city. Once a list and information about each project is compiled along with additional application requirements, the City should apply as soon as the annual application window is opened. If the grant does not cover all projects submitted by the business owners, those projects have already been ranked, and facade grant plans can be updated accordingly based on the approved USDA grant amount.



Figure 64: Façade Grant Use Example, Source: Urban Redevelopment Authority of Pittsburgh, 2019



**04**  
**Adopt and enforce international property maintenance code**

The international property maintenance code (IPMC) is the industry's standard code enforcement process. This program is utilized by more than 2,000 municipalities across the country and has been found legally defensible regularly in a court of law. Adopting the guidelines and language will provide the community with an easy-to-implement and follow the program, one that will provide a standard of enforcement for all properties. The adoption process for the international property maintenance code is as follows:

- |  |   |
|--|---|
| <ul style="list-style-type: none"> <li>✔ Sign-up for membership and order code books</li> <li>✔ Begin reviewing the materials and redacting unnecessary materials from the code book.</li> <li>✔ Prepare necessary compliance and review paperwork for municipal use.</li> </ul> | <ul style="list-style-type: none"> <li>✔ Set citation or infraction limits, processes, and costs.</li> <li>✔ Municipal review and redaction of undesired or unnecessary sections.</li> <li>✔ Adopt the redacted code through the council.</li> <li>✔ Being implemented uniformly across the community.</li> </ul> |
|--|---|

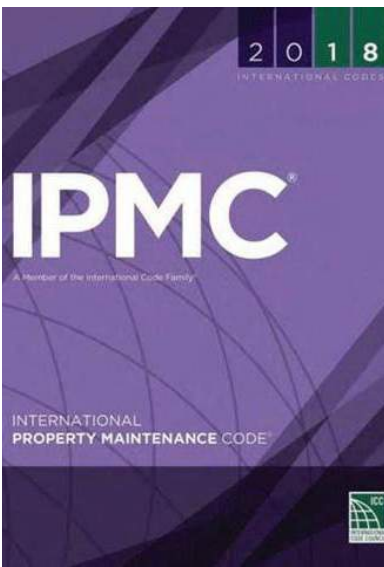


Figure 65: Career Path Graphic, Source: International Property Maintenance Code, 2021





## 05 Facilitate a phased master development for municipal properties

Transforming and revitalizing catalyst properties is one of the quickest and most impactful downtown revitalization tactics. Within the State Street corridor are several underutilized buildings, yet the property between 200 W and 100 W likely has the largest catalyst potential. Some details about the existing site are as follows:

- Located at a prime location for maximum visibility.**
- Only 8,500 Sq Ft of existing structures**
- Opportunity for strategic municipal purchases**

Improvements to the site and facilities are as follows:

- **100,000 Sq. Ft. development area, allowing for multiple uses.**
- **Includes multiple entrances and rentable spaces.**
- **Offers potential alleyway access for deliveries or other necessary uses.**

Based on these characteristics, this property would have the most significant potential to positively impact Downtown. To ensure the property meets underserved needs, the following uses should be included:

PROPOSED USE	SPACE ALLOCATION	LOCATION	PURPOSE
<b>Downtown food hall</b>	5,000	Close to centralized entrance	A food hall is a rotating food preparation facility with multiple smaller stands or facilities. These facilities can both serve as an incubator for growing businesses, as well as provide year-round opportunities for local food trucks.
<b>Ice cream parlor</b>	1,500	High visibility	Nothing says downtown like an ice cream parlor. This facility should offer homemade or local sources ice cream and ample room for people to gather.
<b>Household appliance &amp; electronics store</b>	4,000-6,500 Sq. Ft.	Access to loading	Currently, the community has a shortage in facilities that sell appliances and home electronics. This space will provide convenience for residents to equip their homes from a local establishment.
<b>New &amp; Used clothing stores</b>	2,500-7,000 Sq. Ft.	Front of building	This space would serve as a resource for residents and visitors. With a significant gap in clothing retail stores, Hurricane could quickly reclaim this spending as potential revenue.
<b>Bakery</b>	1,800 Sq. Ft.	Northeast corner	Providing a local alternative to morning snacking and meals will help encourage longer activation of Downtown by both tourists and residents. Having such an amenity in Downtown will create a more convenient variety of shops.
<b>Sporting goods or camping needs</b>	5,000	Near parking	This facility would provide the community with an undermet need, specifically that of camping or non-big box store supplies. Due to the year-round recreation of the Hurricane area, this store is viable all-year round.
<b>Coffee shop/roastery</b>	2,000	High visibility	Currently the community is undermet in facilities for lingering, socializing, and becoming part of the community. This facility, partnered with the bakery, would provide the residents with a necessary space to gather and enjoy their beverages of choice.

Figure 66: Suggested Building Uses

Efforts should be made to purchase and target marketing of parcels to necessary businesses. By acquiring these parcels, the community will be able to make the necessary catalyst changes much more quickly. Such parcels include the Hurricane Car Lot and the properties at 117 and 135 State Street. By purchasing these sites, the City would be the landowner of most of the block, offering the community a strong opportunity for catalyst development through a master developer agreement.



## 06 Diversify cultural amenities

Targeting development of specific amenities that encourage tourism and play into the outdoor recreation culture of Southern Utah will allow the City to reach its full potential of tourism spending. Existing businesses that already do such include Southern Utah Adventure Center and Over the Edge Sports. Additional new and used sporting goods stores and rental businesses will help boost this tourism effort. The City should partner with these existing business owners and help them reach their full potential with strategic location changes and facade or site enhancements.

The City should also consider hosting events that specifically benefit local businesses and craft vendors, as well as tailoring their permitting process to favor business owners with residency in Hurricane. Supporting Hurricane's existing asset businesses and business owners will further develop the identity and pride felt in Downtown.



## 07 Draft and enforce a site development requirement for building permitting in Downtown

Requirements for site development can be hard to regulate and organize. Using site development requirements that include a clear site plan for Downtown developments is a successful strategy for managing what is being designed and included. As part of this process, the City should evaluate what priorities or goals they would like to address within each site. Such goals should pertain to:

- ✓ **Parking**
- ✓ **Landscaping**
- ✓ **Hardscaping**
- ✓ **Entry locations**
- ✓ **Building orientation**
- ✓ **Pedestrian & cyclist amenities**

Once these priorities have been established, the building permitting process can be amended to require that each building have a site plan that clearly addresses such issues. Developing this process will help benefit Downtown by:

- **Creating cohesion within the look and feel of the streetscape and adjacent building designs.**
- **Highlight Downtown's goals and bring them to fruition.**
- **Create an easier understanding of building entrances and wayfinding.**
- **Prioritize the safety and accessibility of pedestrians and cyclists.**



## 08 Site Redevelopment

Reactivation of Downtown anchor buildings will provide one of the strongest ROIs possible and positively impact the overall corridor's economic health. Adjacent to the development outlined within the Targeted Development Scenario in the previous section, some multiple buildings and amenities would benefit from remodeling or rehab. The City Recreation Center and Fine Arts Building have both received heavy utilization by the community and highlight the need for great recreation and civic spaces within Hurricane.

The City will need to thoroughly study both buildings to understand their current uses and the potential for future retrofit or reconstruction that may improve upon these needs. If upgrades are done in conjunction with the Targeted Development, the city will have the opportunity to create an intentionally designed city center and anchor that provides updated recreational amenities for its residents. This can help set the tone aesthetically and architecturally for the rest of Downtown. Designing and implementing these upgrades will also give the community better facilities to meet their needs more effectively.



## 09 Incentivize property rehabilitation

A typical roadblock for property rehabilitation is the perception of unattainable increases in property taxes caused by property reassessments. A property tax deferral can encourage property redevelopment, specifically for municipal-based real estate tax increases. Removing this potential or perceived barrier will aid in building catalyst changes in Downtown. This program should conform to the following elements:

- ✓ **Be provided upon request, not automatically provided.**
- ✓ **Provide tax increase deferral on municipally based real estate tax for five years.**
- ✓ **Allow participants to improve their properties and increase rents before additional revenue is needed.**
- ✓ **Encourage regular maintenance through a rolling 15-year calendar for program participation.**

Implementing the use of GIS software integrated with permitting software, the City will be able to easily track what property upgrades have been done throughout the years by browsing each parcel and owner. Through this rehabilitation program, the community can conduct necessary work and gather additional revenue regularly before being concerned about other tax payments. Additionally, this program will empower property owners to make catalyst improvements with less fiscal impact.



05



# IMPLEMENTATION STRATEGIES





As important as the recommendations are, preparing a priority order or ranking for implementation will ensure the proper steps are completed at the right time. Outlined below are the top prioritized projects and necessary action steps for implementation

## YEAR 0 - 2

1

### ALTER LOCAL ORDINANCES AND ZONING TO REQUIRE RETAIL OR SERVICE-BASED INDUSTRIES IN THE DOWNTOWN CORRIDOR

Land Use Recommendation No. 4

- a. Research best practices from surrounding communities
- b. Craft a set of draft ordinances and proposed changes
  - i. *Identify an area where the ordinance will be implemented*
  - ii. *Determine if current zoning / permitted uses will be followed or if amendments will need to be made to permitted uses within the defined area*
- c. Complete the adoption process with planning and council

2

### IMPLEMENT A VACANT PROPERTY ORDINANCE

Land Use Recommendation No. 1

- a. Research best practices from surrounding communities
- b. Craft a set of draft ordinances and proposed changes
  - i. *Requirements for building use*
  - ii. *Thresholds for vacancy tenure*
  - iii. *What a "used" building entails*
  - iv. *Incentives and penalties related to compliance*
- c. Complete the adoption process with planning and council
- d. Identify, create, and implement tracking mechanisms
- e. Determine penalties for noncompliance and create and enforce enforcement mechanisms
- f. Update vacant property list regularly

3

### PAINT HIGHLY VISIBLE CROSSWALKS

Urban Design Recommendation No. 1

- a. Start with temporary paintings along 100 S in partnership with the community (i.e., high school students, event celebrations, etc.)
- b. Gather funding for permanent thermoplastic crosswalks



<b>4</b>	<b>CREATE A MIXED-USE ZONING DISTRICT, SPECIFICALLY WITHIN THE DOWNTOWN, ALLOWING AND ENCOURAGING A VARIETY OF USES ON UPPER FLOORS SUCH AS DWELLING UNITS, OFFICE SPACES, AND STUDIOS</b>	Land Use Recommendation No. 2	<ul style="list-style-type: none"> <li>a. Research best practices from surrounding communities</li> <li>b. Craft a set of draft ordinances and                             <ul style="list-style-type: none"> <li>i. <i>Identify permitted uses within the mixed-use zone</i></li> <li>ii. <i>Define the permitted density of development in a mixed-use zone</i></li> </ul> </li> <li>c. Update the zoning map to reflect the new mixed-use zone in downtown</li> <li>d. Complete the adoption process with planning and council</li> </ul>
<b>5</b>	<b>CRAFT, ADOPT, AND ENFORCE A SHORT-TERM RENTAL ORDINANCE FOR THE DOWNTOWN</b>	Housing Recommendation No. 8	<ul style="list-style-type: none"> <li>a. Assess existing short-term rentals' current needs</li> <li>b. Identify what the City and the community would like to see.</li> <li>c. Adopt an ordinance specifying the number of short-term rentals allowed in the downtown and what is required of those rentals.</li> <li>d. Allocate funding for enforcement.</li> </ul>
<b>6</b>	<b>CREATE AN OVERLAY ZONE FOR THE DOWNTOWN CORRIDOR (RELATED EXPLICITLY TO FORM-BASED CODES)</b>	Land Use Recommendation No. 3	<ul style="list-style-type: none"> <li>a. Research best practices from surrounding communities</li> <li>b. Identify the areas to be included in the overlay zone                             <ul style="list-style-type: none"> <li>i. <i>Incorporate form-based code into overlay regulations as developed in Implementation Strategy 17</i></li> </ul> </li> <li>c. Craft a set of draft ordinances</li> <li>d. Update the zoning map to reflect the location of the overlay zone</li> <li>e. Complete the adoption process with planning and council</li> </ul>
<b>7</b>	<b>ALLOW MULTI-FAMILY HOUSING TO BE BUILT TO ZERO LOT LINES</b>	Housing Recommendation No. 4	<ul style="list-style-type: none"> <li>a. Establish zero-lot line construction as a right</li> <li>b. Allowing for increased density massing for two and 3-story buildings (capped at a set height of 40') for structures</li> <li>c. Require facilities to be ROW or back of sidewalk bordered</li> </ul>
<b>8</b>	<b>PROVIDE ZONING CHANGES ALLOWING A VARIETY OF RESIDENCES WITHIN THE DOWNTOWN</b>	Housing Recommendation No. 5	<ul style="list-style-type: none"> <li>a. Assess current zoning requirements</li> <li>b. Review and amend the code to allow for increased densities</li> <li>c. Eliminate requirements that prohibit certain housing types</li> <li>d. Rewrite zoning requirements to allow multiple housing types and styles as long as the preferred density is achieved.</li> </ul>



9	
WORK WITH A LOCAL REALTOR(S) TO CREATE A DOWNTOWN PROPERTIES MARKETING DATABASE	<ol style="list-style-type: none"> <li>a. Identify local realtors and create a partnership to implement recommendations.</li> <li>b. Create a list of eligible properties.</li> <li>c. Conduct assessments of the building condition, zoning, potential end-use types, etc. Also, gather owner or listing agent contact information.</li> <li>d. Create marketing materials and flyers for each identified property</li> <li>e. Create a website for available properties</li> <li>f. Share information about available properties with developers                         <ol style="list-style-type: none"> <li>i. <i>Utilize an online platform where interested parties can easily access these marketing materials</i></li> </ol> </li> <li>g. Maintain and update the list regularly</li> </ol>
	Built Environment Recommendation No. 5

10	
UPDATE AND ENFORCE SIGNAGE REGULATIONS	<ol style="list-style-type: none"> <li>a. Research best practices from surrounding communities</li> <li>b. Review existing signage regulations                         <ol style="list-style-type: none"> <li>i. <i>Identify corrections or additions needed to the current regulations</i></li> </ol> </li> <li>c. Craft a set of draft ordinances</li> <li>d. Identify and implement enforcement mechanisms for regulations</li> <li>e. Complete the adoption process with planning and council</li> </ol>
	Built Environment Recommendation No. 1

11	
MAINTAIN AND ENHANCE TREES AND SIDEWALK LEVEL PLANTING AREAS ALONG THE PEDESTRIAN WAY	<ol style="list-style-type: none"> <li>a. Identify existing areas where trees and street-level plantings are already present</li> <li>b. Assess the current condition of existing vegetative areas</li> <li>c. Implement plantings in areas where additional buffering is needed</li> <li>d. Determine ongoing maintenance needs and implement them as needed. Adjust fiscal budgets as required to accommodate necessary maintenance</li> <li>e. Conduct a downtown assessment to identify areas where trees and street-level plantings could be added</li> <li>f. Determine the estimated cost of such installations and budget accordingly</li> </ol>
	Green Infrastructure Recommendation No. 4

12	
CLOSE 100 W FROM 100 S TO STATE STREET	<ol style="list-style-type: none"> <li>a. Lock and reinforce permanent barricades</li> <li>b. Identify locations for, and stripe accessible parking locations</li> <li>c. Create an accessible turn-off for pick-up/drop-off along 100 South</li> <li>d. Remove/reduce curbs along 100W to create zero-grade transitions</li> <li>e. Provide vegetation in pots and moveable items to reduce heat island effect</li> </ol>
	Urban Design Recommendation No. 2



**13**

**REDUCE PARKING REQUIREMENTS, ALLOWING FOR INCREASED DENSITY STRUCTURES**

Housing Recommendation No. 7

- a. Identify current users and parking needs within the downtown
- b. Identify where the required on and off-street parking requirements for existing and new developments should be reduced (based on current and proposed users and needs)
- c. Craft a draft ordinance that updates the appropriate zoning regulations as they relate to on and off-street parking
- d. Complete the adoption process with planning and council
- e. In addition to the proposed ordinance change, locate existing parking lots and structures that can be utilized and potentially shared
- f. Facilitate and encourage shared agreements between businesses where feasible.

**14**

**INTEGRATE A HISTORICAL OR CULTURAL TRAIL THROUGH THE DOWNTOWN CORRIDOR**

Parks & Civic Space Recommendation No. 3

- a. Identify historical and cultural assets throughout the downtown corridor
- b. Create a database with location and background information
- c. Create graphic representation/route of identified assets
- d. Distribute trail routes at places of business, as well as online, so individuals can access it and utilize trail route
- e. Create a QR code or another interactive aspect where individuals can learn more information about an asset as they pass through the downtown

**15**

**OFFER FAÇADE AND SIGNAGE GRANTS**

Economic Development recommendation No. 3

- a. Adopt design guidelines
- b. Identify funding for program implementation and administration
- c. Create a grant application program
- d. Establish criteria for awarding applicants
- e. Allocate funding from outside grant sources or annual budget

**16**

**ADOPT AND ENFORCE INTERNATIONAL PROPERTY MAINTENANCE CODE**

Economic Development Recommendation No. 4

- a. Sign-up for membership and order code books
- b. Review the materials and redact unnecessary materials from the code book
- c. Prepare necessary compliance and review paperwork for municipal use
- d. Set citation or infraction limits, processes, and costs
- e. Adopt the redacted code through the council
- f. Being implemented uniformly across the community



<b>17</b>	<ul style="list-style-type: none"> <li>a. Establish goals for on-site parking, landscaping, hardscaping, entry locations, building orientation, and pedestrian &amp; cyclist amenities.</li> <li>b. Require a site plan as part of the permitting process within downtown</li> <li>c. Create regulations for key goals to be shown on submitted site plans</li> </ul>
<b>DRAFT AND ENFORCE A SITE DEVELOPMENT REQUIREMENT FOR BUILDING PERMITS WITHIN THE DOWNTOWN</b>	
Economic Development Recommendation No. 7	
<b>18</b>	<ul style="list-style-type: none"> <li>a. Adopt an overlay zone within downtown where form-based codes will be implemented (see Implementation Strategy 6)</li> <li>b. Define and outline preferred development standards for construction within the overlay zone.</li> <li>c. Draft proposed guidelines</li> <li>d. Create a mechanism for enforcement of the form-based codes</li> <li>e. Complete the adoption process with planning and council</li> </ul>
<b>PREPARATION AND IMPLEMENTATION OF FORM-BASED CODE, SPECIFICALLY IN THE DOWNTOWN OVERLAY ZONE</b>	
Built Environment Recommendation No. 7	
<b>19</b>	<ul style="list-style-type: none"> <li>a. Coordinate with UDOT about a new corridor and signal warrant study on State Street</li> <li>b. Secure internal funds and external grants (i.e., Dixie MPO) to provide a match for the study</li> <li>c. Help conduct and finalize the investigation in coordination with UDOT</li> </ul>
<b>NEW CORRIDOR AND SIGNAL WARRANT STUDY ON STATE STREET</b>	
Transportation Recommendation No. 1	
<b>20</b>	<ul style="list-style-type: none"> <li>a. Temporary closure (3-4 months) during summer months</li> <li>b. Implement tactical urbanism and programming to increase space utilization (community art, fairs, events, etc.)</li> <li>c. Complete street closure after adoption with the planning commission and council</li> </ul>
<b>CREATE A PEDESTRIAN AND BICYCLE-ONLY STREET ON 100 W BETWEEN 100 S TO STATE STREET</b>	
Urban Design Recommendation No. 2	
<b>21</b>	<ul style="list-style-type: none"> <li>a. Identify areas close to destination entrances that can host accessible parking spots</li> <li>b. Count parking spots within each block</li> <li>c. Implement a minimum of 1 accessible stall for every 25 stalls</li> </ul>
<b>STRATEGICALLY LOCATE ACCESSIBLE PARKING FOR EQUITABLE ACCESS</b>	
Urban Design Recommendation No. 7	
<b>22</b>	<ul style="list-style-type: none"> <li>a. Identify areas where parking demand exceeds capacity, usually near the entrance of destinations and establishments</li> <li>b. Install 2-hour parking limit signs every 25' distance</li> </ul>
<b>INSTALL PARKING SIGNAGE WITH A MAXIMUM OF 2-HOUR TIME LIMIT</b>	
Urban Design Recommendation No. 8	



## YEARS 3 - 7

1.	<p><b>Install parking signage with a maximum of 2-hour time limit</b></p> <ul style="list-style-type: none"> <li>• <i>Identify areas where parking demand exceeds capacity, usually near the entrance of destinations and establishments.</i></li> <li>• <i>Install 2-hour parking limit signs every 25' distance.</i></li> </ul>	Urban Design Recommendation No. 8
2.	<p><b>Increase crossing time at the intersection of State Street and Main Street</b></p> <ul style="list-style-type: none"> <li>• <i>Adjust the crossing time of the signal following recommendations from the State Street corridor and signal warrant study.</i></li> </ul>	Transportation Recommendation No. 5
3.	<b>Add a 4-way stop sign at the intersection of 200 W and 100 N</b>	Transportation Recommendation No. 6
4.	<b>Implement two additional stop signs on 100 N at the corner of 200 W to convert it to a 4-way stop sign. Implement an annual façade program</b>	Built Environment Recommendation No. 6
5.	<b>Implement a tree &amp; vegetation ordinance for installation and maintenance.</b>	Parks & Civic Space Recommendation No. 4
6.	<b>Create downtown design guidelines</b>	Built Environment Recommendation No. 2
7.	<b>Prepare additional hard-surface recreational play spaces within the downtown core.</b>	Parks & Civic Space Recommendation No. 2
8.	<b>Integrate the downtown recreational core into the trails network</b>	Parks & Civic Space Recommendation No. 1
9.	<b>Incentivize development of mixed-uses for developers acquiring underutilized downtown properties.</b>	Built Environment Recommendation No. 4
10.	<b>Install energy-efficient or solar-based lighting along non-DOT roadways</b>	Green Infrastructure Recommendation No. 3
11.	<b>Implement xeriscape design for new construction or rehabilitation</b>	Green Infrastructure Recommendation No. 1
12.	<b>Create small rain gardens within curb bulb-outs and parks downtown</b>	Green Infrastructure Recommendation No. 5
13.	<b>Provide zoning changes to ensure a minimum Floor Area Ratio (FAR) is met.</b>	Land Use Recommendation No. 5
14.	<b>Conduct a building inventory</b>	Housing Recommendation No. 1
15.	<b>Create upper-floor residence allowances utilizing the overlay district</b>	Housing Recommendation No. 2
16.	<b>Encourage attainable housing with deferred taxes or other financial incentives</b>	Housing Recommendation No. 6
17.	<b>Facilitate a phased master development for municipal catalyst site between 100 W and 200 W</b>	Targeted Development Scenario & Economic Development Recommendation No. 5
18.	<b>Create upper housing opportunities on State Street between 100 W and 200 W in conjunction with the Targeted Development Scenario</b>	Housing Recommendation No. 3
19.	<b>Prepare a detailed market analysis and leakage study</b>	Economic Development Recommendation No. 2
20.	<b>Incentivize property rehabilitation</b>	Economic Development Recommendation No. 9



21.	<b>Explore the HAWK signal and median at 100 W</b>	Urban Design Recommendation No. 3
	<ul style="list-style-type: none"> <li>• Run corridor and signal warranty study in coordination with UDOT</li> <li>• Gather funding for design and engineering</li> <li>• Complete design and gather funding for construction</li> <li>• Finalize construction</li> </ul>	
22.	<b>Explore the new traffic signal at 200 W and State Street</b>	Urban Design Recommendation No. 4
	<ul style="list-style-type: none"> <li>• Run corridor and signal warranty study in coordination with UDOT</li> <li>• Gather funding for design, engineering, and implementation</li> <li>• Install new signal</li> </ul>	
23.	<b>Create curb bulb-outs at non-DOT intersections and crosswalks</b>	Urban Design Recommendation No. 5
	<ul style="list-style-type: none"> <li>• Create an implementation strategy for curb bulb-outs in partnership with UDOT</li> <li>• Gather financing for design and engineering</li> <li>• Complete design and gather funding for construction</li> <li>• Finalize construction</li> </ul>	
24.	<b>Help facilitate discussions with property owners for shared parking lots</b>	Urban Design Recommendation No. 6
	<ul style="list-style-type: none"> <li>• Continue to work with current and new property owners on shared parking agreements.</li> <li>• Finalize shared parking agreements.</li> </ul>	
25.	<b>Add curb, gutter, sidewalks, and striped parking along 100 N</b>	Urban Design Recommendation No. 10
	<ul style="list-style-type: none"> <li>• Continue to secure funding for design and construction</li> <li>• Complete design and engineering of the project</li> <li>• Start construction</li> </ul>	
26.	<b>Install wayfinding/parking signage</b>	Transportation Recommendation No. 2
	<ul style="list-style-type: none"> <li>• Conduct a wayfinding plan to determine the location and content of signs.</li> <li>• This plan is best done with the abovementioned lighting and gathering spaces assessment.</li> <li>• Get the plan adopted by the planning commission and council</li> <li>• Start budgeting and securing funding for design and construction</li> <li>• Implement wayfinding and parking signage</li> </ul>	
27.	<b>Install bicycle facilities and sidewalks throughout downtown</b>	Transportation Recommendation No. 3
	<ul style="list-style-type: none"> <li>• Verify locations of bike lanes and marked shared roadways in this map</li> <li>• Implement new bicycle facilities as roadways get re-paved or re-stripped</li> <li>• Finalize implementation of bike lanes and celebrate shared routes downtown</li> </ul>	
28.	<b>Convert parallel parking to angled parking and restripe parking along 100 S to mitigate closure of 100 W</b>	Transportation Recommendation No. 4
	<ul style="list-style-type: none"> <li>• Verify the re-stripping or re-pavement plan of 100 S; if possible, coordinate efforts to simultaneously re-stripe angled parking on 100 S between Main Street and 100 W.</li> <li>• Continue and finalize to re-stripe parking along 100 S between 100 W and 300 W</li> </ul>	



## YEARS 8 - 15

1.	<b>Improve the historic amphitheater, and include a new amphitheater within the Targeted Development Scenario</b>	Parks & Civic Space Recommendation No. 6
2.	<b>Design and implement bioswales along the downtown corridor walkways and parking areas</b>	Green Infrastructure Recommendation No. 2
3.	<b>Increase the size of sidewalks to encourage longer visit durations</b>	Parks & Civic Space Recommendation No. 5
4.	<b>Create mid-sized, hard-surface, community gathering spaces within the downtown</b>	Built Environment Recommendation No. 3
5.	<b>Target business cluster for missing amenities</b>	Economic Development Recommendation No. 1
6.	<b>Diversify cultural amenities</b>	Economic Development Recommendation No. 6
7.	<b>Site Redevelopment</b>	Parks & Civic Space Recommendation No. 6
8.	<b>Consider creating additional municipal parking lots to support site density development</b>	Urban Design Recommendation No. 9
	<ul style="list-style-type: none"> <li>Identify potential parcels for the creation of new municipal parking lots</li> <li>Conduct an appraisal of the property to determine its market value</li> <li>Negotiate with property owners to purchase the parcel at a fair price. This may involve making an offer, counter-offers, and ultimately agreeing on a purchase price.</li> <li>Secure funding to purchase the parcel. This may involve using public funds or seeking grants or loans from the government or other sources.</li> <li>Acquire the parcel. This may involve obtaining necessary permits, transferring ownership, and paying the purchase price.</li> <li>Final design and construction of parking lots.</li> </ul>	
9.	<b>Install more appropriate (pedestrian scale) lighting and other amenities within the corridor</b>	Urban Design Recommendation No. 11
	<ul style="list-style-type: none"> <li>Conduct a lighting assessment to determine key areas where illumination is needed downtown. This may involve conducting a survey or consultation with residents and stakeholders.</li> <li>Start budgeting and securing funding for design and construction.</li> <li>Final construction of new lighting amenities</li> </ul>	
10.	<b>Create additional public gathering spaces adjacent to the ROW space</b>	Urban Design Recommendation No. 12
	<ul style="list-style-type: none"> <li>Conduct a plan for determining the need, location, and types of gathering spaces in downtown Hurricanes.</li> <li>Get the plan adopted by the planning commission and council</li> <li>Start budgeting and securing funding for design and construction</li> <li>Start implementing new gathering amenities and spaces</li> <li>Final construction of new gathering amenities and spaces.</li> </ul>	

# HURRICANE DOWNTOWN

## MASTER PLAN

APRIL  
2023

PREPARED BY



**Downtown Redevelopment Services**  
100 E Main St, Suite 100  
Ravenna, OH 44266



**CRSA**  
175 S Main St, Suite 300  
Salt Lake City, Utah 84101

# AGRICULTURAL PROTECTION AREA APPLICATION

City of Hurricane  
 147 North 870 West  
 Hurricane, UT 84737  
 (435) 635-2811  
 FAX (435) 635-2184

For Office Use Only:  
 File No. 2023-APA-01  
 Receipt No. N/A

Name: Kenneth W. Heaton FCPTD Telephone: (435) 773-5252

Address: 1189 St. James, St. George, UT Fax No. \_\_\_\_\_

Agent (If Applicable): \_\_\_\_\_ Telephone: \_\_\_\_\_

Email: kheaton47@gmail.com

To be included in an agriculture protection area established within Hurricane City land must consist of at least five (5) contiguous acres.

The application shall include:

1. The land in agriculture production that the proposal sponsors wish to become part of an agriculture protection area.
2. Any limits on the types of agriculture production to be allowed within the agriculture protection area;

3. For each parcel of land: (a) The owners of the land contained within the parcel; (b) The tax parcel number or account number of each parcel; and (c) The number or account number of acres as listed on the parcel tax records.

Owners Name(s)	Tax Parcel #	# of acres
<u>Kenneth W. Heaton</u>	<u>164775 (H-3-2-3-344)</u>	<u>5.01</u>
<u>Kenneth W. Heaton</u>	<u>163710 (H-3-2-4-21011)</u>	<u>4.86</u>
<u>Kenneth W. Heaton</u>	<u>146509 (H-3-2-10-4471)</u>	<u>22.71</u>
<u>Kenneth W. Heaton</u>	<u>164742 (H-3-2-3-3315)</u>	<u>8.57</u>

In order for this to be a complete application, it shall include:

- (a) a plat from the county recorder's office showing each parcel of land with each outlined in color to identify proposed area.
- (b) application fee, if any, as outlined on the city fee schedule, available from the city recorder/clerk.

In order to apply for agricultural protection, the area being proposed must be evaluated based upon; **State Code 17-41-305, Criteria to be applied in evaluating proposals for the creation of agriculture protection areas.**

In evaluating a proposal and in determining whether or not to create or recommend the creation of an agriculture protection area the advisory committee, planning commission, and county commission shall apply the following criteria:

1. Whether or not the land is currently being used for *agriculture production*;
2. Whether or not the land is zoned for agriculture use;
3. Whether or not the land is viable for *agriculture production*;
4. The extent and nature of existing or proposed farm improvements; and
5. Anticipated trends in agriculture and technological conditions.

\*NOTE: the term *agricultural production* is defined by state law as follows:

**State Code 17-41-101 Definitions**

“**Agriculture production**” means production for commercial purposes of crops, livestock, and livestock products, the processing or retail marketing of any crops, livestock, and livestock products when more than 50% of the processed or merchandised products are produced by the farm operator.

List the type(s) of *agricultural production* that is currently occurring on the parcel(s) being included:

Fruit (primarily peaches), pecans, pasture - cattle 164775  
163700  
alfalfa pheasants 146509 grazing 164742

In what zoning district is the parcel(s) currently located: \_\_\_\_\_

Is the parcel(s) irrigated? yes / no

Describe the types and number of structures that are located on the parcel(s):

Grainery, feedlot, saddle shed

List the extent and nature of existing or proposed farm improvements:

upgrade irrigation system. Just installed \$5000  
of gated pipe

What do you anticipate to be the trends in agriculture and technical conditions for the *agricultural production* on the parcel(s)?

pressurized irrigation

Signature of Applicants

Date

Kenneth W. Deaton gen ston ECPT 5-11-23

\*\*\*\*\*

(Office Use Only)

Date Received : 5/15/23 Application Complete: YES  NO

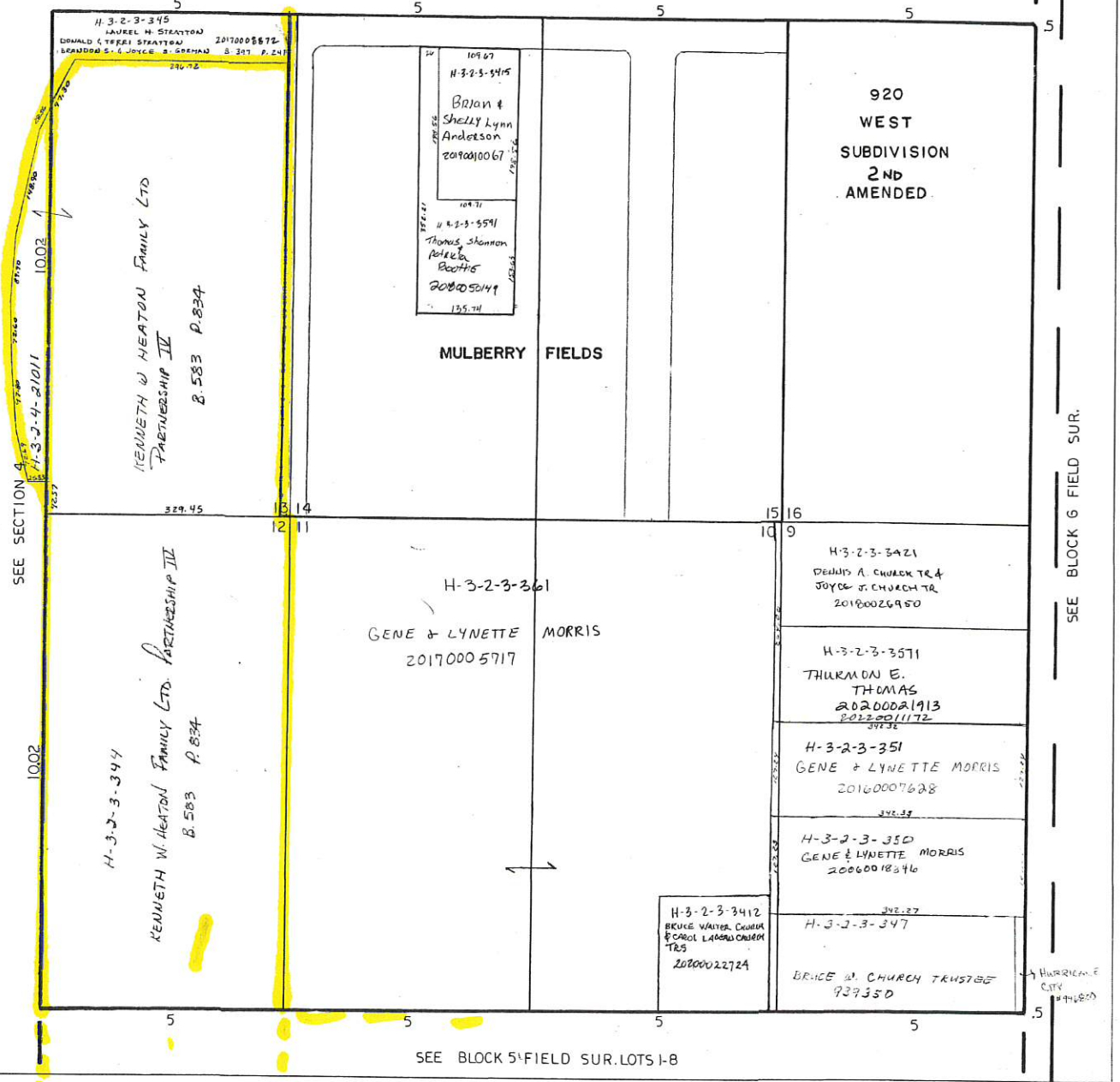
Date application deemed to be complete: 5/15/23 Completion determination made by: B.S.

This plat is furnished for information only. No liability is assumed by Assessor's or Recorder's Office as to correctness of such data.

Washington County Recorder

HURRICANE CITY  
PT. OF BLOCK 5 FIELD SUR.  
SALT LAKE BASE & MERIDIAN  
SCALE: ONE INCH= 100 FEET  
WASHINGTON COUNTY, UTAH

SEE BLOCK 5 FIELD SUR. LOTS 17-24



SEE BLOCK 6 FIELD SUR.

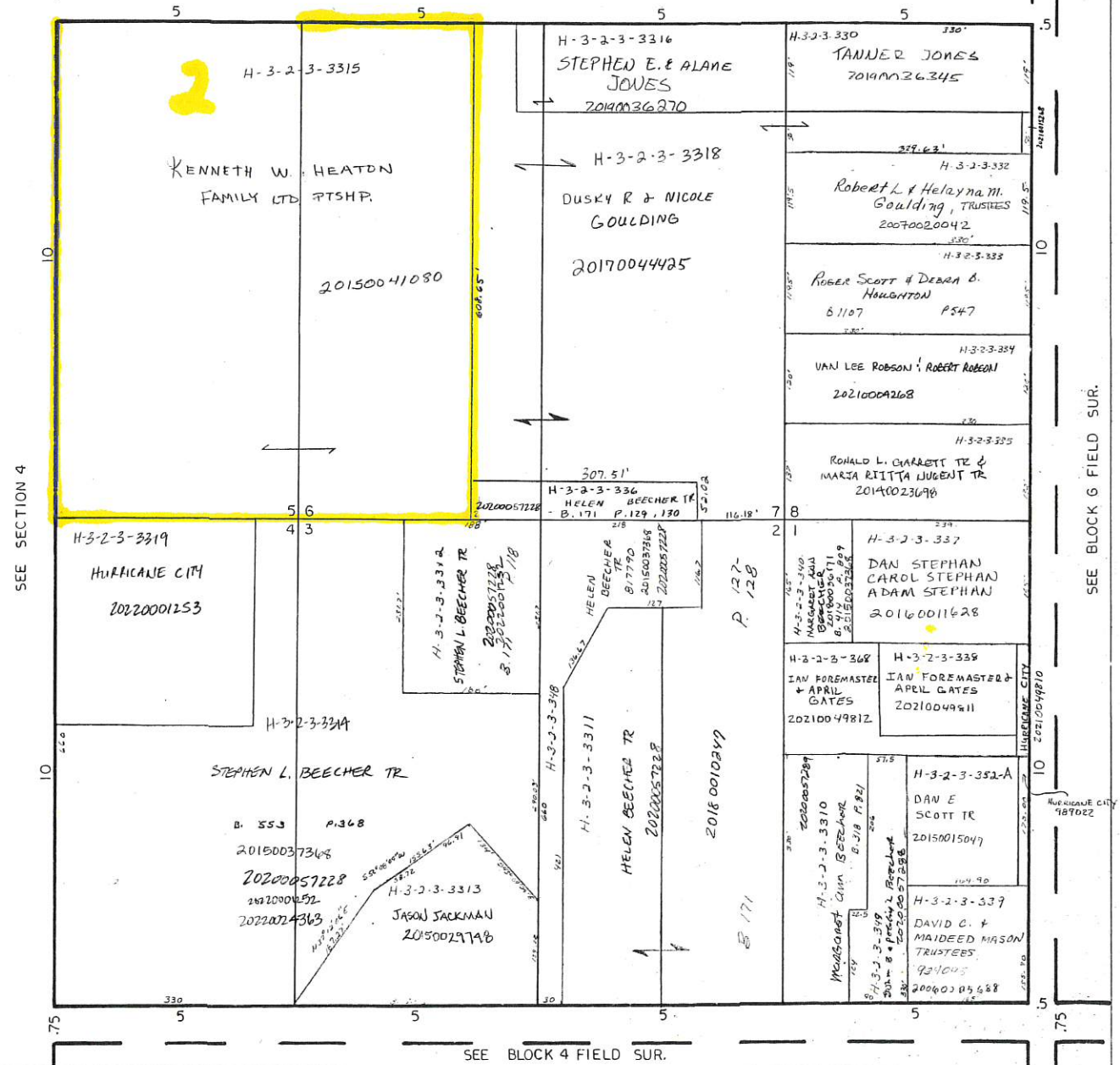
SEE BLOCK 5 FIELD SUR. LOTS 1-8

This plat is furnished for information only. No liability is assumed by Assessor's or Recorder's Office as to correctness of such data.

Washington County Recorder

HURRICANE CITY  
PT. OF BLOCK 5 FIELD SUR.  
SALT LAKE BASE & MERIDIAN  
SCALE: ONE INCH= 100 FEET  
WASHINGTON COUNTY, UTAH

SEE BLOCK 5 FIELD SUR. LOTS 9-16



SEE SECTION 4

SEE BLOCK 6 FIELD SUR.

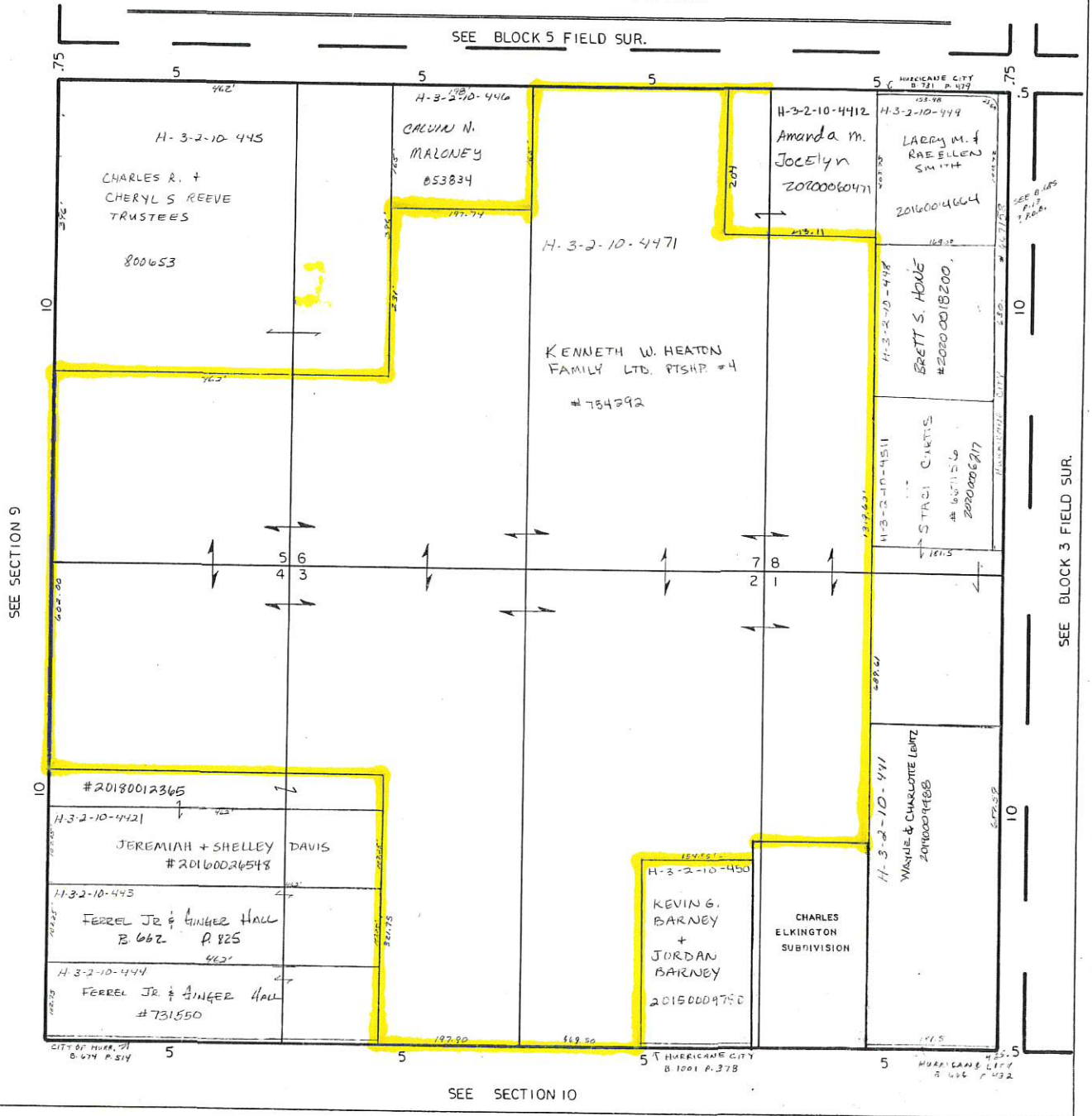
SEE BLOCK 4 FIELD SUR.

This plat is furnished for information only. No liability is assumed by Assessor's or Recorder's Office as to correctness of such data.

Washington County Recorder

HURRICANE CITY  
BLOCK 4 FIELD SUR.  
SALT LAKE BASE & MERIDIAN  
SCALE: ONE INCH= 100 FEET  
WASHINGTON COUNTY, UTAH

SEE BLOCK 5 FIELD SUR.



TAX_ID	NAME	ADDRESS	CITY	STATE	ZIP
H-HUE-2-16	ALAN V GREN ENTERPRISES INC	1018 N SUNSET DR	WASHINGTON	UT	84780-8475
H-MUF-7	ALLRED JAMES L & MARCI N TRS	962 S 990 W	HURRICANE	UT	84737
H-3-2-3-438	ALTON NOLA AUSTIN & RAYMOND WESLEY	535 S 440 E CIR	SAINT GEORGE	UT	84770
H-3-2-3-3415	ANDERSON BRIAN & SHELLY LYNN	1043 WEST 900 SOUTH	HURRICANE	UT	84737
H-HHES-7	ANDERSON CLARK V & MAURINE H TRS	1228 S 840 W	HURRICANE	UT	84737
H-3-2-3-308	BALLARD KENNETH A & JODY A	1095 S 920 W	HURRICANE	UT	84737-2439
H-3-2-10-450	BARNEY KEVIN G, BARNEY JORDAN	952 S 1500 S	HURRICANE	UT	84737
H-3-2-10-423	BEARDALL DANIEL JOHN & JILL SULLIVAN TRS	841 W 1300 S	HURRICANE	UT	84737
H-3-2-3-336	BEECHER HELEN TR	960 W 1300 S	HURRICANE	UT	84737
H-3-2-3-349	BEECHER JOHN B & PEGGY L	940 W 1300 S # 4	HURRICANE	UT	84737
H-3-2-3-340	BEECHER MARGARET ANN	960 WEST 1300 SOUTH	HURRICANE	UT	84737
H-3-2-3-33141	BEECHER STEPHEN L TR	375 W PEBBLE DR	WASHINGTON	UT	84780
H-3-2-3-306	BENNETT TIMOTHY PETER JR & AMANDA JANE TRS	1027 S 920 W	HURRICANE	UT	84737
H-3-2-3-445	BEUTLER MICHAEL & SHELLY	829 S 1100 W	HURRICANE	UT	84737
H-3-2-3-441	BLACK SANDRA L TR	870 S 1100 W	HURRICANE	UT	84737
H-3-2-10-435	BLAU DELRAY R & MONA RAE TRS	114 HICKORY ST	HENDERSON	NV	89015-5217
H-3-2-10-4191	BOHNE ROBERT C & JUDY F TRS	886 W 1500 S	HURRICANE	UT	84737-2538
H-3-2-3-3591	BOOTHE THOMAS SHANNON & PATRICIA	1051 W 900 S	HURRICANE	UT	84737
H-NTW-1	BRINGHURST DOUGLAS K & LISA	980 SOUTH 920 WEST	HURRICANE	UT	84737
H-HHES-8	BULLOCK DALE E TR	1227 S 840 W	HURRICANE	UT	84737
H-WIN-4A	BUTLER KRISTEN TR	782 S 990 W	HURRICANE	UT	84737
H-3-2-3-4314-D	CAHOON KENNETH VERNON & MARCIA	783 S 1100 W	HURRICANE	UT	84737
H-3-2-3-329	CARLISLE NATHAN C, COX TAMARA L	1175 S 920 W	HURRICANE	UT	84737
H-MUF-9	CHANDLER BRIAN K, CAMPBELL- CHANDLER TAMI	914 S 990 W	HURRICANE	UT	84737
H-WIN-5A-1-B-1-C	CHRISTIANSON DAVID J & JULIE J	781 S 990 W	HURRICANE	UT	84737-2575
H-3-2-3-3412	CHURCH BRUCE WALTER & CAROL LADEAN TRS	1096 S 920 W	HURRICANE	UT	84737
H-3-2-3-3421	CHURCH DENNIS A & JOYCE J TRS	992 S 920 W	HURRICANE	UT	84737
H-3-2-3-433-A	CLARK ROBERT W TR	810 S 920 W	HURRICANE	UT	84737-2507
H-3-2-10-41932-A	CLYDE FREDERIC E & KAREN R TRS	870 W 1500 S	HURRICANE	UT	84737
H-3-2-10-4194	CRAM JOHN COREY & NANCY L TRS	1410 S 160 W	HURRICANE	UT	84737
H-3-2-10-4192	CURTIS AMBREA RAE, CURTIS TYSON J	1305 S 920 W	HURRICANE	UT	84737-2534
H-3-2-10-419011	CURTIS JEFFREY D & TONI H	1359 S 920 W	HURRICANE	UT	84737-2534
H-3-2-10-4511	CURTIS STACI TR	1388 S 920 W	HURRICANE	UT	84737
H-3-2-3-303	DANIELS ANN NORWOOD	1239 S 920 W	HURRICANE	UT	84737
H-3-2-3-4315	DAVIS J BRENT & CAROLYN L TRS	754 S 1100 W	HURRICANE	UT	84737-2523
H-3-2-10-4421	DAVIS JEREMIAH & SHELLEY	627 S 600 W	HURRICANE	UT	84737
H-3-2-3-356	DEHART KENNETH L & JANET K	884 W 1300 S	HURRICANE	UT	84737
H-MUF-2	DEMILLE KEVIN, DEMILLE JILL	933 S 990 W	HURRICANE	UT	84737
H-NTW-4	DEMILLE LINDSEY	906 S 920 W	HURRICANE	UT	84737
H-HUE-1-5	DENSLEY RODNEY KIM, DENSLEY LORI ANN	12059 S SHANNEL CIR	RIVERTON	UT	84065
H-HHES-3	DESATOFF ROBIN & RUSSELL	1264 S 840 W	HURRICANE	UT	84737
H-HHES-2	DESATOFF ROXANNE G	1281 S 840 W	HURRICANE	UT	84737
H-CHEL-1	ELKINGTON CHARLES & JESSICA	948 W 1500 S	HURRICANE	UT	84737
H-3-2-10-4212	ENGENESS ARTHUR C JR & TAMMARA A TRS	865 W 1500 S	HURRICANE	UT	84737
H-3-2-10-415	ENHANCED HOME BUILDERS LLC	1460 S WINDSOR PKWY UNIT 4	HURRICANE	UT	84737-1290
H-3-2-4-125	FARHING PAUL N & CAROLA VON HAUSEN TRS	1173 W 650 S	HURRICANE	UT	84737
H-HUE-2-12	FLAVIN JOHN T, SMALLWOOD LINDA A	23060 16TH ST	NEWHALL	CA	91321
H-3-2-10-414	GARNER SANDRA W TR	778 W 1500 S	HURRICANE	UT	84737
H-3-2-3-335	GARRETT RONALD L TR, NUGENT MARJA RIITTA TR	1188 S 920 W	HURRICANE	UT	84737
H-3-2-3-435	GEMMILL ROBERT H & CLAIRE P TRS	1672 N PALO VERDE DR	SAINT GEORGE	UT	84770
H-MUF-10	GENTRY RYAN JOE TR	913 S 1100 W	HURRICANE	UT	84737
H-HUE-1-9	GIBSON MICHAEL & TAMMY	18916 TENDERFOOT TRAIL RD	SANTA CLARITA	CA	91321
H-3-2-4-211	GORMAN JUSTIN N, GORMAN JOYCE S	960 S 1100 W	HURRICANE	UT	84737
H-3-2-4-2131	GORMAN ROWDY G, GORMAN JOYCE S	980 S 1100 W	HURRICANE	UT	84737
H-3-2-3-3318	GOULDING DUSKY R & NICOLE	1130 S 920 W	HURRICANE	UT	84737
H-3-2-3-332	GOULDING ROBERT L & HELAYNA M A TRS	1144 S 920 W	HURRICANE	UT	84737
H-WIN-3A	HADLEY NATHAN & SHAWN	474 W 250 S	HURRICANE	UT	84737
H-3-2-10-444	HALL FERRELL JR & GINGER	1090 W 1500 S	HURRICANE	UT	84737-2530
H-3-2-10-434	HALL LEGRAND T & DONNA L TRS	121 HICKORY ST	HENDERSON	NV	89015-6338
H-3-2-3-4314-C	HANKINS JOHN B & JEANNE S	753 S 1100 W	HURRICANE	UT	84737
H-3-2-3-301	HANSEN CALVIN LEVOY TR	918 W 1300 S	HURRICANE	UT	84737
H-HUE-2-13	HARDESTY SHANNON C	PO BOX 911293	SAINT GEORGE	UT	84791-1293
H-3-2-3-338	HAWKINS AUSTIN TODD, COTTAM AMBERLEE	1242 S 920 W	HURRICANE	UT	84737
H-3-2-3-439	HEIDELBERG S R & LINDA M TRS	810 S 1100 W	HURRICANE	UT	84737
H-3-2-3-4314-G	HESS DALE G & DEBRA D	729 S 1100 W	HURRICANE	UT	84737
H-3-2-10-4170	HILLIER DAVID WAYNE & JOAN MARIE	865 W 1300 S	HURRICANE	UT	84737-2524
H-3-2-3-307	HOLLAND DWAIN E JOSEPH & KATHLEEN TRS	1075 S 920 W	HURRICANE	UT	84737-2439
H-3-2-3-436	HOLLAND JOSEPH & LADAWN	797 S 1100 W	HURRICANE	UT	84737
H-3-2-10-448	HONE BRETT S	1358 S 920 W	HURRICANE	UT	84737
H-3-2-3-333	HOUGHTON ROGER SCOTT & DEBRA B	1150 S 920 W	HURRICANE	UT	84737-2509
H-NTW-3	HOWARD RYAN R & SHELBY	920 S 920 W	HURRICANE	UT	84737
H-MUF-11	HUNT THOMAS WAYNE TR, HUNT MARTHA LYNN TR	933 S 1100 W	HURRICANE	UT	84737
H-3-2-3-3319	HURRICANE CITY	147 N 870 W	HURRICANE	UT	84737

H-3-2-3-314	IMLAY DAVID B & MICHELLE R	1051 S 920 W	HURRICANE	UT	84737-2439
H-3-2-3-3313	JACKMAN JASON	1028 W 1300 S	HURRICANE	UT	84737
H-3-2-3-310	JENSEN LOVINA JEAN TR	1117 S 920 W	HURRICANE	UT	84737
H-3-2-10-4412	JOCELYN AMANDA M	967 W 1300 S	HURRICANE	UT	84737
H-3-2-3-437	JOHNSON BRIAN, GONZALES BRECK	850 S 1100 W	HURRICANE	UT	84737
H-HUE-1-3	JOHNSON GREGORY T & BRYANNE R	509 N 550 W	HURRICANE	UT	84737
H-3-2-3-302	JOLLEY BRADLEY & CHRISTY	1283 S 920 W	HURRICANE	UT	84737
H-3-2-3-3316	JONES STEPHEN E, JONES ALANE	1126 S 920 W	HURRICANE	UT	84737
H-3-2-3-330	JONES TANNER	1112 S 920 W	HURRICANE	UT	84737
H-3-2-3-344	KENNETH W HEATON FAMILY LTD PTSHP IV	1189 ST. JAMES LN	SAINT GEORGE	UT	84790
H-HUE-2-15	KLAYTON CONSTRUCTION INC	985 S 700 W	HURRICANE	UT	84737
H-HHES-4	KUREK BRANDON & BROOKE	1263 S 840 W	HURRICANE	UT	84737
H-HUE-1-4	LANCELOTTI GINO, LANCELOTTI DEBORAH	942 W 1640 S	HURRICANE	UT	84737
H-3-2-4-123	LAW HOLDINGS LLC	5202 W 2250 S	HURRICANE	UT	84737
H-HUE-2-11	LEBARON RONALD DOUGLAS & SAMANTHA	450 N 530 W	HURRICANE	UT	84737
H-3-2-10-441	LEVITZ WAYNE & CHARLOTTE	934 W 1500 S	HURRICANE	UT	84737
H-MUF-4	MADSEN JEFFREY B & NATALIE L	3672 W RIVER VIEW DR	HURRICANE	UT	84737
H-3-2-10-446	MALONEY CALVIN N	1029 W 1300 S # 131-7	HURRICANE	UT	84737-2526
H-HHES-1	MANGELSON STEPHEN P & AMY LYNNE	1282 S 840 W	HURRICANE	UT	84737
H-3-2-3-339	MASON DAVID C & MAIDEEN TRS	1288 S 920 W	HURRICANE	UT	84737-2521
H-3-2-3-328	MATHEWS JANET CRAWFORD TR	805 S RIVER RD # 62	SAINT GEORGE	UT	84790
H-3-2-3-351	MORRIS GENE & LYNETTE	1076 S 920 W	HURRICANE	UT	84737-2439
H-HHES-11	NORTH RAYMOND KARL & ELAINE T TRS	1192 S 840 W	HURRICANE	UT	84737
H-3-2-3-323-A	PERSINGER MICHAEL	2034 ROCKLIN DR	SANDY	UT	84092
H-MUF-8	PETERSON RYAN & JESSICA	938 S 990 W	HURRICANE	UT	84737
H-HUE-2-17	POLLOCK JASON G & RAE LYN G	1751 E 1200 N	SAINT GEORGE	UT	84770-8602
H-3-2-4-124	RAC INC	1267 N 1390 W	SAINT GEORGE	UT	84770
H-3-2-3-43171	RADEMACHER MELINDA S TR	710 S 1100 W	HURRICANE	UT	84737
H-3-2-10-445	REEVE CHARLES R & CHERYL S TRS	559 S 400 W	HURRICANE	UT	84737-2210
H-3-2-10-452	RICH DANIEL R & KARI E	815 W 1300 S	HURRICANE	UT	84737
H-3-2-10-4311	RICHINS KEN & TERESA	1005 W 1500 S	HURRICANE	UT	84737-2530
H-3-2-3-334	ROBSON VAN LEE, ROBSON ROBERT	1170 S 920 W	HURRICANE	UT	84737
H-HHES-5	SCHOENFELD MICHAEL W & MARSHA TRS	1246 S 840 W	HURRICANE	UT	84737
H-MUF-5	SCHOLZEN JEFFREY N TR	993 S 990 W	HURRICANE	UT	84737
H-3-2-3-352-A	SCOTT DAN E TR	1274 S 920 W	HURRICANE	UT	84737
H-HHES-9	SCOTT MICAH & KACIE	1210 S 840 W	HURRICANE	UT	84737
H-3-2-3-337	SEEGMILLER ROGER & CONNIE	1128 N 700 W	OREM	UT	84057
H-HUE-1-6	SMITH DOUG, GLENN JEREMY, SMITH ASHLEE	PO BOX 1137	DRAPER	UT	84020
H-3-2-10-449	SMITH LARRY M & RAE ELLEN	1314 S 920 W	HURRICANE	UT	84737
H-HUE-1-10	SOLARIS DEV LLC	1568 S RIVER RD	SAINT GEORGE	UT	84790
H-3-2-3-354	STEPHAN DAN MARTIN & CAROL	1261 S 920 W	HURRICANE	UT	84737
H-NTW-2	STRATTON DALE & CHRISTINE	952 S 920 W	HURRICANE	UT	84737
H-3-2-4-127	STRATTON DEV LLC	42 S 850 W STE 201	HURRICANE	UT	84737
H-3-2-4-21211	STRATTON DONALD N & TERRI TRS	970 S 1100 W	HURRICANE	UT	84737
H-3-2-3-345	STRATTON LAUREL H, STRATTON DONALD	657 W 1500 S	HURRICANE	UT	84737
H-HUE-2-18	TERRY MICHAEL W & JESSICA H	201 S 2020 E CIR	SAINT GEORGE	UT	84790
H-MUF-3	THIOMBIANO ABRAHAM & CHELSEY DEMILLE	953 S 990 W	HURRICANE	UT	84737
H-3-2-3-3571	THOMAS THURMON E	1028 S 920 W	HURRICANE	UT	84737
H-MUF-12	VAN RY JOHN & WILMA JEAN TRS	PO BOX 142	HURRICANE	UT	84737-0142
H-MUF-13	VAN RY JOHN SCOTT & CHARLOTTE TRS	PO BOX 263	HURRICANE	UT	84737
H-HUE-1-7	VARGAS JUAN	281 N 2470 W	HURRICANE	UT	84737
H-3-2-3-309	VISSER NELLY L TR, VISSER EDGARDO	1155 S 920 W	HURRICANE	UT	84737
H-3-2-10-31111	WADSWORTH COLETTE TR	1810 S 700 W	HURRICANE	UT	84737
H-DKW-1	WADSWORTH DALLAN J & KATTIE	807 W 1500 S	HURRICANE	UT	84737-2538
H-HUE-2-19	WARSHAW HOLDINGS LLC	4363 S ZARAHEMLA DR	SALT LAKE CITY	UT	84124
H-3-2-10-413	WATERS GABRIEL S & MICHELLE M	10204 N YORKSHIRE CT	HIGHLAND	UT	84003
H-HHES-6	WAYMAN CLARENCE I & DOROTHY	1245 S 840 W	HURRICANE	UT	84737-2401
H-3-2-3-353	WHITAKER WAYNE O & DIANE H TRS	1217 S 920 W	HURRICANE	UT	84737
H-3-2-3-313	WILCOCK BENJAMIN & EMILY	1003 S 920 W	HURRICANE	UT	84737
H-MUF-14	WILD NATHAN & ASHLEY TRS	991 S 1100 W	HURRICANE	UT	84737
H-3-2-3-368	WOOD SHAWN K & SHAUNA	1246 S 920 W	HURRICANE	UT	84737
H-3-2-4-44021	WPP HURRICANE LAND LLC	595 S RIVERWOODS PKWY STE 400	LOGAN	UT	84321
H-MUF-1	WRIGHT RAYMOND	915 S 990 W	HURRICANE	UT	84737
H-MUF-6	YARDLEY RYAN P	988 S 990 W	HURRICANE	UT	84737
H-3-2-3-440	ZIMMERMAN BRUCE & LINDA	890 S 1100 W	HURRICANE	UT	84737-2513
H-3-2-9-122	ZIONS GATE ESTATES LLC	1035 N 1050 E	AMERICAN FORK	UT	84003

<b>Agenda Date:</b>	<b>06/08/2022</b>
<b>Application Number:</b>	2023-APA-01
<b>Type of Application:</b>	Agriculture Protection Overlay
<b>Action Type:</b>	Legislative
<b>Applicant:</b>	Kenneth W. Heaton
<b>Agent:</b>	
<b>Request:</b>	A report for the City Council on a proposal to create an Agricultural Protection Area
<b>Location:</b>	948 S 1100 W, 1046 W 1500 S
<b>Zoning:</b>	RA-1, R1-10
<b>General Plan Map:</b>	<a href="#">Single Family</a>

**Discussion:** This request is not to change the zone but protect the land with an Agricultural Protection Area Overlay Zone. The total size is 41.15 acres across four parcels. State and City codes set the standards for review by the Planning Commission and the County Agricultural Protection Area Advisory Committee. An Agricultural Protection Area grants certain legal protections to the property, including preventing forced rezoning the property, providing a defense against nuisance actions, notice to new subdivisions, and limiting eminent domain actions. These protections are found in the Utah Code, sections of which are included in this report.

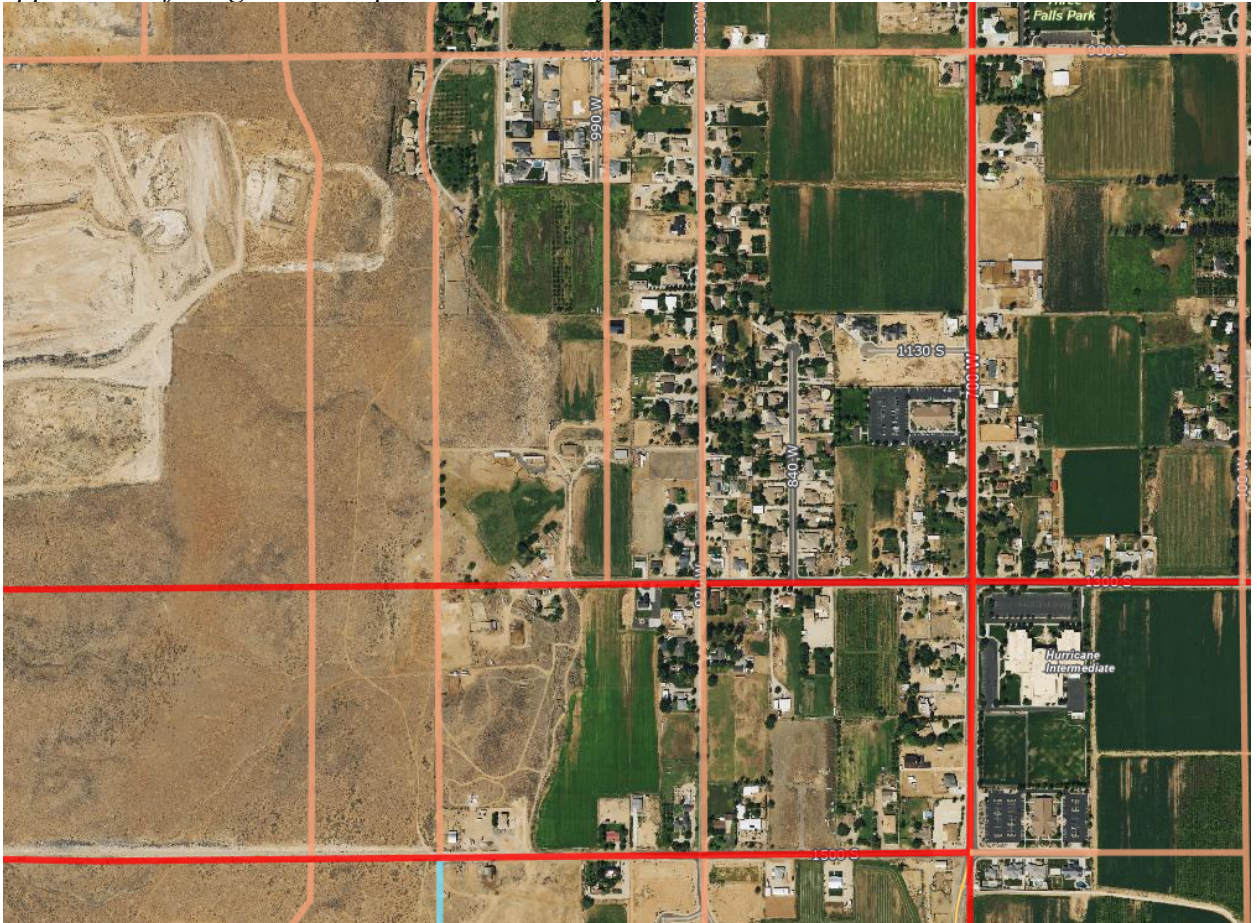
The Dixie Conservation District has indicated that they have given a recommendation of approval for this application but no written recommendation letter has been received. Per Hurricane City Code: *“If the Planning Commission or the Advisory Board fail to submit a report within 45 days, the City Council shall consider their failure to report as an approval of the proposal.”*



The criteria set by State law (17-41-303) for a “written report to the legislative body” is that it:

1. *Analyzes and evaluates the effect of the creation of the proposed area on the planning policies and objectives of the ....municipality.....:*

The 2021 General Plan states that the city should “Encourage the use of and support applications for agricultural protection overlay zones.”



The City does have three future Master Plan Roads (1150 W, 1300 S, and 1500 S all of which are in some state of undeveloped at the location of this property) that traverses the property that should be considered (shown above). An Agricultural Protection Zone could make it more difficult for the City to secure the property for a roadway in the future if needed. The City is in progress of securing the right of way for 1150 W and this could seriously threaten the City’s plans for this area.

At the last City Council meeting there was some discussion of amending the Transportation Master Plan alignment of 1150 W, however staff has received no clear direction at this time and the 2019 Transportation Master Plan still has standing.

2. *Analyzes and evaluates the proposal by applying the criteria contained in Section 17-41-305 –*

*In evaluating a proposal and in determining whether or not to create or recommend the creation of an agriculture protection area, industrial protection area, or critical*

*infrastructure materials protection area, the advisory committee, planning commission, and applicable legislative body shall apply the following criteria:*

- (1) *whether or not the land is currently being used for agriculture production, industrial use, or critical infrastructure materials operations, as the case may be;*
  - a. Per the application, Parcels A and B (labeled above) have fruit orchards and pasture space. Alfalfa is being grown on Parcel D and parcel C is used for pasture. Satellite view and property inspections appear to match this description.
- (2) *whether or not the land is zoned for agriculture use, industrial use, or critical infrastructure materials operations, as the case may be;*
  - a. Parcels A and B are zoned R1-10, single family one unit per 10,000 sq ft. Staff would assume the agricultural use of the property is a legal nonconforming use. Parcels C and D are zoned RA-1, residential agriculture one unit per acre
- (3) *whether or not the land is viable for agriculture production, industrial use, or critical infrastructure materials operations, as the case may be;*
  - a. This is the property's current use. Future viability will depend on a number of factors.
- (4) *the extent and nature of existing or proposed farm improvements, the extent and nature of existing or proposed improvements to or expansion of the industrial use, or the extent and nature of existing or proposed improvements to or expansion of critical infrastructure materials operations, as the case may be; and*
  - a. The current agricultural use is not proposed to be expanded upon. The current agricultural use is not particularly impactful.
- (5)
  - (a) *in the case of an agriculture protection area, anticipated trends in agricultural and technological conditions;*
    - a. The applicant intends to use pressurized irrigation when that becomes available.
  - (b) *in the case of an industrial protection area, anticipated trends in technological conditions applicable to the industrial use of the land in question; or*
  - (c) *in the case of a critical infrastructure materials protection area, anticipated trends in technological conditions applicable to the critical infrastructure materials operations of the land in question.*

3. *Recommends any modifications to the land to be included in the proposed agricultural protection area.*

Staff would recommend the Planning Commission not include the future right of ways for all master plan roadways through this property.

4. *Analyzes and evaluates any objections to the proposal.*

No objections have been received at the time of this writing.

5. *Includes a recommendation to the ....legislative body to either accept, accept and modify, or reject the proposal.*

See the attached letter

**JUC Comments:**

**Public Works:** This should be denied or or conditioned upon the City being able to get the 1150 W right of way.

**Power:** South side of 1300 S and north side of 1500 S along this property has power lines that must remain along with easements. Power department requests a 15' PUE on the west side of the proposed roadway of 1150 W and setbacks be at least 25' from the centerline of the power line.

**Water:** In these Agriculture Protection Areas we need to be able to accommodate planned and future water line easements for transmission and water line looping namely 1150 W.

**Streets:** This zoning will affect future master plan roadway alignments.

**Sewer:** Will impact sewer connectivity in the future.

**Engineering:** Area for 900 S (Planned 60' public street) and 1150 W (Planned 67' public street) should be excluded from the overlay. The storm drain master plan suggests there should be piping along Parcel C's southern boundary. The applicant may want to coordinate a local roadway with their neighbors to help capture and convey drainage. 10.25' should be excluded from the overlay on the north side of Parcel D for future widening of 1300 S (planned 70' public street). 17.25' should be excluded from the overlay on the south side of Parcel D for future widening of 1500 S (planned 77' public streets).

**Fire:** Okay

**Cable:** No comments

**Recommendation:** Staff recommends the Planning Commission review the evaluation criteria, discuss the application with the applicants, and recommend approval of the attached letter to the City Council.



**June 8, 2023**

**Subject: Report to City Council on proposed Agricultural Protection for Parcels H-3-2-4-21011, H-3-2-3-344, H-3-2-3-3315, H-3-2-10-4471**

Hurricane City Council,

The Planning Commission forwards the following findings regarding Agricultural Protection Overlay 2023-APA-01:

1. The land is currently being used for agricultural production.
2. The land is either zoned for agricultural use or is a legal nonconforming use.
3. The land is viable for agricultural use
4. The property is congruent with agriculture protection zones
5. Approving the use of this land for agriculture is consistent with current and expected technological trends in agriculture.

Future Use

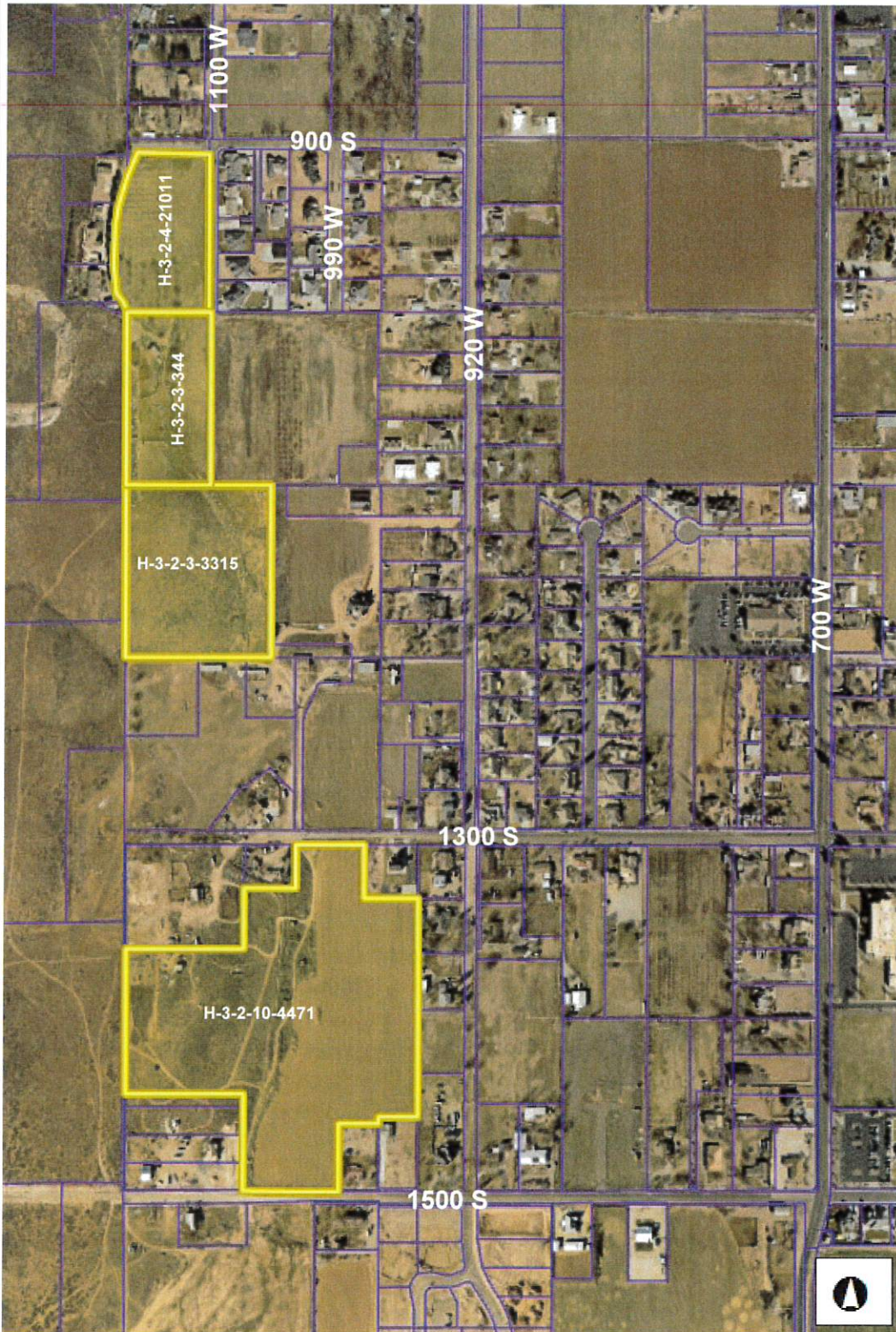
1. The General Plan maps have the area listed as Single Family
2. Future 1150 W and 900 S and current 1300 S and 1500 S are adjacent to this property. The City is in the process of acquiring right of way for 1150 W

### **Recommendation**

The Planning Commission recommends that the City Council approve the overlay request for parcels H-3-2-4-21011, H-3-2-3-344, H-3-2-3-3315, H-3-2-10-4471 with the exception of the property needed for the master planned buildout of 1150 W, 900 S, 1300 S and 1500 S.



# Ag Protection



**Legend**

- Parcels
- Ownership**
  - U.S. Forest Service
  - U.S. Forest Service Wilderness
  - Bureau of Land Management
  - Bureau of Land Management Wilde
  - National Park Service
  - Shivwits Reservation
  - Utah Division of Wildlife Resources
  - Utah Division of Transportation
  - State Park
  - State of Utah
  - Washington County
  - Municipally Owned
  - School District
  - Privately Owned
  - Water
  - Water Conservancy District
  - State Assessed Oil and Gas
  - Mining Claim

**Notes**



WGS\_1984\_Web\_Mercator\_Auxiliary\_Sphere  
DISCLAIMER: The information shown on this map was compiled from different GIS sources. The land base and facility information on this map is for display purposes only and should not be relied upon without independent verification as to its accuracy. Washington County, Utah will not be held responsible for any claims, losses or damages resulting from the use of this map.

# HIGHER WALL CONDITIONAL USE PERMIT

City of Hurricane  
147 N 870 W  
Hurricane, Utah 84737  
(435) 635-2811  
FAX (435) 635-4831

Fee: \$ 250

For office use only: File No. <u>WCU P23-01</u> Receipt No. <u>8.169104</u>
---

## APPLICATION & SUBMITTAL CHECKLIST

Name: GARY APPARCEL Telephone: 559-285-4835  
Address: 2802 S 3440 W, HURRICANE, UT 84737 Fax No. N/A  
Email: gapparcel@att.net  
Agent (If applicable): N/A Agent's Phone: N/A  
Address of Subject Property: 2802 S 3440 W, HURRICANE, UT 84737  
Parcel # of Subject Property: H-DSP-E-211 Zone District: RESIDENTIAL  
Proposed Conditional Use: (Describe, use extra sheet if necessary) PRIVACY Fence by  
SHADE CREATIONS - ATTACHED TO MY SIDE OF EXISTING WALL - 2 Feet above  
516 FOOT BRICK WALL (TOTAL 8 FEET)  
Applicant or property owner signature: [Signature]

### This application shall be accompanied by the following:

1. A plot plan showing the following:
  - Property boundaries, dimensions and existing streets.
  - Location of existing and proposed wall
  - Adjoining property lines and uses within one hundred (100) feet of subject property.
- 2) A reduced copy of all plans (8 1/2 x 11 if readable, or 11 x 17) if original plans are larger.
- 3) Photos of location where higher wall is proposed
- 4) Approximate heights of adjoining structures
- 5) Applicant's signature above is evidence applicant has ownership and control of the property. If staff verification of the ownership status does not match signature, a notarized affidavit will be required as attached.

**NOTE: It is important that all applicable information noted above is submitted with the application. An incomplete application cannot be reviewed until it is complete. Fee is part of the application.**

\*\*\*\*\*

(Office Use Only)

Date Received: 5/23/23 Received by: Brienna Spencer  
Date application deemed to be complete: 5/24/23 Completion determination made by: B.S.

When recorded mail deed and tax notice to:  
Gary M. Apparcel and Brenda J. Apparcel,  
Trustees of the Gary and Brenda Apparcel  
Living Trust dated September 29, 2021  
2590 Herndon Ave. Apt 208  
Clovis, CA 93611

This document has been recorded electronically. Please see attached copy to view the County Recorder's stamp as it now appears in the public record.



Order No. 36183 - LH  
Tax I.D. No. H-DSP-E-211

Space Above This Line for Recorder's Use

### WARRANTY DEED

Martin Perez and Ruth Perez, grantor(s), of St. George, County of Washington, State of Utah, hereby CONVEY and WARRANT to

Gary M. Apparcel and Brenda J. Apparcel, Trustees of the Gary and Brenda Apparcel Living Trust dated September 29, 2021, grantee(s) of Clovis, County of Fresno, State of California, for the sum of TEN DOLLARS AND OTHER GOOD AND VALUABLE CONSIDERATION the following described tract of land in Washington County, State of Utah:

Lot 211, DIXIE SPRINGS SUBDIVISION AMENDMENT AND EXTENSION PLAT "E", according to the Official Plat thereof, on file in the Office of the Recorder of Washington County, State of Utah.

TOGETHER WITH all improvements and appurtenances there unto belonging, and being SUBJECT TO easements, rights of way, restrictions, and reservations of record and those enforceable in law and equity.

WITNESS the hand(s) of said grantor(s), this 5 day of November, 2021.

  
\_\_\_\_\_  
Martin Perez

  
\_\_\_\_\_  
Ruth Perez

STATE OF Utah )  
 )  
 ) :ss.  
COUNTY OF Washington )

On the 5 day of November, 2021, personally appeared before me, Martin Perez and Ruth Perez, the signer(s) of the within instrument who duly acknowledged to me that he/she/they executed the same.

  
\_\_\_\_\_  
NOTARY PUBLIC

My Commission Expires:





REDLEG DESIGN  
 1385 W. 300 N.  
 ST. GEORGE UT 84770  
 435-569-4277

SITE PLAN

JOB #: DS-E211

SOLARIS HOMES  
 DIXIE SPRINGS, PLAT E, LOT 211  
 HURRICANE, UTAH

DRAFTED: 12-27-21  
 REVISIONS:  
 2021

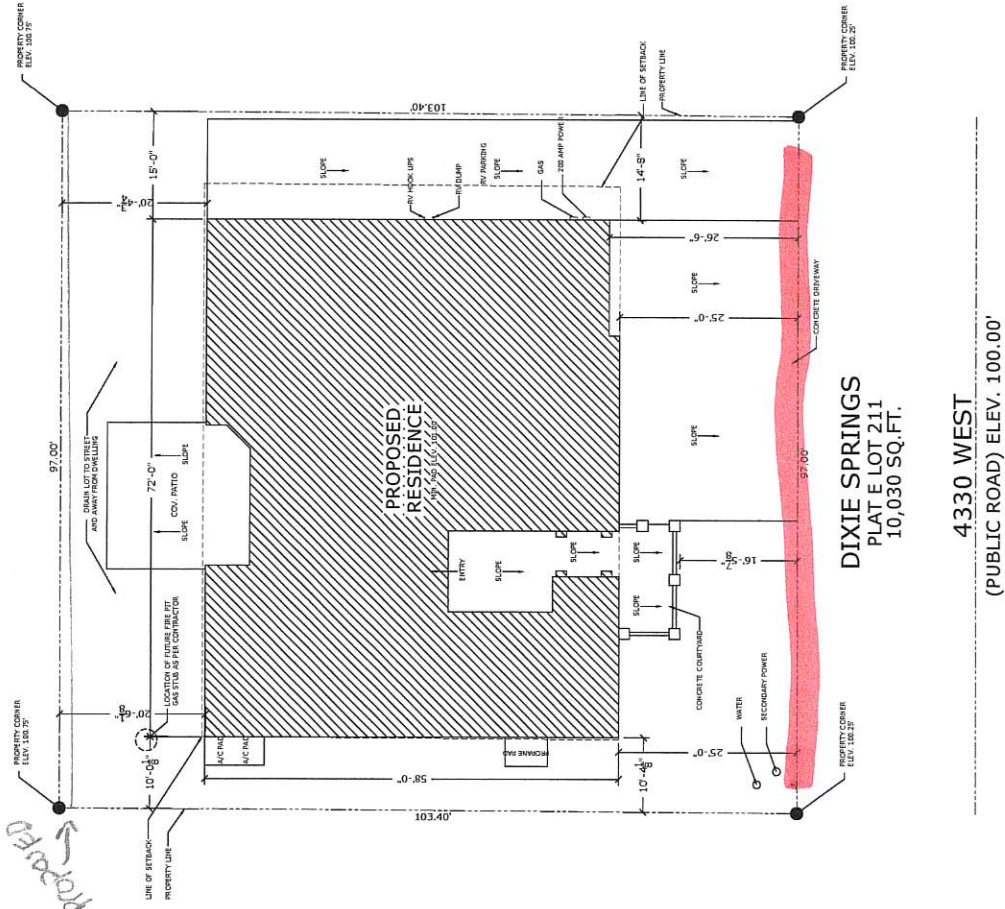
DRAFTER: ROB REID  
 REVISION DATE:

DESIGNER: ROB REID

C1  
 SHEET SCALE:  
 1/4" = 1'-0"



ADJACENT LOT 210



DIXIE SPRINGS  
 PLAT E LOT 211  
 10,030 SQ. FT.

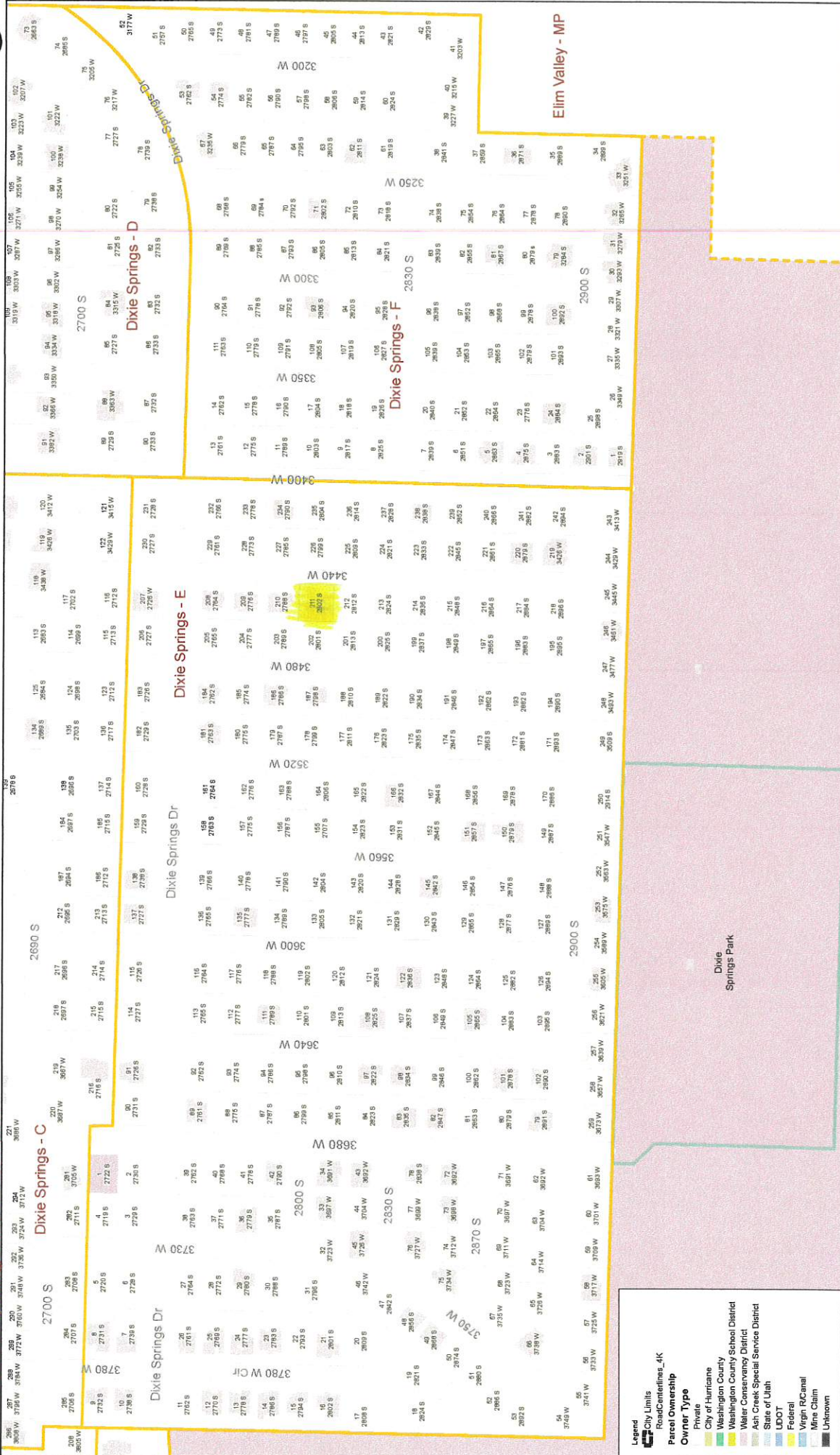
4330 WEST  
 (PUBLIC ROAD) ELEV. 100.00'

ADJACENT LOT 212

SITE PLAN  
 SCALE: 1/4" = 1'-0"

# Subdivision Reference Map

## Dixie Springs Plat E and F



**Legend**

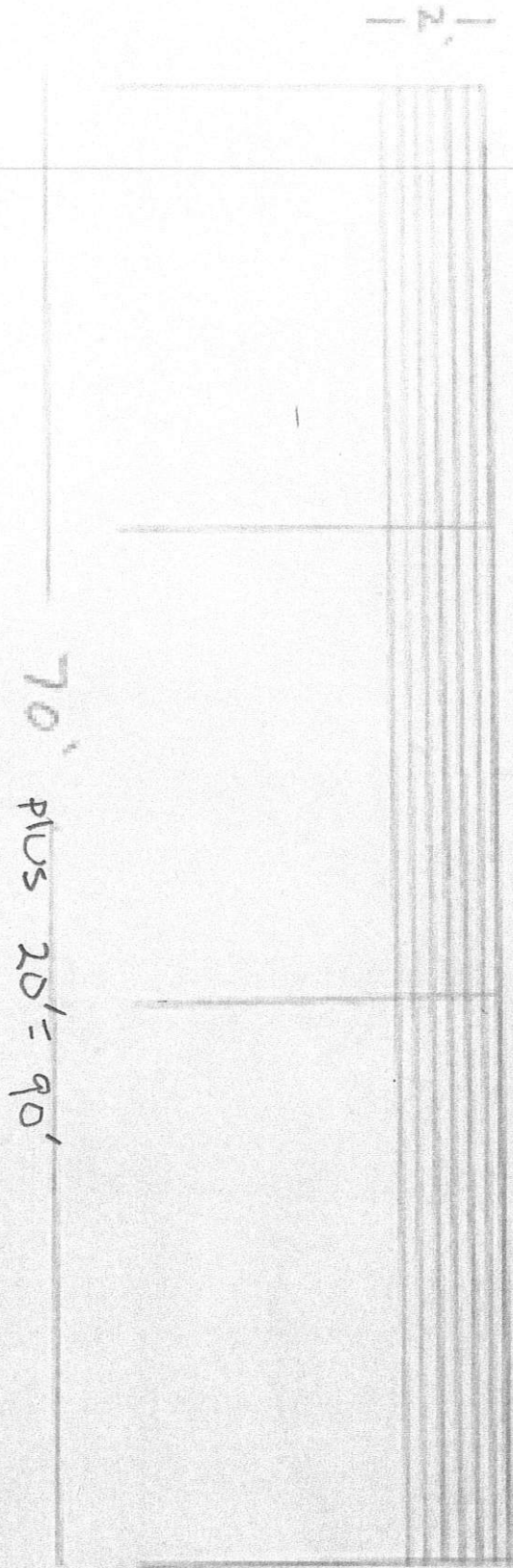
- City Limits
- RoadCentelines\_4K
- Parcel Ownership
- Private
- City of Hurricane
- Washington County
- Washington County School District
- Water Conservancy District
- Ash Creek Special Service District
- State of Utah
- UDOT
- Federal
- Virgin R/Canal
- Mine Claim
- Unknown



City of Hurricane  
GIS Department

This map is provided on an as-is basis. The City of Hurricane makes no warranties, expressed or implied, as to the quality, accuracy, and completeness, of the map and data provided. While the City of Hurricane has reviewed the information on this map, in no event shall the City of Hurricane be liable for damages arising from use by others.

JOB DESCRIPTION:





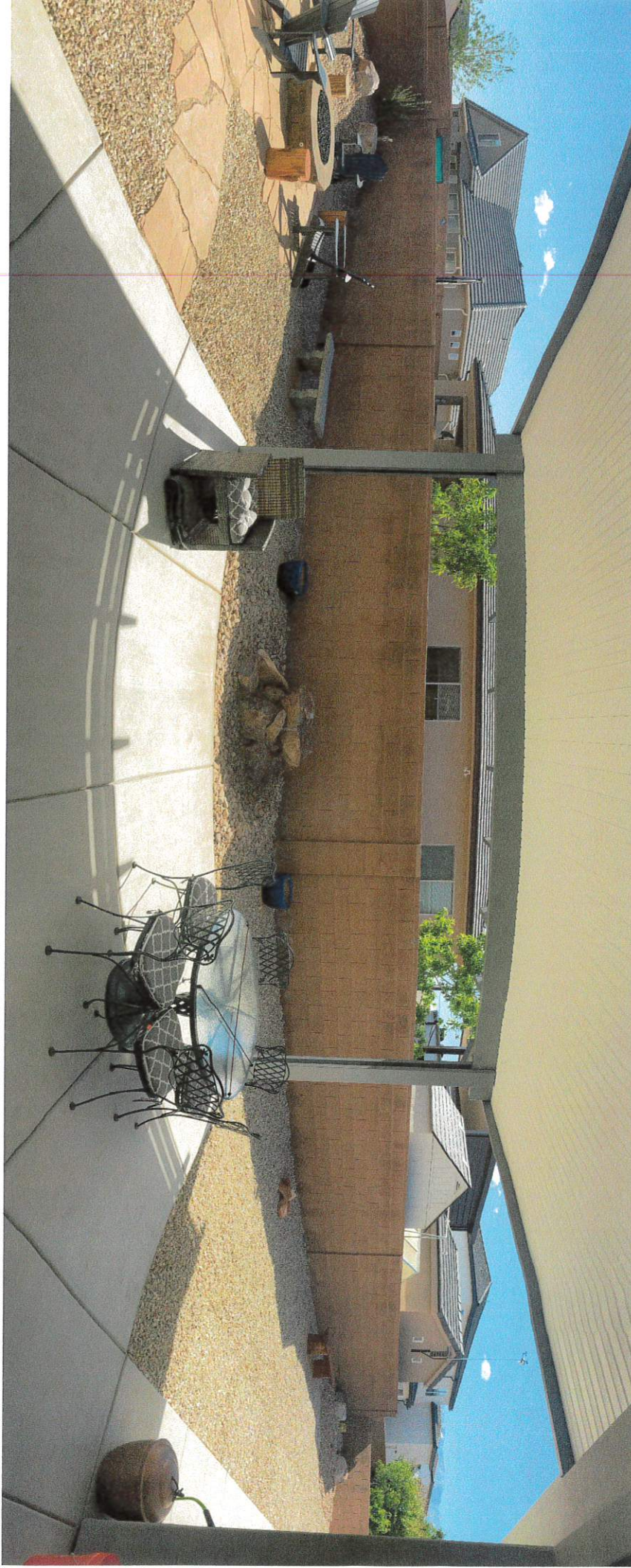
# PRIVACY FENCES

SERVICES | PRIVACY FENCES



IMG\_1098

1 of 7



**Shade Creations**  
 429 Washington Palms Way  
 Washington, UT 84780  
 435.632.7174  
 braxton@shadecreationsawnings.co  
 m



## Estimate

**ADDRESS**

Gary Apparcel  
 2802 S 3440 W  
 Hurricane, UT

**ESTIMATE #** 1173  
**DATE** 05/15/2023  
**EXPIRATION** 06/15/2023  
**DATE**

DATE	DESCRIPTION	AMOUNT
	<b>Privacy Fence</b> FOUR SEASONS ELITEWOOD PRODUCTS  70'x2' Aluminum Privacy Fence. 2"x3" horizontal slats, 1/2" gaps. Attach to block wall. 2' above wall.  Color: Grey \$27/sf	3,780.00

This estimate includes all labor & material cost. **TOTAL** **\$3,780.00**  
 Acceptance of proposal: Buyer and Seller agree to the specifications, pricing and terms of this agreement. A 30% deposit is required, please contact us to move forward.  
 \*Payment in full is due at the time of completion.

Plus 1080.00  
 (ADDING ADD'L  
 20')  
 = \$4860.00

Accepted By

Accepted Date

Make all checks payable to Shade Creations or request to pay through an online link (processing fees will apply). If you have any questions or concerns, see the contact info at top of the page.

THANK YOU FOR YOUR BUSINESS!!

<b>Agenda Date:</b>	06/08/2023
<b>Application Number:</b>	WCUP 23-01
<b>Type of Application:</b>	Conditional Use Permit
<b>Action Type:</b>	Administrative
<b>Applicant:</b>	Gary Apparcel
<b>Agent:</b>	
<b>Request:</b>	A conditional use permit for a wall of greater height
<b>Location:</b>	2802 S 3440 W
<b>Zoning:</b>	R1-10
<b>General Plan Map:</b>	N/A

**Discussion:** The applicant proposes constructing a 8’ wall along the western edge of their property. Hurricane City Code (HCC) 10-37-9 states that: *Height of fences and walls. No fence, wall, or similar structure exceeding six feet in height shall be erected in any rear or side yard except for accessory buildings and structures permitted by this chapter unless by conditional use permit.* However, Title 10 Chapter 7 of the Hurricane City Code has no standards by which to approve walls and fences of a greater height than six feet. In this case staff would recommend approval of any request that could be seen as reasonable. They are proposing adding an aluminum privacy fence to their existing wall.

**Findings:**

1. They are proposing an additional 2’ of wall height
2. Wall is not facing the street and will most likely not be visible from the street.

**Recommendation:** Staff recommends approval of the conditional use permit.



# PLANNING & ZONING HURRICANE CITY UTAH

147 N 870 W Hurricane UT  
PHONE: 435.635.2811 FAX: 435.635.2184

## PRELIMINARY SITE PLAN APPLICATION

*For office use only. \$200.00 Fee*  
File No. PS P23-15  
Receipt No. 8-169024

Name: American Conservation Experience Telephone: (928) 699-1823

Address: 2900 N. Fort Valley Road, Flagstaff AZ 86001 Fax No. \_\_\_\_\_

Email: jkuhn@usaconservation.org Agent Email: jdarchitectpc@msn.com

Agent (If Applicable): James Derby Architect Telephone: (801) 680-1385

Address/Location of Subject Property: 73 S. Main Street

Tax ID of Subject Property: H-114-A-1 Zone District: GC General Commercial

Proposed Use: (Describe, use extra sheet if necessary. Include total number of lots) Expansion and remodel of existing Hostel

This application shall be accompanied by the following:

- 1. A vicinity map showing the general location of the project.
- 2. Three (3) copies of a site plan showing:
  - Topography showing 2' contours, identification of 30% or greater slopes;
  - The layout of proposed uses.
  - Location of open space when applicable.
  - Proposed access to the property and traffic circulation patterns.
  - Adjoining properties and uses;
  - Proposed reservations for parks, playgrounds, school, and any other public facility sites, if any.
- 3. Preliminary utility plan, including water, sewer, and storm drainage plans, and including access points to utilities.
- 4. Tables showing the number of acres in the proposed development and a land use summary; and
- 5. A phased development plan if applicable.
- 6. Warranty deed or preliminary title report or other document (see attached Affidavit) showing evidence that the applicant has control of the property
- 7. Signed and notarized Acknowledgment Water Supply (see attached)

**NOTE:** It is important that all applicable information noted above is submitted with the application. An incomplete application will not be scheduled for Planning Commission consideration. Planning Commission meetings are held on the second Thursday and the fourth Wednesday of each month at 6:00 p.m. Contact the Planning Department for the deadline date for submissions. Once your application is deemed complete, it will be put on the agenda for the next Planning Commission meeting. A deadline missed due to an incomplete application could result in a month's delay.

(Office Use Only)

Date Received: \_\_\_\_\_ Received by: \_\_\_\_\_

Date application deemed to be complete: \_\_\_\_\_ Completion determination made by: \_\_\_\_\_

AFFIDAVIT  
PROPERTY OWNER

STATE OF UTAH )

:SS

COUNTY OF )  
AMERICAN CONSERVATION

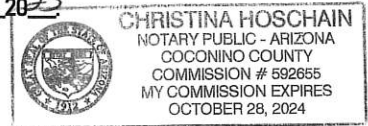
I (we), EXPERIENCE, being duly sworn, depose and say that I (we) am (are) the owner(s) of the property identified in the attached application and that the statements herein contained and the information provided identified in the attached plans and other exhibits are in all respects true and correct to the best of my (our) knowledge. I (we) also acknowledge that I have received written instructions regarding the process for which I am applying and the Hurricane City Planning staff have indicated they are available to assist me in making this application.

[Signature]  
(Property Owner)

NATION DIRECTOR FOR FACILITIES  
(Property Owner)

Subscribed and sworn to me this 10 day of May 2023

[Signature]  
(Notary Public)



Residing in: Flagstaff

My Commission Expires: 10/28/2024

Agent Authorization

I (we), \_\_\_\_\_, the owner(s) of the real property described in the attached application, do authorize as my (our) agent(s) \_\_\_\_\_ to represent me (us) regarding the attached application and to appear on my (our) behalf before any administrative or legislative body in the City considering this application and to act in all respects as our agent in matters pertaining to the attached application.

\_\_\_\_\_  
(Property Owner)

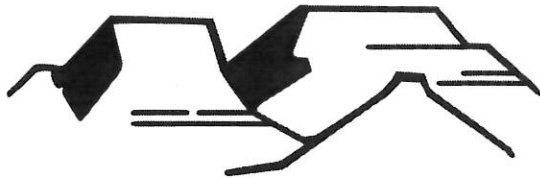
\_\_\_\_\_  
(Property Owner)

Subscribed and sworn to me this \_\_\_\_\_ day of \_\_\_\_\_ 20\_\_.

\_\_\_\_\_  
(Notary Public)

Residing in: \_\_\_\_\_

My Commission Expires: \_\_\_\_\_



# HURRICANE CITY UTAH

## ACKNOWLEDGEMENT OF WATER SUPPLY

I/We, AMERICAN CONSERVATION EXPERIENCE am/are the applicant(s) of the application known as 73 S MAIN ST. located on parcel(s) H-114-A-1 within the City of Hurricane, Washington County, Utah.

By my/our signature(s) below I/we do hereby acknowledge and agree to the following:

- 1 Neither acceptance nor approval of a development application by Hurricane City shall constitute a guarantee that sufficient water will be available to serve the zone designation, project, or permit for which this application is being submitted; and
- 2 Prior to receiving final approval for the application, the applicant may be required by Hurricane City to provide a guarantee of water availability which verifies that there is a sufficient water supply; and
- 3 For any application which may be approved without a guarantee of water availability, the applicant knowingly assumes the entire risk of water availability for the project and/or application; and
- 4 I/we waive any and all claims against Hurricane City that are in any way related to water availability for any project, permit, or use for which the land use application has been approved.

Dated this 10<sup>th</sup> day of MAY, 2023

Applicant Name: AMERICAN CONSERVATION EXPERIENCE

Name of Signer (if different from Applicant Name): JIM KUHN

Title of Signer: NATIONAL DIRECTOR FOR FACILITIES

Signature: [Handwritten Signature]

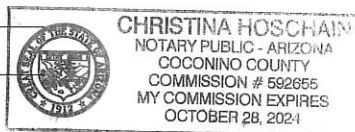
STATE OF Arizona )

COUNTY OF Coconino )

On the 10 day of May, 2023, personally appeared before me Jim Kuhn, the signer of the above instrument, who duly acknowledged to me that he/she executed the same in the capacity indicated on behalf of the Applicant.

Notary Public: [Handwritten Signature]

My Commission Expires: 10/28/2024



All signatures to this Acknowledgment, other than for Applicants signing in an individual capacity, shall be accompanied by documentation acceptable to the City attesting to the fact that the signer hereof is authorized to execute the Acknowledgment on behalf of the Applicant (e.g. a copy of a corporate resolution, copy of a partnership agreement, etc.). (Ord. No. 2022-37, 6-16-2022)

Corporate Warranty Deed Page 1 of 2  
Russell Shirts Washington County Recorder  
07/27/2012 10:42:13 AM Fee \$12.00 By  
SOUTHERN UTAH TITLE

When Recorded Mail Deed and Tax Notice To:  
AMERICAN CONSERVATION EXPERIENCE  
2900 N. Fort Valley Rd.  
Flagstaff, Arizona 86001

Order No. 167505  
Tax I.D. No. H-114-A-1



Space Above This Line for Recorder's Use

**CORPORATE WARRANTY DEED**

**CLARK COUNTY CREDIT UNION**, organized and existing under the laws of the State of Nevada, with its principal office at Las Vegas, County of Clark, State of Nevada, grantor(s), hereby **CONVEYS and WARRANTS** to **AMERICAN CONSERVATION EXPERIENCE, an Arizona Non-Profit Corporation**, grantee(s) of Flagstaff, County of Coconino, State of Arizona, for the sum of **TEN DOLLARS AND OTHER GOOD AND VALUABLE CONSIDERATION** in the following described tract of land in Washington County, State of UTAH:

**SEE EXHIBIT "A" ATTACHED HERETO AND MADE A PART HEREOF.**

TOGETHER WITH all improvements and appurtenances thereunto belonging, and being **SUBJECT TO** easements, restrictions, reservations and rights of way currently appearing of record and those enforceable in law and equity.

The officers who sign this deed hereby certify that this deed and the transfer represented hereby was duly authorized under a resolution duly adopted by the board of directors of the grantor at a lawful meeting duly held and attended by a quorum.

In witness whereof, the grantor(s) has have caused its corporate name and seal to be hereunto affixed by its duly authorized officers this 17 day of July, A. D. 2012.

ATTEST:

[Signature]  
Chairman - Secretary

BY: [Signature]  
Name: Wayne Tew  
Title: President / CEO

State of Clark Nevada  
County of Clark

NOTARY

On the 12 day of July, A. D. 2012, personally appeared before me, Wayne Tew, who being by me duly sworn, did say that he, is the President / CEO of CLARK COUNTY CREDIT UNION, and that said instrument was signed in behalf of said corporation by authority of its by-laws (or by a resolution of its board of directors) and said Debra Tiedeman duly acknowledged to me that said corporation executed the same and that the seal affixed is the seal of said corporation.



My Commission Expires:

Attachment to that certain Corporate Warranty Deed executed by **CLARK COUNTY CREDIT UNION** grantor(s),  
to **AMERICAN CONSERVATION EXPERIENCE, an Arizona Non-Profit Corporation**, grantee(s).

Order No. 167505  
Tax I.D. No. H-114-A-1

**EXHIBIT "A" - LEGAL DESCRIPTION**

**BEGINNING 76.5 feet North of the Southwest Corner of Lot Two (2), Block 19, Plat "A", HURRICANE TOWNSITE SURVEY, as platted on the Official Map of said Survey on file in the Office of the Recorder of Washington County, and running thence North 67.9 feet; thence East 234.4 feet; thence South 144.4 feet; thence West 13.4 feet; thence North 76.5 feet; thence West 221.0 feet, more or less, to the point of beginning.**

\*\*\*

Initials \_\_\_\_\_





**STAFF COMMENTS**

<b>Agenda Date:</b>	<b>06/08/2023</b>
<b>Application Number:</b>	PSP 23-15
<b>Type of Application:</b>	Preliminary Site Plan Application
<b>Action Type:</b>	Administrative
<b>Applicant:</b>	American Conservation Experience
<b>Agent:</b>	James Derby
<b>Request:</b>	Approval of a Preliminary Site Plan
<b>Location:</b>	73 S Main St
<b>Zoning:</b>	GC
<b>General Plan Map:</b>	Downtown

**Discussion:**

The applicant has filed a preliminary site plan for a remodel and expansion of the American Conservation Experience’s hostel in Downtown Hurricane. This property is located just north of the Bradshaw Hotel museum.



### JUC Comments

The following items will need to be addressed:

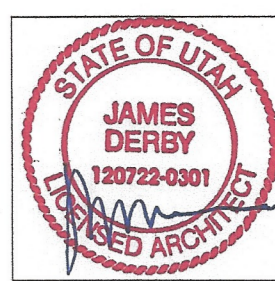
1. **Public Works:** Need to show water service.
2. **Power:** Does the project require additional power service? Talk to the power department about power locations and upgrades.
3. **Water:** Good
4. **Street:** No comments
5. **Sewer:** Sanitary sewer service. Submit plans to Ash Creek. Likely increase in impact fee.
6. **Engineering:** Engineering would encourage cross access with the existing parking lot north of this project. The applicant should specify their access plan. The drive aisles are too narrow for two-way traffic. Where will one-way signs/do not enter signs be posted?

7. **Fire:** Plan does not show existing hydrant. How many occupants? Possible fire suppression required
8. **Phone:** No comment
9. **Cable:** Conduit and fiber on site
10. **Gas:** No comment

**Staff Comments: Preliminary Site Plan**

1. The proposed and current use is as a hostel, this is permitted in the General Commercial zone.
2. Parking Requirements: The site plan indicates that 13 parking spaces are required and 17 stalls are provided. It does not appear that they are removing any parking stalls with this plan.
3. Landscape: The current landscaping is grandfathered in. The applicant is not proposing any changes to the required front landscaping and is not required to.
4. The lot is flat and the staff does not have any concerns about slopes on the property.
5. Most traffic to the site enters on Main St and exits on 100 S. This should be signed as a one way parking lot.
6. Staff would encourage connectivity between the neighboring properties in this area.
7. *A preliminary site plan is not intended to permit actual development of property pursuant to such a plan but shall be prepared merely to represent how the property could be developed. Submittal, review, and approval of an application for a preliminary site plan shall not create any vested rights to development. (Hurricane City Code 10-7-10 (D)(2)(b))*

**Recommendation:** Staff recommends the Planning Commission review this application based on standards with Hurricane City Code. Staff would recommend approval of this preliminary site plan subject to JUC and staff comments.



SIGNED 05.17.2023

COPYRIGHT DATE: MARCH 18, 2020  
THESE DOCUMENTS ARE INSTRUMENTS OF PROFESSIONAL SERVICE AND THE ARCHITECT ASSUMES NO LIABILITY FOR THE CONSTRUCTION OF THE PROJECT UNLESS OTHERWISE SPECIFIED IN WRITING. THE ARCHITECT'S RESPONSIBILITY IS LIMITED TO THE DESIGN AND CONSTRUCTION OF THE PROJECT AS SHOWN ON THESE DOCUMENTS. THE ARCHITECT DOES NOT WARRANT THE ACCURACY OF ANY INFORMATION PROVIDED BY OTHERS. THE ARCHITECT'S LIABILITY IS LIMITED TO THE PROFESSIONAL FEES PAID TO THE ARCHITECT AND DOES NOT INCLUDE CONSEQUENTIAL DAMAGES. REFER TO UTAH PROFESSIONAL ARCHITECT ACT (UCA 33A-1-101) FOR MORE INFORMATION.

ACE HOSTEL REMODEL AND ADDITION  
73 SOUTH MAIN STREET  
HURRICANE, UTAH 84737

JAMES DERBY ARCHITECT PC AIA NCARB  
175 N LODGE RD  
CENTRAL UTAH 84722  
PH (801) 886-1385



JOB NO.	A2022-08
DATE	05.17.2023
DRAWN	JD
CHECKED	JD
SHEET TITLE	SITE PLAN
SHEET NO.	A11

### REFERENCE NOTES

- 1 NEW VAN ACCESSIBLE PARKING STALL AND 6' CONCRETE PAVING
- 2 NEW H.C. PARKING SIGN PER DETAIL 19/A6.1
- 3 NEW HANDICAPPED RAMP AND STAIRS PER DETAIL XXX
- 4 EXISTING GRAVEL PARKING / DRIVE TO REMAIN
- 5 NEW REGULAR AND RECYCLE DUMPSTER ENCLOSURE PER CIVIL DWGS. W/ 6' CONCRETE APRON
- 6 GAS METER LOCATION PER MECHANICAL DWGS.
- 7 CONDENSING UNIT LOCATION PER MECHANICAL DWGS.
- 8 EXISTING TREE TO BE REMOVED
- 9 NEW LANDSCAPING
- 10 NEW CONCRETE CURB PER CIVIL DWGS.
- 11 BIKE RACK-SONIC WAVE RACK BY THE PARK AND FACILITIES CO. MODEL 536-1428 OR APPROVED EQUAL
- 12 EXISTING LANDSCAPING

### SITE DATA

ZONING: GC GENERAL COMMERCIAL

SITE AREA:	39 ACRES	16,920 SF
BUILDING AREA:	2800 SF	16 %
PARKING LOT, DRIVE:	10,795 SF	64 %
SIDEWALK:	432 SF	3%
LANDSCAPE:	2953 SF	17%

SET BACKS:  
 FRONT YARD SETBACK: 20 FEET  
 SIDE YARD SETBACK: 10 FEET  
 REAR YARD SETBACK: 10 FEET

BUILDING HEIGHT

MAXIMUM BUILDING HEIGHT:	35 FEET
ACTUAL BUILDING HEIGHT:	22 FEET

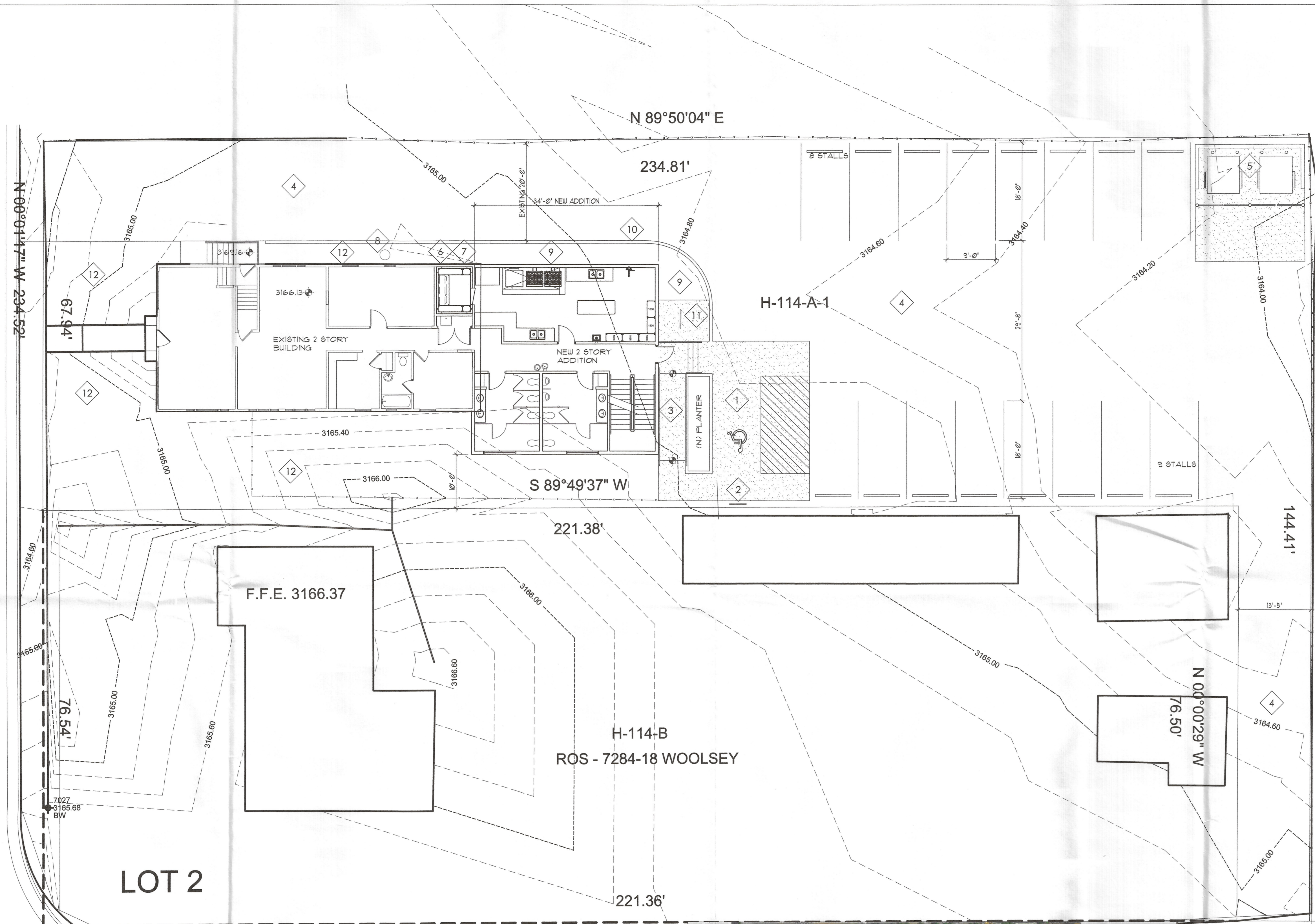
LANDSCAPE REQUIREMENTS

PARKING LOT LANDSCAPING: MIN. 1% OF TOTAL PARKING LOT AREA.  
 SF X 1% = SF MIN. SF PROVIDED %

FOUNDATION LANDSCAPING: MIN. 50% OF BUILDING FRONTAGE.  
 FRONTAGE: LANDSCAPE: 1% OF FRONTAGE

### PARKING REQUIREMENTS

USE TYPE: HOSTEL:	2 SPACES	2 CARS
	1 SPACE / SLEEPING UNIT	13 CARS
REQUIRED PARKING:	15 CARS	
ACTUAL PARKING:	17 CARS	



MAIN STREET

N 00°01'47" W 234.52'

LOT 2

100 SOUTH

1 ARCHITECTURAL SITE PLAN  
1" = 10'-0"

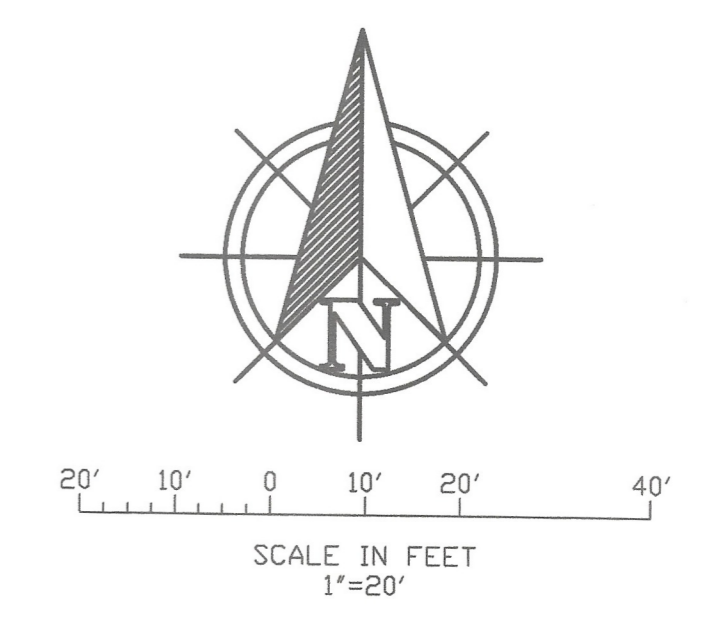
2 VICINITY MAP  
NTS



# ACE UTILITY AS BUILT SURVEY



- LEGEND**
- ROOF DRAIN
  - WATER METER
  - GAS METER
  - ELECTRICAL METER
  - POWER POLE
  - AIR CONDITIONER
  - POWER LINE



NO.	REVISIONS	BY	DATE
5			
4			
3			
2			
1			

ACE UTILITY AS BUILT	JOB NO. ACE 2022
HURRICANE, UTAH	SHEET NO. 3

**From:** Treasa <treasa@pv-eng.com>  
**Sent:** Tuesday, April 18, 2023 11:36 AM  
**To:** Fred Resch; Brienna Spencer; Stephen Nelson  
**Cc:** Karl Rasmussen; lifetimehomesinc@gmail.com  
**Subject:** Planning Commission Agenda- Extension for Strawberry Fields

Fred,  
Please except the following request for an extension on the following plat (Preliminary Plat originally handed in May 2, 2022):

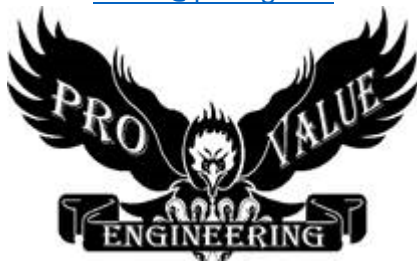
- Strawberry Fields

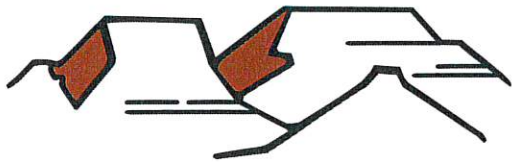
Thank you, let me know if you have any questions.

Thank You,  
Treasa Anderson  
ProValue Engineering Secretary  
Have A GREAT DAY!  
Office: 435-668-8307

Office Hours: Monday-Thursday 8 AM-5 PM

[treasa@pv-eng.com](mailto:treasa@pv-eng.com)





PLANNING & ZONING  
**HURRICANE CITY**  
 UTAH

147 N 870 W Hurricane UT  
 PHONE: 435.635.2811 FAX: 435.635.2184

**PRELIMINARY PLAT APPLICATION**

*For office use only: \$150.00 Fee*  
 File No. 2622-PP-08  
 Receipt No. 8.157973

Name: Western MTG & Realty Co. Telephone: \_\_\_\_\_  
 Address: 6670 W Court St, Pasco, WA 99301 Fax No. \_\_\_\_\_  
 Email: \_\_\_\_\_ Agent Email: Karl@po-erg.com  
 Agent (If Applicable): Karl Rasmussen Telephone: 435-668-8307  
 Address/Location of Subject Property: In the SW Corner of the NE Corner of section 12T, 42R, 14W  
 Tax ID of Subject Property: A piece of: H-4-2-12-1208-GS1 Zone District: R1-8  
 Proposed Use: (Describe, use extra sheet if necessary. Include total number of lots) \_\_\_\_\_

Submittal Requirements: The preliminary plat application shall provide the following:

1. Description: In a title block located in the lower right-hand corner of the sheet the following is required:
  - a. The proposed name of the subdivision.
  - b. The location of the subdivision, including the address and section, township and range.
  - c. The names and addresses of the owner or subdivider, if other than the owner.
  - d. Date of preparation, and north point.
  - e. Scale shall be of sufficient size to adequately describe in legible form, all required conditions of Chapter 39, City Subdivision regulations.
2. Existing Conditions: The preliminary plat shall show:
  - a. The location of the nearest monument.
  - b. The boundary of the proposed subdivision and the acreage included.
  - c. All property under the control of the subdivider, even though only a portion is being subdivided. (Where the plat submitted covers only a part of the subdivider's tract, a sketch of the prospective street system of the unplatted parts of the subdivider's land shall be submitted, and the street system of the part submitted shall be considered in light of existing Master Street Plan or other Commission studies.)
  - d. The location, width, and names/numbers of all existing streets within two hundred (200) feet of the subdivision and of all prior streets or other public ways, utility rights of way, parks and other public open spaces, within and adjacent to the tract.
  - e. The location of all wells and springs or seeps, proposed, active and abandoned, and of all reservoirs or ponds within the tract and at a distance of at least one hundred feet (100') beyond the tract boundaries.
  - f. Existing sewers, water mains, culverts, or other underground facilities within the tract, indicating the pipe sizes, grades, manholes and the exact locations.
  - g. Existing Hurricane Canal Company facilities; other ditches, canals, natural drainage channels and open waterways and any proposed realignments.

- h. Contours at vertical intervals not greater than five (5) feet.
- i. Identification of potential geotechnical constraints on the project site (such as expansive rock and soil, collapsible soil, shallow bedrock, and caliche, gypsiferous rock and soil, potentially unstable rock or soil units including fault lines, shallow groundwater, and windblown sand) and recommendations for their mitigation.
- j. Information on whether property is located in desert tortoise take area

3. Proposed Plan: The subdivision plans shall show:

- a. The layout of streets, showing location, widths, and other dimensions of proposed streets, crosswalks, alleys, and easements.
- b. The layout, numbers, and typical dimensions of lots.
- c. Parcels of land intended to be dedicated or temporarily reserved for public use or set aside for use of property owners in the subdivision.
- d. Easements for water, sewers, drainage, utilities, lines and other purposes.
- e. Typical street cross sections and street grades where required by the Planning Commission. (All street grades over 5% should be noted on the preliminary plat)
- f. A tentative plan or method by which the subdivider proposes to handle the storm water drainage for the subdivision.
- g. Approximate radius of all center line curves on highways or streets.
- h. Each lot shall abut a street shown on the subdivision plat or on an existing publicly dedicated street. (Double frontage or flag lots shall be prohibited except where conditions make other design undesirable)
- i. In general, all remnants of lots below minimum size left over after subdividing of a larger tract must be added to adjacent lots, rather than allow to remain as unusable parcels.
- j. Where necessary, copies of any agreements with adjacent property owners relevant to the proposed subdivision shall be presented to the Planning Commission.
- k. A letter from both the local sanitary sewer provider and culinary water provider indicating availability of service.
- l. Will this subdivision be phased? If yes show possible phasing lines.
- m. A tentative plan or method for providing non-discriminatory access to the subdivision for purposes of placement of communications infrastructure, and for purposes of placement of utility infrastructure.

4. Required copies of plans:

- a. Three copies of all full-scale drawings
- b. One copy of each drawing on a 11 x 17-inch sheets. (8 1/2 x 11 is acceptable if the project is small and the plans are readable at that size).

5. Warranty deed or preliminary title report or other document (see attached Affidavit) showing evidence that the applicant has control of the property

**NOTE: It is important that all applicable information noted above is submitted with the application. An incomplete application will not be scheduled for Planning Commission consideration. A deadline missed due to an incomplete application, could result in a month's delay. Planning Commission meetings are held on the second Thursday and fourth Wednesday of each month at 6:00 p.m. The deadline to submit an application to be placed on an agenda is no later than 12:00 (noon) 20 full calendar days before the Planning Commission meeting at which you plan for your application to be heard.**

.....  
(Office Use Only)

Date Received: \_\_\_\_\_ Application Complete: YES      NO

Date application deemed to be complete: \_\_\_\_\_ Completion determination made by: \_\_\_\_\_

PRELIMINARY PLAT APPLICATION page 4 (General Information)

PURPOSE

The preliminary plat application is the first step in land development process in those instances where land is divided for eventual sale. The process is established to ensure that all proposed divisions of land conform to the City General Plan and to adopted development standards of the Land Use Ordinance.

WHEN REQUIRED

The preliminary plat is required any time land is to be divided, re-subdivided or proposed to be divided into two (2) or more lots, parcels, sites, units, plots or other division of land for the purpose, whether immediate or future, for offer, sale, lease or development either on installment plan or upon any and all other plans, terms and conditions. It is not required on agricultural divisions where the agricultural parcel is divided to be combined with another agricultural parcel, nor is it required when two un-subdivided properties are merged, or where the boundary between two un-subdivided properties are adjusted.

**The preliminary plat must be approved before a final plat can be processed and recorded.**

Subdivision approval process:

- Determine current zoning for property.
- If zoning fits desired subdivision, meet with Planning staff to discuss the proposed project.
- Obtain a preliminary plat application form and complete the application and have a plat prepared by an engineer to meet the requirements on the preliminary plat application.
- Submit completed application, preliminary plat, and required \$150 fee to Planning Department before noon on the Wednesday two weeks before desired Planning Commission meeting. (See submittal dates sheet)
- Appear at the scheduled Planning Commission meeting to discuss preliminary plat, hear comments, answer questions, and receive recommendation of approval or disapproval from Planning Commission.
- Appear at the next scheduled City Council meeting that occurs after the Planning Commission meeting at which a recommendation was received. Hear City Council comments, answer questions, and receive City Council decision. City Council can approve the plat with conditions, recommend changes and send it back to the Planning Commission, or deny the plat.

If the preliminary plat is approved, have construction drawings for utilities and streets prepared by an engineer according to Hurricane City standards. Construction drawings must be approved by both the Hurricane Joint Utilities Committee and the Ash Creek Special Service District. Approval means changes are made and all required signatures are obtained.

Once the preliminary plat is approved by the City Council, the applicant has one year after receiving approval of the preliminary plat to submit the final plat to the Planning Commission for action. The Planning Commission may authorize a one-year time extension, provided the extension request is made before the one-year time limit is reached. The Planning Commission will review and make recommendation to the City Council on the final plat

when submitted. The City Council will typically review the Planning Commission recommendation within 1-2 weeks after the Commission action on the final plat.

#### APPEALS

The Planning Commission makes a recommendation to the City Council, so there is no appeal. The City Council's action on a preliminary plat is final unless appealed to the appropriate court.



**Update 06/08/2023:** The applicant is requesting an extension of the final plat deadline for this project. Per city ordinance, a final plat must be submitted within one year of approval of a preliminary plat, however, the applicant can request an extension of one year with the Planning Commission. Staff finds this request to be acceptable. This is part of the Gateway to Sand Hollow PID and utilities have not yet been fully brought to this site yet. Below is the original report from when the preliminary plat was first applied.

<b>Agenda Date:</b>	06/08/2023
<b>Application Number:</b>	2022-PP-08
<b>Type of Application:</b>	Preliminary Plat and Preliminary Site Plan Application
<b>Action Type:</b>	Administrative
<b>Applicant:</b>	Western Mortgage and Realty Co
<b>Agent:</b>	Karl Rasmussen
<b>Request:</b>	Preliminary Plat Application
<b>Location:</b>	Approximately 1400 S 3700 W
<b>Zoning:</b>	R1-8
<b>General Plan Map:</b>	Planned Community

**Discussion:**

The applicant is seeking preliminary plat and preliminary site plan approval for a subdivision containing 163 lots located within the Sand Hollow Gateway PID for the Strawberry Fields Estates subdivision. This property is due east of the approved Elim Estates subdivision and further east from Marla at Elim Valley. This property requires extensive PID infrastructure to be put in and is planned to be served by several PID roadways. Construction has begun on Bash Parkway which will bring utilities to this property. Any approval should be conditioned on sufficient buildout of the Sand Hollow Gateway PID infrastructure

**JUC Comments**

The following items will need to be addressed:

1. **Power:** Need Bash Parkway to be put in for power
2. **Sewer:** Sewer line in Bash Parkway
3. **Streets:** Infrastructure from PID needs to be in. Traffic study required
4. **Water:** Water model required. PID improvements need to be done before approval.
5. **Engineer:** Check block lengths. Connectivity looks good
6. **Fire:** No issues



7. It's not clear what the open space in the northeast corner is for. No open space is required by code; however, there is a PID planned park to the north of this development.
8. A will serve letter from Hurricane City Water and Ash Creek Sewer District has not been provided
9. Utilities: The property is isolated but has services planned for the area. The Sand Hollow Gateway PID has begun construction on Bash Parkway (the northern boundary of this plat) which will bring utilities and road connections to this site. Hurricane City Code states the following:

*10-37-4: ADEQUATE PUBLIC FACILITIES:*

*Land shall be developed only where existing infrastructure is in place or will be timely provided to service proposed development. The city may require an analysis to be completed to determine whether adequate public facilities are available to service a development and whether such development will change existing levels of service or will create a demand which exceeds acceptable levels of service for roadways, intersections, bridges, storm drainage facilities, water lines, water pressure, sewer lines, fire and emergency response times, and other similar public services. The city may disapprove a proposed development if demand for public services exceeds accepted levels of service. No subsequent approval of such development shall be given until either the developer or the city installs improvements calculated to raise service levels to the standard adopted by the City. (Ord. 03-5-1, 5-1-2003, eff. 6-1-2003)*

**Recommendation:** Staff recommends the Planning Commission review this application based on standards with Hurricane City Code. Any approval of the application should have the following conditions.

1. Sufficient buildout of Bash Parkway and the Sand Hollow Gateway PID such that utilities have reached the site.
2. Staff and JUC comments, including shortening any block that is over 800' long.

# PRELIMINARY PLAT FOR: STRAWBERRY FIELDS ESTATES

LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.&M.  
SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737

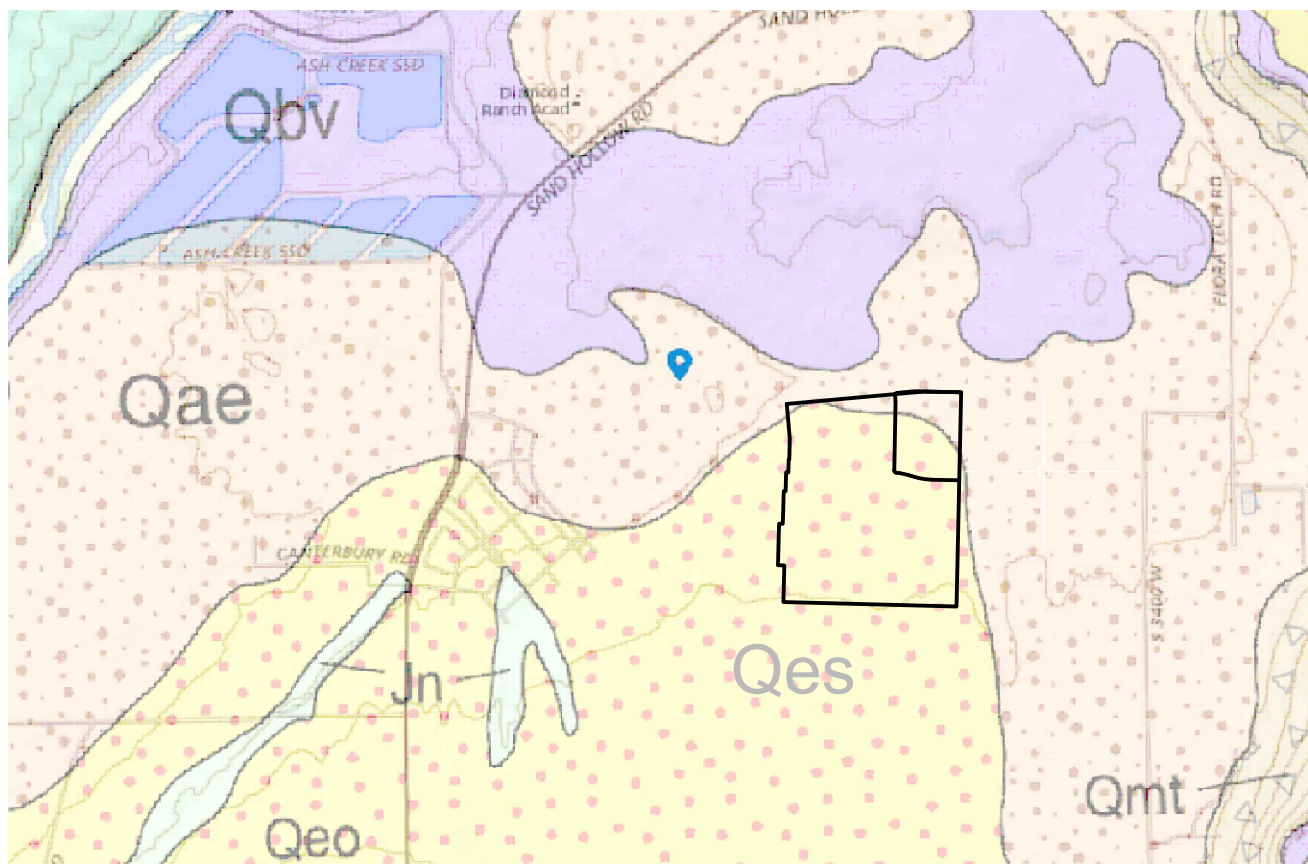
## DESERT TORTOISE TAKE AREA

THE PROJECT IS NOT LOCATED IN A DESERT TORTOISE TAKE AREA. (PER RED CLIFFS DESERT RESERVE TAKE AREA 7 MAP)

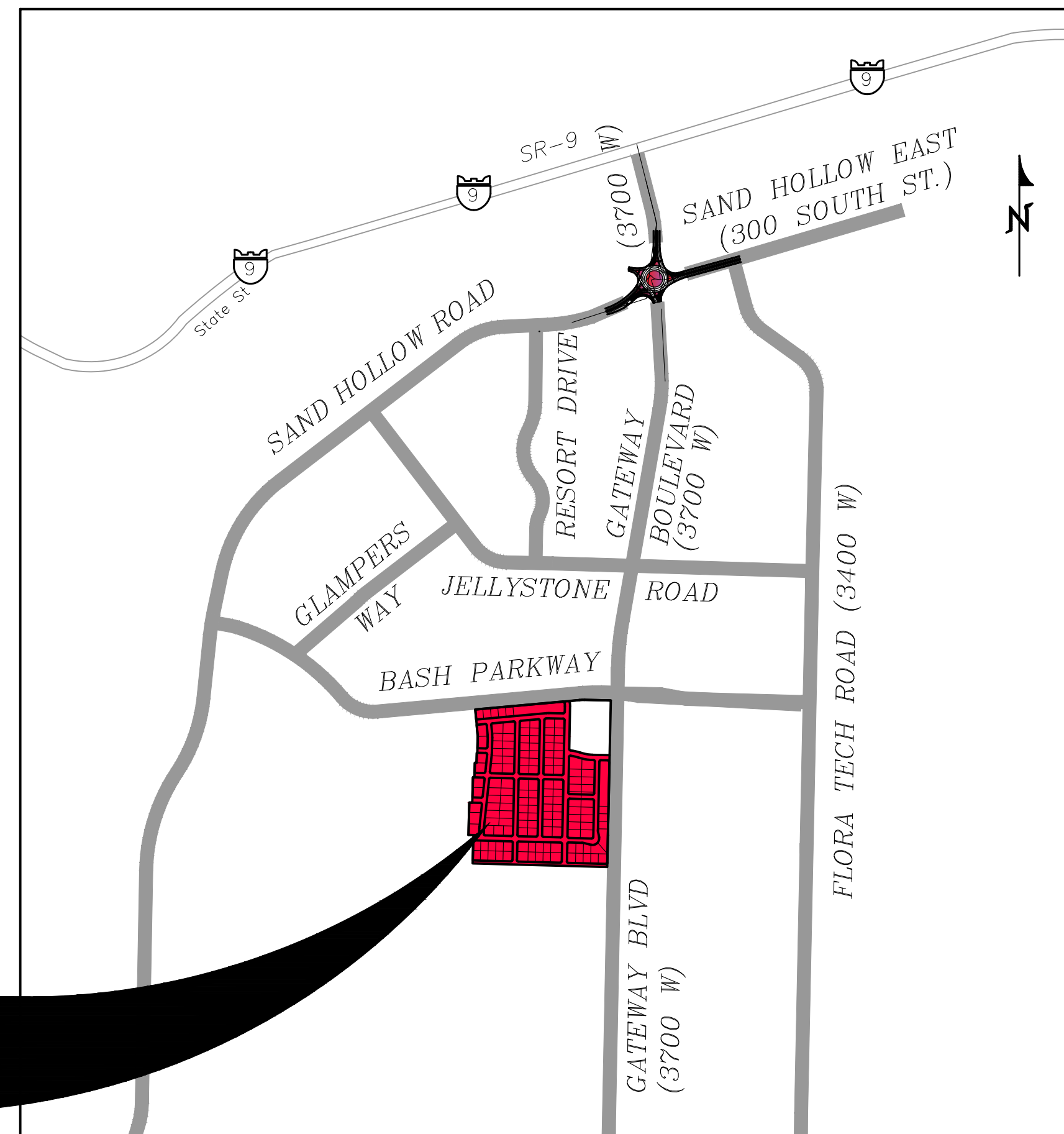
## GEOLOGICAL HAZARDS

FAULTS & SURFACE FAULTING	NONE
LIQUEFACTION	H2 - HIGH2-SIMILAR IN TEXTURE TO HIGH CATEGORY, NO GROUND-WATER INFORMATION
FLOODING	ZONE X-AREA OF MINIMAL FLOOD HAZARD
LANDSLIDE HAZARD	NONE
ROCKFALL HAZARD	NONE
BRECCIA PIPES AND PALEOKARST	NONE
CALICHE	CA - CALICHE
COLLAPSIBLE SOIL	CSA-STREAM/TERRACE ALLUVIUM >=3%
EXPANSIVE SOIL	ESL-LOW SUSCEPTIBILITY TO EXPANSION
GYPHSIFEROUS SOIL & ROCK	NONE
PIPING & EROSION	P&ES-SOILS SUSCEPTIBLE TO PIPING AND EROSION
SHALLOW BEDROCK	BRB-BURIED: AREAS WHERE DEPTH TO BEDROCK IS GENERALLY <=10 FEET BENEATH SOIL COVER
WIND BLOWN SAND	WBSH-HIGH: MODERN SAND-DUNE OR SHEET-SAND DEPOSITS
SHALLOW GROUND WATER	NONE

THE MITIGATION OF POTENTIAL HAZARDS WILL BE RECOMMENDED IN THE FUTURE GEOTECHNICAL REPORT.



**PROJECT LOCATION**



## VICINITY MAP

N. T. S.

## GEOLOGICAL NARRATIVE

QAE: ALLUVIAL AND EOLIAN DEPOSITS (HOLOCENE TO UPPER PLEISTOCENE):-

MODERATELY SORTED GRAVEL, SAND, AND SILT DEPOSITED IN SMALL CHANNELS AND ON ALLUVIAL FLATS, AND WELL-SORTED, FINE- TO MEDIUM-GRAINED, REDDISH-BROWN EOLIAN SAND LOCALLY REWORKED BY ALLUVIAL PROCESSES; YOUNGER DEPOSITS (QAE) FORM ACTIVE DEPOSITIONAL SURFACES, WHEREAS OLDER DEPOSITS (QAE0) TYPICALLY FORM INCISED, INACTIVE SURFACES; AS MUCH AS ABOUT 20 FEET (6 M) THICK.

QES: EOLIAN SAND DEPOSITS (HOLOCENE TO UPPER PLEISTOCENE):-

WELL SORTED, FINE- TO MEDIUM-GRAINED, WELL-ROUNDED, FROSTED QUARTZ SAND; SAND IS RECYCLED PRINCIPALLY FROM THE NAVAJO SANDSTONE AND KAYENTA FORMATION; LOCALLY FORMS SMALL DUNES PARTLY STABILIZED BY VEGETATION; LOCALLY INCLUDES THICK CALICIC SOILS (HARDPAN OR CALICHE); TYPICALLY LESS THAN 20 FEET (6 M) THICK.

## LAND USE SUMMARY

TOTAL LOTS: 163  
AVERAGE LOT SIZE: 8804 SQ FT (0.202 AC)

## NOTES

STORM DRAINAGE WILL FOLLOW RECOMMENDATIONS GIVEN IN THE GATEWAY AT SAND HOLLOW DRAINAGE STUDY REPORT. WATER WILL BE CONVEYED USING CURB AND GUTTER AS WELL AS DRAINAGE PIPE. DRAINAGE WILL BE CARRIED TO A COMMUNAL BASIN BEFORE MOVING FURTHER DOWN STREAM. ANY OFFSITE DRAINAGE WILL BE ACCOUNTED FOR AND PIPE SIZED ACCORDINGLY.

## CURVE TABLE

CURVE	LENGTH	RADIUS	CHORD DIST.	CHORD BRG.	DELTA
C1	22.36'	15.00'	20.34'	S52°29'00"E	85°23'50"
C2	33.18'	173.94'	33.13'	S4°19'13"E	10°55'44"
C3	23.55'	15.00'	21.21'	S46°09'42"W	89°58'10"
C4	23.56'	15.00'	21.21'	S43°51'12"E	90°00'00"
C5	24.96'	226.00'	24.95'	S87°58'57"W	6°19'41"
C6	19.21'	173.94'	19.20'	N87°59'01"E	6°19'41"
C7	21.90'	15.00'	20.01'	N42°58'30"E	83°39'25"
C8	25.22'	15.00'	22.35'	S47°01'30"E	96°20'35"
C9	21.90'	15.00'	20.01'	N42°58'30"E	83°39'25"
C10	25.22'	15.00'	22.35'	N47°01'30"W	96°20'35"
C11	23.56'	15.00'	21.21'	S39°48'34"W	89°59'17"
C12	23.57'	15.00'	21.22'	N50°11'26"W	90°00'43"
C13	9.97'	177.50'	9.97'	N3°34'33"W	3°13'05"
C14	10.72'	177.50'	10.72'	N0°14'13"W	3°27'35"
C15	25.93'	222.50'	25.92'	S1°50'45"E	6°40'39"
C16	23.56'	15.00'	21.21'	N46°29'44"E	90°00'18"
C17	24.77'	15.00'	22.05'	N41°11'12"W	94°37'50"
C18	18.00'	222.50'	18.00'	S3°48'39"W	4°38'08"
C19	22.35'	15.00'	20.34'	N48°48'35"E	85°21'44"
C20	24.78'	15.00'	22.05'	S41°11'25"E	94°38'16"
C21	24.78'	15.00'	22.05'	S41°11'25"E	94°38'16"
C22	22.35'	15.00'	20.34'	S48°48'35"W	85°21'44"
C23	23.65'	15.00'	21.28'	N46°19'07"E	90°20'39"
C24	23.47'	15.00'	21.15'	N43°40'53"W	89°39'21"
C25	23.47'	15.00'	21.15'	N43°40'53"W	89°39'21"
C26	23.65'	15.00'	21.28'	N46°19'07"E	90°20'39"
C27	23.65'	15.00'	21.28'	S46°19'07"W	90°20'39"
C28	23.47'	15.00'	21.15'	N43°40'53"W	89°39'21"
C29	23.47'	15.00'	21.15'	S43°40'53"E	89°39'21"
C30	23.65'	15.00'	21.28'	N46°19'07"E	90°20'39"
C31	23.65'	15.00'	21.28'	S46°19'07"W	90°20'39"
C32	23.47'	15.00'	21.15'	S43°40'53"E	89°39'21"
C33	22.35'	15.00'	20.34'	S48°48'48"W	85°22'10"
C34	24.77'	15.00'	22.05'	N41°11'12"W	94°37'50"
C35	18.00'	222.50'	17.99'	N3°48'40"E	4°38'06"

## CURVE TABLE

CURVE	LENGTH	RADIUS	CHORD DIST.	CHORD BRG.	DELTA
C36	14.36'	177.50'	14.36'	S3°48'40"W	4°38'06"
C37	23.56'	15.00'	21.21'	N46°29'37"E	90°00'00"
C38	23.56'	15.00'	21.21'	S43°30'23"E	90°00'00"
C39	23.65'	15.00'	21.28'	N46°19'12"E	90°20'49"
C40	23.47'	15.00'	21.15'	S43°40'48"E	89°39'11"
C41	23.65'	15.00'	21.28'	S46°19'12"W	90°20'49"
C42	23.47'	15.00'	21.15'	N43°40'48"W	89°39'11"
C43	23.65'	15.00'	21.28'	N46°19'12"E	90°20'49"
C44	23.47'	15.00'	21.15'	N43°40'48"W	89°39'11"
C45	23.65'	15.00'	21.28'	S46°19'12"W	90°20'49"
C46	23.47'	15.00'	21.15'	N43°40'48"W	89°39'11"
C47	23.65'	15.00'	21.28'	S46°19'12"W	90°20'49"
C48	23.47'	15.00'	21.15'	S43°40'48"E	89°39'11"
C49	23.65'	15.00'	21.28'	S46°19'12"W	90°20'49"
C50	23.47'	15.00'	21.15'	S43°40'48"E	89°39'11"
C51	20.68'	12.02'	18.22'	N50°32'17"E	98°32'50"
C52	23.65'	15.00'	21.28'	S46°19'12"W	90°20'49"
C53	23.47'	15.00'	21.15'	S43°40'48"E	89°39'11"
C54	14.36'	100.00'	14.35'	N84°23'35"W	8°13'37"
C55	7.80'	100.00'	7.80'	N78°02'41"W	4°28'11"
C56	73.00'	65.00'	69.22'	S72°01'04"W	64°20'41"
C57	23.63'	15.00'	21.26'	N46°21'22"E	90°16'30"
C58	58.31'	65.00'	56.37'	S14°08'53"W	51°23'41"
C59	21.17'	95.00'	21.13'	S5°09'55"E	12°46'05"
C60	31.32'	20.00'	28.22'	N43°38'38"W	89°43'30"
C61	31.51'	20.00'	28.35'	N46°21'22"E	90°16'30"
C62	31.54'	20.00'	28.37'	S46°19'12"W	90°20'49"
C63	31.29'	20.00'	28.20'	S43°40'48"E	89°39'11"
C64	25.17'	15.00'	22.32'	S49°12'46"W	96°07'56"
C65	81.30'	398.81'	81.16'	N76°52'52"W	11°40'48"
C66	34.03'	450.81'	34.02'	N73°12'14"W	4°19'31"
C67	106.12'	450.81'	105.87'	N82°06'36"W	13°29'13"
C68	23.58'	15.00'	21.23'	N43°49'03"W	90°04'20"
C69	23.54'	15.00'	21.20'	S46°10'57"W	89°55'40"
C70	23.58'	15.00'	21.23'	N43°49'03"W	90°04'20"

## STRAWBERRY FIELDS LEGAL DESCRIPTION

BEGINNING AT A POINT LOCATED S88°30'47"E ALONG THE SECTION LINE 1903.48 FEET FROM THE WEST QUARTER CORNER OF SECTION 12, TOWNSHIP 42 SOUTH, RANGE 14 WEST, SALT LAKE BASE AND MERIDIAN; THENCE N01°29'50"E 311.22 FEET; THENCE S88°34'15"E 29.41 FEET; THENCE N01°30'03"E 252.04 FEET; THENCE S88°30'44"E 15.41 FEET; THENCE N01°30'06"E 144.99 FEET; THENCE S88°36'31"E 16.47 FEET; THENCE N01°29'25"E 99.98 FEET; THENCE S88°28'11"E 5.00 FEET; THENCE N01°29'08"E 162.00 FEET; THENCE N05°02'42"W 268.18 FEET; THENCE N84°49'07"E 916.00 FEET; THENCE S05°10'55"E 27.75 FEET; THENCE SOUTHERLY ALONG THE ARC OF A 200.00 FOOT RADIUS CURVE TO THE RIGHT A DISTANCE OF 22.09 FEET, THROUGH A CENTRAL ANGLE OF 06°19'43" (LONG CHORD BEARS: S02°01'04"E 22.08 FEET); THENCE S01°08'48"W 462.77 FEET; THENCE EASTERLY ALONG THE ARC OF A NON-TANGENT CURVE TO THE RIGHT, HAVING A RADIUS OF 424.81 FEET AND A RADIAL BEARING OF S01°08'48"W, A DISTANCE OF 132.06 FEET, THROUGH A CENTRAL ANGLE OF 17°48'44" (LONG CHORD BEARS: S79°56'51"E 131.53 FEET); THENCE EASTERLY ALONG THE ARC OF A 424.81 FOOT RADIUS REVERSE CURVE TO THE LEFT (LONG CHORD BEARS: S79°56'51"E 131.53 FEET), CENTER POINT LIES N18°57'31"E THROUGH A CENTRAL ANGLE OF 17°48'44" A DISTANCE OF 132.06 FEET; THENCE S88°51'12"E 168.49 FEET; THENCE S01°13'07"W 1084.98 FEET; THENCE N88°30'25"W 1337.50 FEET; THENCE N01°30'13"E 282.05 FEET; THENCE N88°31'47"W 49.35 FEET; THENCE N01°29'50"E 10.25 FEET TO THE POINT OF BEGINNING.

AREA CONTAINS 1919721 SQUARE FEET OR 44.071 ACRES.

## OWNERS:

WESTERN MORTGAGE & REALTY COMPANY  
TO BE SOLD TO LIFETIME HOMES INC.

## CONTACT REPRESENTATIVE:

JIMMY DEGRAFFENRIED  
(801)-830-5490  
LIFETIMEHOMESINC@GMAIL.COM

NO.	REVISION	DATE	BY

**PROVALUE ENGINEERING, INC.**  
Engineers - Land Surveyors - Land Planners  
20 South 850 West, Suite 1  
Hurricane City, Utah 84751  
Phone: (435) 868-8801



PRELIMINARY PLAT FOR:  
**STRAWBERRY FIELDS ESTATES**  
LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.&M.  
SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737

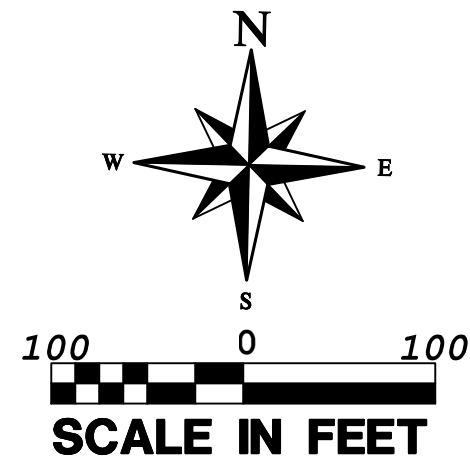
DATE: 06/02/2022  
SCALE: N/A  
JOB NO:  
694-001  
SHEET NO:  
**1 OF 4**

# FULL SITE PRELIMINARY PLAT FOR: STRAWBERRY FIELDS ESTATES

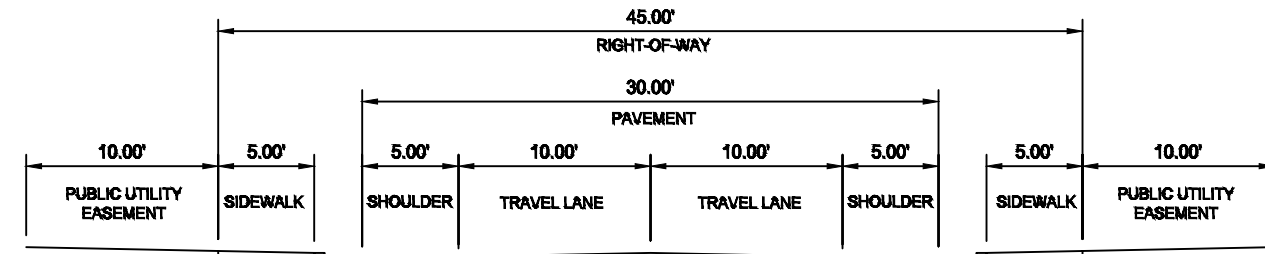
LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.&M.  
SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737

### LEGEND

- PROPERTY LINE
- ADJACENT PROPERTY LINE
- STREET CENTERLINE
- 10' PUBLIC UTILITY EASEMENT (PUE)
- SECTION LINE
- STRAWBERRY FIELDS ESTATES BOUNDARY
- PHASE LINE
- EXISTING MINOR CONTOUR 1'
- EXISTING MAJOR CONTOUR 5'
- PROPOSED PUBLIC UTILITY EASEMENT
- SECTION CORNER AS DESCRIBED

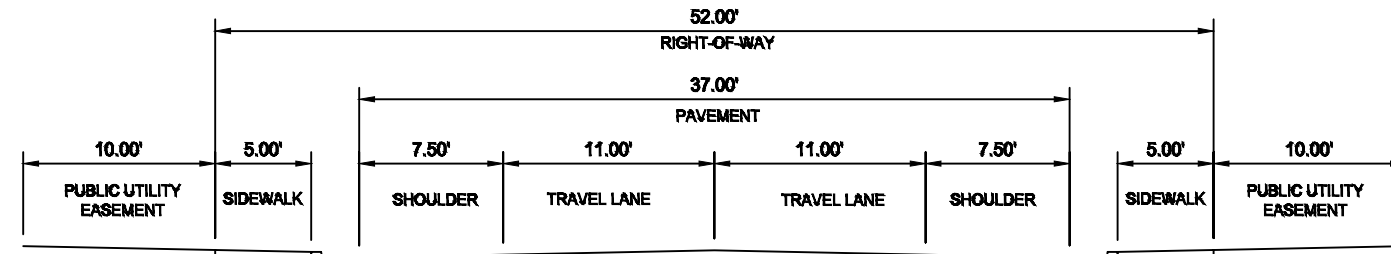


#### RESIDENTIAL LOCAL



#### 45' ROAD TYPICAL SECTIONS

#### RESIDENTIAL STANDARD



#### 52' ROAD TYPICAL SECTIONS

2642.11' S0107'24"W BASIS OF BEARING

2643.24' S01103'06"W

1903.48' S88°30'47"E

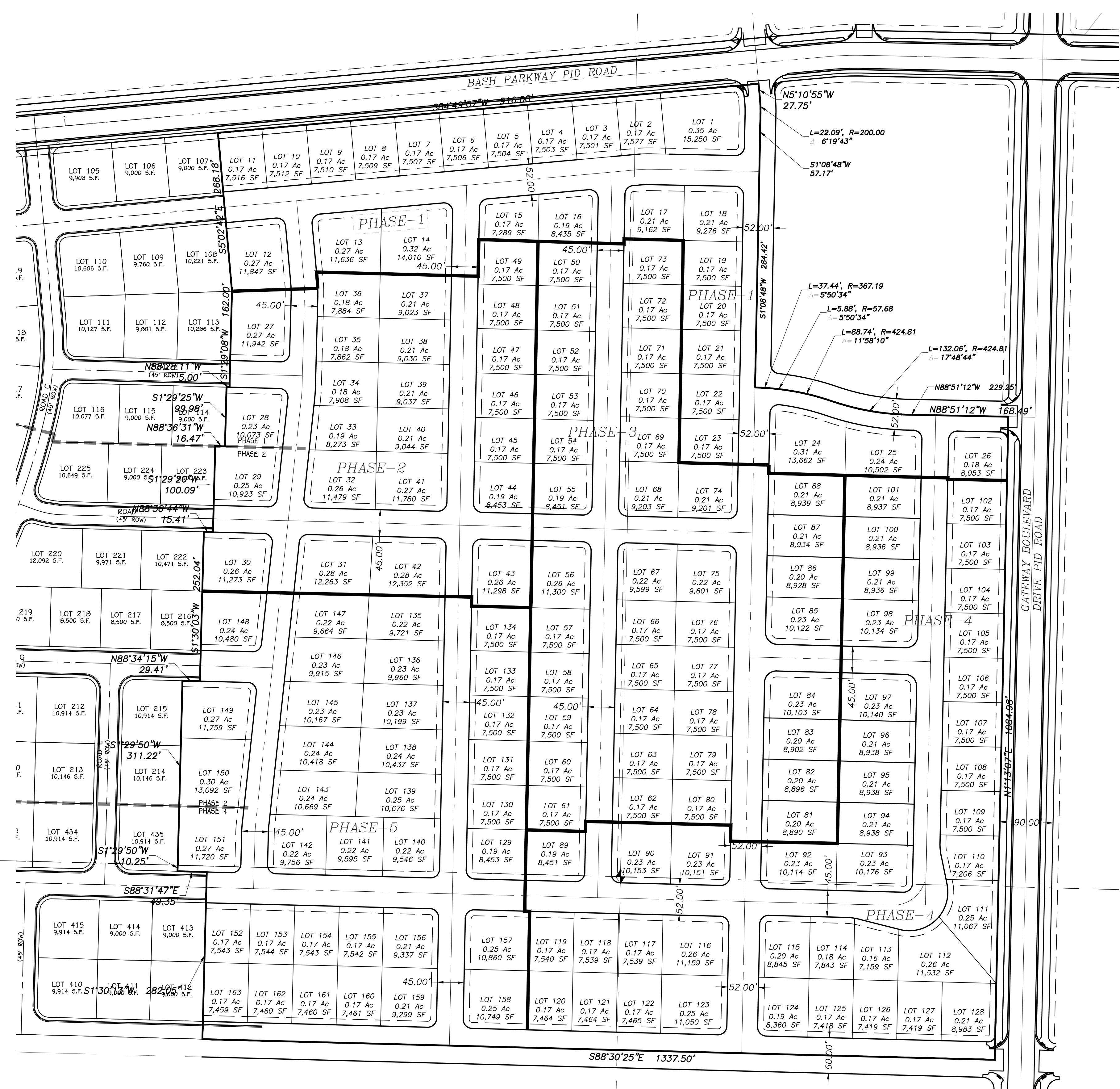
2643.24' S01103'06"W

2643.24' S01103'06"W

FOUND BRASS CAP  
SET 1978  
NORTHWEST CORNER  
SECTION 12,  
T42S, R14W, S.L.B.&M.

FOUND BRASS CAP MONUMENT  
WEST QUARTER CORNER  
SECTION 12,  
T42S, R14W, S.L.B.&M.

FOUND BRASS CAP  
MONUMENT  
SOUTHWEST CORNER  
SECTION 12,  
T42S, R14W, S.L.B.&M.



NO.	REVISIONS	DESCRIPTION	DATE	BY

**PROVALUE ENGINEERING, INC.**  
Engineers - Land Surveyors - Land Planners  
20 South 950 West, Suite 1  
Hurricane, Utah 84751  
Phone: (435) 868-8801



PRELIMINARY PLAT FOR:  
**STRAWBERRY FIELDS ESTATES**  
LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.&M.  
SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737

DATE: 06/01/2022
SCALE: 1"=100'
JOB NO: 694-001
SHEET NO: <b>2 OF 4</b>

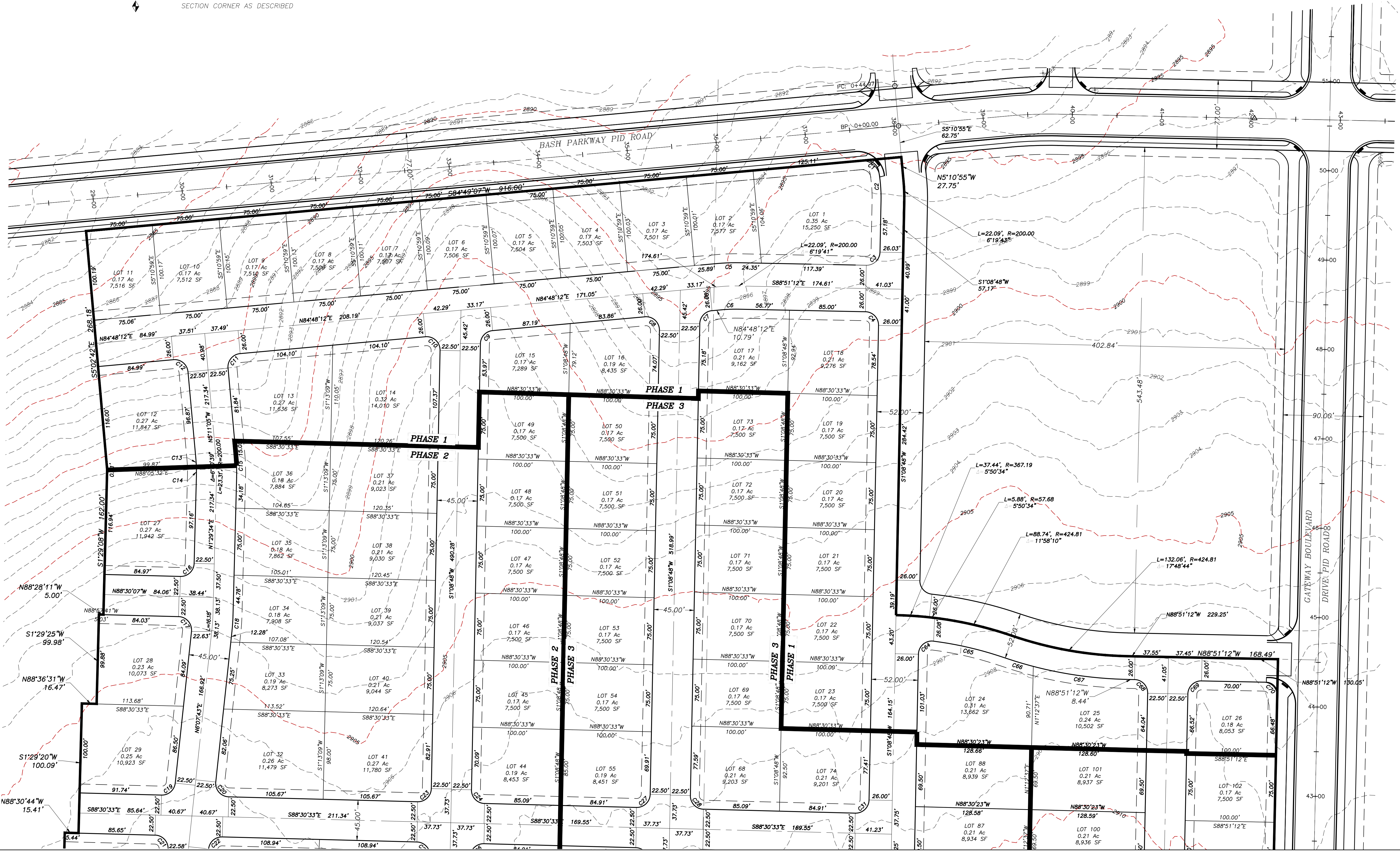
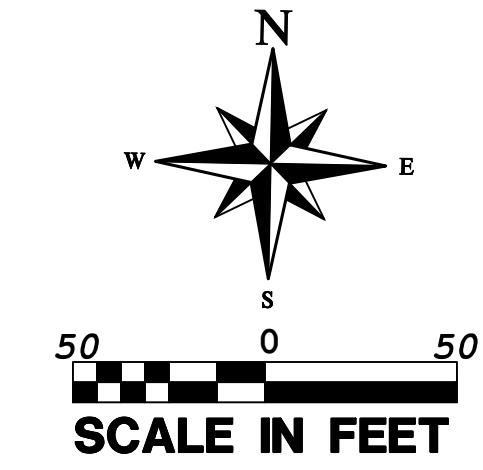
COPYRIGHT © 2013 PROVALUE ENGINEERING, INC.

# PRELIMINARY PLAT FOR: STRAWBERRY FIELDS ESTATES

LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.#M.  
SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737

## LEGEND

- PROPERTY LINE
- ADJACENT PROPERTY LINE
- STREET CENTERLINE
- 10' PUBLIC UTILITY EASEMENT (PUE)
- SECTION LINE
- STRAWBERRY FIELDS ESTATES BOUNDARY
- PHASE LINE
- EXISTING MINOR CONTOUR 1'
- EXISTING MAJOR CONTOUR 5'
- PROPOSED PUBLIC UTILITY EASEMENT
- SECTION CORNER AS DESCRIBED



NO.	REVISIONS	DESCRIPTION	DATE	BY

**PROVALUE ENGINEERING, INC.**  
Engineers - Land Surveyors - Land Planners  
20 South 850 West, Suite 1  
Hurricane, Utah 84753  
Phone: (435) 868-8801



PRELIMINARY PLAT FOR:  
**STRAWBERRY FIELDS ESTATES**  
LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.#M.  
SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737

DATE: 06/02/2022  
SCALE: 1"=50'

JOB NO:  
694-001

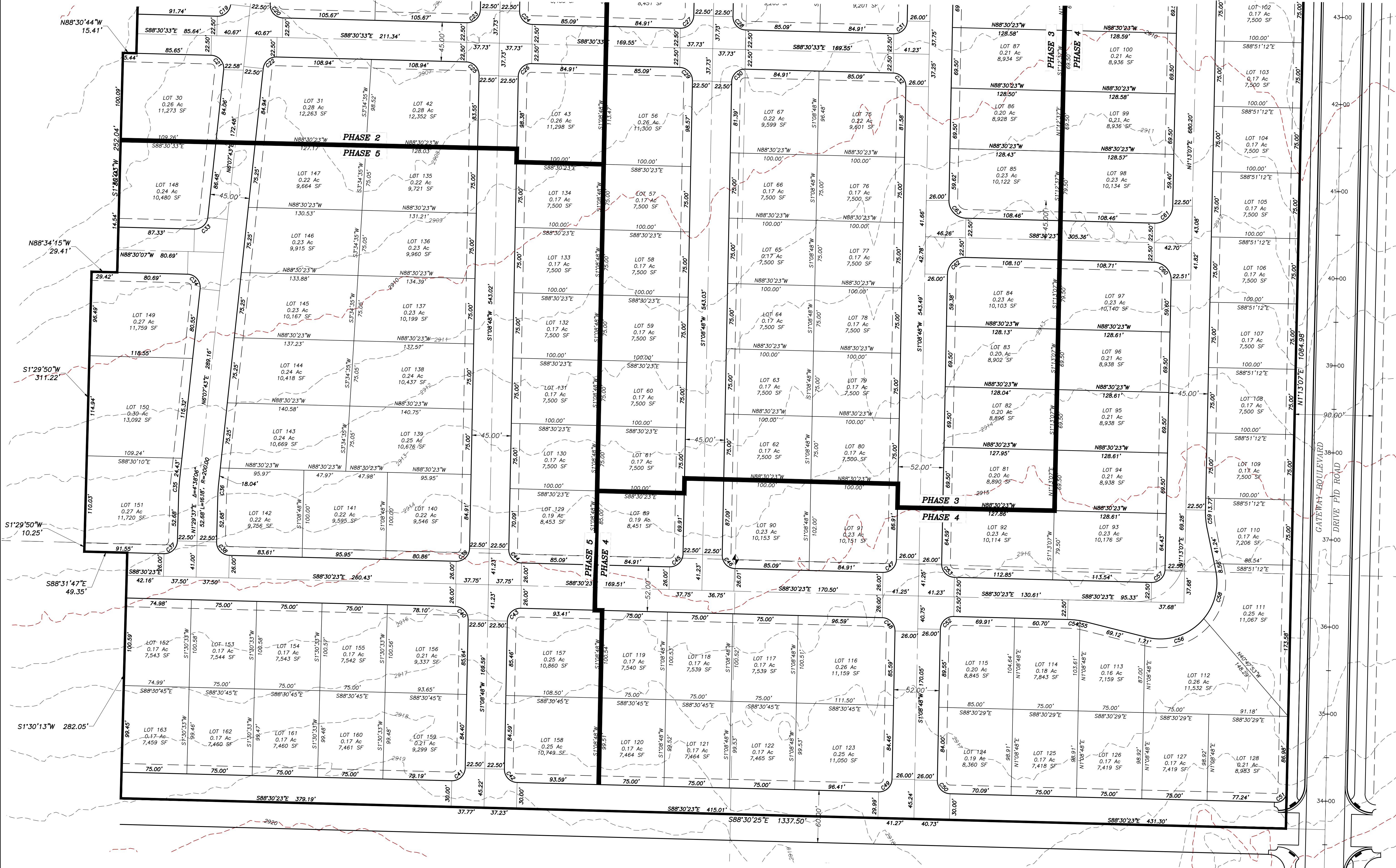
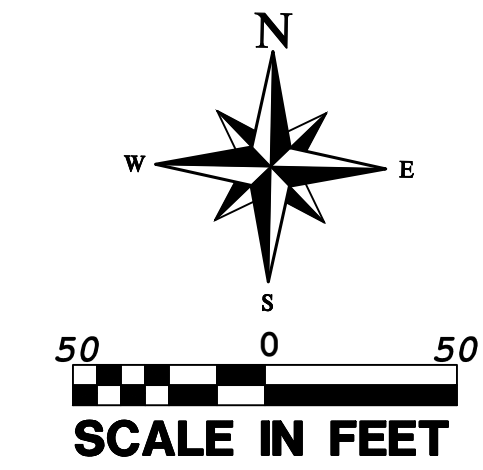
SHEET NO:  
**3 OF 4**

**LEGEND**

- PROPERTY LINE
- ADJACENT PROPERTY LINE
- STREET CENTERLINE
- - - 10' PUBLIC UTILITY EASEMENT (PUE)
- SECTION LINE
- PHASE LINE
- STRAWBERRY FIELDS ESTATES BOUNDARY
- - - PROPOSED PUBLIC UTILITY EASEMENT
- - - EXISTING MINOR CONTOUR 1'
- - - EXISTING MAJOR CONTOUR 5'
- SECTION CORNER AS DESCRIBED

# PRELIMINARY PLAT FOR: STRAWBERRY FIELDS ESTATES

LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.#M.  
SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737



NO.	REVISIONS	DATE	BY

**PROVALUE ENGINEERING, INC.**  
Engineers - Land Surveyors - Land Planners  
29 South 950 West, Suite 1  
Hurricane, Utah 84753  
Phone: (435) 868-8801



**PRELIMINARY PLAT FOR:  
STRAWBERRY FIELDS ESTATES**  
 LOCATED IN SECTION 1, T42S, R42S, R14W, S.L.B.#M.  
 SAND HOLLOW RD, HURRICANE, WASHINGTON COUNTY, UTAH 84737

DATE: 06/02/2022  
 SCALE: 1"=50'  
 JOB NO. 694-001  
 SHEET NO. **4 OF 4**

**Hurricane Planning Commission  
Meeting Minutes  
May 17, 2023**

Minutes of the Hurricane City Planning Commission meeting held on May 17, 2023, at 5:00 p.m. in the City Council Chambers located at 147 N. 870 West Hurricane UT, 84737

**Members Present:** Mark Sampson, Paul Farthing, Shelley Goodfellow, Rebecca Bronemann, Brad Winder, and Kelby Iverson

**Members Excused:** Ralph Ballard and Michelle Cloud.

**Staff Present:** Planning Director Stephen Nelson, Assistant Planner Fred Resch III, City Attorney Dayton Hall, Planning Technician Brienna Spencer, and City Engineer Representative Jeremy Pickering

**5:00 p.m. - Call to Order**

Roll Call

Pledge of Allegiance, led by Paul Farthing

Prayer and/or thought by invitation given by Shelley Goodfellow

Declaration of any conflicts of interest

**NEW BUSINESS**

**1. Discussion and consideration of a recommendation to the City Council on a proposed amendment to the Hurricane City Standards for Design and Construction and Hurricane City's Drainage Manual to require development within the Frog Hollow Sub-basin to retain stormwater from 10-year storm events.**

Stephen Nelson explained that during the last meeting, the Planning Commission discussed amending the drainage manual. This amendment would change the drainage standard for those in the Frog Hollow sub-basin area. He reported that ProValue prepared preliminary numbers and estimates to guide the discussion. Jeremy Pickering reviewed a detention hydrograph example. He explained the method for determining detention and retention volumes. Washington County Water Conservancy District has requested that the city require property owners within this basin area to retain rainwater until it exceeds a ten-year storm. He noted that Hurricane City standards require basin drainage within twenty-four hours. Mr. Nelson reported that there are large sections north of 3000 South with drainage swells designed to retain water. These swells will not be affected by this standard. Karl Rasmussen reviewed the findings of his study. He noted that the cubic feet requirement for the detention of a ten-year storm is similar to a one-hundred-year storm. If the City does not accept this change to the drainage manual, every property owner must retain the water, not just detain it. The district is being gracious by allowing the release of anything over a ten-year storm into the existing basin. Mr. Rasmussen explained that the Conservancy District wants the extraction overlay to go before the Council after the drainage manual is amended. If the City Council approves the drainage manual amendment and extraction overlay, Balance

of Nature will work with the Conservancy District to take the excess material from the pond. Mr. Nelson reported that the Conservancy District is concerned about the contamination of the clay pit from the first flow of water. Their concern is that contamination makes the clay unusable for dam construction.

Kelby Iverson shared that this will affect 20,000 acres of Bureau of Land Management land and 3,000-5,000 acres of private land, and further development in the Frog Hollow area will heighten historical flows. He asked for clarification on the current retention and detention standard. Mr. Pickering explained that the current standard requires comparing three diverse types of storms, and the highest volume must be detained on the property. Mr. Pickering reviewed the difference between full retention and partial retention/detention. If the entire area is required to retain fully, the basins will be larger than a detention basin. Mr. Iverson's primary concern is that property owners have purchased a property with a specific understanding of the requirements for development; this change could require more of them. He inquired about the requirements for one-acre lots and why a detention basin was required for Moccasin Flats. Mr. Pickering reported that the impact of a single-family home on one acre would be negligible and usually does not require an analysis. Due to the significant change to the area, Moccasin Flats was required to have a detention basin.

Shelley Goodfellow read page thirty of the drainage manual. She would like to know if any pollution has entered the Conservancy District's clay pit within the past twenty years. Mr. Nelson can't speak directly to pollution of the clay pits, however, the Conservancy District has brought up these concerns several times in the past. She asked if it is necessary to update the drainage manual if the current zoning is kept in place. Retention basins are larger than detention basins; this will increase the need for mosquito abatement, and ponds represent a safety concern. She feels that Balance of Nature and the Conservancy District are impacting this portion of land, and now they are passing the cost to surrounding property owners.

Paul Farthing feels the impact will be negligible and not burden taxpayers or landholders. The clay is needed for Toquerville and Warner Valley reservoirs. Rebecca Bronemann feels that the Conservancy District needs to be more specific and provide studies for their request.

*Shelley Goodfellow motioned a recommendation of denial to the City Council based on the findings that a study has not been completed to know the impact and necessity for this and that the current zoning will have a minimal impact; it contradicts paragraph one of the same section within the drainage manual regarding safety and other reasons for not creating retention ponds within the City. The financial burden of the Washington County Conservancy District and Balance of Nature development is being passed onto other property owners. This will be imposed on the affected property owners without direct notice. The detention basins will become unsightly and increase mosquitoes. Kelby Iverson seconded the motion. Brad Winder, Kelby Iverson, and Shelley Goodfellow – aye. Paul Farthing, Rebecca Bronemann, and Mark Sampson – nay. Sending a split vote to the City Council.*

Martha Honey lives in Sky Ranch. She stated that agriculture runoff also contains toxins. She commented that the low spots will eventually be populated. This is a complicated situation, and everyone needs to work together.

A representative of Balance of Nature shared that they are building retention ponds to hold the water. Their intent is not to pass this burden onto surrounding property owners. However, Balance of Nature is only required to retain the water on their property, they are not required to retain the water that drains onto their property. They are trying to be good neighbors and create a beautiful area.

*Kelby Iverson motioned to dismiss. Paul Farthing seconded the motion. Unanimous.*

**Adjournment at 6:13 p.m.**

DRAFT

**Hurricane Planning Commission  
Meeting Minutes  
May 25, 2023**

Minutes of the Hurricane City Planning Commission meeting held on May 25, 2023, at 5:00 p.m. in the City Council Chambers located at 147 N. 870 West Hurricane UT, 84737

**Members Present:** Mark Sampson, Paul Farthing, Shelley Goodfellow, Ralph Ballard, Michelle Cloud, Rebecca Bronemann, Brad Winder, and Kelby Iverson

**Members Excused:**

**Staff Present:** Planning Director Stephen Nelson, Assistant Planner Fred Resch III, City Attorney Dayton Hall, Planning Technician Brienna Spencer, and City Engineer Representative Jeremy Pickering

**5:00 p.m. - Planning Commission Business:**

**1. Presentation and Discussion on the Downtown Master Plan**

Stephen Nelson reviewed the status of the Downtown Master Plan. He reported that Hurricane City applied for a grant from the Utah Department of Transportation (UDOT) and was awarded \$45,000 to help fund projects within the downtown area that impact UDOT roadways. Critical sections of the Downtown Master Plan include existing conditions, market analysis, public input recommendations, and implementation strategies. An analysis of the ground conditions determined the existing conditions.

The Steering Committee determined that the review of ground conditions should focus on the six blocks between 300 North and 200 South. The consultant reviewed the local market and services during the market analysis to determine economic potential. The analysis determined that specific retail stores are lacking in the downtown area. The public input synopsis summarizes public feedback. It was heard from the public and Steering Committee that a catalyst site should be determined. The catalyst site is located between 100 West and 200 West on State Street. The site was chosen because the city owns a large portion, and it is used for the recreation center. The City Council supports keeping the existing fine arts and school building, with redevelopment along the frontage for commercial development.

Brad Winder asked if Downtown Master Plan recommends any structures to be demolished. Mr. Nelson explained that the plan is passive and recommends that the City incorporate tools and incentives. The plan also recommends adopting and enforcing the internal property maintenance code. The code is a set standard that has been upheld in court. Mark Sampson asked if the city has had a downtown plan in the past. Mr. Nelson reported that the city contracted a consultant who proposed planning methods and guidelines. However, there has not been a formal Downtown Master Plan. Ralph Ballard asked if the plan encourages historic buildings in the downtown area. Mr. Nelson explained that the goal is for new structures to have the same look and feel as the classic main street.

Mr. Nelson reviewed further recommendations of the Downtown Master Plan. There were recommendations for high-visibility crosswalks. The Planning Commission discussed their safety concerns regarding general traffic, speed limits, and traffic signals. Mr. Nelson reported that the land use recommendations could be resolved under a form-based code. He explained that a form-based code focuses on form and type of development. Mrs. Goodfellow asked Mr. Nelson to review the short-term rentals section. Mr. Nelson reported that the city asked the consultant to provide recommendations on

balancing short-term rentals in the downtown area. The recommendations include spacing rental complexes, parking, and prioritizing rentals in targeted development. The plan recommends creating a special short-term or transient lodging section within Hurricane City Code.

**6:00 p.m. - Call to Order**

Roll Call

Pledge of Allegiance led by Paul Farthing.

Prayer and/or thought by invitation given by Ralph Ballard.

*Kelby Iverson motioned to approve the agenda. Ralph Ballard seconded the motion. Unanimous.*

Declaration of any conflicts of interest.

**Public Hearings**

**1. A Request for a Variation of Zoning Standards within the Canyon Villas development for the definition of a park model, which limits each unit's size to 400 sq ft without the issuance of "room additions" by a building permit. This variation would allow the applicant to develop a park model development with larger units. The approved 180 units and density remain the same. Located at approximately 250 N and 2800 W.**

Carol Tanner lives in Sky Ridge. Ms. Tanner stated that the request is triple the current code and will result in parking spaces within setbacks and fire safety zones. There is no way to guarantee the success of this development, and the surrounding community hopes this does not become a short-term rental mobile park.

Multiple attendees agree with Ms. Tanner's comments.

Rick Williams concurs with Ms. Tanner. Many of the neighbors are unhappy about this variation request. However, the development is better than a traditional RV park. The residents would like an eight-foot sound wall to separate the properties, a three-foot walking wall, and limiting the unit size to two bedrooms. They would also like the homes to remain harmonious with the surrounding homes.

Jobe Venuti reported that the easement is already recorded and addressed. He thinks this will be a good product and will not affect surrounding property values.

**OLD BUSINESS**

**1. 2022-PP-28: Discussion and consideration of a recommendation to the City Council on a preliminary plat for Kolob Views, a 20 lot single family subdivision located at 850 S 1300 W. Stratton Development Applicant. Colt Stratton Agent.**

Kyson Spendlove reported that the applicant feels this is the best plan for the property and ties it into the existing developments. Stephen Nelson reported that there are four non-compliant items with the current design. The applicant failed to submit the sensitive lands application and will serve letters from sewer and water. It does not comply with the transportation master plan, and there was a failure to place property notes on the plan. Mr. Nelson reviewed the master planned roadway for this area. He reported

that staff recommends continuing the roadway along the boundary line and denying the plat as presented.

Mr. Spendlove reported that the applicant feels the master plan roadway does not make sense, and this proposal does not take property for the roadway. Mr. Nelson explained that based on Alpha Engineering's preliminary site work, the City Council wants to follow the master plan alignment. Paul Farthing lives in the area and opposes the master plan road in a forty-year-old neighborhood. He noted slope issues at the intersection of this road and 650 South. Mr. Nelson reported the sensitive land application is necessary due to the thirty percent slope. Shelley Goodfellow agrees that the sensitive land applicant is important. She also agrees with Mr. Farthing's statement that master plans do not always consider the typography of the area. Kelby Iverson feels this is a better idea for the roadway. Rebecca Bronemann asked if this could be sent to the City Council with the outstanding issues. Mr. Nelson recommends denying the application. Even if the master plan roadway was not an issue, there are other problems with the application. He reported that the topography is considered when planning roadway designs. Michelle Cloud feels denying the applicant due to the missing documents is necessary. Jeremy Pickering reviewed the connections of the master planned roadway and explained that minor collector roads are meant to discourage parking on the roadways.

*Kelby Iverson motioned a recommendation of denial of 2022-PP-28 to the City Council based on documentation that is not available yet and the roadways that are not aligning with the Transportation Master Plan. Noting that the Planning Commission is inclined to support Interstate Rock's realignment of the roadway. Shelley Goodfellow seconded the motion. Unanimous.*

## NEW BUSINESS

**1. 2023-ZC-12: Discussion and consideration of a recommendation to the City Council on a Request for a Variation of Zoning Standards within the Canyon Villas development for the definition of a park model, which limits each unit's size to 400 sq ft without the issuance of "room additions" by a building permit. This variation would allow the applicant to develop a park model development with larger units. The approved 180 units and density remain the same. Located at approximately 250 N and 2800 W. Sunwood Homes of Southern Utah Applicant. James Jessop Agent.**

Stephen Nelson reported that the Planning Commission approved this plan as a park model development without a zone change. However, while meeting with the developer, he realized that the proposed units are not park models. Hurricane City code allows the expansion of park models with a building permit. The applicant has provided a cross-section and additional pedestrian circulation plans. Michelle Cloud feels this answers the four zone change question in the affirmative and asks for clarification of ownership. The applicant's representative explained that the applicant would own that land, and the buyer would own the unit. Mr. Nelson reported that a park model subdivision could not be subdivided per code. Rebecca Bronemann asked for clarification on the parking plan. The representative reported there is room for a two-car driveway on each lot. Kelby Iverson clarified that the increased size of the units decreases the open space and asked if this request is allowed under the code. Mr. Nelson reviewed the developments allowed within the mobile home/RV code. Shelley Goodfellow asked how the increased unit size could accommodate the required parking. The representative shared that the unit's length was increased, not the width. Mrs. Goodfellow prefers to consider the parking based on bedroom count; she also prefers two bedrooms.

Ralph Ballard asked for clarification of the lease terms. Brett Howcroft shared that this is a thirty-year mortgage with up to a forty-year lease. Mr. Ballard asked about the permeable open space on the lots. Mr. Nelson shared that only fifty percent of the development can be occupied by structures. Paul Farthing feels a four hundred-square-foot home is ideal for retirees. However, this proposal increases

the development to one hundred eighty units with one thousand square feet or more per unit. This will increase the number of people living in the park and create a less than ideal development in the Sky Mountain area. Mr. Ballard shared that residents were negatively impacted when Zion's Gate RV/Brentwood was sold to a new owner, and the lot rent increased. He is concerned about the short-term land lease and would prefer private ownership of the lots. Mrs. Goodfellow feels there is a market for young families that need more than one bedroom. However, three-bedroom units will affect the neighbors.

*Shelley Goodfellow motioned a recommendation of approval of 2023-ZC-12 to the City Council with a limit of up to 800 square feet per unit. Paul Farthing seconded the motion. Michelle Cloud – aye, Rebecca Bronemann – aye, Shelley Goodfellow – aye, Ralph Ballard – aye, Brad Winder – aye, Paul Farthing – aye, Mark Sampson – aye, and Kelby Iverson – aye. Unanimous.*

**2. 2022-PP-13: Discussion and consideration of a possible approval of an extension of the preliminary plat approval for Sky Valley West, a 24-lot single family subdivision located at 600 N and 3400 W. Frank Lindhardt/Shoshone Land Company Applicant.**

Frank Lindhardt reported that the preliminary plat was approved last year. However, the applicant chose to slow down construction due to market changes. Jeremy Pickering noted that there is a Dixie Power transmission line across the property.

*Kelby Iverson motioned to approve the extension of 2022-PP-13 subject to staff and JUC comments. Paul Farthing seconded the motion. Unanimous.*

**3. 2023-PP-15: Discussion and consideration of a recommendation to the City Council on a preliminary plat for Sky Ridge PDO, a two-lot subdivision located at 100 N 2600 W. Shadow Glen 420 Inc Applicant. Frank Lindhart Agent.**

Stephen Nelson reported that the will serve letters were received.

*Michelle Cloud motioned a recommendation of approval of 2023-PP-15 to the City Council. Ralph Ballard seconded the motion. Unanimous.*

**4. 2023-AFP-11: Discussion and consideration of a recommendation to the City Council on an amended final plat for Sky Valley Phase 1A located at approximately 3100 W Coronado Dr. Shoshone Land Co Applicant. Frank Lindhardt Agent.**

Frank Lindhardt reported that these units were initially approved as townhomes. However, due to opposition from residents in the area, the applicant has changed the project to single-family homes. Stephen Nelson reported that the plat does not meet the amended plat standard, and staff has concerns regarding the common space.

*Paul Farthing motioned a recommendation of approval of 2023-AFP-11 subject to staff and JUC comments. Brad Winder seconded the motion. Unanimous.*

**5. 2023-CUP-10: Discussion and consideration of a possible approval of a sign of greater height and size located at 505 S Sand Hollow Road. Blow Sand LLC/Glampers Inn Applicant. Turnbow Signs Agent.**

Chad Christensen represents Turnbow Signs. Stephen Nelson reported Hurricane City sign ordinance allows a maximum size of 200 square feet. However, if they combine multiple tenants on a single sign,

the ordinance gives the Planning Commission authority to approve a greater size. The staff recommends approving the increased square footage, not the height. Mark Sampson asked how this is different from the fifty-five-foot sign on I-15. Mr. Nelson reported that the height of the sign on I-15 was approved under the PDO code. Shelley Goodfellow feels the sign ordinance is in place for a reason, and the sign is functional without the increased height. Rebecca Bronemann noted that four-story buildings are not allowed within the city, and there is no reason the sign should exceed those standards. Kelby Iverson asked if the sign and lantern were reduced to meet the code, would it still accomplish the applicant's task? Chad Christensen reported that it could meet the applicant's needs. However, the applicant's primary concern is joint investors.

*Michelle Cloud motioned to approve 2023-CUP-10 for greater size and deny the greater height with a limit of thirty-five feet. Noting that the code allows does not allow a greater height sign. Shelley Goodfellow seconded the motion. Unanimous.*

**6. 2023-PSP-12: Discussion and consideration of a possible approval of a preliminary site plan for the LDS Seminary Building located at 310 W 100 S. Mike Davey, BHD Architects Applicant.**

Mike Davey reported that the existing seminary building is not structurally sound due to collapsing soils. The church proposes to build a new building with improvements on the site. Stephen Nelson noted that it may be necessary to grant an exception for the parking. Rebecca Bronemann asked if it was necessary to combine the lots. Mr. Davey shared that they are in the process of combining the parcels into one lot.

*Kelby Iverson motioned to approve 2023-PSP-23 subject to staff and JUC comments. Rebecca Bronemann seconded the motion. Unanimous.*

**7. 2023-PP-16: Discussion and consideration of a recommendation to the City Council on a preliminary plat for Azure Ridge, a two-lot commercial subdivision located at the northwest corner of Turf Sod Road and Sand Hollow Road. David Crowther Applicant. Dave Nasal Agent.**

Stephen Nelson reviewed the history of the parcel. The applicant wants to subdivide the parcel to prepare the property for development and work through the roadway dedication. The applicant is proposing roadway dedications on sand hollow and turf side roads. He noted that the will serve letters have been provided.

*Rebecca Bronemann motioned a recommendation of approval of 2023-PP-16 subject to staff and JUC comments. Paul Farthing seconded the motion. Unanimous.*

**8. 2023-PSP-13: Discussion and consideration of a possible approval of a preliminary site plan for Rush Funplex, a family fun center located at the corner of N Coral Canyon Blvd and Foothills Canyon Dr. The Rush Funplex Applicant. Matt Gertge Agent.**

Stephen Nelson shared that the Rush Funplex is similar to Fiesta Fun. However, the lot line boundaries on the preliminary site plan do not exist, and the proposed uses for shared parking still need to be solidified. He noted that approval of the preliminary site plan allows the applicant to proceed with detailed planning and does not give entitlement.

*Shelley Goodfellow motioned to approve 2023-PSP-13 subject to any staff and JUC comments. Rebecca Bronemann seconded the motion. Unanimous.*

**9. 2023-PSP-14: Discussion and consideration of a possible approval of a preliminary site plan for Hurricane West LDS Meetinghouse, located at 4300 W Dixie Springs Dr. Brittany Johnson, Uncommon Architects Applicant. Darin Mano Agent.**

Stephen Nelson explained that a larger parcel was reserved for the church building when the subdivision was created, and the applicant is taking additional lots to accommodate parking for the church. The applicant has provided two alternatives for access. However, engineering would prefer single access for traffic flow, but either access could work. Brittany Johnson noted that two points of access are common for church parking. The applicant understands that the access is a variance from the zoning code. However, the owner only wants to shift the points of access if required to do so. Ms. Johnson reviewed the proposed access points.

*Shelley Goodfellow motioned to approve 2023-PSP-14, allowing the applicant to choose whichever parking layout they want. Kelby Iverson seconded the motion. Unanimous.*

**10. Discussion on the Pearce Cabins onsite management plan**

Stephen Nelson reviewed the history of the overall project. The applicant has asked to meet and discuss the onsite management code with the Planning Commission. The applicant provided different management options that feel they meet the intent of the code and why the code is excessive for their use. Mr. Nelson reviewed the updated short-term rental code. Dayton Hall thinks the code is clear, and this development falls within the definition requiring the management office to remain continuously open. Chris Pearce understands the code. Phase one of this development was approved without this requirement, and he requests approval of the remaining phases under a continuation of the previous approval. The code was changed in the middle of development, and dealing with this is cumbersome. Mr. Nelson reported that phase one could continue without this requirement. However, any extension of the project will have to comply with the new code. Mr. Hall explained that the current use is grandfathered into the previous code. Expansion of the use excludes the development from the grandfathered protection. Brent Pearce reported that the applicant expressed their intent to expand the project when approval was granted for phase one. Mr. Nelson stated that it is essential to note that phase one was brought in as an independent project, and phase two was not reviewed or approved by the city. Shelley Goodfellow feels eleven units is significantly higher and is closer to the size of a motel. Mr. Pearce shared that twenty-hour onsite management is a financial burden. The Planning Commission does not feel there is a substantial reason to consider an exception to the code.

**11. Continued discussion and presentation on the Downtown Master Plan (if needed)**

**12. Discussion and presentation on inclusionary zoning**

Stephen Nelson reviewed the changes and definitions added to the inclusionary zoning. He noted that additional sections were added to clarify and capture the Planning Commission's previous discussion of the code.

**Approval of Minutes:**

**1. April 27, 2023**

Kelby Iverson requested to clarify his opposition to the drainage standards for Frog Hollow. His opposition was not pertaining to accommodating the Water District. He opposed enforcing the drainage standard upon those willing to comply with the underlying zoning. This change pertains to line twenty-six of page six.

*Kelby Iverson motioned to approve the minutes as amended. Rebecca Bronemann seconded the motion. Unanimous.*

Paul Farthing motioned to adjourn the meeting. Michelle Cloud seconded the motion. Unanimous.

**Adjournment at 9:03 p.m.**

DRAFT