



# Utah Transit Authority

## Board of Trustees

### REGULAR MEETING AGENDA

669 West 200 South  
Salt Lake City, UT 84101

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**Wednesday, April 8, 2026**

**9:00 AM**

**FrontLines Headquarters**

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The UTA Board of Trustees will meet in person at UTA FrontLines Headquarters (FLHQ) - 669 W. 200 S., Salt Lake City, Utah.

**For remote viewing, public comment, and special accommodations instructions, please see the meeting information following this agenda.**

1. **Call to Order and Opening Remarks** Chair Carlton Christensen
2. **Pledge of Allegiance** Chair Carlton Christensen
3. **Safety First Minute** Jeff Acerson
4. **Public Comment** Chair Carlton Christensen
5. **Consent** Chair Carlton Christensen
  - a. Approval of the March 25, 2026, Board of Trustees Meeting Minutes
6. **Reports**
  - a. Executive Director Report Jay Fox
    - Olympic Transportation Working Group
  - b. Strategic Plan Minute: Organizational Excellence - Trapeze Update Jay Fox
  - c. Pension Committee Report Jeff Acerson
  - d. Financial Report - Preliminary February 2026 Viola Miller  
Brad Armstrong
7. **Resolutions**
  - a. R2026-04-01 - Resolution Adopting Revised Board Policy 1.2 Ethics Neiufi longi
  - b. R2026-04-02 - Authorizing Execution of an Interlocal Cooperation Agreement with Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency for the S-Line Extension David Osborn  
Ethan Ray

**8. Contracts, Disbursements and Grants**

- |    |  |                             |
|----|--|-----------------------------|
| a. | Revenue Contract: S-Line Funding Agreement (Salt Lake City Community Reinvestment Agency)  | David Osborn<br>Ethan Ray   |
| b. | Change Order: S-Line Extension Change Order 6 - Phase 1 Deductive Budget Adjustment (Kiewit West Infrastructure Co.)   | David Osborn<br>Ethan Ray   |
| c. | Change Order: S-Line Extension Change Order 7 - Phase 2 Construction Services (Kiewit West Infrastructure Co.)   | David Osborn<br>Ethan Ray   |
| d. | Revenue Contract: Ratification of Master Transit Agreement and Service Order 1 Between Utah Transit Authority and Utah County for Operation of Enhanced Transit Service in Utah County | Hal Johnson                 |
| e. | Contract: Lawn Care and Landscape Maintenance (Roth Landscape Services, LLC)   | Kevin Anderson<br>Guy Miner |
| f. | Contract: Lawn Care and Landscape Maintenance (Yard Masters Inc.)  | Kevin Anderson<br>Guy Miner |
| g. | Change Order: Program Management Services Change Order No. 04 - Contract Extension and Budget Adjustment (HNTB Corporation)  | Daniel Hofer                |
| h. | Pre-Procurements<br>- Fire Alarm Systems Remediation   | Todd Mills                  |

**9. Service and Fare Approvals**

- |    |   |             |
|----|---|-------------|
| a. | Fare Agreement: Special Events and Police Service Agreement for Salt Lake City Marathon (High Altitude Special Events "HASE") | Monica Howe |
|----|---|-------------|

**10. Discussion Items**

- |    |   |              |
|----|---|--------------|
| a. | Capital Program Report - First Quarter 2026 | Daniel Hofer |
|----|---|--------------|

**11. Other Business**

Chair Carlton Christensen

- |    |  |  |
|----|--|--|
| a. | Next Meeting: Wednesday, April 22, 2026 at 9:00 a.m. |  |
|----|--|--|

**12. Adjourn**

Chair Carlton Christensen

**Meeting Information:**

- Special Accommodation: Information related to this meeting is available in alternate formats upon request by contacting [adacompliance@rideuta.com](mailto:adacompliance@rideuta.com) or (801) 287-3536. Requests for accommodations should be made at least two business days in advance of the scheduled meeting.
- Meeting proceedings may be viewed remotely by following the meeting video link on the UTA Public Meeting Portal - <https://rideuta.legistar.com/Calendar.aspx>
- In the event of technical difficulties with the remote connection or live-stream, the meeting will proceed in person and in compliance with the Open and Public Meetings Act.
- Public Comment may be given live during the meeting by attending in person at the meeting location OR by joining the remote Zoom meeting.
  - o Comments are limited to 3 minutes per commenter.
  - o One person's time may not be combined with another person's time.
  - o Distribution of handouts or other materials to meeting participants or attendees is not allowed.
  - o To support a respectful meeting environment, actions or words that disrupt the meeting, intimidate other participants, obstruct the view or hearing of others, or may cause safety concerns are not allowed.
  - o To join by Zoom:
    - Use this link: [https://bit.ly/UTA\\_BOT\\_04-08-26](https://bit.ly/UTA_BOT_04-08-26) and follow the instructions to register for the meeting.
    - Use the "raise hand" function in Zoom to indicate you would like to make a comment.
- Public Comment may also be given through alternate means. See instructions below.
  - o Comment online at <https://www.rideuta.com/Board-of-Trustees>
  - o Comment via email at [boardoftrustees@rideuta.com](mailto:boardoftrustees@rideuta.com)
  - o Comment by telephone at 801-743-3882 option 5 (801-RideUTA option 5) – please specify that your comment is for the upcoming Board of Trustees meeting.
  - o Comments submitted before 2:00 p.m. on Tuesday, April 7 will be distributed to board members prior to the meeting and added to the public record.
- Meetings are audio and video recorded and live-streamed.
- Motions, including final actions, may be taken in relation to any topic listed on the agenda.



U T A

# Utah Transit Authority

669 West 200 South  
Salt Lake City, UT 84101

## MEETING MEMO

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**Board of Trustees**

**Date: 4/8/2026**

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**TO:** Board of Trustees  
**FROM:** Curtis Haring, Board Manager  
**PRESENTER(S):** Chair Carlton Christensen

**TITLE:**

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**Approval of the March 25, 2026, Board of Trustees Meeting Minutes**

**AGENDA ITEM TYPE:**

Minutes

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**RECOMMENDATION:**

Approve the minutes of the March 25, 2026, Board of Trustees meeting.

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**BACKGROUND:**

A meeting of the UTA Board of Trustees was held in person at UTA Frontlines Headquarters and broadcast live via the UTA Public Meeting Web Portal on Wednesday, March 25, 2026 at 9:00 a.m.

Minutes from the meeting document the actions of the Board and summarize the discussion that took place in the meeting. A full audio recording of the meeting is available on the [Utah Public Notice Website](https://www.utah.gov/pmn/sitemap/notice/1057979.html) <<https://www.utah.gov/pmn/sitemap/notice/1057979.html>> video feed is available through the [UTA Public Meeting Portal](https://rideuta.granicus.com/player/clip/441) <<https://rideuta.granicus.com/player/clip/441>>.

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**ATTACHMENTS:**

- 2026-03-25\_BOT\_Minutes\_Unapproved



# Utah Transit Authority

## Board of Trustees

### MEETING MINUTES - Draft

669 West 200 South  
Salt Lake City, UT 84101

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**Wednesday, March 25, 2026**

**9:00 AM**

**FrontLines Headquarters**

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**Present:** Chair Carlton Christensen  
Trustee Jeff Acerson  
Trustee Beth Holbrook

Also attending were UTA staff and interested community members.

**1. Call to Order and Opening Remarks**

Chair Carlton Christensen welcomed attendees and called the meeting to order at 9:00 a.m.

**2. Pledge of Allegiance**

Attendees recited the Pledge of Allegiance.

**3. Safety First Minute**

Katie Morrison, UTA Communications Director, delivered a brief safety message.

**4. Public Comment**

To view public comment in its entirety, see the meeting video located at [https://rideuta.granicus.com/player/clip/441?meta\\_id=74210](https://rideuta.granicus.com/player/clip/441?meta_id=74210)

**In Person/Virtual Comment**

In person comment was given by Mahanatma Kavr who spoke on union leaders, safety, respect and pay of operators.

**Online Comment**

No online comment was received.

**5. Consent**

**a. Approval of the March 11, 2026, Board of Trustees Meeting Minutes**

A motion was made by Trustee Holbrook, and seconded by Trustee Acerson, to approve the consent agenda. The motion carried by a unanimous vote.

**6. Reports****a. Executive Director Report  
- Quiet Zone Update**

Jay Fox, UTA Executive Director, gave a quiet zone update. Waivers have been requested while work continues on train crossings in North Salt Lake and South Salt Lake. The anticipated completion date is September 2026.

**b. Strategic Plan Minute: Quality of Life - Radio Systems Replacement Update**

Jay Fox was joined by Jarvie Curtis, Manager of Operational Technology and Systems, who said the UTA's radio system has exceeded its useful life and replacement is part of our state of good repair plan. At the end of 2024, UTA selected a replacement for the current radio system and will be moving to a radio over internet protocol (ROIP) system supported by a third-party provider.

In 2025, UTA was focused on Phase I of the radio replacement system and the procurement of all of the hardware and support services. Several key milestones have been achieved and are on-track for a successful Phase I design and implementation.

Discussion ensued. Questions on project delivery timelines, the balance between having the latest technology versus complete implantation, and if the system being put into place could/would be compatible with public safety radio systems were posed by the board and answered by staff.

**7. Resolutions****a. R2026-03-05 - Resolution Approving the Authority's 2026 Annual Service Plan**

Jaron Robertson, UTA Director of Planning, requested formal adoption of the 2026 Annual Service Plan. Mid Valley Express (MVX) service will begin during April Change Day. Ski services have been suspended early, especially in areas where ski resorts have closed (Snow Basin and Powder Mountain).

Discussion ensued. Questions about how future on-demand services would be incorporated into service plans, service classification, and where ski bus operators are moved to when service is ended early were asked by the board and answered by staff. Additionally, staff noted that the resolution did not incorporate changes due to the upcoming Hill Air Force Base air show; those changes will be discussed at a future time.

A motion was made by Trustee Acerson, and seconded by Trustee Holbrook, that this resolution be approved. The motion carried by the following vote:

Aye: Chair Christensen, Trustee Acerson, and Trustee Holbrook

**b. R2026-03-06 - Resolution Adopting the Fare Rates and Types of Fare Media of the Agency**

Monica Howe, UTA Fares Director, explained the revised fare rates and types of fare media which will be effective April 12, 2026. Questions were asked about how change information will be shared with the public before the April Change Day. Howe reassured the Trustees that her team is working with marketing on a campaign. Signs will also be placed on ticket machines, etc.

A motion was made by Trustee Holbrook, and seconded by Trustee Acerson, that this resolution be approved. The motion carried by the following vote:

Aye: Chair Christensen, Trustee Acerson, and Trustee Holbrook

**c. R2026-03-07 - Resolution Adopting the South Salt Lake Downtown Connect Station Area Plan**

Paul Drake, UTA Director of Real Estate and TOD, was joined by Valarie Williams, UTA TOC Project Specialist I, The proposed South Salt Lake Downtown Connect Station Area Plan will better position UTA and South Salt Lake to coordinate redevelopment of the station area. This aligned coordination will promote future transit-oriented development in an efficient and fiscally responsible manner.

A motion was made by Trustee Acerson, and seconded by Trustee Holbrook, that this resolution be approved. The motion carried by the following vote:

Aye: Chair Christensen, Trustee Acerson, and Trustee Holbrook

**8. Contracts, Disbursements and Grants**

**a. Contract: 2026 Support Fleet Order (Ken Garff Ford West Valley)**

Kyle Stockley, UTA Director of Capital Vehicles, said the Vehicle Procurement Team plans to acquire 22 vehicles, totaling \$1,158,655, to replace aging units in UTA's support fleet. This purchase includes two (2) Explorers, seven (7) F-150s, eight (8) transit vans, and five (5) F-350s.

A motion was made by Trustee Holbrook, and seconded by Trustee Acerson, that this contract be approved. The motion carried by a unanimous vote.

**b. Contract: Onsite Wellness Clinic Services Contract (CareATC, Inc.)**

Ann Green-Barton, UTA Chief People Officer, was joined by JD Tazoi, UTA Director of Total Rewards. UTA completed the process in fall 2025 to reprocur onsite health and wellness clinic services for three more years. As a result of this process, the evaluation committee selected CareATC as the vendor to continue providing these services beginning March 26, 2026.

Discussion followed about the return on investment having services on-site vs. other options and the contract's ability to expand to future locations were asked by the Board and answered by staff.

A motion was made by Trustee Acerson, and seconded by Trustee Holbrook, that this

contract be approved. The motion carried by a unanimous vote.

**c. Change Order: Next Generation Fare Collection System Change Order No. 17 - Mobile Fare Solution: Virtual Transit Card (Scheidt & Bachman USA, Inc.)**

Brian Reeves, UTA Associate CFO, was joined by Monica Howe, and Katie Morrison, Communications Director, to request approval of \$4,057,963 with Scheidt & Bachmann USA, Inc. for a Virtual Transit Card program.

Discussion ensued. The Board asked and staff answered questions related to how virtual cards would integrate with existing mobile apps, communication strategies once implemented, and previous expenses.

A motion was made by Trustee Holbrook, and seconded by Trustee Acerson, that this change order be approved. The motion carried by a unanimous vote.

**d. Pre-Procurements**

- Capital Vehicle Consulting Contract
- North Temple Escalator Design
- North Temple Escalator Construction

Todd Mills, UTA Director of Supply Chain was joined by Dave Osborn, UTA Project Manager III and Kyle Stockley, Director of Capital Vehicles, and explained three Request for Qualification/Quotes (RFQU).

Discussion ensued. Questions about the Utah Department of Transportation's assistance in vehicles procurement, and the potential scope of the North Temple escalator design and replacement were asked and answered by staff.

**9. Discussion Items**

**a. 2025 Annual Transit-Oriented Communities Report and Real Estate Inventory**

Paul Drake was joined by Spencer Burgoyne, UTA Manager Property Administration, to present the annual Transit-Oriented Communities Report and Real Estate Inventory to the Board as required.

Discussion ensued. Questions regarding the affordability of commercial properties were asked and answered by staff.

**b. April 2026 Change Day/MVX Grand Opening**

Ali Oliver, UTA Director of Ops Service Delivery, was joined by Camille Glenn, UTA Regional General Manager of Salt Lake and Mt. Ogden Service Units, and Katie Morrison. Staff gave an overview of the upcoming changes for the April 2026 Change Day, including the opening of Midvalley Express (MVX) Bus Rapid Transit line.

Discussion ensued. Questions regarding staffing, the amount of remaining free FAREPAY cards designated for qualified riders, and reporting were asked and answered

by staff.

**c. Annual Ridership Campaign**

Katie Morrison presented a media plan outlining paid and organic outreach efforts planned through June 30, 2026, designed to support and increase ridership. The campaign will also include three videos highlighting themes of purpose, independence, and memories, reinforcing the role UTA plays in riders' everyday lives.

**d. Board Policy Revisions - 1.2 Ethics**

Neiufi longi, UTA Public Policy Analyst, presented proposed revisions including updates in practice, required timeframes, and clarifying language. These revisions will incorporate statutory ethics requirements for Authority Officers and establish a three-person Ethics Committee.

Discussion ensued. A question regarding language that will be obsolete with the passage of SB 197 was asked and answered by staff.

**e. UTA Policy - UTA.06.04 Non-Revenue Vehicle Fleet**

Jon Larson, UTA Chief Capital Services Officer, explained how the non-revenue vehicle fleet policy applies to Utah Transit Authority's non-revenue vehicles (NRVs). This policy includes requirements for authorized users, replacement and expansion guidelines, funding, maintenance, and information related to disposal and reallocation.

**10. Other Business**

- a. Next Meeting: Wednesday, April 8, 2026 at 9:00 a.m.

**11. Closed Session**

- a. **Strategy Session to Discuss Topics as Defined in Utah Code 52-4-205 (1):**  
- **Collective Bargaining**

Chair Christensen indicated there were matters to be discussed in closed session related to collective bargaining. A motion was made by Trustee Acerson, and seconded by Trustee Holbrook, for a closed session. The motion carried by a unanimous vote, and closed session convened at 10:39 a.m.

**12. Open Session**

A motion was made by Trustee Holbrook and seconded by Trustee Acerson to return to open session. The motion carried by a unanimous vote, and the meeting reconvened in open session at 10:45 a.m.

**13. Adjourn**

A motion was made by Trustee Acerson, and seconded by Trustee Holbrook, to adjourn the meeting. The motion carried by a unanimous vote and the meeting adjourned at 10:48 a.m.

Transcribed by Cherilyn Bradford  
Executive Assistant to the Board  
Utah Transit Authority

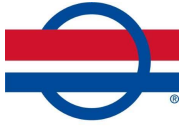
This document is not intended to serve as a full transcript as additional discussion may have taken place; please refer to the meeting materials or audio located at <https://www.utah.gov/pmn/sitemap/notice/1057979.html> for entire content. Meeting materials, along with a time-stamped video recording, are also accessible at <https://rideuta.granicus.com/player/clip/441>.

This document along with the digital recording constitute the official minutes of this meeting.

Approved Date:

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Carlton J. Christensen  
Chair, Board of Trustees



U T A

# Utah Transit Authority

669 West 200 South  
Salt Lake City, UT 84101

## MEETING MEMO

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**Board of Trustees**

**Date: 4/8/2026**

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**TO:** Board of Trustees  
**FROM:** Jay Fox, Executive Director  
**PRESENTER(S):** Jay Fox, Executive Director

**TITLE:**

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**Executive Director Report**  
- **Olympic Transportation Working Group**

**AGENDA ITEM TYPE:**

Report

**RECOMMENDATION:**

Informational report for discussion

**DISCUSSION:**

Jay Fox, Executive Director, will provide a report on the following:

- Olympic Transportation Working Group
-



# Utah Transit Authority

## MEETING MEMO

669 West 200 South  
Salt Lake City, UT 84101

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**Board of Trustees**

**Date: 4/8/2026**

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**TO:** Board of Trustees  
**FROM:** Jay Fox, Executive Director  
**PRESENTER(S):** Jay Fox, Executive Director

**TITLE:**  
**Strategic Plan Minute: Organizational Excellence - Trapeze Update**

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**AGENDA ITEM TYPE:**  
Report

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**RECOMMENDATION:**  
Informational report for discussion.

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**BACKGROUND:**  
At the end of 2022, UTA adopted its 2022-2030 Strategic Goals and Objectives. The strategic minute provides an update on one of the five UTA strategic priorities - Quality of Life, Customer Experience, Organizational Excellence, Community Support, and Economic Return.

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**DISCUSSION:**  
This strategic plan minute highlights our Organizational Excellence strategic priority. The Enterprise Strategy Office owns this strategic initiative of implementing Trapeze systems. This is a multi-year strategic initiative. The report will highlight key milestones achieved thus far in 2025 and outlines 2026 goals.

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**FISCAL IMPACT:**  
N/A

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**ATTACHMENTS:**

- None



U T A

# Utah Transit Authority

669 West 200 South  
Salt Lake City, UT 84101

## MEETING MEMO

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**Board of Trustees**

**Date: 4/8/2026**

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**TO:** Board of Trustees  
**FROM:** Jeff Acerson, UTA Trustee  
**PRESENTER(S):** Jeff Acerson, UTA Trustee

**TITLE:**  
**Pension Committee Report**

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**AGENDA ITEM TYPE:**  
Report

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**RECOMMENDATION:**  
Receive the report for the Pension Committee.

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**BACKGROUND:**  
The Pension Committee met on April 2, 2026 with Trustee Jeff Acerson acting as Chair of the committee.

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**DISCUSSION:**  
Trustee Acerson will provide an update on Pension Committee Activities

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**FISCAL IMPACT:**  
N/A

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**ATTACHMENTS:**

- N/A



# Utah Transit Authority

## MEETING MEMO

669 West 200 South  
Salt Lake City, UT 84101

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**Board of Trustees**

**Date: 4/8/2026**

---

**TO:** Board of Trustees  
**THROUGH:** Jay Fox, Executive Director  
**FROM:** Viola Miller, Chief Financial Officer  
**PRESENTER(S):** Viola Miller, Chief Financial Officer  
Brad Armstrong, Director Budget & Financial Strategy

**TITLE:**

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**Financial Report - Preliminary February 2026**

**AGENDA ITEM TYPE:**

Report

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**RECOMMENDATION:**

Informational report for discussion

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**BACKGROUND:**

The Board of Trustees Policy No. 2.1, Financial Management, directs the Chief Financial Officer to present monthly financial statements stating the Authority's financial position, revenues, and expenses to the Board of Trustees as soon as practical with monthly and year-to-date budget versus actual reports to be included in the monthly financial report. The preliminary February 2026 Monthly Financial Statements have been prepared in accordance with the Financial Management Policy and will be presented to the Board. Also provided is the monthly Board Dashboard which summarizes key information from the preliminary February 2026 Monthly Financial Statements.

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**DISCUSSION:**

At the April 8, 2026, meeting, the Chief Financial Officer will review the Board Dashboard key items, passenger revenues, sales tax collections, operating expense variances, and capital budget status. The Chief Financial Officer will also present key metrics in Accounting, Supply Chain, and Fares and receive questions from the Board of Trustees.

**ALTERNATIVES:**

N/A

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**FISCAL IMPACT:**

N/A

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**ATTACHMENTS:**

- Preliminary February 2026 Board Dashboard
- Preliminary February 2026 Financial Statements

# Utah Transit Authority

## Preliminary Board Dashboard: Feb 28, 2026

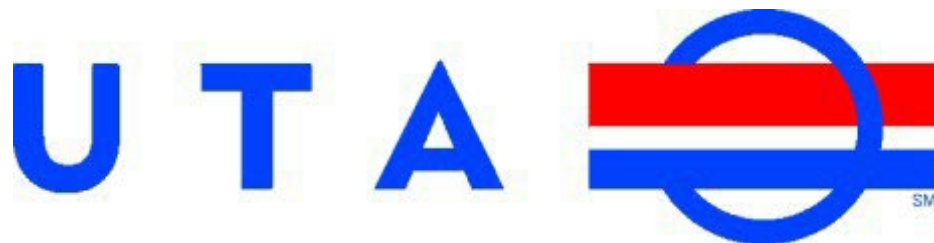
<b>Financial Metrics</b>	Feb Actual	Feb Budget	Fav / (Unfav)	%	YTD Actual	YTD Budget	Fav / (Unfav)	%
Sales Tax (Jan '26 mm \$)	\$ 36.0	\$ 37.2	\$ (1.2)	● -3.2%	\$ 36.0	\$ 37.2	\$ (1.2)	● -3.2%
Fare Revenue (mm)	\$ 3.1	\$ 3.2	\$ (0.2)	● -4.8%	\$ 6.4	\$ 6.4	\$ (0.0)	● -0.4%
Operating Exp (mm)	\$ 35.1	\$ 39.6	\$ 4.5	● 11.3%	\$ 73.7	\$ 79.6	\$ 5.9	● 7.4%
Subsidy Per Rider (SPR)	\$ 9.90	\$ 10.65	\$ 0.75	● 7.0%	\$ 10.20	\$ 10.65	\$ 0.45	● 4.2%
UTA Diesel Price (\$/gal)	\$ 2.53	\$ 3.45	\$ 0.92	● 26.8%	\$ 2.36	\$ 3.45	\$ 1.09	● 31.6%
<b>Operating Metrics</b>	Feb Actual	Feb-25	F / (UF)	%	YTD Actual	YTD 2025	F / (UF)	%
Ridership (mm)	3.24	3.27	(0.03)	● -0.9%	6.60	6.70	(0.10)	● -1.6%
<b>Energy Cost by Type (Monthly Avg YTD)</b>								
	<b>Diesel Bus</b> (Cost per Mile)				\$ 0.42			
	<b>Diesel CR</b> (Cost per Mile)				\$ 3.61			
	<b>Unleaded Gas</b> (Cost per Mile)				\$ 0.28			
	<b>CNG</b> (Cost per Mile)				\$ 0.28			
	<b>Bus Propulsion Power</b> (Cost per Mile)				\$ 0.54	Data Pending (2025 = \$0.60), using Jan cost only		
	<b>TRAX Propulsion Power</b> (Cost per Mile)				\$ 0.88	Data Pending (2025 = \$0.93), Jan = \$0, Feb = \$0.88		

"Sales Tax" lists the amount of sales tax revenue received for the month listed in bold. All other data reflects the month listed in the table title.

Energy Costs are lacking invoices so amounts appear low through Feb 2026.

**Utah Transit Authority**  
**Financial Statement**  
(Unaudited)

February 28, 2026



**KEY ITEM REPORT  
(UNAUDITED)  
As of February 28, 2026**

EXHIBIT 1-1

	2026 YTD ACTUAL	2026 YTD BUDGET	VARIANCE FAVORABLE (UNFAVORABLE)	% FAVORABLE (UNFAVORABLE)
1 Operating Revenue	\$ (6,752,680)	\$ (6,777,946)	\$ (25,266)	0%
2 Operating Expenses	73,698,659	79,593,147	5,894,488	7%
3 <b>Net Operating Income (Loss)</b>	<b>(66,945,979)</b>	<b>(72,815,201)</b>	<b>5,869,222</b>	<b>8%</b>
4 Capital Revenue	(28,968,925)	(42,475,333)	(13,506,408)	-32%
5 Capital Expenses	48,834,552	56,942,200	8,107,648	14%
6 <b>Net Capital Income (Loss)</b>	<b>(19,865,627)</b>	<b>(14,466,867)</b>	<b>(5,398,761)</b>	<b>-37%</b>
7 Sales Tax	(73,468,894)	(73,468,894)	-	0%
8 Other Revenue	(84,784,772)	(75,376,000)	9,408,772	12%
9 Debt Service	14,898,011	14,962,109	64,098	0%
10 Sale of Assets	80,070	-	(80,070)	
11 <b>Net Non-Operating Income (Loss)</b>	<b>143,275,584</b>	<b>133,882,785</b>	<b>9,392,799</b>	<b>7%</b>
12 <b>Contribution to Cash Balance</b>	<b>\$ 56,463,978</b>	<b>\$ 46,600,717</b>	<b>\$ 9,863,261</b>	<b>-21%</b>
13 Amortization	(461,490)			
14 Depreciation	24,966,064			
15 <b>Total Non-cash Items</b>	<b>\$ 24,504,575</b>			

**STATISTICS**

**RIDERSHIP**

2025 YE Actual	Feb 2026	Feb 2025	Difference	2026 YTD	2025 YTD	Difference
16 40,336,358	3,237,422	3,267,042	(29,620)	6,597,445	6,702,282	(104,837)

**OPERATING SUBSIDY PER RIDER -**

	SPR
17 Net Operating Expense	\$ 73,698,659
18 Less: Passenger Revenue	- (6,390,180)
19 Subtotal	67,308,479
20 Divided by: Ridership	÷ 6,597,445
21 Subsidy per Rider	<u>\$ 10.20</u>

**SUMMARY FINANCIAL DATA  
(UNAUDITED)**

EXHIBIT 1-2

As of February 28, 2026

**BALANCE SHEET**

	<b>2/28/2026</b>	<b>2/28/2025</b>	<b>Change February</b>
<b>CURRENT ASSETS</b>			
1 Cash	\$ 22,968,182	\$ 23,290,842	-1%
2 Investments (Unrestricted)	414,384,945	342,215,690	21%
3 Investments (Restricted)	202,936,446	144,081,661	41%
4 Receivables	95,043,210	90,539,450	5%
5 Receivables - Federal Grants	318,932	246,015	30%
6 Inventories	57,081,326	48,049,036	19%
7 Prepaid Expenses	7,937,104	2,079,867	282%
<b>8 TOTAL CURRENT ASSETS</b>	<b>\$ 800,670,146</b>	<b>\$ 650,502,561</b>	
9 Property, Plant & Equipment (Net)	2,982,732,032	2,904,709,363	3%
10 Other Assets	85,247,926	160,097,691	-47%
<b>11 TOTAL ASSETS</b>	<b>\$ 3,868,650,104</b>	<b>\$ 3,715,309,615</b>	
12 Current Liabilities	\$ 137,576,131	\$ 123,530,321	11%
14 Net Pension Liability	133,377,587	142,283,669	-6%
15 Outstanding Debt	2,261,706,401	2,271,508,557	0%
16 Net Investment in Capital Assets	830,075,425	759,903,706	9%
17 Restricted Net Position	120,026,413	129,524,065	-7%
18 Unrestricted Net Position	385,888,147	288,559,297	34%
<b>19 TOTAL LIABILITIES &amp; EQUITY</b>	<b>\$ 3,868,650,104</b>	<b>\$ 3,715,309,615</b>	

**RESTRICTED AND DESIGNATED CASH AND CASH EQUIVALENTS RECONCILIATION**

<b>RESTRICTED RESERVES</b>			
20 2025 Bond Proceeds	96,059,110		-
21 Debt Service Interest Payable	41,563,049	59,542,090	-30%
22 Risk Contingency Fund	8,254,790	8,197,659	1%
23 Catastrophic Risk Reserve Fund	1,209,041	1,169,950	3%
24 Box Elder County ROW (sales tax)	4,533,297	5,019,209	-
25 Utah County 4th Qtr (sales tax)	34,807,294	26,100,384	33%
26 Amounts held in escrow	16,509,866	44,047,607	-63%
<b>27 TOTAL RESTRICTED RESERVES</b>	<b>\$ 202,936,446</b>	<b>\$ 144,076,899</b>	
<b>DESIGNATED GENERAL AND CAPITAL RESERVES</b>			
28 General Reserves	\$ 83,500,000	72,100,000	
29 Service Sustainability Reserves	13,900,000	12,017,000	
30 Capital Reserve	77,200,000	45,616,000	
31 Debt Reduction Reserve	30,000,000	30,000,000	
<b>32 TOTAL DESIGNATED GENERAL AND CAPITAL RESERVES</b>	<b>\$ 204,600,000</b>	<b>\$ 159,733,000</b>	
<b>33 TOTAL RESTRICTED AND DESIGNATED CASH AND EQUIVALENTS</b>	<b>\$ 407,536,446</b>	<b>\$ 303,809,899</b>	

**SUMMARY FINANCIAL DATA**

EXHIBIT 1-3

(UNAUDITED)

As of February 28, 2026

**REVENUE & EXPENSES**

	ACTUAL Feb-26	ACTUAL Feb-25	YTD 2026	YTD 2025
<b>OPERATING REVENUE</b>				
1 Passenger Revenue	\$ (3,073,624)	\$ (3,512,318)	\$ (6,390,180)	\$ (6,637,487)
2 Advertising Revenue	(181,250)	(179,167)	(362,500)	(358,333)
3 <b>TOTAL OPERATING REVENUE</b>	<b><u>\$ (3,254,874)</u></b>	<b><u>\$ (3,691,484)</u></b>	<b><u>\$ (6,752,680)</u></b>	<b><u>\$ (6,995,820)</u></b>
<b>OPERATING EXPENSE</b>				
4 Bus Service	\$ 12,504,164	\$ 12,236,870	\$ 25,903,744	\$ 26,241,802
5 Commuter Rail	2,417,818	1,634,819	4,774,977	4,243,031
6 Light Rail	5,066,986	3,999,269	10,294,042	8,632,367
7 Maintenance of Way	655,968	2,016,410	1,965,230	4,488,339
8 Paratransit Service	1,905,330	2,210,725	4,672,440	4,634,232
9 RideShare/Van Pool Services	237,378	290,840	377,295	700,624
10 Operations Support	5,327,237	5,157,888	11,813,332	5,588,486
11 Administration	5,511,070	4,589,727	10,935,220	10,120,024
12 Microtransit	1,498,621	1,367,540	2,962,380	6,251,856
13 Non-Departmental	-	-	-	60
14 <b>TOTAL OPERATING EXPENSE</b>	<b><u>\$ 35,124,573</u></b>	<b><u>\$ 33,504,088</u></b>	<b><u>\$ 73,698,659</u></b>	<b><u>\$ 70,900,822</u></b>
15 <b>NET OPERATING (INCOME) LOSS</b>	<b><u>\$ 31,869,699</u></b>	<b><u>\$ 29,812,604</u></b>	<b><u>\$ 66,945,979</u></b>	<b><u>\$ 63,905,001</u></b>
<b>NON-OPERATING EXPENSE (REVENUE)</b>				
16 Investment Revenue	(1,937,498)	(1,345,551)	(3,536,447)	(3,414,765)
17 Sales Tax Revenue <sup>1</sup>	(36,282,478)	(35,825,323)	(73,468,894)	(72,354,789)
18 Other Revenue <sup>2</sup>	(630,090)	(614,784)	794,172	(1,441,802)
19 Fed Operations/Preventative Maint. Revenue	(50,599,335)	(519,187)	(82,042,496)	(6,997,110)
20 Bond Interest	7,107,450	4,611,081	14,214,900	9,222,162
21 Bond Interest UTCT	139,793	148,357	279,585	296,714
22 Bond Cost of Issuance/Fees	-	(23,500)	-	(23,500)
23 Lease Interest	201,590	10,594	403,526	256,267
24 Sale of Assets	74,197	(232,552)	80,070	(237,802)
25 <b>TOTAL NON-OPERATING EXPENSE (REVENUE)</b>	<b><u>\$ (81,926,372)</u></b>	<b><u>\$ (33,790,865)</u></b>	<b><u>\$ (143,275,584)</u></b>	<b><u>\$ (74,694,626)</u></b>
26 <b>CONTRIBUTION TO RESERVES</b>	<b><u>\$ 50,056,673</u></b>	<b><u>\$ 3,978,262</u></b>	<b><u>\$ 76,329,605</u></b>	<b><u>\$ 10,789,625</u></b>
<b>OTHER EXPENSES (NON-CASH)</b>				
27 Bond Premium/Discount Amortization	(914,284)	(344,047)	(1,828,569)	(688,093)
28 Bond Refunding Cost Amortization	615,963	1,102,053	1,231,927	2,204,106
29 Future Revenue Cost Amortization	67,576	67,576	135,152	135,152
30 Depreciation	12,296,841	10,131,037	24,966,064	22,098,388
31 <b>NET OTHER EXPENSES (NON-CASH)</b>	<b><u>\$ 12,066,096</u></b>	<b><u>\$ 10,956,620</u></b>	<b><u>\$ 24,504,575</u></b>	<b><u>\$ 23,749,553</u></b>

<sup>1</sup> Current Year Sales Taxes YTD Include Actuals Plus Two Prior Month Accruals

<sup>2</sup> Includes investment unrealized gain/loss.

**BUDGET TO ACTUAL REPORT  
(UNAUDITED)**

EXHIBIT 1-4

As of February 28, 2026

**CURRENT MONTH**

	<b>ACTUAL</b>	<b>BUDGET</b>	<b>VARIANCE</b>	<b>%</b>
	<b>Feb-26</b>	<b>Feb-26</b>	<b>FAVORABLE (UNFAVORABLE)</b>	<b>FAVORABLE (UNFAVORABLE)</b>
<b>OPERATING REVENUE</b>				
1 Passenger Revenue	\$ (3,073,624)	\$ (3,227,762)	\$ (154,138)	-5%
2 Advertising Revenue	(181,250)	(181,000)	250	0%
<b>3 TOTAL OPERATING REVENUE</b>	<b><u>\$ (3,254,874)</u></b>	<b><u>\$ (3,408,762)</u></b>	<b><u>\$ (153,888)</u></b>	<b>-5%</b>
<b>OPERATING EXPENSE</b>				
4 Bus Service	\$ 12,504,164	13,300,760	\$ 796,595	6%
5 Commuter Rail	2,417,818	2,688,509	270,691	10%
6 Light Rail	5,066,986	4,459,311	(607,676)	-14%
7 Maintenance of Way	655,968	1,466,866	810,898	55%
8 Paratransit Service	1,905,330	2,772,275	866,945	31%
9 RideShare/Van Pool Services	237,378	380,206	142,828	38%
10 Microtransit	1,498,621	1,497,263	(1,358)	0%
11 Operations Support	5,327,237	6,240,678	913,441	15%
12 Administration	5,511,070	6,813,559	1,302,489	19%
13 Non-Departmental	-	-	-	
<b>14 TOTAL OPERATING EXPENSE</b>	<b><u>\$ 35,124,573</u></b>	<b><u>\$ 39,619,426</u></b>	<b><u>\$ 4,494,853</u></b>	<b>11%</b>
<b>15 NET OPERATING (INCOME) LOSS</b>	<b><u>\$ 31,869,699</u></b>	<b><u>\$ 36,210,664</u></b>	<b><u>\$ 4,340,965</u></b>	<b>12%</b>
<b>NON-OPERATING EXPENSE (REVENUE)</b>				
16 Investment Revenue	\$ (1,937,498)	\$ (1,199,000)	\$ 738,498	62%
17 Sales Tax Revenue	(36,282,478)	(36,282,478)	-	0%
18 Other Revenue	(630,090)	(1,239,000)	(608,910)	-49%
19 Fed Operations/Preventative Maint. Revenue	(50,599,335)	(43,500,000)	7,099,335	16%
20 Bond Interest	7,107,450	7,107,450	-	0%
21 Bond Interest UTCT	139,793	139,793	0	0%
22 Bond Cost of Issuance/Fees	-	-	-	0%
23 Lease Interest	201,590	233,812	32,222	14%
24 Sale of Assets	74,197	-	(74,197)	100%
<b>25 TOTAL NON-OPERATING EXPENSE (REVENUE)</b>	<b><u>\$ (81,926,372)</u></b>	<b><u>\$ (74,739,423)</u></b>	<b><u>\$ 7,186,949</u></b>	<b>10%</b>
<b>26 CONTRIBUTION TO RESERVES</b>	<b><u>\$ 50,056,673</u></b>	<b><u>\$ 38,528,759</u></b>		

**BUDGET TO ACTUAL REPORT BY CHIEF  
(UNAUDITED)  
As of February 28, 2026**

EXHIBIT 1-4A

**CURRENT MONTH**

	<b>ACTUAL</b>	<b>BUDGET</b>	<b>ORIGINAL</b>	<b>VARIANCE</b>	<b>%</b>
	<b>Feb-26</b>	<b>Feb-26</b>	<b>BUDGET</b>	<b>FAVORABLE</b>	<b>FAVORABLE</b>
			<b>TOTAL</b>	<b>(UNFAVORABLE)</b>	<b>(UNFAVORABLE)</b>
<b>OPERATING EXPENSE</b>					
1 Board of Trustees	\$ 292,177	\$ 316,137	\$ 3,793,665	\$ 23,960	8%
2 Chief Communication Officer	233,549	437,464	5,249,676	203,915	47%
3 Chief Development Officer	469,510	698,628	8,383,487	229,118	33%
4 Chief Enterprise Strategy Officer	2,549,566	2,988,875	34,596,116	439,309	15%
5 Chief Finance Officer	1,200,781	1,656,201	19,976,092	455,420	27%
6 Chief Operating Officer	26,597,858	29,166,142	356,112,046	2,568,284	9%
7 Chief Planning and Engagement Of	2,468,831	2,544,356	29,690,639	75,525	3%
8 Chief People Officer	923,823	1,166,119	13,993,559	242,296	21%
9 Executive Director	388,477	645,504	7,789,631	257,027	40%
10 Non-Departmental	-	-	8,996,776	0	
<b>11 TOTAL OPERATING EXPENSE</b>	<b>\$ 35,124,573</b>	<b>\$ 39,619,426</b>	<b>\$ 488,581,686</b>	<b>\$ 4,494,853</b>	<b>11%</b>

**YEAR TO DATE**

	<b>ACTUAL</b>	<b>BUDGET</b>	<b>VARIANCE</b>	<b>%</b>
	<b>Feb-26</b>	<b>Feb-26</b>	<b>FAVORABLE</b>	<b>FAVORABLE</b>
			<b>(UNFAVORABLE)</b>	<b>(UNFAVORABLE)</b>
<b>OPERATING EXPENSE</b>				
12 Board of Trustees	\$ 550,409	\$ 632,274	\$ 81,865	13%
13 Chief Communication Officer	422,408	874,928	452,520	52%
14 Chief Development Officer	1,014,546	1,397,256	382,710	27%
15 Chief Enterprise Strategy Officer	5,170,107	5,482,425	312,318	6%
16 Chief Finance Officer	2,626,070	3,312,402	686,332	21%
17 Chief Operating Officer	55,406,495	59,272,376	3,865,880	7%
18 Chief Planning and Engagement Of	4,654,850	4,998,240	343,390	7%
19 Chief People Officer	1,927,530	2,332,238	404,708	17%
20 Executive Director	877,721	1,291,008	413,287	32%
21 Non-Departmental	-	-	0	
<b>22 TOTAL OPERATING EXPENSE</b>	<b>\$ 72,650,137</b>	<b>\$ 79,593,147</b>	<b>\$ 6,943,010</b>	<b>9%</b>

**BUDGET TO ACTUAL REPORT  
(UNAUDITED)**

EXHIBIT 1-5

As of February 28, 2026

**YEAR TO DATE**

	<b>ACTUAL</b>	<b>BUDGET</b>	<b>VARIANCE</b>	<b>%</b>
	<b>Feb-26</b>	<b>Feb-26</b>	<b>FAVORABLE (UNFAVORABLE)</b>	<b>FAVORABLE (UNFAVORABLE)</b>
<b>OPERATING REVENUE</b>				
1 Passenger Revenue	\$ (6,390,180)	\$ (6,415,946)	\$ (25,766)	0%
2 Advertising Revenue	(362,500)	(362,000)	500	0%
<b>3 TOTAL OPERATING REVENUE</b>	<b><u>\$ (6,752,680)</u></b>	<b><u>\$ (6,777,946)</u></b>	<b><u>\$ (25,266)</u></b>	<b>0%</b>
<b>OPERATING EXPENSE</b>				
4 Bus Service	\$ 25,903,744	\$ 26,961,329	\$ 1,057,585	4%
5 Commuter Rail	4,774,977	5,409,110	634,133	12%
6 Light Rail	10,294,042	9,006,455	(1,287,587)	-14%
7 Maintenance of Way	1,965,230	2,969,816	1,004,586	34%
8 Paratransit Service	4,672,440	5,558,694	886,254	16%
9 RideShare/Van Pool Services	377,295	760,412	383,117	50%
10 Microtransit	2,962,380	2,994,526	32,146	1%
11 Operations Support	11,813,332	12,496,159	682,827	5%
12 Administration	10,935,220	13,436,646	2,501,426	19%
13 Non-Departmental	-	-	-	
<b>14 TOTAL OPERATING EXPENSE</b>	<b><u>\$ 73,698,659</u></b>	<b><u>\$ 79,593,147</u></b>	<b><u>\$ 5,894,488</u></b>	<b>7%</b>
<b>15 NET OPERATING (INCOME) LOSS</b>	<b><u>\$ 66,945,979</u></b>	<b><u>\$ 72,815,201</u></b>	<b><u>\$ 5,869,222</u></b>	<b>8%</b>
<b>NON-OPERATING EXPENSE (REVENUE)</b>				
16 Investment Revenue	\$ (3,536,447)	\$ (2,398,000)	\$ 1,138,447	47%
17 Sales Tax Revenue	(73,468,894)	(73,468,894)	-	0%
18 Other Revenue	794,172	(2,478,000)	(3,272,172)	-132%
19 Fed Operations/Preventative Maint. Revenue	(82,042,496)	(70,500,000)	11,542,496	16%
20 Bond Interest	14,214,900	14,214,900	-	0%
21 Bond Interest UTCT	279,585	279,585	0	0%
22 Bond Cost of Issuance/Fees	-	-	-	
23 Lease Interest	403,526	467,624	64,098	14%
24 Sale of Assets	80,070	-	(80,070)	
<b>25 TOTAL NON-OPERATING EXPENSE (REVENUE)</b>	<b><u>\$ (143,275,584)</u></b>	<b><u>\$ (133,882,785)</u></b>	<b><u>\$ 9,392,799</u></b>	<b>7%</b>
<b>26 CONTRIBUTION TO RESERVES</b>	<b><u>\$ 76,329,605</u></b>	<b><u>\$ 61,067,584</u></b>		

**CAPITAL PROJECTS  
(UNAUDITED)  
As of February 28, 2026**

EXHIBIT 1-6

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	<b>2026</b>	<b>ANNUAL</b>	
	<b>ACTUAL</b>	<b>BUDGET</b>	<b>PERCENT</b>
<b>EXPENSES</b>			
1 Capital Services	\$ 47,822,928	\$ 276,763,000	17.3%
2 Enterprise Strategy	686,765	17,051,000	4.0%
3 Executive Director (Safety)	46,733	1,485,000	3.1%
4 Finance	200,036	32,646,000	0.6%
5 Operations	55,871	8,974,000	0.6%
6 People	-	50,000	0.0%
7 Planning & Engagement	22,219	2,244,000	1.0%
9 <b>TOTAL</b>	<b><u>\$ 48,834,552</u></b>	<b><u>\$ 339,213,000</u></b>	<b>14.4%</b>
<b>REVENUES</b>			
10 GRANT	\$ 28,404,016	\$ 111,703,000	25.4%
11 STATE CONTRIBUTION	545,269	45,672,000	1.2%
12 LEASES (PAID TO DATE)	-	36,714,000	0.0%
13 BONDS	-	47,285,000	0.0%
14 LOCAL PARTNERS	19,640	13,478,000	0.1%
15 UTA FUNDING	19,865,627	84,361,000	23.5%
16 <b>TOTAL</b>	<b><u>\$ 48,834,552</u></b>	<b><u>\$ 339,213,000</u></b>	<b>14.4%</b>

**FAREBOX RECOVERY & SPR  
(UNAUDITED)**

EXHIBIT 1-7

As of February 28, 2026

**BY SERVICE**

	CURRENT MONTH		YEAR TO DATE	
	Feb-26	Feb-25	2026	2025
<b>UTA</b>				
Fully Allocated Costs	35,124,573	33,504,088	73,698,659	69,675,160
Passenger Farebox Revenue	3,073,624	3,512,318	6,390,180	6,573,863
Passengers	3,237,422	3,267,042	6,597,445	6,702,282
Farebox Recovery Ratio	8.8%	10.5%	8.7%	9.4%
Actual Subsidy per Rider	\$9.90	\$9.18	\$10.20	\$9.41
<b>BUS SERVICE</b>				
Fully Allocated Costs	17,869,101	16,981,523	37,386,848	35,957,718
Passenger Farebox Revenue	1,499,017	1,754,304	3,042,749	3,078,565
Passengers	1,696,396	1,662,611	3,431,751	3,400,857
Farebox Recovery Ratio	8.4%	10.3%	8.1%	8.6%
Actual Subsidy per Rider	\$9.65	\$9.16	\$10.01	\$9.67
<b>LIGHT RAIL SERVICE</b>				
Fully Allocated Costs	8,541,829	8,292,039	17,770,211	16,950,386
Passenger Farebox Revenue	569,889	775,854	1,164,512	1,382,681
Passengers	987,691	1,029,909	2,032,810	2,141,714
Farebox Recovery Ratio	6.7%	9.4%	6.6%	8.2%
Actual Subsidy per Rider	\$8.07	\$7.30	\$8.17	\$7.27
<b>COMMUTER RAIL SERVICE</b>				
Fully Allocated Costs	4,071,410	3,459,046	8,385,698	7,831,301
Passenger Farebox Revenue	447,687	486,926	915,133	780,473
Passengers	310,065	326,222	651,297	664,598
Farebox Recovery Ratio	11.0%	14.1%	10.9%	10.0%
Actual Subsidy per Rider	\$11.69	\$9.11	\$11.47	\$10.61
<b>MICROTRANSIT</b>				
Fully Allocated Costs	1,637,119	1,495,051	3,245,526	1,999,503
Passenger Farebox Revenue	50,323	62,395	101,908	101,048
Passengers	50,279	49,284	104,170	102,830
Farebox Recovery Ratio	3.1%	4.2%	3.1%	5.1%
Actual Subsidy per Rider	\$31.56	\$29.07	\$30.18	\$18.46
<b>PARATRANSIT</b>				
Fully Allocated Costs	2,420,392	2,651,620	5,792,546	5,544,031
Passenger Farebox Revenue	104,106	52,388	385,067	544,848
Passengers	73,815	73,517	149,748	154,916
Farebox Recovery Ratio	4.3%	2.0%	6.6%	9.8%
Actual Subsidy per Rider	\$31.38	\$35.36	\$36.11	\$32.27
<b>RIDESHARE</b>				
Fully Allocated Costs	584,723	624,809	1,117,831	1,392,220
Passenger Farebox Revenue	402,603	380,450	780,811	686,248
Passengers	119,176	125,498	227,669	237,367
Farebox Recovery Ratio	68.9%	60.9%	69.9%	49.3%
Actual Subsidy per Rider	\$1.53	\$1.95	\$1.48	\$2.97

BY TYPE

	CURRENT MONTH		YEAR TO DATE	
	Feb-26	Feb-25	2026	2025
<b>FULLY ALLOCATED COSTS</b>				
Bus Service	\$17,869,101	\$16,981,523	\$37,386,848	\$35,957,718
Light Rail Service	\$8,541,829	\$8,292,039	\$17,770,211	\$16,950,386
Commuter Rail Service	\$4,071,410	\$3,459,046	\$8,385,698	\$7,831,301
Microtransit	\$1,637,119	\$1,495,051	\$3,245,526	\$1,999,503
Paratransit	\$2,420,392	\$2,651,620	\$5,792,546	\$5,544,031
Rideshare	\$584,723	\$624,809	\$1,117,831	\$1,392,220
<b>UTA</b>	<b>\$35,124,574</b>	<b>\$33,504,088</b>	<b>\$73,698,660</b>	<b>\$69,675,159</b>
<b>PASSENGER FAREBOX REVENUE</b>				
Bus Service	\$1,499,017	\$1,754,304	\$3,042,749	\$3,078,565
Light Rail Service	\$569,889	\$775,854	\$1,164,512	\$1,382,681
Commuter Rail Service	\$447,687	\$486,926	\$915,133	\$780,473
Microtransit	\$50,323	\$62,395	\$101,908	\$101,048
Paratransit	\$104,106	\$52,388	\$385,067	\$544,848
Rideshare	\$402,603	\$380,450	\$780,811	\$686,248
<b>UTA</b>	<b>\$3,073,625</b>	<b>\$3,512,317</b>	<b>\$6,390,180</b>	<b>\$6,573,863</b>
<b>PASSENGERS</b>				
Bus Service	1,696,396	1,662,611	3,431,751	3,400,857
Light Rail Service	987,691	1,029,909	2,032,810	2,141,714
Commuter Rail Service	310,065	326,222	651,297	664,598
Microtransit	50,279	49,284	104,170	102,830
Paratransit	73,815	73,517	149,748	154,916
Rideshare	119,176	125,498	227,669	237,367
<b>UTA</b>	<b>3,237,422</b>	<b>3,267,041</b>	<b>6,597,445</b>	<b>6,702,282</b>
<b>FAREBOX RECOVERY RATIO</b>				
Bus Service	8.4%	10.3%	8.1%	8.6%
Light Rail Service	6.7%	9.4%	6.6%	8.2%
Commuter Rail Service	11.0%	14.1%	10.9%	10.0%
Microtransit	3.1%	4.2%	3.1%	5.1%
Paratransit	4.3%	2.0%	6.6%	9.8%
Rideshare	68.9%	60.9%	69.9%	49.3%
<b>UTA</b>	<b>8.8%</b>	<b>10.5%</b>	<b>8.7%</b>	<b>9.4%</b>
<b>ACTUAL SUBSIDY PER RIDER</b>				
Bus Service	\$9.65	\$9.16	\$10.01	\$9.67
Light Rail Service	\$8.07	\$7.30	\$8.17	\$7.27
Commuter Rail Service	\$11.69	\$9.11	\$11.47	\$10.61
Microtransit	\$31.56	\$29.07	\$30.18	\$18.46
Paratransit	\$31.38	\$35.36	\$36.11	\$32.27
Rideshare	\$1.53	\$1.95	\$1.48	\$2.97
<b>UTA</b>	<b>\$9.90</b>	<b>\$9.18</b>	<b>\$10.20</b>	<b>\$9.41</b>

**SUMMARY OF ACCOUNTS RECEIVABLE  
(UNAUDITED)**

EXHIBIT 1-9

As of February 28, 2026

<u>Classification</u>	<u>Total</u>	<u>Current</u>	<u>31-60 Days</u>	<u>61-90 Days</u>	<u>90-120 Days</u>	<u>Over 120 Days</u>
1 Federal Grants Government <sup>1</sup>	\$ 318,932	\$ 318,932	-	-	-	-
2 Sales Tax Contributions	77,596,421	36,282,478	\$ 41,313,943	-	-	-
3 Warranty Recovery	1,163,325	1,163,325	-	-	-	-
4 Build America Bond Subsidies	-	-	-	-	-	-
5 Product Sales and Development	1,774,636	1,622,032	(5,194)	1,216	572	156,011
6 Pass Sales	48	107,250	43	1,870	683	(109,797)
7 Property Management	197,456	79,763	21,347	13,265	2,552	80,530
8 Vanpool/Rideshare	180,862	116,228	12,943	-	-	51,691
9 Salt Lake City Agreement	508,471	508,471	-	-	-	-
10 Planning	-	-	-	-	-	-
11 Capital Development Agreements	810,442	806,033	-	-	(1)	4,410
12 Other	12,811,548	326,105	(87,596)	19,827	-	407,625
13 <b>Total</b>	<b>\$ 95,362,142</b>	<b>\$ 41,330,617</b>	<b>\$ 41,255,486</b>	<b>\$ 36,178</b>	<b>\$ 3,805</b>	<b>\$ 590,469</b>

**Percentage Due by Aging**

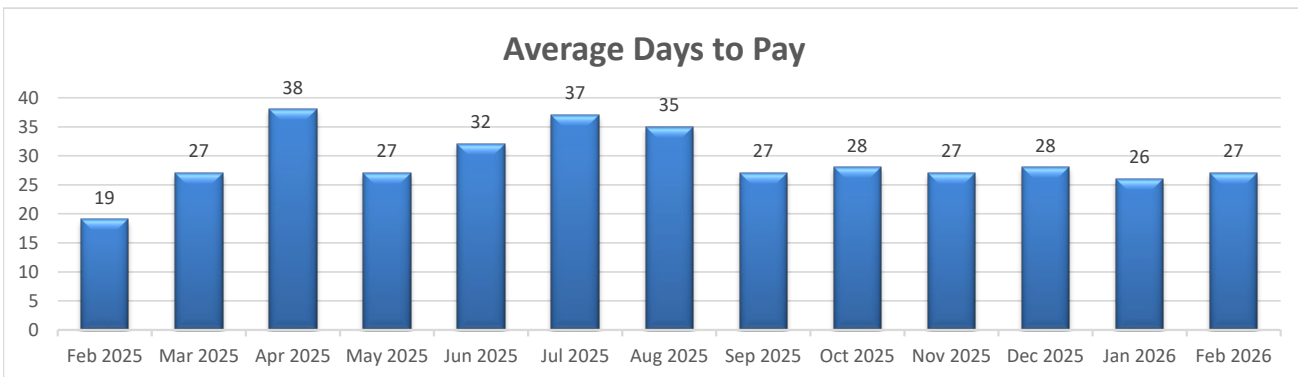
14 Federal Grants Government <sup>1</sup>	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15 Sales Tax Contributions	46.8%	53.2%	0.0%	0.0%	0.0%	0.0%
16 Warranty Recovery	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17 Build America Bond Subsidies	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18 Product Sales and Development	91.4%	-0.3%	0.1%	0.0%	8.8%	
19 Pass Sales	222834.0%	88.3%	3885.3%	1418.0%	-228125.6%	
20 Property Management	40.4%	10.8%	6.7%	1.3%	40.8%	
21 Vanpool/Rideshare	64.3%	7.2%	0.0%	0.0%	28.6%	
22 Salt Lake City Agreement	100.0%	0.0%	0.0%	0.0%	0.0%	
23 Planning						
24 Capital Development Agreements	99.5%	0.0%	0.0%	0.0%	0.5%	
25 Other	2.5%	-0.7%	0.2%	0.0%	3.2%	
26 <b>Total</b>	<b>43.3%</b>	<b>43.3%</b>	<b>0.0%</b>	<b>0.0%</b>	<b>0.6%</b>	

<sup>1</sup> Federal preventive maintenance funds and federal RideShare funds

**SUMMARY OF APPROVED DISBURSEMENTS OVER \$200,000  
FROM Feb 1, 2026 THROUGH Feb 28, 2026  
(UNAUDITED)**

EXHIBIT 1-10

<u>Contract #</u>	<u>Vendor</u>	<u>Check #</u>	<u>Date</u>	<u>Check Total</u>
01903143	UNITED WAY COMMUNITY SERV	907426	2/4/2026	(209,888.26)
02033995	VIA TRANSPORTATION INC	907428	2/4/2026	(276,723.64)
02203566	SCHEIDT & BACHMANN USA, INC.	907429	2/4/2026	(446,840.10)
01140676	Stacy and Witbeck, Inc.	907430	2/4/2026	(655,493.00)
01140027	Stacy and Witbeck, Inc.	907430	2/4/2026	(431,170.08)
01140027	Stacy and Witbeck, Inc.	907430	2/4/2026	(662,902.00)
20039894	Cambridge Associates, LLC.	907443	2/6/2026	(1,317,861.92)
R2025-04-01	MUTUAL OF AMERICA LIFE INS.	907445	2/6/2026	(1,486,104.50)
20039895	UTA/ATU JOINT INSURANCE TRUST	907446	2/6/2026	(275,897.48)
20039900	UTAH ST TAX (WITHHOLDING ONLY)	907447	2/6/2026	(345,305.18)
R2025-04-01	VANTAGEPOINT TRANSFER AGENTS -	907448	2/6/2026	(1,729,447.76)
20039898	UNITED STATES TREASURY	907451	2/6/2026	(682,089.90)
20039898	UNITED STATES TREASURY	907452	2/6/2026	(324,572.20)
20039898	UNITED STATES TREASURY	907453	2/6/2026	(1,387,990.90)
00017464	Landmark Companies Inc	907504	2/11/2026	(205,235.77)
02003243	MV PUBLIC TRANSPORTATION	907505	2/11/2026	(285,930.10)
20039667	Utah Dept of Transportation (U	907507	2/11/2026	(3,000,000.00)
UT94-134VT-1	WASATCH FRONT REG COUNCIL	907581	2/18/2026	(318,270.00)
24384313	The Driver Provider	907582	2/18/2026	(332,007.00)
02033995	VIA TRANSPORTATION INC	907583	2/18/2026	(552,806.58)
20039894	Cambridge Associates, LLC.	907597	2/19/2026	(1,362,040.73)
20039895	UTA/ATU JOINT INSURANCE TRUST	907600	2/19/2026	(274,923.78)
20039900	UTAH ST TAX (WITHHOLDING ONLY)	907601	2/19/2026	(356,723.96)
R2025-04-01	VANTAGEPOINT TRANSFER AGENTS -	907602	2/19/2026	(206,701.92)
20039898	UNITED STATES TREASURY	907605	2/19/2026	(1,036,736.28)
20039898	UNITED STATES TREASURY	907606	2/19/2026	(242,501.02)
20039898	UNITED STATES TREASURY	907607	2/19/2026	(714,246.49)
20039894	Cambridge Associates, LLC.	907609	2/20/2026	(1,299,931.40)
20039895	UTA/ATU JOINT INSURANCE TRUST	907612	2/20/2026	(274,545.85)
20039900	UTAH ST TAX (WITHHOLDING ONLY)	907613	2/20/2026	(343,123.71)
20039898	UNITED STATES TREASURY	907616	2/20/2026	(995,814.36)
20039898	UNITED STATES TREASURY	907617	2/20/2026	(232,884.34)
20039898	UNITED STATES TREASURY	907618	2/20/2026	(675,551.09)
R2025-04-01	ROCKY MOUNTAIN POWER	396520	2/25/2026	(448,105.29)
01126114	PENN MACHINE COMPANY LLC	907697	2/25/2026	(291,132.00)
24384313	The Driver Provider	907698	2/25/2026	(411,688.68)
00243813	PAULSEN CONSTRUCTION, INC.	907699	2/25/2026	(453,683.60)
02303738	TRAPEZE SOFTWARE GROUP, INC.	907701	2/25/2026	(212,980.90)
02303738	TRAPEZE SOFTWARE GROUP, INC.	907701	2/25/2026	(346,028.10)
02303791	Stadler US, Inc	907702	2/25/2026	(41,825,631.05)
00213530	PEHP (Use for Admin)	907730	2/25/2026	(286,187.44)
20040035	Hickman Land Title Company	907731	2/25/2026	(492,199.00)
00213531	SELECT HEALTH	907732	2/25/2026	(1,037,230.50)





# Utah Transit Authority

## MEETING MEMO

669 West 200 South  
Salt Lake City, UT 84101

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**Board of Trustees**

**Date:** 4/8/2026

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**TO:** Board of Trustees  
**FROM:** Annette Royle, Chief of Board Strategy and Governance  
**PRESENTER(S):** Neiufi Longi, Public Policy Analyst

**TITLE:**  
**R2026-04-01 - Resolution Adopting Revised Board Policy 1.2 Ethics**

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**AGENDA ITEM TYPE:**  
Resolution

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**RECOMMENDATION:**  
Approve Resolution R2026-04-01 Adopting Revised Board Policy 1.2 Ethics, as presented.

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**BACKGROUND:**  
The Special Districts Act 17B-1-301 and the Utah Public Transit District Act section 17B-2a-8 outline the duties and powers of a large public transit district's Board of Trustees. One of those duties includes developing and approving board policies, ordinances, and bylaws.

The Board of Trustees last updated Board Policy 1.2 Ethics in December 2022.

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**DISCUSSION:**  
The agency is recommending that Board Policy 1.2 Ethics be revised as presented.

The proposed revisions include updates in practice, required timeframes, and clarifying language. The revisions will:

- Incorporate statutory ethics requirements for Authority Officers (as designated in UTA Bylaws) to comply with the Utah Public Officers' and Employee's Ethics Act, and to submit a code of conduct and a financial disclosure report annually.
- Establishes an Ethics Committee and defines their duties to perform organizational ethics governance and administration.

Revised Board Policy 1.2 received feedback at the March 25th Board of Trustees meeting and is now back for adoption by resolution.

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**ALTERNATIVES:**

The Board may modify the proposed policy revisions prior to adoption.

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**FISCAL IMPACT:**

None

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**ATTACHMENTS:**

- R2026-04-01 Resolution Adopting Revised Board Policy 1.2 Ethics

**RESOLUTION OF THE BOARD OF TRUSTEES OF THE UTAH TRANSIT  
AUTHORITY ADOPTING REVISED BOARD POLICY 1.2 -- ETHICS**

R2026-04-01

April 8, 2026

WHEREAS, the Utah Transit Authority (the “Authority”) is a large public transit district organized under the laws of the State of Utah and created to transact and exercise all of the powers provided for in the Utah Limited Purpose Local Government Entities-Special Districts Act and the Utah Public Transit District Act; and

WHEREAS, UTAH CODE §17B-2a-808.1 empowers the board of trustees of a large public transit district, such as the Authority, to develop and approve board policies, ordinances, and bylaws; and

WHEREAS, the Board of Trustees (the “Board”) adopts Board Policies and Bylaws to provide leadership and governance to the Authority and fulfill its statutory governance responsibilities of the Authority; and

WHEREAS, the Board of the Authority last updated its Board Policy 1.2 – Ethics in December 2022; and

WHEREAS, the Board of the Authority wishes to revise Board Policy 1.2 – Ethics to incorporate statutory ethics requirements and to create an Ethics Committee; and

WHEREAS, the Board wishes to adopt this revised Board Policy.

NOW, THEREFORE, BE IT RESOLVED by the Board of Trustees of the Utah Transit Authority:

1. That the revised Board Policy 1.2 – Ethics, attached as Exhibit A, is hereby adopted.
2. That the Board of Trustees formally ratifies actions taken by the Authority, including those taken by the Executive Director, Board staff, staff, and counsel that are necessary or appropriate to give effect to this Resolution.
3. That the corporate seal be attached hereto.

Approved and adopted this 8th day of April 2026.

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Carlton Christensen, Chair  
Board of Trustees

ATTEST:

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Secretary of the Authority

(Corporate Seal)

Approved As To Form:


Signed by:  
  
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Legal Counsel

Exhibit A  
(Revised Board Policy 1.2 - Ethics)



# Ethics

## Board of Trustees Policy No. 1.2

### Application: Board of Trustees and Local Advisory Council

I. Purpose: ~~The purpose of this policy is to set forth standards of ethical conduct for the Board of Trustees (Trustees), Officers of the Authority (as defined in UTA Bylaws Article II, Section 4. [Authority Officers]) and Local Advisory Council Members (Members).~~ The individuals comprising the Authority’s Board of Trustees, Authority Officers, and ~~the~~ Members of the Local Advisory Council commit themselves to observe high professional and ethical standards in order to maintain public confidence in the integrity of the Authority. ~~The purpose of this policy is to set forth standards of ethical conduct for Trustees and Members.~~

### II. Definitions:

A. *“Conflict of Interest”* means a personal or economic interest, outside employment, outside interest or other circumstance or relationship that impairs the ability to discharge duties in an ethical manner consistent with the best interests of the Authority.

B. *“Ethics ~~Officer~~Committee”* means ~~the individual appointed by the Board of Trustees to act in that capacity on behalf of the Authority~~ the committee established by this policy to perform the duties of organizational ethics governance and administration defined in section III.B.

~~C. “Member” means an individual appointed to the Local Advisory Council.~~

~~D. “Relative”~~ means a father, mother, husband, wife, son, daughter, sister, brother, uncle, aunt, nephew, niece, first cousin, grandparent, and grandchild, including in-laws, step relations, and relationships through adoption, as well as individuals in a personal relationship of a romantic or intimate nature.

~~E.C. “Trustee” means an individual appointed to the Authority’s Board of Trustees.~~

### III. Policy:

#### A. Ethical Standards

1. Members, Trustees, Authority Officers, ~~the Executive Director~~, Chief Officers, and employees of the Authority will comply with applicable statutory ethical requirements, including those set forth in the Utah Public Officers’ and Employees’ Ethics Act.
2. Trustees, as employees of the Authority, will also comply with UTA Policy UTA.01.01 – Ethics.

#### B. Ethics Committee

1. The Ethics Committee will consist of the following positions:
  - a. Member of the Board of Trustees, appointed by the Chair of the Board of Trustees.

- b. Representative appointed by the Office of the Utah Attorney General with legal ethics expertise.
  - c. Director of Internal Audit, who is appointed Ethics Officer in UTA Bylaws (Article II, Section 6).
- 2. The Ethics Committee will:
  - a. Oversee the Authority Ethics Program.
  - b. Maintain strict confidentiality of all activities, conversations, reports, and actions.
  - c. Receive reports from UTA's Ethics Hotline or other ethical complaints; conduct investigations or appoint independent investigator(s); oversee investigations, and deliver final reports required in Section III.G. of this policy.
  - d. Meet annually to review and make recommendations for updates to Authority ethics policies, code of conduct certification, financial disclosure form, and updates to the ethics training program.
  - e. Perform all other duties required by this policy including oversight of conflict of interest, code of conduct, and financial disclosure activities.

### B-C. Conflicts of Interest

1. Members and Trustees will ~~promptly~~ disclose any Conflicts of Interest in writing to the applicable Chair and to the Authority's Ethics ~~Officer-Committee~~ within ten (10) business days of identifying the Conflict of Interest.
2. The Ethics ~~Officer-Committee~~ will notify the appointing jurisdiction of a Member or Trustee in writing of any Conflicts of Interest disclosed or identified.
3. In the event a Member or Trustee becomes aware of a Conflict of Interest during or immediately prior to a Board of Trustees meeting or Local Advisory Council meeting, the Member or Trustee will verbally disclose the Conflict of Interest in the meeting. The disclosure will be reflected in the meeting minutes.
4. A Member or Trustee having a Conflict of Interest will recuse ~~themselves/himself or herself~~ from deliberations and votes related to the Conflict of Interest and leave the meeting for the duration of that issue being discussed.
5. If a Member or Trustee discloses a Conflict of Interest on an issue being considered and does not voluntarily recuse ~~themselves/himself or herself~~, the Board of Trustees or Local Advisory Council, as applicable, may, by simple majority, vote to:
  - a. Determine if the Member or Trustee with the Conflict of Interest will be recused from voting on the particular issue;
  - b. Determine if the Member or Trustee with the Conflict of Interest will participate in a discussion on the particular issue;
  - c. Determine if the Member or Trustee with the Conflict of Interest will leave the meeting room during discussions on the particular issue; or
  - d. Determine other conditions or actions as appropriate.
6. A Member, ~~or~~ Trustee's, a Member or Trustee's Relative, or an entity owned or affiliated with a Member or Trustee that has ownership of investment property within a 0.5 mile radius, ~~as the crow flies~~, from any FrontRunner station, TRAX station, or transit-oriented development will be considered to be a Conflict of Interest requiring disclosure. The ownership of investment property within a 0.5 mile radius of a bus stop is not considered to be a Conflict of Interest.

~~C.D.~~        Nepotism

1. The Authority will not hire or conduct business with Relatives of current Trustees and Members.
2. If a Trustee or Member is appointed and has a Relative who currently works for or conducts business with the Authority, the Trustee ~~or~~ Member will disclose the relationship to their appointing authority and take steps to mitigate any Conflict of Interest following their appointment. If the Trustee or Member fails to mitigate the Conflict of Interest regarding the Relative, the Board of Trustees or the Local Advisory Council may take any of the actions described in Section ~~III.BC.(5)~~.

~~D.E.~~ Code of Conduct

1. As set forth in the Authority’s Bylaws, Trustees, Members and ~~Trustees~~Authority Officers will complete and submit an Annual Certification of Code of Conduct. The Annual Certification of Code of Conduct must be completed and submitted prior to being seated and at least annually thereafter on October 31. If October 31 falls on a Saturday or Sunday, the Code of Conduct will be due on the Monday following October 31.
2. The Ethics ~~Officer~~Committee, in consultation with the ~~Director of Board Governance~~Chief of Board Strategy and Governance and Legal Counsel, will revise the Annual Certification of ~~Board Member~~ Code of Conduct, as needed.

~~E.F.~~ Financial Disclosure Report

1. As set forth in the Authority’s Bylaws, Trustees, Members and ~~Trustees~~Authority Officers will complete and submit a Financial Disclosure Report. The Financial Disclosure Report must be completed and submitted prior to being seated and at least annually thereafter on October 31. If October 31 falls on a Saturday or Sunday, the Financial Disclosure Report will be due on the Monday following October 31.
2. Trustees, Members and ~~Trustees~~Authority Officers will submit a revised Financial Disclosure Report within ten (10) business days if there is a material change in the information previously disclosed in the most recently completed Financial Disclosure Report.
3. The Ethics ~~Officer and Legal Counsel~~Committee will review Financial Disclosure Reports for Conflicts of Interest. If Conflicts of Interest have been disclosed, the Ethics ~~Officer~~ Committee and ~~Director of Board Governance~~Chief of Board Strategy and Governance will attempt to resolve them with the Trustee, Member or ~~Trustee~~Authority Officer. If the issue cannot be resolved, the Ethics ~~Officer~~ Committee will submit the disclosure to the Chair of the Board of Trustees and the Chair of the Local Advisory Council. If the issue involves the Chair of the Board of Trustees or the Chair of the Local Advisory Council, the Ethics ~~Officer~~ Committee will submit the disclosure to the Audit Committee.
4. The Ethics ~~Officer~~ Committee will notify the appointing jurisdiction of a Member or Trustee in writing of any Conflicts of Interest disclosed in the Financial Disclosure Report.
5. Financial Disclosure Reports will be classified as public records under the Government Records Access and Management Act.
6. The Authority’s Ethics ~~Officer~~ Committee, in consultation with the ~~Director of Board Governance~~ Chief of Board Strategy and Governance and Legal Counsel, will revise the Financial Disclosure Report as needed.

F.G. Ethics Complaints

1. Ethics complaints alleging violations of the Utah Public Officers' and Employees' Ethics Act against Trustees, Members, ~~Authority Officer, the Executive Director,~~ Chief Officers, and employees will be referred to the Political Subdivision Ethics Review Commission for resolution.
2. Ethics complaints involving Trustees, Members, ~~an Authority Officer, the Executive Director,~~ Chief Officers, and employees that do not allege violations of the Utah Public Officers' and Employees' Ethics Act or do not meet the requirements for a complaint under the standards of the Political Subdivision Ethics Review Commission will be investigated by the ~~Ethics Officer~~ Ethics Committee or an independent investigator appointed by the ~~Ethics Officer~~ Committee, if necessary.
  - a. The ~~Ethics Officer~~ Committee will submit written findings of investigations involving Trustees, Officers of the Authority, and Members to the Chair of the Board of Trustees and the Chair of the Local Advisory Council.
  - b. If ~~the an~~ investigation involves the Chair of the Board of Trustees or the Chair of the Local Advisory Council, the ~~Ethics Officer~~ Committee will submit the written findings of the investigation to the Audit Committee.
  - ~~2.c. The Executive Director will designate an investigator to investigate ethics Co~~ omplaints against any member of the Ethics Officer Committee will be investigated by the remaining Ethics Committee members. If necessary, the Chair of the Board may appoint an individual to temporarily serve on the Ethics Committee or designate an independent investigator to investigate ethics complaints relating to any member(s) of the Ethics Committee.
3. Following the receipt of written findings of an ethics investigation from the ~~Ethics Officer~~ Committee or the Political Subdivision Ethics Review Commission, the Local Advisory Council or the Board of Trustees may consider the adoption of a resolution of public censure and/or a resolution recommending the removal of a Member or Trustee as determined by a majority vote at a regularly scheduled meeting. ~~If a resolution is adopted, a~~ copy of the findings and the resolution, if applicable, will be forwarded to the appropriate appointing authority.

IV. Cross References: Utah Public Transit District Act, Utah Code §17B-2a-8; Government Records Access and Management Act, Utah Code §63G-2; Utah Public Officers' and Employees' Ethics Act, Utah Code §67-16-101; Political Subdivisions Ethics Review Commission Utah Code §63A-15-103; UTA Policy UTA.01.01 - Ethics.

Approved this ~~21st~~ day of ~~December~~ 2026

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Carlton Christensen - Chair, Board of Trustees

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Secretary of the Authority

Approved as to form and content:

Legal Counsel for the Authority

Revision/Review History:

Date of Local Advisory Council Consultation	Board of Trustees Approval (Resolution Number)	Action
06-12-2019	R2019-06-01 (06-19-2019)	Combined Board Policy Nos. 4.1.13 – Nepotism, 4.1.10 – Annual Certification of Board Member Code of Conduct, 2.1.9 – Financial Disclosure/Conflict of Interest, 4.4.1 – Actual and Potential Conflicts of Interest, and 4.4.7 – Ethics Review Commission into Board Policy 1.2 – Ethics.
09-07-2022	R2022-12-07 (12-21-2022)	Updated cross references and roles for key staff and counsel.
<u>N/A</u>	<u>R2026-XX-XX</u> <u>(04-XX-2026)</u>	<u>Added Authority Officers within scope of policy; replaced Ethics Officer position and duties with an Ethics Committee.</u>



# Utah Transit Authority

## MEETING MEMO

669 West 200 South  
Salt Lake City, UT 84101

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**Board of Trustees**

**Date: 4/8/2026**

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**TO:** Board of Trustees  
**FROM:** Jon Larsen, Chief Capital Services Officer  
**PRESENTER(S):** David Osborn, Acting Director of Capital Services  
Ethan Ray, Project Manager II

**TITLE:**  
**R2026-04-02 - Authorizing Execution of an Interlocal Cooperation Agreement with Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency for the S-Line Extension**

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**AGENDA ITEM TYPE:**  
Resolution

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**RECOMMENDATION:**  
Approve Resolution R2026-04-02 authorizing execution of an Interlocal Cooperation Agreement (ILA) with Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency (CRA) for the S-Line Extension as presented.

---

**BACKGROUND:**  
UTA has been working collaboratively with Salt Lake City to extend the S-Line Streetcar from the existing end of the line at Fairmont Station (1040 East) to the heart of the Sugar House business district at Highland Drive. This project will help support economic development and serve the growing population of Sugar House by providing an improved connection to Sugar House. The project has included close coordination with the CRA, who owns property adjacent to the project. The Utah Department of Transportation (UDOT) has also been a collaborator on this project due to the State funding contribution. The Salt Lake City Council approved this ILA on March 24, 2026.

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**DISCUSSION:**  
This Interlocal Cooperation Agreement includes the terms and conditions for the implementation of the S-Line Extension project. The agreement addresses the alignment and use of Salt Lake City Streets for the project. It also identifies the terms for the operations and maintenance of the S-Line Extension project. Salt Lake City will be providing UTA license to construct, operate and maintain the project in the City right-of-way and property. The license will have an initial term concluding on December 31, 2060. The term may automatically renew for an additional term of twenty-five (25) years unless either party provides notice at least one hundred and eighty (180) days prior to the expiration of the initial term that it does not intend to renew the agreement.

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The agreement also outlines Salt Lake City's contribution of \$395,000 to the project.

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**ALTERNATIVES:**

An agreement is needed with Salt Lake City and CRA to advance the S-Line Extension project. If desired, UTA could pursue a different type of agreement(s), however doing so would cause delays to the project implementation.

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**FISCAL IMPACT:**

The total project cost is \$43,567,129. In 2021, the State of Utah contributed \$12,000,000 through House Bill 433 to fund the project's design phase. The project has since been awarded and secured an additional \$18,900,000 in State Transit Transportation Investment Fund (TTIF) funding.

As part of the TTIF award, a required local match of \$12,310,000 will be met through multiple sources, including a Salt Lake City property donation valued at \$5,140,070 and a UTA in kind contribution of a light rail vehicle (rehabilitation of vehicle 1137) valued at \$2,530,000. Additional local funding includes contributions from UTA (\$3,750,000 - Funding is included in the 2026 Approved Capital Budget (MSP259)), Salt Lake City (\$395,000), and the Salt Lake City Community Reinvestment Agency (\$852,059). The available sources of match (\$12,667,129) exceed the minimum required amount.

TTIF funding and match were approved by the Board on March 11, 2026. Please note there were minor adjustments to the match amounts from Salt Lake City and Community Reinvestment Agency following the March 11<sup>th</sup> Board meeting, which increased the overall available funding by \$32,000.

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**ATTACHMENTS:**

- R2026-04-02 Authorizing Execution of ILA with Salt Lake City and the Salt Lake City Community Reinvestment Agency for the S-Line Extension, including Exhibit A: 26-P00547 Sugar House Streetcar Extension Interlocal Agreement (with Exhibits A & B)

**RESOLUTION OF THE BOARD OF TRUSTEES OF THE UTAH TRANSIT AUTHORITY  
AUTHORIZING EXECUTION OF AN INTERLOCAL COOPERATION AGREEMENT  
WITH SALT LAKE CITY CORPORATION AND THE SALT LAKE CITY COMMUNITY  
REINVESTMENT AGENCY FOR THE S-LINE EXTENSION**

R2026-04-02

April 8, 2026

WHEREAS, the Utah Transit Authority (the "Authority") is a large public transit district organized under the laws of the State of Utah and created to transact and exercise all of the powers provided for in the Utah Limited Purpose Special Government Entities - Special Districts Act and the Utah Public Transit District Act (collectively the "Act"); and

WHEREAS, the Utah Interlocal Cooperation Agreement Act, UTAH CODE § 11-13-101, *et seq.* enables components of government to execute an Interlocal Cooperation Agreement ("ILA") so both entities may obtain mutual advantage and economies of scale, among other things; and

WHEREAS, the Act empowers the Board of Trustees ("Board") of the Authority with the ability to execute ILAs on behalf of the Authority; and

WHEREAS, Salt Lake City Corporation, the Salt Lake City Community Reinvestment Agency, and the Authority have reached agreement for the Extension of the S-Line Streetcar; and

WHEREAS, this agreement allows for the design, construction, and operation of an extension of the Authority's S-Line Streetcar Line in the Sugarhouse Neighborhood of Salt Lake City; and

WHEREAS, the Board of the Authority recognizes the mutual advantage of this agreement, and desires to, in accordance with the Utah Interlocal Cooperation Agreement Act, approve and authorize the execution of an ILA with Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency for the extension of the S-Line Streetcar.

NOW, THEREFORE, BE IT RESOLVED by the Board:

1. That the Board hereby approves the Interlocal Cooperation Agreement with Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency in substantially the same form as attached as Exhibit A.
2. That the Executive Director is authorized to execute the Interlocal Cooperation Agreement with Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency in substantially the same form as attached as Exhibit A.
3. That the Board hereby ratifies any and all actions previously taken by the Authority's management, staff, and counsel related to negotiating and

R2026-04-02

1

implementing the Interlocal Cooperation Agreement with Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency.

4. That the corporate seal shall be affixed hereto.

APPROVED AND ADOPTED this 8th day of April 2026.

---

Carlton Christensen, Chair  
Board of Trustees

ATTEST:

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Secretary of the Authority

(Corporate Seal)

Approved as to Form:


Signed by:  
  
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\_\_\_\_\_  
Legal Counsel

EXHIBIT A  
(Sugar House Streetcar Extension Interlocal Cooperation Agreement  
With Salt Lake City Corporation and the Salt Lake City Community Reinvestment Agency)

## SUGAR HOUSE STREETCAR EXTENSION INTERLOCAL AGREEMENT

This Sugar House Streetcar Extension Interlocal Agreement (the “Agreement”) is entered into as the day this Agreement is executed by both Parties (the “Execution Date”), by and between the UTAH TRANSIT AUTHORITY, a large public transit district organized pursuant to the laws of the State of Utah (hereinafter “UTA”), SALT LAKE CITY COMMUNITY REINVESTMENT AGENCY, a Utah political subdivision (hereinafter “CRA”), and SALT LAKE CITY CORPORATION, a Utah municipality and political subdivision (hereinafter “City”), each a “Party” and collectively the “Parties.”

### RECITALS

WHEREAS, City, CRA, and UTA are public agencies as defined by the Interlocal Cooperation Act, Utah Code Section 11-13-101 *et seq.*, and as such, may enter into an agreement with one another to provide services that they are each authorized by statute to provide; and

WHEREAS, the City owns and has jurisdiction over a network of streets within its municipal boundaries; and

WHEREAS, UTA owns and operates the Sugar House Streetcar in the rail corridor known as the “Sugar House Spur,” which runs easterly from UTA’s North/South TRAX light rail corridor for approximately two miles through South Salt Lake City (“SSL”) and Salt Lake City at approximately 2200 South; and

WHEREAS, SSL is not a party to this Agreement as the limits of construction are outside of their city limits; and

WHEREAS, UTA is engaged in a project to design, construct, and operate the S-Line Streetcar Extension that will extend from McClelland Street parallel to Sugarmont Drive going east towards Simpson Avenue and across Highland Drive to the Sugar House Shopping Center in Salt Lake City (the “Project” or the “S-Line Streetcar Extension”), serving Salt Lake City; and

WHEREAS, the CRA currently owns property under a portion of the Project, but, shortly after the execution of this Agreement, intends to convey the impacted property to the City; and

WHEREAS, a portion of the Project will be constructed within certain public rights-of-way and/or property owned by the City, as generally identified in “Exhibit A” (“Licensed Property Map”), and as more specifically depicted in the S-Line Streetcar Extension alignment illustrations attached hereto as Exhibit “B” (“Design Drawings”); and

WHEREAS, the S-Line Extension will contain an exclusive alignment between McClelland Street Crossing and the intersection of 1100 East & Simpson Avenue, and then operate in mixed-traffic conditions until and across Highland Drive, with a terminus at Sugar House Shopping Center on privately-owned property (as depicted in Exhibit “A”); and

WHEREAS, the Parties have collaborated on this Project for several years; and

WHEREAS, UTA is the contracting party for the Project and has retained a progressive design-build contractor for the nearly-completed design and construction of the Project; and

WHEREAS, UTA will cause the Project to be designed and constructed, at a minimum, in accordance with UTA's Design Criteria Manual of standards and requirements, and, where applicable, in accordance with the Manual of Standard Specifications and Standard Plans, as published by the Utah Chapter of APWA, and the City Public Utilities Department Performance Specifications and Design Criteria for culinary water, sanitary sewer and storm drain facilities, and the FHWA Manual of Uniform Traffic Control Devices, and the Americans With Disabilities Act, and all applicable building codes, laws and regulations; and

WHEREAS, the State has provided funding for the Project, and City has committed \$395,000.00 towards the Project, and UTA will bear additional Project costs; and

WHEREAS, the City signed an Interlocal Agreement Regarding The Sugar House Streetcar Project, dated December 13, 2011, for the construction and operation of the initial streetcar system along the Sugar House Spur in Salt Lake City, and signed an additional Interlocal Agreement Regarding the Sugar House Corridor Management, dated December 8, 2013, outlining management policies for the Streetcar Project and providing for reciprocal license agreements for the use of certain City-owned property for the project and use of certain UTA-owned property by the City ("Historic Agreements"); and

WHEREAS, pursuant to the Historic Agreements, the City desires to permit UTA to use the rights-of-way and property required for the S-Line Streetcar Extension; and

WHEREAS, UTA and the City desire to define the Parties' roles and responsibilities with respect to the design and construction of the Project and with respect to the operation and maintenance of the completed S-Line Streetcar Extension.

## **AGREEMENT**

NOW THEREFORE, based on the stated Recitals, which are incorporated herein by reference, and for and in consideration of the mutual covenants and agreements hereinafter set forth, the mutual benefits to the Parties to be derived here from, and for other valuable consideration, the receipt and sufficiency of which the Parties acknowledge, it is hereby agreed as follows:

### **ARTICLE I INCORPORATED TERMS AND DEFINITIONS**

For purposes of this Agreement, the following definitions shall apply:

1.1 "City Facilities" means all City-owned surface, aerial, or subsurface public improvements of any kind which are located on the City or CRA property, including, without limitation, public utility facilities, water and sewer lines, gas lines, curbs and gutters, sidewalks, street paving, storm drains, canals, trees and landscaping, traffic signals, streetlights, electrical wiring, controllers, poles and related facilities, lighting facilities, and fire protection facilities.

1.2 "Communications Infrastructure" means UTA's duct bank for fiber and other lines necessary for the control of the track and station. This duct bank is located under the trackway, as shown in Exhibit "B."

1.3 "Foul Zone" means the area that is within ten (10') feet of the center line of the S-Line Streetcar Extension Trackway. When the S-Line Streetcar Extension Trackway crosses a signalized grade crossing, the foul zone is reduced to four (4') feet from the rail head, measured from the outer side of the

rail head that is facing away from the center of the tracks.

1.4 “Licensed Property” means the public right-of-way between the rail alignment curbs, consisting of the McClelland Street Crossing, Exclusive Trackway, and Mixed-Flow segments as depicted in Exhibits “A” and “B”. The Licensed Property includes any property owned by the CRA that CRA plans to convey to the City.

1.5 “S-Line Streetcar Extension Trackway” means the sections of track and area within the curb limits of the trackway to be constructed on or adjacent to Salt Lake City Streets as part of the Project.

1.6 “Salt Lake City Streets” means those public streets within the City’s municipal boundaries that are not designated as State highways under Utah Code Ann. Title 72, Chapter 4. Until such property is conveyed to the City, Salt Lake City Streets also means certain portions of CRA property under the Licensed Property.

1.7 “Salt Lake City Street Improvements” means the land, roadway materials, curb, gutter, sidewalks, traffic signal infrastructure and equipment and other improvements to be acquired, installed, constructed, reconstructed, or relocated on Salt Lake City Streets as part of the Project, and which are outside of the curb limits of the S-Line Streetcar Extension Trackway and are not considered UTA Facilities. The Salt Lake City Street Improvements include relocated curb and gutter, relocated sidewalks, modified roadway widths, added traffic signals and control devices for grade crossings, and other additional appurtenances required for the Project.

1.8 “Party” and “Parties” means UTA or Salt Lake City, and UTA and Salt Lake City, respectively.

1.9 “UTA Facilities” means the S-Line Streetcar Extension Trackway, appurtenant improvements, and Communications Infrastructure serving the S-Line Streetcar Extension.

## **ARTICLE II ALIGNMENT AND USE OF SALT LAKE CITY STREETS**

2.1 The S-Line Streetcar Extension alignment is depicted on Exhibits “A” and “B” as attached hereto and incorporated herein.

2.2 The City (and, as applicable, CRA) grants to UTA a license in, and authorizes UTA to use, on a non-exclusive basis, the following portions of Salt Lake City Streets and land (collectively referred to as the “Licensed Property”) for the use of UTA to construct, operate and maintain the Project in the City right-of-way and property as depicted in the plans and maps attached as Exhibits “A” and “B”:

2.2.1 Such surface areas and air rights of City streets and property along the Project as necessary for UTA to construct the trackwork, grade crossing, signals, Communication Infrastructure, overhead contact system, and other appurtenances as necessary for the efficient operation of the Project already occupied by existing or currently planned City streets or land. UTA’s use of such property shall be strictly limited to the terms, conditions, limitations, and restrictions contained herein.

2.2.2 Such subsurface areas of City streets and property along the Project alignment as are necessary for UTA to install Communications Infrastructure such as duct banks, conduit, and fiber as necessary for the efficient operation of the Project, but not to include such subsurface areas that are already occupied by existing or currently planned utilities or other subsurface areas where the location of the Project's Communications Infrastructure would have an unreasonable negative impact on existing or currently planned utilities.

2.2.3 Whenever the City undertakes or approves the planned maintenance or construction of any facility owned, maintained, or operated by the City (including without limitation, installation of traffic signals, streetlights, sidewalks and pedestrian amenities where the improvement is so constructed or shall become a public facility owned, maintained, or operated by the City) and such maintenance or construction impacts the Project, the City shall:

- (a) Provide notice of such maintenance or construction to UTA at least one hundred and twenty (120) days prior to the commencement of such work; and
- (b) Provide UTA with copies of pertinent portions of the plans and specifications for any street improvement project so that UTA may consider and comment on such improvement project in regard to impacts on the S-Line.
- (c) No later than thirty (30) days after receipt of such notice and such plans and specifications, UTA shall complete its review of such facilities at no charge or expense to the City so as to accommodate such maintenance or construction at least thirty (30) days prior to commencement of such work.
- (d) If any relocation of UTA's Communication Infrastructure is required, UTA may, within forty-five (45) days after receipt of written notice requesting such relocation, submit to the City written alternatives to such relocation. Upon receipt, the City shall evaluate such alternatives and shall advise UTA in writing if one or more of the proposed alternatives are suitable to accommodate the work which would otherwise necessitate relocation of UTA's Communication Infrastructure. UTA agrees not to relocate Communication Infrastructure outside of the S-Line Streetcar Extension Trackway. In the event that the City is satisfied that there is no reasonable alternative, UTA shall relocate its Communication Infrastructure as otherwise provided in this Section. The City's determination shall be conclusive and not subject to any review, provided the City has acted in a non-discriminatory, non-arbitrary manner.
- (e) If the City requires the relocation (temporary or permanent) of UTA's Communication Infrastructure for the purpose of repairing, maintaining, or constructing any City Facility, UTA shall make such relocation and be responsible for all costs of such relocation. UTA may propose alternatives to such relocation to the same extent and subject to the same limitations as set forth in this Section.
- (f) Notwithstanding the foregoing, in the event of an emergency requiring immediate attention or access by the City to City Facilities or City Occupied Property, the City shall not be obligated to provide UTA prior notice, plans, or specifications as required in this Section. The City will notify UTA of any emergency work as early as practicable. In the event of such an emergency and upon request by the City, UTA agrees to stop S-Line operations at a location prior to the emergency area to prevent any

conflicts between S-Line operations and the emergency situation.

2.3 UTA accepts the Licensed Property “as-is” and hereby acknowledges that City has made no representation or warranty whatsoever with respect to the condition of the Licensed Property or fitness for a particular purpose, including but not limited to title matters inconsistent with use of the Licensed Property. As a provision of the Agreement, the City shall have the right to review and approve relevant Project deliverables and improvements that will be the responsibility of the City to operate and maintain as part of the Project. Should UTA fail to provide the City the right to review and approve relevant Project deliverables, the City has the right to refuse to accept the responsibility to operate and maintain such improvements. Such Project deliverables and improvements include UTA’s construction of a sidewalk adjacent to the Licensed Property during construction of the Project, which will be maintained by City and reconstructed by City to meet City standards. Upon satisfactory completion of the Project, the City shall accept such improvements that the City approved.

2.4 The final, precise locations and boundaries of the Licensed Property, City Facilities, and UTA Facilities shall be subject to change until delivery and acceptance of the final civil Design Drawings by both Parties. Because the Project is primarily a betterment for the benefit of public use and furthers specific goals and objectives set forth in the various City plans and official documents, and because UTA shall, at its own expense, operate, maintain, repair, and restore the Licensed Property, including paying all utilities and expenses associated with operation of the S-Line Streetcar Extension, the benefits to the City under this Agreement are adequate to provide fair and adequate consideration for use of the Licensed Property, without further compensation from UTA.

2.5 The City shall contribute \$375,000.00 towards Project costs to be used for reconstructing Simpson Ave including all associated paving, sidewalk improvements, landscaping, and utility work, and up to an additional \$20,000 for the replacement of a water meter servicing Fairmont Park.

2.6 The City shall own the Salt Lake City Street Improvements and will have no ownership interest in the UTA Facilities.

2.7 The Parties do not intend that UTA be deemed a “tenant” pursuant to Utah Code Ann. § 78B-6-801 *et seq.* Rather, the Parties intend that the Parties’ rights and remedies under this Agreement be determined solely by the express terms of this Agreement.

### **ARTICLE III TERM**

3.1 This Agreement is executed with the anticipation that UTA and the City will be able to fund the project with local funding, sufficient to complete the Project.

3.2 The rights set forth above are for an initial term concluding on December 31, 2060. The term may automatically renew for an additional term of twenty-five (25) years unless either Party provides notice at least one hundred and eighty (180) days prior to the expiration of the initial term that it does not intend to renew the Agreement. Because the forecasted use of the S-Line Streetcar Extension is expected to exceed the initial term and subsequent renewals thereof, the Parties agree to cooperate in seeking from the City’s governing body an extended term, reserving unto the City full legislative discretion. Notwithstanding the foregoing, City may not terminate this agreement so long as UTA is providing public transit service for City residents on the Licensed Property.

3.3 Notwithstanding section 3.2, the City may terminate this Agreement prior to the expiration of the initial term if (i) UTA permanently terminates or abandons transit service on the Licensed Property, (ii) UTA does not provide transit service on the Licensed Property for a period of two (2) years, except when due to a force majeure event and any time necessary to reconstruct UTA Facilities, or (iii) UTA commits an uncured material, continuing breach of this Agreement.

3.4 Prior to terminating the Agreement for one of the reasons stated in section 3.3, the City shall provide UTA with written notice of its intent to terminate and its basis for doing so. If UTA does not recommence transit service or cure the breach, as applicable, within six (6) months from the delivery date of the City's notice to UTA, the rights granted in the Agreement will terminate.

3.5 Upon the expiration of this Agreement or earlier termination of the of this Agreement, all UTA Facilities located on the Licensed Property shall, at the option of the City, be removed, and the Licensed Property shall be restored to a condition consistent with the then current condition of adjoining streets or other public facilities with respect to grade, appearance, quality, finish and type of construction, at the sole cost and expense of UTA. Restoration shall be performed within ninety (90) days of such expiration or termination, or such longer period as shall be required by the nature of the work and agreed to by the City. If UTA fails to restore the Licensed Property, the City may perform such work after thirty (30) days prior written notice to UTA, and UTA hereby agrees to pay all costs of the City in connection with such work, including any collection costs and attorney's fees.

#### **ARTICLE IV OPERATION AND MAINTENANCE**

4.1 UTA shall operate the S-Line Streetcar Extension and provide regular service to the general public in accordance with applicable Federal and State law.

4.1.1 In further consideration of UTA's use of the Licensed Property and as part of UTA's operation and maintenance responsibilities, UTA shall pay all expenses associated with maintaining the Project in an operable condition and Licensed Property, including, but not limited to, gas, electrical, water, sewer, recycling, and trash service.

4.1.2 In recognition of the safety concerns, the Parties intend that no part of the S-Line Streetcar Extension or S-Line Streetcar Extension Trackway will be considered public forums.

4.1.3 Without the prior written approval of the City, no commercial advertising shall be allowed on UTA Facilities except that advertising for the S-Line Streetcar Extension itself shall be allowed to the extent it conforms to the Salt Lake City Code, subject to UTA approval of any advertising thereupon.

4.1.4 UTA shall obtain all required City agreements, permits, and property rights before proceeding with constructing the Project, performing any maintenance or improvement on or near City Facilities or Licensed Property, including but not limited to public way permits, Transportation permits, and Department of Public Utilities permits.

4.1.5 As part of the Project, traffic signal systems along the S-Line Streetcar Extension

alignment will be programmed to give priority to S-Line Streetcar Extension vehicles (except that the highest priority will always be given to emergency vehicles) where the Parties deem appropriate, with the intent of striking an appropriate balance between S-Line Streetcar Extension efficiency and other local and regional traffic needs. The initial signal timing will be determined by the Parties as part of the design process. Once the S-Line Streetcar Extension is operational, upon the request of one Party, the Parties shall meet to assess system performance and discuss potential adjustments to the traffic signal priority system. S-Line Extension vehicles traveling on City streets shall be subject to all generally applicable traffic control ordinances and regulations. The Parties agree to work together to attempt to implement a solution that meets each Party's needs to the greatest extent possible and to take advantage of technologies to improve system performance for both the S-Line Streetcar Extension and general traffic.

4.16 UTA shall maintain, repair, and replace the Project and Licensed Property as necessary to keep the Project in good operating condition at UTA's expense. The Project shall be maintained consistent with the Design Drawings and any other applicable law. UTA shall be responsible for all routine maintenance and long-term maintenance of the UTA Facilities and other storm drainage, facilities and in the Foul Zone. UTA shall keep the Licensed Property free of weeds, garbage, graffiti, and unsightly or deleterious objects or structures. UTA shall be responsible for the concrete associated with trackage and shall be maintained as a smooth, safe, and consistent surface, free of depression or obstructions and consistent with the grade of the public streets and the City shall be responsible for the road pavement. The City shall be responsible for all routine maintenance and long-term maintenance of the City Facilities, including City streets, sidewalks, trails, and pedestrian crossings of the S-Line Streetcar Extension Trackway outside of the Foul Zone. Notwithstanding, the City will maintain the landscape buffer within the Foul Zone so long as there is adequate separation in place to satisfy the applicable safety requirements and allow City staff to work freely.

4.17 The City shall be responsible for snow removal on City Facilities including any pedestrian crossing of the S-Line Streetcar Extension Trackway.

4.18 The City shall also be responsible for all routine and long-term maintenance of any new landscaping installed in either the City Facilities or Salt Lake City Street Improvements, including the landscape buffer and sidewalk to the north of the S-Line Extension alignment.

4.19 UTA shall repair any damage to or disturbance of the City Facilities, surface or subsurface of the Licensed Property or other City streets that may become damaged or disturbed in connection with the installation, maintenance, removal, operation, or replacement of the S-Line Streetcar Extension. Any such maintenance within the S-Line Streetcar Extension System will be coordinated between the Parties to minimize disruption to the S-Line Streetcar Extension, other traffic, and public utilities. Should any repair or maintenance work impact the City-owned canal under Highland Drive, UTA shall not schedule or conduct any repair or maintenance work without first obtaining written approval from the City's Department of Public Utilities. Prior to UTA's performance of any maintenance or repair work, UTA will obtain any permits required by the City in connection with such work including a permit to work in the right of way, and shall abide by the reasonable requirements thereof. Any work impacting the City's canal is limited to a seasonal

window from October 15<sup>th</sup> and March 1<sup>st</sup>.

4.1.10 In the event that City requires access to City Facilities for maintenance, repair, upgrades, or other work on City Facilities, UTA shall cooperate with City to ensure City has full access to the City Facilities. UTA agrees to prioritize City maintenance, repair, upgrades and other work during the City's preferred business hours of Monday through Friday from 7:00 AM to 5:00 PM. To the extent any UTA facilities or structures must be modified, relocated, or removed to permit City access or to accommodate modifications to such City Facilities, UTA shall cooperate with City to make such modifications at UTA's cost.

4.1.11 The Parties shall in good faith endeavor to conduct their maintenance, repair, replacement and other activities in a manner that will avoid disruption to other Parties' activities whenever possible. By way of example, the City shall coordinate with UTA regarding maintenance work on the City Streets, and may agree to perform such work when the Project is not operating, in order to avoid disruption. Conversely, UTA shall coordinate with the City when performing maintenance work on the Project within the City Streets, and may agree to perform such work in off-peak traffic times to minimize disruptions to residents, businesses, and other traffic. In cases of emergency or exigent circumstances, the Party effecting the repair shall immediately notify and cooperate with the other party

4.1.12 To coordinate maintenance responsibilities, the Parties shall exchange routine and emergency contact information and keep such information current on "Exhibit "C", which exhibit is intended by the Parties to be a living document to be updated periodically ("Exhibit C" is attached hereto and hereby incorporated into and made a part of this Agreement by reference).

4.2 UTA acknowledges the existence of public and private utility facilities under the S-Line Streetcar Extension. The City reserves the right to enter the S-Line Streetcar Extension, and authorize other utility owners to do the same, as and when reasonably necessary to inspect, maintain, repair, or replace facilities located on, under, or adjacent to the S-Line Streetcar Extension. The City shall coordinate such work with UTA in advance, in accordance with the terms of this Agreement, in order to ensure safety and to minimize disruption of S-Line Streetcar Extension operations and agrees wherever possible to perform such work when the Project is not operating, in order to avoid disruption.

4.2.1 The City may grant additional franchises and permits in the future for utility facilities under or over the S-Line Streetcar Extension, so long as such facilities do not unreasonably interfere with operations of the S-Line Streetcar Extension. The City shall notify UTA of (i) any requests for new crossings of the S-Line Streetcar Extension, and (ii) any applications for street cuts or work permits near the S-Line Streetcar Extension. City permits over, under, across, or parallel to the S-Line Streetcar Extension shall require a Right-of-Entry Agreement and TRAX Access Permits from UTA prior to construction or maintenance of said utilities. UTA shall have four (4) weeks to review and object to such requests or applications prior to work commencing. The Parties shall cooperate to minimize disruption to S-Line Streetcar Extension service caused by the granting of any new permits for new utility facilities.

4.3 UTA has selected and engaged a qualified firm to design and construct the S-Line Streetcar Extension.

43.1 All construction within the City streets, but outside of the S-Line Extension Trackway, shall comply with the City's currently adopted engineering standards.

43.2 Throughout the construction process, the City will have continuous access to the Project site to monitor Project construction and to ensure the construction meets applicable City specifications and is performed in accordance with the final design documents.

4.4 Except for where the bollards and chain provide a necessary buffer, including the landscape buffer, sidewalk to the north of the S-Line Extension alignment discussed herein, and adjacent utilities managed by the City, the City shall obtain a Roadway Worker Protection permit from UTA, and a Right of Entry license, if performing work within ten (10) feet of centerline of track, or within ten (10) feet of the catenary lines.

## **ARTICLE V WARRANTIES & INDEMNITY**

5.1 UTA agrees to warrant the materials and workmanship of the Project for a minimum of one (1) year. If the Project includes work on the City canal under Highland Drive, the warranty is required to cover two full, operating irrigation seasons. The warranty period shall commence upon the acceptance of the City Facilities in writing by City Engineer. UTA's responsibility under its warranty is limited to repair or replacement of the defective condition, materials, or workmanship of the Project. The City will be beneficiary of such warranty, and each Party shall bear its own legal costs and fees incurred while enforcing such warranty. UTA will respond to any warranty request from the City within 48 hours, providing a corrective work plan to protect the Canal, if necessary.

5.2 UTA shall use the Licensed Property at its own risk and agrees to indemnify, defend, and hold harmless the City, CRA and their respective officers, officials, employees, and representatives for, from, and against all liabilities, claims, damages, losses, suits, judgments, causes of action and costs (including court costs and attorneys' fees,) of any nature, kind or description ("Losses") resulting from or by reason of UTA's use of the Licensed Property or any activities conducted thereon, including, but not limited to: (a) negligence or fault on the part of UTA or any employees, officials, agents or contractors of UTA related to the design, construction, maintenance or other work performed by or on behalf of UTA on the S-Line Streetcar Extension within the Licensed Property; (b) negligence or fault on the part of UTA or any employees, officials, agents, or contractors of UTA in the use or operation of the S-Line Streetcar Extension within the Licensed Property; or (c) UTA's breach of any provision of this Agreement. In the event any Losses are caused by the joint or concurrent negligence of UTA and the City, UTA shall indemnify the City only in proportion to UTA's own negligence and/or fault. Likewise, the City agrees to indemnify, defend, and hold harmless UTA and UTA's officers, officials, employees, and representatives for, from, and against all liabilities, claims, damages, losses, suits, judgments, causes of action and costs (including court costs and attorneys' fees,) of any nature, kind or description ("UTA Losses") resulting from negligence or fault on the part of the City or any of its employees, officials, or agents related to its involvement with the Project. In the event any UTA Losses are caused by the joint or concurrent negligence of UTA and the City, the City shall indemnify UTA only in proportion to the City's own negligence. This Section shall survive

expiration or earlier termination of this Agreement.

5.3 Governmental Immunity: UTA, City, and CRA are governmental entities under the Governmental Immunity Act of Utah, Section 63G-7-101 *et seq.* 1953 of the Utah Code (as amended) (the “Governmental Immunity Act”). Notwithstanding any provision to the contrary in this Agreement, (i) the obligations to indemnify, defend and/or hold harmless in this Agreement are limited to the dollar amounts set forth in the Governmental Immunity Act and are further limited only to the claims that arise from the negligent acts or omissions or fault of the parties, and (ii) nothing in this Agreement shall be construed to be a waiver of any Party of any defenses or limits of liability available under the Government Immunity Act.

**ARTICLE VI  
ENTIRE AGREEMENT – COUNTERPARTS**

6.1 This Agreement shall constitute the entire agreement and understanding of the Parties with respect to the subject matter hereof, and shall supersede all offers, negotiations and other agreements with respect thereto. Any amendment to this Agreement must be in writing and executed by the authorized representatives of each Party. This Agreement may be executed in any number of counterparts and by each of the Parties hereto on separate counterparts, each of which when so executed and delivered shall be an original, but all such counterparts shall together constitute but one and the same instrument. Any signature page of this Agreement may be detached from any counterpart and reattached to any other counterpart hereof. The facsimile transmission of a signed original of this Agreement or any counterpart hereof and the retransmission of any signed facsimile transmission hereof shall be the same as delivery of an original. To facilitate execution of this Agreement, the Parties may execute and exchange and sign this agreement through electronic or digital signatures, electronic mail or mailed counterparts of the signature pages, which shall be valid, binding and admissible as though original.

6.2 This Agreement is binding upon all of the assigns, grantees and successors in interest to each of the Parties and shall remain in full force and effect until amended as provided herein.

**ARTICLE VII  
FORUM SELECTION AND CHOICE OF LAW**

7.1 This Agreement shall be construed and interpreted under the laws of the State of Utah and the Parties agree that any action or proceeding brought concerning this Agreement may be brought only in the courts of Salt Lake County, Utah, and each Party hereto hereby consents to the jurisdiction of such courts.

**ARTICLE VIII  
NONWAIVER**

8.1 No covenant or condition of this Agreement may be waived by any Party, unless done so in writing, clearly stating such waiver. Forbearance or indulgence by any party in any regard whatsoever shall not constitute a waiver of the covenants or conditions to be performed by the other.

**ARTICLE IX**

## **SEVERABILITY**

9.1 This Agreement is executed by the Parties under current interpretation of any and all applicable federal, state, county, municipal, or other local statutes, ordinances, or laws. Furthermore, each and every separate division hereof shall have independent and severable status from each other division, or combination thereof, for the determination of legality, so that if any separate division herein is determined to be unconstitutional, illegal, violative of trade or commerce, in contravention of public policy, void, invalid or unenforceable for any reason, that separate division shall be treated as a nullity but such holding or determination shall have no effect upon the validity or enforceability of each and every other division, or other combination thereof.

## **ARTICLE X RECORDATION OF MEMORANDUM**

10.1 The Parties agree that City may record a memorandum of this Agreement against City's title in the Official Records of the Salt Lake County Recorder's Office. Such memorandum will be terminated upon expiration or earlier termination of this Agreement.

## **ARTICLE XI DEFAULT**

11.1 The failure to perform any provision of this Agreement by any Party, if such failure is not cured within thirty (30) days after written notice specifying such failure has been given to the non-performing party by the other party, shall constitute a default by the non-performing party. Provided, if a default cannot reasonably be cured within said thirty (30) day period, the non-performing party shall not be in default of this Agreement if it commences to cure the default within the thirty (30) day period, and diligently and in good faith continues thereafter to cure the default.

11.2 Upon the occurrence of any of the aforesaid events of default, the non-defaulting party shall have the option to pursue any remedy provided by law. Pursuit of any remedy hereunder shall not preclude pursuit of any other remedy, nor shall pursuit of any remedy constitute a forfeiture or waiver of any of the damages accruing to the non-defaulting party by reason of the violation of any of the terms, provisions and covenants herein contained. The specific remedies to which the non-defaulting party may resort under the terms of this Agreement are cumulative and are not intended to be exclusive of any other remedies or means of redress to which the non-defaulting party may be lawfully entitled in case of any breach or threatened breach of any provision of this Agreement by the other party and may include termination of the Agreement consistent with Section 3. The failure of the non-defaulting party to insist in any one or more cases upon the strict performance of any of the covenants of this Agreement or to exercise any option herein contained shall not be construed as a waiver or relinquishment for the future of such covenant or option. A receipt by the non-defaulting party of any money payment with knowledge of the breach of any covenant or agreement hereof shall not be deemed a waiver of such breach. In addition to the other remedies provided in this Agreement, the non-defaulting party shall be entitled to restrain by injunction the violation or attempted or threatened violation by the defaulting party of any of the covenants, conditions or provisions of this Agreement or to a decree compelling specific performance of any of such covenants, conditions or provisions.

## **ARTICLE XII**

**INTERLOCAL PROVISIONS**

12.1 In satisfaction of the Interlocal Cooperation Act and in connection with this Agreement, the Parties agree as follows:

- a. This Agreement shall be approved by each Party pursuant to § 11-13-202.5;
- b. This Agreement shall be reviewed as to the proper form and compliance with applicable law by an authorized attorney on behalf of each Party pursuant to § 11-13-202.5;
- c. An executed original counterpart shall be filed with the keeper of records for each Party pursuant to § 11-13-209;
- d. The term of this Agreement does not exceed fifty (50) years pursuant to § 11-13-216 of the Interlocal Act;
- e. No separate legal entity is created by the terms of this Agreement. No real or personal property shall be acquired jointly by the Parties as a result of this Agreement. To the extent that a Party acquires, holds, or disposes of any real or personal property for use in the joint or cooperative undertaking contemplated by this Agreement, such Party shall do so in the same manner that it deals with other property of such Party; and
- f. Except as otherwise specifically provided herein, each Party shall be responsible for its own costs of any action taken pursuant to this Agreement, and for any financing of such costs.

IN WITNESS WHEREOF, the Parties hereto have caused this Agreement to be executed as of the date this Agreement is signed by both Parties.

APPROVED AS TO FORM:

By:   
 Tim Merrill  
 Assistant Attorney General

ATTEST AND COUNTERSIGN:

**UTAH TRANSIT AUTHORITY**

By: \_\_\_\_\_  
 Jay Fox  
 Executive Director

By: \_\_\_\_\_  
 Jon Larsen  
 Chief of Capital Services

By: \_\_\_\_\_  
 David Osborn  
 Acting Director of Capital Design & Construction

**SALT LAKE CITY CORPORATION**

By: \_\_\_\_\_  
City Recorder

By: \_\_\_\_\_  
Erin Mendenhall  
Mayor

APPROVED AS TO FORM

By: \_\_\_\_\_  
Allison Parks  
Deputy City Attorney

**SALT LAKE CITY COMMUNITY  
REINVESTMENT AGENCY**

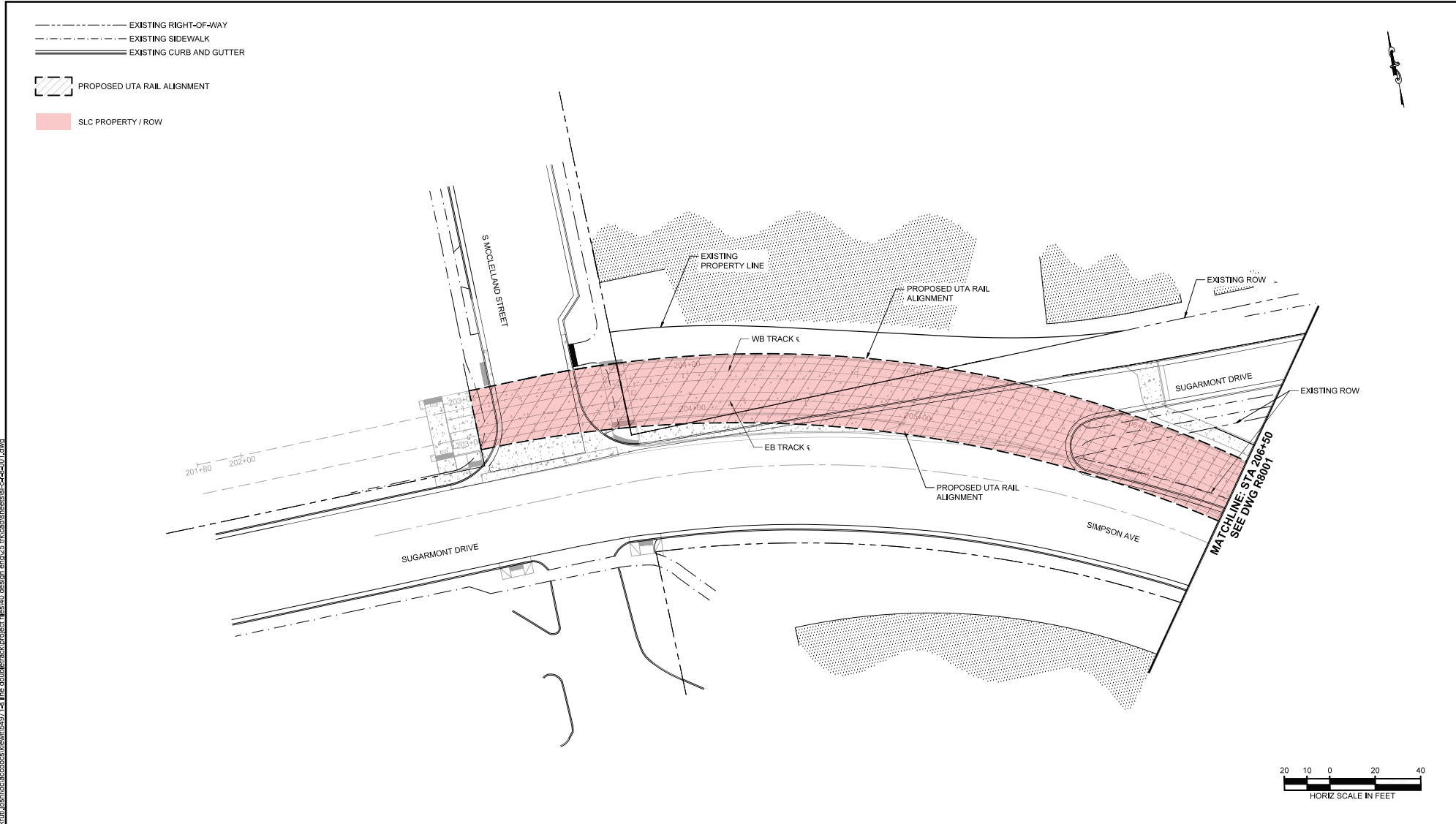
By: \_\_\_\_\_  
City Recorder

By: \_\_\_\_\_  
Erin Mendenhall  
Executive Director

APPROVED AS TO FORM

By: \_\_\_\_\_  
Jennifer Huntsman

**EXHIBIT A – Licensed Property Map**



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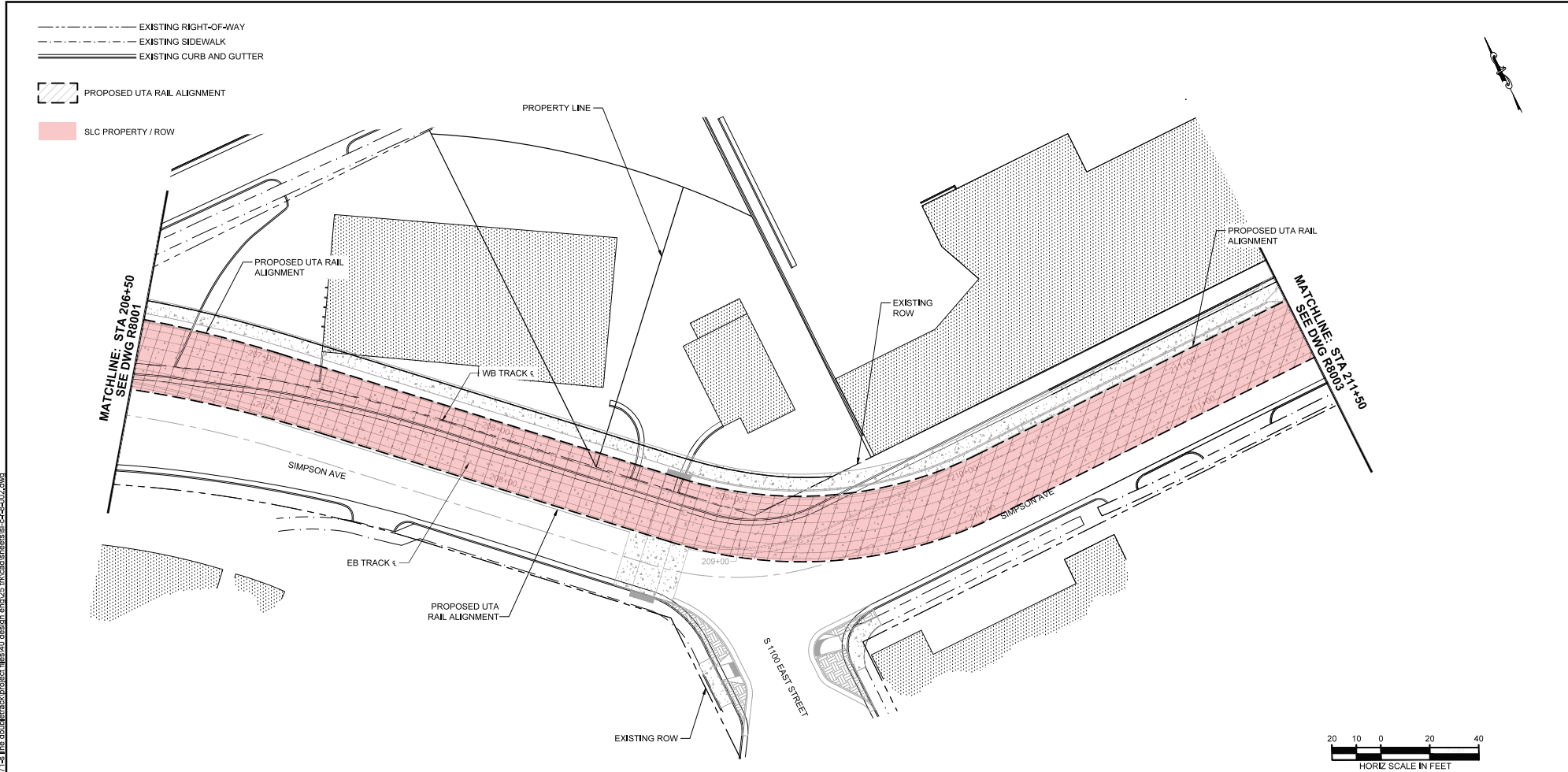
REVISION	DATE	DESCRIPTION



Designed By	S. JOSHI
Drawn By	S. JOSHI
Checked By	F. NANNENGA
Approved By	F. NANNENGA

**S-LINE EXTENSION PROJECT**  
  
**SITE PLAN**  
**WB STA 201+80.27 TO WB STA 206+50**

Scale	1" = 20'
CADD Filename	SLC-R-001.dwg
Submitted Date	03-12-26
UTA Contract No.	24-03849
Drawing No.	R8001
Sheet No.	



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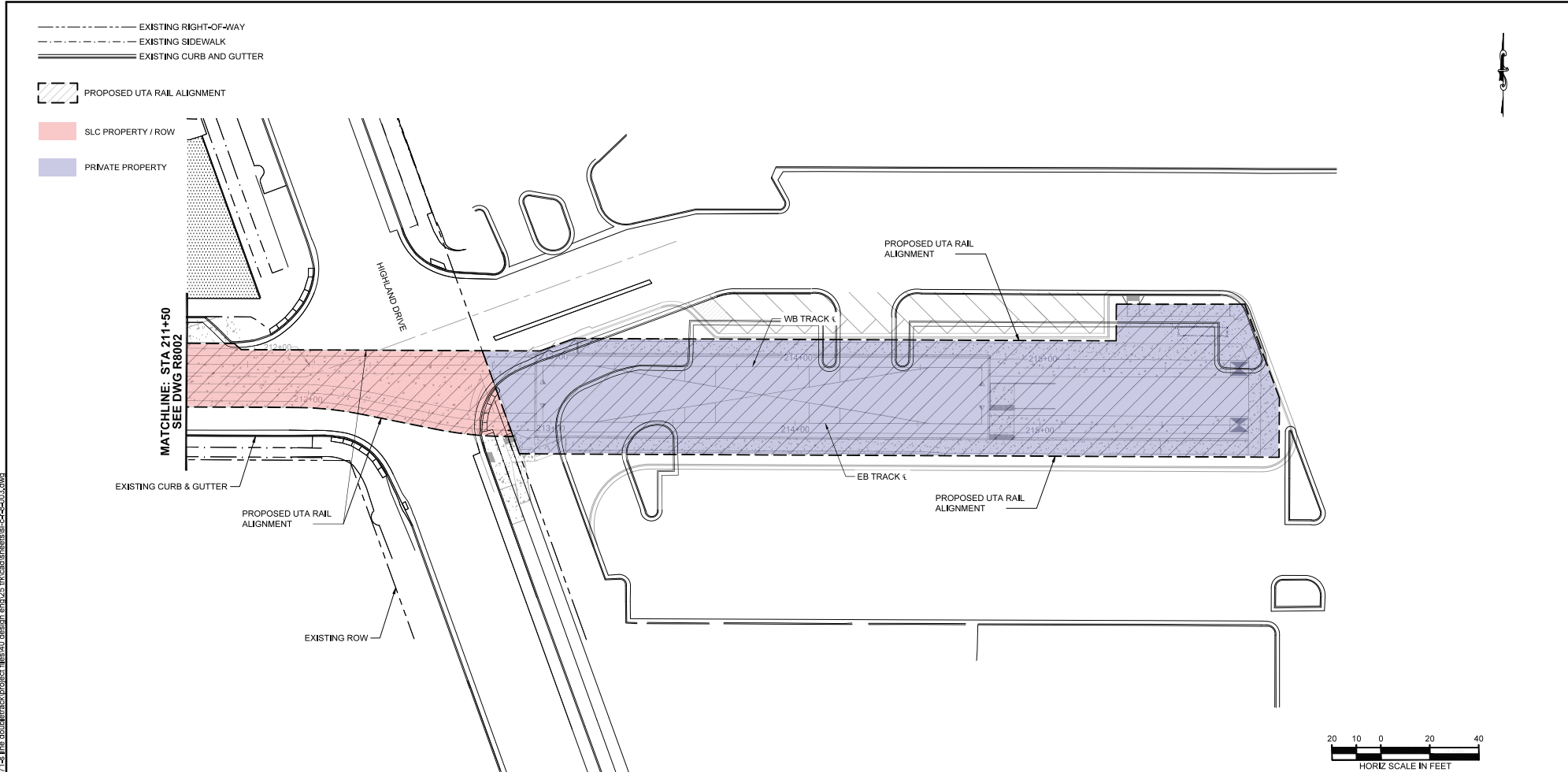
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Designed By	S. JOSHI
Drawn By	S. JOSHI
Checked By	F. NANNENGA
Approved By	F. NANNENGA

S-LINE EXTENSION PROJECT  
  
SITE PLAN  
WB STA 206+50 TO WB STA 211+50

Scale	1" = 20'
CADD Filename	SL-C-R-002.dwg
Submitted Date	03-12-26
UTA Contract No.	24-03849
Drawing No.	R8002
Sheet No.	



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Designed By	S. JOSHI
Drawn By	S. JOSHI
Checked By	F. NANNENGA
Approved By	F. NANNENGA

S-LINE EXTENSION PROJECT  
  
 SITE PLAN  
 WB STA 211+50 TO WB STA 215+84.18

Scale:	1" = 20'
CADD Filename:	SLC-R-8-003.dwg
Submitted Date:	03-12-26
UTA Contract No.:	24-03849
Drawing No.:	R8003
Sheet No.:	

**EXHIBIT B – Design Drawings**

# S-LINE EXTENSION PROJECT

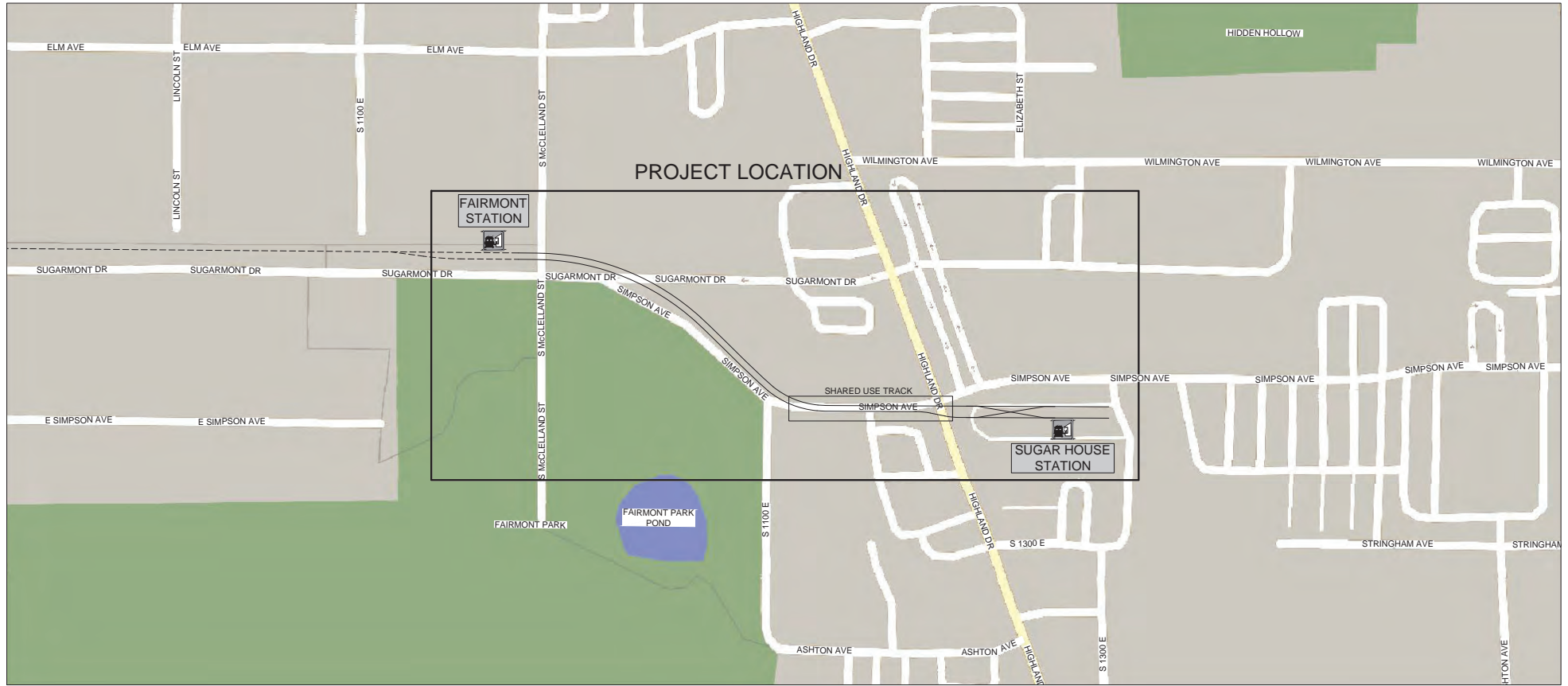
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**Not for Construction**

UTA CONTRACT NO. 24-03849

JANUARY 16, 2026

## 100% DESIGN SUBMITTAL - CIVIL PACKAGE



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1/16/2026

REV	DATE	Description



Designed By:	S. JOSHI
Drawn By:	S. JOSHI
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT	
GENERAL	
COVER SHEET	

State:	NTS
CADD Filename:	SL-C-G-0-000.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	G0000
Sheet No.:	

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SHEET NO.	DWG NO.	TITLE	DESCRIPTION	SHEET NO.	DWG NO.	TITLE	DESCRIPTION
<b>GENERAL</b>				<b>CIVIL STANDARD DRAWINGS</b>			
	G0000	COVER SHEET			C1006	HIGHLAND DR	PLAN AND PROFILE STA 10+00 TO STA 11+63
	G0001	INDEX OF DRAWINGS	SHEET 1 OF 3		C1007	EMERGENCY DRIVEWAY	PLAN AND PROFILE STA 0+00 TO STA 3+50
	G0002	INDEX OF DRAWINGS	SHEET 2 OF 3		C1101	SIMPSON CURB AND GUTTER	PLAN AND PROFILE STA 0+00 TO STA 3+50
	G0003	INDEX OF DRAWINGS	SHEET 3 OF 3		C1102	SIMPSON CURB AND GUTTER	PLAN AND PROFILE STA 3+50 TO STA 7+00
	G0010	STANDARD ABBREVIATIONS	SHEET 1 OF 2		C1103	SIMPSON CURB AND GUTTER	PLAN AND PROFILE STA 7+00 TO STA 9+00
	G0011	STANDARD ABBREVIATIONS	SHEET 2 OF 2		C1104	McCLELLAND ST NW CURB AND GUTTER	PLAN AND PROFILE STA 0+00 TO STA 1+00
	G0012	STANDARD SYMBOLS			C1105	McCLELLAND ST NE CURB AND GUTTER	PLAN AND PROFILE STA 0+00 TO STA 1+00
	G0013	GENERAL NOTES			C1106	S 11TH E SW CURB AND GUTTER	PLAN AND PROFILE STA 0+00 TO STA 1+00
	K0001	TRACK KEY MAP			C1107	S 11TH E SE CURB & GUTTER	PLAN AND PROFILE STA 0+00 TO STA 1+00
	R0201	SURVEY CONTROL DATA			C1108	HIGHLAND SE CURB AND GUTTER	PLAN AND PROFILE STA 0+00 TO STA 4+50
<b>TRACK TYPICAL SECTIONS</b>					C1109	PARKING LOT CURB AND GUTTER	PLAN AND PROFILE STA 0+00 TO STA 3+50
	K4201	TRACK TYPICAL SECTIONS	SHEET 1 OF 4		C1201	SIMPSON SIDEWALK	PLAN AND PROFILE STA 50+12 TO STA 54+00
	K4202	TRACK TYPICAL SECTIONS	SHEET 2 OF 4		C1202	SIMPSON SIDEWALK	PLAN AND PROFILE STA 54+00 TO STA 57+25
	K4203	TRACK TYPICAL SECTIONS	SHEET 3 OF 4		C3001	ROADWAY	CURB RETURN DETAILS
	K4204	TRACK TYPICAL SECTIONS	SHEET 4 OF 4		C3002	ROADWAY	CURB RETURN DETAILS
<b>TRACK ALIGNMENT DATA</b>					C3003	ROADWAY	CURB RETURN DETAILS
	K6201	HORIZONTAL ALIGNMENT DATA			C3004	ROADWAY	CURB RETURN DETAILS
<b>TRACK PLAN AND PROFILE</b>					C3005	ROADWAY	CURB RETURN DETAILS
	K1201	TRACK PLAN AND PROFILE	WB STA 201+80.27 TO WB STA 206+50		C3006	SUGAR HOUSE PLATFORM AREA	PEDESTRIAN RAMP AND SIDEWALK PLAN
	K1202	TRACK PLAN AND PROFILE	WB STA 206+50 TO WB STA 211+50		C3007	ROADWAY	EMERGENCY DRIVEWAY GRADING
	K1203	TRACK PLAN AND PROFILE	WB STA 211+50 TO WB STA 215+84.18				
<b>TRACK &amp; ROADWAY CROSS SECTIONS</b>					C3010	CIVIL	BOLLARD AND SWING GATE DETAILS
	K5201	TRACK CROSS SECTIONS	WB STA 201+80.27 TO WB STA 204+50		C3011	CIVIL	CURB TRANSITIONS AND DETAILS
	K5202	TRACK CROSS SECTIONS	WB STA 205+00 TO WB STA 206+50		C3013	CIVIL	GATE DETAILS
	K5203	TRACK CROSS SECTIONS	WB STA 207+00 TO WB STA 208+50		C3014	CIVIL	PEDESTRIAN SAFETY RAILING
	K5204	TRACK CROSS SECTIONS	WB STA 209+00 TO WB STA 210+50				
	K5205	TRACK CROSS SECTIONS	WB STA 211+00 TO WB STA 212+50		D1201	DRAINAGE PLAN	WB STA 201+80.27 TO WB STA 206+50
	K5206	TRACK CROSS SECTIONS	WB STA 213+00 TO WB STA 214+50		D1202	DRAINAGE PLAN	WB STA 206+50 TO WB STA 211+50
	K5207	TRACK CROSS SECTIONS	WB STA 215+00 TO WB STA 215+84		D1203	DRAINAGE PLAN	WB STA 211+50 TO WB STA 215+84.18
<b>TRACK CHART</b>							
	K7001	TRACK CHART	WB STA 202+92.17 TO WB STA 215+84.18				
<b>TRACKWORK STANDARD DRAWINGS</b>							
	K3001	TRACKWORK	RAIL LUBRICATOR		T1201	TRAFFIC SIGNAL PLANS	WB STA 201+60 TO WB STA 206+42
	K3002	TRACKWORK	TRACK SLAB TYPICAL SECTION		T1202	TRAFFIC SIGNAL PLANS	WB STA 206+42 TO WB STA 211+50
	K3003	TRACKWORK	TRACK SLAB DETAILS		T1203	TRAFFIC SIGNAL PLANS	WB STA 211+50 TO WB STA 215+85
	K3004	TRACKWORK	MISC DETAILS		T1204	STRIPING & SIGNING PLANS	WB STA 201+60 TO WB STA 206+42
	K3005	TRACKWORK	SIGNAL TRACK CONNECTION BOX		T1205	STRIPING & SIGNING PLANS	WB STA 206+42 TO WB STA 211+50
	K3006	TRACKWORK	STREETCAR DYNAMIC ENVELOPE AND LOADING DIAGRAM		T1206	STRIPING & SIGNING PLANS	WB STA 211+50 TO WB STA 215+85
	K3007	TRACKWORK	MISC. EMBEDDED TRACK SECTIONS AND DETAILS		T3201	MAST ARM DETAIL	
	K3008	TRACKWORK	6" INFILL SLAB DETAILS		T3202	MAST ARM POLE DETAIL	
	K3009	TRACKWORK	TRACK DRAINAGE DETAILS		T3203	MAST ARM POLE FOUNDATION DETAILS	
	K3010	TRACKWORK	TRACK CONNECTION BOX		T3204	MAST ARM POLE POLE EXTENSION	FOR LUMINAIRE DETAIL
	K3011	TRACKWORK	DRAINAGE CATCH BASIN		T3205	SIGNAL HEAD FOR VEHICLES DETAIL	
	K3012	TRACKWORK	TRACK DRAIN ARRAY AND PLATFORM DRAIN DETAILS		T3206	SIGNAL HEAD MOUNTING DETAIL	
	K3013	TRACKWORK	INSULATED JOINT DETAILS		T3207	SIGNAL POLE BREAK AWAY TYPE DETAIL	
	K3014	TRACKWORK	NO. 6 DOUBLE CROSSOVER RAIL LAYOUT		T3208	SIGNAL HEAD FOR PEDESTRIANS DETAIL	
	K3015	TRACKWORK	NO. 6 DOUBLE CROSSOVER GEOMETRY AND LAYOUT 23'-0" TRACK CENTERS		T3209	SIGNAL PUSH BUTTON DETAIL	
	K3016	TRACKWORK	NO. 6 DIRECT FIXATION DOUBLE CROSSOVER LAYOUT		T3210	SIGNAL CONTROLLER FOUNDATION DETAIL	
	K3017	TRACKWORK	NO. 6 DIRECT FIXATION DOUBLE CROSSOVER BLOCKOUT DETAILS		T3211	JUNCTION BOX AND BURIED RACEWAY DETAIL	
	K3018	TRACKWORK	SPECIAL TRACKWORK M-23A SWITCH MACHINE LAYOUT		T3212	UNDERGROUND SERVICE PEDESTAL DETAIL	
	K3019	TRACKWORK	DF FASTENER DETAIL AND SECTION		T3213	PEDESTRIAN BLANK OUT SIGN AND RAISED	PAVEMENT MARKER DETAILS
	K3020	TRACKWORK	DIRECT FIXATION ON SLAB DETAILS		T7001	SIGNAL CIRCUIT DIAGRAM	S MCCLELLAND STREET
<b>DEMOLITION PLANS</b>					T7002	SIGNAL CIRCUIT DIAGRAM	1100 EAST STREET
	C4001	DEMOLITION PLAN	WB STA 201+80.27 TO WB STA 206+50		T7003	SIGNAL CIRCUIT DIAGRAM	HIGHLAND DRIVE
	C4002	DEMOLITION PLAN	WB STA 206+50 TO WB STA 211+50				
	C4003	DEMOLITION PLAN	WB STA 211+50 TO WB STA 215+84.18				
<b>CIVIL AND ROADWAY DETAILS</b>							
	C1001	SIMPSON PLAN AND PROFILE	STA 100+00 TO STA 104+00		P0201	STATION	STRUCTURAL SYMBOLS, ABBREVIATIONS, AND NOTES
	C1002	SIMPSON PLAN AND PROFILE	STA 104+00 TO STA 108+00		P1201	SUGAR HOUSE	PLATFORM PLAN
	C1003	SIMPSON PLAN AND PROFILE	STA 108+00 TO STA 111+00		P3201	STATION	TYPICAL STRUCTURAL DETAILS
	C1004	McCLELLAND ST	PLAN AND PROFILE STA 10+00 TO STA 10+88		P3202	STATION	STRUCTURAL DRAWINGS 20'-0" DOUBLE CANOPY
	C1005	S 11TH E PLAN AND PROFILE	STA 10+00 TO STA 11+00		P3203	STATION	SUGAR HOUSE STATION PLATFORM ELEVATION
					P3204	STATION	SUGAR HOUSE STATION PLATFORM ELEVATION & SECTION
					P3205	STATION	20 FOOT DOUBLE CANOPY ELEVATIONS
					P3206	STATION	20 FOOT DOUBLE CANOPY SECTION
					P3207	STATION	20 FOOT STANDARD CANOPY FLOOR PLAN
					P3208	STATION	DRILLED SHAFT FOUNDATION CANOPY DETAILS

REV	DATE	Description





Submitted By: \_\_\_\_\_



Approved By: \_\_\_\_\_

**S-LINE EXTENSION PROJECT**

**GENERAL**

**INDEX OF DRAWINGS**

**SHEET 1 OF 3**

Title: NTS CADD Filename: SL-C-G-0-001.dwg Submitted Date: 01-16-26 UTA Contract No.: 24-03849 Drawing No.: G0001 Sheet No.:	
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SHEET NO.	DWG NO.	TITLE	DESCRIPTION	SHEET NO.	DWG NO.	TITLE	DESCRIPTION
	P3209	STATION	20 FOOT DOUBLE CANOPY DETAILS		Y7010	OVERHEAD CONTACT SYSTEM	TECHNICAL SHEET SCAT VERTICAL, WIND & RADIAL LOADS
	P3210	STATION	STRUCTURAL DRAWINGS CANOPY DETAILS		Y7011	OCS TECHNICAL SHEET	PAINTOGRAPH SECURITY AND MAXIMUM STAGGERS FOR BALLASTLESS TRACK
	P3211	STATION	20 FOOT DOUBLE CANOPY REFLECTED CEILING & ROOF PLANS		Y7012	OVERHEAD CONTACT SYSTEM	TECHNICAL SHEET HANGER LENGTH TABLE
	P3212	STATION	MISCELLANEOUS PLATFORM DETAILS		Y7013	OVERHEAD CONTACT SYSTEM	TECHNICAL SHEET INSTALLATION TENSIONS AND SAGS
	P3213	STATION	MECHANICAL DETAILS		Y7014	OVERHEAD CONTACT SYSTEM	TECHNICAL SHEET TRACK MOVEMENT & STAGGER CHANGE
	P3214	STATION	STANDARD PLATFORM AND SNOWMELT DETAILS		Y7015	OVERHEAD CONTACT SYSTEM	CONDUCTOR PARTICULARS
	P3215	STATION	SNOWMELT DETAILS		Y7016	OCS VERTICAL	ELECTRICAL CLEARANCE REQUIREMENTS
	P3216	FREEZELESS POST HYDRANT	DETAIL		Y7100	OVERHEAD CONTACT SYSTEM	MASTER OVERLAP CHART AND SECTIONING DIAGRAM
	P3217	GRAPHIC SIGN STANDARDS	STANDARD FONTS		Y7180	OVERHEAD CONTACT SYSTEM	TYPICAL STANDARD SPANS
	P3218	GRAPHIC SIGN STANDARDS	TYPE 4 - RAIL PLATFORM SIGN DETAILS		Y7182	OVERHEAD CONTACT SYSTEM	TYPICAL ANCHOR SPANS
	P3219	GRAPHIC SIGN STANDARDS	TYPE 2B STATION IDENTIFICATION SIGN DETAILS		Y7183	OCS DIAMOND CROSSOVER	
	P3220	GRAPHIC SIGN STANDARDS	PAVEMENT MARKINGS AND PRISM		Y7190	OVERHEAD CONTACT SYSTEM	TYPICAL CENTER POLE STRUCTURES
	P4201	STATION	FOUNDATION PLAN AND SECTIONS		Y7191	OVERHEAD CONTACT SYSTEM	TYPICAL SURGE ARRESTOR STRUCTURE
	P4202	STATION	RAMP FOUNDATION SECTIONS		Y7192	OVERHEAD CONTACT SYSTEM	BYPASS DISCONNECT SWITCH STRUCTURE
<b>PLATFORM ELECTRICAL</b>					Y7194	OVERHEAD CONTACT SYSTEM	TYPICAL HEADSPAN STRUCTURES
	E0202	PLATFORM ELECTRICAL	LEGENDS, NOTES AND ABBREVIATIONS		Y7201	TAPERED TUBULAR STEEL POLE	ASSEMBLIES, TYPE PA, PB, PC, PD, & PE
	E0208	SUGAR HOUSE STATION PLATFORM	JUNCTION BOX AND TRENCH DETAILS		Y7202	OVERHEAD CONTACT SYSTEM	OCS POLE ID NUMBER ASSEMBLY SD-01 AND SCHEDULE
	E0209	PLATFORM ELECTRICAL	SUGAR HOUSE STATION PLATFORM TVM BASE, CALL BOX, AND CARD READER DETAILS		Y7203	OVERHEAD CONTACT SYSTEM	
	E0210	PLATFORM ELECTRICAL	PLATFORM LIGHT INSTALLATION AND POLE FOUNDATION DETAILS		Y7207	OCS POLE BRACKET ASSEMBLIES	FOR TUBULAR POLES TYPES BC-XX AND TB-02-XX
	E1205	SUGAR HOUSE STATION PLATFORM	ELECTRICAL PLAN		Y7210	OVERHEAD CONTACT SYSTEM	HEADSPAN ASSEMBLIES HS-1, HS-1A, HS-2, AND HS-2AX
	E3207	SUGAR HOUSE STATION PLATFORM	PCC DETAILS		Y7211	OVERHEAD CONTACT SYSTEM	CANTILEVER ASSEMBLY PULL-OFF TYPES CA-01L, CA-01M AND CA-01H
	E7203	SUGAR HOUSE STATION PLATFORM	ONE-LINE AND LIGHTING CONTROL DIAGRAMS		Y7212	OVERHEAD CONTACT SYSTEM	CONFIGURATION AND LOADING DIAGRAM TYPES CA-01L, CA-01M, AND CA-01H
	E8204	PLATFORM ELECTRICAL	SUGAR HOUSE STATION PLATFORM PANEL SCHEDULE		Y7213	OVERHEAD CONTACT SYSTEM	CANTILEVER ASSEMBLY PULL-OFF CA-03L, CA-03M, AND CA-03H
<b>RETAINING WALL PLAN AND PROFILE</b>					Y7214	OVERHEAD CONTACT SYSTEM	CONFIGURATION AND LOADING DIAGRAM TYPES CA-03L, CA-03M, AND CA-03H
	W0001	GENERAL NOTES, STANDARD SYMBOLS	AND ABBREVIATIONS		Y7215	OVERHEAD CONTACT SYSTEM	CANTILEVER ASSEMBLY OUT-OF-RUNNING TYPES CA-05AM, AH AND CA-05BM, BH
	W1201	RETAINING WALL 1	SITUATION AND LAYOUT		Y7216	OVERHEAD CONTACT SYSTEM	CONFIGURATION AND LOADING DIAGRAM TYPES CA-05AM, AH AND CA-05BM, BH
	W1202	RETAINING WALL 2	SITUATION AND LAYOUT		Y7220	OVERHEAD CONTACT SYSTEM	TWO TRACK PULL-OFF PO-2 AND PO-2A
	W3001	WALL DETAILS			Y7221	OVERHEAD CONTACT SYSTEM	CATENARY BRIDLE ASSEMBLY PULL-OFF TYPE BB-02D
<b>UTILITIES</b>					Y7222	OVERHEAD CONTACT SYSTEM	CONFIGURATION AND LOADING DIAGRAM TYPE BB-02D
	U0100	UTILITIES	GENERAL NOTES		Y7223	OVERHEAD CONTACT SYSTEM	SECTION INSULATOR ASSEMBLY TYPE MLI-SI-01
	U1201	EXISTING UTILITY PLAN	WB STA 201+80.27 TO WB STA 206+50		Y7224	OVERHEAD CONTACT SYSTEM	FIXED TERMINATION ASSEMBLY TYPE FT-01
	U1202	EXISTING UTILITY PLAN	WB STA 206+50 TO WB STA 211+50		Y7225	OVERHEAD CONTACT SYSTEM	CATENARY BRIDLE TERMINATION ASSEMBLIES BT-01, BT-02 & BT-03
	U1203	EXISTING UTILITY PLAN	WB STA 211+50 TO WB STA 215+84.18		Y7226	OVERHEAD CONTACT SYSTEM	MIDPOINT ANCHOR ASSEMBLY TYPE MP-01
	U1204	EXISTING UTILITY PLAN	SUGARMONT AVENUE AND CRA PROPERTIES		Y7227	OVERHEAD CONTACT SYSTEM	BALANCE WEIGHT FOR TUBULAR POLE TYPE BW-04
	U1205	EXISTING UTILITY MATRIX	SHEET 1 OF 4		Y7228	OVERHEAD CONTACT SYSTEM	SPRING TENSIONER
	U1206	EXISTING UTILITY MATRIX	SHEET 2 OF 4		Y7231	OVERHEAD CONTACT SYSTEM	FEEDER CONNECTION ASSEMBLY TYPE FR-04
	U1207	EXISTING UTILITY MATRIX	SHEET 3 OF 4		Y7233	OVERHEAD CONTACT SYSTEM	JUMPER ASSEMBLIES TYPES JF-1, JP-1 AND JS-1
	U1208	EXISTING UTILITY MATRIX	SHEET 4 OF 4		Y7235	OVERHEAD CONTACT SYSTEM	HANGER ASSEMBLIES TYPES HA-1 THROUGH HA-7
	U1209	EXISTING UTILITY	TEST HOLE DATA		Y7236	OCS CUT-IN INSULATOR AND	CONTACT BRIDGE ASSEMBLIES TYPE CI-01, CI-02, CC-01
	U1210	UTILITY RELOCATION PLAN	STA 203+00 TO STA 205+00		Y7237	OVERHEAD CONTACT SYSTEM	MULTI-CANTILEVER BRACKETS
	U1211	UTILITY RELOCATION PLAN	STA 205+00 TO STA 208+00		Y7303	OVERHEAD CONTACT SYSTEM	WIRING LAYOUT & ALLOCATION SCHEDULE WB STA 201+00 TO WB STA 207+00
	U1212	UTILITY RELOCATION PLAN	STA 208+00 TO STA 210+00		Y7304	OVERHEAD CONTACT SYSTEM	WIRING LAYOUT & ALLOCATION SCHEDULE WB STA 207+00 TO WB STA 211+17
	U1213	UTILITY RELOCATION PLAN	STA 210+00 TO STA 212+00		Y7305	OVERHEAD CONTACT SYSTEM	WIRING LAYOUT & ALLOCATION SCHEDULE WB STA 212+00 TO WB STA 215+96.33
	U1214	UTILITY RELOCATION PLAN	STA 212+00 TO STA 215+84.18				
	U1220	PROPOSED SEWER	HIGHLAND DRIVE SEWER BY-PASS		Y0800	COMMUNICATIONS	SYMBOLS, ABBREVIATIONS AND GENERAL NOTES
	U1221	PROPOSED GAS LINE	SUGAR HOUSE STATION		Y1801	COMMUNICATIONS	SYSTEMWIDE SINGLE LINE DIAGRAM
	U1222	PROPOSED WATER LINE	HIGHLAND DRIVE STATION		Y1802	COMMUNICATIONS	SIGNAL FIBER OPTIC CABLE PLAN
	U1225	PROPOSED GAS LINE PLAN	SIMPSON AVENUE		Y3803	COMMUNICATIONS	SIGNAL BETA INTERLOCKING FIBER ALLOCATION TABLE
	U1226	PROPOSED WATER LINE PLAN	HIGHLAND DRIVE		Y3804	COMMUNICATIONS	SIGNAL HIGHLAND DRIVE FIBER ALLOCATION TABLE
	U1227	PROPOSED WATER LINE PROFILE	HIGHLAND DRIVE		Y1810	COMMUNICATIONS	IT FIBER OPTIC CABLE PLAN
	U1231	PROPOSED WATER LINE PLAN	SIMPSON AVENUE		Y3811	COMMUNICATIONS	FAIRMONT PCC IT FIBER ALLOCATION TABLE
	U1232	PROPOSED WATER LINE PROFILE	SIMPSON AVENUE		Y3812	COMMUNICATIONS	SUGAR HOUSE PCC IT FIBER ALLOCATION TABLE
	U1233	WATERLINE	CATHODIC PROTECTION		Y8813	COMMUNICATIONS	SUGAR HOUSE STATION RISER DIAGRAM
	U1234	CATHODIC PROTECTION	DETAILS 1 OF 2		Y8814	SUGAR HOUSE STATION	PCC COMM CABINET RACK ELEVATION
	U1235	CATHODIC PROTECTION	DETAILS 2 OF 2		Y3815	COMMUNICATIONS	SUGAR HOUSE STATION CABLE SCHEDULE
<b>OVERHEAD CONTACT SYSTEM</b>							
	Y0001	OVERHEAD CONTACT SYSTEM	ABBREVIATIONS AND GENERAL NOTES		Y1235	DUCTBANK PLAN	WB STA 201+00 TO WB STA 207+00
	Y0002	OVERHEAD CONTACT SYSTEM	STANDARD SYMBOLS AND STRUCTURAL NOTES		Y1236	DUCTBANK PLAN	WB STA 207+00 TO WB STA 212+00
	Y3002	OCS POLE FOUNDATIONS			Y1237	DUCTBANK PLAN	WB STA 212+00 TO WB STA 215+88.5
	Y7005	OCS TECHNICAL SHEET	UNIVERSAL PANTOGRAPH CLEARANCE ENVELOPE		Y1238	ELECTRICAL PLAN	STA 201+00 TO STA 207+00
	Y7006	OVERHEAD CONTACT SYSTEM	DRAWING HIERARCHY AND CONTENT REQUIREMENTS		Y1239	ELECTRICAL PLAN	STA 207+00 TO STA 212+00
	Y7009	OCS TECHNICAL SHEET	MAX PERMISSIBLE MIDSPAN OFFSET, BLOW-OFF AND CATENARY DROOP		Y1240	ELECTRICAL PLAN	WB STA 212+00 TO WB STA 215+85.29

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REV	DATE	Description



Designed By:	T. COVEYOU
Drawn By:	T. COVEYOU
Checked By:	D. TREADWELL
Approved By:	A. DIGIROLAMO

**S-LINE EXTENSION PROJECT**  
  
**GENERAL**  
  
**INDEX OF DRAWINGS**  
**SHEET 2 OF 3**

Scale:	NTS
CADD Filename:	SL-C-G-0-002.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Sheet No.:	G0002

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SHEET NO.	DWG NO.	TITLE	DESCRIPTION	SHEET NO.	DWG NO.	TITLE	DESCRIPTION
	Y3215	SYSTEMWIDE ELECTRICAL	PULLBOXES DETAILS AND SCHEDULE				
	Y3216	SYSTEMWIDE RACEWAY	DETAILS AND SCHEDULE				
	Y4217	SYSTEMWIDE ELECTRICAL	RACEWAY SECTIONS SHEET-1				
	Y4218	SYSTEMWIDE ELECTRICAL	RACEWAY SECTIONS SHEET-2				
	Y7314	TRACTION POWER SYSTEM	RACEWAY SECTIONS				
	Y1241	RMP UTILITY RELOCATION PLAN					
	Y1242	STREET LIGHTING PLAN					
	Y1243	TRENCH SECTION					
<b>SIGNALS</b>							
	Z0001	SIGNALS SYMBOLS					
	Z0002	SIGNALS ABBREVIATIONS					
	Z7001	SIGNALS SYSTEM	OVERALL SCHEMATIC WB STA 196+00 TO WB STA 207+65				
	Z7002	SIGNALS SYSTEM	OVERALL SCHEMATIC WB STA 207+65 TO WB STA 216+00				
	Z7003	EASTBOUND CONTROL LINE PLAN	STA. 186+00 TO STA. 203+00				
	Z7004	EASTBOUND CONTROL LINE PLAN	STA. 203+00 TO STA. 215+85				
	Z7005	WESTBOUND CONTROL LINE PLAN	STA. 186+00 TO STA. 203+00				
	Z7006	WESTBOUND CONTROL LINE PLAN	STA. 203+00 TO STA. 215+85				
	Z7007	SIGNALS	TRACK CIRCUIT ARRANGEMENT				
	Z7008	SIGNALS	ROUTE AND ASPECT CHART BETA INTERLOCKING				
	Z7009	SIGNALS	ROUTE AND ASPECT CHART HIGHLAND DRIVE INTERLOCKING				
	Z8001	SIGNALS TRACK AND CABLE PLAN	1 OF 2				
	Z8002	SIGNALS TRACK AND CABLE PLAN	2 OF 2				
<b>SIGNALS STANDARD DRAWINGS</b>							
	Z3001	SIGNALS WAYSIDE	SIGNALING DETAILS 1 OF 2				
	Z3002	SIGNALS WAYSIDE	SIGNALING DETAILS 2 OF 2				
	Z3003	SIGNALS	SWITCH HEATER LAYOUT				
	Z3004	SIGNAL TYPICAL BONDING DETAILS					
	Z3005	SIGNALS	HOUSE INSTALLATION DETAILS				
	Z3006	SIGNALS	TYPICAL WHEEL SENSOR INSTALLATION				
	Z3007	SIGNALS	TYPICAL WHEEL SENSOR EMBEDDED TRACK INSTALLATION				
	Z3008	SIGNALS	TYPICAL VEHICLE DETECTOR LOOP INSTALLATION				
	Z3009	SIGNALS	PEDESTRIAN CROSSING BLANK OUT SIGN				

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REVISION	DATE	DESCRIPTION	



Designed By:	T. COVEYOU
Drawn By:	T. COVEYOU
Checked By:	D. TREADWELL
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
 GENERAL  
 INDEX OF DRAWINGS  
 SHEET 3 OF 3

Scale:	NTS
CADD Filename:	SL-C-G-0-003.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	G0003
Sheet No.:	

STANDARD ABBREVIATIONS

TRACK AND ROAD ALIGNMENT

HORIZONTAL

Table listing abbreviations for track and road alignment, including CS (Point of Change from Circular Curve to Spiral), D, Dc (Degree of Curve), and various curve and alignment terms.

VERTICAL

Table listing abbreviations for vertical alignment, including K (Rate of Vertical Curvature), L (Length of Vertical Curve), and P (Point of Intersection).

GENERAL ABBREVIATIONS

Large table listing general abbreviations such as AASHTO (American Association of State Highway & Transportation Officials), DET (Detention Basin), and HP (High Pressure or Heel Plate).

Vertical text on the left margin: tkcar00\_covet\_inhdtsp-0-010.dwg

Revision table with columns: REV, DATE, Description. Includes revision symbols.



Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

Designed By: T. COVEYOU  
Drawn By: T. COVEYOU  
Checked By: A. DIGIROLAMO  
Approved By: A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
GENERAL  
STANDARD ABBREVIATIONS  
SHEET 1 OF 2

Metadata table with fields: State: NTS, CADD Filename: SL-C-G-010.dwg, Submission Date: 01-16-26, UTA Contract No.: 24-03849, Drawing No.: G0010, Sheet No.:



### STANDARD SYMBOLS

EXISTING		PROPOSED		EXISTING		PROPOSED		EXISTING		PROPOSED	
	RETAINING WALL				EXISTING TRACK				AREA LIGHT		
	DRAINAGE DITCH OR SWALE				PROPOSED UTA TRACK				STREET LIGHT		
	CULVERT				ABANDONED WATER				STREET LIGHT		
	RIGHT-OF-WAY LINE				SEWER				LIGHT VAULT: E-ELECTRIC, T-TELEPHONE, W-WATER		
	EASEMENT LINE				STORM DRAIN				TREE		
	CITY BOUNDARY				WATER				VEGETATION		
	BUILDING LINE				UNDERGROUND POWER				EXISTING GROUND (SECTIONS) RR SWITCH		
	FENCE				GAS				BUSH		
	CURB AND GUTTER				FIBER OPTIC				GAS PIPELINE MARKER		
	CURB/WALL				OVERHEAD POWER				MARSH		
	BARRIER				TELEPHONE				PERMANENT MONUMENT		
	SPOT ELEVATION (PHOTOGRAMMETRIC)				OVERHEAD FIBER OPTIC				SECTION CORNER		
	EDGE OF PAVEMENT				CABLE T.V. BOX				STATION PLATFORM		
	ACCESS ROAD				ELECTRIC BOX				PLATFORM WALKWAY		
	TOP OF CUT				GAS METER				IMPACTED BUILDING, ROADWAY, PARKING LOT		
	TOP OF FILL				STREET LIGHT BOX				FUTURE ROAD CROSSING		
	BUMPING POST				WATER METER				ROAD CROSSING		
	HIGH POINT				TRAFFIC SIGNAL BOX				CHECK-IN LOOP		
	LOW POINT				TELEPHONE BOX				CHECK-OUT LOOP		
	FLOW DIRECTION				WATER BOX				LOOP NOT USED		
	AERIAL CONTROL POINT				FIBER OPTIC BOX				MATRIX DETECTION ZONE		
	TRAFFIC SIGNAL MAST ARM AND POLE				UTILITY BOX				PEER CALL (DELAY SHOWN IF ANY)		
	TRAFFIC SIGNAL HEAD				IRRIGATION BOX				TRACK DRAIN		
	UTA SIGNAL HOUSE				CLEANOUT				OCS FOUNDATION		
	COMMUNICATION/SIGNAL BOX				STORM DRAIN MANHOLE						
	LH TURNOUT				ELECTRIC MANHOLE						
	RH TURNOUT				UTILITY MANHOLE						
	SINGLE CROSSOVER				SANITARY SEWER MANHOLE						
	DIAMOND CROSSOVER				INTERCEPTOR MANHOLE						
	POINT OF SWITCH (MANUALLY OPERATED)				TELEPHONE MANHOLE						
	POINT OF SWITCH (POWER OPERATED)				WATER MANHOLE						
	LH DOUBLE POINT SPLIT SWITCH (DPSS) DERAIL				FIBER OPTIC MANHOLE						
	RH DOUBLE POINT SPLIT SWITCH (DPSS) DERAIL				MONITORING WELL						
	RAIL LUBRICATOR INFRASTRUCTURE				WATER VALVE						
	CURVE NUMBER:				GAS TEST VALVE						
	CROSSOVER NUMBER:				FIRE HYDRANT						
	WB TRACK $\epsilon$				STAND PIPE						
	EB TRACK $\epsilon$				DROP INLET						
	CROSSOVERS BY UTA				LARGE SIGN						
					SIGN						
					BOLLARD POST						
					FLAG POLE						
					POWER POLE						
					GUY						

NOTE: SOME SYMBOLS/LINES MAY BE SHOWN ENLARGED FOR CLARITY.

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REV	DATE	Description



Designed By:	T. COVEYOU
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT	
GENERAL	
STANDARD SYMBOLS	
State:	NTS
CADD Filename:	SL-C-G-0-012.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	G0012
Sheet No.:	

GENERAL NOTES - S-LINE EXTENSION

1. ALL MATERIALS AND WORK REQUIRED WITHIN UTA RIGHT-OF-WAY SHALL CONFORM TO UTA SPECIFICATIONS.
2. RIGHT-OF-WAY LINES SHOWN ARE BASED ON RECORDED SURVEY INFORMATION ON RECORD WITH SALT LAKE CITY AND SALT LAKE COUNTY.
3. CONTRACTOR TO VERIFY VERTICAL CLEARANCES FROM THE PROPOSED TRACKS TO OVERHEAD FACILITIES (ELECTRIC, CABLE, TELEPHONE).
4. EXISTING TOPOGRAPHY AND PLANIMETRICS ARE PRODUCED FROM AERIAL SURVEY AND TOPOGRAPHIC FIELD SURVEY. TOPOGRAPHY BELOW VEGETATION OR TREE COVERAGE AND UNDER STRUCTURES IS APPROXIMATE AND REQUIRES CONFIRMATION.
5. "ORIGINAL GROUND" OR "EXISTING GROUND" SHOWN ON PROFILES REFERS TO THE APPROXIMATE EXISTING GROUND LINE AT THE DESIGNATED CENTERLINE OR CONTROL LINE.
6. SEE SURVEY CONTROL DATA SHEETS FOR PROJECT HORIZONTAL CONTROL, VERTICAL DATUM, AND MONUMENT INFORMATION.
7. ALL HORIZONTAL AND VERTICAL DISTANCES ARE IN FEET AND/OR DECIMALS OF A FOOT UNLESS NOTED OTHERWISE.
8. ALL GRADIENTS ARE IN PERCENT UNLESS NOTED OTHERWISE.
9. THE UTA STREETCAR TRACK IS DESIGNED PER THE UTA STREETCAR DESIGN CRITERIA, REVISION, NO. 1 APPROVED SEPTEMBER 2024.
10. STATIONING SHOWN ON TYPICAL SECTIONS IS APPROXIMATE. ALL STATIONING REFERS TO WB TRACK, UNLESS NOTED OTHERWISE.
11. GUIDELINES FROM THE AMERICAN RAILWAY ENGINEERING AND MAINTENANCE-OF-WAY ASSOCIATION'S (AREMA) "MANUAL FOR TRACK ENGINEERING", 2025 EDITION, ARE USED FOR VARIOUS ASPECTS OF THE DESIGN.
12. GUIDELINES FROM THE FHWA "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD), 2023, 11th EDITION, AND UTAH DEPARTMENT OF TRANSPORTATION STANDARD DRAWINGS, LATEST REVISION, ARE USED FOR VARIOUS ASPECTS OF THE DESIGN.
13. UTA TRACK PROFILES REPRESENT TOP OF RAIL PROFILE UNLESS NOTED OTHERWISE.
14. THE RAIL PROFILE IS ALWAYS CARRIED ON THE LOW RAIL THROUGH SUPERELEVATED HORIZONTAL CURVES AND SPIRALS.
15. SUPERELEVATION IS ACCOMPLISHED BY MAINTAINING TOP OF INSIDE (LOW) RAIL AT PROFILE GRADE AND BY RAISING OUTSIDE (HIGH) RAIL AN AMOUNT EQUAL TO THE SUPERELEVATION.
16. UTILITIES SHOWN ON THE PLAN SHEETS ARE BASED ON INFORMATION RECEIVED FROM MUNICIPALITIES, AGENCIES, AND PRIVATE COMPANIES. ALL EXISTING UTILITY LOCATIONS ARE APPROXIMATE AND REQUIRE FIELD VERIFICATION THROUGH POTHOLING AND FIELD SURVEY.
17. TRACKS ON PLAN AND PROFILE DRAWINGS ARE INDICATED BY CENTERLINE OF TRACK. EXCEPT WHERE INDICATED, OFFSET DISTANCES FROM TRACK TO OTHER FACILITIES ARE MEASURED FROM CENTERLINE OF TRACK.
18. STATIONING THROUGH DIVERGING LEGS OF TURNOUTS IS MEASURED ON THE CENTERLINE OF TRACK AND ALONG THE TANGENT EXTENSION LINES OF THE TURNOUT. TRACK LENGTHS THROUGH THE TURNOUT RUN FROM THE POINT OF SWITCH (PT\_SW) TO THE POINT OF INTERSECTION OF THE TURNOUT (PITO) AND ALONG THE TANGENT OF THE TURNOUT CLOSURE CURVE TO THE END OF THE TURNOUT CURVE AND BEYOND.
19. TURNOUT HEADBLOCK TIES AND SWITCH MACHINES SHALL BE INSTALLED ON THE SIDE OF TRACK INDICATED ON THE PLANS BY THE TRIANGLE SWITCH TARGET SYMBOL.
20. THE TOP OF RAIL ELEVATIONS OF THE EASTBOUND TRACK SHALL BE EQUAL TO THE ADJACENT WESTBOUND TRACK TOP OF RAIL, EXCEPT AS NOTED. THE PROFILE GRADE OF THE EASTBOUND TRACK IS PROJECTED RADIALLY IN CURVED SECTIONS AND PERPENDICULARLY IN TANGENT SECTIONS FROM THE PROFILE GRADE OF WESTBOUND TRACK.

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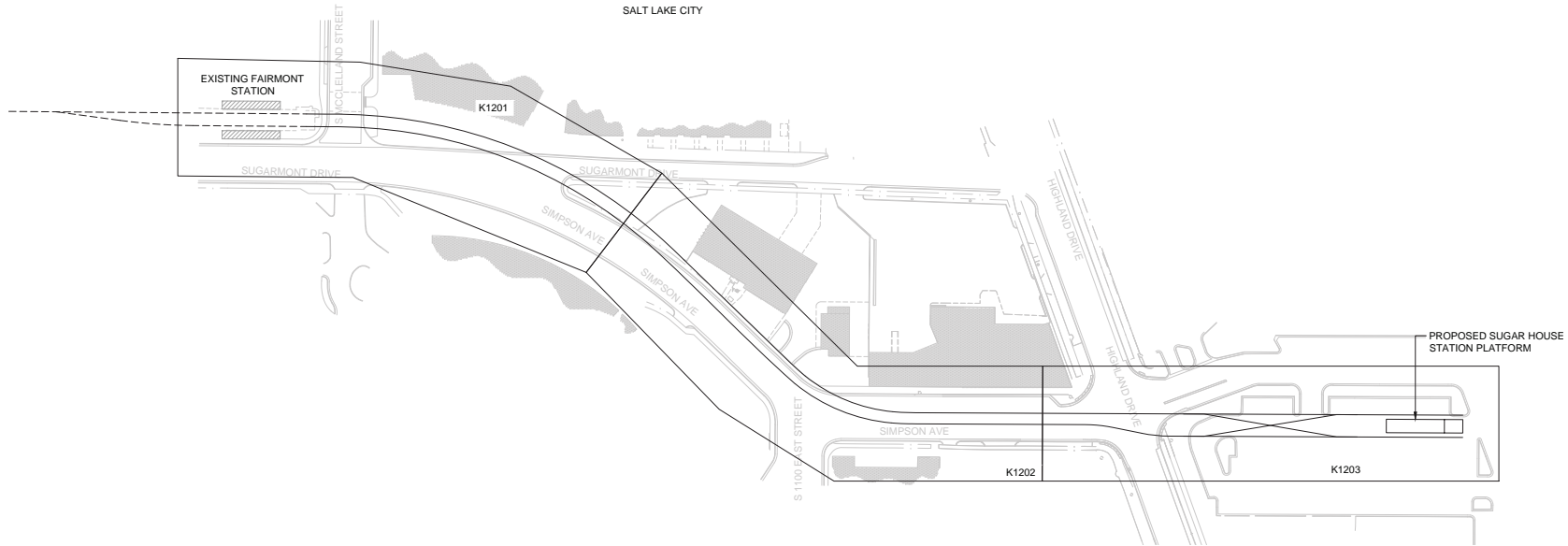


Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

Designed By: T. COVEYOU
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

S-LINE EXTENSION PROJECT  GENERAL  GENERAL NOTES	State: NTS
	CADD Filename: SL-C-G-0-013.dwg
	Submitted Date: 01-16-26
	UTA Contract No.: 24-03849
Drawing No.: G0013	Sheet No.:



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Designed By:	S. JOSHI
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Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

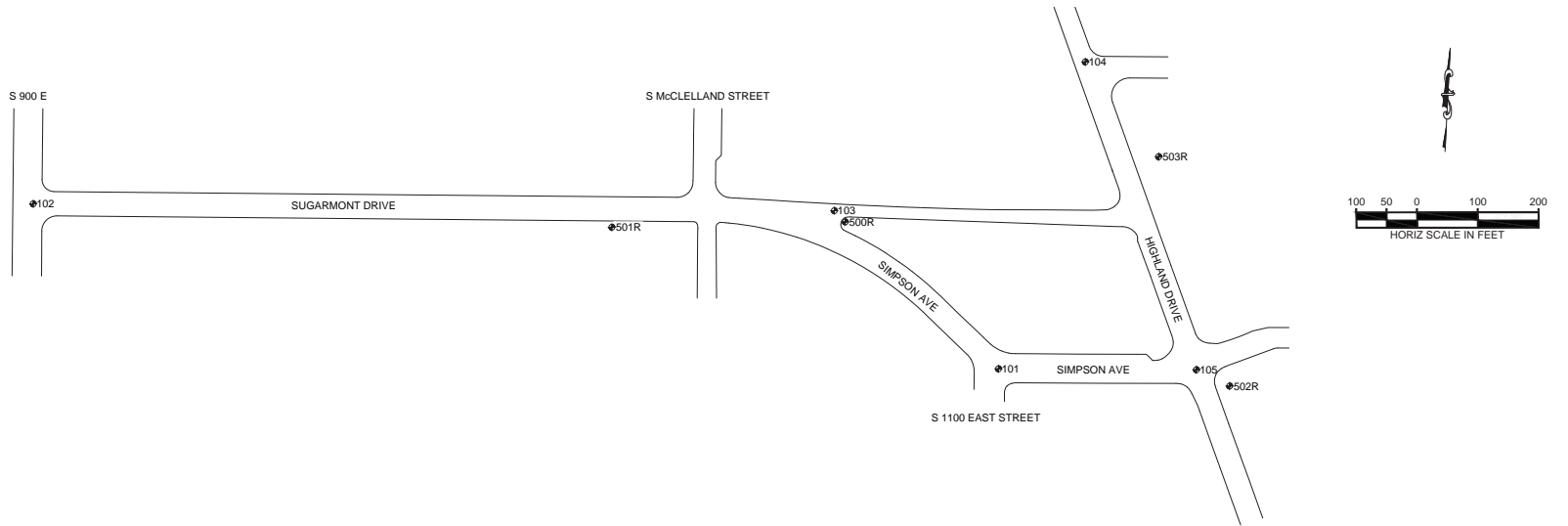
**S-LINE EXTENSION PROJECT**  
  
**TRACK KEY MAP**

Scale:	NTS
CADD Filename:	SL-C-K-0-001.dwg
Submit Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K0001
Sheet No.:	

Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

# TOPOGRAPHIC SURVEY

TAX ID NO: (REFER TO RECORD OF SURVEY)  
 LYING WITHIN THE SOUTHEAST QUARTER (SE ¼) OF SECTION 17,  
 TOWNSHIP 1 SOUTH, RANGE 1 EAST, SALT LAKE BASE AND MERIDIAN



## △ PROJECT CONTROL (SPC)

HORIZONTAL AND VERTICAL DATA WAS COLLECTED BY GPS AND CONVENTIONAL METHODS REFERENCING UTAHS TURN GPS (VRS) NETWORK. MAP SCALING PARAMETERS ARE DEFINED AS FOLLOWS:

PROJECT DATUM = NORTH AMERICAN DATUM OF 1983 (N.A.D. 83)  
 VERTICAL DATUM = N.A.V.D. 88 (REFERENCING GEOID MODEL 18)  
 COORDINATE SYSTEM = U.S. STATE PLANE 1983, UTAH CENTRAL ZONE (4302) PROJECTED TO GROUND COORDINATES.

UNITS = US SURVEY FOOT  
 FALSE NORTHING = 0000.00  
 FALSE EASTING = 0000.00  
 GROUND SCALE FACTOR = 1.0001871262  
 ELIPSOID HEIGHT = 4312.98'

SCALING LATITUDE = N 40°43'18.87249"  
 SCALING LONGITUDE = W 111°51'34.59096"

SURVEY CONTROL COORDINATES WERE OBTAINED USING STANDARD RTK (RVS) OBSERVATION METHODS.

CONTROL POINTS ARE SUBJECT TO THE EFFECTS OF SURFACE ACTIVITY AND SEASONAL MOVEMENTS. POINTS MAY BE SUBJECT TO INTENTIONAL OR UNINTENTIONAL DAMAGE OR REMOVAL. CONTROL POINT POSITIONS SHOULD BE FIELD VERIFIED PRIOR TO ANY SUBSEQUENT SURVEY OR CONSTRUCTION ACTIVITIES.

POINT	LATITUDE (N)	LONGITUDE (W)	NORTHING	EASTING	ELEVATION	DESCRIPTION
101	40°43'18.87267"	111°51'34.59132"	7431908.164	1540734.983	4360.771'	1.5' BC MON
102	40°43'21.49277"	111°51'55.24684"	7432179.826	1539145.332	4333.117'	3.5' BC MON
103	40°43'21.43367"	111°51'38.11544"	7432168.490	1540464.627	4356.140'	1.5' BC MON
104	40°43'23.87051"	111°51'32.76021"	7432143.495	1540878.033	4364.330'	1.5' BC MON
105	40°43'18.87060"	111°51'30.35942"	7431906.647	1541060.892	4370.235'	1.5' BC MON
500R	40°43'21.25256"	111°51'37.88085"	7432150.084	1540482.619	4357.911'	REDCON SCRIBE
501R	40°43'21.14992"	111°51'42.87099"	7432141.247	1540098.277	4351.920'	REDCON SCRIBE
502R	40°43'18.60183"	111°51'29.64744"	7431879.859	1541115.617	4374.707'	REDCON R/C
503R	40°43'22.33323"	111°51'31.18303"	7432257.400	1540998.869	4369.340'	REDCON MAG NAIL

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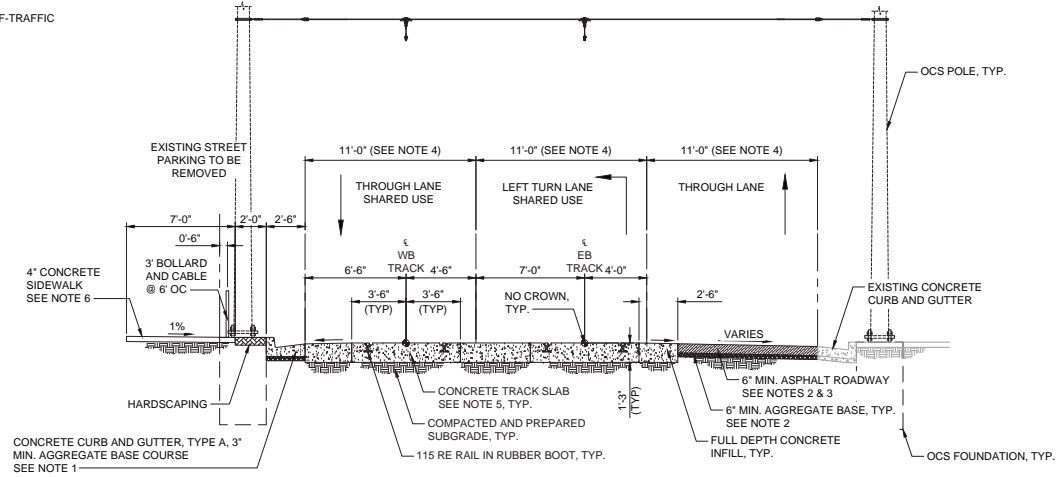
Designed By: S. JOSHI
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

S-LINE EXTENSION PROJECT  SURVEY CONTROL DATA	State: NTS
	CADD Filename: SL-C-R-0-201.dwg
	Submitted Date: 01-16-26
	UTA Contract No.: 24-03849
Drawing No.: R0201	Sheet No.:



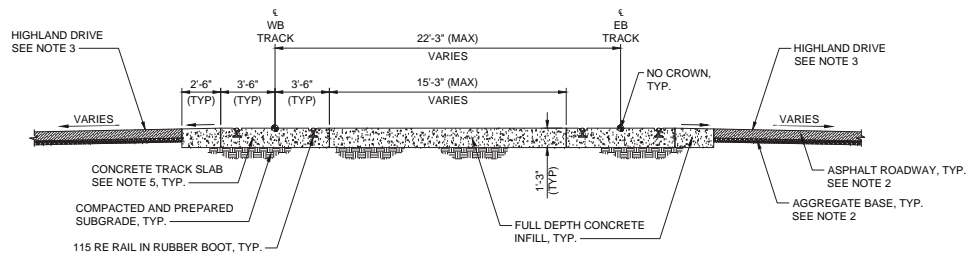
**NOTES:**

1. SEE DWG NO. C3011 FOR CONCRETE CURB AND GUTTER DETAILS.
2. SEE ROADWAY SHEETS FOR PAVEMENT RECONSTRUCTION LIMITS.
3. FINAL LIFT OF AC PAVEMENT TO BE DELAYED UNTIL AFTER MAINTENANCE-OF-TRAFFIC STRIPING IS REMOVED.
4. SEE DRAWINGS T1202 AND T1203 FOR TRAFFIC LANE DETAILS.
5. SEE DWG NO. K3002 FOR TYPICAL TRACK SLAB DETAILS.
6. SEE DRAWINGS C1201 AND C1202 FOR SIDEWALK PLAN AND PROFILE.



**PROPOSED SHARED USE TRACK TYP. SECTION**

WB STA 208+72.00 TO WB STA 211+94.07



**HIGHLAND INTERSECTION TRACK TYP. SECTION**

WB 211+94.07 TO WB STA 212+92.38

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Designed By:	T. COVEYOU
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

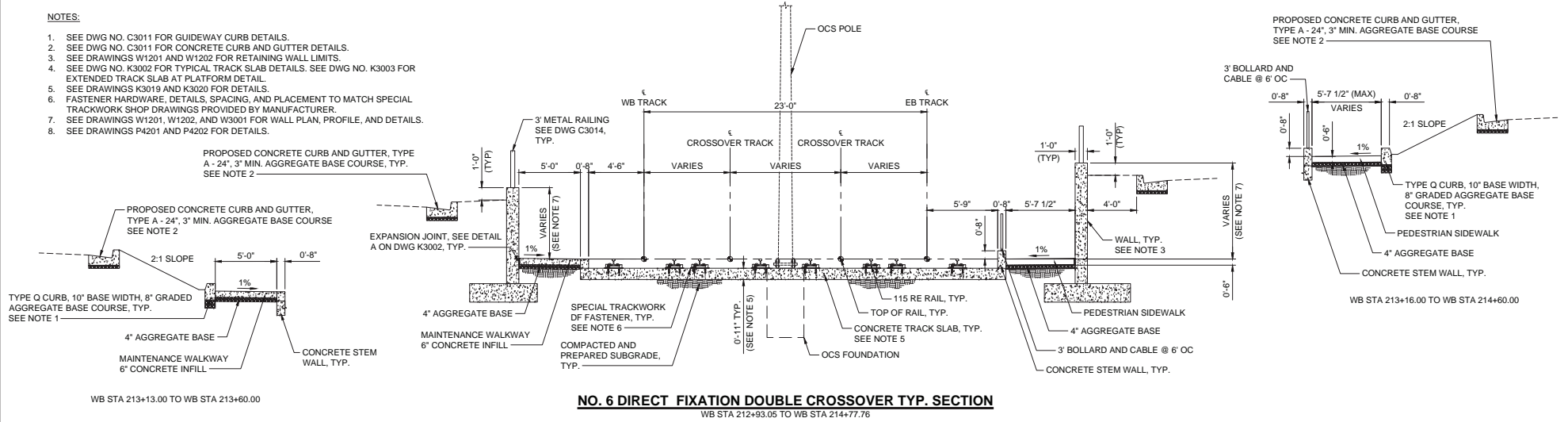
S-LINE EXTENSION PROJECT

TRACK TYPICAL SECTIONS  
SHEET 2 OF 4

Scale:	1/4"=1'-0"
CADD Filename:	SL-C-K-4-202.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K4202
Sheet No.:	

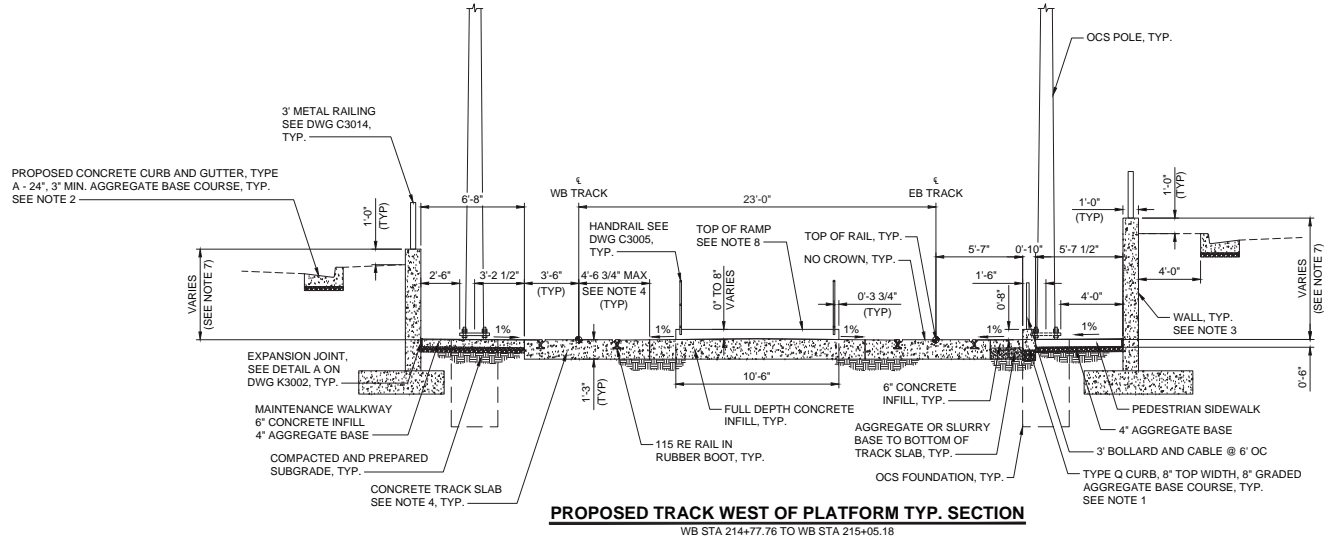
**NOTES:**

1. SEE DWG NO. C3011 FOR GUIDEWAY CURB DETAILS.
2. SEE DWG NO. C3011 FOR CONCRETE CURB AND GUTTER DETAILS.
3. SEE DRAWINGS W1201 AND W1202 FOR RETAINING WALL LIMITS.
4. SEE DWG NO. K3002 FOR TYPICAL TRACK SLAB DETAILS. SEE DWG NO. K3003 FOR EXTENDED TRACK SLAB AT PLATFORM DETAIL.
5. SEE DRAWINGS K3019 AND K3020 FOR DETAILS.
6. FASTENER HARDWARE, DETAILS, SPACING, AND PLACEMENT TO MATCH SPECIAL TRACKWORK SHOP DRAWINGS PROVIDED BY MANUFACTURER.
7. SEE DRAWINGS W1201, W1202, AND W3001 FOR WALL PLAN, PROFILE, AND DETAILS.
8. SEE DRAWINGS P4201 AND P4202 FOR DETAILS.



**NO. 6 DIRECT FIXATION DOUBLE CROSSOVER TYP. SECTION**

WB STA 212+93.05 TO WB STA 214+77.76



**PROPOSED TRACK WEST OF PLATFORM TYP. SECTION**

WB STA 214+77.76 TO WB STA 215+05.18

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Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

Designed By: T. COVEYOU  
 Drawn By: T. COVEYOU  
 Checked By: A. DIGIROLAMO  
 Approved By: A. DIGIROLAMO

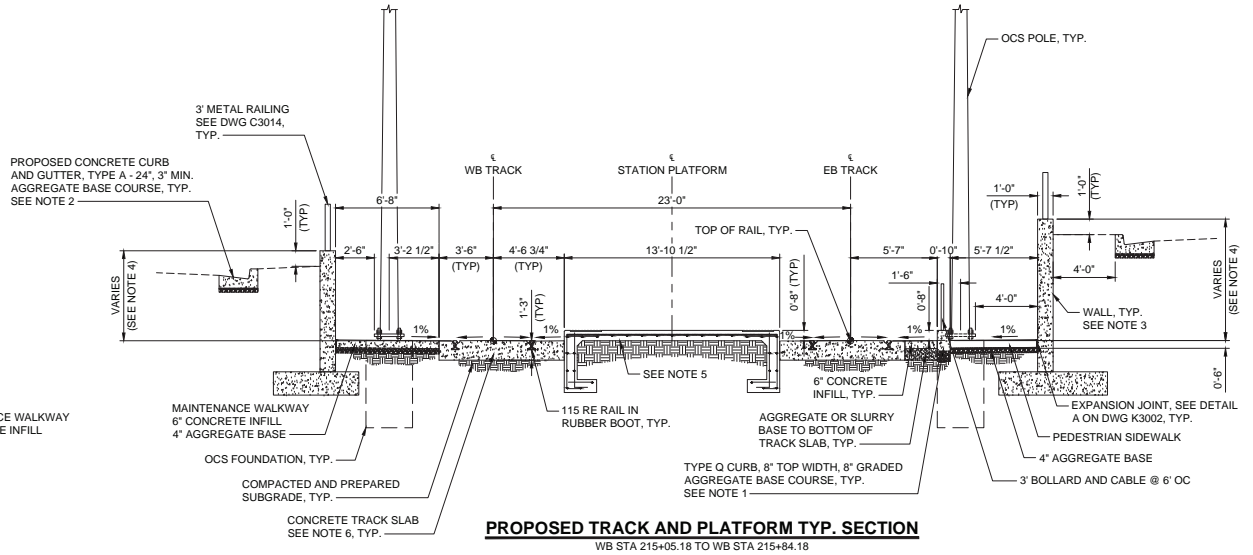
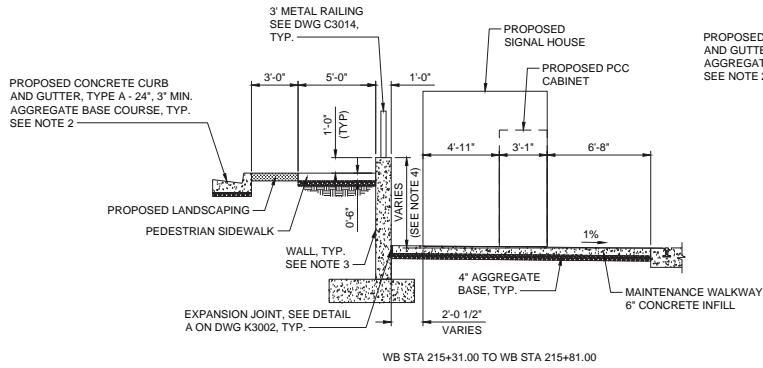
S-LINE EXTENSION PROJECT

TRACK TYPICAL SECTIONS  
 SHEET 3 OF 4

Scale:	1/4"=1'-0"
CADD Filename:	SL-C-K-4-203.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K4203
Sheet No.:	

**NOTES:**

1. SEE DWG NO. C3011 FOR GUIDEWAY CURB DETAILS.
2. SEE DWG NO. C3011 FOR CONCRETE CURB AND GUTTER DETAILS.
3. SEE DRAWINGS W1201 AND W1202 FOR RETAINING WALL LIMITS.
4. SEE DRAWINGS W1201, W1202, AND W3001 FOR WALL PLAN, PROFILE, AND DETAILS.
5. SEE DRAWINGS P4201 AND P4202 FOR DETAILS.
6. SEE DWG NO. K3002 FOR TYPICAL TRACK SLAB DETAILS. SEE DWG NO. K3003 FOR EXTENDED TRACK SLAB AT PLATFORM DETAIL.



**PROPOSED TRACK AND PLATFORM TYP. SECTION**

WB STA 215+05.18 TO WB STA 215+84.18

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Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Designed By:	T. COVEYOU
Drawn By:	T. COVEYOU
Checked By:	A. DIGRIOLAMO
Approved By:	A. DIGRIOLAMO

S-LINE EXTENSION PROJECT

TRACK TYPICAL SECTIONS  
SHEET 4 OF 4

Scale:	1/4"=1'-0"
CADD Filename:	SL-C-K-4-204.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K4204
Sheet No.:	

EXTENSION - WB TRACK																
CURVE / XO / TO NUMBER	DESCRIPTION	STATION	DEGREE OF CURVE (ARC)	RADIUS (FT)	Lc (FT)	Ls IN (FT)	Ls OUT (FT)	Eq (IN)	Ea (IN)	Eu (IN)	DESIGN V (MPH)	CURVE DELTA	NORTHING	EASTING	TANGENT LENGTH (FT)	BEARING
	POB	202+92.17											7432222.415	1540210.550		
	TS	203+13.05											7432222.235	1540231.431	20.88	S 89° 30' 24" E
	SC	203+44.05											7432221.648	1540262.424		
C240	PI	205+28.47	11°27'33"	500.00	349.31	31.00	31.00	1.78	0.00	1.78	15	40° 01' 41" RT	7432220.381	1540446.841		
	CS	206+93.36											7432091.871	1540579.114		
	ST	207+24.36											7432070.540	1540601.607		
	TS	208+65.33											7431972.488	1540702.882	140.96	S 45° 55' 35" E
	SC	208+96.33											7431951.619	1540725.791		
C250	PI	209+49.29	33°42'12"	170.00	99.01	31.00	31.00	2.33	0.00	2.33	10	33° 22' 09" LT	7431914.087	1540763.203		
	CS	209+95.34											7431914.792	1540816.192		
	ST	210+26.34											7431913.712	1540847.162		
	PS	212+95.63											7431912.507	1541116.456	557.84	S 89° 44' 37" E
X130	PITO	213+16.88											7431912.412	1541137.706		S 89° 44' 37" E
	PITO	214+53.93											7431911.799	1541274.747	137.05	S 89° 44' 37" E
X140	PS	214+75.18											7431911.704	1541295.997		S 89° 44' 37" E
	POE	215+84.18											7431911.216	1541404.996	109.00	S 89° 44' 37" E

EXTENSION - EB TRACK																
CURVE / XO / TO NUMBER	DESCRIPTION	STATION	DEGREE OF CURVE (ARC)	RADIUS (FT)	Lc (FT)	Ls IN (FT)	Ls OUT (FT)	Eq (IN)	Ea (IN)	Eu (IN)	DESIGN V (MPH)	CURVE DELTA	NORTHING	EASTING	TANGENT LENGTH (FT)	BEARING
	POB	202+92.88											7432209.415	1540210.438		
	TS	203+13.76											7432209.236	1540231.318	20.88	S 89° 30' 24" E
	SC	203+44.76											7432208.640	1540262.311		
S240	PI	205+23.98	11°45'54"	487.00	339.42	31.00	31.00	1.83	0.00	1.83	15		7432207.426	1540441.532		
	CS	206+84.18											7432082.524	1540570.067		
	ST	207+15.18											7432061.200	1540592.565		
	TS	208+53.99											7431964.651	1540692.288	138.80	S 45° 55' 35" E
	SC	208+84.99											7431943.732	1540715.152		
S250	PI	209+43.17	31°18'33"	183.00	108.95	31.00	31.00	2.16	0.00	2.16	10		7431902.618	1540756.360		
	CS	209+93.94											7431903.232	1540814.567		
	ST	210+24.94											7431902.219	1540845.541		
	PC	211+94.24											7431901.461	1541014.843	169.30	S 89° 44' 37" E
S260	PI	212+08.91	38°11'50"	150.00	29.25	0.00	0.00	2.64	0.00	2.64	10		7431901.396	1541029.516		
	PT	212+23.49											7431898.488	1541043.897		
	PC	212+53.49											7431892.543	1541073.302	30.00	S 78° 34' 12" E
S270	PI	212+68.17	38°11'50"	150.00	29.25	0.00	0.00	2.64	0.00	2.64	10		7431889.635	1541087.684		
	PT	212+82.75											7431889.57	1541102.357		
	PS	212+96.74											7431889.507	1541116.353	13.99	S 89° 44' 37" E
X140	PITO	213+17.99											7431889.412	1541137.603		S 89° 44' 37" E
	PITO	214+55.04											7431888.799	1541274.644	137.05	S 89° 44' 37" E
X130	PS	214+76.29											7431888.704	1541295.894		S 89° 44' 37" E
	POE	215+85.29											7431888.216	1541404.893	109.00	S 89° 44' 37" E

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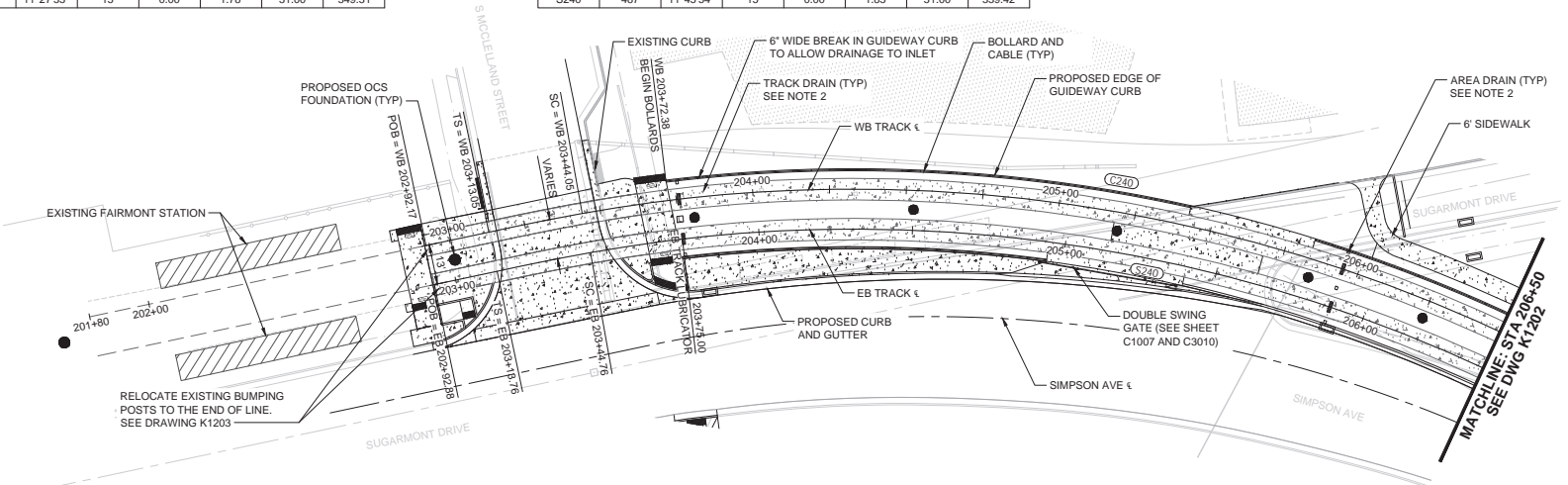
Designed By:  
S. JOSHI  
Drawn By:  
S. JOSHI  
Checked By:  
A. DIGIROLAMO  
Approved By:  
A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
  
HORIZONTAL ALIGNMENT DATA

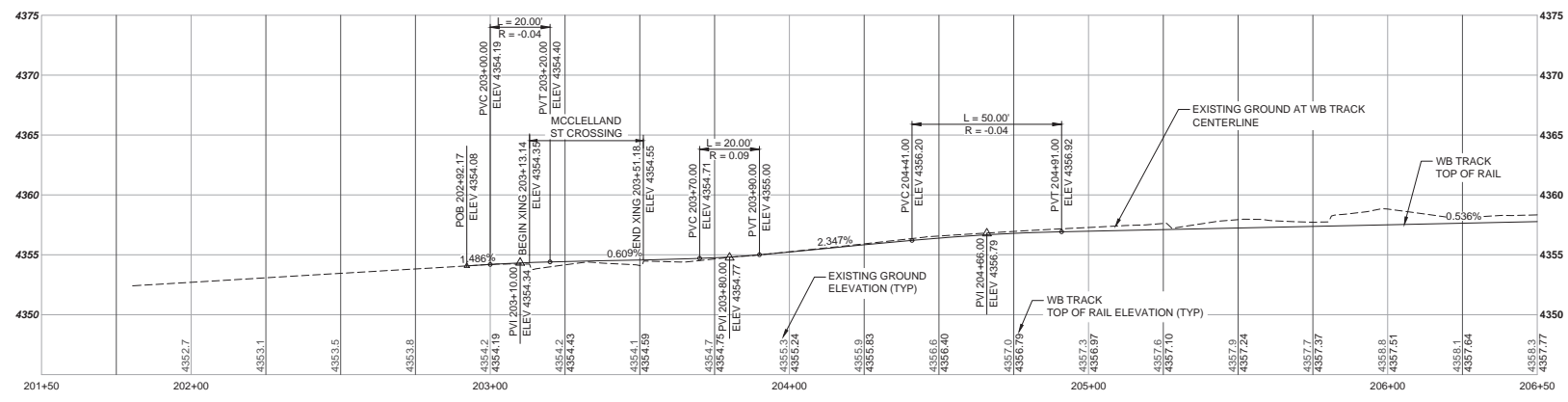
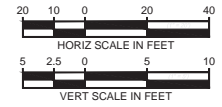
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Submitted Date: 01-16-26  
UTA Contract No.: 24-03849  
Drawing No.: K6201  
Sheet No.:

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ID NO.	R (FT)	D (ARC)	V (MPH)	Ea (IN)	Eu (IN)	Ls (FT)	Lc (FT)
C240	500	11°27'33"	15	0.00	1.78	31.00	349.31

CURVE DATA - EB TRACK							
ID NO.	R (FT)	D (ARC)	V (MPH)	Ea (IN)	Eu (IN)	Ls (FT)	Lc (FT)
S240	487	11°45'54"	15	0.00	1.83	31.00	339.42



- NOTES:
- SEE DRAWING G0013 NOTE 20 FOR EB PROFILE INFORMATION.
  - SEE DRAWINGS D1201-D1203 FOR DRAINAGE PLAN AND PROFILES.
  - SEE DRAWING C1204 FOR McCLELLAND ST CROSSING PLAN AND PROFILE.



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REV	DATE	Description

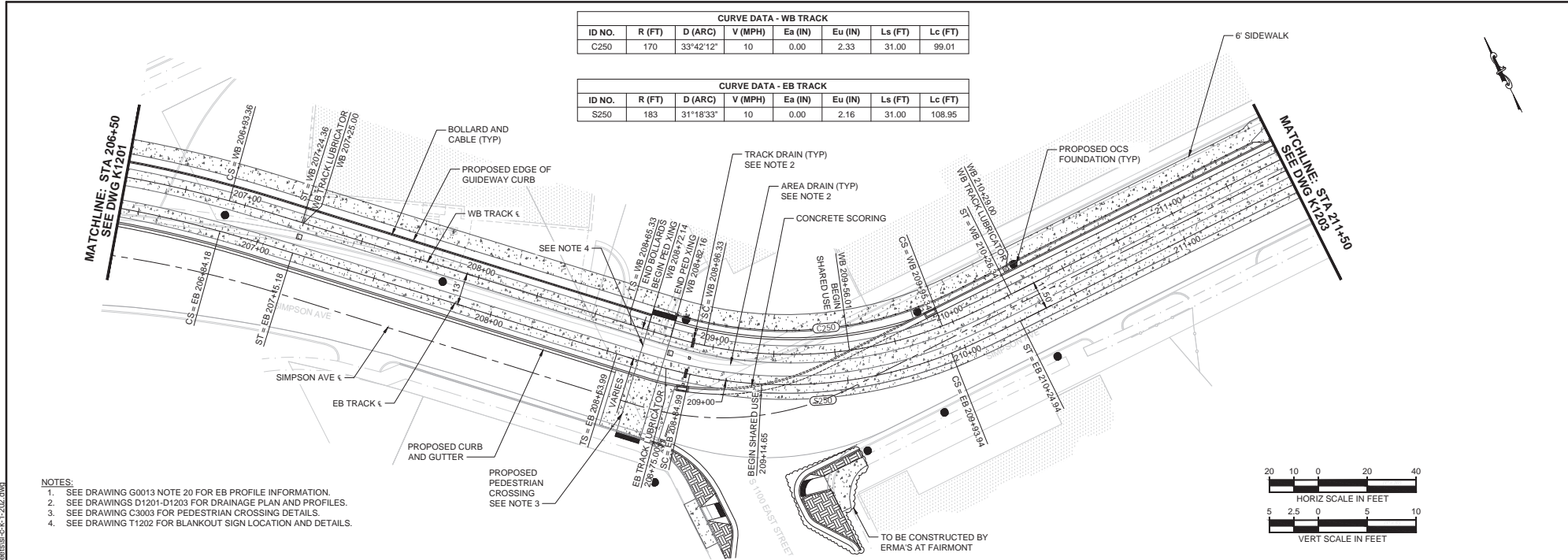


Designed By:	S. JOSHI
Drawn By:	S. JOSHI
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT

TRACK PLAN AND PROFILE  
WB STA 201+80.27 TO WB STA 206+50

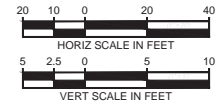
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UTA Contract No.:	24-03849
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Sheet No.:	



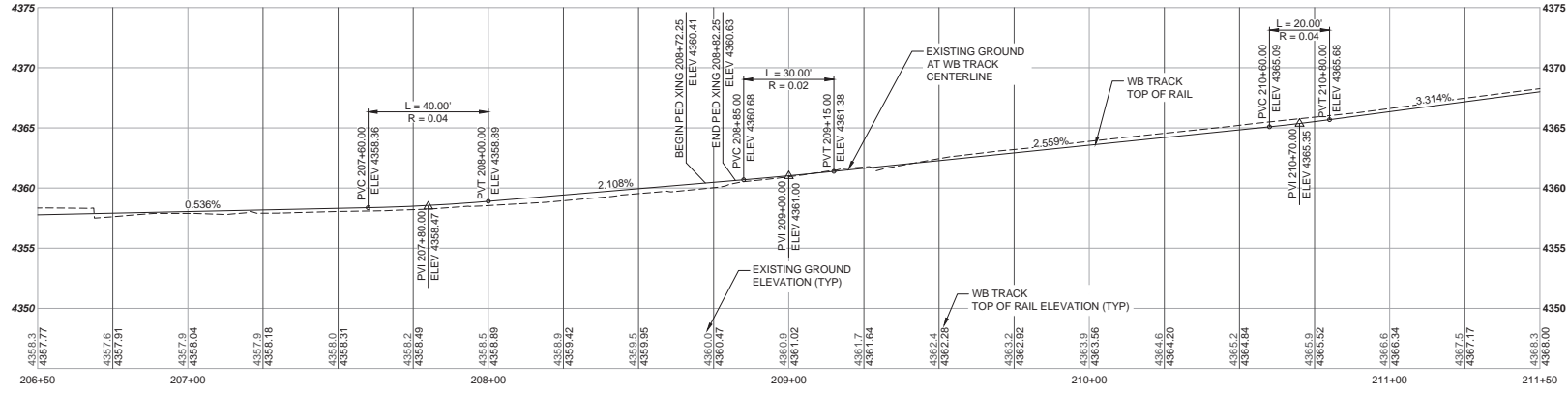
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ID NO.	R (FT)	D (ARC)	V (MPH)	Ea (IN)	Eu (IN)	Ls (FT)	Lc (FT)
C250	170	33°42'12"	10	0.00	2.33	31.00	99.01

CURVE DATA - EB TRACK							
ID NO.	R (FT)	D (ARC)	V (MPH)	Ea (IN)	Eu (IN)	Ls (FT)	Lc (FT)
S250	183	31°18'33"	10	0.00	2.16	31.00	108.95

- NOTES:
1. SEE DRAWING G0013 NOTE 20 FOR EB PROFILE INFORMATION.
  2. SEE DRAWINGS D1201-D1203 FOR DRAINAGE PLAN AND PROFILES.
  3. SEE DRAWING C3003 FOR PEDESTRIAN CROSSING DETAILS.
  4. SEE DRAWING T1202 FOR BLANKOUT SIGN LOCATION AND DETAILS.



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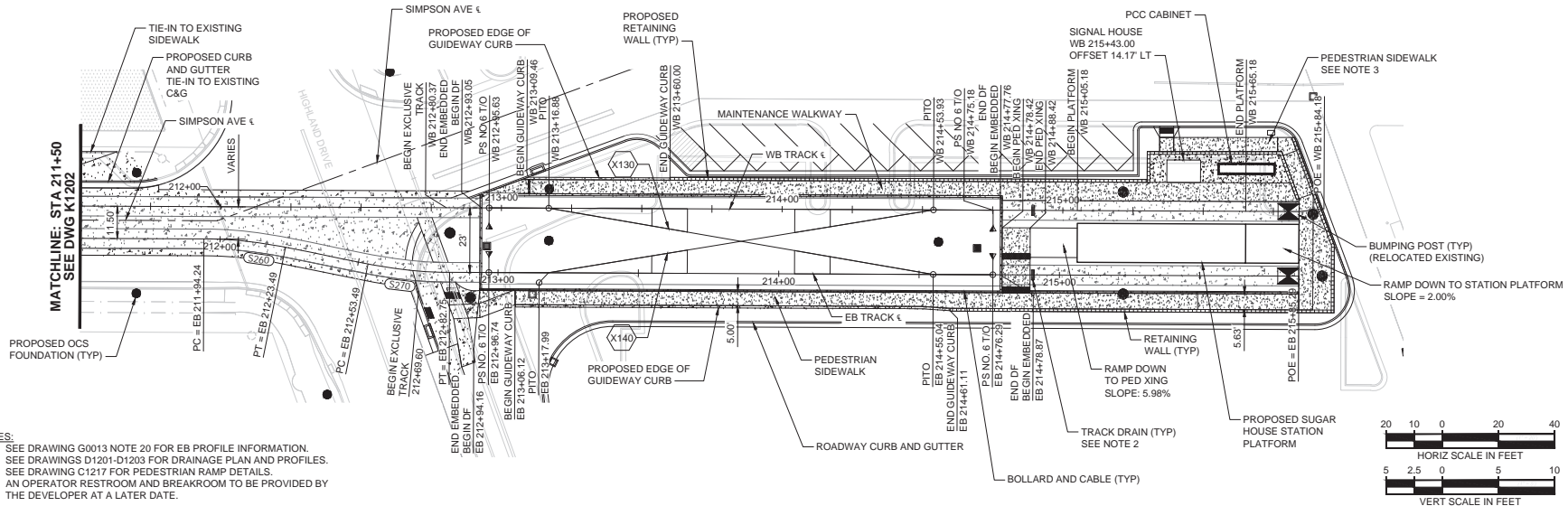


Designed By:	S. JOSHI
Drawn By:	S. JOSHI
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

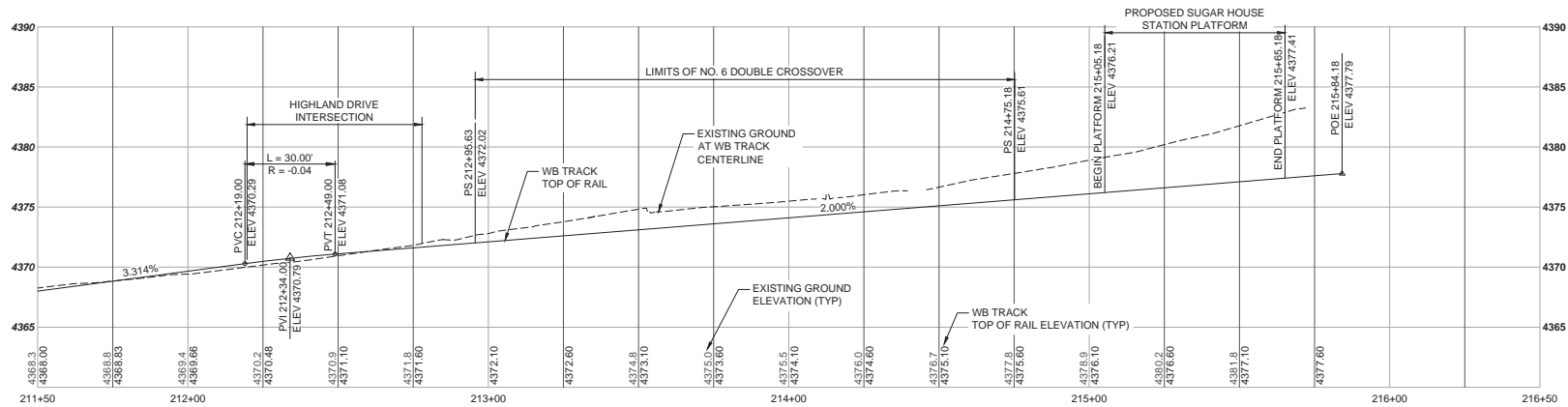
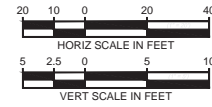
S-LINE EXTENSION PROJECT  
 TRACK PLAN AND PROFILE  
 WB STA 206+50 TO WB STA 211+50

Scale:	H: 1" = 20' V: 1" = 5'
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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K1202
Sheet No.:	

CURVE DATA - EB TRACK							
ID NO.	R (FT)	D (ARC)	V (MPH)	Ea (IN)	Eu (IN)	Ls (FT)	Lc (FT)
S260	150	38°11'50"	10	0.00	2.64	0.00	29.25
S270	150	38°11'50"	10	0.00	2.64	0.00	29.25



- NOTES:**
- SEE DRAWING G0013 NOTE 20 FOR EB PROFILE INFORMATION.
  - SEE DRAWINGS D1201-D1203 FOR DRAINAGE PLAN AND PROFILES.
  - SEE DRAWING C1217 FOR PEDESTRIAN RAMP DETAILS.
  - AN OPERATOR RESTROOM AND BREAKROOM TO BE PROVIDED BY THE DEVELOPER AT A LATER DATE.



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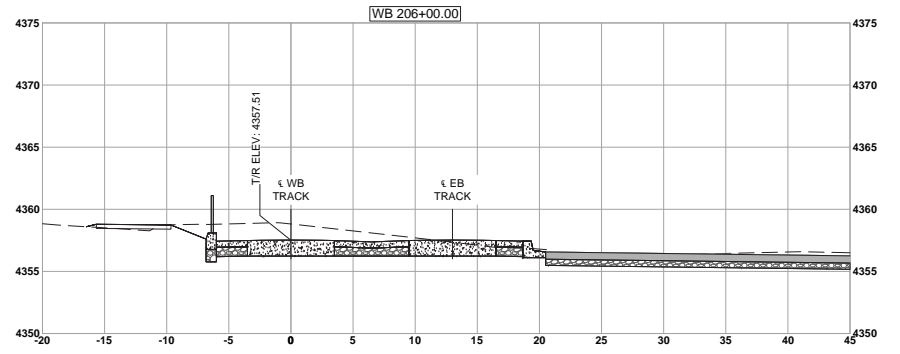
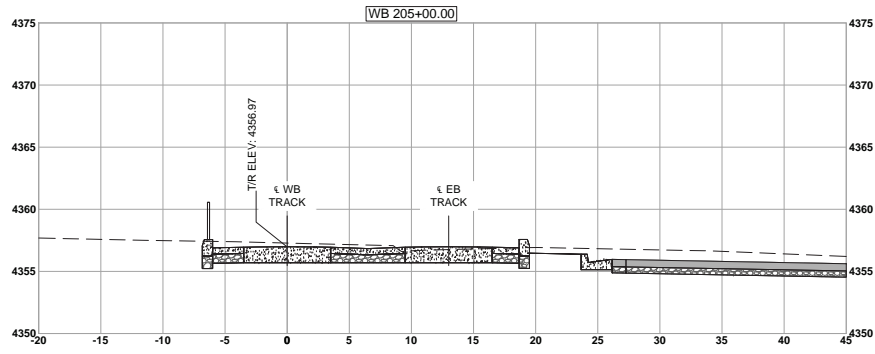
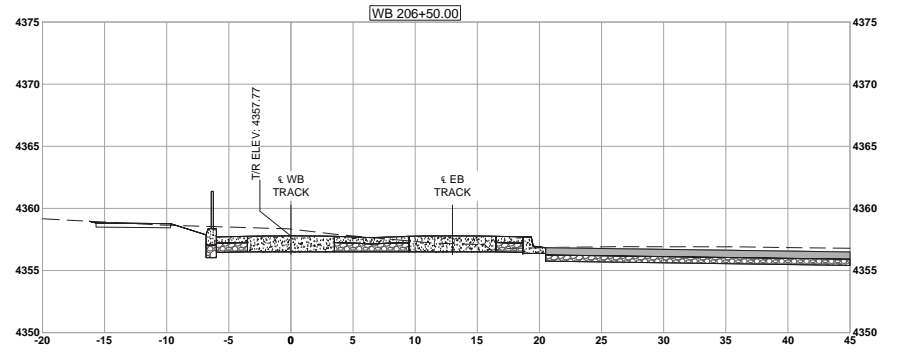
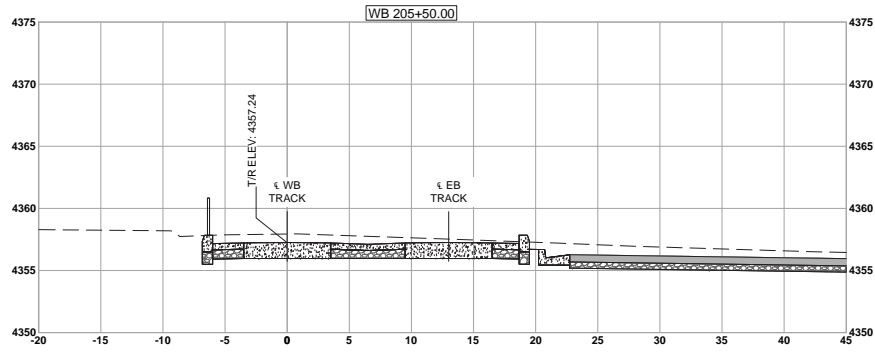
Designed By:	S. JOSHI
Drawn By:	S. JOSHI
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

**S-LINE EXTENSION PROJECT**

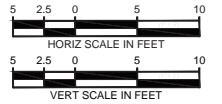
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WB STA 211+50 TO WB STA 215+84.18

Scale:	H: 1" = 20' V: 1" = 5'
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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K1203
Sheet No.:	





NOTES:  
 1. ALL STATIONING AND TOP OF RAIL (T/R) ELEVATIONS REFERENCED HEREIN ARE BASED ON WB TRACK.



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 Sheet: 1 of 1

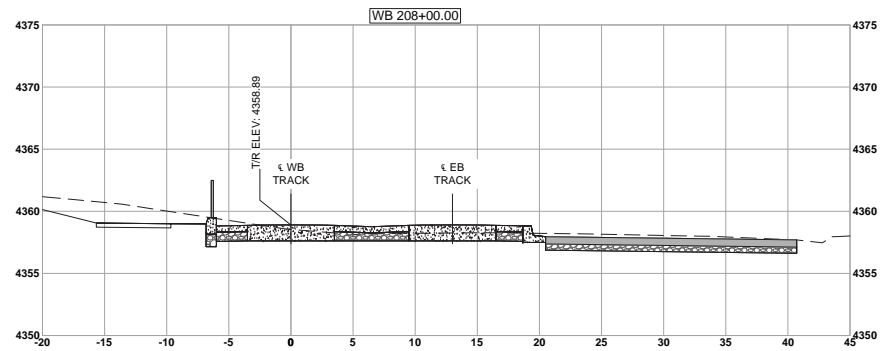
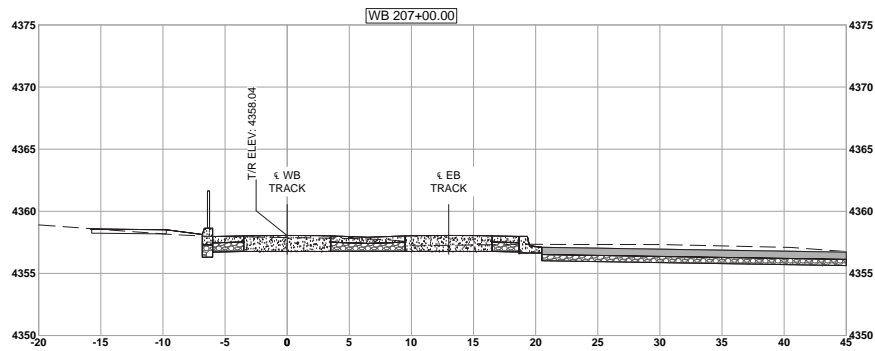
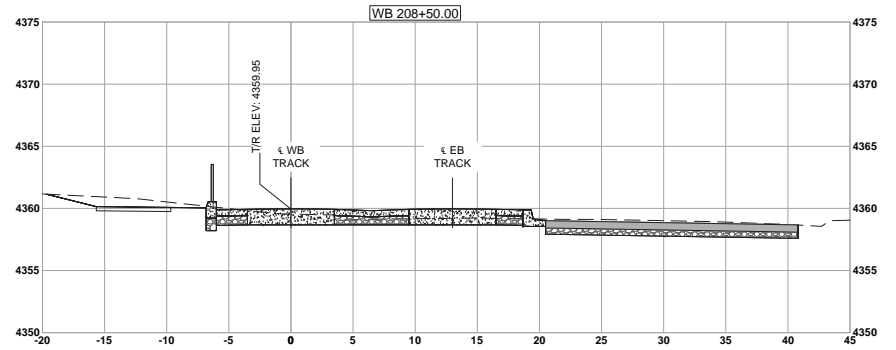
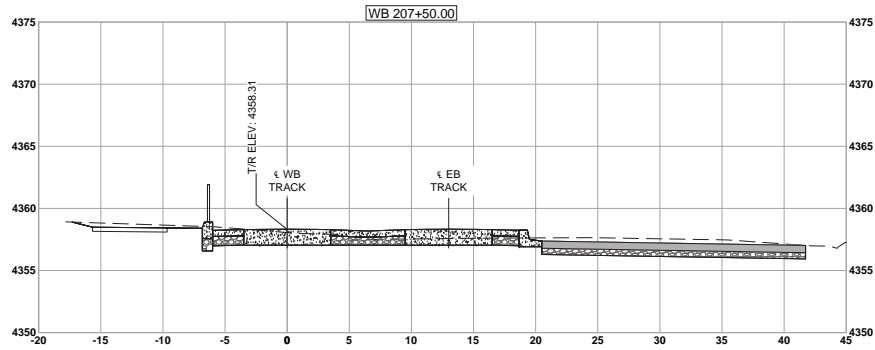
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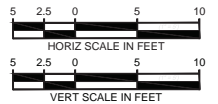
Designed By: S. JOSHI
Drawn By: S. JOSHI
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
 TRACK CROSS SECTIONS  
 WB STA 205+00 TO WB STA 206+50

State:	H: 1" = 5' V: 1" = 5'
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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
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NOTES:  
1. ALL STATIONING AND TOP OF RAIL (T/R) ELEVATIONS REFERENCED HEREIN ARE BASED ON WB TRACK.



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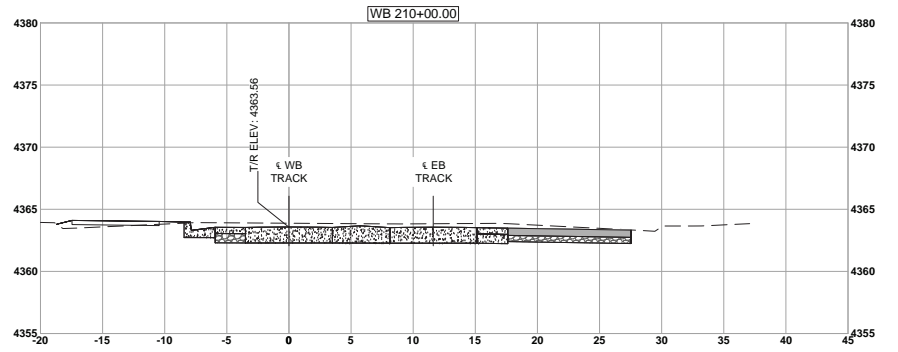
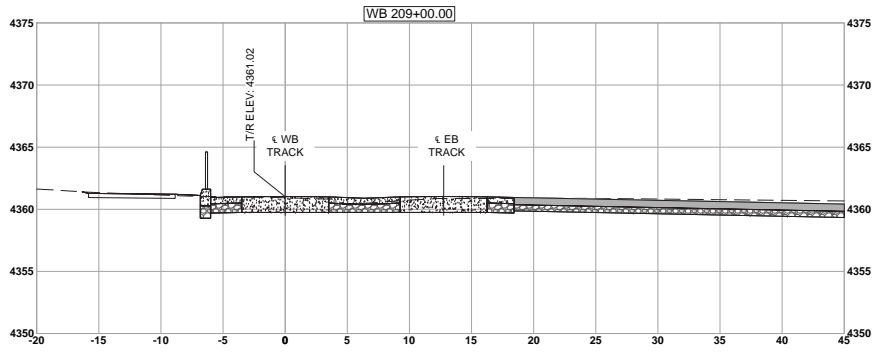
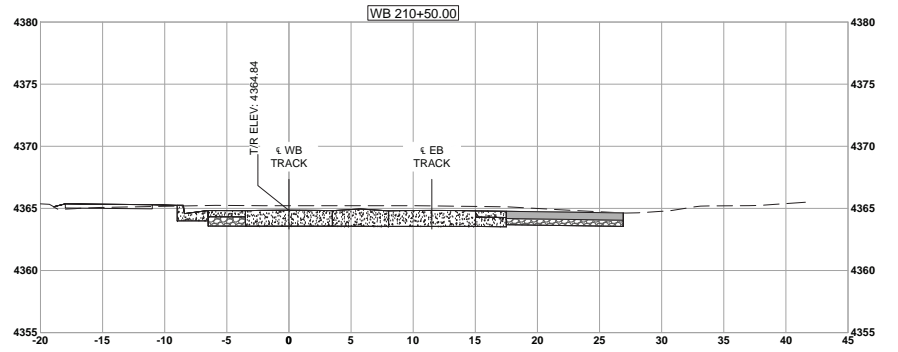
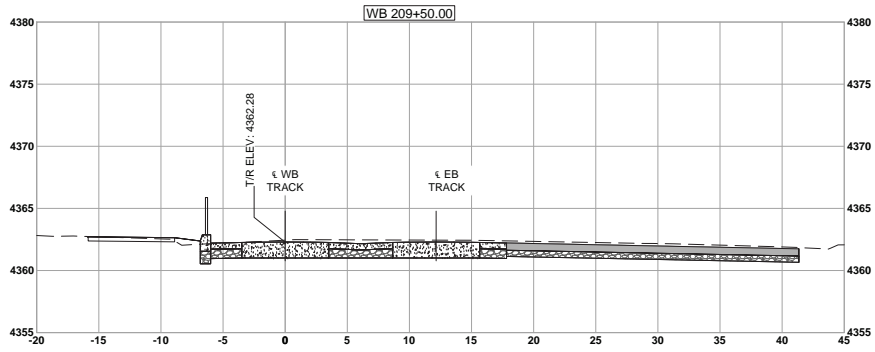


Designed By:	S. JOSHI
Drawn By:	S. JOSHI
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

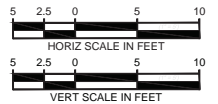
S-LINE EXTENSION PROJECT

TRACK CROSS SECTIONS  
WB STA 207+00 TO WB STA 208+50

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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K5203
Sheet No.:	



NOTES:  
 1. ALL STATIONING AND TOP OF RAIL (T/R) ELEVATIONS REFERENCED HEREIN ARE BASED ON WB TRACK.



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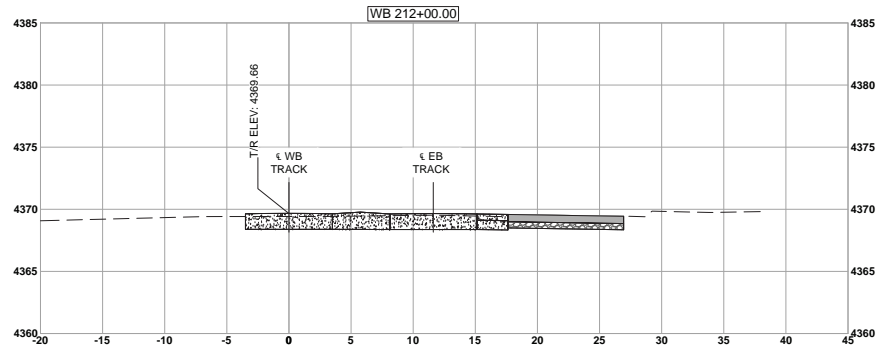
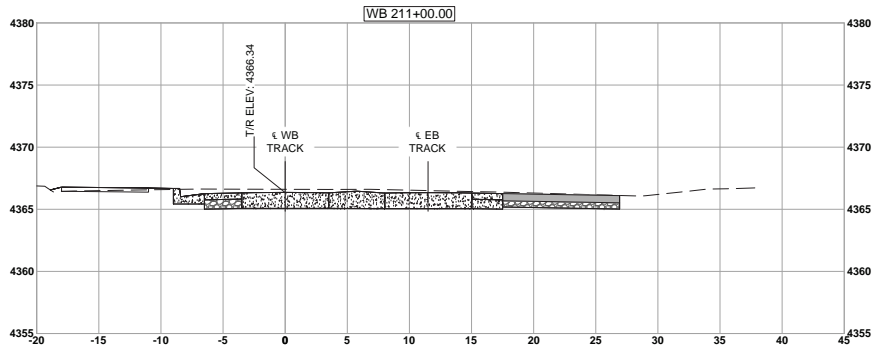
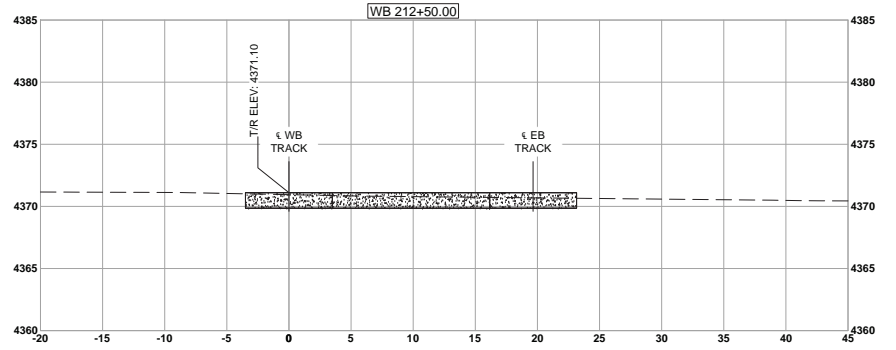
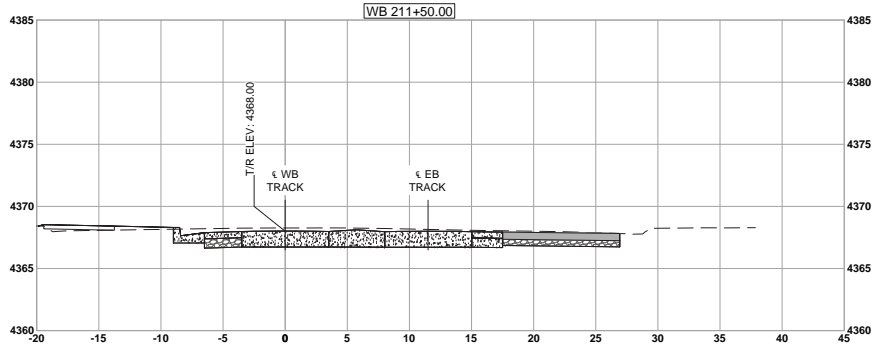


Designed By:  
S. JOSHI  
 Drawn By:  
S. JOSHI  
 Checked By:  
A. DIGIROLAMO  
 Approved By:  
A. DIGIROLAMO

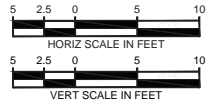
S-LINE EXTENSION PROJECT

TRACK CROSS SECTIONS  
 WB STA 209+00 TO WB STA 210+50

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 Submitted Date: 01-16-26  
 UTA Contract No.: 24-03849  
 Drawing No.: K5204  
 Sheet No.:



NOTES:  
1. ALL STATIONING AND TOP OF RAIL (T/R) ELEVATIONS REFERENCED HEREIN ARE BASED ON WB TRACK.



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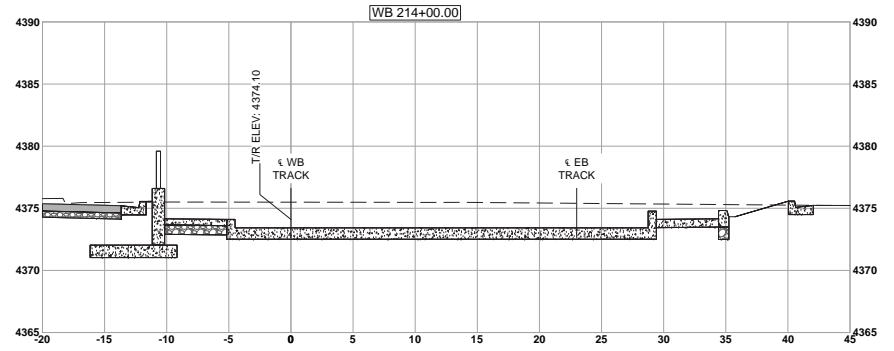
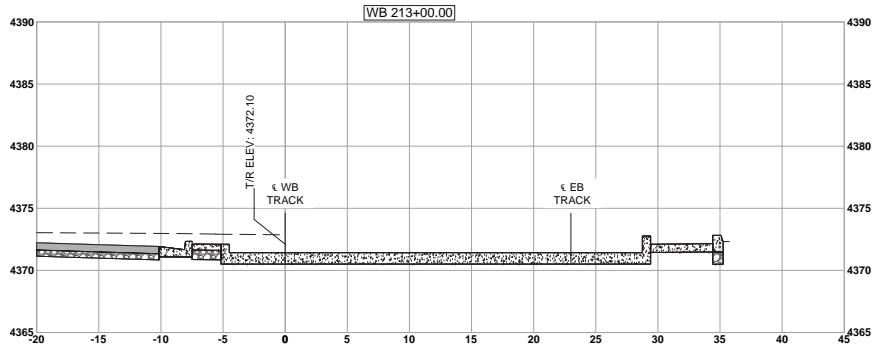
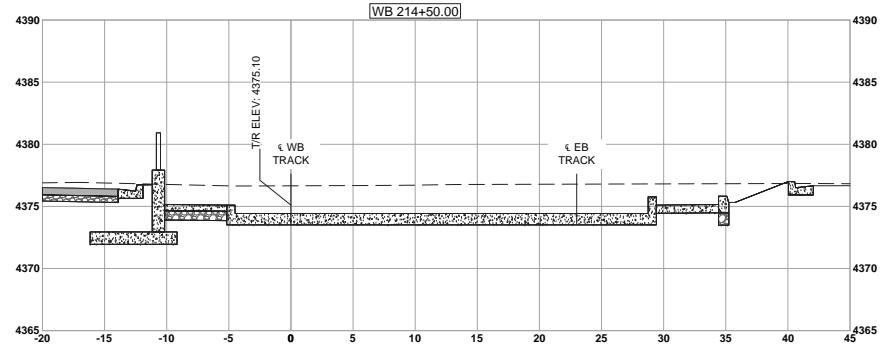
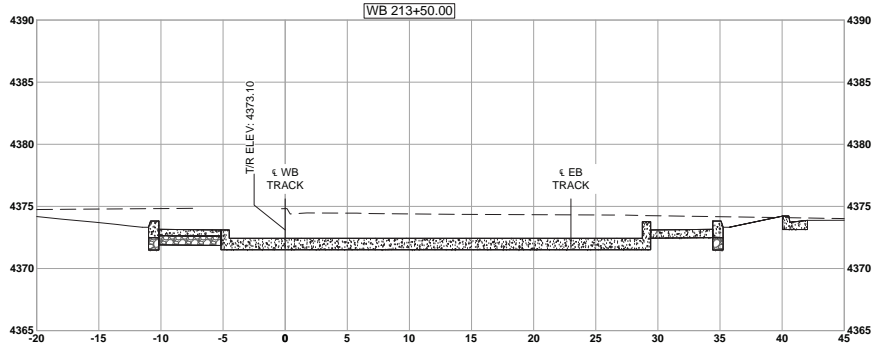
Designed By:	S. JOSHI
Drawn By:	S. JOSHI
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT

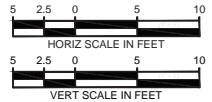
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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K5205
Sheet No.:	

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NOTES:  
 1. ALL STATIONING AND TOP OF RAIL (T/R) ELEVATIONS REFERENCED HEREIN ARE BASED ON WB TRACK.



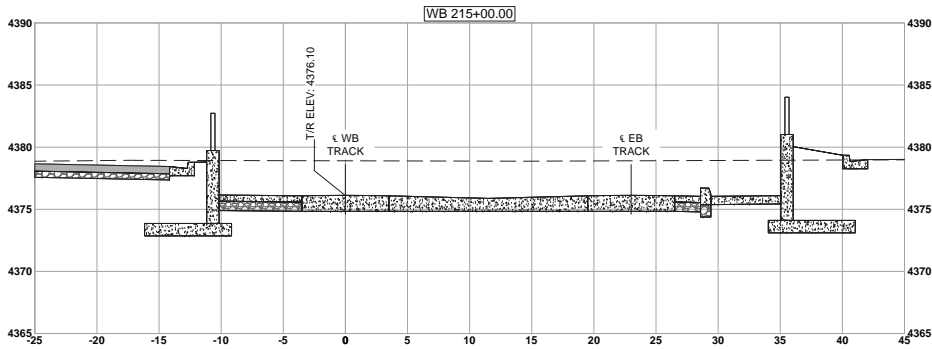
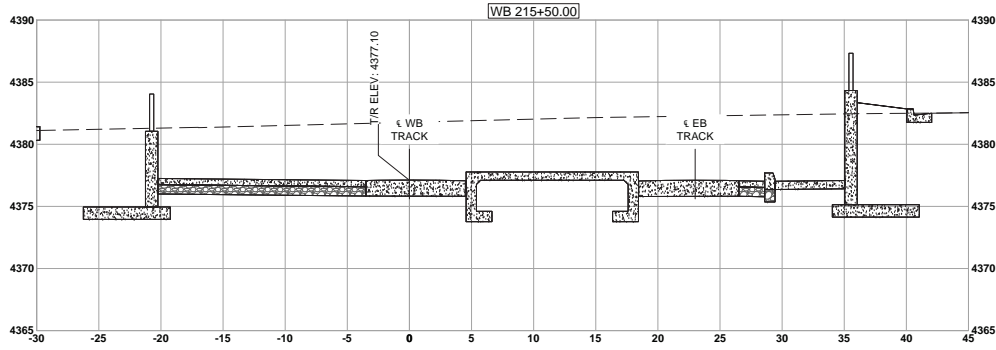
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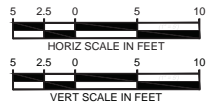
Designed By:  
S. JOSHI  
 Drawn By:  
S. JOSHI  
 Checked By:  
A. DIGIROLAMO  
 Approved By:  
A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
 TRACK CROSS SECTIONS  
 WB STA 213+00 TO WB STA 214+50

State: H: 1" = 5' V: 1" = 5'  
 CADD Filename: SL-C-K-5-206.dwg  
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 Drawing No.: K5206  
 Sheet No.:



NOTES:  
 1. ALL STATIONING AND TOP OF RAIL (T/R) ELEVATIONS REFERENCED HEREIN ARE BASED ON WB TRACK.



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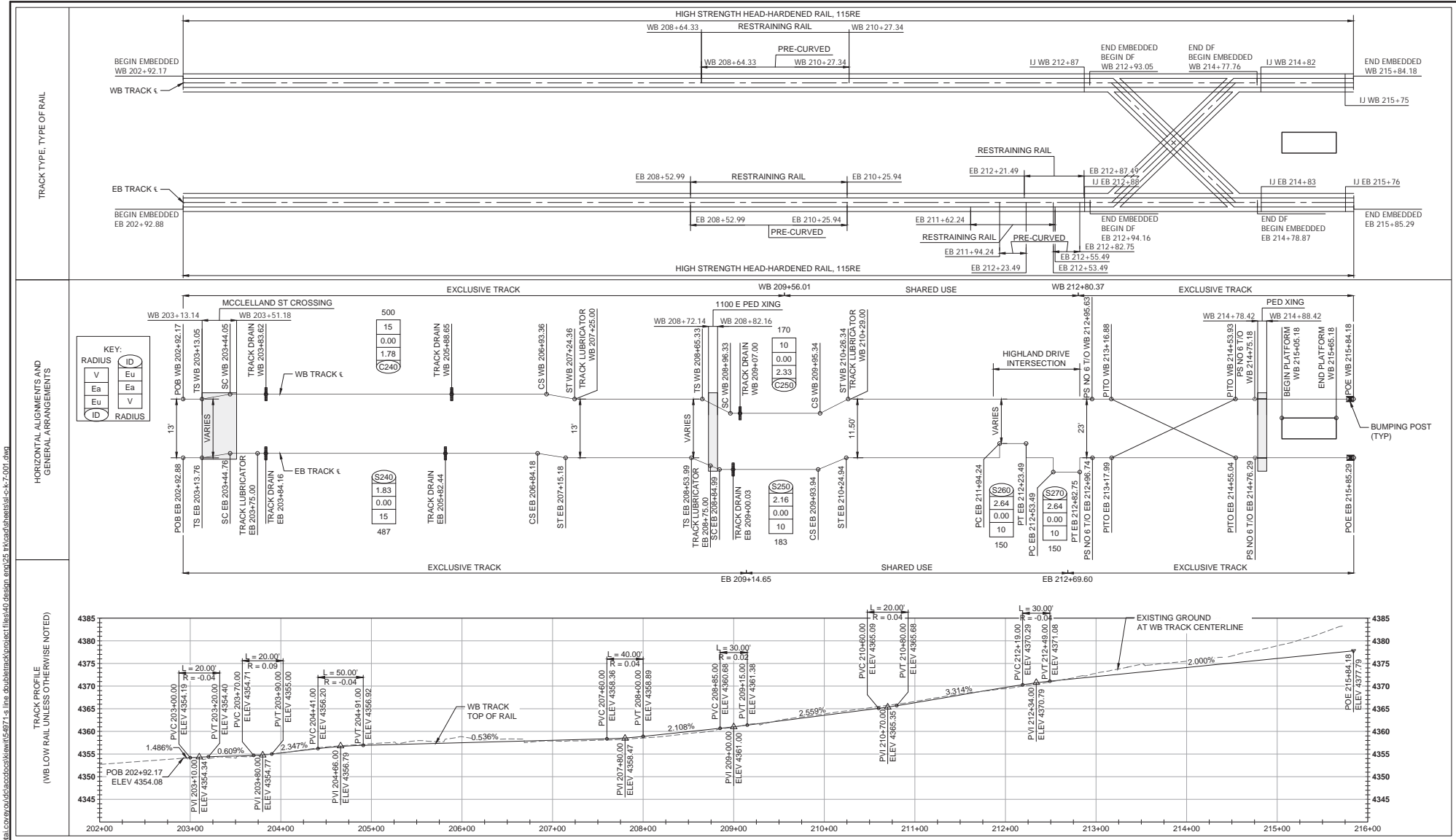


Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Designed By: S. JOSHI
Drawn By: S. JOSHI
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

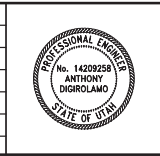
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 TRACK CROSS SECTIONS  
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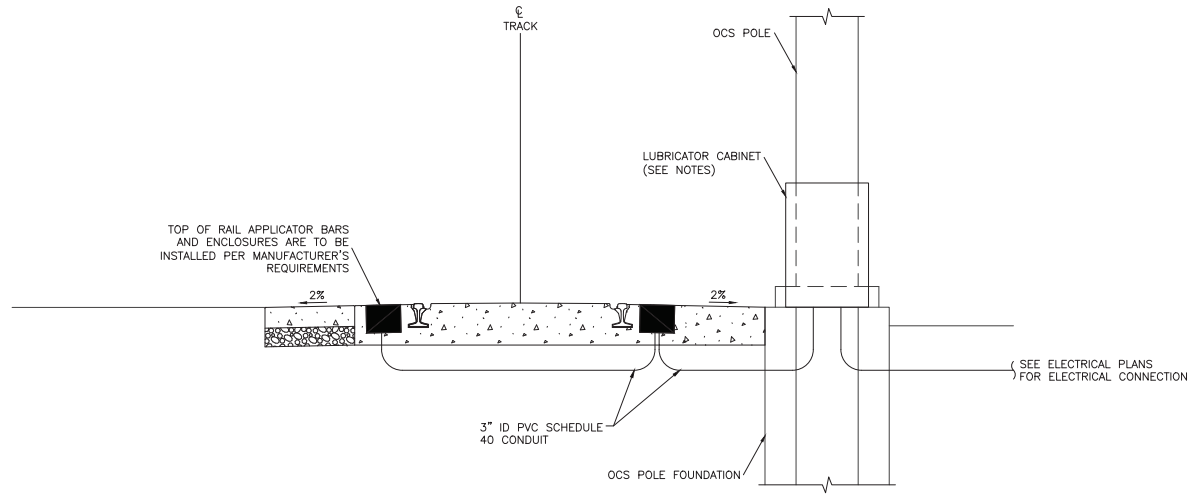
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REV	DATE	Description



Designed By: S. JOSHI	<b>S-LINE EXTENSION PROJECT</b>  <b>TRACK CHART</b> WB STA 202+92.17 TO WB STA 215+84.18
Drawn By: S. JOSHI	
Checked By: A. DIGIROLAMO	
Approved By: A. DIGIROLAMO	

Scale: H: 1" = 50' V: 1" = 10'
CADD Filename: SL-C-K-7-001.dwg
Submitted Date: 01-16-26
UTA Contract No.: 24-03849
Drawing No.: K7001



**RAIL LUBRICATORS DETAIL**  
SCALE: NTS

NOTES:

1. REFER TO DWG. K1201-K1203 FOR LOCATION OF LUBRICATOR CABINETS AND TRACK BOXES.
2. LUBRICATOR CABINETS, TOP OF RAIL APPLICATOR BARS, SENSORS, AND ENCLOSURES ARE TO BE INSTALLED PER MANUFACTURER'S REQUIREMENTS.

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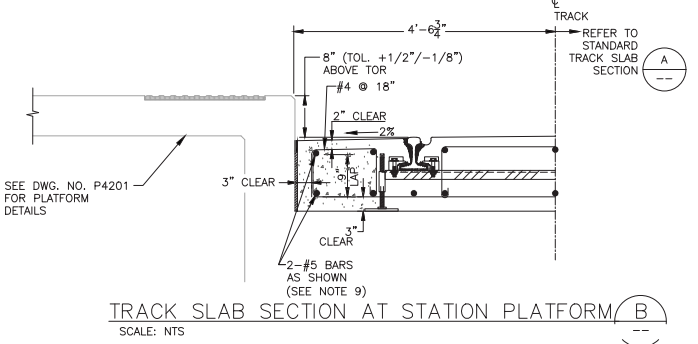
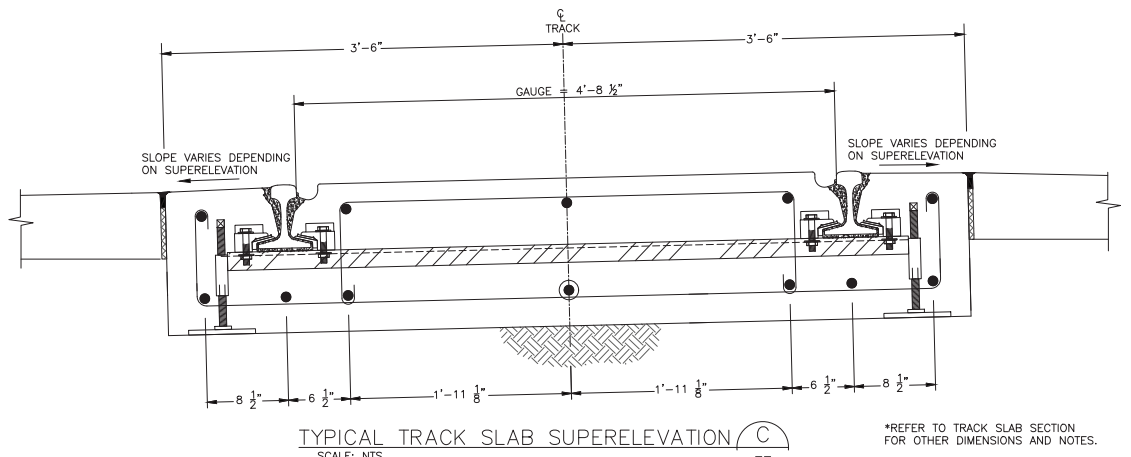
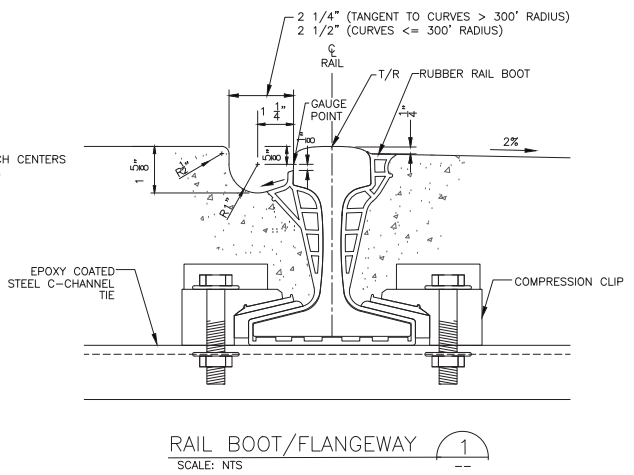
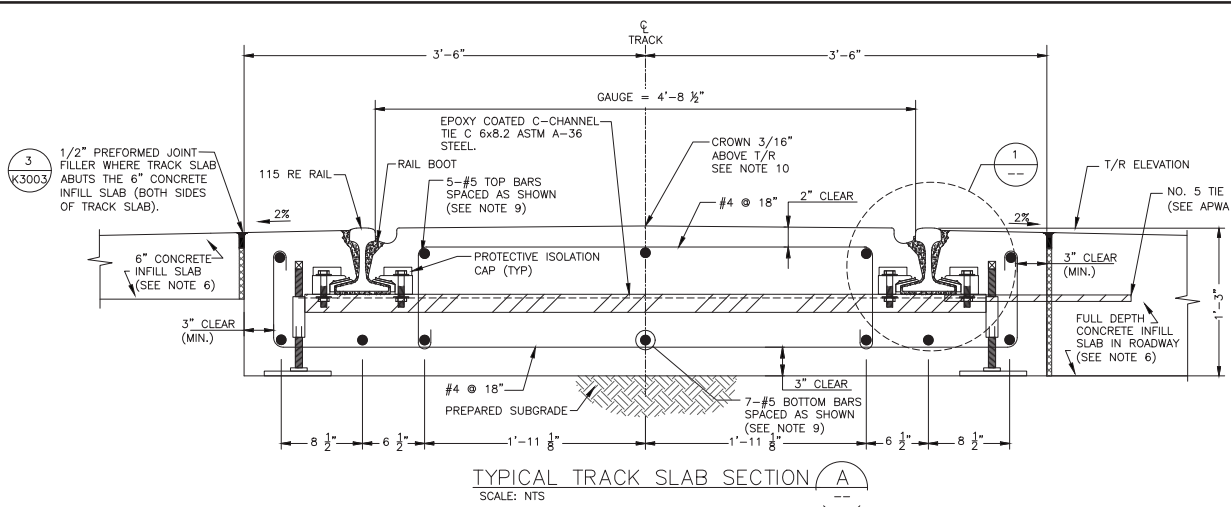
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Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Designed By: F. NANNENGA
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

<b>S-LINE EXTENSION PROJECT</b>  STANDARD DRAWINGS  TRACKWORK RAIL LUBRICATOR	State: NTS
	CADD Filename: SL-C-K-3-001.dwg
	Submitted Date: 01-16-26
	UTA Contract No.: 24-03849
Drawing No.: K3001	Sheet No.:



- THE SHAPE OF THE TRACK SLAB SHALL BE MAINTAINED AS SHOWN, BUT ROTATED AS A UNIT IN AREAS OF SUPERELEVATION.
- ROUND ALL CORNERS ALONG THE TOP SURFACE OF SLAB TO 1/2" RADIUS.
- ALL REBAR IN TRACK SLAB SHALL BE EPOXY COATED AND SHALL HAVE A TENSILE STRENGTH OF 60 KSI.
- ALL CONCRETE IS TO HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
- SHOULD THE CONTRACTOR ENCOUNTER UNSUITABLE OR SOFT MATERIAL DURING TRACK SLAB EXCAVATION, SEE SPECIFICATION 02225 SECTION 3.1.D.
- A HOT Poured JOINT SEALANT IS TO BE PLACE FOR THE TOP 2 INCHES OF THE PREFORMED JOINTS ACCORDING TO ASTM D-3405 OR APPROVED EQUAL. SEE DETAIL 3 ON DWG K3003.
- PROVIDE EXPANSION JOINTS EVERY 60' MAXIMUM IN THE CONCRETE INFILL. SEE NOTES ON JOINT DETAIL 1 ON DWG K3003.
- TRACK GAUGE SHALL BE THE STANDARD GAUGE OF 4'-8.5", MEASURED BETWEEN THE INNER (GAUGE) SIDES OF THE HEADS OF THE RAILS AT A DISTANCE OF 5/8" BELOW THE TOP OF THE RAILS.
- LAP LENGTH FOR #5 LONGITUDINAL BARS SHALL BE 1'-6"
- THE TRACK SLAB'S SURFACE THROUGH INTERSECTIONS SHALL BE FINISHED TO TOP OF RAIL AND SHOULD NOT INCLUDE A CROWN AND 2% SIDE SLOPES AS SHOWN IN SECTION A. ALSO, THE TRACK SLAB SHALL BE FINISHED TO TOP OF RAIL AND NOT 1/4" LOW AS DEPICTED IN DETAIL 1.
- SEE SPECIFICATIONS 02462 - EMBEDDED TRACK CONSTRUCTION AND 03310 - CONCRETE WORK FOR CONCRETE FINISH REQUIREMENTS.

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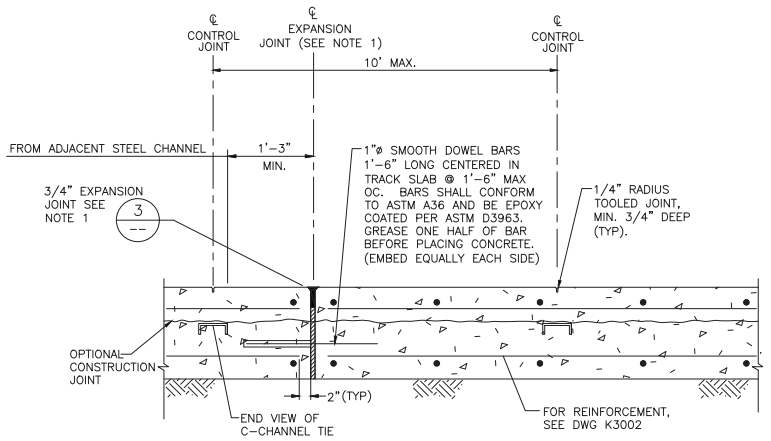
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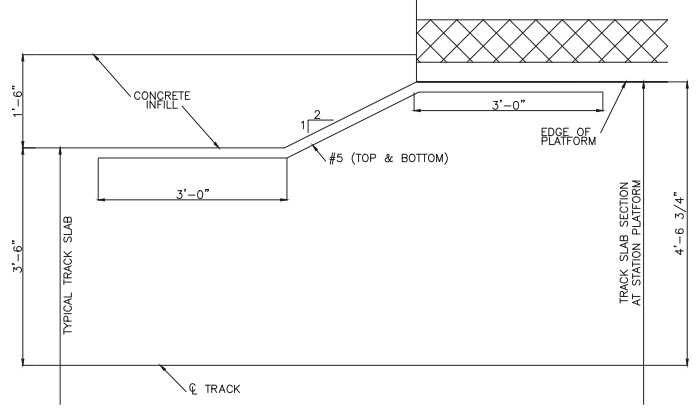
Designed By:  
F. NANNENGA  
Drawn By:  
T. COVEYOU  
Checked By:  
A. DIGIROLAMO  
Approved By:  
A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
TRACK SLAB TYPICAL SECTION

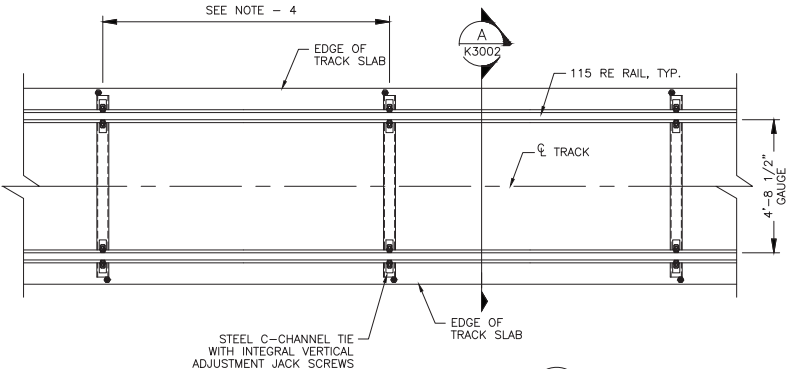
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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3002
Sheet No.:	



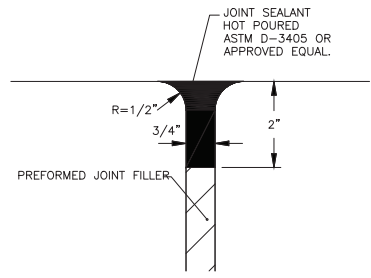
TRACK SLAB EXPANSION JOINT DETAIL (1)  
SCALE: NTS



EXTENDED WIDTH TRACK SLAB AT PLATFORMS - PLAN (4)  
SCALE: NTS



TRACK SLAB FASTENING SYSTEM - PLAN (2)  
SCALE: NTS



JOINT SEALANT DETAIL (3)  
SCALE: NTS

NOTES:

- EXPANSION JOINT SPACING 60' MAXIMUM.
- CONTROL JOINTS TO BE SPACED EVENLY BETWEEN EXPANSION JOINTS AND SHALL BE NO FURTHER APART THAN 10 FEET. CONTROL JOINT SHALL BE LOCATED OVER C-CHANNEL CROSS-TIES.
- FOR TRACK SLAB SECTION, SEE DWG. K3002.
- C-CHANNEL TIES ARE TO BE SPACED AT 10'-0" O.C. IN TANGENT AND CURVES > 500 FT. RADIUS OR AS REQUIRED FOR PROPER SETTING OF LINE, GAUGE AND GRADE, PRIOR TO AND DURING PLACEMENT OF CONCRETE. SPACING SHALL BE 5'-0" O.C. 10'-0" PRIOR TO TS AND END 10'-0" AFTER ST FOR CURVES <= 500 FT. RADIUS.
- STEEL TIE SHALL BE FABRICATED FROM C 6X8.2# WITH 3/16" THICK WEB AND WITH TWO 3/8" HOLES FOR TRAPPED AIR TO ESCAPE. TIES SHALL BE COATED WITH EPOXY COATING.

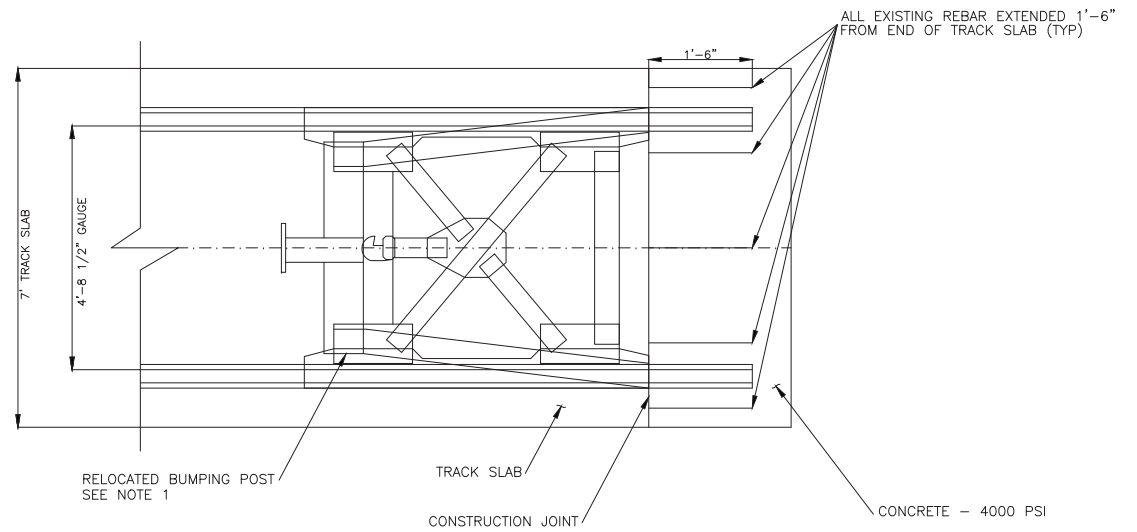
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Designed By: F. NANNENGA
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
TRACK SLAB DETAILS

State: NTS
CADD Filename: SL-C-K-3-003.dwg
Submitted Date: 01-16-26
UTA Contract No.: 24-03849
Drawing No.: K3003
Sheet No.:



RAIL END DETAIL  
SCALE: NTS

- NOTES:  
1. ATTACH TO RAIL BEFORE POURING TRACK SLAB

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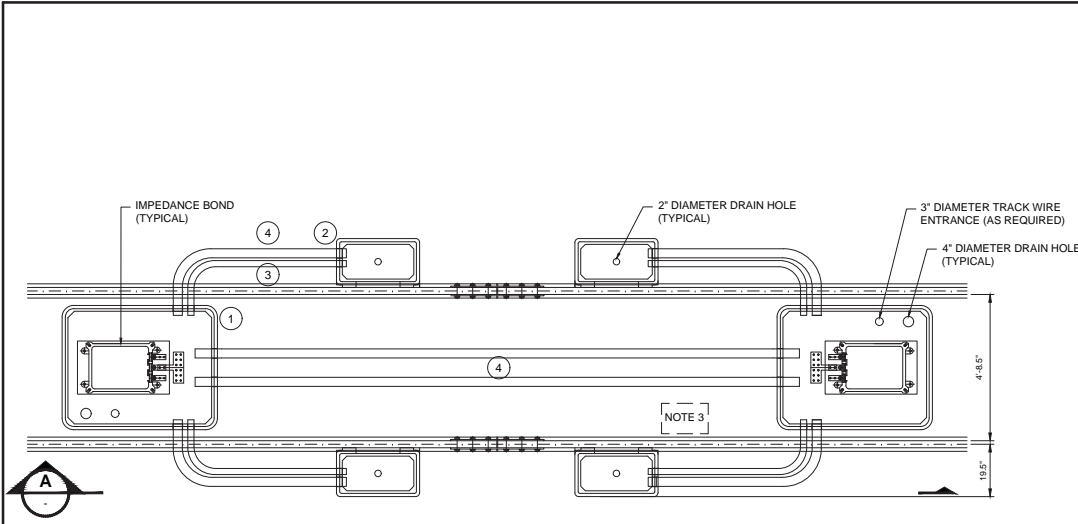
REV	DATE	Description



Designed By:  
F. NANNENGA  
Drawn By:  
T. COVEYOU  
Checked By:  
A. DIGIROLAMO  
Approved By:  
A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
MISC DETAILS

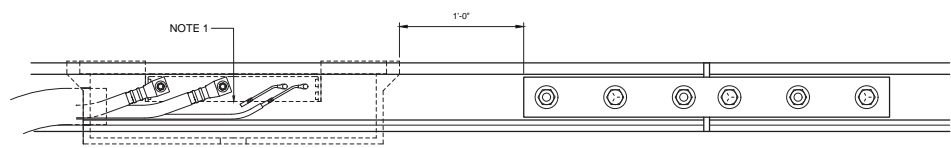
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UTA Contract No.:	24-03849
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Sheet No.:	



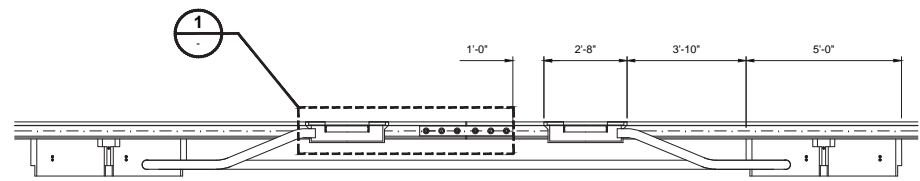
**TRACK AND IMPEDANCE BOND BOX ARRANGEMENT**  
SCALE: 1" = 1'-0"

TRACK BOX AND CONDUIT MATERIAL SUMMARY			
REF	DESCRIPTION	MANUFACTURER*	MANUFACTURER PART NUMBER*
1	60" x 48" x 20" EMBEDDED TRACK BOX ASSEMBLY	ARMORCAST PRODUCTS CO.	A6004860A-S
2	32" x 15" x 8" EMBEDDED TRACK BOX ASSEMBLY	ARMORCAST PRODUCTS CO.	A6003014A
3	2" PVC CONDUIT SCH. 40	VARIOUS	VARIOUS
4	3" PVC CONDUIT SCH. 40	VARIOUS	VARIOUS

\* SUGGESTED



**PIN BRAZED TRACK CIRCUIT CONNECTIONS  
BOLTED POWER RETURN CONNECTIONS**  
SCALE: NTS



**SIGNAL TRACK CONNECTION BOX**  
SCALE: 1" = 1'-0"

NOTES:

1. INCREASE CUT-OUT SIZE TO ACCOMMODATE POWER RETURN CONNECTIONS. ENLARGED CUT-OUT PART OF PURCHASED BOX OR MODIFY BOX IN FIELD.
2. TRACK CONNECTIONS SPACING BASED ON AREMA AND MANUFACTURER RECOMMENDATIONS AND ADJUSTED TO FIT TRACK CONNECTION ACCESS CUTOUT IN TRACK BOX.
3. AXLE COUNTER WHEEL SENSOR BOX AS REQUIRED BY DESIGN. LOCATION MAY DIFFER FROM THAT SHOWN ON THIS DRAWING.
4. IMPEDANCE BOND BOX MAY BE LOCATED OUTSIDE OF TRACK AREA AS REQUIRED BY DESIGN.

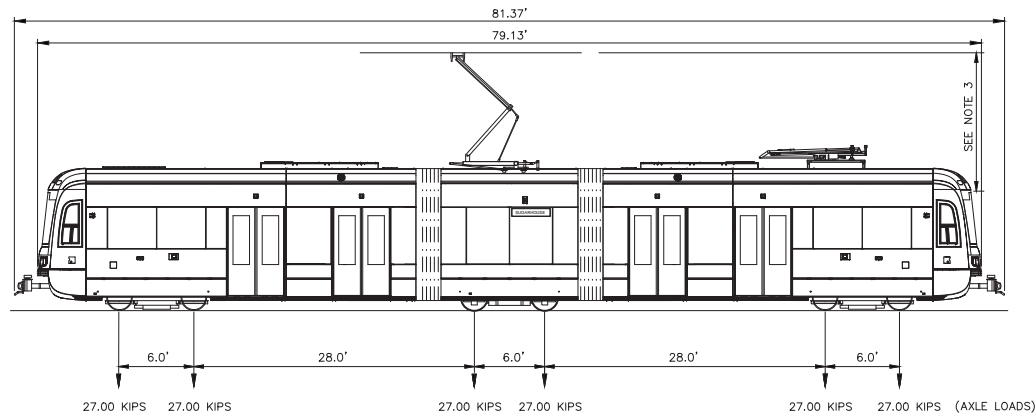
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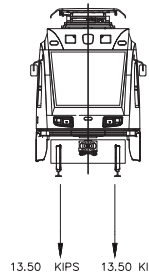
Designed By: J. MITCHELL
Drawn By: J. MITCHELL
Checked By: D. NGUYEN
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
SIGNAL TRACK CONNECTION BOX

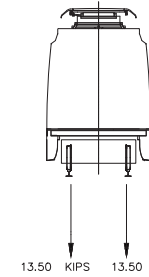
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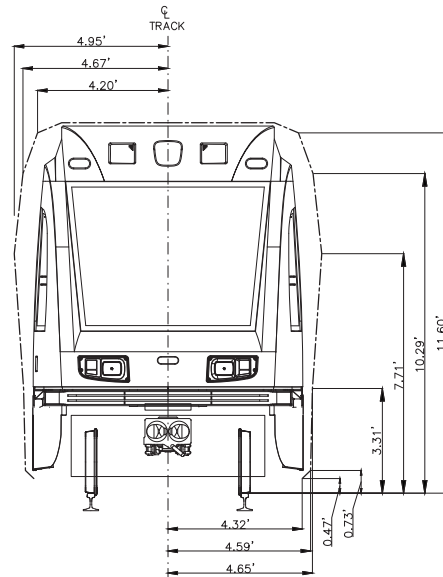
LRV LENGTH AND LOADING DIAGRAM



WHEEL LOADS (END TRUCKS)



WHEEL LOADS (CENTER TRUCKS)



STREET CAR DYNAMIC CLEARANCE ENVELOPE

Radius (ft)	Mid-Overhang (ft)	End Overhang (ft)
82	5.62	6.13
100	5.50	5.89
150	5.31	5.53
200	5.22	5.35
250	5.16	5.27
300	5.13	5.21
350	5.10	5.18
400	5.08	5.15
450	5.06	5.12
500	5.06	5.11
600	5.03	5.08
700	5.02	5.06
800	5.01	5.05
900	5.00	5.03
1000	5.00	5.02
2000	4.97	4.98
3000	4.96	4.97
4000	4.96	4.96
5000	4.95	4.96
10000	4.95	4.95

STREET CAR DYNAMIC CLEARANCE TABLE

NOTES:

- TOTAL VEHICLE LOAD - 162.00 KIPS = 81.00 TONS.
- SIEMENS S70 LRV SHOWN WITH UTA LRT DESIGN CRITERIA REVISION 5 LOADING INFORMATION.
- CLEARANCE ENVELOPE IS FOR TANGENT TRACK ONLY.

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12/21/2025		
REV	DATE	Description



Designed By:  
F. NANNENGA

Drawn By:  
T. COVEYOU

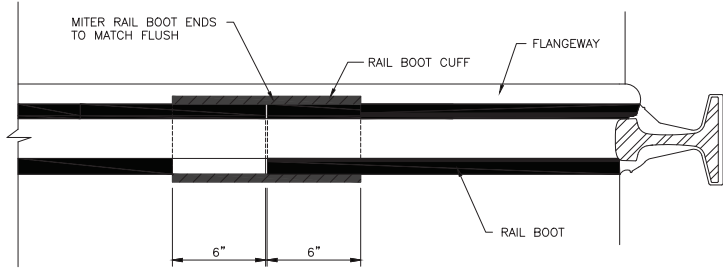
Checked By:  
A. DIGIROLAMO

Approved By:  
A. DIGIROLAMO

S-LINE EXTENSION PROJECT

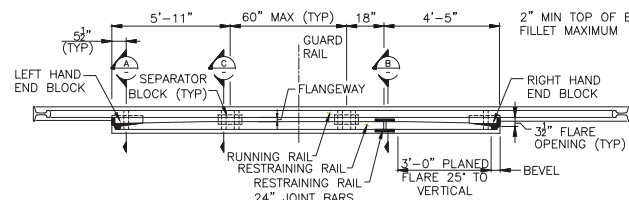
STANDARD DRAWINGS  
TRACKWORK  
STREETCAR DYNAMIC ENVELOPE  
AND LOADING DIAGRAM

State:	NTS
CADD Filename:	SL-C-K-3-006.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3006
Sheet No.:	



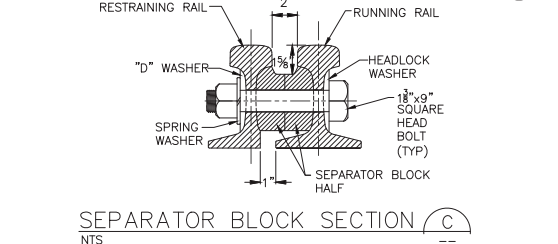
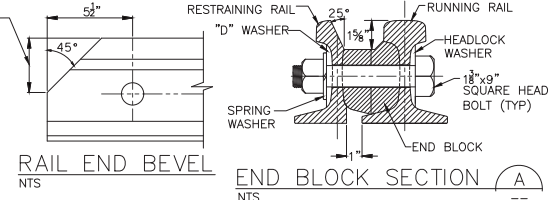
**RAIL BOOT SPLICE**  
NTS

- NOTES:**
1. INSTALL CUFF IN ACCORDANCE WITH MANUFACTURER'S PROCEDURES. ENSURE CONTINUOUS ELECTRICAL ISOLATION IS ACHIEVED ACROSS THE SPLICE.
  2. THE SPLICE SHALL BE INSTALLED IN A MANNER THAT PERMITS ANY WATER FLOWING IN THE BOOT TO PASS THROUGH SPLICE.

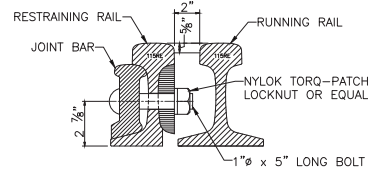


**SCHEMATIC RESTRAINING RAIL**  
NTS

RESTRAINING RAILS COMPLETE	
BILL OF MATERIAL	
QUANTITY	DESCRIPTION
1	TEE RESTRAINING RAIL, USED 115RE RAIL
1 EA	END BLOCKS, LEFT AND RIGHT HAND
AS REQ'D	ADJUSTABLE SEPARATOR BLOCK
AS REQ'D	BOLTS 1 3/8" DIA X 9" GRADE 8 WITH SQUARE HEAD, SQUARE NUTS, SPRING WASHERS, HEADLOCKS, AND "D" WASHERS
AS REQ'D	RESTRAINING RAIL PLATES COMPLETE WITH ADJUSTABLE SPRING CLIP SHOULDERS



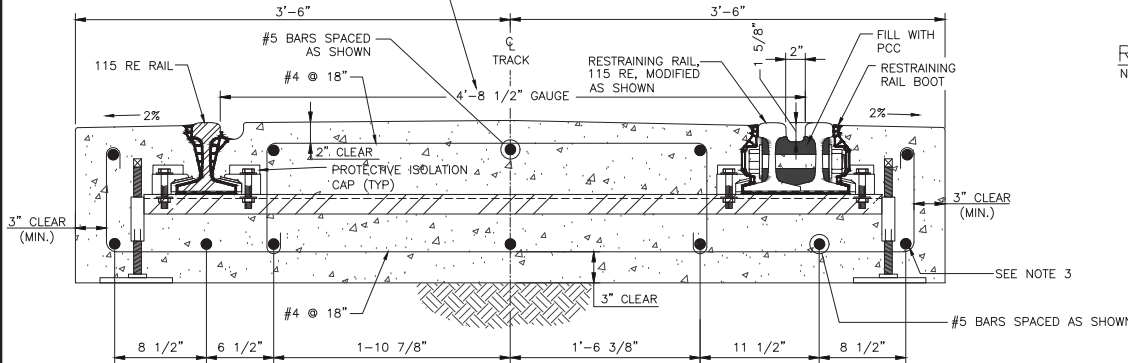
- NOTES:**
1. ENDS OF RESTRAINING RAIL AND END OF BLOCKS SHALL BE BEVELLED AT A 45° ANGLE PER AREMA PLAN 504-89.
  2. END AND SEPARATOR BLOCKS MAY BE OF STEEL, CAST IRON OR DUCTILE IRON.



**RESTRAINING RAIL JOINT ASSEMBLY**  
NTS

- NOTES:**
1. PROVIDE EXPANSION JOINTS EVERY 60' MAXIMUM IN THE CONCRETE INFILL PER APWA 231 (SIDEWALK SPECIFICATIONS). SEE DETAIL 3 ON DWG K3003.
  2. PROVIDE CONTROL JOINTS AT 10'-0" MAX.
  3. A TRACK SLAB ANALYSIS IS BEING PERFORMED TO DETERMINE IF ANY REBAR IS NEEDED.

4'-8 1/2" GAUGE (TANGENT AND RADIUS >= 280')  
4'-8 3/4" GAUGE (200' < RADIUS < 280')  
4'-9" GAUGE (82' <= RADIUS < 200')



**RESTRAINING RAIL**  
NTS

- NOTES:**
1. AT ALL RESTRAINING RAIL APPLICATIONS, PROVIDE FLANGEWAY OF 2" BETWEEN RUNNING RAIL AND RESTRAINING RAIL, AND INDICATED TRACK GAUGE, AS SHOWN.
  2. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS SHOWING MODIFIED BOOT DETAILS, PRIOR TO INSTALLATION.

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 12/21/2025

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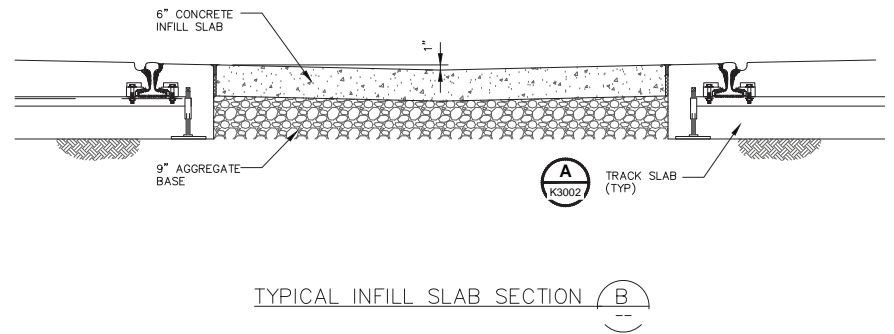
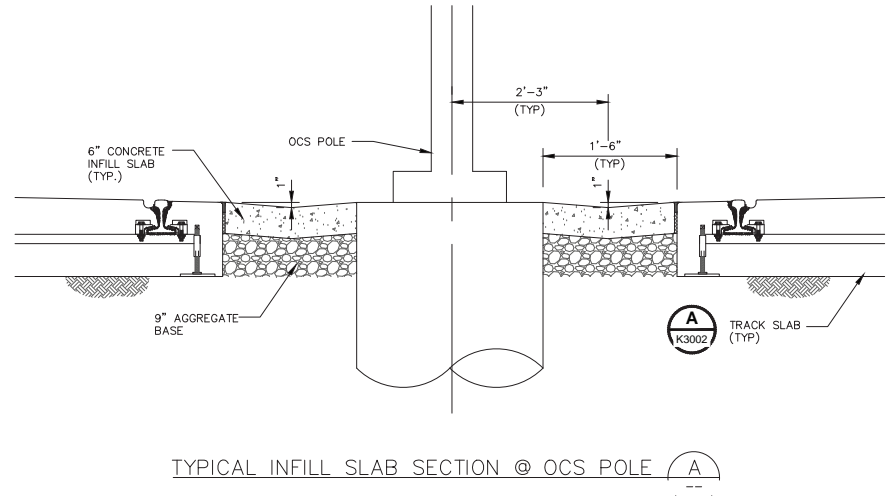
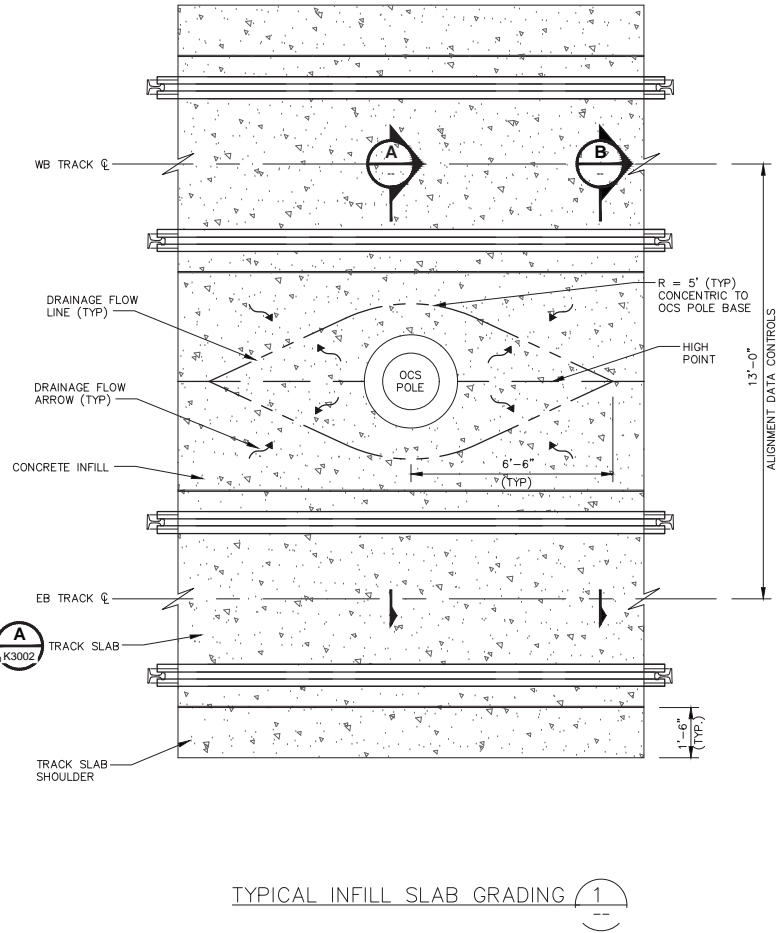


Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Designed By:	F. NANNENGA
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

**S-LINE EXTENSION PROJECT**  
  
STANDARD DRAWINGS  
TRACKWORK  
MISC. EMBEDDED TRACK  
SECTIONS AND DETAILS

State:	NTS
CADD Filename:	SL-C-K-3-007.dwg
Submit Date:	01-16-26
UTA Contract No.:	24-03849
Sheet No.:	K3007



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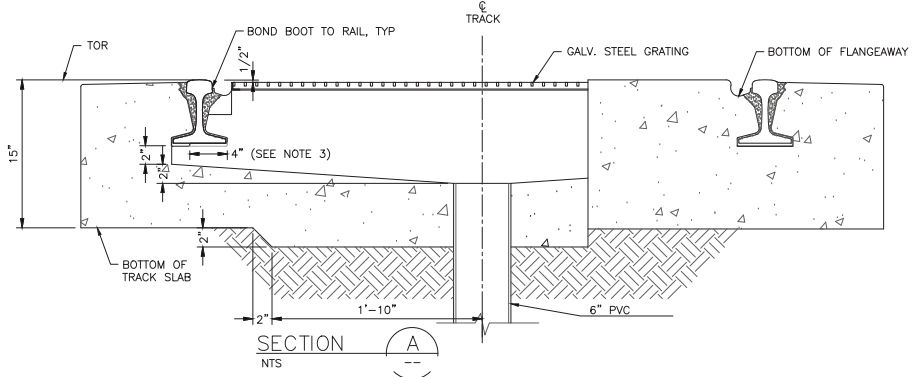
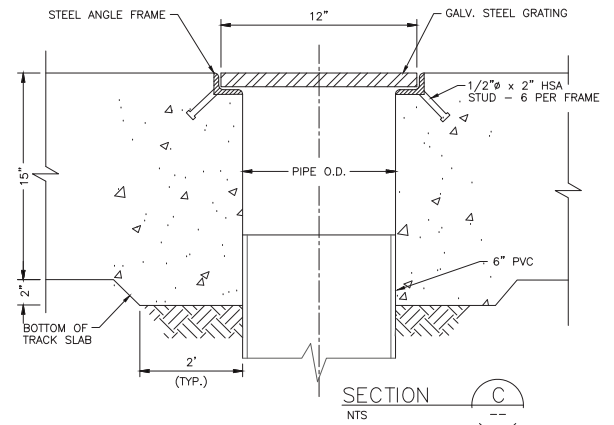
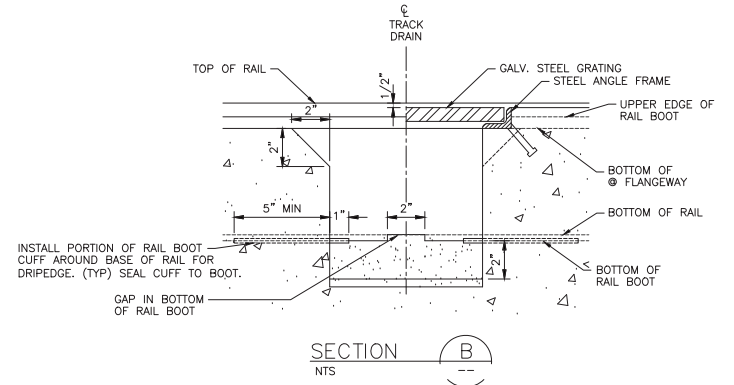
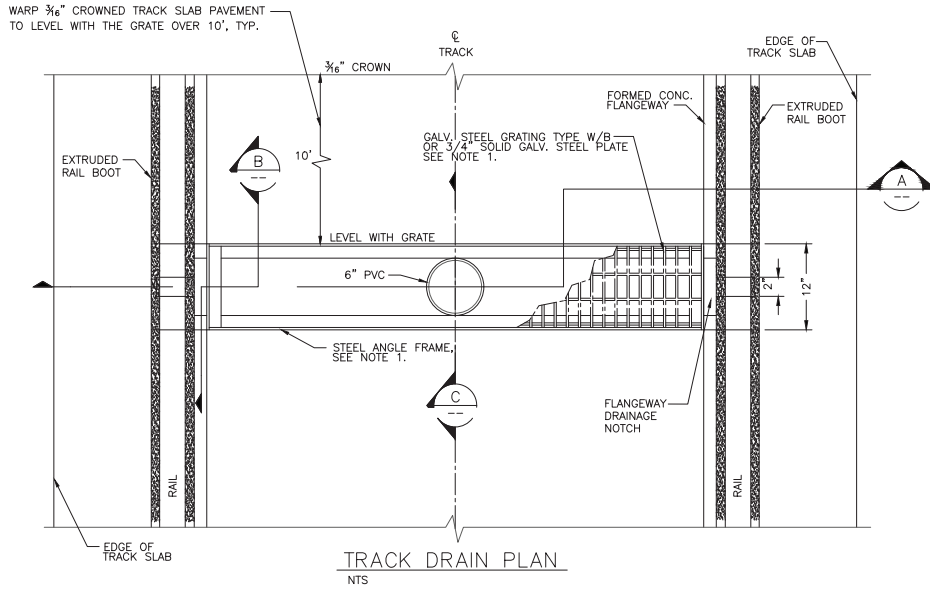
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Designed By:	F. NANNENGA
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
6" INFILL SLAB DETAILS

State:	NTS
CADD Filename:	SL-C-K-3-008.dwg
Submit Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3008
Sheet No.:	



- NOTES:
1. SUBMIT STEEL ANGLE FRAME AND GALVANIZED STEEL GRATING SHOP DRAWINGS TO ENGINEER FOR APPROVAL PRIOR TO FABRICATION. GRATING SHALL BE BOLTED TO THE STEEL ANGLE FRAME AT SIX (MIN) LOCATIONS. STEEL ANGLE FRAME SHALL BE HOT DIPPED GALVANIZED. FRAME AND GRATE SHALL BE DESIGNED TO MEET AASHTO HS-20 LOADING.
  2. INFILL CONCRETE SURFACE WITHIN 18" SHALL BE GRADED TO MATCH TOP OF STEEL GRATING.
  3. REMOVE SOME OF THE BOTTOM OF BOOT AT CENTER OF DRAIN TO ALLOW BOOT DRAINAGE. REMOVED PORTION SHALL BE 2" WIDE.

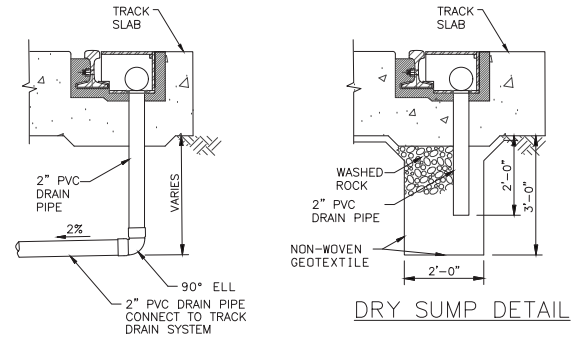
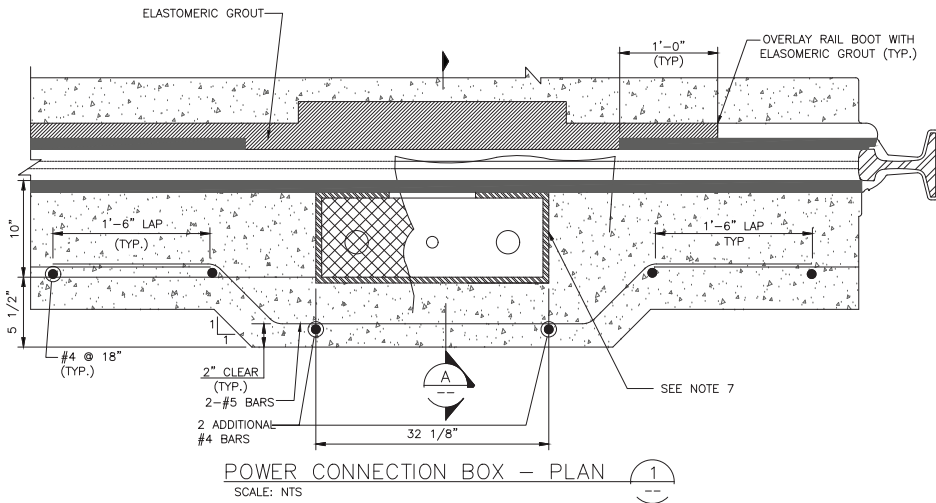
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Designed By:  
F. NANNENGA  
Drawn By:  
T. COVEYOU  
Checked By:  
A. DIGIROLAMO  
Approved By:  
A. DIGIROLAMO

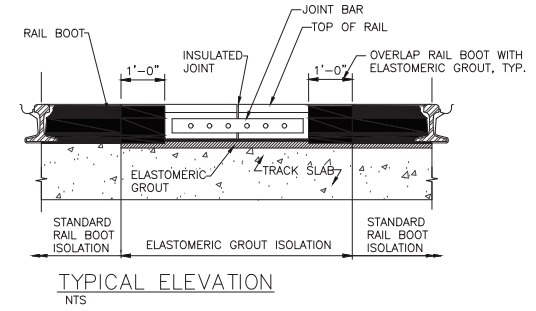
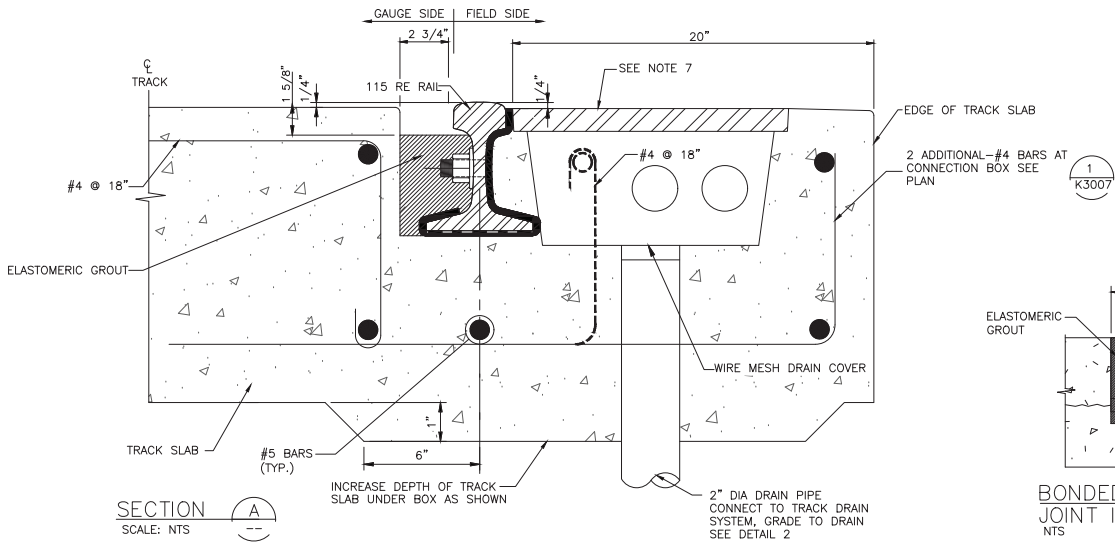
S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
TRACK DRAINAGE DETAILS

State:	NTS
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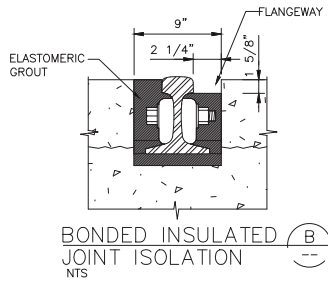


NOTE:  
DRY SUMP CAN BE USED WHEN TRACK DRAIN SYSTEM IS NOT WITHIN 50' OF CONNECTION BOX. SEE DRY SUMP DETAIL.

**CONNECTION TO TRACK DRAIN DETAIL (2)**  
SCALE: NTS



- NOTES:**
- SEE DUCTBANK PLANS FOR LOCATIONS OF SIGNAL TRACK CONNECTION BOXES.
  - CONDUIT ENTRANCE HOLE DIAMETER, QUANTITY, AND LOCATION MAY VARY ACCORDING TO LOCATION OF BOX. SEE SYSTEMS PLANS FOR CONDUIT REQUIREMENTS FOR EACH INDIVIDUAL BOX.
  - SIGNAL CONNECTION BOX OR EQUIVALENT SHALL BE CONSTRUCTED AS SHOWN; CONTRACTOR SHALL SUBMIT SIGNAL CONNECTION BOX SHOP DRAWING FOR REVIEW.
  - REINFORCING SHOWN ON THIS DRAWING SHOWS REQUIRED MODIFICATIONS. REFER TO DWG. K3004 FOR STD. TRACK SLAB REINFORCING DETAILS.
  - WHEN SIGNAL TRACK BOX IS USED FOR RAIL LUBRICATOR, THE 1" DIA HOLE SHOULD BE EXPANDED TO 2" DIA. ONLY THE REQUIRED HOLES SHOULD BE DRILLED.
  - FINAL TRACK CIRCUIT CONNECTION BOX LOCATION TO BE LOCATED AND COORDINATED WITH SYSTEMS CONTRACTOR.
  - POWER CONNECTION BOX, SEE DWG. K3005 FOR DETAILS.



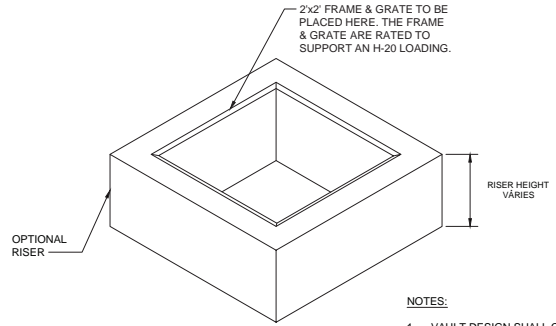
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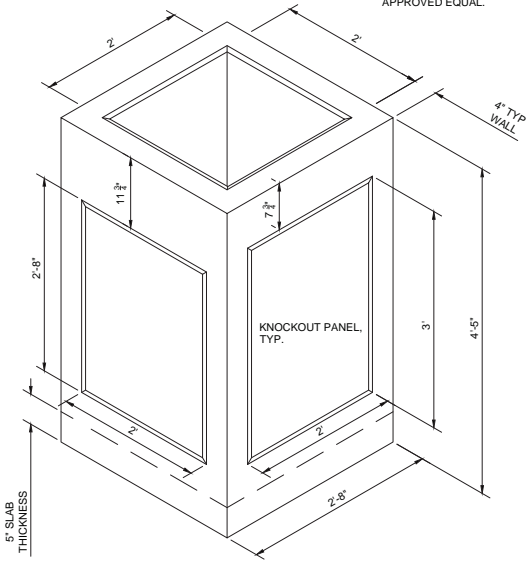


Designed By: F. NANNENGA
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

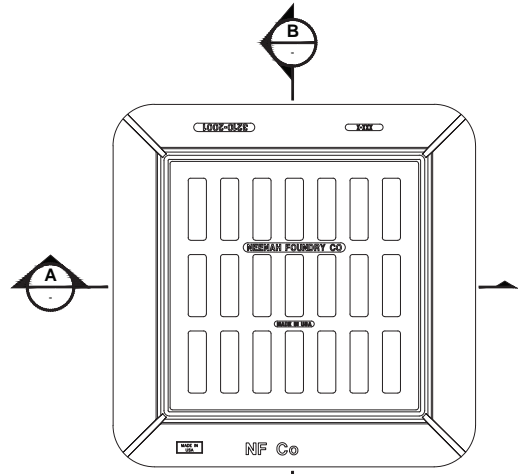
S-LINE EXTENSION PROJECT	
STANDARD DRAWINGS	
TRACKWORK	
TRACK CONNECTION BOX	
Submitted Date: 01-16-26	Sheet No. K3010



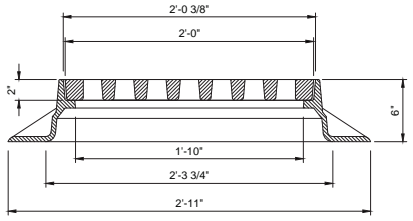
- NOTES:
1. VAULT DESIGN SHALL COMPLY WITH ASTM C-458 & C-913 WITH LESS THAN 2' OF EARTH COVER AND AN AASHTO HS-20 LOADING.
  2. INLET SHOWN IS FROM GENEVA PIPE/NORTHWEST PIPE COMPANY OR APPROVED EQUAL.



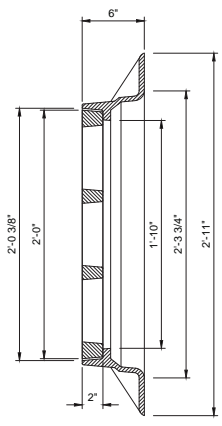
**PRECAST CONCRETE CATCH BASIN 2'x2'x4'** 1  
NTS



**2'x2' FRAME AND GRATE** 2  
2' = 1'-0"



**SECTION A**  
2' = 1'-0"



**SECTION B**  
2' = 1'-0"

- NOTES:
1. GRATE SHOWN IS FROM NEENAH FOUNDRY CATALOG #3433.
  2. GRATE IS AASHTO RATED FOR HS-20 LOADING.
  3. ALL DIMENSIONS SHOWN ARE IN ENGLISH.
  4. COMPONENT NO'S: FRAME 3210-2001, GRATE 3210-0002.
  5. MATERIAL: CAST GRAY IRON ASTM A-48, CLASS 35B.
  6. FINISH: NO PAINT.
  7. WEIGHT: FRAME 164#, GRATE 166#.

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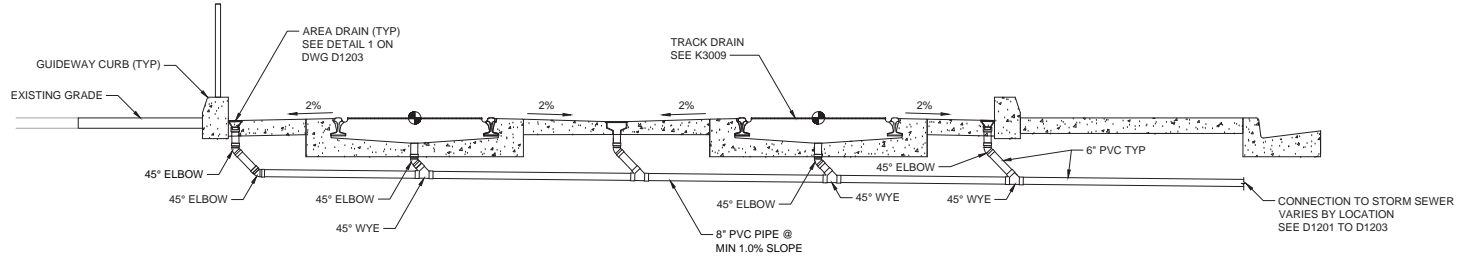
12/21/2025	REV	DATE	Description



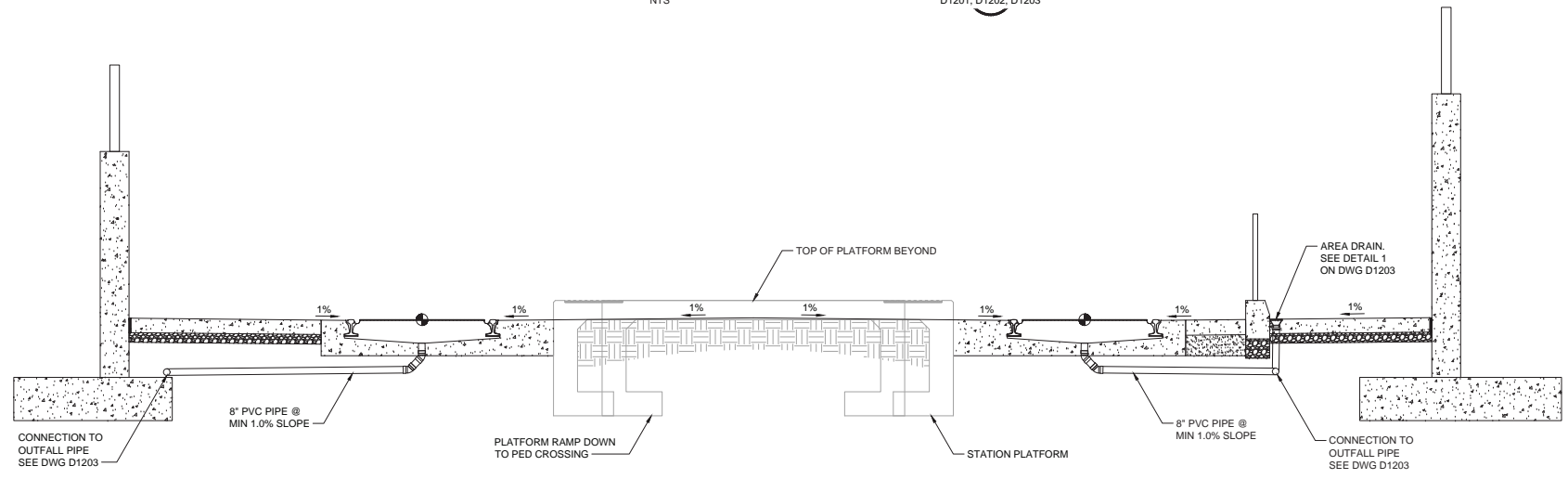
Designed By:  
F. NANNENGA  
Drawn By:  
T. COVEYOU  
Checked By:  
A. DIGIROLAMO  
Approved By:  
A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
DRAINAGE CATCH BASIN

State:	AS NOTED
CADD Filename:	SL-C-K-3-011.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3011
Sheet No.:	



**TYPICAL TRACK DRAIN ARRAY DETAIL 1**  
 NTS D1201, D1202, D1203



**DRAIN DETAIL AT SUGAR HOUSE STATION 2**  
 NTS D1203

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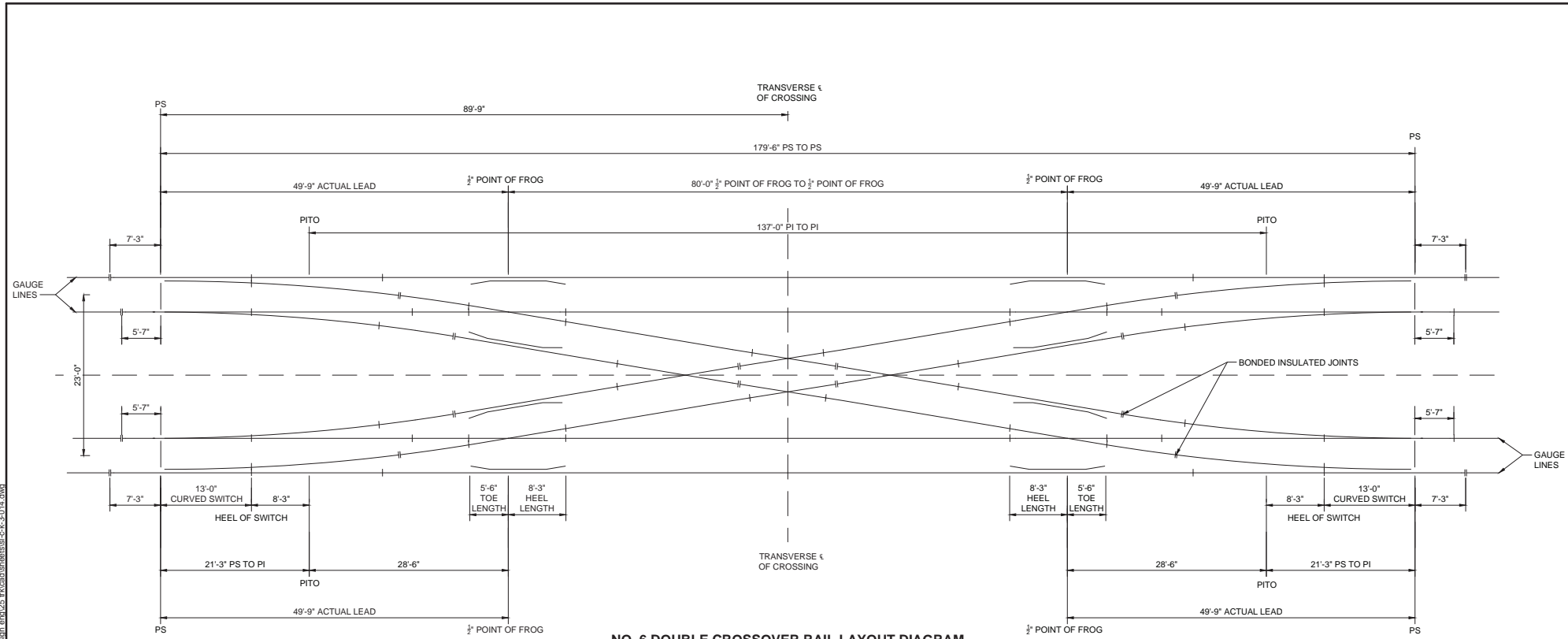
Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Designed By:	F. NANNENGA
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
 STANDARD DRAWINGS  
 TRACKWORK  
 TRACK DRAIN ARRAY  
 AND PLATFORM DRAIN DETAILS

State:	NTS
CADD Filename:	SL-C-K-3-012.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3012
Sheet No.:	





**NO. 6 DOUBLE CROSSOVER RAIL LAYOUT DIAGRAM**

LEGEND:

- ||| BONDED INSULATED JOINTS  
(CONTRACTOR RESPONSIBLE FOR  
LOCATING INSULATED JOINTS IN THE  
FIELD FOR THE JOINTS NOT PROVIDED BY  
THE SUPPLIER)
- | FIELD WELDED JOINT

NOTES:

1. ALL RAIL AND COMPONENTS SHALL CONFORM TO THE 115RE RAIL SECTION.
2. ALL RAILS ARE HIGH STRENGTH.
3. CLOSURE RAIL LENGTHS ALLOW 1" FOR EACH WELDED JOINT AND 3/8" FOR EACH INSULATED JOINT.
4. ALL SPECIAL TRACKWORK CROSSOVER PLATES MUST BE INSULATED.
5. BONDED INSULATED JOINTS VARY BY LOCATION. PLACE JOINTS IN ACCORDANCE WITH TRAIN CONTROL DRAWINGS.
6. SUPPLY COMPLETE WITH RAIL CLIPS, SHOULDER, INSULATORS, AND PADS.
7. EXACT DIMENSIONS, LIMITS, SPACING, AND DETAILS OF CROSSOVER TO BE DETERMINED BY SUPPLIER CROSSOVER SHOP DRAWING.
8. SEE DRAWING K3018 FOR M-23A SWITCH MACHINE LAYOUT.

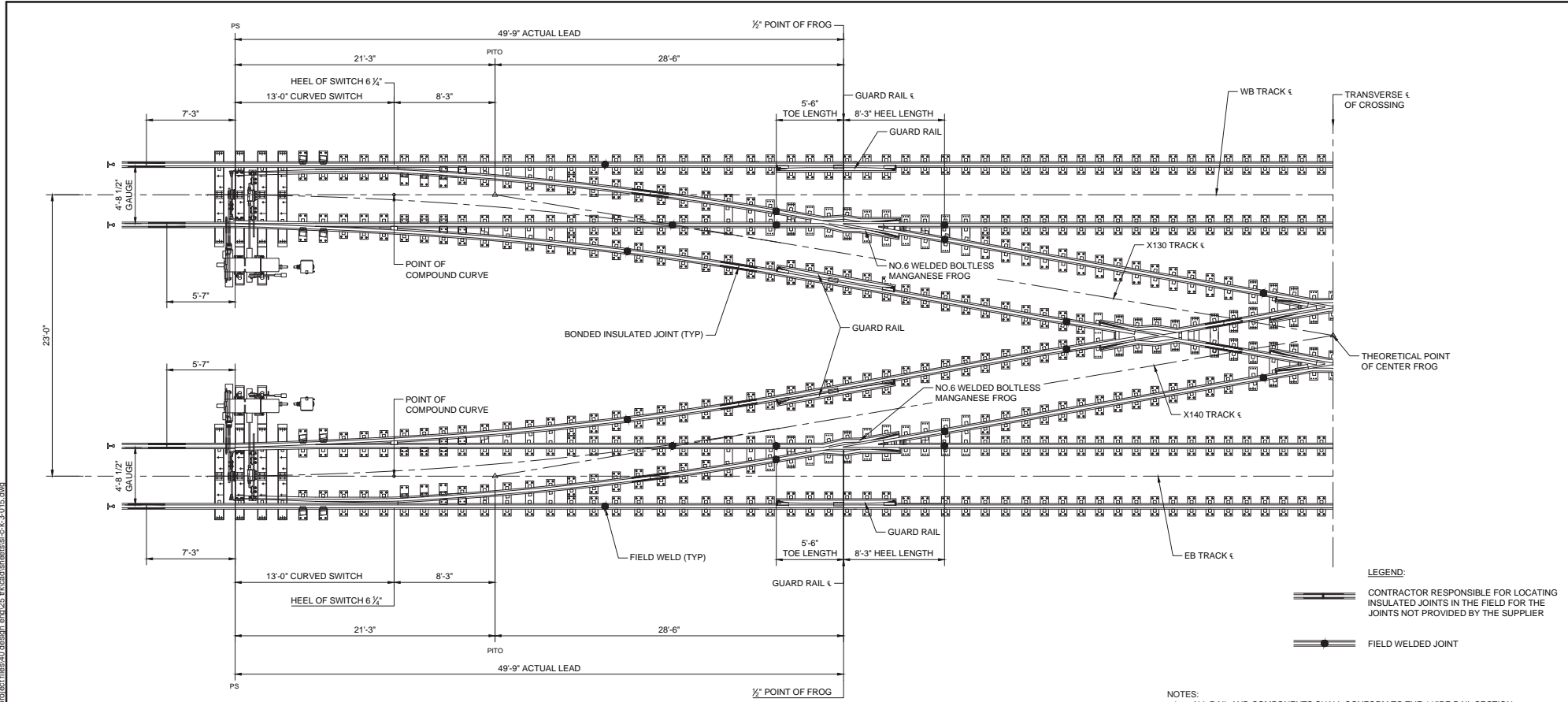
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Designed By:	F. NANNENGA
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
NO. 6 DOUBLE CROSSOVER RAIL LAYOUT

State:	NTS
CADD Filename:	SL-C-K-3-014.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3014
Sheet No.:	



**NO. 6 DIRECT FIXATION DOUBLE CROSSOVER 23'-0" TRACK CENTERS**

- LEGEND:**
- CONTRACTOR RESPONSIBLE FOR LOCATING INSULATED JOINTS IN THE FIELD FOR THE JOINTS NOT PROVIDED BY THE SUPPLIER
  - FIELD WELDED JOINT

- NOTES:**
1. ALL RAIL AND COMPONENTS SHALL CONFORM TO THE 115RE RAIL SECTION.
  2. ALL RAILS ARE HIGH STRENGTH.
  3. CLOSURE RAIL LENGTHS ALLOW 1" FOR EACH WELDED JOINT AND 3/4" FOR EACH INSULATED JOINT.
  4. THE DESIGN CONFIGURATION AND LAYOUT ARE FOR INFORMATIONAL PURPOSES ONLY. FINAL DIMENSIONS, LIMITS, GEOMETRY, DETAILS, AND FASTENER SPACING, TO BE DETERMINED BY SUPPLIER TURNOUT/CROSSOVER SHOP DRAWING.
  5. STANDARD SPECIAL TRACKWORK DIRECT FIXATION FASTENERS SHALL BE FURNISHED WHERE SPECIAL TRACKWORK FASTENERS ARE NOT REQUIRED.
  6. ALL SPECIAL TRACKWORK TURNOUT PLATES MUST BE INSULATED.
  7. BONDED INSULATED JOINTS VARY BY LOCATION. PLACE JOINTS IN ACCORDANCE WITH TRAIN CONTROL DRAWINGS.
  8. SUPPLY COMPLETE WITH RAIL CLIPS, SHOULDER, INSULATORS, AND PADS.
  10. CROSSOVER IS SYMMETRICAL ABOUT TRANSVERSE CL.
  11. SEE DRAWING K3018 FOR M-23A SWITCH MACHINE LAYOUT.

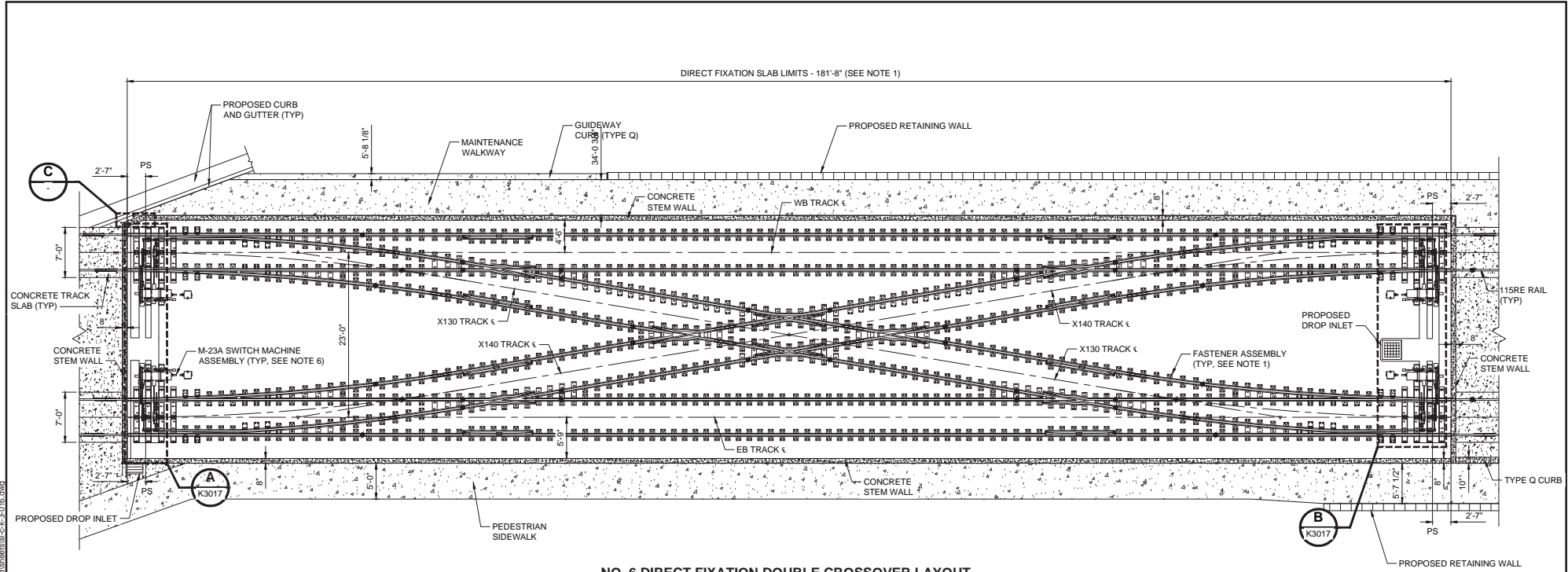
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Approved By:  
A. DIGIROLAMO

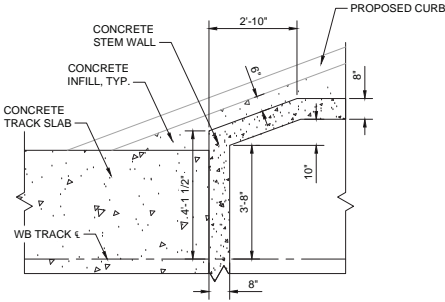
S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
NO. 6 DOUBLE CROSSOVER GEOMETRY  
AND LAYOUT 23'-0" TRACK CENTERS

State:	NTS
CADD Filename:	SL-C-K-3-015.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3015
Sheet No.:	





**NO. 6 DIRECT FIXATION DOUBLE CROSSOVER LAYOUT**

NTS



**SLAB CORNER NORTH WEST SIDE**  
SCALE: 1" = 2'

- LEGEND:**
-  CONTRACTOR RESPONSIBLE FOR LOCATING INSULATED JOINTS IN THE FIELD FOR THE JOINTS NOT PROVIDED BY THE SUPPLIER
  -  FIELD WELDED JOINT

- NOTES:**
1. THE DESIGN CONFIGURATION AND LAYOUT ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY. FINAL LIMITS OF CROSSOVER, DIMENSIONS, DETAILS AND FASTENER LOCATION AND SPACING TO BE DETERMINED BY SUPPLIER CROSSOVER SHOP DRAWING.
  2. CONTRACTOR TO ADJUST THESE DIMENSIONS AFTER SUPPLIED WITH FULL SET OF TURNOUT SHOP DRAWINGS FROM THE SUPPLIER.
  3. STANDARD DIRECT FIXATION FASTENER TO BE USED WHERE SPECIAL TRACKWORK FASTENERS ARE NOT REQUIRED.
  4. DRAINAGE BLOCKOUTS TO BE PLACED AT THE END OF DIRECT FIXATION AREA AND ADJACENT TO SWITCH MACHINE TO COLLECT RUNOFF WATER. BLOCKOUTS TO NOT BE PLACED IN CONFLICT WITH FASTENING SYSTEM.
  5. SEE DRAWING K3017 FOR DRAINAGE BLOCKOUTS DETAIL.
  6. SEE DRAWING K3018 FOR M-23A SWITCH MACHINE LAYOUT.

REV	DATE	Description

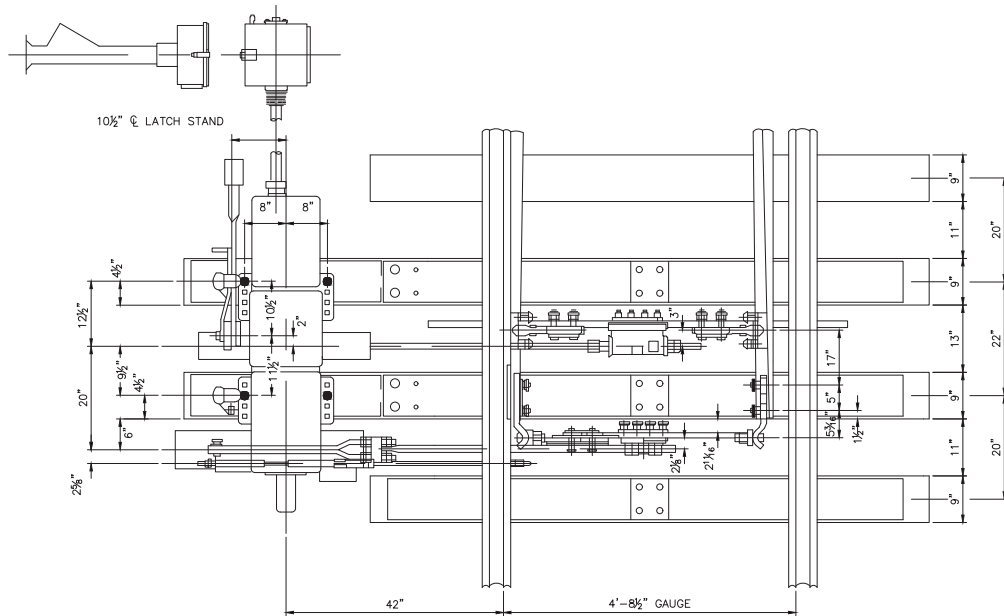


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A. DIGIROLAMO

**S-LINE EXTENSION PROJECT**  
**STANDARD DRAWINGS**  
**TRACKWORK**  
**NO. 6 DIRECT FIXATION DOUBLE CROSSOVER LAYOUT**

State:	AS SHOWN
CADD Filename:	SL-C-K-3-016.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Sheet No.:	K3016





**M-23A SWITCH MACHINE LH LAYOUT**

**NOTES:**

1. M-23A SWITCH MACHINES SHALL BE INSTALLED ON ALL POWER TURNOUTS INDICATED ON TRACK PLAN SHEET.
2. PLAN SHEET INDICATES SIDE PLACEMENT FOR SWITCH MACHINE.
3. GAUGE PLATE EXTENSIONS SHALL BE PROVIDED WITH SWITCH MACHINE.
4. GAUGE PLATES SHALL BE INSTALLED WITH ELASTOMERIC PADS PROVIDED BY SWITCH MACHINE MANUFACTURER.
5. CONTRACTOR TO PLACE ALL FIELD WELDS IN BETWEEN DF CONNECTIONS. ALL FIELD WELDS ARE SHOWN SCHEMATICALLY ON PLANS.
6. LH LAYOUT SHOWN. RH LAYOUT IS MIRROR IMAGE.

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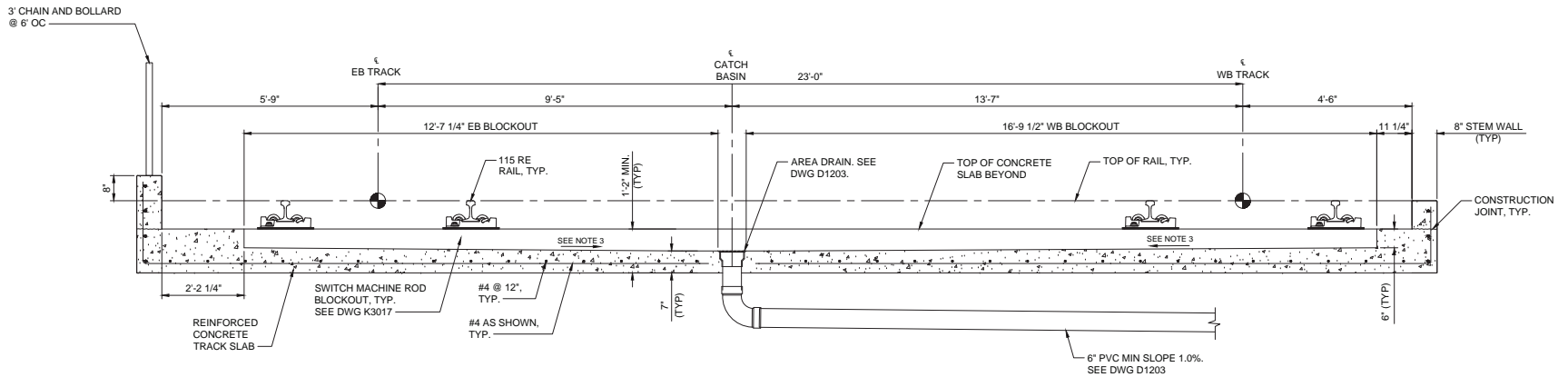
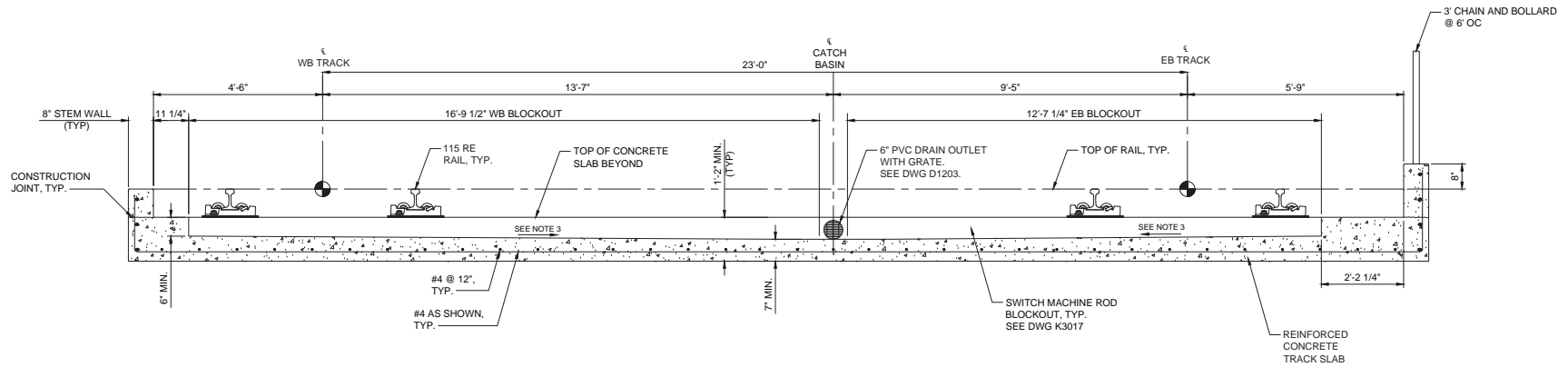


Designed By:  
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A. DIGIROLAMO  
Approved By:  
A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
SPECIAL TRACKWORK M-23A  
SWITCH MACHINE LAYOUT

State:	NTS
CADD Filename:	SL-C-K-3-018.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3018
Sheet No.:	

Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_



- NOTES:
- SEE SHEET K3020 FOR SWITCH ROD BLOCKOUT REINFORCEMENT.
  - SWITCH ROD DEPTH VARIES AS INDICATED IN SECTION. MAINTAIN MINIMUM 6" BLOCKOUT DEPTH.
  - MAINTAIN MINIMUM 0.5% SLOPE TOWARDS CATCH BASIN.

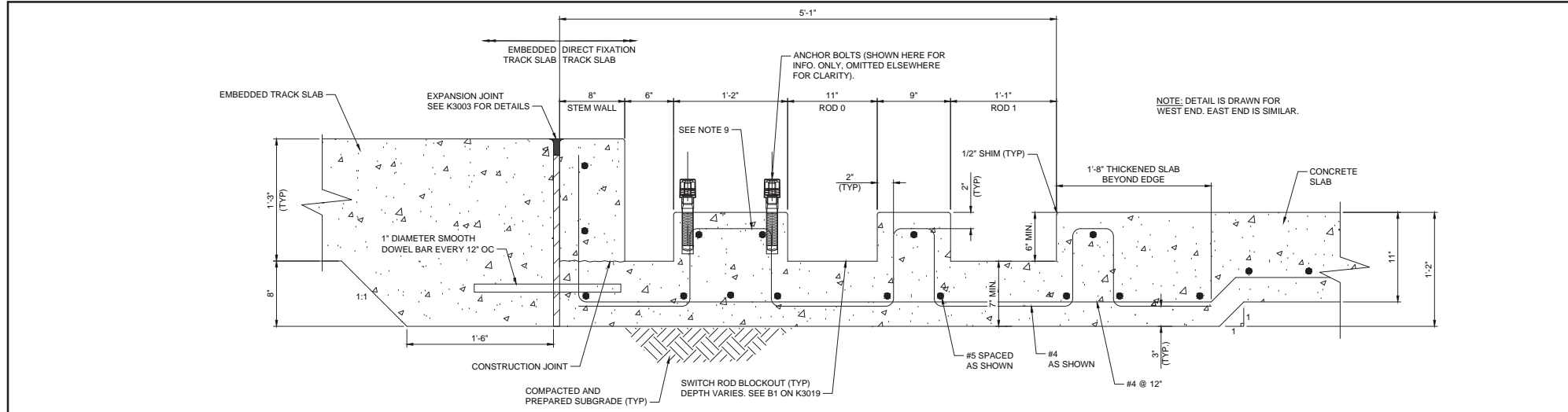
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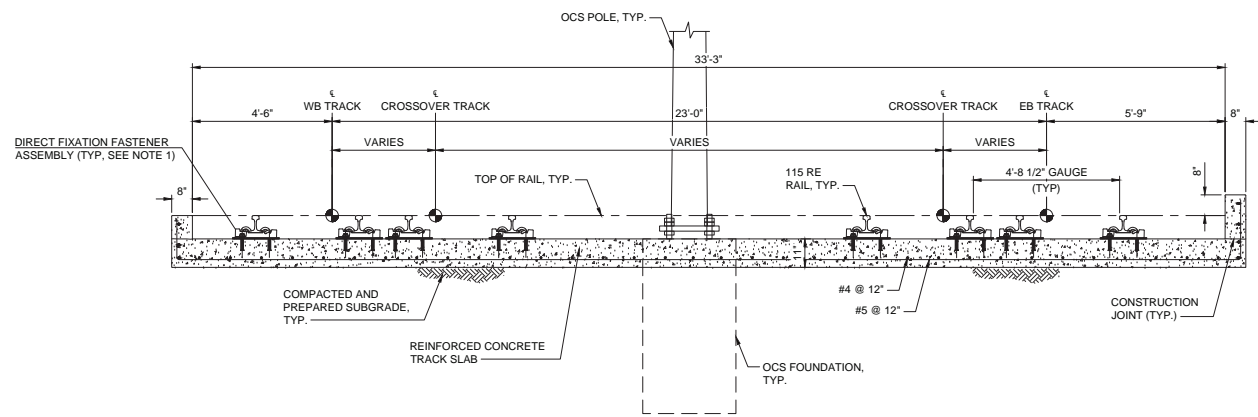
Designed By:	F. NANNENGA
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	A. DIGIROLAMO

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
TRACKWORK  
DF FASTENER DETAIL AND SECTION

Scale:	1" = 1'-6"
CADD Filename:	SL-C-K-3-019.dwg
Submit Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3019
Sheet No.:	



**SECTION - SWITCH ROD BLOCKOUT REINFORCING (A)**  
SCALE: 1" = 0'-6"



**SECTION - DIRECT FIXATION ON SLAB (B)**  
SCALE: 1" = 2'-0"

- NOTES:**
1. FASTENER HARDWARE, DETAILS, SPACING, AND PLACEMENT TO MATCH SPECIAL TRACKWORK SHOP DRAWINGS PROVIDED BY MANUFACTURER.
  2. ANCHOR INSERTS TO BE INSTALLED BY EITHER THE DRILL AND GROUT METHOD OR CAST-IN-PLACE WITH CONCRETE. FINAL CONFIGURATION OF FASTENER MUST ALLOW FOR 1 1/2" MIN. CLEARANCE FROM BOTTOM OF RAIL TO TOP OF SLAB TO ALLOW FOR ROUTING OF SYSTEMS CABLES.
  3. UP TO 1/2" MAX. OF SHIMMING IS ALLOWED UNDER FASTENER ASSEMBLY IF NEEDED. SHIMS TO BE HDPE OR APPROVED EQUAL.
  4. SWITCH ROD BLOCKOUT REINFORCING IS DETAILED FOR THE SOUTHWEST SWITCH. USE SYMMETRICAL LAYOUT FOR THE OTHER THREE SWITCHES.
  5. RAIL AND FASTENERS OMITTED FROM SECTION "A" FOR CLARITY.
  6. DRAINAGE SLOTS SHALL CONNECT ALL SWITCH ROD BLOCKOUTS AS SHOWN ON DWG K3019.
  7. DEPTH OF SWITCH ROD BLOCKOUT VARIES, 6" MIN.
  8. ADJUST REBAR SPACING AS NEEDED TO ALLOW FOR 2" CLEAR AROUND ANCHOR BOLTS.

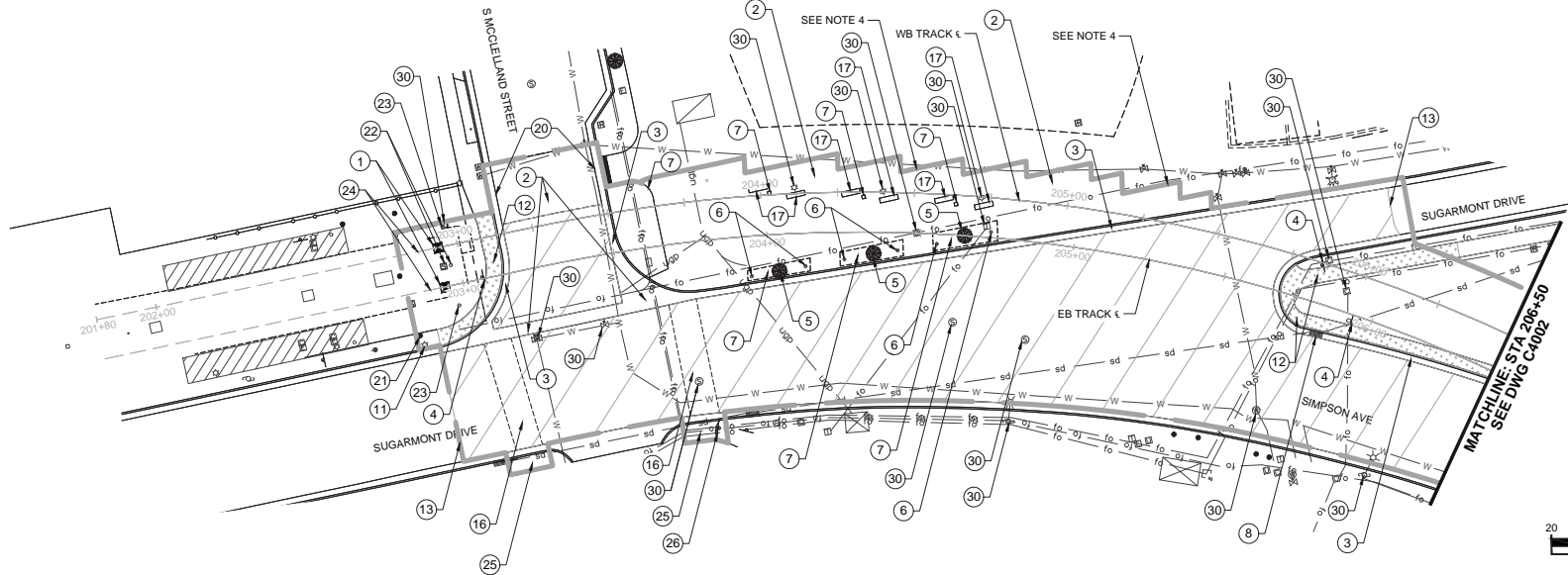
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Designed By: F. NANNENGA
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: A. DIGIROLAMO

**S-LINE EXTENSION PROJECT**  
**STANDARD DRAWINGS**  
**TRACKWORK**  
**DIRECT FIXATION ON SLAB DETAILS**

State:	AS NOTED
CADD Filename:	SL-C-K-3-020.dwg
Submission Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	K3020
Sheet No.:	



**DEMOLITION NOTES:**

- ① RELOCATE BUMPING POST TO END OF LINE
- ② REMOVE CONCRETE
- ③ REMOVE CURB AND GUTTER
- ④ REMOVE SIGN AND PROVIDE TO OWNER
- ⑤ REMOVE TREE (SEE NOTE 11)
- ⑥ RELOCATE DECORATIVE LIGHTING (SEE NOTE 9)
- ⑦ REMOVE PLANTER
- ⑧ REMOVE DRAIN INLET (SEE NOTE 13)
- ⑨ PROTECT DRAINAGE MANHOLE IN PLACE (SEE NOTE 13)
- ⑩ PROTECT TRAFFIC SIGNAL POLE IN PLACE (SEE TRAFFIC PLANS)
- ⑪ PROTECT LIGHT POLE IN PLACE
- ⑫ REMOVE SIDEWALK
- ⑬ SAWCUT ASPHALT/CONCRETE, TYP., SEE NOTE 2
- ⑭ REMOVE TRAFFIC SIGNAL POLE (SEE TRAFFIC PLANS)
- ⑮ REMOVE AND REPLACE TACTILE PAVING
- ⑯ REMOVE BRICK CROSSWALK, REPLACE WITH BRICK TEXTURED EPOXY
- ⑰ REMOVE BENCH (SEE NOTE 9)
- ⑱ REMOVE DRAINAGE MANHOLE (SEE NOTE 13)
- ⑲ REMOVE AND RELOCATE LIGHT
- ⑳ REMOVE STEEL GUTTER PLATES
- ㉑ PROTECT BOLLARD IN PLACE
- ㉒ PROTECT OCS POLE/BOX IN PLACE
- ㉓ REMOVE OCS POLE
- ㉔ REMOVE TRACK (SEE NOTE 12)
- ㉕ REMOVE PEDESTRIAN RAMP
- ㉖ RELOCATE SIGN (SEE TRAFFIC PLANS)
- ㉗ PROTECT TRAFFIC SIGNAL BOX IN PLACE (SEE TRAFFIC PLANS)
- ㉘ REMOVE TRAFFIC SIGNAL BOX (SEE TRAFFIC PLANS)
- ㉙ PROTECT DRAIN INLET IN PLACE (SEE NOTE 13)
- ㉚ SEE NOTE 5

**LEGEND:**

- LIMITS OF CONSTRUCTION
- AC PAVEMENT REMOVAL
- SIDEWALK REMOVAL  
SEE NOTE 8

**NOTES:**

1. RESTORE ALL DISTURBED GROUND SURFACES TO ORIGINAL CONDITION.
2. SAWCUT AND MATCH EXISTING PAVEMENT.
3. PROTECT EXISTING INFRASTRUCTURE NOT BEING REMOVED.
4. IN APARTMENT PLAZA AREA, CONCRETE SHALL BE REMOVED TO THE NEAREST CONTRACTION JOINT.
5. FOR EXACT UTILITY TREATMENT DETAILS SUCH AS SALVAGE, REMOVE, RELOCATE, AND PROTECT IN PLACE, SEE UTILITY RELOCATION PLAN DRAWINGS U1210 AND U1211.
6. CURB REMOVAL IS TO BE TO THE NEXT CONTRACTION JOINT FROM THE IMPACTED AREA AND IS TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION.
7. IN CRA, ALL EXISTING DEVELOPMENT INCLUDING BUT NOT LIMITED TO: BUILDINGS, DRIVEWAYS, PARKING AREAS, UTILITIES, SIGNS, POLES FOUNDATIONS, ETC WILL BE REMOVED BY OTHERS OUTSIDE THIS CONTRACT.
8. IT IS ANTICIPATED FOR THIS SUBMITTAL THAT THE LIMITS OF CRA DEMO ARE THE PROPERTY LINES FROM BACK OF SIDEWALK TO BACK OF SIDEWALK.
9. ITEM TO BE SALVAGED AND RETURNED TO OWNER.
10. EXISTING BUILDINGS ON THE PROPERTY OWNED BY CRA ALONG SIMPSON AVE. WILL ENCROACH OVER PROPERTY LINES. BUILDINGS THAT ENCROACH INTO THE PUBLIC RIGHT OF WAY WILL NEED TO BE DEMOLISHED PRIOR TO FINAL PLAT APPROVAL.
11. COORDINATE WITH APARTMENT COMPLEX TO DETERMINE IF ITEM CAN BE SALVAGED AND RETURNED TO OWNER, RELOCATED, OR REMOVED.
12. BEGIN REMOVAL AT INSULATED JOINTS. SEE K1201 FOR MORE DETAILS.
13. SEE DRAWINGS D1201 - D1203 FOR MORE INFORMATION.

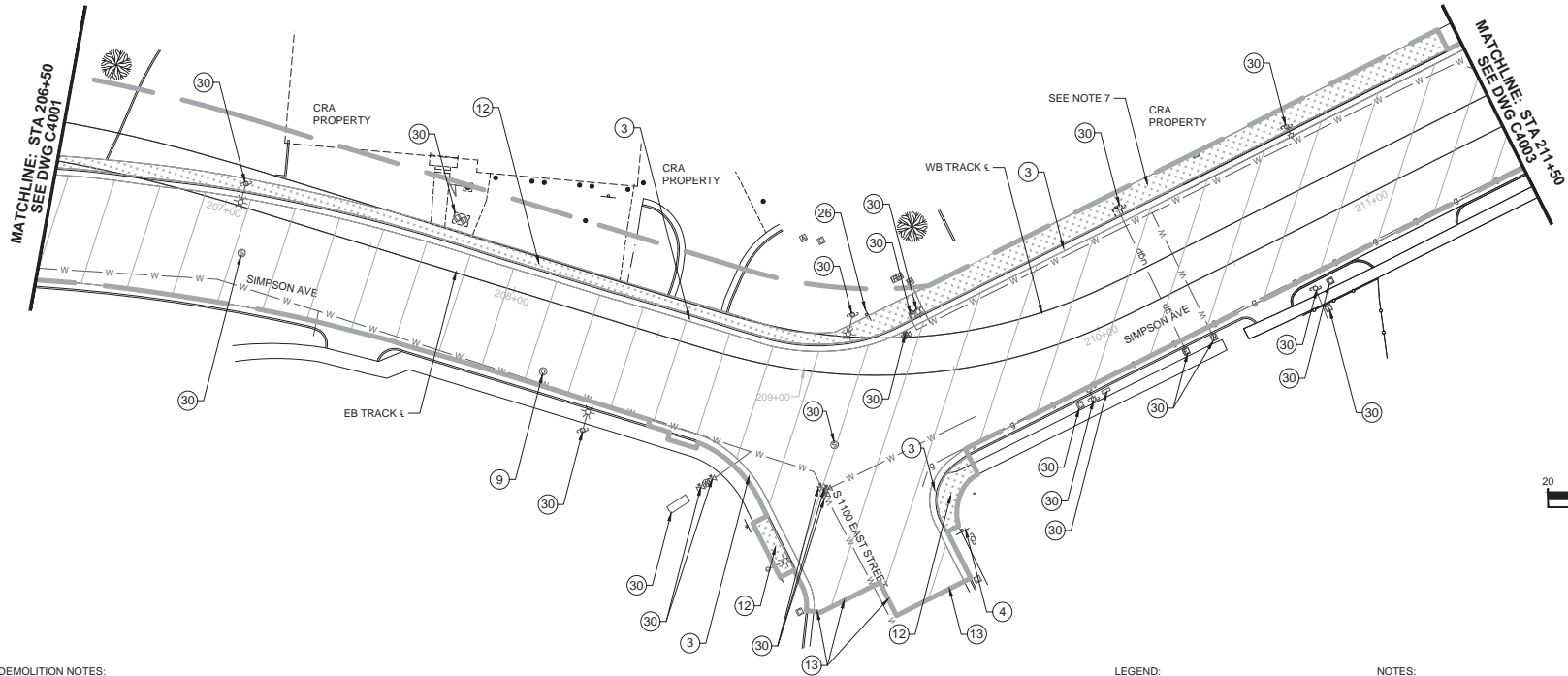
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Designed By:	T. COVEYOU
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**  
**WB STA 201+80.27 TO WB STA 206+50**  
**DEMOLITION PLAN**

Scale:	1" = 20'
CADD Filename:	SL-C-4-001.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C4001
Sheet No.:	



**DEMOLITION NOTES:**

- ① RELOCATE BUMPING POST TO END OF LINE
- ② REMOVE CONCRETE
- ③ REMOVE CURB AND GUTTER
- ④ REMOVE SIGN AND PROVIDE TO OWNER
- ⑤ REMOVE TREE
- ⑥ RELOCATE DECORATIVE LIGHTING
- ⑦ REMOVE PLANTER
- ⑧ REMOVE DRAIN INLET (SEE NOTE 9)
- ⑨ PROTECT DRAINAGE MANHOLE IN PLACE (SEE NOTE 9)
- ⑩ PROTECT TRAFFIC SIGNAL POLE IN PLACE (SEE TRAFFIC PLANS)
- ⑪ PROTECT LIGHT POLE IN PLACE
- ⑫ REMOVE SIDEWALK
- ⑬ SAWCUT ASPHALT/CONCRETE, TYP., SEE NOTE 2
- ⑭ REMOVE TRAFFIC SIGNAL POLE (SEE TRAFFIC PLANS)
- ⑮ REMOVE AND REPLACE TACTILE PAVING
- ⑯ REMOVE BRICK CROSSWALK, REPLACE WITH BRICK TEXTURED EPOXY
- ⑰ REMOVE BENCH
- ⑱ REMOVE DRAINAGE MANHOLE (SEE NOTE 9)
- ⑲ REMOVE AND RELOCATE LIGHT
- ⑳ REMOVE STEEL GUTTER PLATES
- ㉑ PROTECT BOLLARD IN PLACE
- ㉒ PROTECT OCS POLE/BOX IN PLACE
- ㉓ REMOVE OCS POLE
- ㉔ REMOVE TRACK
- ㉕ REMOVE PEDESTRIAN RAMP
- ㉖ RELOCATE SIGN (SEE TRAFFIC PLANS)
- ㉗ PROTECT TRAFFIC SIGNAL BOX IN PLACE (SEE TRAFFIC PLANS)
- ㉘ REMOVE TRAFFIC SIGNAL BOX (SEE TRAFFIC PLANS)
- ㉙ PROTECT DRAIN INLET IN PLACE (SEE NOTE 9)
- ㉚ SEE NOTE 4

**LEGEND:**

- LIMITS OF CONSTRUCTION
- AC PAVEMENT REMOVAL
- SIDEWALK REMOVAL, SEE NOTE 7

**NOTES:**

1. RESTORE ALL DISTURBED GROUND SURFACES TO ORIGINAL CONDITION.
2. SAWCUT AND MATCH EXISTING PAVEMENT.
3. PROTECT EXISTING INFRASTRUCTURE NOT BEING REMOVED.
4. FOR EXACT UTILITY TREATMENT DETAILS SUCH AS SALVAGE, REMOVE, RELOCATE, AND PROTECT IN PLACE, SEE UTILITY RELOCATION PLAN DRAWINGS U1210 - U1214, U1225, U1231, AND U1232.
5. CURB REMOVAL IS TO BE TO THE NEXT CONTRACTION JOINT FROM THE IMPACTED AREA AND IS TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION.
6. IN CRA, ALL EXISTING DEVELOPMENT INCLUDING BUT NOT LIMITED TO: BUILDINGS, DRIVEWAYS, PARKING AREAS, UTILITIES, SIGNS, POLES FOUNDATIONS, ETC WILL BE REMOVED BY OTHERS OUTSIDE THIS CONTRACT.
7. SIDEWALK ALONG SIMPSON BETWEEN 1100 EAST STREET AND HIGHLAND INTERSECTION MAY BE REMOVED AS PART OF CRA SITE DEMO. IF NOT, THIS PROJECT WILL REMOVE IT.
8. EXISTING BUILDINGS ON THE PROPERTY OWNED BY CRA ALONG SIMPSON AVE. WILL ENCROACH OVER PROPERTY LINES. BUILDINGS THAT ENCROACH INTO THE PUBLIC RIGHT OF WAY WILL NEED TO BE DEMOLISHED PRIOR TO FINAL PLAT APPROVAL.
9. SEE DRAWINGS D1201 - D1203 FOR MORE INFORMATION.

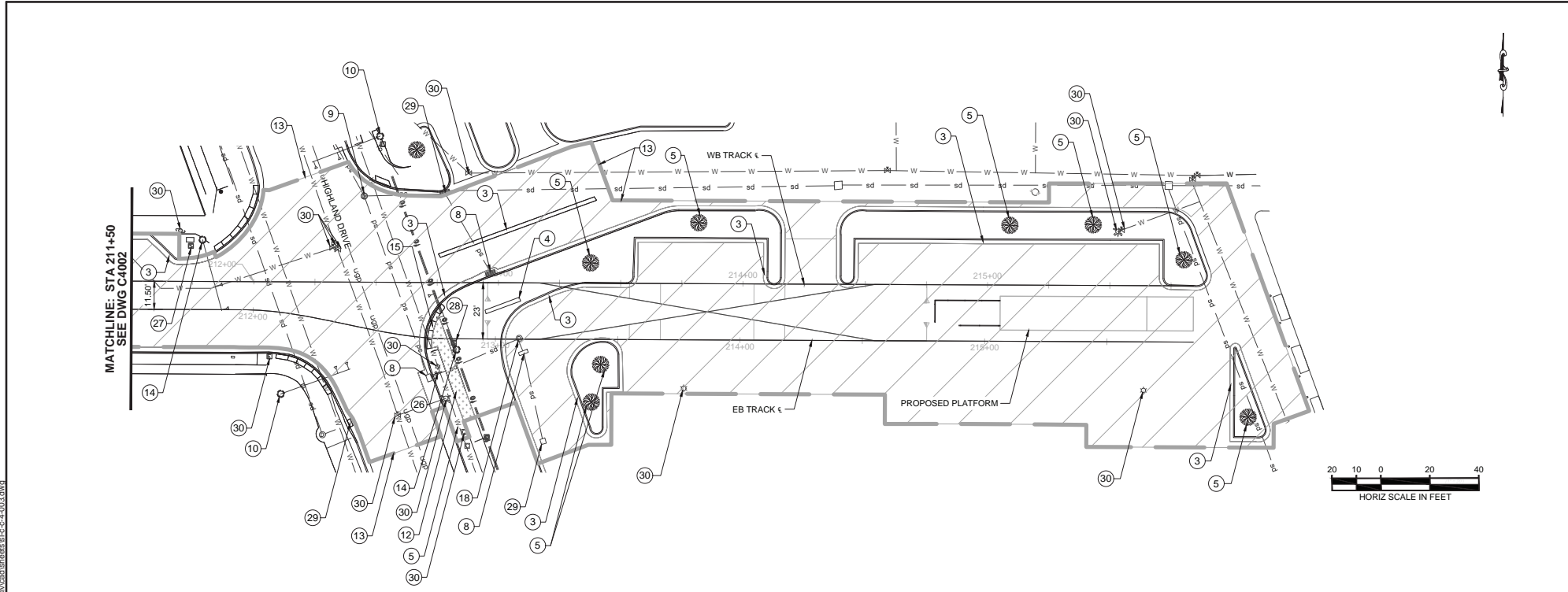
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Designed By: T. COVEYOU
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
 DEMOLITION PLAN  
 WB STA 206+50 TO WB STA 211+50

Scale:	1" = 20'
CADD Filename:	SL-C-4-002.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C4002
Sheet No.:	



**DEMOLITION NOTES:**

- |  |   |
|--|---|
| ① RELOCATE BUMPING POST TO END OF LINE                     | ⑩ REMOVE BRICK CROSSWALK, REPLACE WITH BRICK TEXTURED EPOXY   |
| ② REMOVE CONCRETE  | ⑪ REMOVE BENCH  |
| ③ REMOVE CURB AND GUTTER                                   | ⑫ REMOVE DRAINAGE MANHOLE (SEE NOTE 10)                       |
| ④ REMOVE SIGN AND PROVIDE TO OWNER                         | ⑬ REMOVE AND RELOCATE LIGHT (SEE ROADWAY PLANS)               |
| ⑤ REMOVE TREE  | ⑭ REMOVE STEEL GUTTER PLATES                                  |
| ⑥ RELOCATE DECORATIVE LIGHTING                             | ⑮ PROTECT BOLLARD IN PLACE                                    |
| ⑦ REMOVE PLANTER   | ⑯ PROTECT OCS POLE/BOX IN PLACE                               |
| ⑧ REMOVE DRAIN INLET (SEE NOTE 10)                         | ⑰ REMOVE OCS POLE   |
| ⑨ PROTECT DRAINAGE MANHOLE IN PLACE (SEE NOTE 10)          | ⑱ REMOVE TRACK  |
| ⑩ PROTECT TRAFFIC SIGNAL POLE IN PLACE (SEE TRAFFIC PLANS) | ⑲ REMOVE PEDESTRIAN RAMP                                      |
| ⑪ PROTECT LIGHT POLE IN PLACE                              | ⑳ RELOCATE SIGN (SEE TRAFFIC PLANS)                           |
| ⑫ REMOVE SIDEWALK  | ㉑ PROTECT TRAFFIC SIGNAL BOX IN PLACE (SEE TRAFFIC PLANS)     |
| ⑬ SAWCUT ASPHALT/CONCRETE, TYP., SEE NOTE 2                | ㉒ REMOVE TRAFFIC SIGNAL BOX (SEE TRAFFIC PLANS)               |
| ⑭ REMOVE TRAFFIC SIGNAL POLE (SEE TRAFFIC PLANS)           | ㉓ PROTECT DRAIN INLET IN PLACE (SEE NOTE 10)                  |
| ⑮ REMOVE AND REPLACE TACTILE PAVING                        | ㉔ SEE NOTE 4  |
|  | ㉕ REMOVE AND RESTORE LANDSCAPING AND IRRIGATION (SEE NOTE 11) |

**LEGEND:**

- LIMITS OF CONSTRUCTION
- AC PAVEMENT REMOVAL
- SIDEWALK REMOVAL

**NOTES:**

- RESTORE ALL DISTURBED GROUND SURFACES TO ORIGINAL CONDITION.
- SAWCUT AND MATCH EXISTING PAVEMENT.
- PROTECT EXISTING INFRASTRUCTURE NOT BEING REMOVED.
- FOR EXACT UTILITY TREATMENT DETAILS SUCH AS SALVAGE, REMOVE, RELOCATE, AND PROTECT IN PLACE, SEE UTILITY RELOCATION PLAN DRAWINGS U1213, U1214, U1220 - U1222, U1225 - U1227, U1231, AND U1232. CURB REMOVAL IS TO BE TO THE NEXT CONTRACTION JOINT FROM THE IMPACTED AREA AND IS TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION.
- IN CRA, ALL EXISTING DEVELOPMENT INCLUDING BUT NOT LIMITED TO: BUILDINGS, DRIVEWAYS, PARKING AREAS, UTILITIES, SIGNS, POLES FOUNDATIONS, ETC WILL BE REMOVED BY OTHERS OUTSIDE THIS CONTRACT.
- ITEM TO BE SALVAGED AND RETURNED TO OWNER.
- EXISTING BUILDINGS ON THE PROPERTY OWNED BY CRA ALONG SIMPSON AVE. WILL ENROACH OVER PROPERTY LINES. BUILDINGS THAT ENROACH INTO THE PUBLIC RIGHT OF WAY WILL NEED TO BE DEMOLISHED PRIOR TO FINAL PLAT APPROVAL.
- SEE CIVIL SHEETS C1108 AND C1109 FOR DEMOLITION GEOMETRY.
- SEE DRAWINGS D1201 - D1203 FOR MORE INFORMATION.
- SEE DRAWINGS C1003 FOR THE LOCATIONS OF THE PROPOSED LANDSCAPING.

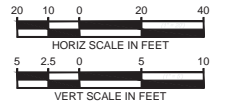
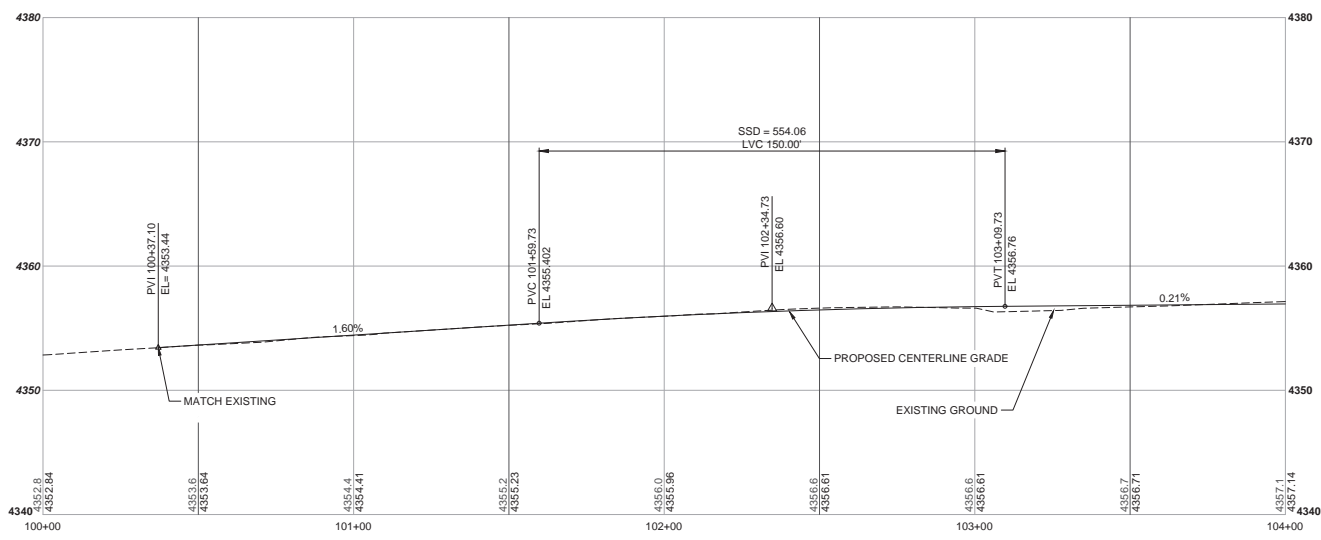
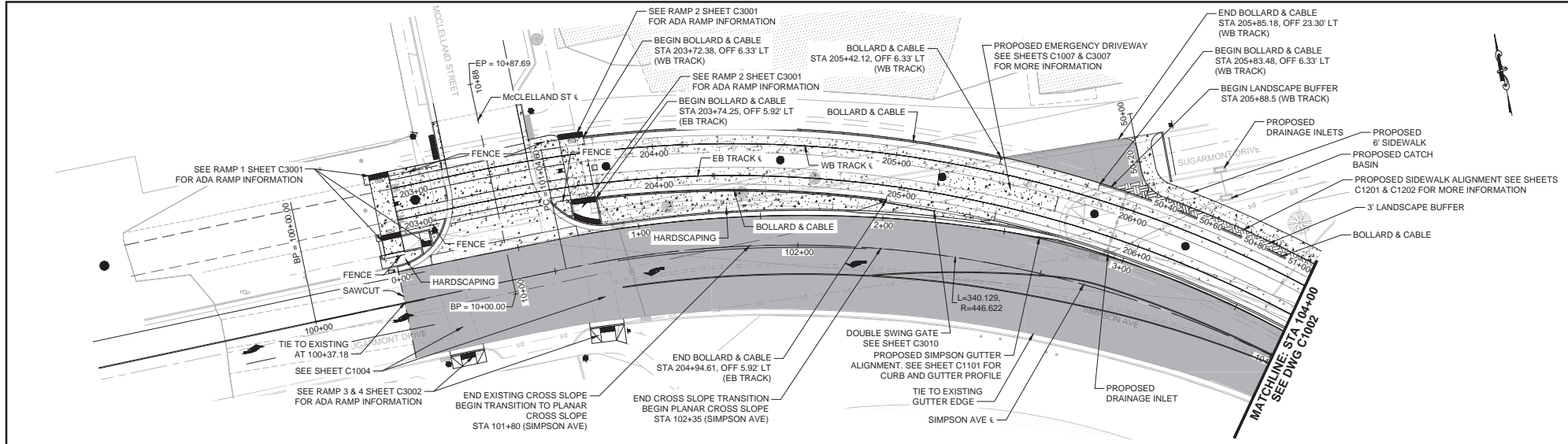
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Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

**S-LINE EXTENSION PROJECT**  
  
**DEMOLITION PLAN**  
**WB STA 211+50 TO WB STA 215+84.18**

Scale:	1" = 20'
CADD Filename:	SL-C-4-003.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C4003
Sheet No.:	



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
  - EP = END POINT

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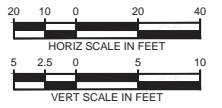
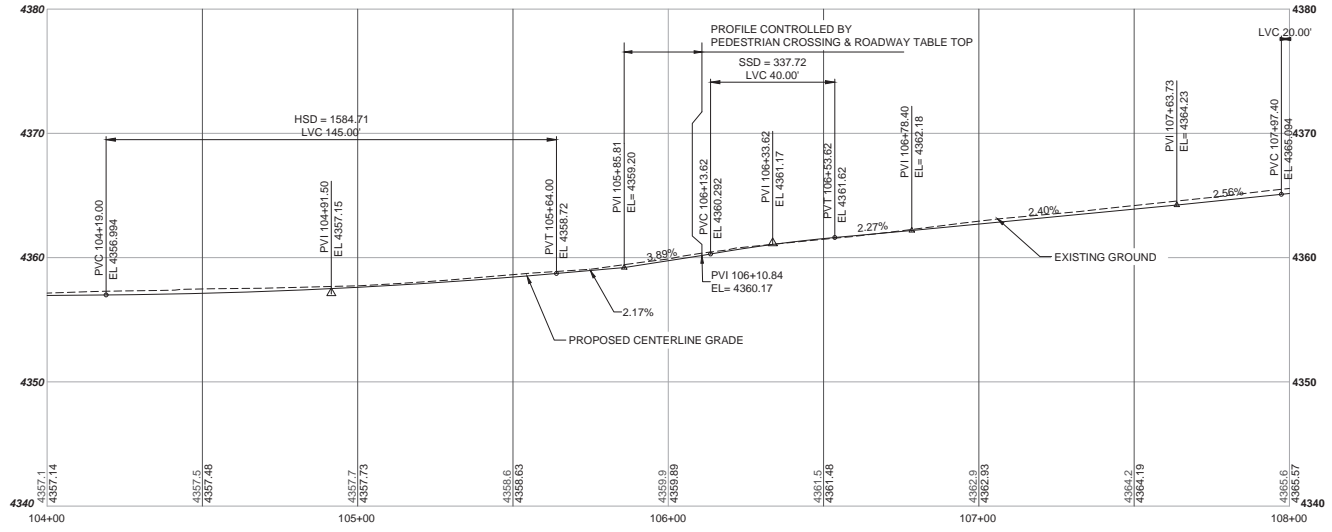
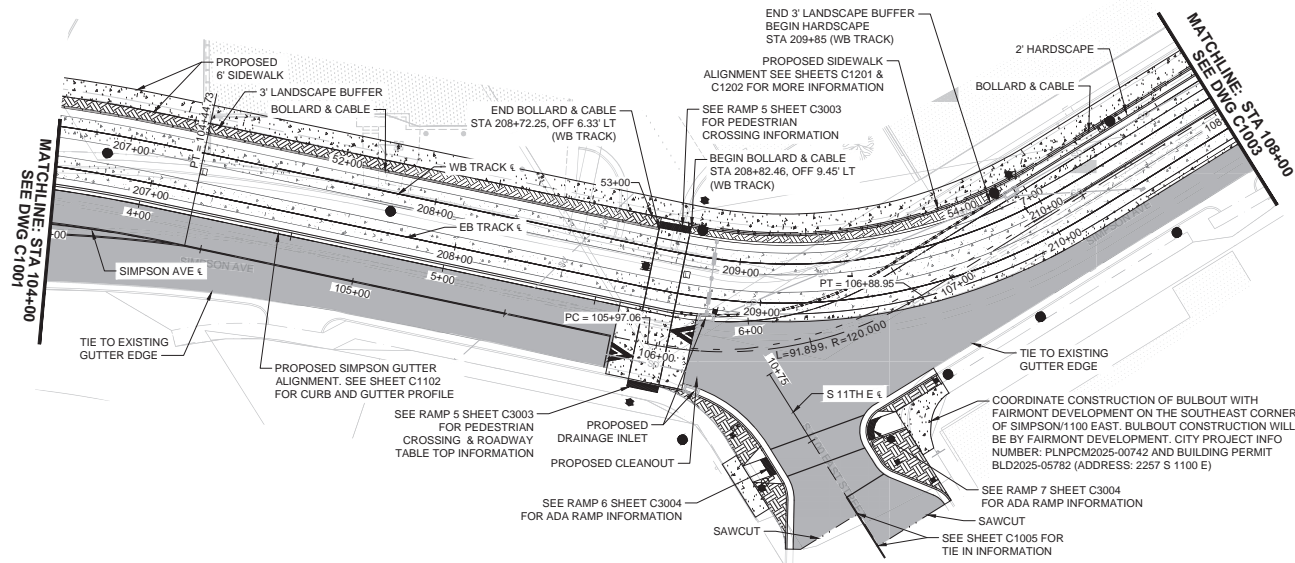
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 Drawn By: M. HARTMANN  
 Checked By: A. DIGIROLAMO  
 Approved By: F. NANNENGA

**S-LINE EXTENSION PROJECT**  
  
**SIMPSON PLAN AND PROFILE**  
 STA 100+00 TO STA 104+00

Scale: H: 1" = 20' V: 1" = 5'	
CADD Filename: SL-C-1-001.dwg	
Submitted Date: 01-16-26	
UTA Contract No.: 24-03849	
Drawing No.: C1001	Sheet No.:



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  - CONCRETE
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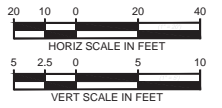
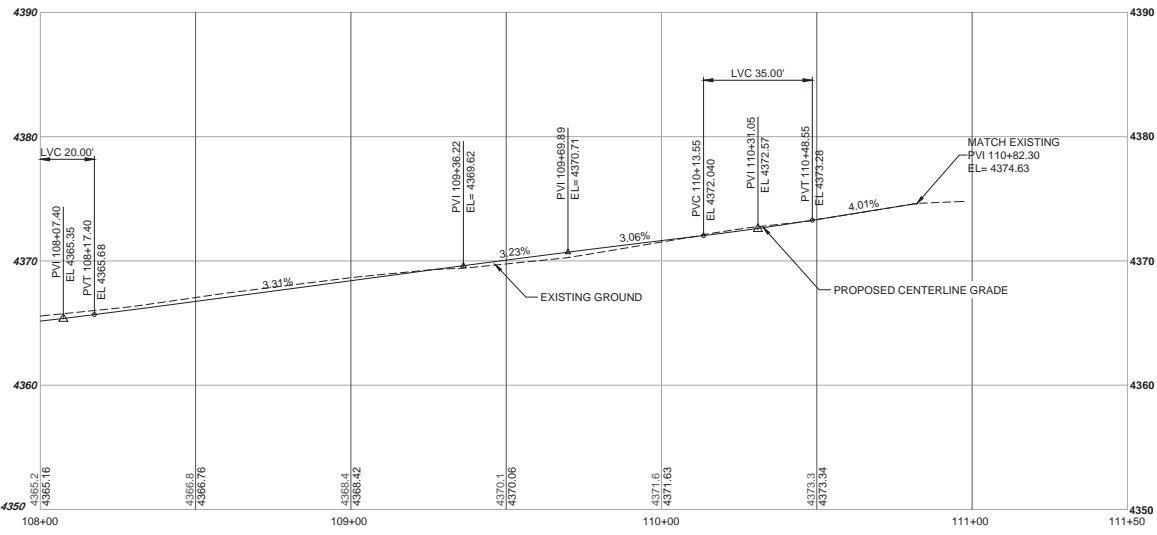
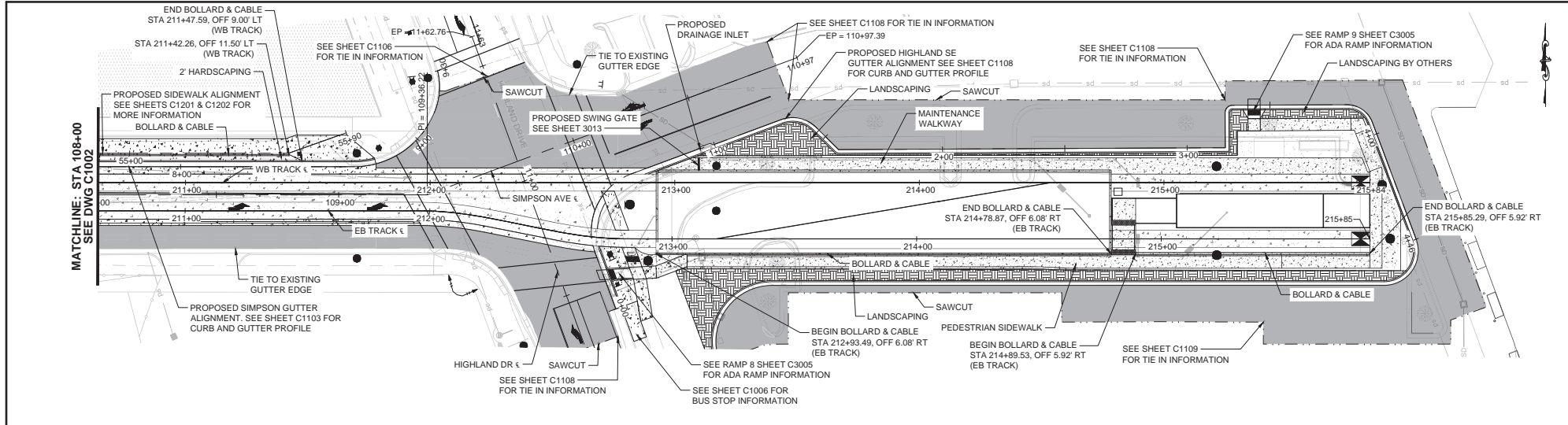
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 Drawn By: M. HARTMANN  
 Checked By: A. DIGIROLAMO  
 Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
 SIMPSON PLAN AND PROFILE  
 STA 104+00 TO STA 108+00

Scale: H: 1" = 20' V: 1" = 5'
CADD Filename: SL-C-1-002.dwg
Submitted Date: 01-16-26
UTA Contract No.: 24-03849
Drawing No.: C1002
Sheet No.:



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
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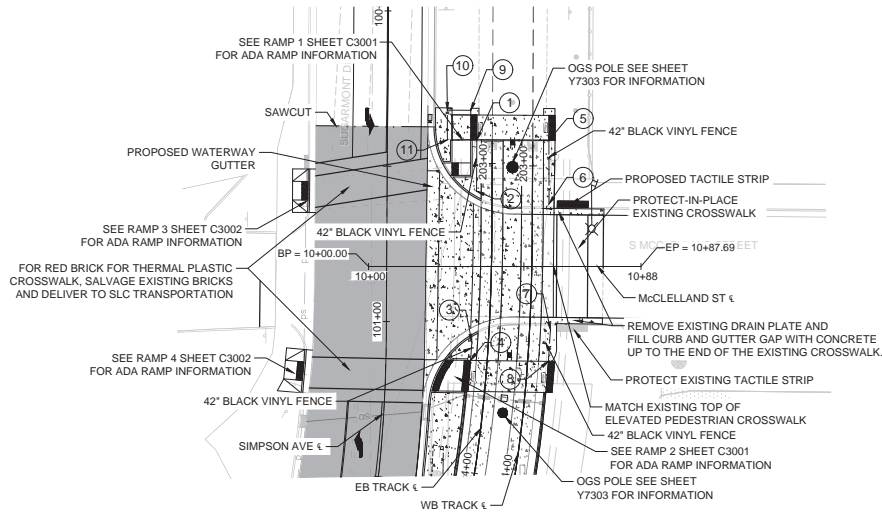
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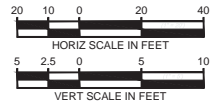
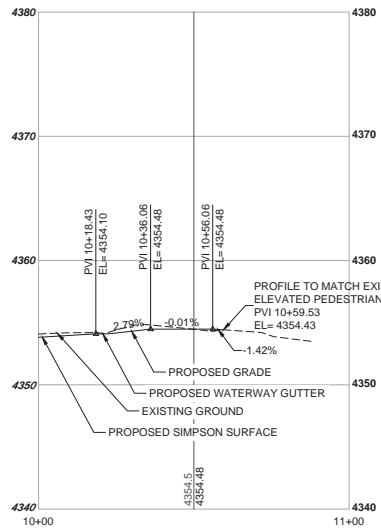
Designed By:	M. HARTMANN
Drawn By:	M. HARTMANN
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**  
**SIMPSON PLAN AND PROFILE**  
**STA 108+00 TO STA 111+00**

State:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-1-003.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1003



- ① BEGIN 42" BLACK VINYL FENCE STA 202+92.42, OFF 5.00' RT (EB TRACK)
- ② END 42" BLACK VINYL FENCE STA 203+08.92, OFF 5.00' RT (EB TRACK)
- ③ BEGIN 42" BLACK VINYL FENCE STA 203+57.82, OFF 5.35' RT (EB TRACK)
- ④ END 42" BLACK VINYL FENCE STA 203+63.82, OFF 5.36' RT (EB TRACK)
- ⑤ BEGIN 42" BLACK VINYL FENCE STA 202+91.70, OFF 5.21' LT (WB TRACK)
- ⑥ END 42" BLACK VINYL FENCE STA 203+12.21, OFF 5.35' LT (WB TRACK)
- ⑦ BEGIN 42" BLACK VINYL FENCE STA 203+53.46, OFF 5.41' LT (WB TRACK)
- ⑧ END 42" BLACK VINYL FENCE STA 203+62.46, OFF 6.67' LT (WB TRACK)
- ⑨ BEGIN 42" BLACK VINYL FENCE STA 202+82.92, OFF 6.97' RT (EB TRACK)
- ⑩ 42" BLACK VINYL FENCE CORNER STA 202+82.89, OFF 14.53' RT (EB TRACK)
- ⑪ END 42" BLACK VINYL FENCE STA 202+92.44, OFF 14.48' RT (EB TRACK)



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
  - EP = END POINT

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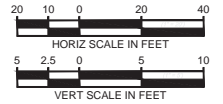
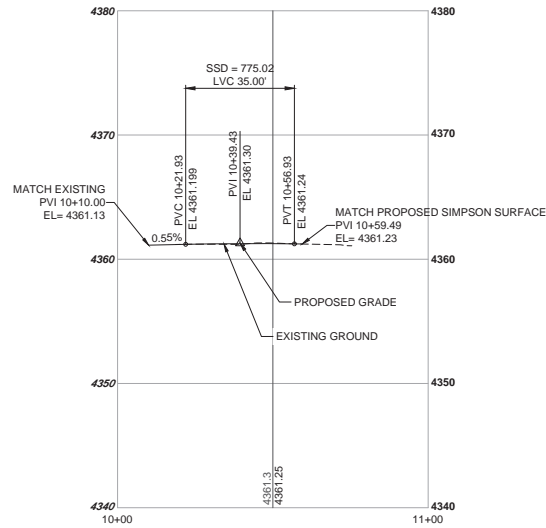
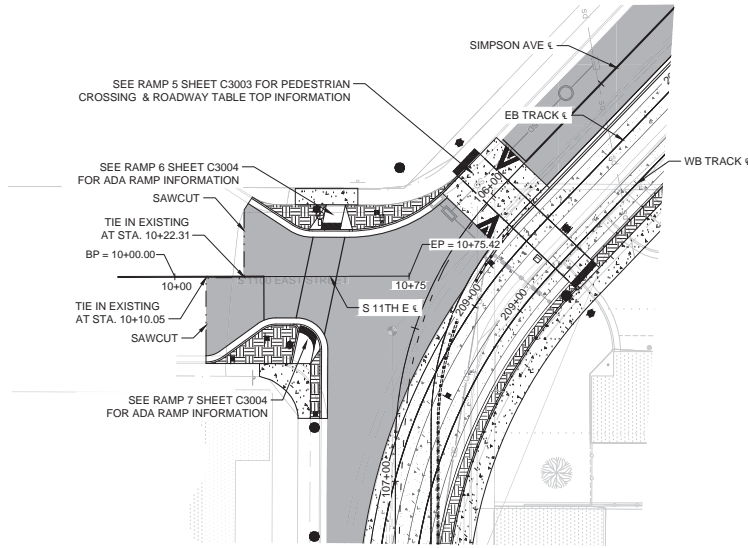


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Drawn By:	M. HARTMANN
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**

**McCLELLAND ST  
PLAN AND PROFILE  
STA 10+00 TO STA 10+88**

Scale:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-C-1-004.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1004
Sheet No.:	



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
  - EP = END POINT

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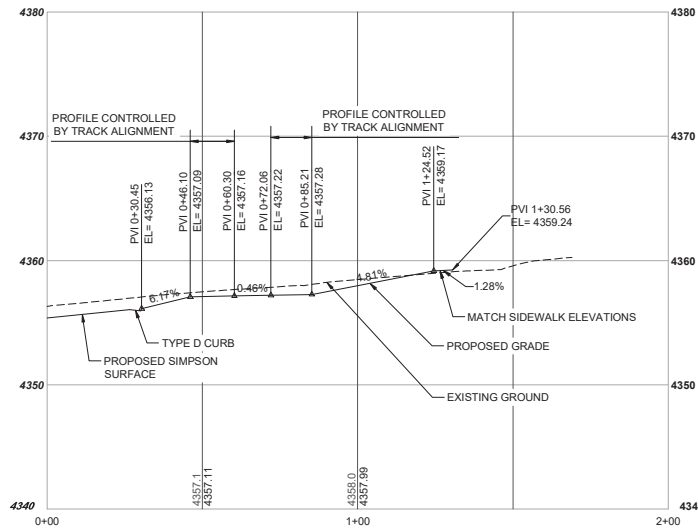
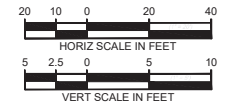
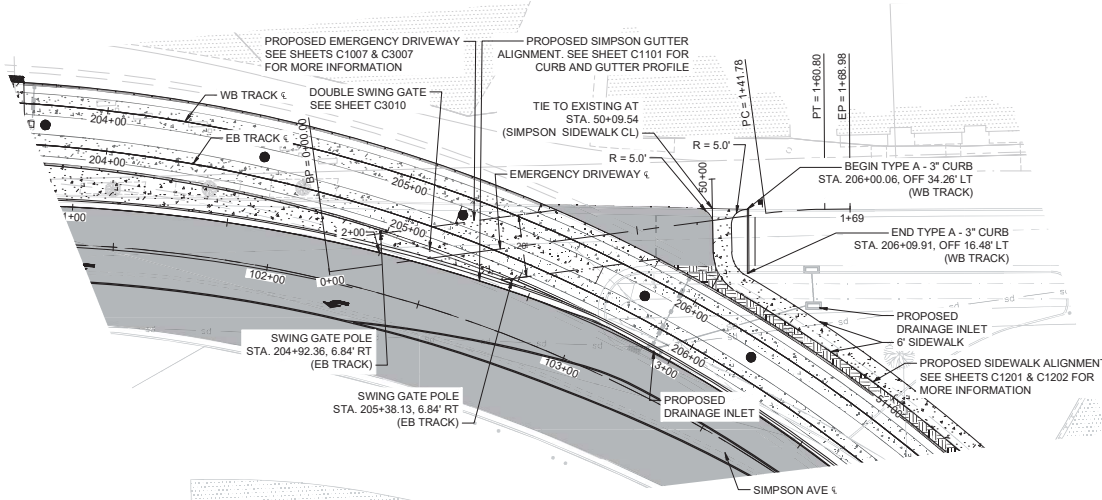
Designed By: M. HARTMANN
Drawn By: M. HARTMANN
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT

S 11TH E PLAN AND PROFILE  
STA 10+00 TO STA 11+00

State: H: 1" = 20' V: 1" = 5'
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Submitted Date: 01-16-26
UTA Contract No.: 24-03849
Drawing No.: C1005
Sheet No.:





- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
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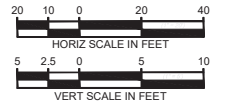
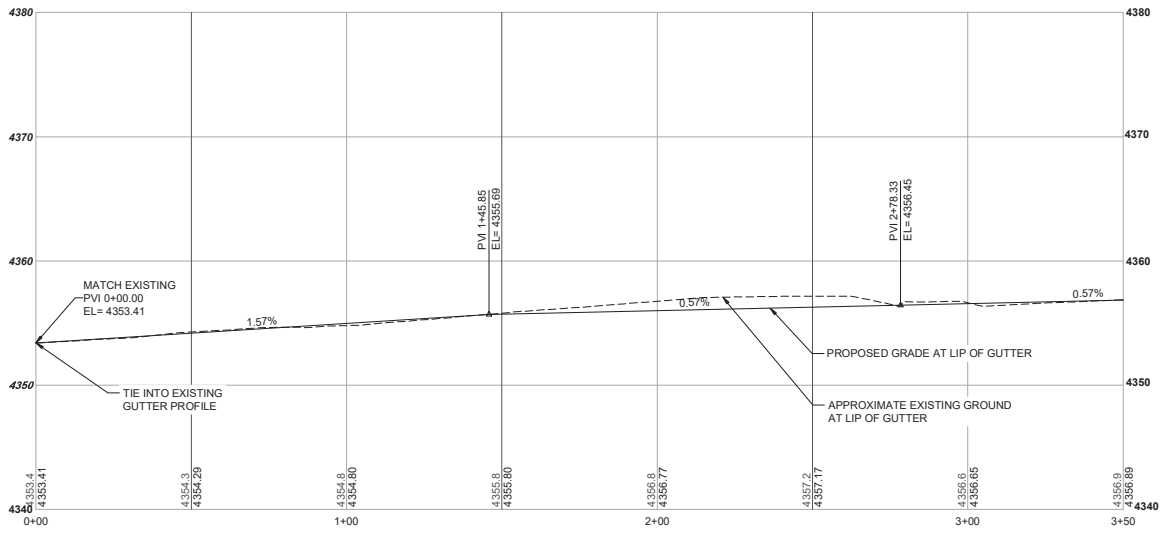
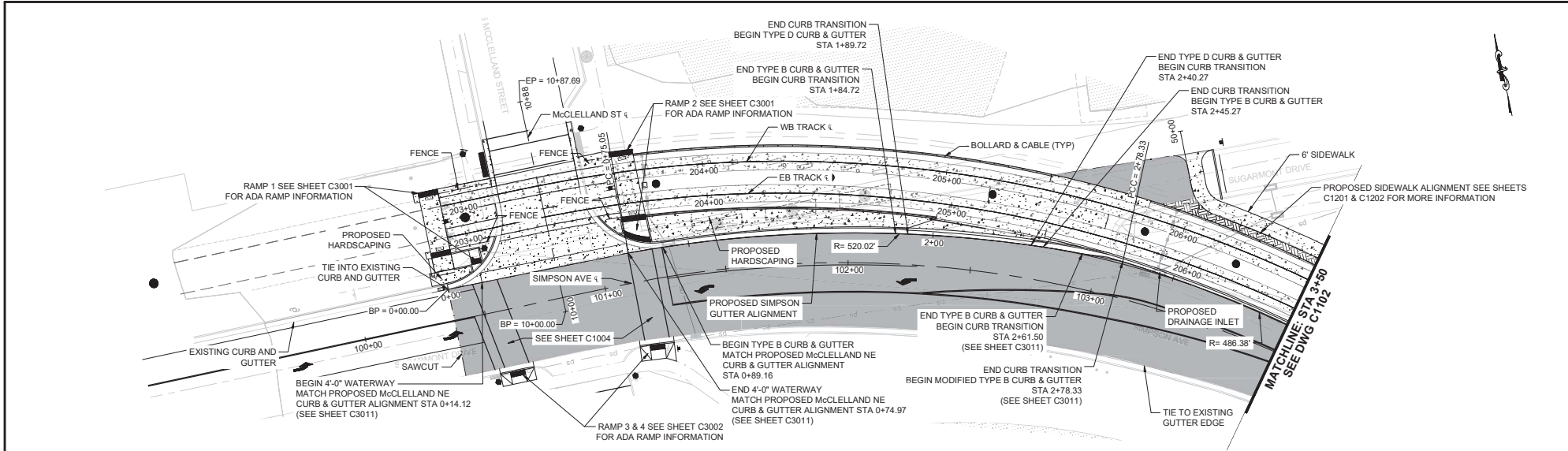
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Drawn By: M. HARTMANN
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT	
EMERGENCY DRIVEWAY PLAN AND PROFILE STA 0+00 TO STA 1+50	

Title: H: 1" = 20' V: 1" = 5'	
CADD Filename: SL-C-C-1-007.dwg	
Submitted Date: 01-16-26	
UTA Contract No.: 24-03849	
Drawing No.: C1007	Sheet No.:



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
  - EP = END POINT

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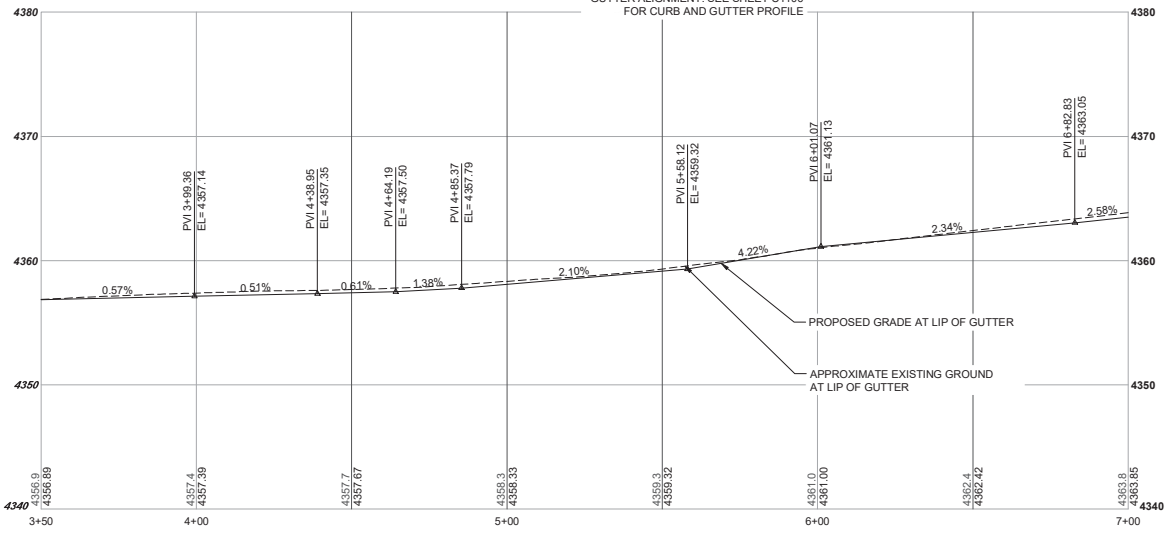
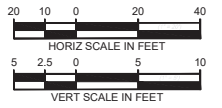
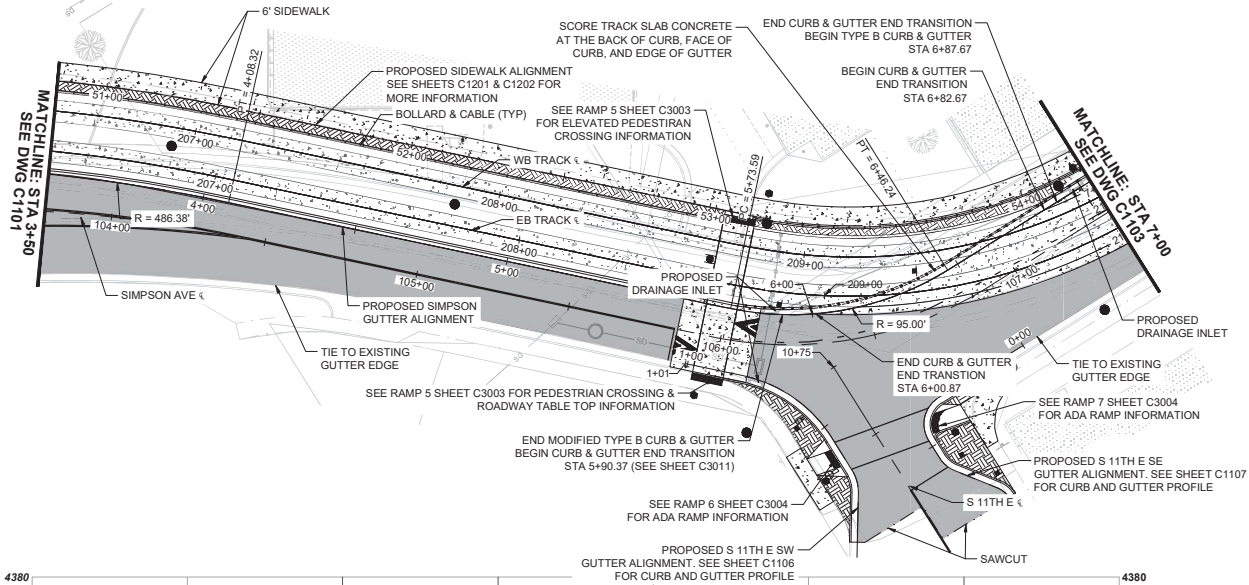
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 Drawn By: M. HARTMANN  
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 Approved By: F. NANNENGA

**S-LINE EXTENSION PROJECT**  
  
**SIMPSON CURB AND GUTTER  
 PLAN AND PROFILE  
 STA 0+00 TO STA 3+50**

Sheet:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-1-101.dwg
Submittal Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1101
Sheet No.:	



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
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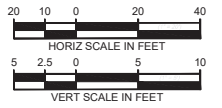
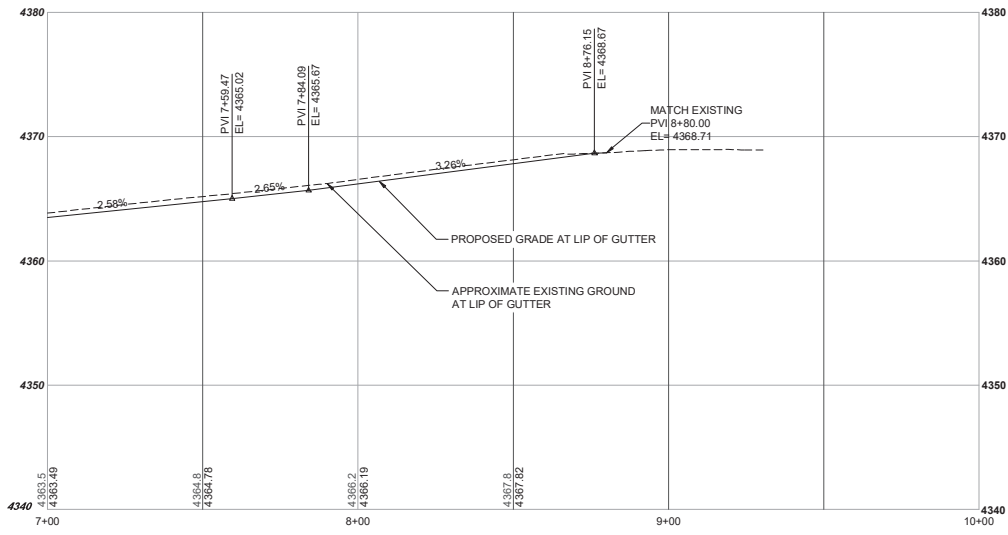
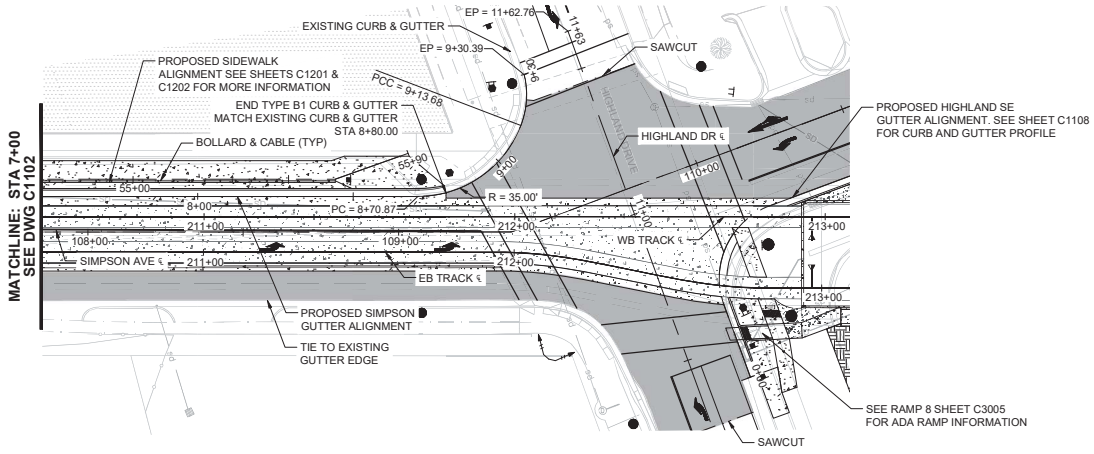



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M. HARTMANN  
 Drawn By:  
M. HARTMANN  
 Checked By:  
A. DIGIROLAMO  
 Approved By:  
F. NANNENGA

S-LINE EXTENSION PROJECT  
 SIMPSON CURB AND GUTTER  
 PLAN AND PROFILE  
 STA 3+50 TO STA 7+00

Scale:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-1-102.dwg
Submittal Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1102
Sheet No.:	

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- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
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**M. HARTMANN**

Drawn By:  
**M. HARTMANN**

Checked By:  
**A. DIGIROLAMO**

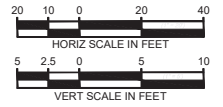
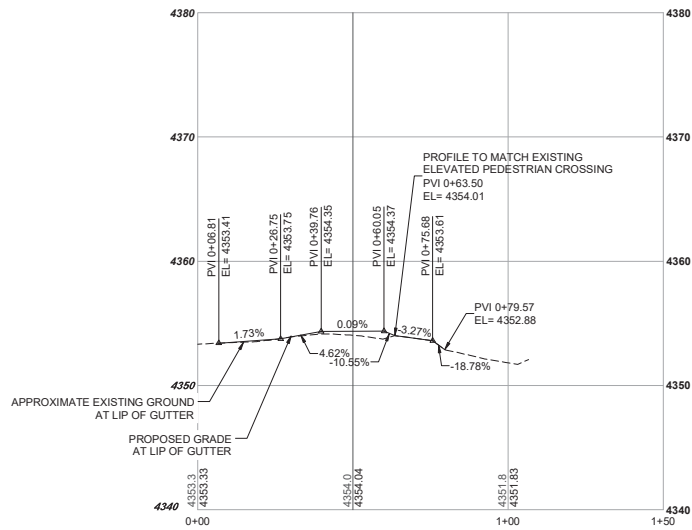
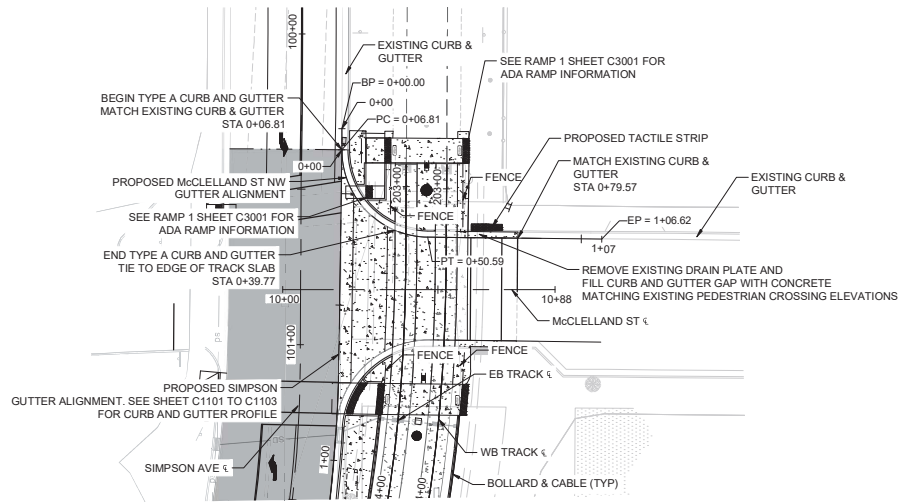
Approved By:  
**F. NANNENGA**

**S-LINE EXTENSION PROJECT**

**GENERAL  
SIMPSON CURB AND GUTTER  
PLAN AND PROFILE  
STA 7+00 TO STA 9+00**

Scale:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-C-1-103.dwg
Submission Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1103
Sheet No.:	

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- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
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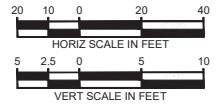
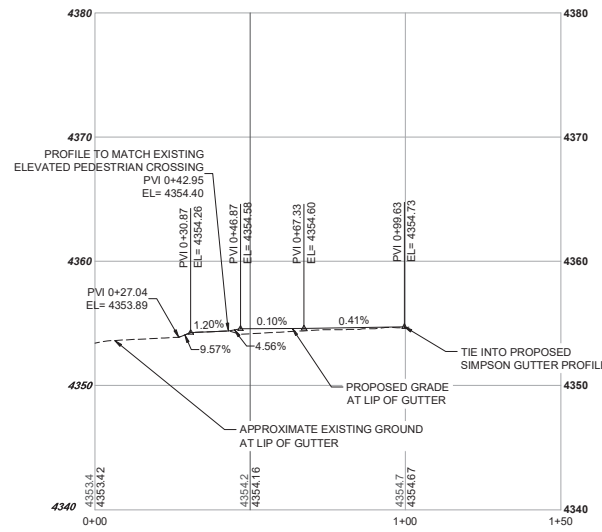
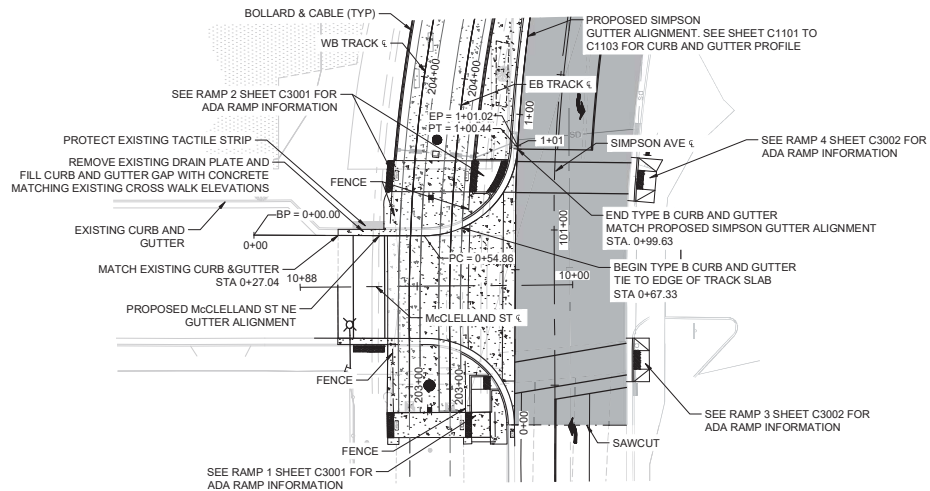


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Drawn By:	M. HARTMANN
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**

**McCLELLAND ST NW CURB AND GUTTER  
PLAN AND PROFILE  
STA 0+00 TO STA 1+00**

Scale:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-C-1-104.dwg
Submittal Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1104
Sheet No.:	



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
  - EP = END POINT

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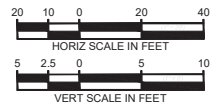
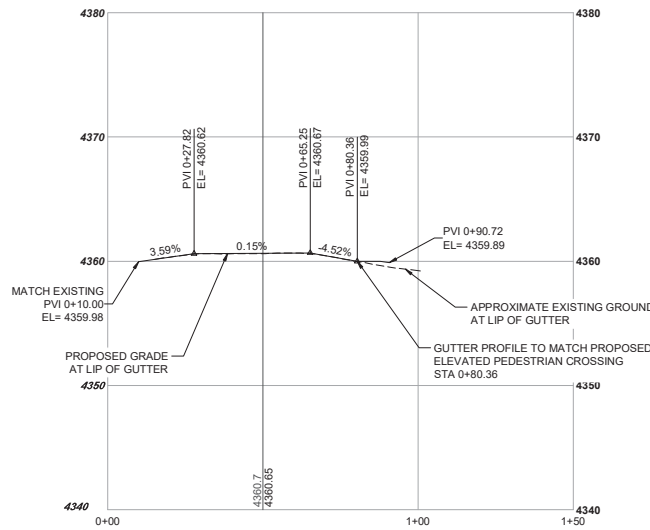
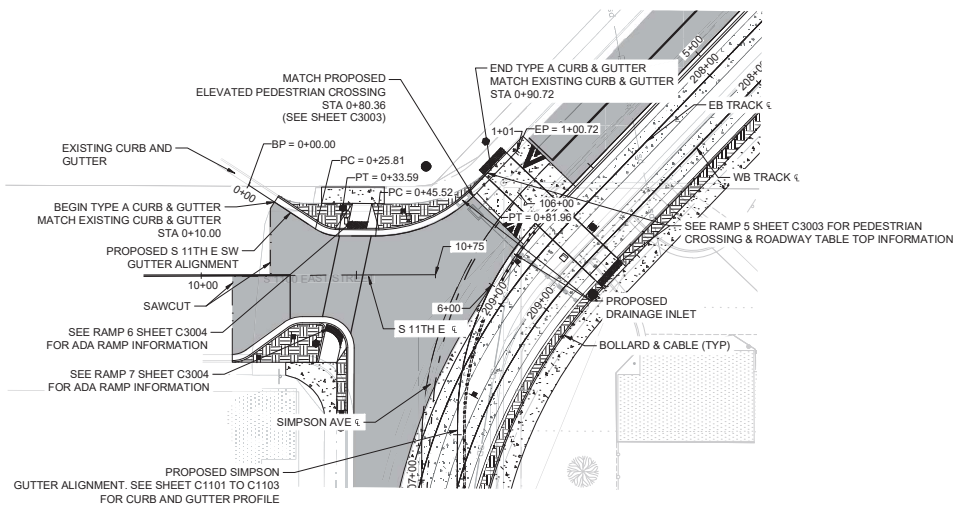





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M. HARTMANN  
Drawn By:  
M. HARTMANN  
Checked By:  
A. DIGIROLAMO  
Approved By:  
F. NANNENGA

S-LINE EXTENSION PROJECT  
McCLELLAND ST NE CURB AND GUTTER  
PLAN AND PROFILE  
STA 0+00 TO STA 1+00

Scale: H: 1" = 20' V: 1" = 5'
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Submitted Date: 01-16-26
UTA Contract No.: 24-03849
Drawing No.: C1105
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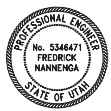
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- LEGEND:**
-  PAVEMENT
  -  LANDSCAPING
  -  CONCRETE
  - BP = BEGIN POINT
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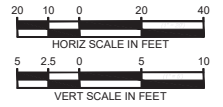
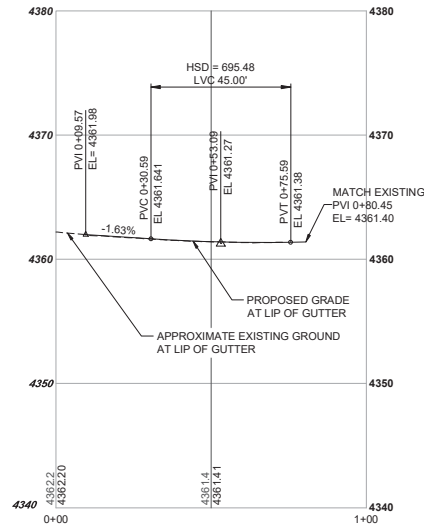
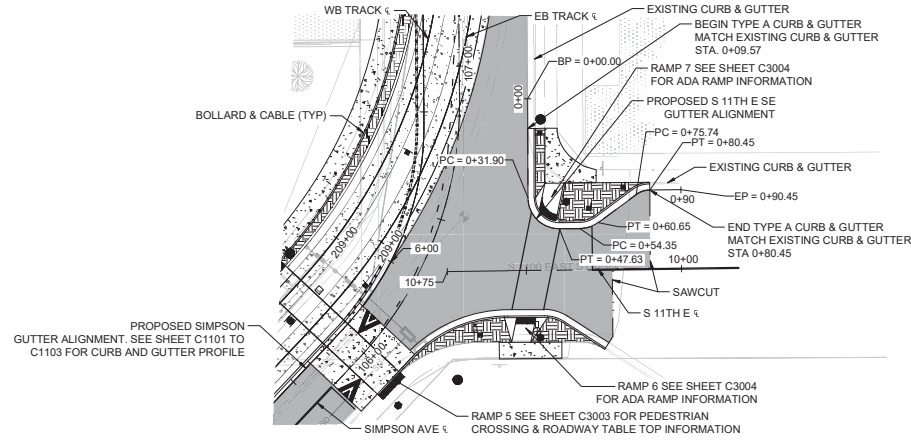


Designed By: M. HARTMANN
Drawn By: M. HARTMANN
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

**S-LINE EXTENSION PROJECT**

**S 11TH E SW CURB AND GUTTER  
PLAN AND PROFILE  
STA 0+00 TO STA 1+00**

Scale: H: 1" = 20' V: 1" = 5'
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Submittal Date: 01-16-26
UTA Contract No.: 24-03849
Drawing No.: C1106
Sheet No.:



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  - CONCRETE
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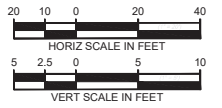
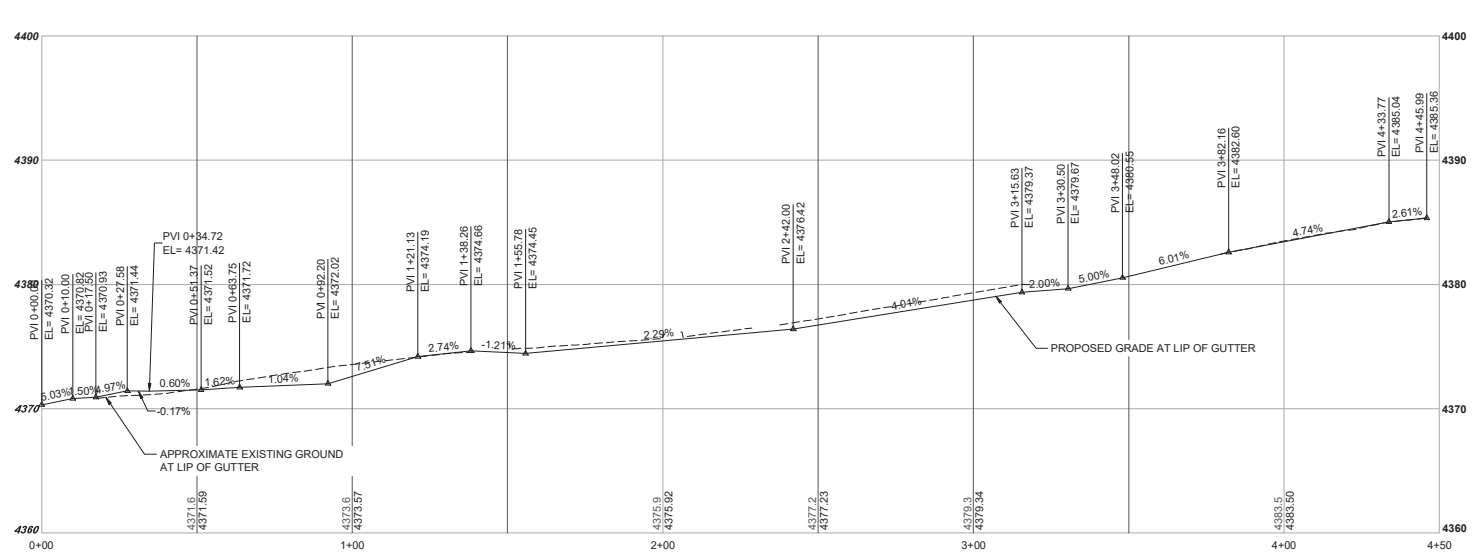
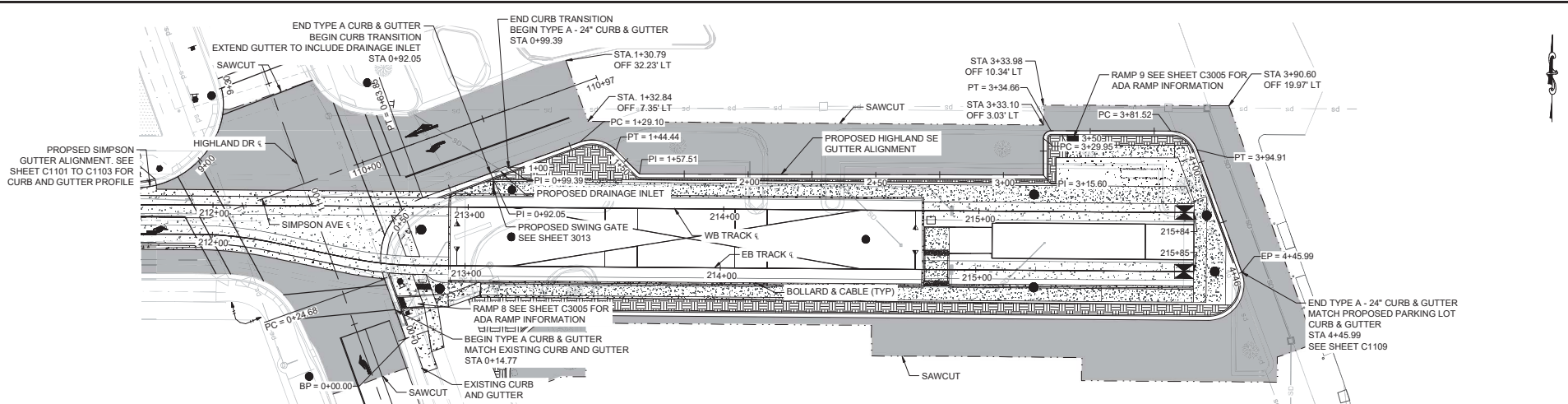
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Designed By:	M. HARTMANN
Drawn By:	M. HARTMANN
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**  
**S 11TH E SE CURB AND GUTTER**  
**PLAN AND PROFILE**  
**STA 0+00 TO STA 1+00**

Scale:	H: 1" = 20' V: 1" = 5'
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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1107
Sheet No.:	



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
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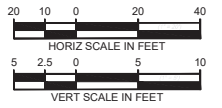
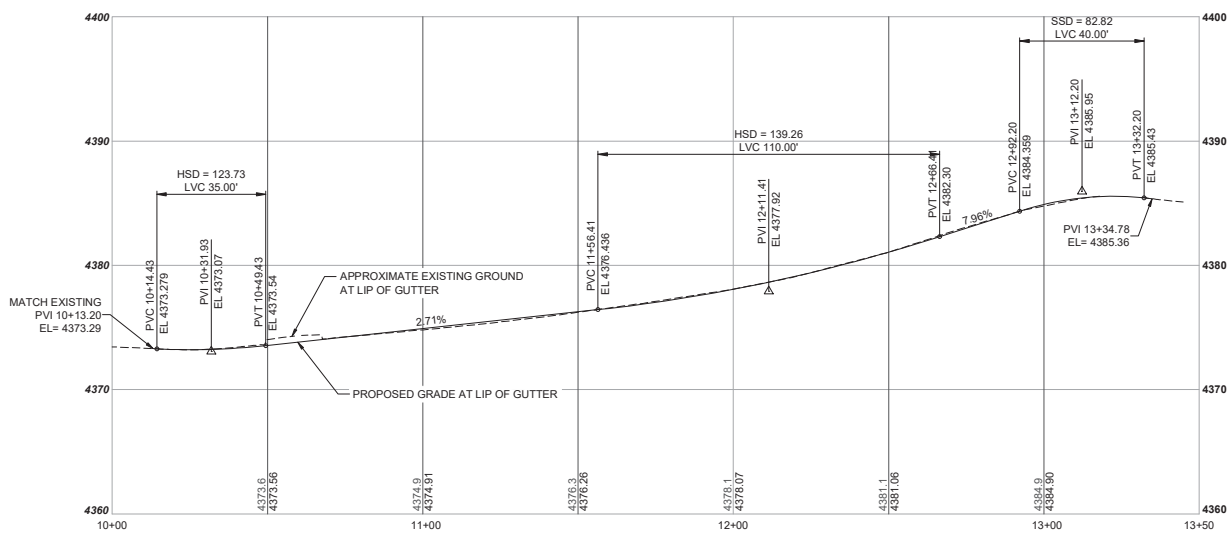
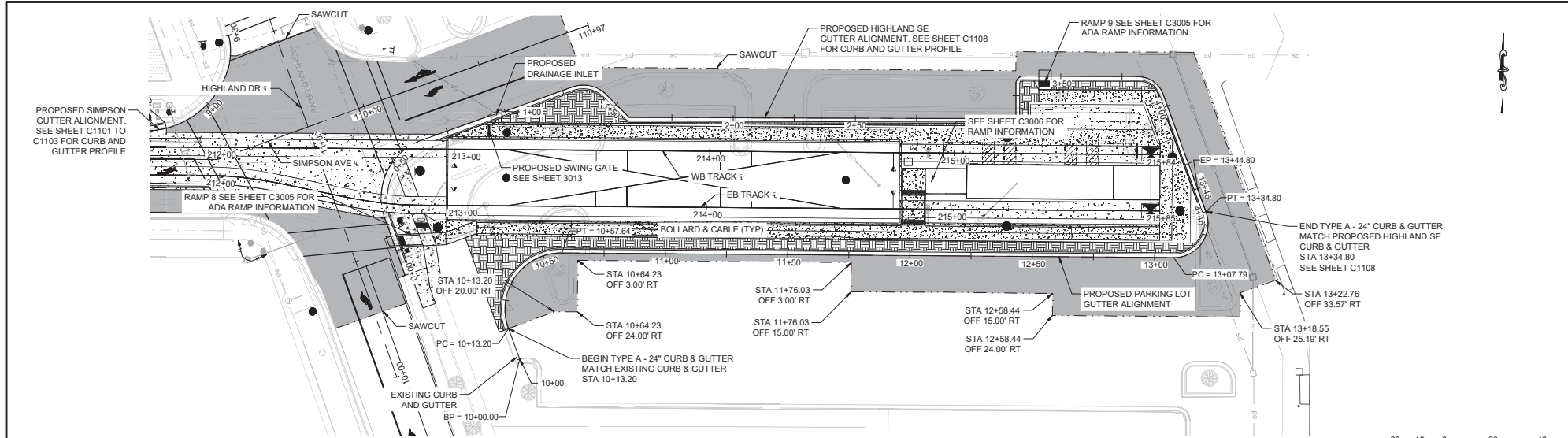
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REV	DATE	Description	



Designed By:	M. HARTMANN
Drawn By:	M. HARTMANN
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**  
**HIGHLAND SE CURB AND GUTTER**  
**PLAN AND PROFILE**  
**STA 0+00 TO STA 4+50**

Scale:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-C-1-108.dwg
Submission Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1108
Sheet No.:	



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
  - EP = END POINT

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REV	DATE	Description	



Designed By:  
**M. HARTMANN**

Drawn By:  
**M. HARTMANN**

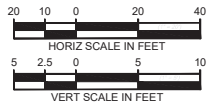
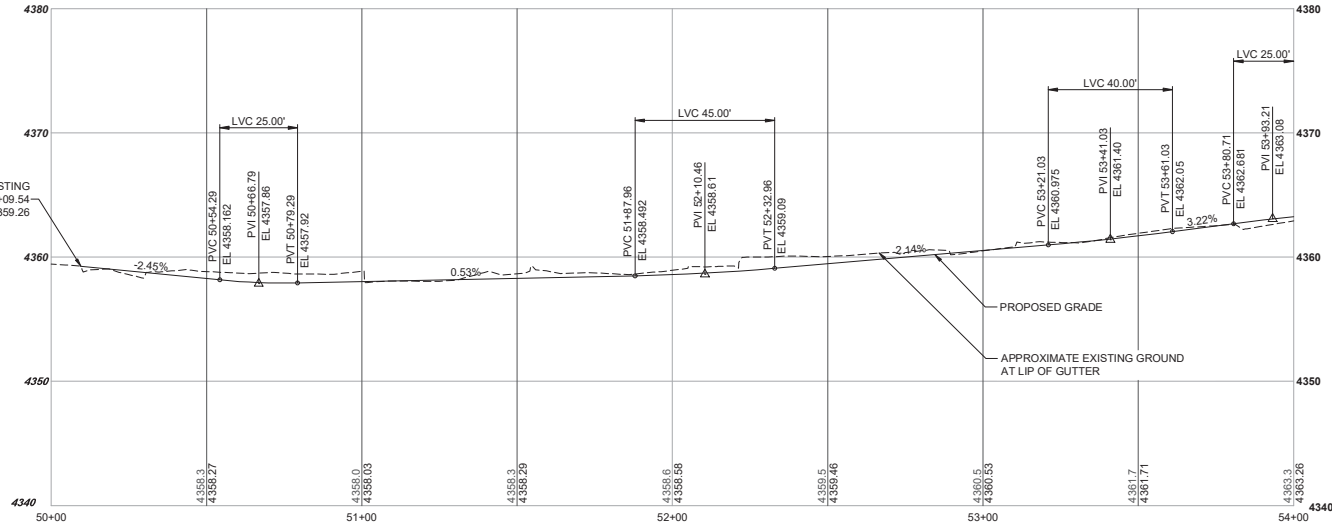
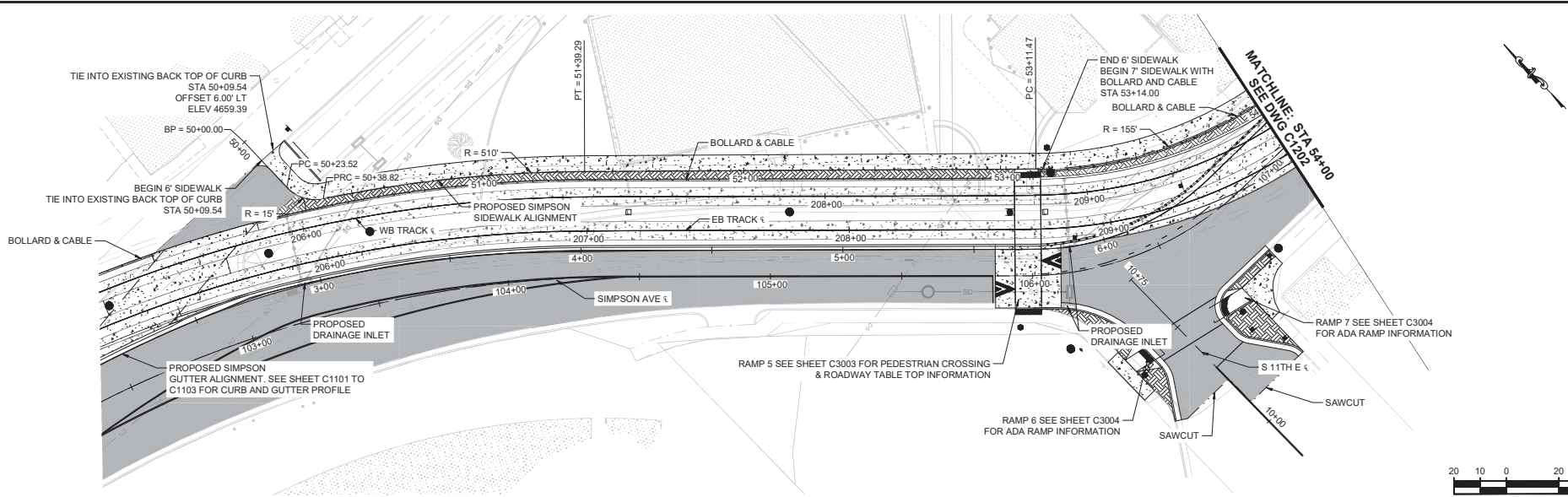
Checked By:  
**A. DIGIROLAMO**

Approved By:  
**F. NANNENGA**

**S-LINE EXTENSION PROJECT**

**PARKING LOT CURB AND GUTTER  
PLAN AND PROFILE  
STA 0+0 TO STA 3+50**

Scale:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-C-1-109.dwg
Submittal Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1109
Sheet No.:	



- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
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  - EP = END POINT

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**M. HARTMANN**

Drawn By:  
**M. HARTMANN**

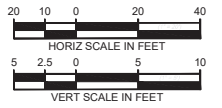
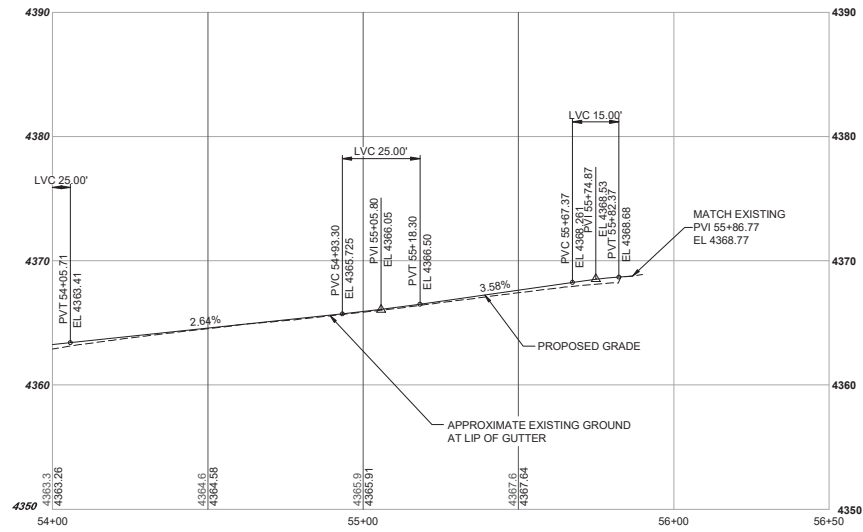
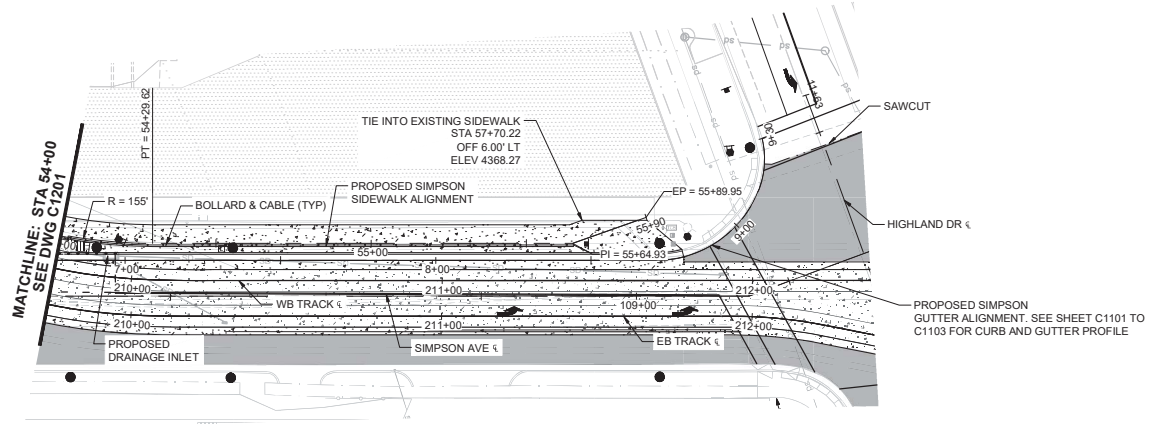
Checked By:  
**A. DIGIROLAMO**

Approved By:  
**F. NANNENGA**

**S-LINE EXTENSION PROJECT**

**SIMPSON SIDEWALK  
PLAN AND PROFILE  
STA 50+12 TO STA 54+00**

Scale: H: 1" = 20' V: 1" = 5'	
CADD Filename: <b>SL-C-1-201.dwg</b>	
Submitted Date: 01-16-26	
UTA Contract No.: 24-03849	
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- LEGEND:**
- PAVEMENT
  - LANDSCAPING
  - CONCRETE
  - BP = BEGIN POINT
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Designed By:  
**M. HARTMANN**

Drawn By:  
**M. HARTMANN**

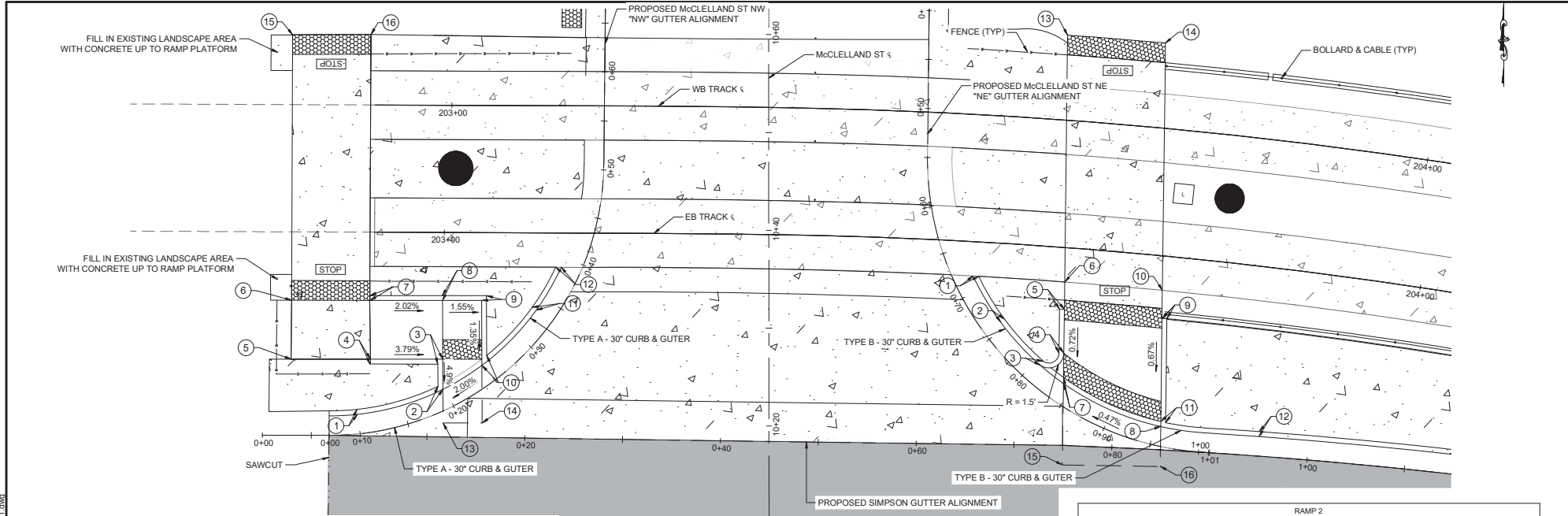
Checked By:  
**A. DIGIROLAMO**

Approved By:  
**F. NANNENGA**

**S-LINE EXTENSION PROJECT**

**SIMPSON SIDEWALK  
PLAN AND PROFILE  
STA 54+00 TO STA 57+25**

Scale:	H: 1" = 20' V: 1" = 5'
CADD Filename:	SL-C-1-202.dwg
Submittal Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C1202
Sheet No.:	



RAMP 1 POINT TABLE				
POINT#	"NW" STATION	OFFSET	ELEV	DESCRIPTION
1	0+09.94	2.00' LT	4353.25'	BEGIN TRANSITION FOR GUTTER PAN SLOPE REDUCTION
	0+09.94	2.50' LT	4353.75'	TOP BACK OF CURB
2	0+19.45	2.50' LT	4354.05'	TOP BACK OF CURB
	0+19.79	2.00' LT	4353.55'	END TRANSITION FOR GUTTER PAN SLOPE REDUCTION
3	0+20.61	4.53' LT	4354.20'	TOP BACK OF CURB - BEGIN END CURB TRANSITION
	0+21.42	4.72' LT	4353.70'	FRONT FACE OF CURB - BEGIN END CURB TRANSITION
4	0+12.44	6.87' LT	4353.98'	FRONT OF SIDEWALK - END OF CURB
	0+12.57	7.37' LT	4353.98'	FRONT OF SIDEWALK - END OF CURB
5	0+02.92	7.77' LT	4353.98'	MATCH EXISTING SIDEWALK
	0+02.95	13.81' LT	4353.94'	MATCH EXISTING SIDEWALK
7	0+14.74	13.21' LT	4353.94'	FRONT OF SIDEWALK - END OF CURB
	0+15.02	13.69' LT	4353.94'	FRONT OF SIDEWALK - END OF CURB
8	0+25.98	9.71' LT	4354.29'	FRONT FACE OF CURB - BEGIN END CURB TRANSITION
	0+26.41	10.11' LT	4353.79'	TOP BACK OF CURB - BEGIN END CURB TRANSITION
9	0+30.10	6.96' LT	4353.73'	FRONT FACE OF CURB - REFERENCE POINT
	0+31.04	6.92' LT	4354.23'	TOP BACK OF CURB - REFERENCE POINT
10	0+24.83	2.00' LT	4353.64'	BEGIN TRANSITION FROM GUTTER PAN SLOPE REDUCTION
	0+25.94	2.50' LT	4354.14'	TOP BACK OF CURB
11	0+33.41	2.00' LT	4353.98'	GRADE BREAK - BEGIN END OF CURB TRANSITION
	0+33.41	2.50' LT	4354.48'	GRADE BREAK - BEGIN END OF CURB TRANSITION
12	0+38.62	2.50' LT	4354.33'	END OF CURB - MATCH TRACK SLAB ELEVATION
	0+38.87	2.00' LT	4354.33'	END OF CURB - MATCH TRACK SLAB ELEVATION
13	0+18.33	1.13' RT	4354.60'	CLEAR SPACE
	0+21.67	2.92' RT	4354.62'	CLEAR SPACE
14	0+63.45	31.94' LT	4353.93'	MATCH EXISTING SIDEWALK

RAMP 2 POINT TABLE				
POINT#	"NE" STATION	OFFSET	ELEV	DESCRIPTION
1	0+68.39	2.00' LT	4354.60'	END OF CURB - MATCH TRACK SLAB ELEVATION
	0+68.69	2.50' LT	4354.60'	END OF CURB - MATCH TRACK SLAB ELEVATION
2	0+74.28	2.00' LT	4354.59'	BEGIN TRANSITION FOR GUTTER PAN SLOPE REDUCTION
	0+74.28	2.50' LT	4355.05'	GRADE BREAK - END END OF CURB TRANSITION
3	0+80.66	2.50' LT	4355.07'	TOP BACK OF CURB
	0+80.72	2.00' LT	4354.57'	END TRANSITION FOR GUTTER PAN SLOPE REDUCTION
4	0+81.42	4.18' LT	4354.91'	TOP BACK OF CURB - BEGIN END CURB TRANSITION
	0+81.88	4.49' LT	4354.60'	FRONT FACE OF CURB - BEGIN END CURB TRANSITION
5	0+77.65	7.48' LT	4354.63'	TOP BACK OF CURB - END CURB
	0+78.11	7.83' LT	4354.63'	FRONT FACE OF CURB - END CURB
6	0+75.16	9.69' LT	4354.65'	MATCH TRACK SLAB ELEVATION
	0+83.55	2.50' LT	4354.58'	TOP BACK OF CURB - REFERENCE POINT
7	0+83.92	2.00' LT	4354.58'	FRONT FACE OF CURB - REFERENCE POINT
	0+95.65	2.00' LT	4354.63'	BEGIN TRANSITION FROM GUTTER PAN SLOPE REDUCTION
8	0+91.66	12.64' LT	4354.70'	FRONT FACE OF CURB - END CURB
	0+92.48	12.81' LT	4354.70'	TOP BACK OF CURB - END CURB
9	0+89.67	15.33' LT	4354.72'	MATCH TRACK SLAB ELEVATION
	0+95.54	2.50' LT	4354.63'	FRONT FACE OF CURB - BEGIN END CURB TRANSITION
10	0+96.09	2.50' LT	4355.14'	TOP BACK OF CURB - BEGIN END CURB TRANSITION
	"SN" 0+94.94	2.00' LT	4354.65'	END TRANSITION FROM GUTTER PAN SLOPE REDUCTION
11	"SN" 0+94.94	2.50' LT	4355.32'	TOP BACK OF CURB
	0+42.36	14.17' LT	4355.32'	MATCH EXISTING SIDEWALK PLAZA
12	0+43.06	24.17' LT	4354.65'	MATCH EXISTING SIDEWALK PLAZA
	0+87.94	5.21' RT	4354.54'	CLEAR SPACE
13	0+96.45	2.00' RT	4354.70'	CLEAR SPACE

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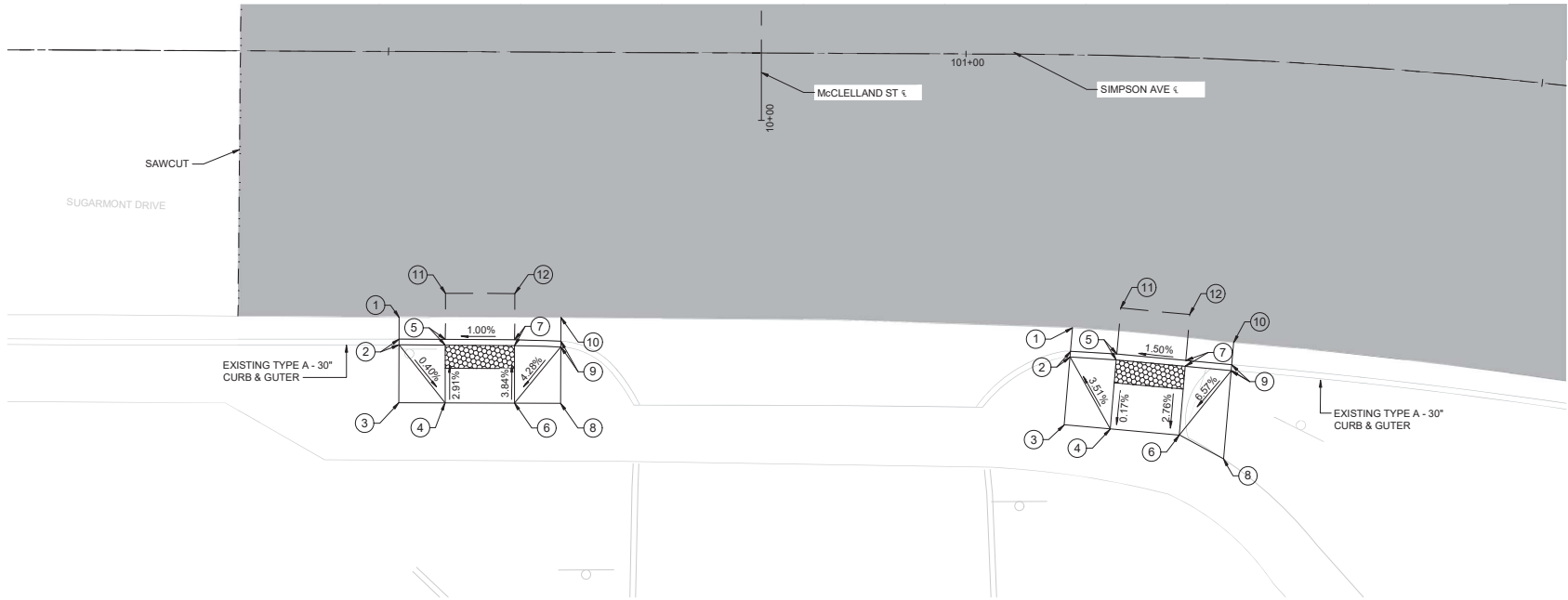
REV	DATE	Description



Designed By: M. HARTMANN  
 Drawn By: M. HARTMANN  
 Checked By: A. DIGIROLAMO  
 Approved By: F. NANNENGA

**S-LINE EXTENSION PROJECT**  
  
 STANDARD DRAWINGS  
  
 ROADWAY  
 CURB RETURN DETAILS

Scale:	NTS
CADD Filename:	SL-C-C-3001.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C3001
Sheet No.:	



RAMP 3				
POINT TABLE				
POINT#	"S" STATION	OFFSET	ELEV	DESCRIPTION
1	100+51.02	23.05' RT	4352.77'	GUTTER - MATCH EXISTING
2	100+51.02	24.92' RT	4352.74'	FRONT FACE OF CURB - MATCH EXISTING
	100+51.03	25.42' RT	4353.08'	TOP BACK OF CURB - MATCH EXISTING
3	100+51.03	30.44' RT	4352.99'	MATCH EXISTING SIDEWALK
4	100+55.03	30.44' RT	4353.05'	MATCH EXISTING SIDEWALK
5	100+55.02	24.93' RT	4352.91'	FRONT FACE OF CURB
	100+55.03	25.44' RT	4352.91'	TOP BACK OF CURB
6	100+61.03	30.43' RT	4353.16'	MATCH EXISTING SIDEWALK
	100+61.03	24.86' RT	4352.97'	FRONT FACE OF CURB
7	100+61.03	25.44' RT	4352.97'	TOP BACK OF CURB
	100+65.03	30.43' RT	4353.25'	MATCH EXISTING SIDEWALK
8	100+65.02	25.05' RT	4352.99'	FRONT FACE OF CURB - MATCH EXISTING
	100+65.03	25.55' RT	4353.43'	TOP BACK OF CURB - MATCH EXISTING
9	100+65.02	22.98' RT	4353.13'	GUTTER - MATCH EXISTING
11	100+55.02	20.94' RT	4353.06'	CLEAR SPACE
12	100+61.02	20.94' RT	4353.16'	CLEAR SPACE

RAMP 4				
POINT TABLE				
POINT#	"S" STATION	OFFSET	ELEV	DESCRIPTION
1	101+09.61	23.67' RT	4353.69'	GUTTER - MATCH EXISTING
2	101+09.46	25.71' RT	4353.56'	FRONT FACE OF CURB - MATCH EXISTING
	101+09.42	26.21' RT	4353.96'	TOP BACK OF CURB - MATCH EXISTING
3	101+08.96	32.07' RT	4353.85'	MATCH EXISTING SIDEWALK
4	101+13.26	32.36' RT	4353.71'	MATCH EXISTING SIDEWALK
5	101+13.70	25.87' RT	4353.72'	FRONT FACE OF CURB
	101+13.67	26.37' RT	4353.72'	TOP BACK OF CURB
6	101+19.74	32.70' RT	4353.64'	MATCH EXISTING SIDEWALK
	101+20.06	26.23' RT	4353.81'	FRONT FACE OF CURB
7	101+20.03	26.73' RT	4353.81'	TOP BACK OF CURB
	101+23.96	34.61' RT	4353.54'	MATCH EXISTING SIDEWALK
8	101+24.30	26.43' RT	4353.96'	FRONT FACE OF CURB - MATCH EXISTING
	101+24.28	26.93' RT	4354.12'	TOP BACK OF CURB - MATCH EXISTING
9	101+24.38	24.51' RT	4353.90'	GUTTER - MATCH EXISTING
10	101+13.96	21.87' RT	4353.95'	CLEAR SPACE
12	101+20.26	22.24' RT	4353.89'	CLEAR SPACE

REVISION	DATE	DESCRIPTION



Designed By:  
M. HARTMANN

Drawn By:  
M. HARTMANN

Checked By:  
A. DIGIROLAMO

Approved By:  
F. NANNENGA

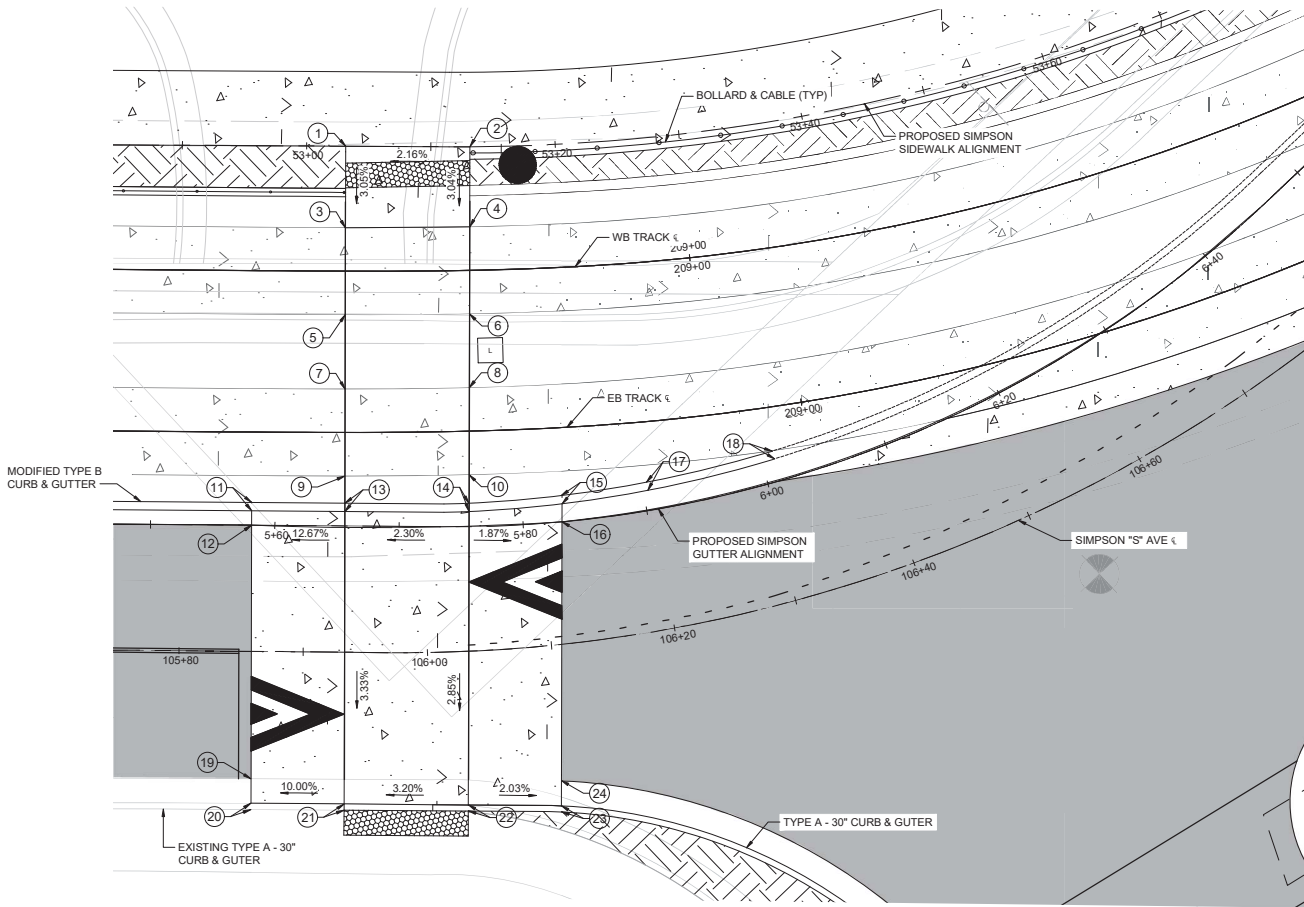
S-LINE EXTENSION PROJECT

STANDARD DRAWINGS

ROADWAY

CURB RETURN DETAILS

State:	NTS
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Submission Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C3002
Sheet No.:	

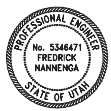


RAMP 5 - PEDESTRIAN WALK POINT TABLE				
POINT#	"S" STATION	OFFSET	ELEV	DESCRIPTION
1	105+91.90	40.60' LT	4360.59'	MATCH SIDEWALK ELEVATION
2	106+05.46	40.33' LT	4360.81'	MATCH SIDEWALK ELEVATION
3	105+91.94	34.04' LT	4360.39'	MATCH TRACK SLAB ELEVATION
4	106+04.72	33.90' LT	4360.61'	MATCH TRACK SLAB ELEVATION
5	105+91.98	27.04' LT	4360.39'	MATCH TRACK SLAB ELEVATION
6	106+04.03	26.92' LT	4360.60'	MATCH TRACK SLAB ELEVATION
7	105+92.01	21.05' LT	4360.39'	MATCH TRACK SLAB ELEVATION
8	106+03.53	20.98' LT	4360.60'	MATCH TRACK SLAB ELEVATION
9	105+92.05	14.05' LT	4360.39'	MATCH TRACK SLAB ELEVATION
10	106+03.00	14.00' LT	4360.60'	MATCH TRACK SLAB ELEVATION

RAMP 5 - PEDESTRIAN WALK Cont. POINT TABLE				
POINT#	"S" STATION	OFFSET	ELEV	DESCRIPTION
11	105+84.57	11.20' LT	4359.44'	FRONT FACE OF CURB
	105+84.56	11.86' LT	4360.19'	BACK TOP OF CURB
12	105+84.57	10.03' LT	4359.32'	LIP OF GUTTER
	105+92.07	11.86' LT	4360.39'	FRONT FACE OF CURB
13	105+92.07	11.20' LT	4360.39'	BACK TOP OF CURB
	106+02.85	11.71' LT	4360.60'	FRONT FACE OF CURB
14	106+02.80	11.04' LT	4360.60'	BACK TOP OF CURB
	106+11.08	10.97' LT	4360.46'	FRONT FACE OF CURB
15	106+11.18	11.63' LT	4361.21'	BACK TOP OF CURB
	106+18.73	9.34' LT	4360.33'	LIP OF GUTTER
16	106+18.98	10.79' LT	4360.75'	GRADE BREAK - BEGIN END OF CURB TRANSITION
	106+19.00	11.46' LT	4361.50'	GRADE BREAK - BEGIN END OF CURB TRANSITION
17	106+30.09	11.00' LT	4361.08'	END OF CURB - MATCH TRACK SLAB ELEVATION
	106+30.08	11.65' LT	4361.08'	END OF CURB - MATCH TRACK SLAB ELEVATION
18	105+84.70	10.38' RT	4359.07'	LIP OF GUTTER
	105+84.71	12.38' RT	4358.86'	FRONT FACE OF CURB
19	105+84.71	12.88' RT	4359.36'	BACK TOP OF CURB
	105+92.21	12.36' RT	4359.61'	FRONT FACE OF CURB
20	105+92.21	12.86' RT	4359.61'	BACK TOP OF CURB
	106+01.52	12.54' RT	4359.93'	FRONT FACE OF CURB
21	106+01.50	13.04' RT	4359.93'	BACK TOP OF CURB
	106+08.27	13.14' RT	4359.78'	FRONT FACE OF CURB
22	106+08.22	13.70' RT	4360.28'	BACK TOP OF CURB
	106+08.46	11.19' RT	4359.99'	LIP OF GUTTER

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Designed By	M. HARTMANN
Drawn By	M. HARTMANN
Checked By	A. DIGIROLAMO
Approved By	F. NANNENGA

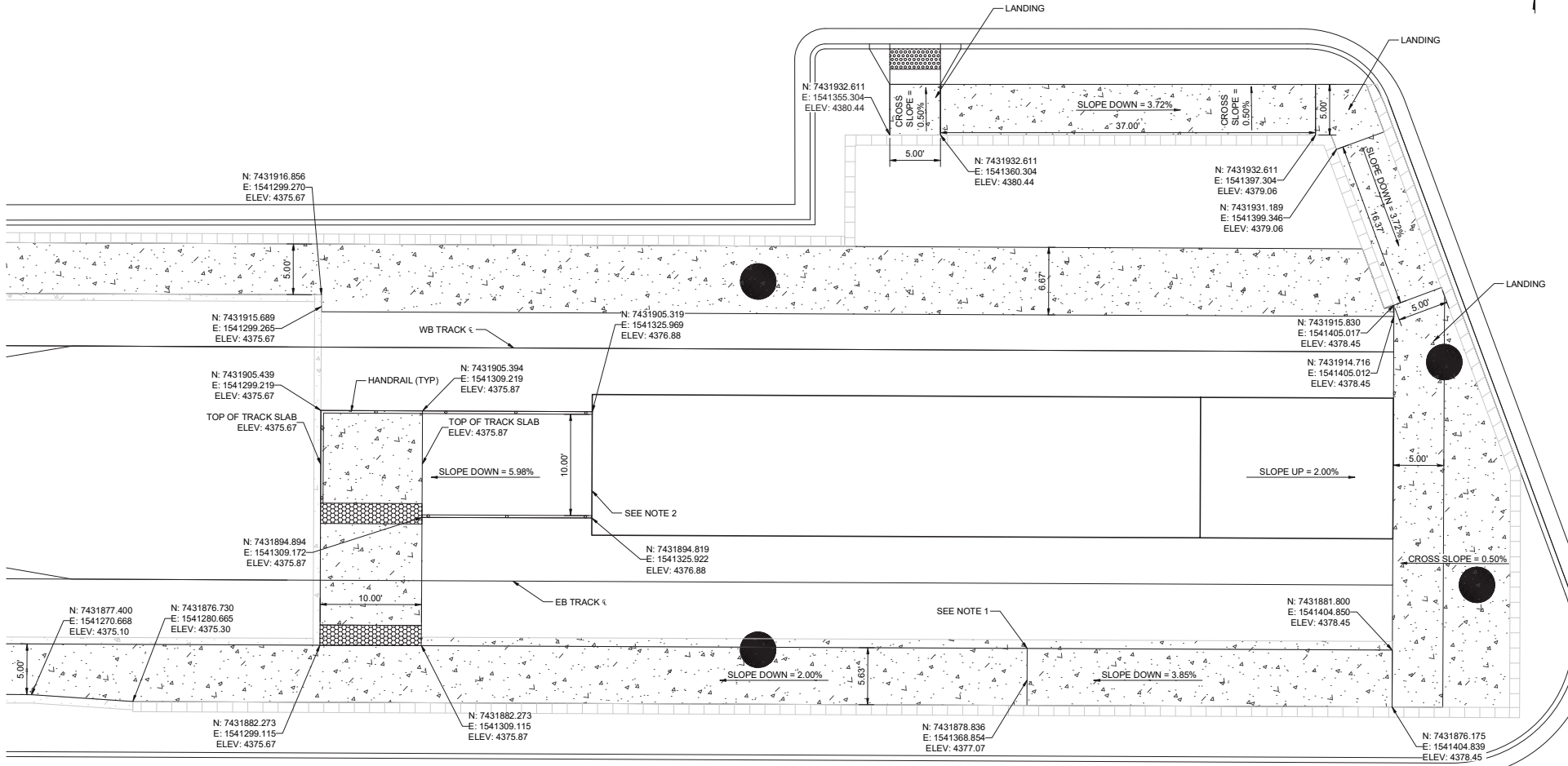
S-LINE EXTENSION PROJECT  
ROADWAY CURB RETURN DETAILS

State	NTS
CADD Filename	SL-C-3-003.dwg
Submitted Date	01-16-26
UTA Contract No.	24-03849
Drawing No.	C3003
Sheet No.	





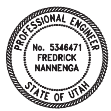
- NOTES:  
 1. SEE DRAWING C3005 FOR ELEVATIONS AT THE WEST END OF THE SIDEWALK.  
 2. SEE DRAWING P3203 FOR STATION PLATFORM RAMP ELEVATIONS.



**HIGHLAND PLATFORM AREA - SIDEWALK PLAN AND ELEVATION PLAN**  
 SCALE: 1" = 5'



11/2/2023			
REV	DATE	Description	



Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

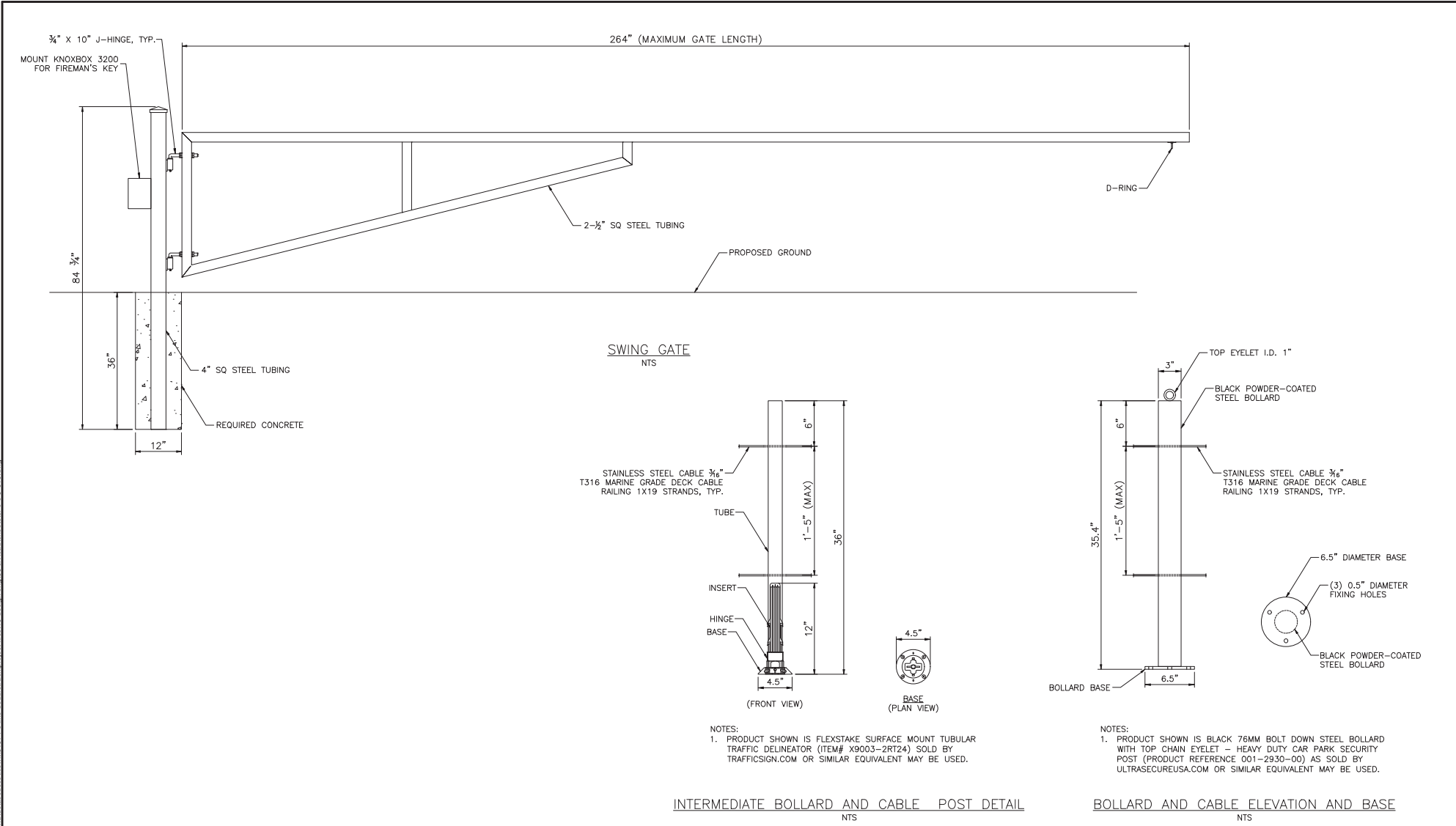
Designed By: S. JOSHI  
 Drawn By: S. JOSHI  
 Checked By: A. DIGIROLAMO  
 Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
 SUGAR HOUSE PLATFORM AREA  
 PEDESTRIAN RAMP AND SIDEWALK PLAN

Scale:	H: 1" = 5'
CADD Filename:	SL-C-3-006.dwg
Submital Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	C3006
Sheet No.:	



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NOTES:  
 1. PRODUCT SHOWN IS FLEXSTAKE SURFACE MOUNT TUBULAR TRAFFIC DELINEATOR (ITEM# X9003-2RT24) SOLD BY TRAFFICSIGN.COM OR SIMILAR EQUIVALENT MAY BE USED.

NOTES:  
 1. PRODUCT SHOWN IS BLACK 76MM BOLT DOWN STEEL BOLLARD WITH TOP CHAIN EYELET - HEAVY DUTY CAR PARK SECURITY POST (PRODUCT REFERENCE 001-2930-00) AS SOLD BY ULTRASECUREUSA.COM OR SIMILAR EQUIVALENT MAY BE USED.

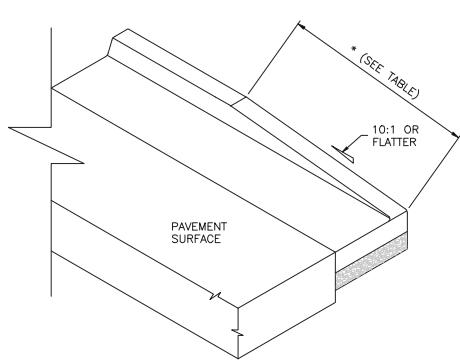
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REV	DATE	Description	



Designed By: M. HARTMANN
Drawn By: M. HARTMANN
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
 STANDARD DRAWINGS  
 CIVIL  
 BOLLARD AND SWING GATE DETAILS

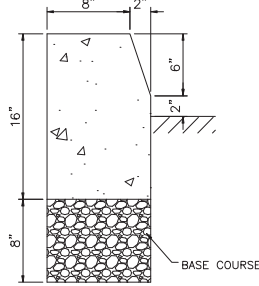
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Submital Date: 01-16-26
UTA Contract No.: 24-03849
Drawing No.: C3010
Sheet No.:



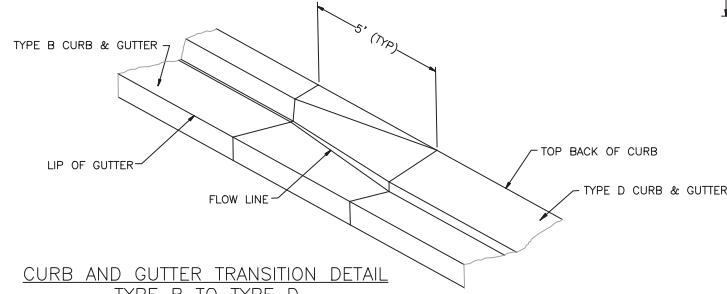
CURB & GUTTER END TRANSITION  
NTS

MIN END TRANSITION LENGTH	
CURB TYPE	* (FT)
A	5
B	7
A - 24"	5
9" MODIFIED	9

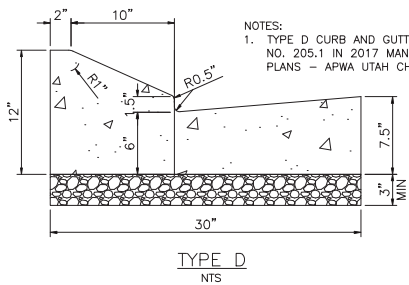
NOTES:  
1. TYPE Q CURB BASED ON PLAN NO. 209 IN 2017 MANUAL OF STANDARD PLANS - APWA UTAH CHAPTER



TYPE Q CURB  
NTS

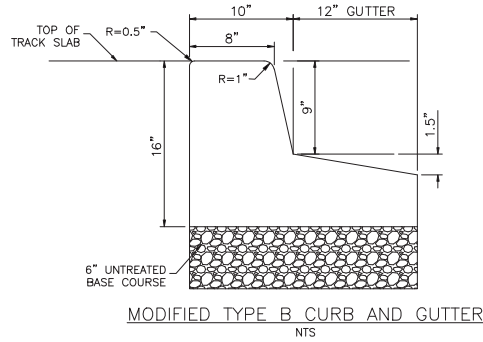


CURB AND GUTTER TRANSITION DETAIL  
TYPE B TO TYPE D  
NTS



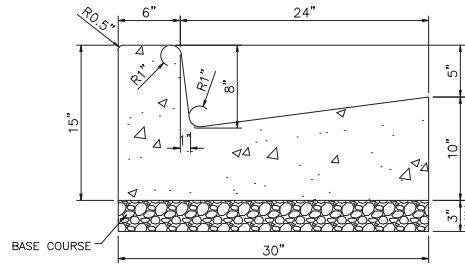
NOTES:  
1. TYPE D CURB AND GUTTER BASED ON PLAN NO. 205.1 IN 2017 MANUAL OF STANDARD PLANS - APWA UTAH CHAPTER.

TYPE D  
NTS



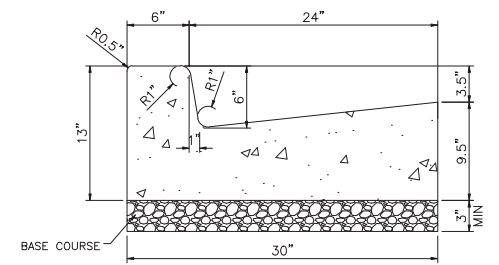
MODIFIED TYPE B CURB AND GUTTER  
NTS

NOTES:  
1. TYPE B CURB AND GUTTER BASED ON PLAN NO. 205.2 IN 2025 MANUAL OF STANDARD PLANS - APWA UTAH CHAPTER.  
2. 2" CONTRACTION JOINT USED, SEE PLAN NO. 205.2.

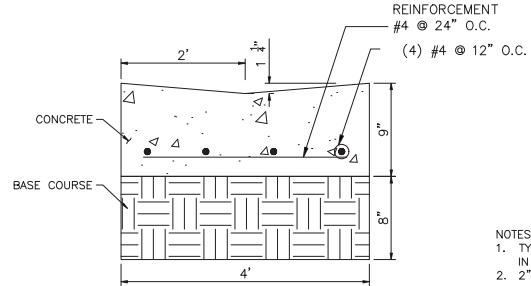


TYPE B - 30" STANDARD  
NTS

NOTES:  
1. TYPE A CURB AND GUTTER BASED ON PLAN NO. 205.2 IN 2025 MANUAL OF STANDARD PLANS - APWA UTAH CHAPTER.  
2. 2" CONTRACTION JOINT USED, SEE PLAN NO. 205.2.

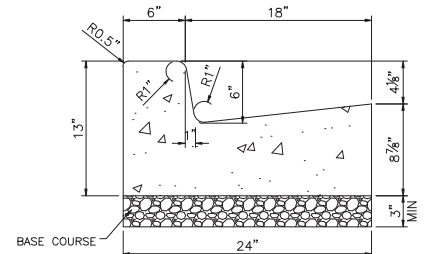


TYPE A - 30" STANDARD  
NTS



4'-0" WATERWAY  
NTS

NOTES:  
1. TYPE A - 24" CURB AND GUTTER BASED ON PLAN NO. 205.1 IN 2025 MANUAL OF STANDARD PLANS - APWA UTAH CHAPTER.  
2. 2" CONTRACTION JOINT USED, SEE PLAN NO. 205.1.



TYPE A - 24" STANDARD  
NTS

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REV	DATE	Description

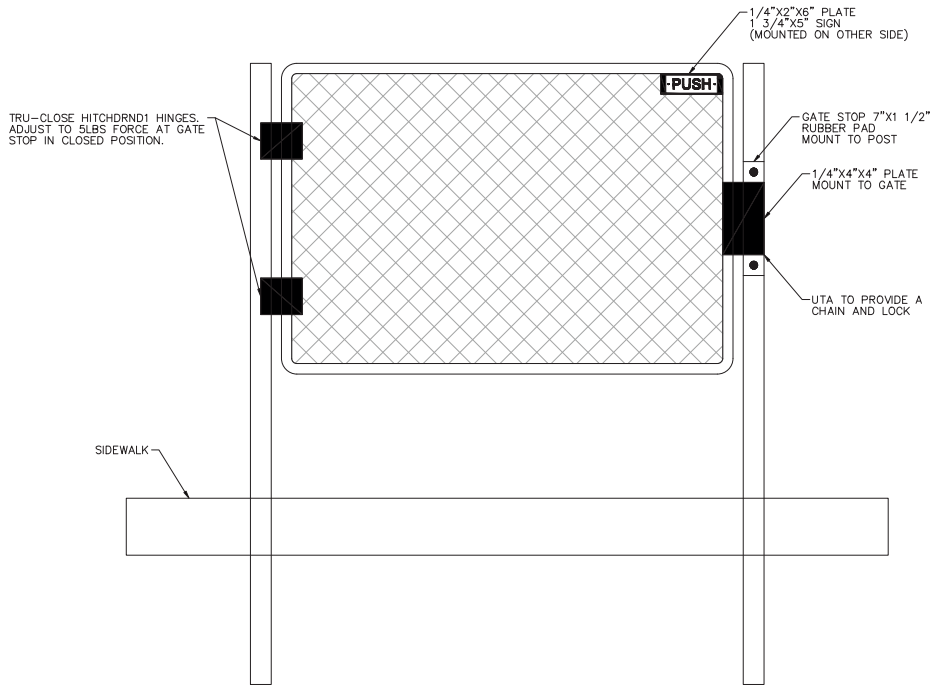


Designed By:  
**M. HARTMANN**  
Drawn By:  
**M. HARTMANN**  
Checked By:  
**A. DIGIROLAMO**  
Approved By:  
**F. NANNENGA**

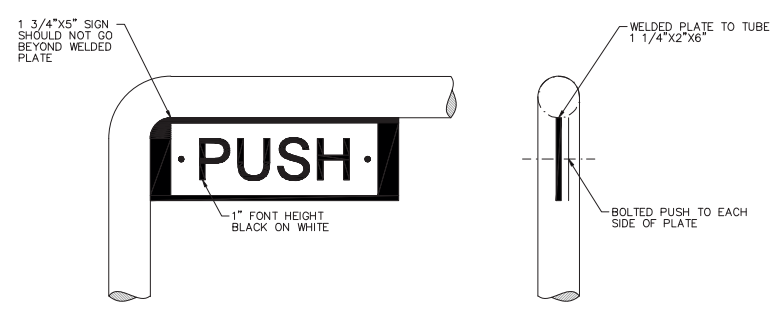
S-LINE EXTENSION PROJECT  
  
CIVIL  
CURB TRANSITIONS AND DETAILS

State	NTS
CADD Filename	SL-C-3-011.dwg
Submitted Date	01-16-26
UTA Contract No.	24-03849
Drawing No.	C3011
Sheet No.	

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GATE DETAIL



PUSH SIGN DETAIL

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REV	DATE	Description	

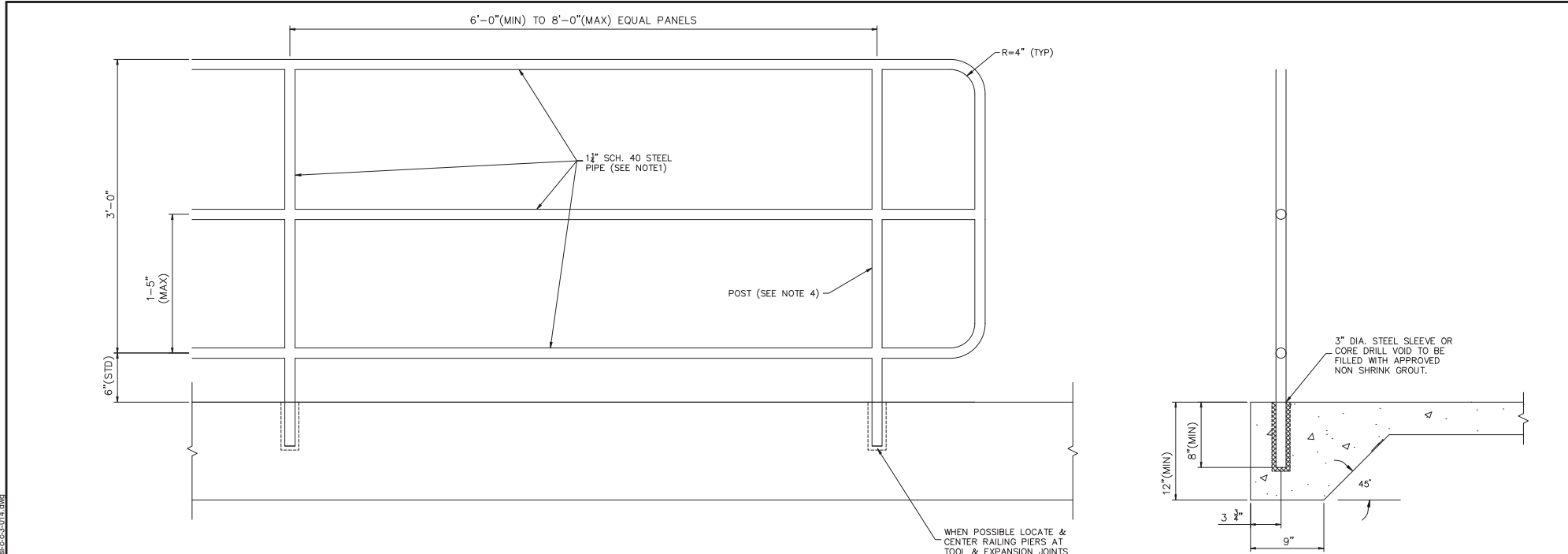


Designed By	T. COVEYOU
Drawn By	T. COVEYOU
Checked By	A. DIGIROLAMO
Approved By	F. NANNENGA

S-LINE EXTENSION PROJECT	
STANDARD DRAWINGS	
CIVIL GATE DETAILS	

State	NTS
CADD Filename	SL-C-3-013.dwg
Submital Date	01-16-26
UTA Contract No.	24-03849
Drawing No.	C3013
Sheet No.	

Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

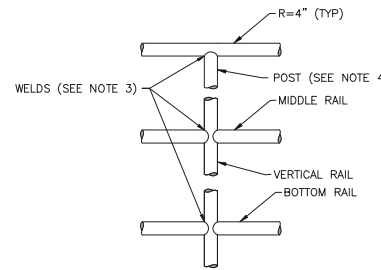


PLAN VIEW

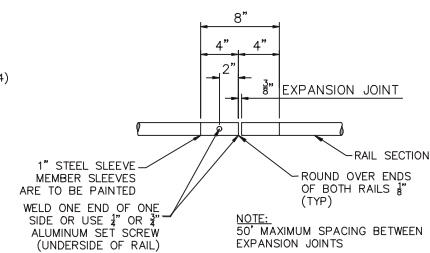
SECTION VIEW

NOTES:

1. CONSTRUCT PROPOSED STEEL PIPE RAIL OF 1 1/4" SCHEDULE 40 PLAIN END STEEL PIPE MEETING THE REQUIREMENTS OF ASTM A53.
2. CONSTRUCT STEEL PIPE IN ACCORDANCE WITH SECTION 05520 HANDRAILS AND RAILINGS STANDARD SPECIFICATION.
3. WELD IN ACCORDANCE WITH 05520 HANDRAILS AND RAILINGS STANDARD SPECIFICATIONS.
4. ALL POST SECTIONS ARE TO BE CONSTRUCTED PLUMB, NOT PERPENDICULAR TO THE SIDEWALK.
5. USE APPROVED NON-SHRINK GROUT IN ACCORDANCE WITH SECTION 05520 HANDRAILS AND RAILINGS STANDARD SPECIFICATIONS FOR HANDRAIL FOOTINGS.
6. PLACEMENT OF HANDRAIL IN RELATION TO WALL AND SIDEWALK MAY BE MODIFIED AS DIRECTED BY THE ENGINEER.
7. PIPE SURFACE TO BE PREPARED BEFORE PRIMING IN ACCORDANCE WITH 05520 HANDRAILS AND RAILINGS STANDARD SPECIFICATIONS.
8. PIPE TO BE DELIVERED TO WORK SITE PRIMED AND PAINTED.

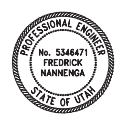


RAIL CONNECTION



RAILING EXPANSION JOINT

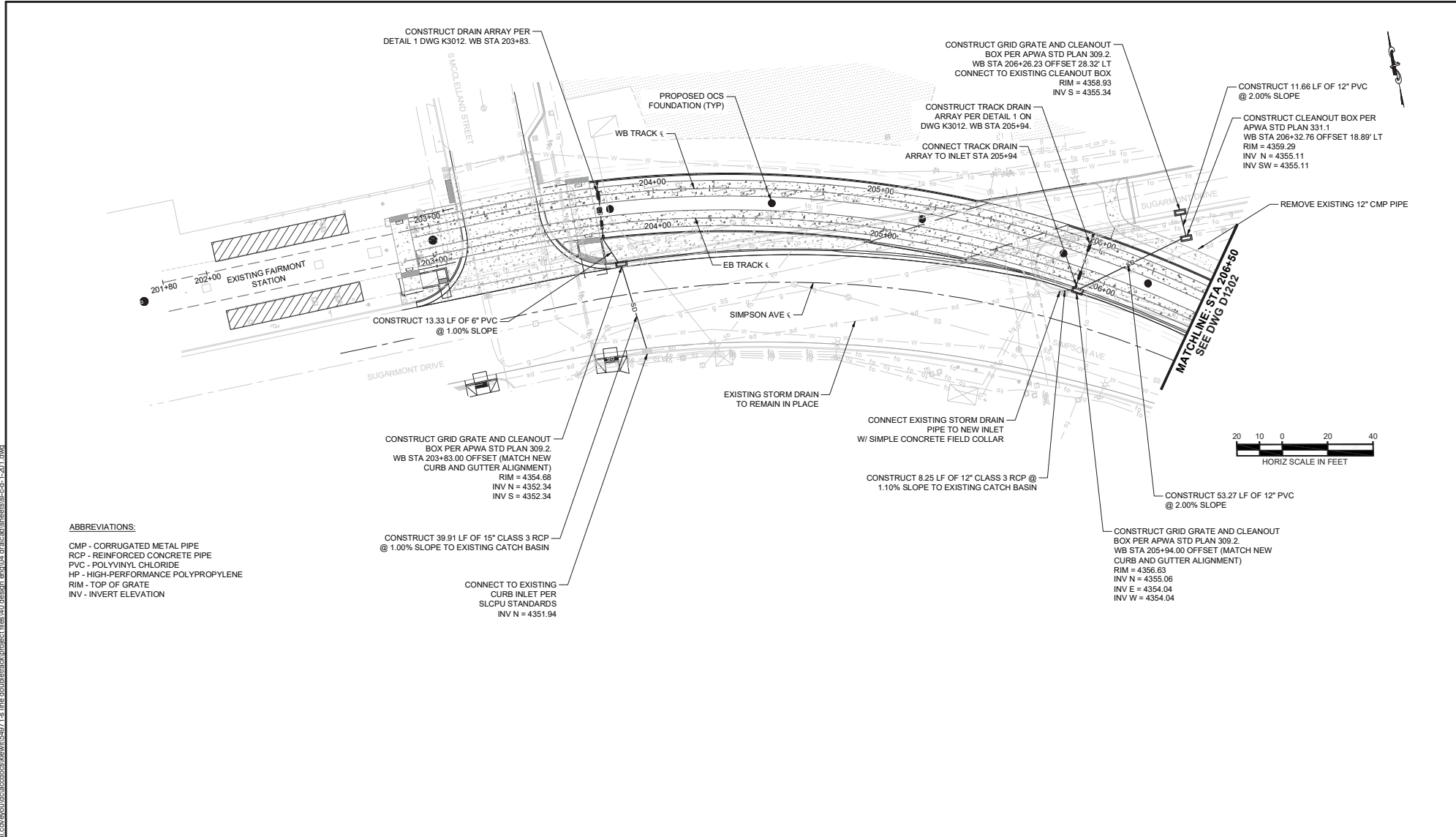
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REV	DATE	Description



Designed By  
M. HARTMANN  
Drawn By  
M. HARTMANN  
Checked By  
A. DIGIROLAMO  
Approved By  
F. NANNENGA

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
CIVIL  
PEDESTRIAN SAFETY RAILING

State	NTS
CADD Filename	SL-C-3-014.dwg
Submittal Date	01-16-26
UTA Contract No.	24-03849
Drawing No.	C3014
Sheet No.	



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REV	DATE	Description



Designed By:	E. JONES
Drawn By:	E. JONES
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**

**DRAINAGE PLAN**  
WB STA 201+80.27 TO WB STA 206+50

Scale: 1" = 20'

CADD Filename: SL-C-D-1-201.dwg

Submital Date: 01-16-26

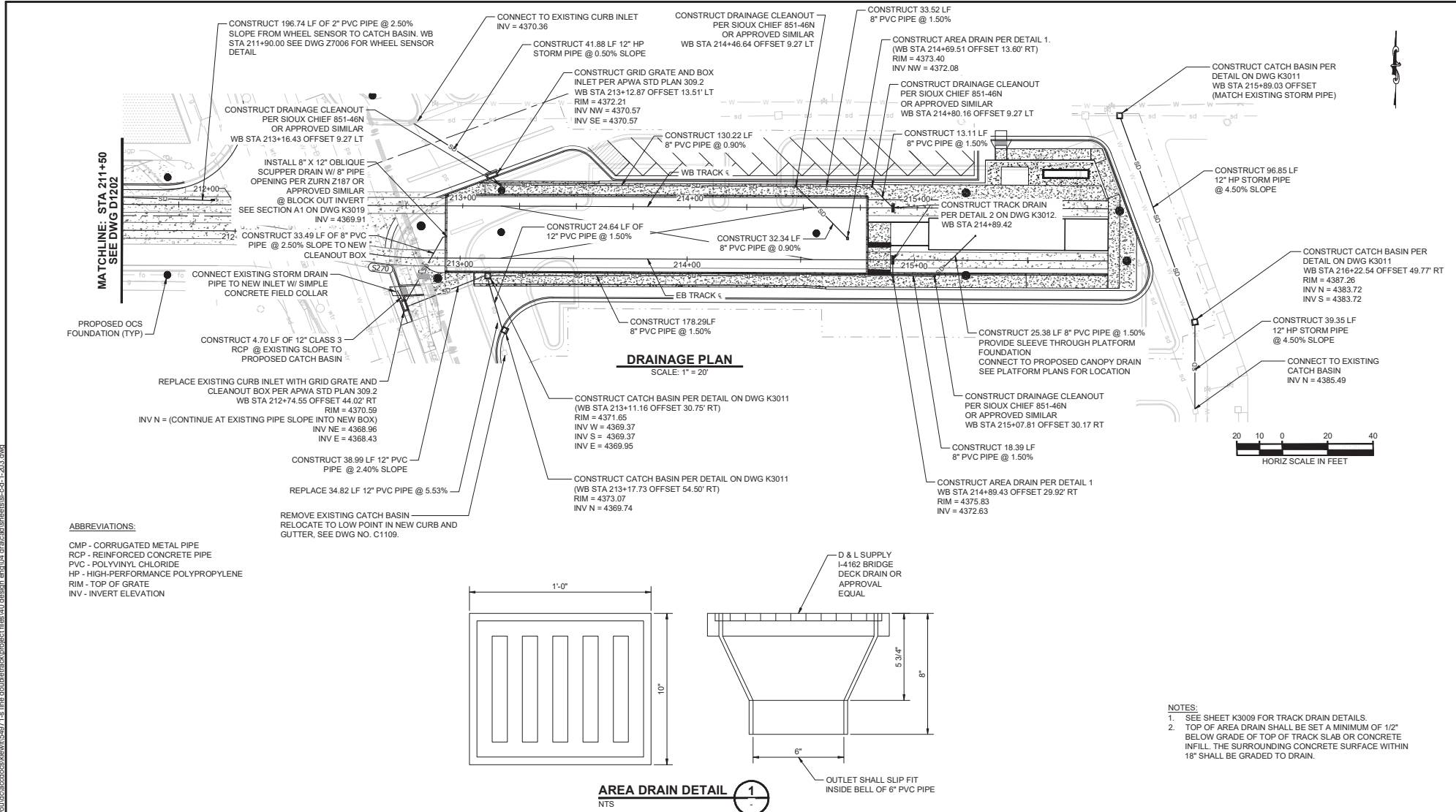
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Drawing No.: D1201

Sheet No.:

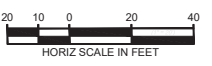
1/16/2026



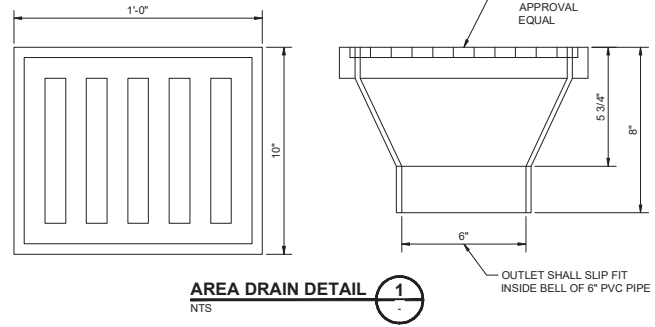


MATCHLINE: STA 211+50  
SEE DWG D1202

**DRAINAGE PLAN**  
SCALE: 1" = 20'



- ABBREVIATIONS:**
- CMP - CORRUGATED METAL PIPE
  - RCP - REINFORCED CONCRETE PIPE
  - PVC - POLYVINYL CHLORIDE
  - HP - HIGH-PERFORMANCE POLYPROPYLENE
  - RIM - TOP OF GRATE
  - INV - INVERT ELEVATION



- NOTES:**
- SEE SHEET K3009 FOR TRACK DRAIN DETAILS.
  - TOP OF AREA DRAIN SHALL BE SET A MINIMUM OF 12" BELOW GRADE OF TOP OF TRACK SLAB OR CONCRETE INFILL. THE SURROUNDING CONCRETE SURFACE WITHIN 18" SHALL BE GRADED TO DRAIN.

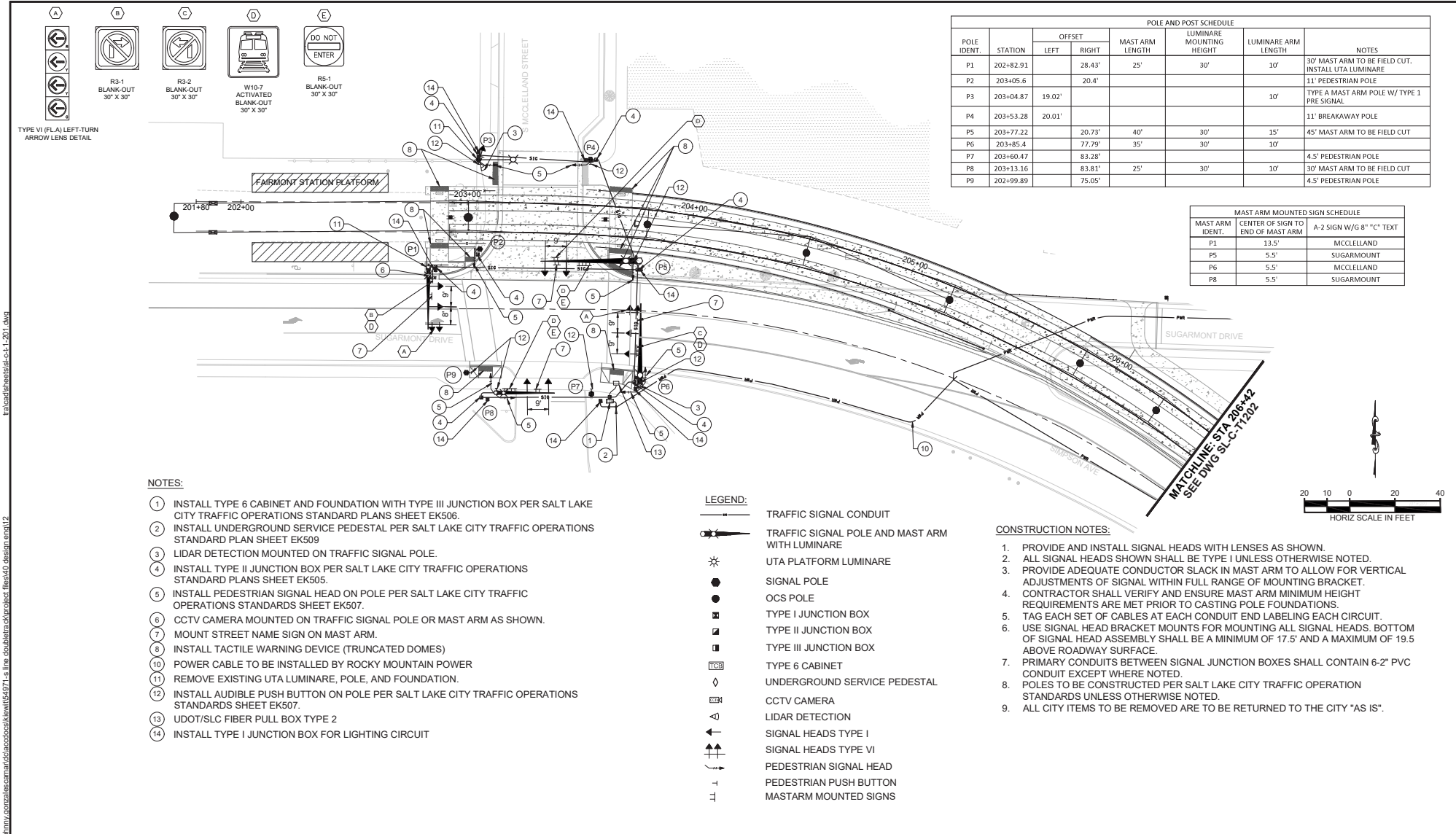
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REV	DATE	Description



Designed By:	E. JONES
Drawn By:	E. JONES
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

S-LINE EXTENSION PROJECT	
DRAINAGE PLAN WB STA 211+50 TO WB STA 215+84.18	
State:	AS SHOWN
CADD Filename:	SL-C-D-1-203.dwg
Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	D1203
Sheet No.:	



POLE AND POST SCHEDULE							
POLE IDENT.	STATION	OFFSET		MAST ARM LENGTH	LUMINAIRE MOUNTING HEIGHT	LUMINAIRE ARM LENGTH	NOTES
		LEFT	RIGHT				
P1	202+82.91		28.43'	25'	30'	10'	30' MAST ARM TO BE FIELD CUT. INSTALL UTA LUMINAIRE
P2	203+05.6		20.4'				11' PEDESTRIAN POLE
P3	203+04.87	19.02'				10'	TYPE A MAST ARM POLE W/ TYPE 1 PRE SIGNAL
P4	203+53.28	20.01'					11' BREAKAWAY POLE
P5	203+77.22		20.73'	40'	30'	15'	45' MAST ARM TO BE FIELD CUT
P6	203+85.4		77.79'	35'	30'	10'	
P7	203+60.47		83.28'				4.5' PEDESTRIAN POLE
P8	203+13.16		83.81'	25'	30'	10'	30' MAST ARM TO BE FIELD CUT
P9	202+99.89		75.05'				4.5' PEDESTRIAN POLE

MAST ARM MOUNTED SIGN SCHEDULE		
MAST ARM IDENT.	CENTER OF SIGN TO END OF MAST ARM	A-2 SIGN W/G 8" "C" TEXT
P1	13.5'	MCCLELLAND
P5	5.5'	SUGARMOUNT
P6	5.5'	MCCLELLAND
P8	5.5'	SUGARMOUNT

**NOTES:**

- 1 INSTALL TYPE 6 CABINET AND FOUNDATION WITH TYPE III JUNCTION BOX PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK506.
- 2 INSTALL UNDERGROUND SERVICE PEDESTAL PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLAN SHEET EK509
- 3 LIDAR DETECTION MOUNTED ON TRAFFIC SIGNAL POLE.
- 4 INSTALL TYPE II JUNCTION BOX PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK505.
- 5 INSTALL PEDESTRIAN SIGNAL HEAD ON POLE PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARDS SHEET EK507.
- 6 CCTV CAMERA MOUNTED ON TRAFFIC SIGNAL POLE OR MAST ARM AS SHOWN.
- 7 MOUNT STREET NAME SIGN ON MAST ARM.
- 8 INSTALL TACTILE WARNING DEVICE (TRUNCATED DOMES)
- 9 POWER CABLE TO BE INSTALLED BY ROCKY MOUNTAIN POWER
- 10 REMOVE EXISTING UTA LUMINAIRE, POLE, AND FOUNDATION.
- 12 INSTALL AUDIBLE PUSH BUTTON ON POLE PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARDS SHEET EK507.
- 13 UDOT/SLC FIBER PULL BOX TYPE 2
- 14 INSTALL TYPE I JUNCTION BOX FOR LIGHTING CIRCUIT

**LEGEND:**

- TRAFFIC SIGNAL CONDUIT
- TRAFFIC SIGNAL POLE AND MAST ARM WITH LUMINAIRE
- ☼ UTA PLATFORM LUMINAIRE
- SIGNAL POLE
- OCS POLE
- TYPE I JUNCTION BOX
- TYPE II JUNCTION BOX
- TYPE III JUNCTION BOX
- ▣ TYPE 6 CABINET
- ◇ UNDERGROUND SERVICE PEDESTAL
- 📷 CCTV CAMERA
- ▲ LIDAR DETECTION
- ← SIGNAL HEADS TYPE I
- ↕ SIGNAL HEADS TYPE VI
- ↑ PEDESTRIAN SIGNAL HEAD
- ↑ PEDESTRIAN PUSH BUTTON
- ↑ MASTARM MOUNTED SIGNS

**CONSTRUCTION NOTES:**

1. PROVIDE AND INSTALL SIGNAL HEADS WITH LENSES AS SHOWN.
2. ALL SIGNAL HEADS SHOWN SHALL BE TYPE I UNLESS OTHERWISE NOTED.
3. PROVIDE ADEQUATE CONDUCTOR SLACK IN MAST ARM TO ALLOW FOR VERTICAL ADJUSTMENTS OF SIGNAL WITHIN FULL RANGE OF MOUNTING BRACKET.
4. CONTRACTOR SHALL VERIFY AND ENSURE MAST ARM MINIMUM HEIGHT REQUIREMENTS ARE MET PRIOR TO CASTING POLE FOUNDATIONS.
5. TAG EACH SET OF CABLES AT EACH CONDUIT END LABELING EACH CIRCUIT.
6. USE SIGNAL HEAD BRACKET MOUNTS FOR MOUNTING ALL SIGNAL HEADS. BOTTOM OF SIGNAL HEAD ASSEMBLY SHALL BE A MINIMUM OF 17.5' AND A MAXIMUM OF 19.5' ABOVE ROADWAY SURFACE.
7. PRIMARY CONDUITS BETWEEN SIGNAL JUNCTION BOXES SHALL CONTAIN 6-2" PVC CONDUIT EXCEPT WHERE NOTED.
8. POLES TO BE CONSTRUCTED PER SALT LAKE CITY TRAFFIC OPERATION STANDARDS UNLESS OTHERWISE NOTED.
9. ALL CITY ITEMS TO BE REMOVED ARE TO BE RETURNED TO THE CITY "AS IS".



11/02/2018			
REV	DATE	Description	



Designed By	J. GONZALES
Drawn By	J. GONZALES
Checked By	L. B. KHAN
Approved By	F. NANNENGA

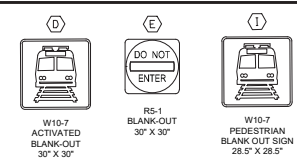
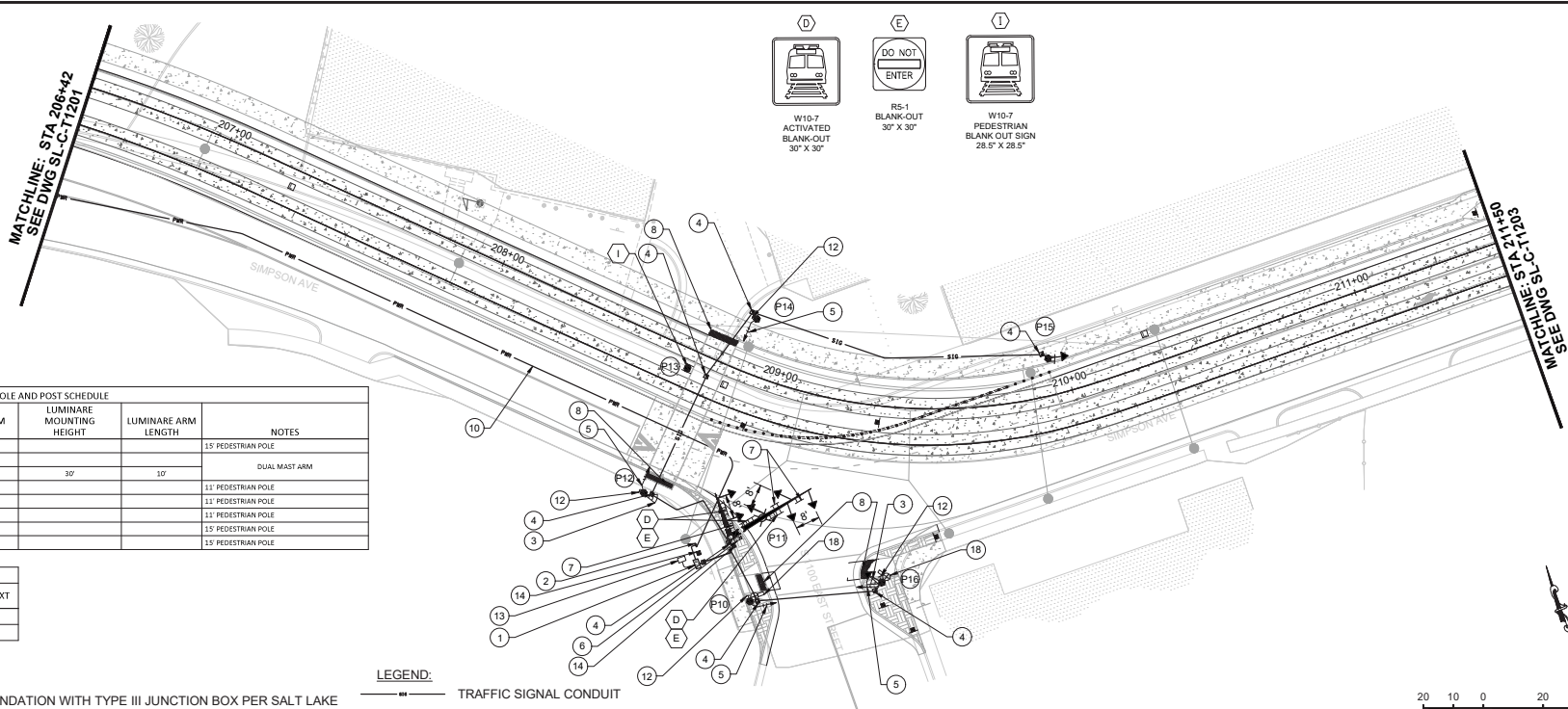
S-LINE EXTENSION PROJECT  
 TRAFFIC SIGNAL PLANS  
 WB STA 201+60 TO WB STA 206+42

Scale	1" = 20'
CADD Filename	SL-C-T-1-201.dwg
Submital Date	1-16-26
UTA Contract No.	24-03849
Drawing No.	T1201
Sheet No.	

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POLE AND POST SCHEDULE							
POLE IDENT.	STATION	OFFSET		MAST ARM LENGTH	LUMINARE MOUNTING HEIGHT	LUMINARE ARM LENGTH	NOTES
		LEFT	RIGHT				
P10	209+07.33		83.93'				15' PEDESTRIAN POLE
P11	208+93.58		64.35'	15'	30'	10'	DUAL MAST ARM
P12	208+79.1		51.25'				11' PEDESTRIAN POLE
P13	208+80.11		6.3'				11' PEDESTRIAN POLE
P14	208+84.42	18.04'					11' PEDESTRIAN POLE
P15	209+91.61	9.49'					15' PEDESTRIAN POLE
P16	209+35.15		46.36'				15' PEDESTRIAN POLE

MAST ARM MOUNTED SIGN SCHEDULE		
MAST ARM IDENT.	CENTER OF SIGN TO END OF MAST ARM	A-Z SIGN W/G 8" "C" TEXT
P11	12"	SUGARMOUNT
	5'12"	SIMPSON

**NOTES:**

- 1 INSTALL TYPE 6 CABINET AND FOUNDATION WITH TYPE III JUNCTION BOX PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK506.
- 2 INSTALL UNDERGROUND SERVICE PEDESTAL PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLAN SHEET EK509
- 3 LIDAR DETECTION MOUNTED ON TRAFFIC SIGNAL POLE.
- 4 INSTALL TYPE II JUNCTION BOX PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK505.
- 5 INSTALL PEDESTRIAN SIGNAL HEAD ON POLE PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARDS SHEET EK507.
- 6 CCTV CAMERA MOUNTED ON TRAFFIC SIGNAL POLE OR MAST ARM AS SHOWN.
- 7 MOUNT STREET NAME SIGN ON MAST ARM.
- 8 INSTALL TACTILE WARNING DEVICE (TRUNCATED DOMES)
- 10 POWER CABLE TO BE INSTALLED BY ROCKY MOUNTAIN POWER
- 12 INSTALL AUDIBLE PUSH BUTTON ON POLE PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARDS SHEET EK507.
- 13 UDOT/SLC FIBER PULL BOX TYPE 2
- 14 INSTALL TYPE I JUNCTION BOX FOR LIGHTING CIRCUIT
- 16 INSTALL UTA PTZ CAMERA ON POLE.

**LEGEND:**

- TRAFFIC SIGNAL CONDUIT
- TRAFFIC SIGNAL POLE AND MAST ARM WITH LUMINARE
- PEDESTRIAN SIGNAL POLE
- OCS POLE
- TYPE I JUNCTION BOX
- TYPE II JUNCTION BOX
- TYPE III JUNCTION BOX
- TYPE 6 CABINET
- UNDERGROUND SERVICE PEDESTAL
- CCTV CAMERA
- LIDAR DETECTION
- SIGNAL HEADS TYPE I
- SIGNAL HEADS TYPE VI
- PEDESTRIAN SIGNAL HEAD
- PEDESTRIAN PUSH BUTTON
- MAST ARM MOUNTED SIGNS
- RAISED PAVEMENT MARKER
- UTA PTZ CAMERA

**CONSTRUCTION NOTES:**

1. PROVIDE AND INSTALL SIGNAL HEADS WITH LENSES AS SHOWN.
2. ALL SIGNAL HEADS SHOWN SHALL BE TYPE I UNLESS OTHERWISE NOTED.
3. PROVIDE ADEQUATE CONDUCTOR SLACK IN MAST ARM TO ALLOW FOR VERTICAL ADJUSTMENTS OF SIGNAL WITHIN FULL RANGE OF MOUNTING BRACKET.
4. CONTRACTOR SHALL VERIFY AND ENSURE MAST ARM MINIMUM HEIGHT REQUIREMENTS ARE MET PRIOR TO CASTING POLE FOUNDATIONS.
5. TAG EACH SET OF CABLES AT EACH CONDUIT END LABELING EACH CIRCUIT.
6. USE SIGNAL HEAD BRACKET MOUNTS FOR MOUNTING ALL SIGNAL HEADS. BOTTOM OF SIGNAL HEAD ASSEMBLY SHALL BE A MINIMUM OF 17.5' AND A MAXIMUM OF 19.5' ABOVE ROADWAY SURFACE.
7. PRIMARY CONDUITS BETWEEN SIGNAL JUNCTION BOXES SHALL CONTAIN 6-2" PVC CONDUIT EXCEPT WHERE NOTED.
8. POLES TO BE CONSTRUCTED PER SALT LAKE CITY TRAFFIC OPERATION STANDARDS.
9. PTZ CAMERA REQUIRES NEMA BOX WITH A SWITCH AND SHALL WIRED FROM THE NEAREST PCC FIBER BOX.
10. INSTALL PEDESTRIAN BLANK OUT SIGN. SEE SHEET 3213
11. ALL CITY ITEMS TO BE REMOVED ARE TO BE RETURNED TO THE CITY "AS IS".



REV	DATE	Description

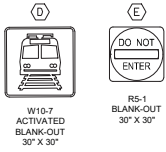


Designed By:	J. GONZALES
Drawn By:	J. GONZALES
Checked By:	B. KHAN
Approved By:	F. NANNENGA

**S-LINE EXTENSION PROJECT**

**TRAFFIC SIGNAL PLANS**  
WB STA 206+42 TO WB STA 211+50

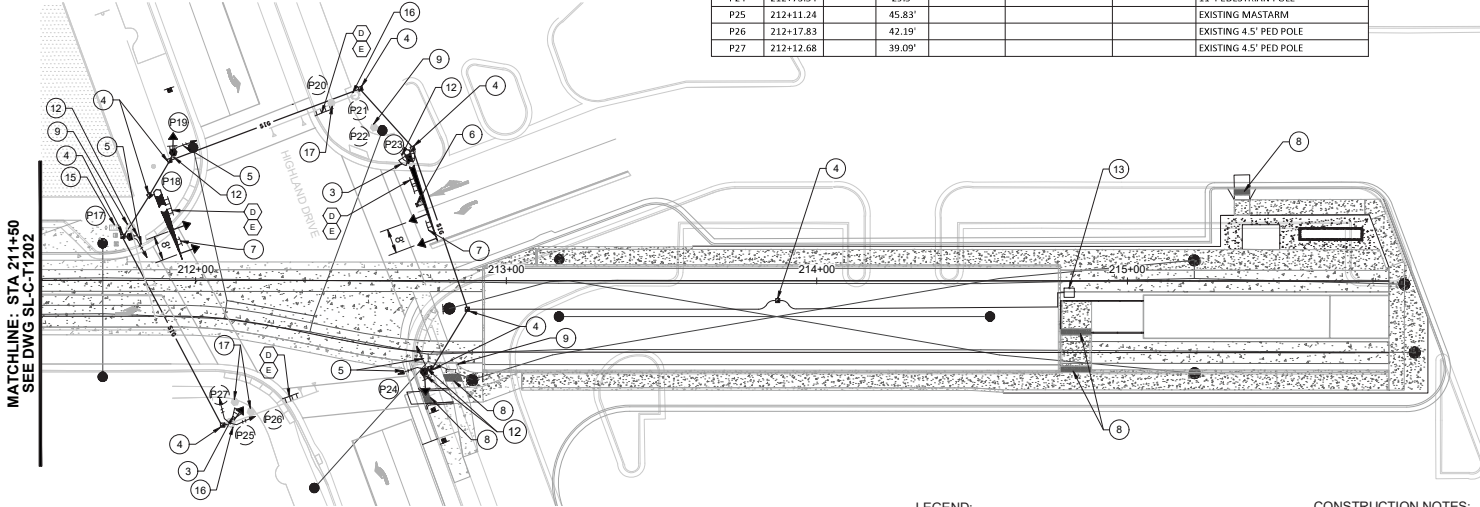
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CADD Filename:	SL-C-T-1-202.dwg
Submital Date:	1-16-26
UTA Contract No.:	24-03849
Sheet No.:	T1202



POLE IDENT.	STATION	OFFSET		MAST ARM LENGTH	LUMINARE MOUNTING HEIGHT	LUMINARE ARM LENGTH	NOTES
		LEFT	RIGHT				
P17	211+79.01	14.14'					11' PEDESTRIAN POLE
P18	211+87.36	28.02'		20'			
P19	211+92.70	41.15'					11' PEDESTRIAN POLE
P20	212+43.69	57.26'					EXISTING 4.5' PED POLE
P21	212+51.38	59.7'					EXISTING MAST ARM
P22	212+43.69	57.26'					EXISTING 4.5' PED POLE
P23	212+57.9	41.16'		30'	30'	10'	
P24	212+73.54		29.5'				11' PEDESTRIAN POLE
P25	212+11.24		45.83'				EXISTING MASTARM
P26	212+17.83		42.19'				EXISTING 4.5' PED POLE
P27	212+12.68		39.09'				EXISTING 4.5' PED POLE



MAST ARM MOUNTED SIGN SCHEDULE		
MAST ARM IDENT.	CENTER OF SIGN TO END OF MAST ARM	A-2 SIGN W/G 8" "C" TEXT
P18	5'	HIGHLAND
P22	5'	HIGHLAND



MATCHLINE: STA 211+50 SEE DWG SL-C-T1-202

**NOTES:**

- 3 LIDAR DETECTION MOUNTED ON TRAFFIC SIGNAL POLE.
- 4 INSTALL TYPE II JUNCTION BOX PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK505.
- 5 INSTALL PEDESTRIAN SIGNAL HEAD ON POLE PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARDS SHEET EK507.
- 6 CCTV CAMERA MOUNTED ON TRAFFIC SIGNAL POLE OR MAST ARM AS SHOWN.
- 7 MOUNT STREET NAME SIGN ON MAST ARM.
- 8 INSTALL TACTILE WARNING DEVICE (TRUNCATED DOMES)
- 9 REMOVE, SALVAGE, AND RETURN ALL CITY OWNED POLES, SIGNS, PED PUSH BUTTONS AND SIGNAL RELATED PARTS TO SALT LAKE CITY TRAFFIC SIGNAL CREW. OBLITERATE EXISTING FOUNDATION.
- 12 INSTALL AUDIBLE PUSH BUTTON ON POLE PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARDS SHEET EK507.
- 13 UDOT/SLC FIBER PULL BOX TYPE 2
- 15 EXISTING SIGNAL CABINET AND CONTROLLER (PROTECT IN PLACE)
- 16 EXISTING SIGNAL POLE, MAST ARM, EQUIPMENT, AND STREET NAME SIGN (PROTECT IN PLACE)
- 17 EXISTING PEDESTRIAN POLE AND PUSH BUTTON (PROTECT IN PLACE).

**LEGEND:**

- TRAFFIC SIGNAL CONDUIT
- TRAFFIC SIGNAL POLE AND MAST ARM WITH LUMINARE
- UTA PLATFORM LUMINARE
- PEDESTRIAN SIGNAL POLE
- OCS POLE
- TYPE II JUNCTION BOX
- TYPE III JUNCTION BOX
- TYPE 6 CABINET
- UNDERGROUND SERVICE PEDESTAL
- CCTV CAMERA
- LIDAR DETECTION
- SIGNAL HEADS TYPE I
- SIGNAL HEADS TYPE VI
- PEDESTRIAN SIGNAL HEAD
- PEDESTRIAN PUSH BUTTON
- EXISTING TRAFFIC SIGNAL MAST ARM
- ROADSIDE SIGN AND POST
- BLANK OUT SIGNS (R3-1, R3-2, R5-1, W10-7)

**CONSTRUCTION NOTES:**

1. PROVIDE AND INSTALL SIGNAL HEADS WITH LENSES AS SHOWN.
2. ALL SIGNAL HEADS SHOWN SHALL BE TYPE I UNLESS OTHERWISE NOTED.
3. PROVIDE ADEQUATE CONDUCTOR SLACK IN MAST ARM TO ALLOW FOR VERTICAL ADJUSTMENTS OF SIGNAL WITHIN FULL RANGE OF MOUNTING BRACKET.
4. CONTRACTOR SHALL VERIFY AND ENSURE MAST ARM MINIMUM HEIGHT REQUIREMENTS ARE MET PRIOR TO CASTING POLE FOUNDATIONS.
5. TAG EACH SET OF CABLES AT EACH CONDUIT END LABELING EACH CIRCUIT.
6. USE SIGNAL HEAD BRACKET MOUNTS FOR MOUNTING ALL SIGNAL HEADS. BOTTOM OF SIGNAL HEAD ASSEMBLY SHALL BE A MINIMUM OF 17.5' AND A MAXIMUM OF 19.5' ABOVE ROADWAY SURFACE.
7. PRIMARY CONDUITS BETWEEN SIGNAL JUNCTION BOXES SHALL CONTAIN 6-2" PVC CONDUIT EXCEPT WHERE NOTED.
8. POLES TO BE CONSTRUCTED PER SALT LAKE CITY TRAFFIC OPERATION STANDARDS.

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REV	DATE	Description



Designed By	J. GONZALES
Drawn By	J. GONZALES
Checked By	B. KHAN
Approved By	F. NANNENGA

**S-LINE EXTENSION PROJECT**

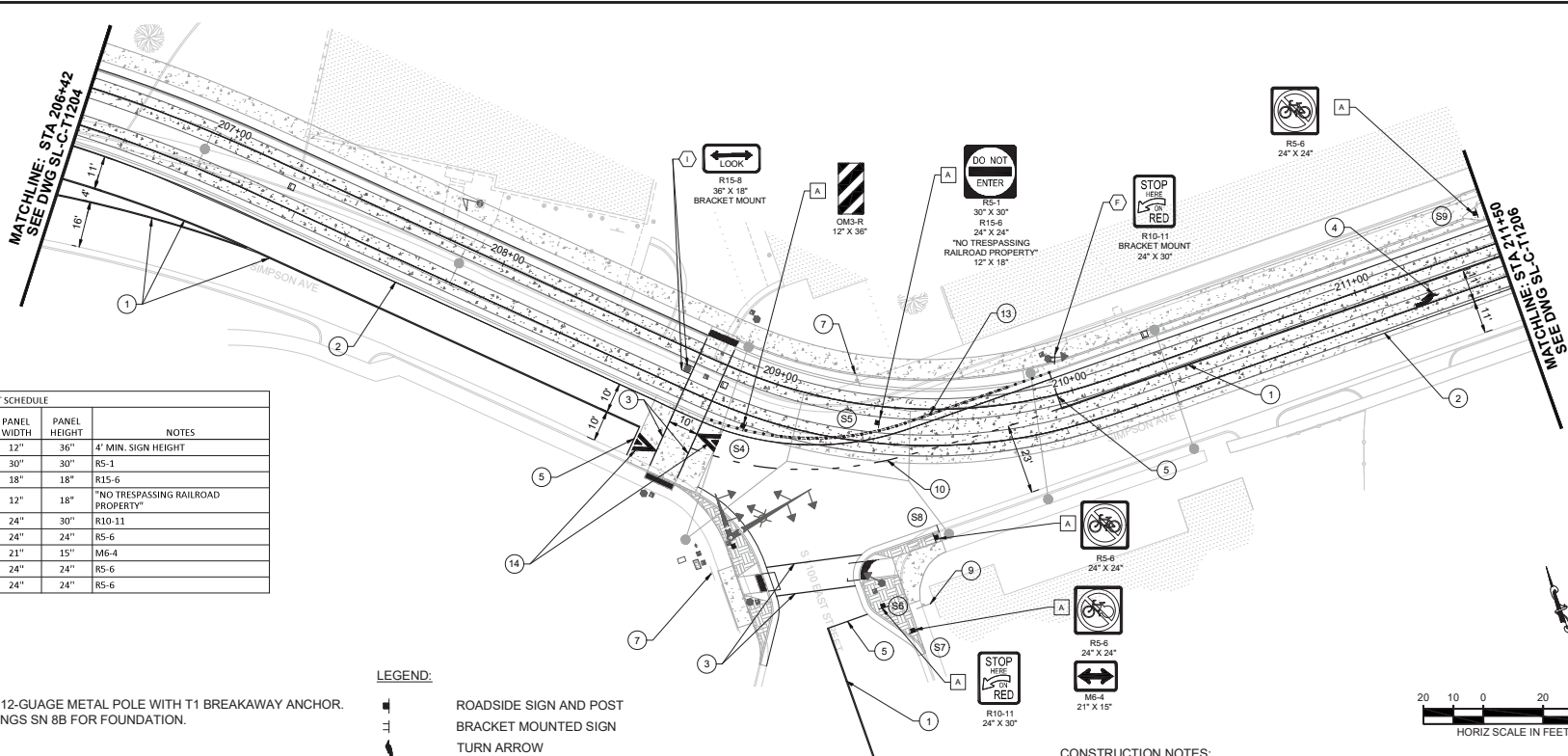
TRAFFIC SIGNAL PLANS  
WB STA 211+50 TO WB STA 215+85

Scale	1" = 20'
CADD Filename	SL-C-T1-203.dwg
Submitted Date	1-16-26
UTA Contract No.	24-03849
Drawing No.	T1203
Sheet No.	



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POST IDENT.	STATION	OFFSET		POST HEIGHT	PANEL		NOTES
		LEFT	RIGHT		WIDTH	HEIGHT	
S4	208+94.07		16.87'	7'	12"	36"	4' MIN. SIGN HEIGHT
S5	209+35.48		53.56'	10"	30"	30"	RS-1
					18"	18"	R15-6
					12"	18"	"NO TRESPASSING RAILROAD PROPERTY"
S6	209+41.02		66.14'	10"	24"	30"	R10-11
					24"	24"	RS-6
S7	209+48.49		73.91'	11'	21"	15"	M6-4
S8	209+53.42		43'	9'	24"	24"	RS-6
S9	211+45.9	11.82'		9'	24"	24"	RS-6

**NOTES:**

- [A] MOUNT SIGNS TO 2 INCH SQUARE 12-GUAGE METAL POLE WITH T1 BREAKAWAY ANCHOR. SEE UDOT 2025 STANDARD DRAWINGS SN 8B FOR FOUNDATION.
- [1] 4" DOUBLE YELLOW SOLID STRIPE
- [2] 8" SOLID WHITE STRIPE
- [3] 12" SOLID WHITE CROSSWALK STRIPE TO BE INSTALLED PER 2009 MUTCD 3B-16.
- [4] INSTALL TURN ARROW MARKING PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK514
- [5] 24" SOLID WHITE STRIPE TO BE INSTALLED PER 2009 MUTCD 3B-16. STRIPE TO BE INSTALLED A MINIMUM OF 4' IN ADVANCE OF NEAREST CROSSWALK.
- [7] EXISTING SIGN (PROTECT IN PLACE).
- [8] RELOCATE EXISTING SIGN AS SHOWN
- [9] REMOVE, SALVAGE, AND RETURN ALL CITY OWNED SIGNS AND POSTS TO SALT LAKE CITY TRAFFIC SIGNAL CREW. OBLITERATE EXISTING FOUNDATION.
- [10] 4" YELLOW DOTTED STRIPE.
- [13] INSTALL RAISED PAVEMENT MARKERS WITH 3' SPACING.
- [14] INSTALL CHEVRONS ON APPROACH SIDE OF RAISED CROSSWALKS.

**LEGEND:**

- [Symbol] ROADSIDE SIGN AND POST
- [Symbol] BRACKET MOUNTED SIGN
- [Symbol] TURN ARROW
- [Symbol] THRU ARROW
- [Symbol] THRU-TURN ARROW
- [Symbol] CHEVRON MARKING FOR RAISED CROSSWALK
- [Symbol] RAISED PAVEMENT MARKER

**CONSTRUCTION NOTES:**

1. MINIMUM SIGN HEIGHT SHALL BE SEVEN FEET MEASURED FROM BOTTOM OF SIGN TO STREET PAVEMENT, EXCEPT WHERE NOTED.
2. ALL NEW TRAFFIC PAVEMENT MARKINGS SHALL BE REFLECTIVE TAPE OR REFLECTIVE ACRYLIC PAINT AS SPECIFIED BY UDOT STANDARDS.
3. REMOVE ANY CONFLICTING PAVEMENT MARKINGS.
4. STRIPING TO CONTINUE TO CONSTRUCTION EXTENTS (MATCH EXISTING).
5. ALL CITY ITEMS TO BE REMOVED ARE TO BE RETURNED TO THE CITY "AS IS".



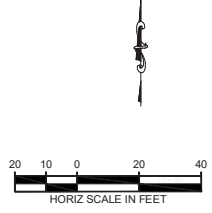
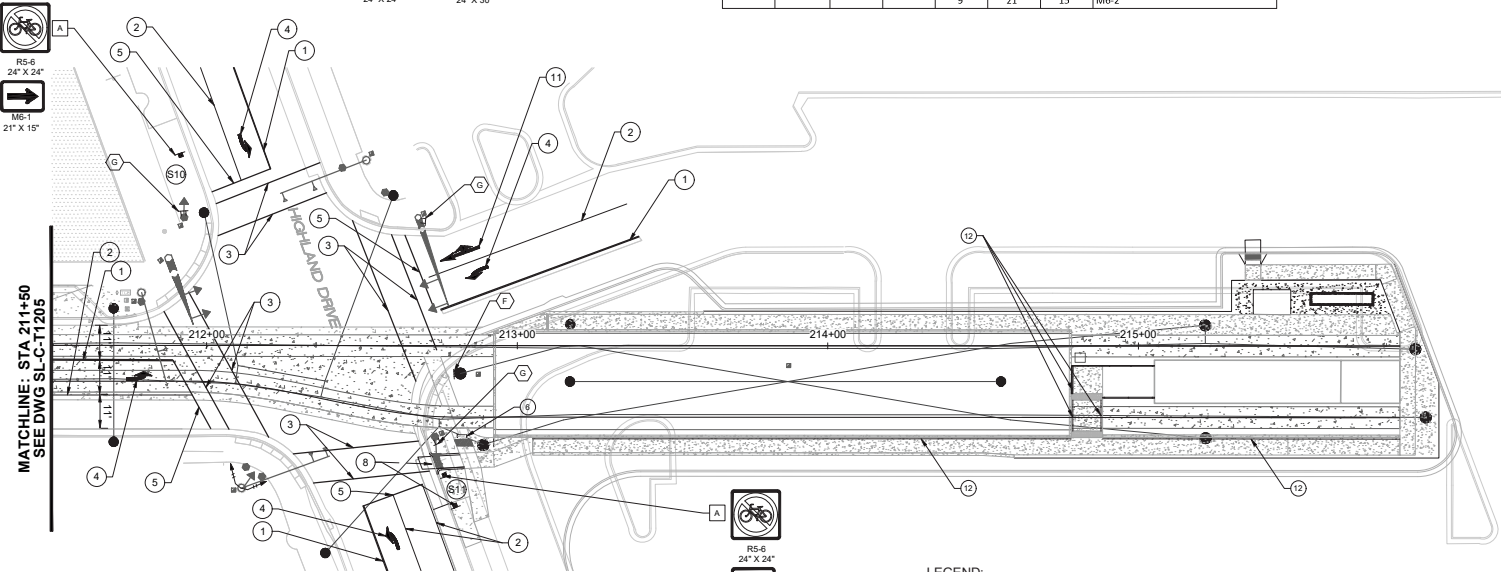
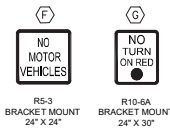



Designed By: J. GONZALES  
 Drawn By: J. GONZALES  
 Checked By: B. KHAN  
 Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
 STRIPING & SIGNING PLANS  
 WB STA 206+42 TO WB STA 211+50

Scale:	1" = 20'
CADD Filename:	SL-C-T-1-205.dwg
Submital Date:	1-16-26
UTA Contract No.:	24-03849
Drawing No.:	T1205
Sheet No.:	

POST IDENT.	STATION	OFFSET		POST HEIGHT	PANEL WIDTH	PANEL HEIGHT	NOTES
		LEFT	RIGHT				
S10	211+91.53	6.21'		9'	24"	24"	R5-6
					9'	21"	15"
S11	212+76.39		41.05'	9'	24"	24"	R5-6
					9'	21"	15"



MATCHLINE: STA 211+50 SEE DWG SL-C-T1-205

**NOTES:**

- A MOUNT SIGN TO 2 INCH SQUARE 12-GUAGE METAL POST PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK511.
- 1 4" DOUBLE YELLOW SOLID STRIPE
- 2 8" SOLID WHITE STRIPE
- 3 12" SOLID WHITE CROSSWALK STRIPE TO BE INSTALLED PER 2009 MUTCD 3B-16.
- 4 INSTALL TURN ARROW MARKING PER SALT LAKE CITY TRAFFIC OPERATIONS STANDARD PLANS SHEET EK514
- 5 24" SOLID WHITE STRIPE TO BE INSTALLED PER 2009 MUTCD 3B-16. STRIPE TO BE INSTALLED A MINIMUM OF 4' IN ADVANCE OF NEAREST CROSSWALK.
- 6 "STOP" PAVEMENT MESSAGE TO BE AFFIXED DIRECTLY BEHIND TACTILE WARNING DEVICE (WHITE LETTERING ON RED BACKGROUND).
- 8 RELOCATE EXISTING SIGN AS SHOWN
- 11 INSTALL THRU-RIGHT ARROW MARKING PER UTAH MUTCD.
- 12 4" SOLID WHITE LINE

**LEGEND:**

- ROADSIDE SIGN AND POST
- BRACKET MOUNTED SIGN
- TURN ARROW
- THRU ARROW
- THRU-TURN ARROW
- CHEVRON, ARKING FOR RAISED CROSSWALK
- RAISED PAVEMENT MARKER

**CONSTRUCTION NOTES:**

1. MINIMUM SIGN HEIGHT SHALL BE SEVEN FEET MEASURED FROM BOTTOM OF SIGN TO STREET PAVEMENT, EXCEPT WHERE NOTED.
2. ALL NEW TRAFFIC PAVEMENT MARKINGS SHALL BE REFLECTIVE TAPE OR REFLECTIVE ACRYLIC PAINT AS SPECIFIED BY UDOT STANDARDS.
3. REMOVE ANY CONFLICTING PAVEMENT MARKINGS.
4. STRIPING TO CONTINUE TO CONSTRUCTION EXTENTS (MATCH EXISTING).
5. ALL CITY ITEMS TO BE REMOVED ARE TO BE RETURNED TO THE CITY "AS IS".

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REV	DATE	Description



Designed By	J. GONZALES
Drawn By	J. GONZALES
Checked By	B. KHAN
Approved By	F. NANNENGA

**S-LINE EXTENSION PROJECT**

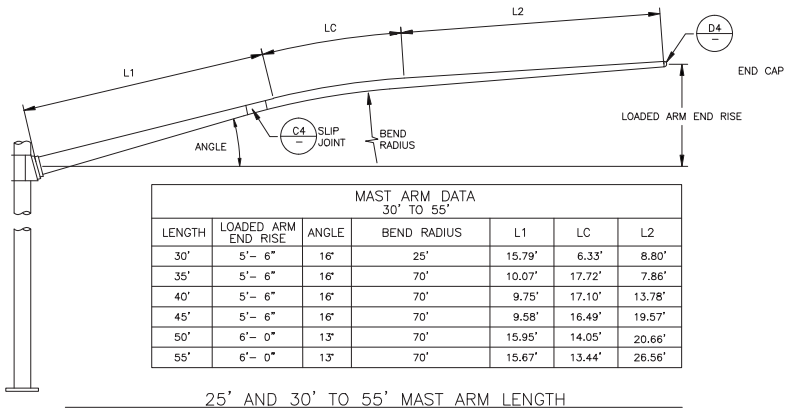
**STRIPING & SIGNING PLANS**  
WB STA 211+50 TO WB STA 215+85

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UTA Contract No.	24-03849
Drawing No.	T1206
Sheet No.	

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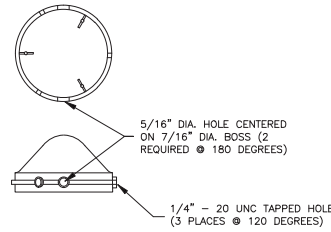
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11/15/2018

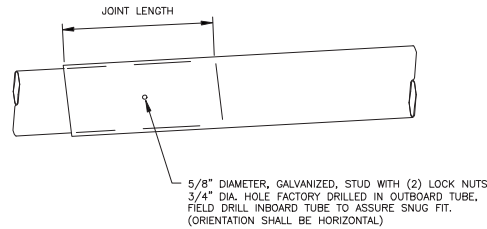


MAST ARM DATA 30' TO 55'						
LENGTH	LOADED ARM END RISE	ANGLE	BEND RADIUS	L1	LC	L2
30'	5'- 6"	16°	25'	15.79'	6.33'	8.80'
35'	5'- 6"	16°	70'	10.07'	17.72'	7.86'
40'	5'- 6"	16°	70'	9.75'	17.10'	13.78'
45'	5'- 6"	16°	70'	9.58'	16.49'	19.57'
50'	6'- 0"	13°	70'	15.95'	14.05'	20.66'
55'	6'- 0"	13°	70'	15.67'	13.44'	26.56'

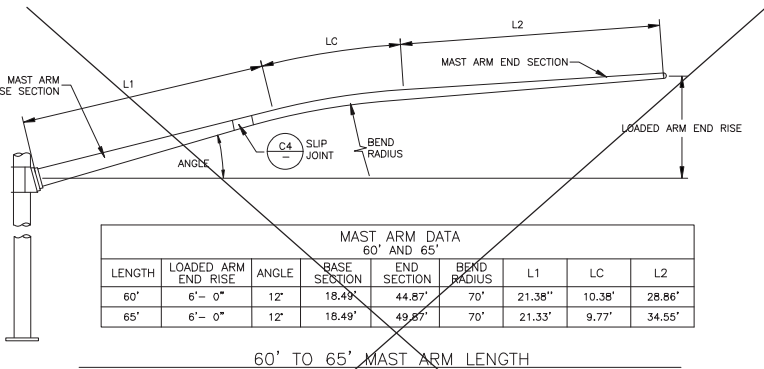
25' AND 30' TO 55' MAST ARM LENGTH



D4 ARM END CAP  
SCALE: NTS

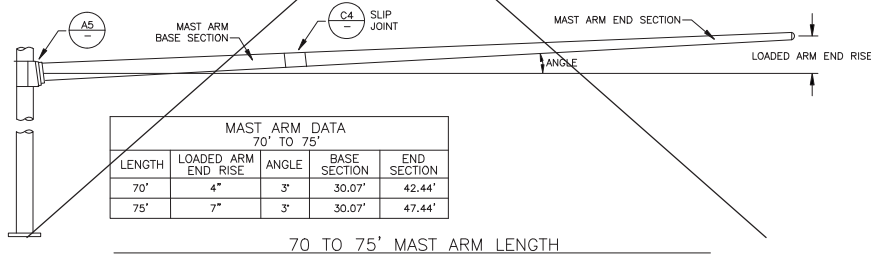


C4 SLIP JOINT  
SCALE: NTS



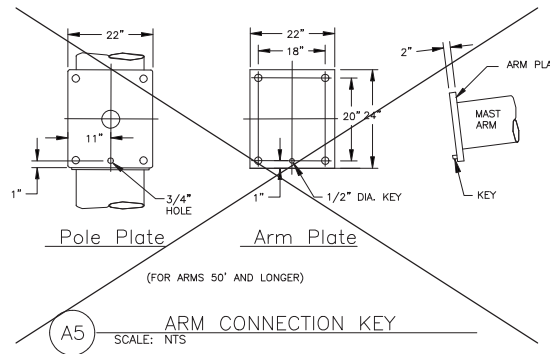
MAST ARM DATA 60' AND 65'						
LENGTH	LOADED ARM END RISE	ANGLE	BASE SECTION	END SECTION	BEND RADIUS	L1
60'	6'- 0"	12°	18.49'	44.87'	70'	21.38'
65'	6'- 0"	12°	18.49'	49.87'	70'	21.33'

60' TO 65' MAST ARM LENGTH



MAST ARM DATA 70' TO 75'				
LENGTH	LOADED ARM END RISE	ANGLE	BASE SECTION	END SECTION
70'	4"	3°	30.07'	42.44'
75'	7"	3°	30.07'	47.44'

70 TO 75' MAST ARM LENGTH



A5 ARM CONNECTION KEY  
SCALE: NTS

**GENERAL SHEET NOTES**

**STEEL**

- GRADE C STEEL (ASTM A 570).
- ALL STEEL MATERIALS ARE HOT-DIPPED GALVANIZED (ASTM A 123).

**POLE**

- INSTALL POLE PLUMB. DO NOT RAKE POLE TO INCREASE SIGNAL HEAD CLEARANCE.

**PAINT**

- POWDER COAT GLOSS BLACK.
- DO NOT DAMAGE PAINT FINISH DURING TRANSPORT OR PLACEMENT.

**BOLTS**

- TORQUE BOLTS TO 40 FT-LBS., RELEASE, THEN TORQUE TO 80 FT-LBS.

**SHEET KEYNOTES**

1 SLIP JOINT

- FIELD ASSEMBLE TWO-PIECE MAST ARM SLIP JOINT TO ACHIEVE A SNUG FIT.
- APPLY ANTI-SIEZE COMPOUND.
- PROVIDE MINIMUM OVERLAP NOT LESS THAN 1.5 TIMES INSIDE DIAMETER OF END SECTION.

DETAILS PROVIDED ON THIS SHEET ARE APPLICABLE STANDARD DETAILS DESIGNED AND DRAWN BY SALT LAKE CITY TRANSPORTATION DIVISION.

REV	DATE	Description



Designed By:  
J. GONZALES

Drawn By:  
J. GONZALES

Checked By:  
B. KHAN

Approved By:  
F. NANNENGA

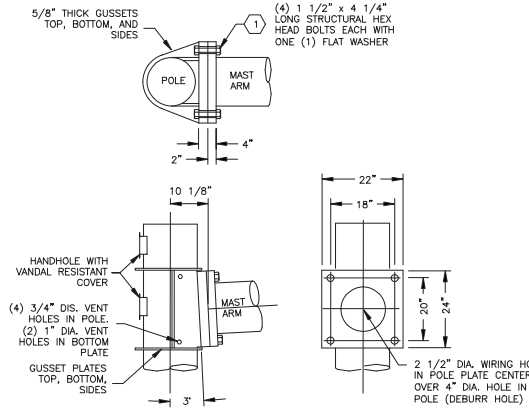
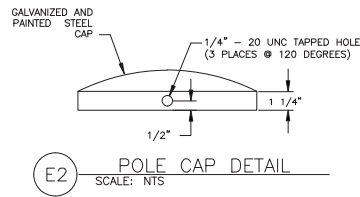
S-LINE EXTENSION PROJECT

STANDARD DRAWINGS

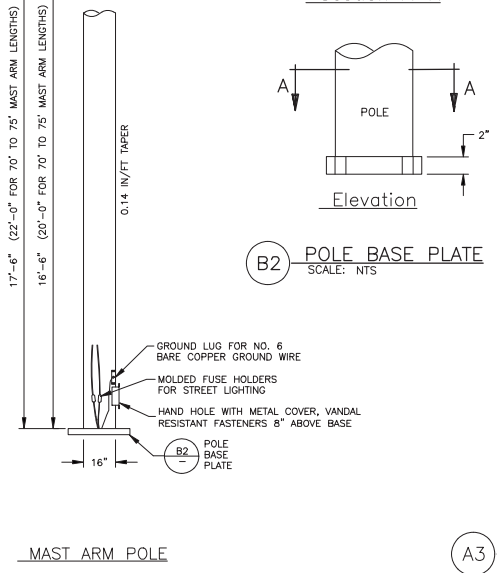
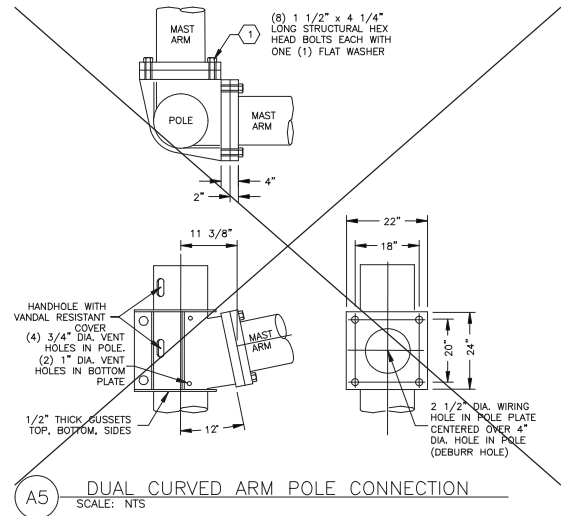
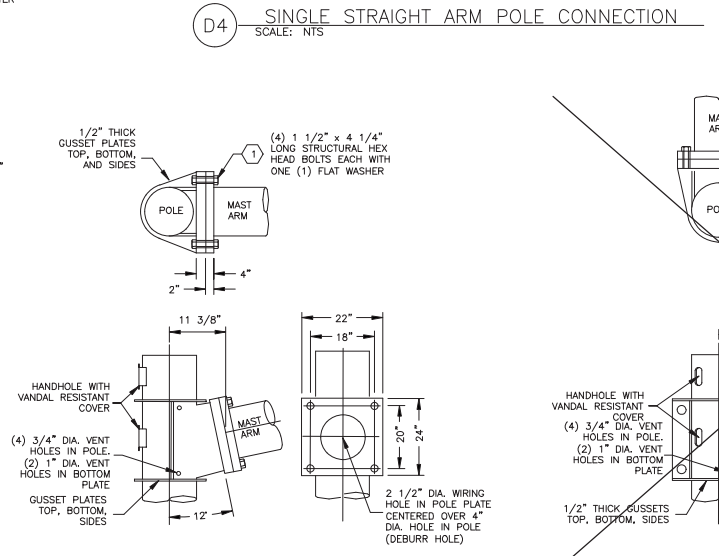
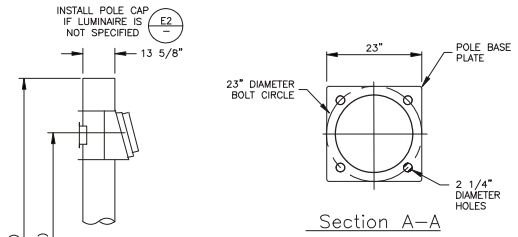
MAST ARM DETAIL

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Submit Date	1-16-26
UTA Contract No.	24-03849
Drawing No.	T3201
Sheet No.	

GENERAL SHEET NOTES	
<b>STEEL</b>	
◦ GRADE C STEEL (ASTM A 570).	
◦ ALL STEEL MATERIALS ARE HOT-DIPPED GALVANIZED (ASTM A 123).	
<b>POLE</b>	
◦ INSTALL POLE PLUMB. DO NOT RAKE POLE TO INCREASE SIGNAL HEAD CLEARANCE.	
<b>PAINT</b>	
◦ POWDER COAT GLOSS BLACK.	
◦ DO NOT DAMAGE PAINTED FINISH DURING TRANSPORT OR PLACEMENT.	
SHEET KEYNOTES	
<b>1 BOLTS</b>	
◦ TORQUE BOLTS TO 40 FT-LBS., RELEASE, THEN TORQUE TO 80 FT-LBS.	



DETAILS PROVIDED ON THIS SHEET ARE APPLICABLE STANDARD DETAILS DESIGNED AND DRAWN BY SALT LAKE CITY TRANSPORTATION DIVISION.



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REV	DATE	Description

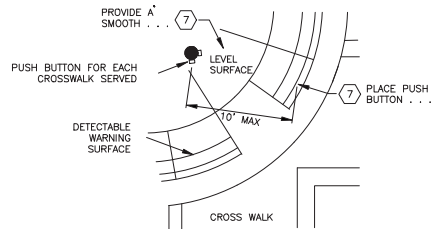


Designed By:	J. GONZALES
Drawn By:	J. GONZALES
Checked By:	B. KHAN
Approved By:	F. NANNENGA

S-LINE EXTENSION PROJECT		Scale: 1" = 40'
STANDARD DRAWINGS		CADD Filename: SL-C-T-3-202.dwg
MAST ARM POLE DETAIL		Submit Date: 1-16-26
		UTA Contract No.: 24-03849
Sheet No.: T3202		

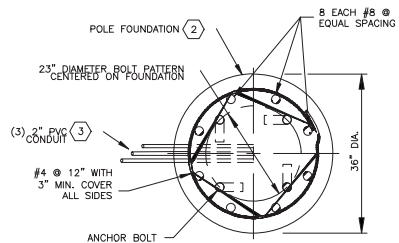
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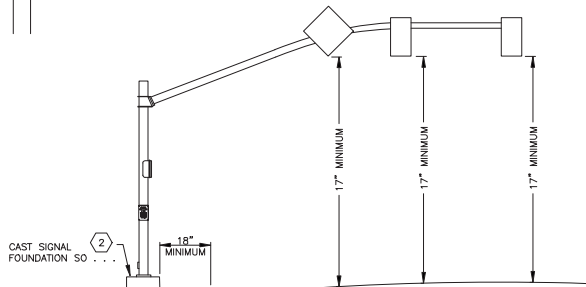


Plan View

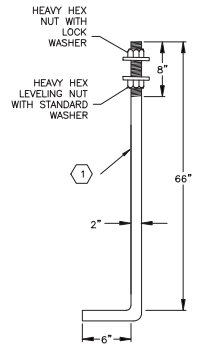
DETAILS PROVIDED ON THIS SHEET ARE APPLICABLE STANDARD DETAILS DESIGNED AND DRAWN BY SALT LAKE CITY TRANSPORTATION DIVISION.



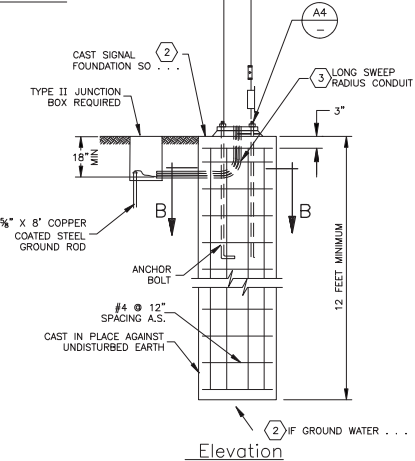
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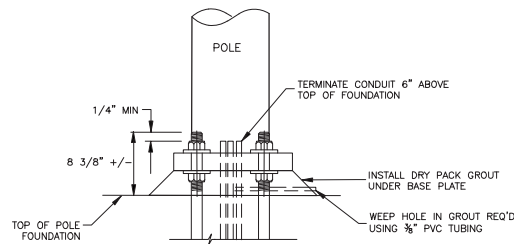
C3 FOUNDATION ELEVATION  
SCALE: NTS



Anchor Bolt Detail



Elevation



A4 POLE BASE CONNECTION  
SCALE: NTS

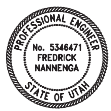
GENERAL SHEET NOTES

- STEEL
  - o GRADE C STEEL (ASTM A 570).
  - o ALL STEEL MATERIALS ARE HOT-DIPPED GALVANIZED (ASTM A 123).
- PAINT
  - o POWDER COAT GLOSS BLACK.
  - o DO NOT DAMAGE PAINTED FINISH DURING TRANSPORT OR PLACEMENT.
- VEGETATION
  - o APPROVAL FROM ENGINEER AND CITY FORESTER IS REQUIRED BEFORE TRIMMING OR CUTTING ANY TREE OR SHRUB.

SHEET KEYNOTES

- 1 ANCHOR BOLT
  - o STEEL (ASTM A 307).
  - o GALVANIZED (ASTM A 123).
  - o DO NOT WELD ANCHOR BOLTS TO REINFORCING STEEL.
  - o INSTALL BOLTS SQUARE WITH ROADWAY OR AT ANGLE SPECIFIED ON PLAN SHEETS.
  - o INSTALL POLE PLUMB. DO NOT RAKE POLE TO INCREASE SIGNAL HEAD CLEARANCE.
  - o TIGHTEN NUTS TO 150 FT-LBS.
- 2 FOUNDATION
  - o EPOXY COATED OR GALVANIZED DEFORMED STEEL BAR CAGE (ASTM A 36). DO NOT WELD REINFORCING STEEL.
  - o CONCRETE CLASS 4000.
  - o CAST SIGNAL FOUNDATION SO TOP OF FOUNDATION IS AT THE ELEVATION OF THE CROWN OF THE ROAD OR FLUSH WITH SIDEWALK, WHICHEVER IS HIGHER.
  - o CAST ALL FOUNDATIONS IN PLACE AGAINST UNDISTURBED EARTH WITH 12 FEET OF ITS LENGTH BELOW FINISH GRADE.
  - o USE A CIRCULAR FORM FOR THE TOP 18" OF FOUNDATION EMBEDMENT.
  - o IF GROUND WATER IS ENCOUNTERED DURING FOUNDATION EXCAVATION, PROVIDE FULL LENGTH CIRCULAR FORM FOR FOUNDATION, EXCAVATE ADDITIONAL 2' DEEP BELOW FOUNDATION, INSTALL 2" SEWER ROCK BELOW FOUNDATION, DEWATER PRIOR TO PLACING CONCRETE, AND FILL ANNULAR SPACE BETWEEN CIRCULAR FORM AND EXCAVATION SIDES WITH CLSM (CONTROLLED LOW STRENGTH MATERIAL) (FLOWABLE FILL).
- 3 CONDUIT
  - o PLACE ALL CONDUIT IN SAME TRENCH WHERE POSSIBLE.
  - o CAP ON BOTH ENDS PRIOR TO CONCRETE POUR.
- 4 PUSH BUTTON
  - o PROVIDE A SMOOTH, LEVEL, HARD SURFACE WITHIN 12 HORIZONTAL INCHES OF A PUSH BUTTON FOR WHEEL CHAIR ACCESS.
  - o PLACE PUSH BUTTON WITHIN TEN FEET OF THE CENTER OF THE CURB CUT OR THE CENTER OF THE CROSS WALK IF THE CURB CUT EXCEEDS CROSS WALK WIDTH.
  - o IF A PUSH BUTTON CANNOT BE MOUNTED ON THE POLE AND STILL COMPLY WITH THE 10 FEET MAXIMUM DISTANCE REQUIREMENT, A PEDESTRIAN PUSH BUTTON POLE MUST BE PROVIDED.

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REV	DATE	Description	



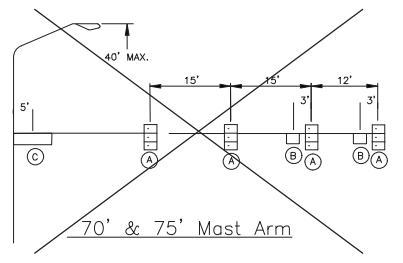
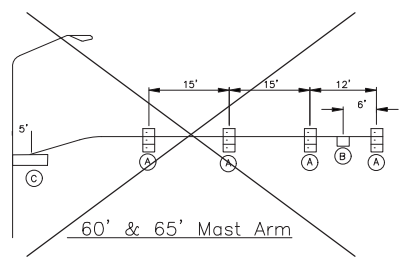
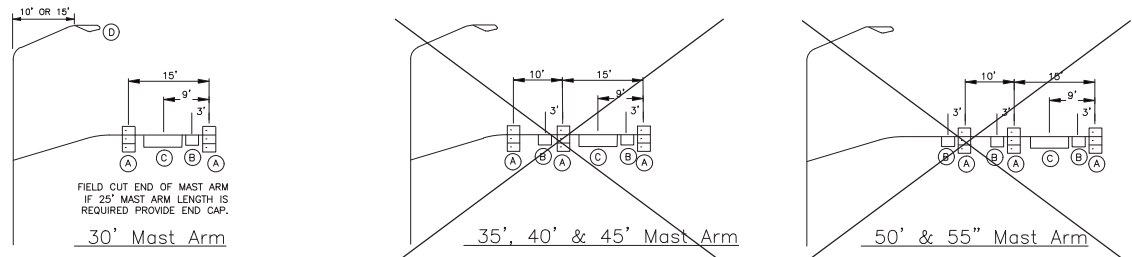
Designed By:	J. GONZALES
Drawn By:	J. GONZALES
Checked By:	B. KHAN
Approved By:	F. NANNENGA

S-LINE EXTENSION PROJECT  
STANDARD DRAWINGS  
MAST ARM POLE FOUNDATION DETAILS

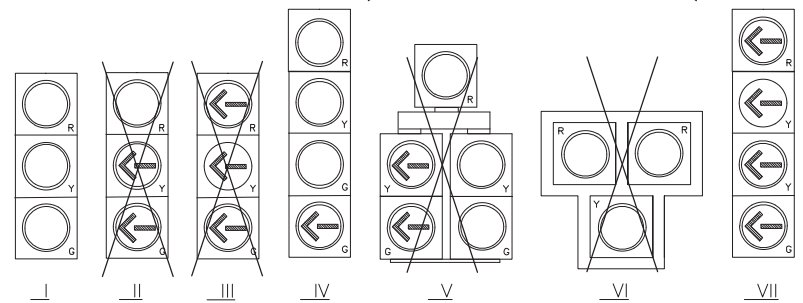
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Drawing No.:	T3203
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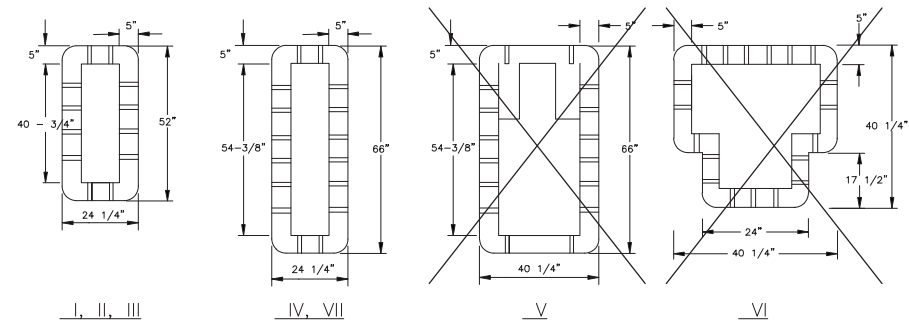
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DEVICE	DESCRIPTION/SIZE	PROJECTED AREA (SF)	WEIGHT (LBS)
(A) SIGNAL	12", 3-SECTION WITH BACK PLATE	8.7	55
(B) SIGN	REGULATORY 24" X 30"	5.0	20
(C) SIGN	STREET NAME 16' X 96" (MAX)	10.7	43
(D) LUMINAIRE	ROADWAY LUMINAIRE	3.3	65



C4 SCALE: NTS SIGNAL HEAD TYPES



A4 SCALE: NTS LOUVERED BACK PLATE TYPES  
NOTE: FINISH IS FLAT BLACK

**GENERAL SHEET NOTES**

**GENERAL**

- MAINTAIN AT LEAST 10' CLEARANCE FROM ALL SIGNAL HARDWARE TO OVERHEAD POWER LINES.
- CONTRACTOR MUST RECEIVE APPROVAL FROM ENGINEER AND CITY FORESTER BEFORE TRIMMING TREES OR SHRUBS.

**PAINT**

- VISORS: GLOSS BLACK 12" x 12" TUNNEL TYPE.
- HEADS: GLOSS BLACK.
- BACK PLATE: FLAT BLACK.
- DO NOT DAMAGE PAINTED FINISH DURING TRANSPORT OR PLACEMENT.

**SIGNAL LIGHT HEAD**

- ONE-PIECE DIE-CAST ALUMINUM HOUSING.
- ALUMINUM DOOR WITH STAINLESS STEEL HINGE PINS. DOOR SWINGS 180 DEGREES.
- ONE-PIECE ALUMINUM VISOR SECURED TO THE DOOR WITH STAINLESS STEEL HARDWARE AND REMOVABLE.
- SIZED FOR 1 1/2" MOUNTING FIXTURES AT EACH END AND ATTACHING BACK-PLATES.

**BACK PLATE**

- PROVIDE AND INSTALL FOR ALL TYPE I, II, III, AND TYPE IV SIGNAL HEADS.

**TERMINAL BLOCK**

- LOCATED IN TOP SECTION FOR TYPE I, II, AND III SIGNAL HEADS.

**LED MODULES**

- PROVIDE LED MODULES IN ALL HEADS.
- ORIENT LED LENSES FOR TOP UPWARD ALIGNMENT.

**INSTALLATION**

- INSTALL TYPE V ASSEMBLIES SO HINGED SIGNAL FACE SWINGS OPEN TO THE OUTSIDE.
- PROVIDE 17"-0" MINIMUM CLEARANCE FROM BOTTOM OF SIGNAL HEAD ASSEMBLY TO FINISHED ROADWAY SURFACE. ASSEMBLY INCLUDES SIGNAL HEAD, BACK PLATE AND SIGNAL HEAD MOUNTING HARDWARE.
- LOCATE CENTER OF END SIGNAL HEAD 1 FOOT FROM END OF MAST ARM. FOR NON-TYPICAL INSTALLATIONS PROVIDE NOT LESS THAN 8'-0" SPACING.

DETAILS PROVIDED ON THIS SHEET ARE APPLICABLE STANDARD DETAILS DESIGNED AND DRAWN BY SALT LAKE CITY TRANSPORTATION DIVISION.

**GENERAL NOTES (THIS SHEET ONLY):**

- DIMENSIONS BETWEEN SIGNAL HEADS AS SHOWN ON PLAN SHEETS SHALL BE USED.

REV	DATE	Description



Designed By:  
J. GONZALES

Drawn By:  
J. GONZALES

Checked By:  
B. KHAN

Approved By:  
F. NANNENGA

**S-LINE EXTENSION PROJECT**

**STANDARD DRAWINGS**

**SIGNAL HEAD FOR VEHICLES DETAIL**

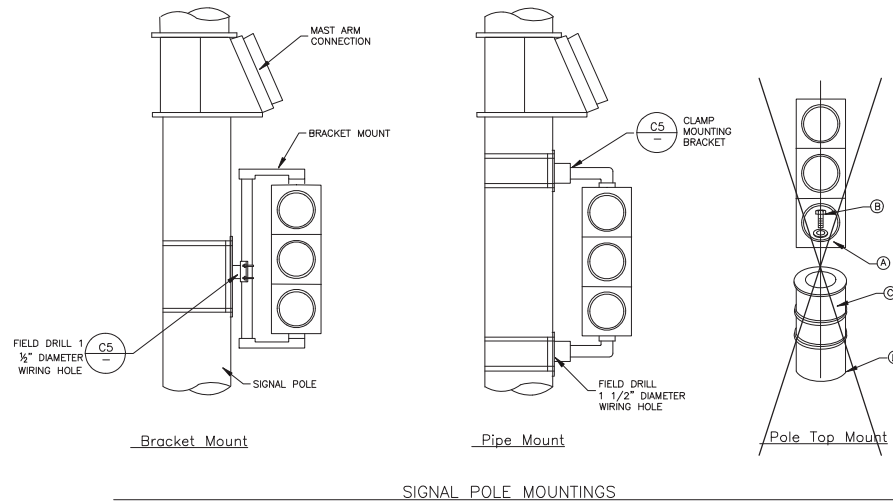
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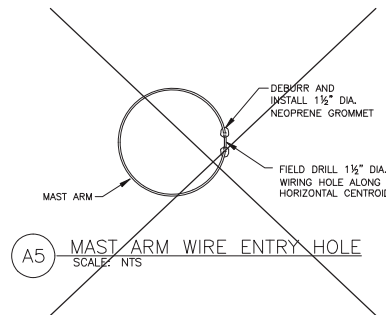
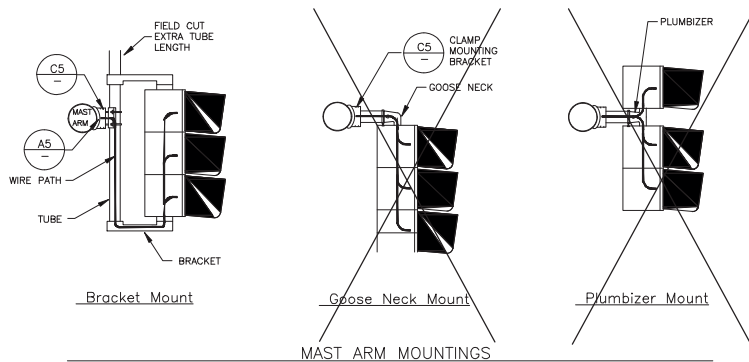
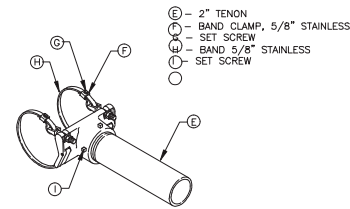
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- GENERAL SHEET NOTES**
- POLE TOP MOUNT**
  - BRONZE FITTINGS, GALVANIZED STEEL PIPE INSTALLATION
  - FIELD DRILL 1-1/2" DIAMETER WIRING HOLE ALONG THE HORIZONTAL CENTROID OF MAST ARM AT EACH SIGNAL HEAD LOCATION. DEBURR AND INSTALL NEOPRENE GROMMET FOR WIRE PROTECTION PRIOR TO INSTALLING SIGNAL HEAD BRACKET
  - PROVIDE PIPE MOUNT WHEN ATTACHING SIGNAL HEADS TO SIDES OF POLES
  - PAINT
  - GLOSS BLACK POWDER COAT.
  - DO NOT DAMAGES PAINTED FINISH DURING TRANSPORT OR PLACEMENT
  - GOOSE NECK HANGER
  - 2 1/8" I.D. THREADED FOR TENON.
  - LOCK BOLT, 3/8" X 4" STAINLESS STEEL OR ZINC PLATED WITH STAINLESS STEEL DOUBLE HEX NUTS, WASHERS AND LOCK WASHERS.

DETAILS PROVIDED ON THIS SHEET ARE APPLICABLE STANDARD DETAILS DESIGNED AND DRAWN BY SALT LAKE CITY TRANSPORTATION DIVISION.



- A - 1-1/2" NEOPRENE WASHER
- B - 1-1/2" LOCK NIPPLE
- C - 4-1/2" INSIDE DIAMETER POLE TOP MOUNTED TERMINAL COLLAR
- D - TRAFFIC SIGNAL POLE



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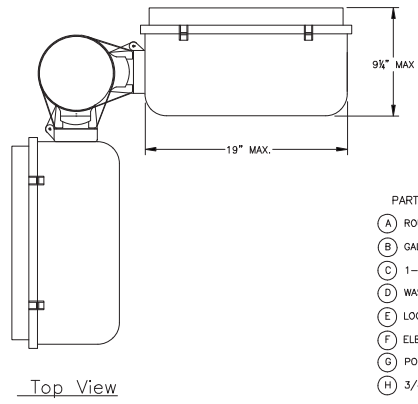


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Approved By: F. NANNENGA

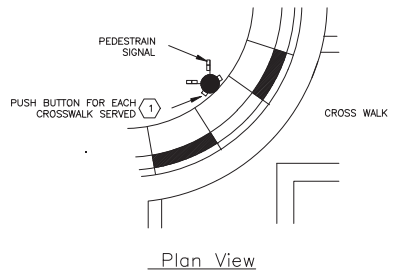
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Drawing No.: T3206	Sheet No.:



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Top View



Plan View

- PARTS LIST
- (A) ROUND POLE PLATE
  - (B) GALVANIZED PIPE 1-1/2" x 12" THREADED BOTH ENDS
  - (C) 1-1/2" PLAIN ELBOW 90 DEGREES.
  - (D) WASHER, STAINLESS STEEL
  - (E) LOCK NIPPLE, 1-1/2 x 1-3/4"
  - (F) ELBOWS, 90 DEGREE 1-1/2" SERRATED
  - (G) POLE SHAFT
  - (H) 3/4" STAINLES STEEL BANDS

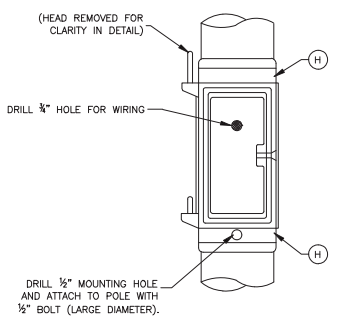
**GENERAL SHEET NOTES**

- PAINT
  - o POWDER COAT GLOSS BLACK.
  - o DO NOT DAMAGE PAINTED FINISH DURING TRANSPORT OR PLACEMENT.
- SIGNAL HEAD
  - o WITH LED COUNTDOWN TIMER AND AUDIBLE.

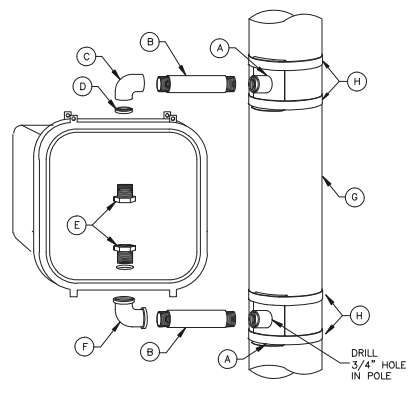
**SHEET KEYNOTES**

1 PEDESTRIAN PUSH BUTTON  
 o MOUNT PUSH BUTTONS 42" ABOVE AND WITHIN 12 INCHES OF A SMOOTH WHEEL CHAIR ACCESSIBLE FLAT SURFACE.

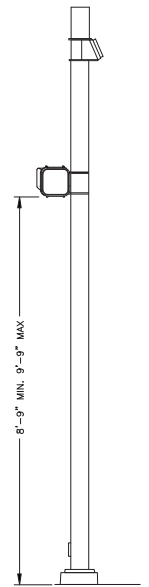
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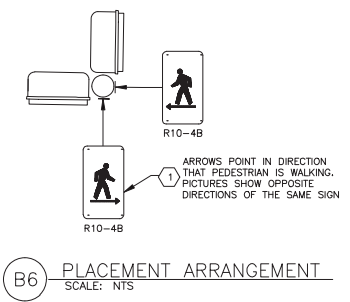
Clamshell Mount



Pipe Mount



A2 POLE MOUNTING  
SCALE: NTS



B6 PLACEMENT ARRANGEMENT  
SCALE: NTS

A5 SIGNAL HEAD MOUNTING DETAIL  
SCALE: NTS

REV	DATE	Description

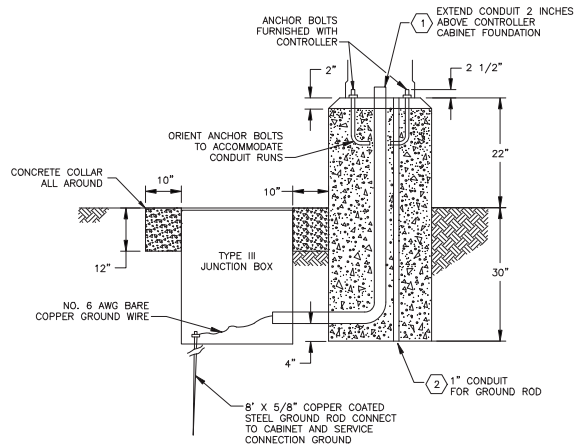


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Drawn By: J. GONZALES
Checked By: B. KHAN
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT	
STANDARD DRAWINGS	
SIGNAL HEAD FOR PEDESTRIANS DETAIL	

Scale: 1" = 40'
CADD Filename: SL-C-T-3-208.dwg
Submission Date: 1-16-26
UTA Contract No.: 24-03849
Drawing No.: T3208
Sheet No.:





**GENERAL SHEET NOTES**

**ORIENTATION**

- WHEN THE CABINET DOOR IS OPENED ALL THE WAY, THE ROTATION DOES NOT BLOCK THE SIDEWALK. THERE IS A FLAT AREA FOR A SIGNAL TECHNICIAN TO WORK ON THE CABINET. THE TECHNICIAN IS STANDING IN THE RIGHT-OF-WAY AND THE TECHNICIAN IS NOT STANDING IN SOMEONE'S FLOWER BED.

**FINISHING**

- FILL THE SPACE BETWEEN FOUNDATION AND SIDEWALK WITH 4" THICK CONCRETE FLATWORK.

**TRENCHING**

- PLACE ALL CONDUITS IN THE SAME TRENCH WHERE POSSIBLE.

**WIRE**

- ATTACH THE GROUNDED SIDE OF THE POWER SUPPLY TO THE CONTROL CABINET GROUND TERMINAL.
- IDENTIFY AND LABEL ALL FIELD TERMINALS.

**KEYNOTES**

**1 CONDUIT**

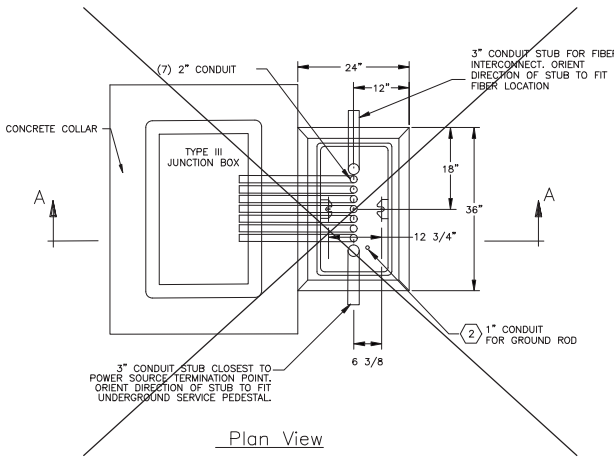
- MAINTAIN 1" MINIMUM SPACING BETWEEN CONDUITS IN CABINET BASE.
- CAP OR PLUG CONDUITS AT BOTH ENDS UNTIL USED.
- SEAL ALL CONDUITS INSIDE JUNCTION BOX AND CABINET AFTER WIRING IS COMPLETE.

**2 GROUNDING**

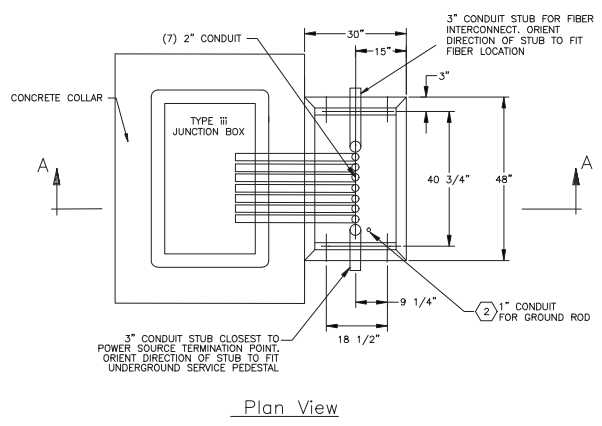
- GROUND CABINET WITH A COPPER COATED STEEL GROUND ROD PLACED IN A 1" PVC CONDUIT FOUNDATION SLEEVE.

DETAILS PROVIDED ON THIS SHEET ARE APPLICABLE STANDARD DETAILS DESIGNED AND DRAWN BY SALT LAKE CITY TRANSPORTATION DIVISION.

Section A-A



A2 CABINET BASE TYPE 5 (24 x 36)  
SCALE: NTS



A5 CABINET BASE TYPE 6 (30 X 48)  
SCALE: NTS

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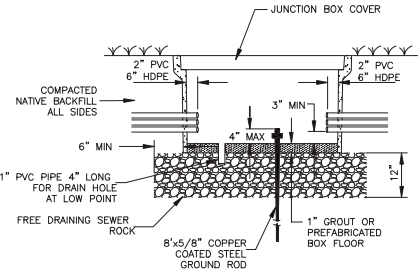


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Checked By: B. KHAN
Approved By: F. NANNENGA

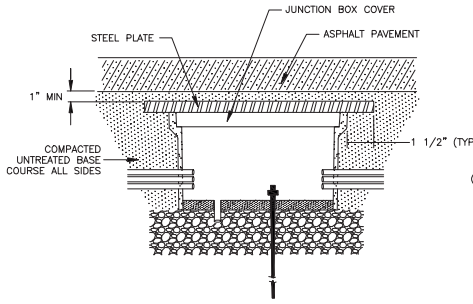
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STANDARD DRAWINGS  
SIGNAL CONTROLLER FOUNDATION DETAIL

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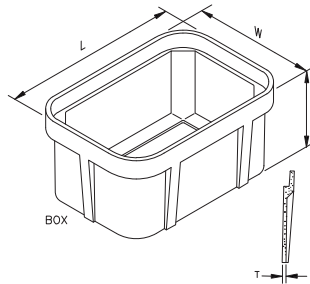
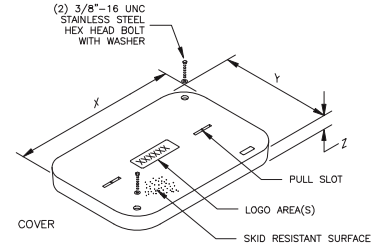
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D1 LANDSCAPE BOX DETAIL  
SCALE: NTS

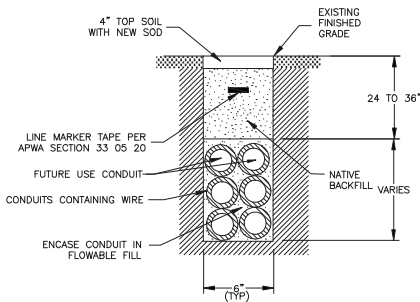


D3 ROADWAY BOX DETAIL  
SCALE: NTS

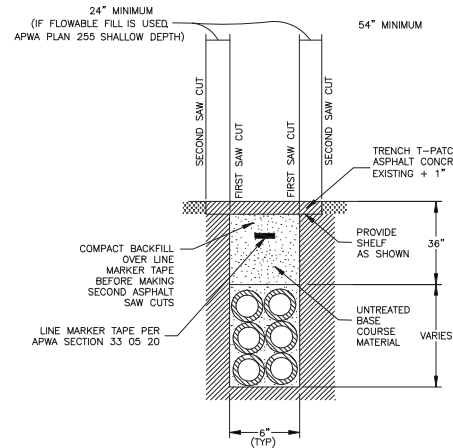


JUNCTION BOX TYPE AND DIMENSIONS							
TYPE	L	W	H	T	X	Y	Z
I	20 1/4	13 3/8	12	1/2	18 1/8	11 1/4	1 3/4
II	32 1/4	19 1/4	18	1 1/4	30 1/2	17 1/2	2
III	37 5/8	26	24	2	35 5/8	24	3
D	49 5/8	32 1/8	24	2	47 5/8	30 1/8	3

A4 JUNCTION BOX AND LID DIMENSIONS  
SCALE: NTS



A1 LANDSCAPE TRENCH DETAIL  
SCALE: NTS



A3 ROADWAY TRENCH DETAIL  
SCALE: NTS

**GENERAL SHEET NOTES**

**GENERAL**  
 •SET ALL JUNCTION BOXES TO MATCH GRADE OF SURROUNDING TERRAIN.  
 •OTHER JUNCTION BOXES SUBJECT TO ENGINEER'S ACCEPTANCE.

**JUNCTION BOX**  
 •MADE FROM POLYMER CONCRETE CONSISTING OF SAND AND AGGREGATE BOUND TOGETHER WITH A POLYMER RESIN COMPLYING WITH THE FOLLOWING PROPERTIES:

PROPERTY	ASTM	POLYMER
TENSILE STRENGTH	C 496	1,700 psi
FLEXURAL STRENGTH	D 790	7,500 psi
IMPACT RESISTANCE	D 2444	72 FOOT-POUNDS
COMPRESSIVE STRENGTH	C 109	11,000 psi
EFFECTS OF ACIDS	D 543	VERY RESISTANT
EFFECT OF ALKALIZES	D 543	VERY RESISTANT
SKID RESISTANCE	COEFFICIENT	0.50

•SUPPORT WHEEL LOAD OF 12000 LBS AND A MAXIMUM DEFLECTION OF 1/2" WITH 8000 LBS. LOAD USING A 10" x 10" LOAD.  
 •LOGO AREA  
 "SIGNAL" FOR SIGNALS  
 "STREET LIGHTING" FOR STREET LIGHTS  
 "SIGNAL" FOR INTERCONNECTS  
 •CAST CONDUIT HOLES IN JUNCTION BOX AT THE TIME OF CASTING OR DRILL OR PUNCH AT THE TIME OF PLACEMENT WITH NO STRUCTURAL DAMAGE TO BOX.  
 •ALL DRILLED HOLES SHALL BE THE SAME DIAMETER AS OUTSIDE DIAMETER OF CONDUIT.  
**CONDUITS**  
 •INSTALL CONDUITS THROUGH SIDE OF JUNCTION BOX. DO NOT INSTALL CONDUITS THROUGH BOTTOM. SEAL ALL CONDUIT HOLES USING A RUBBER SLEEVE GASKET AND EXPANDING FOAM.  
 •ALL CONDUITS TO CONTAIN 700 POUND FLAT BRAIDED NYLON PULL WIRE IN ADDITION TO CIRCUIT WIRES.  
 •PROVIDE SEALING COMPOUND IN ENDS OF EACH CONDUIT RUN.  
**BACKFILL**  
 •95 PERCENT OR GREATER DENSITY.  
**ACCESSORIES**  
 •STAINLESS STEEL BOLTS, NUTS, WASHERS, ETC.  
 •SEWAGE  
 •USE EXOTHERMIC WELD CONNECTIONS.

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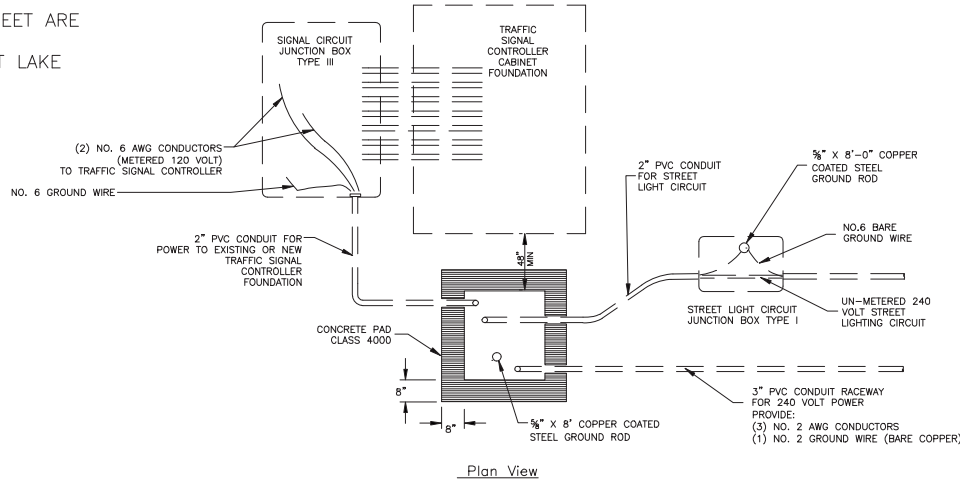


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Drawn By:	J. GONZALES
Checked By:	B. KHAN
Approved By:	F. NANNENGA

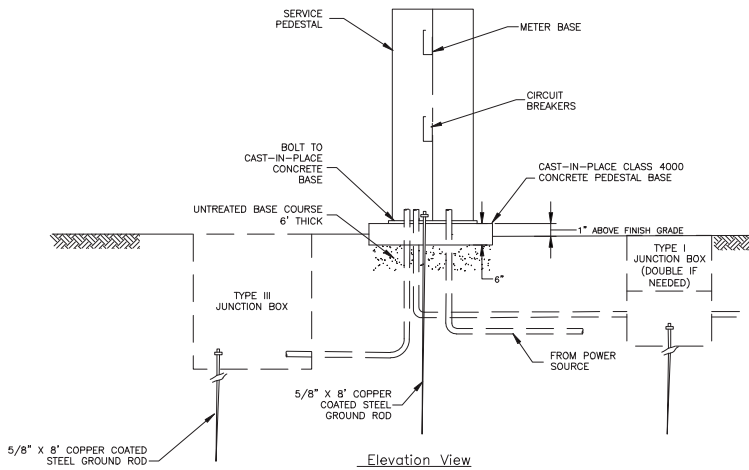
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 STANDARD DRAWINGS  
 JUNCTION BOX AND BURIED RACEWAY DETAIL

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Submission Date:	1-16-26
UTA Contract No.:	24-03849
Drawing No.:	T3211
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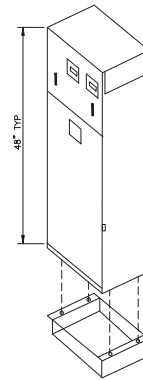
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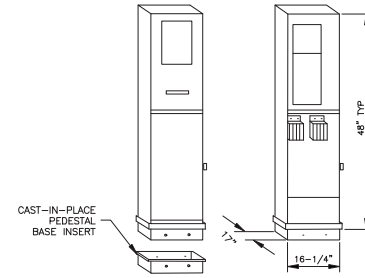
Plan View



Elevation View



UNDERGROUND SERVICE PEDESTAL WITH UNINTERRUPTIBLE POWER SOURCE (WHERE SPECIFIED)



Oblique Views

**GENERAL SHEET NOTES**

**UNDERGROUND SERVICE PEDESTAL**  
 ORATED 100 AMP AT 120/240 VOLTS.  
 PROVIDE FOR 6 FULL SIZE POLES (INCLUDING MAINS) FOR BOTH METERED AND UN-METERED CIRCUIT BREAKER INTERIORS.  
 USE PLUG-IN CIRCUIT BREAKERS.  
 LABEL ALL BREAKERS.  
 USE DETACHABLE BASE FOR PRE-INSTALLATION IN CONCRETE FOUNDATION.  
 SECURE PIANO HINGED DOORS WITH MASTER PADLOCK NO. 1B005.

**LOCATION**  
 LOCATE SERVICE PEDESTAL ON EITHER SIDE OF CONTROLLER FOUNDATION.  
 LOCATION MAY BE MODIFIED TO BEST SUIT FIELD CONDITIONS AND ENGINEER APPROVAL.  
 MAINTAIN 48 INCHES MINIMUM CLEARANCE AROUND EACH UNIT WITH DOORS OPENED TO ANY POSITION.  
 LOCATE PEDESTAL SO WATER DRAINS AWAY FROM FOUNDATIONS AND JUNCTION BOXES.  
 SITE PREPARATION INCLUDING GRADING, MAY BE REQUIRED BEFORE PLACING EQUIPMENT.

**METER SOCKET**  
 INCLUDE METER SOCKET WITH TEST BLOCK (SAFETY SOCKET) REQUIRED FOR CIRCUIT BREAKERS.

**METERS AND CIRCUIT BREAKER**

A. LIGHTING - UNMETERED  
 50 AMP 2 POLE MAIN CIRCUIT BREAKER  
 (1) 20 AMP 2 POLE CIRCUIT BREAKER

B. TRAFFIC SIGNAL - METERED  
 70 AMP 2 POLE MAIN CIRCUIT BREAKER  
 (1) 40 AMP 1 POLE CIRCUIT BREAKER

UNDERGROUND SERVICE PEDESTAL FOR STREET LIGHTING AND TRAFFIC SIGNAL CURCUIT

1/1/2020			
REV	DATE	Description	



Designed By: J. GONZALES  
 Drawn By: J. GONZALES  
 Checked By: B. KHAN  
 Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
 STANDARD DRAWINGS  
 UNDERGROUND SERVICE PEDESTAL DETAIL

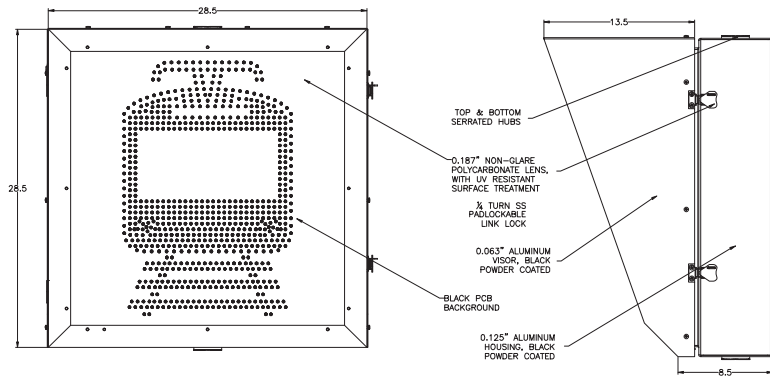
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1/15/2026

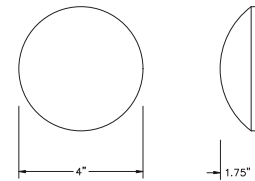


SIGN FEATURES

- \* FILLED MESSAGE
- \* MADE OF 720 HIGH INTENSITY AMBER LED'S
- \* HEADLIGHTS MADE OF 26 HIGH INTENSITY WHITE LED'S
- \* PROGRESSIVE DIMMING IN 1000 STEPS

PEDESTRIAN BLANK OUT SIGN

SCALE: NTS



RAISED PAVEMENT MARKER

SCALE: NTS

ALL DIMENSIONS IN INCHES

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Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

Designed By: J. GONZALES
Drawn By: J. GONZALES
Checked By: K. KHAN
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT	
STANDARD DRAWINGS	
PEDESTRIAN BLANK OUT SIGN AND RAISED PAVEMENT MARKER DETAILS	

Scale:	1" = 40'
CADD Filename:	SL-C-T-3-213.dwg
Submital Date:	1-16-26
UTA Contract No.:	24-03849
Drawing No.:	T3213
Sheet No.:	



**NOTES:**

PROVIDE AND INSTALL 3" PVC CONDUIT FROM POWER SOURCE TO UNDERGROUND SERVICE PEDESTAL (USP) WITH UNINTERRUPTABLE POWER SOURCE AS SHOWN. CABLE TO BE INSTALLED BY OTHERS.

COORDINATE POWER SERVICES FROM POWER POLES WITH RMP. SEE UTILITY SERVICES NOTES.

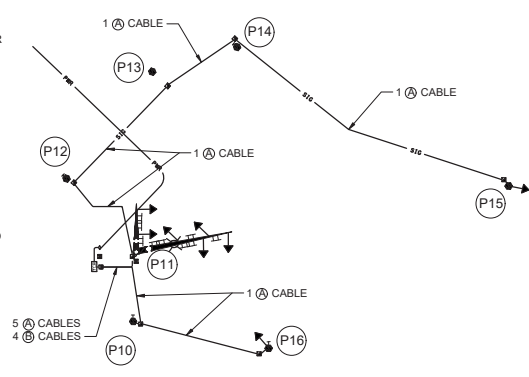
PROVIDE AND INSTALL 120 V POWER SUPPLY FROM USP TO CONTROLLER USING 2 SINGLE-CONDUCTOR NO.10 COPPER CABLES AND NO.6 BARE COPPER GROUND WIRE.

INSTALL TRAFFIC SIGNAL CIRCUIT, PEDESTRIAN SIGNAL CIRCUIT AND DETECTION CIRCUIT CABLE IN SHARED 3" PVC BETWEEN TCS AND TYPE 3 JUNCTION BOX.

INSTALL PEDESTRIAN PUSH BUTTON CIRCUIT IN SEPARATE 2" PVC BETWEEN TCS AND TYPE 3 JUNCTION BOX.

INSTALL LIGHTING CIRCUIT IN SEPARATE 2" PVC BETWEEN USP AND TYPE 1 JUNCTION BOX.

PROVIDE A SINGLE 3" CONDUIT FROM THE FIBER SPLICER JUNCTION BOX TO THE SIGNAL CABINET FOR DROP CABLE.

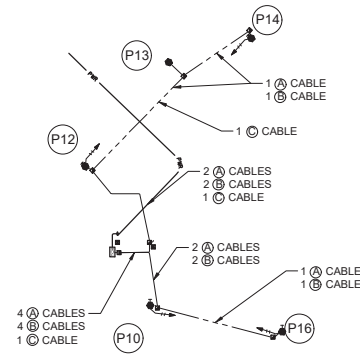


**SIGNAL CIRCUIT**

A 7-CONDUCTOR NO.14 AWG CABLE WITH NO.6 BARE COPPER WIRE IN CONDUIT AS SHOWN.

**BLANK OUT SIGN CIRCUIT**

B 2-CONDUCTOR NO. 12 AWG CABLE



**PEDESTRIAN SIGNAL CIRCUIT**

A 7-CONDUCTOR NO.14 AWG CABLE IN THE SAME CONDUIT AS SIGNAL CIRCUIT.

**PUSH BUTTON CIRCUIT**

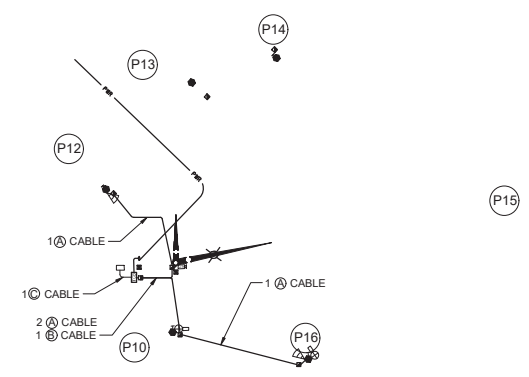
B 4-CONDUCTOR NO.14 AWG. WITH NO.6 BARE COPPER GROUND WIRE.

**PEDESTRIAN BLANK OUT SIGN**

C 2-CONDUCTOR NO. 12 AWG CABLE IN THE SAME CONDUIT AS SIGNAL CIRCUIT

**NOTES:**

PEDESTRIAN SIGNAL CONDUCTOR SHALL NOT SHARE CONDUIT WITH PUSH BUTTON CONDUCTOR.



**LIDAR DETECTION CIRCUIT**

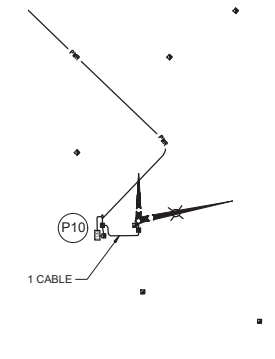
A CAT6 GAME CHANGER ETHERNET CABLE.

**CCTV CAMERA CIRCUIT**

B CAT5 ETHERNET PATCH CABLE.

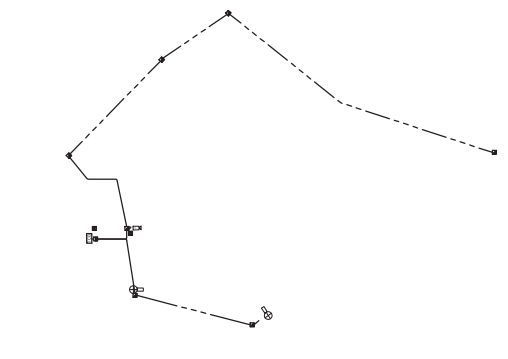
**FIBER CONNECTION**

C 6-STRAND PRE-TERMINATED DROP CABLE.



**LIGHTING CIRCUIT**

2 SINGLE CONDUCTOR NO.6 COPPER CABLE TYPE RHH/USE-RHW WITH BARE COPPER NO.6 GROUND WIRE IN 2" CONDUIT.



**FUTURE USE CONDUIT**

2-2" PVC EACH WITH ONE CONTINUOUS NO.14 AWG COPPER THHH PULL WIRE.

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Designed By:  
J. GONZALES  
Drawn By:  
J. GONZALES  
Checked By:  
B. KHAN  
Approved By:  
F. NANNENGA

S-LINE EXTENSION PROJECT  
  
SIGNAL CIRCUIT DIAGRAM  
1100 EAST STREET

Scale	1" = 20'
CADD Filename	SL-C-T-7-002.dwg
Submit Date	1-16-26
UTA Contract No.	24-03849
Drawing No.	T7002
Sheet No.	



**STRUCTURAL ABBREVIATIONS**

@	AT	SW	SIDEWALK
AB	ANCHOR BOLT	SYMM	SYMMETRICAL
ABUT	ABUTMENT	T&B	TOP & BOTTOM
AGG	AGGREGATE	T&C	TOP BACK OF CURB
APPROX	APPROXIMATE	TEMP	TEMPORARY
BLDG	BUILDING	T/R	TOP OF RAIL
BOT	BOTTOM	TOC	TOP OF CONCRETE
BRD	BRIDGE	TOS	TOP OF STEEL
BRG	BEARING	TYP	TYPICAL
CJ	CONTROL JOINT	UNO	UNLESS NOTED OTHERWISE
CL	CENTER LINE	VAR	VARIABLE
CLR	CLEAR	VERT	VERTICAL
COL	COLUMN	W/	WITH
CONC	CONCRETE	WB	WESTBOUND
CONN	CONNECTION	W/O	WITHOUT
CONST	CONSTRUCTED	WWF	WELDED WIRE FABRIC
CONST JT	CONSTRUCTION JOINT		
CONT	CONTINUOUS		
CWR	CONTINUOUSLY WELDED RAIL		
C&G	CURB & GUTTER		
DIA Ø	DIAMETER		
DET	DETAIL		
DR	DRAIN		
DRG	DRAWING		
EB	EAST BOUND		
EF	EACH FACE		
EJ	EXPANSION JOINT		
ELEV	ELEVATION		
EMB	EMBANKMENT		
EQ	EQUAL		
EW	EACH WAY		
EXIST	EXISTING		
EXP	EXPANSION		
EXT	EXTERIOR		
FG	FINISH GRADE		
FL	FLOOR		
FS	FAR SIDE		
FT	FEET		
FTG	FOOTING		
GALV	GALVANIZED		
GND	GROUND		
GRTG	GRATING		
HORIZ	HORIZONTAL		
IBC	INTERNATIONAL BUILDING CODE		
ID	INSIDE DIAMETER		
IN	INCH		
INT	INTERIOR		
INV	INVERT		
JT	JOINT		
LLH	LONG LEG HORIZONTAL		
LLV	LONG LEG VERTICAL		
LT	LEFT		
MAX	MAXIMUM		
MIN	MINIMUM		
MISC	MISCELLANEOUS		
N/A	NOT APPLICABLE		
NB	NORTH BOUND		
NIC	NOT IN CONTRACT		
NO #	NUMBER		
NS	NEAR SIDE		
NTS	NOT TO SCALE		
OC	ON CENTER		
OD	OUTSIDE DIAMETER		
OPNG	OPENING		
OPP	OPPOSITE		
PL	PLATE		
PLAT	PLATFORM		
PSF	POUNDS PER SQUARE FOOT		
PSI	POUNDS PER SQUARE INCH		
P/MT	PAVEMENT		
REIN	REINFORCEMENT		
REQ'D	REQUIRED		
RET	RETAINING		
RR	RAILROAD		
RT	RIGHT		
RW	RETAINING WALL		
SB	SOUTH BOUND		
SHT	SHEET		
SJ	SAWED JOINT		
SOG	SLAB ON GRADE		
STA	STATION		
STD	STANDARD		
STIFF	STIFFENER		
STL	STEEL		
STRUCT	STRUCTURE		

**GENERAL STRUCTURAL NOTES**

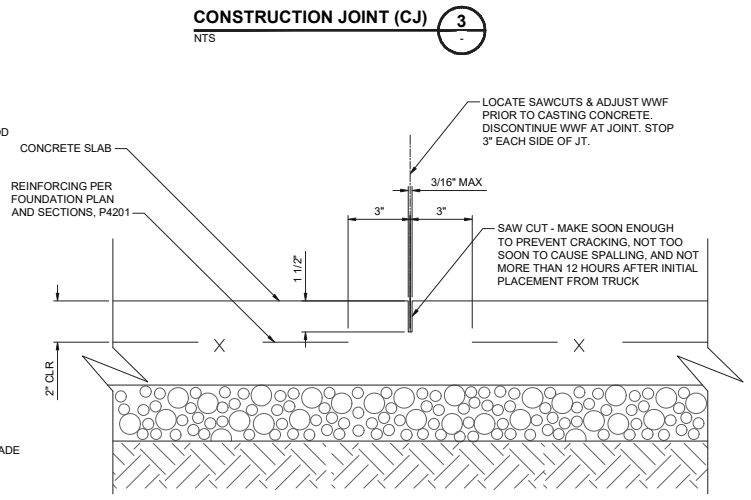
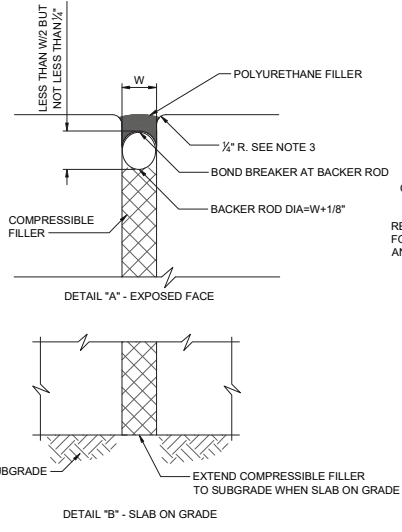
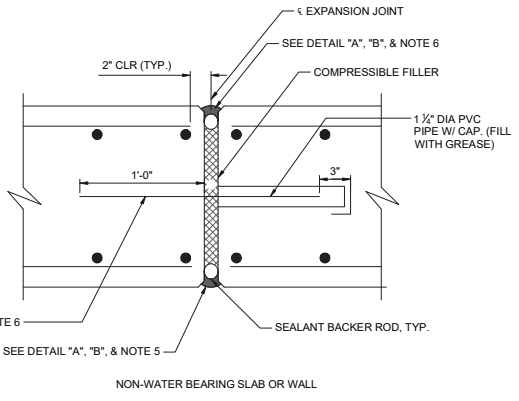
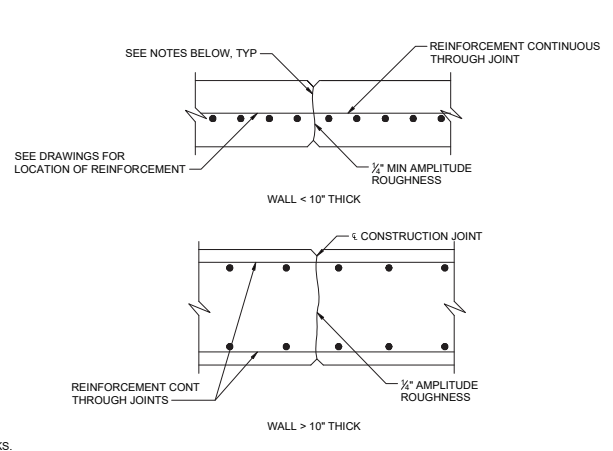
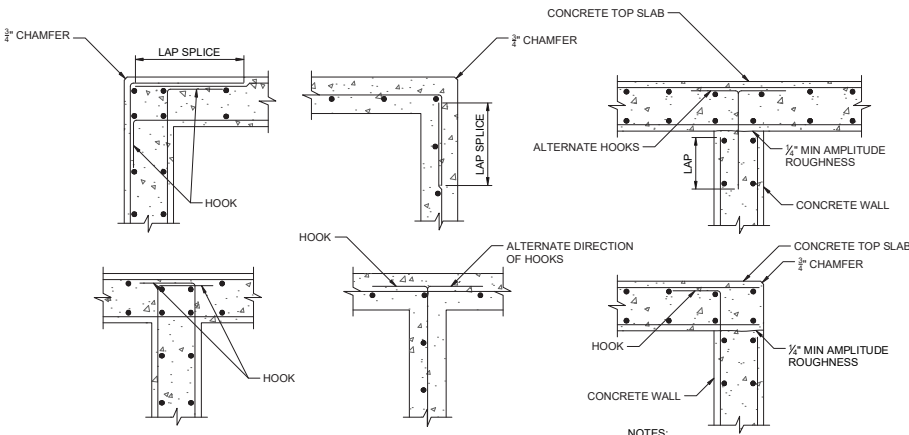
<p><b>GENERAL:</b></p> <ol style="list-style-type: none"> <li>ALL STRUCTURAL DRAWINGS SHALL BE READ IN CONJUNCTION WITH THE SPECIFICATIONS AND ALL OTHER DRAWINGS RELATING TO THE WORK.</li> <li>EMBEDDED ITEMS SUCH AS PIPES, INSERTS, SLEEVES AND CONDUITS, AND ANY RECESSES OR OPENINGS REQUIRED FOR UTILITY, ARCHITECTURAL, MECHANICAL, AND ELECTRICAL INSTALLATIONS ARE NOT SHOWN ON THE STRUCTURAL DRAWINGS. CONTRACTOR TO REFER TO THE UTILITY, ARCHITECTURAL, MECHANICAL, AND ELECTRICAL DRAWINGS FOR THE LOCATIONS AND DETAILS OF THESE ITEMS.</li> <li>THE VERTICAL CONTROL OF ALL TRACK STRUCTURES IS BASED ON ELEVATION OF TOP OF RAIL.</li> <li>BEFORE FABRICATION AND/OR CONSTRUCTION, ALL DIMENSIONS SHALL BE VERIFIED WITH ARCHITECTURAL DRAWINGS.</li> <li>NO PIPES OR SLEEVES FOR MECHANICAL OR PLUMBING TRADES SHALL PASS THROUGH STRUCTURAL MEMBERS, UNLESS SHOWN ON THE STRUCTURAL DRAWINGS, WITHOUT THE APPROVAL OF THE STRUCTURAL ENGINEER.</li> <li>SEWER AND UTILITY LINES ARE NOT INDICATED ON STRUCTURAL DRAWINGS. REFER TO CIVIL, PLUMBING, AND ELECTRICAL DRAWINGS FOR THEIR LOCATION, PROFILE, AND DETAILS. THE CONTRACTOR MUST COORDINATE SEWER AND UTILITY LINES WITH FOUNDATIONS SHOWN ON STRUCTURAL DRAWINGS. ANY INTERFERENCE BETWEEN SEWER AND UTILITY LINES AND THE STRUCTURE FOUNDATIONS SHALL BE BROUGHT TO THE ATTENTION OF THE STRUCTURAL ENGINEER BEFORE PROCEEDING WITH THE CONSTRUCTION.</li> </ol> <p><b>APPLICABLE CODES:</b></p> <ol style="list-style-type: none"> <li>AS A MINIMUM, THE INTERNATIONAL BUILDING CODE (IBC), CURRENT EDITION AT THE DATE OF ADVERTISEMENT FOR BIDS, WILL GOVERN THE DESIGN, MATERIALS, AND CONSTRUCTION ON THIS PROJECT. MODIFICATIONS, SUPPLEMENTS, AND OTHER ORDINANCES APPLICABLE WITHIN EACH JURISDICTION SHALL ALSO GOVERN WITHIN THE SPECIFIC JURISDICTION.</li> <li>IN THE CASE OF SPECIAL TYPES OF CONSTRUCTION, ADDITIONAL SPECIFICATIONS OR CODES MAY ALSO APPLY. THESE ADDITIONAL CODES ARE NOTED ON THE APPLICABLE DRAWINGS OR IN THE APPLICABLE SPECIFICATION SECTION.</li> <li>LOADS:             <ul style="list-style-type: none"> <li>JOINT</li> <li>ROOF LIVE: 17 PSF</li> <li>ROOF SNOW: 36 PSF ROOF SNOW LOAD, P1</li> <li>1.00 SNOW EXPOSURE FACTOR</li> <li>1.00 SNOW LOAD IMPORTANCE FACTOR</li> <li>1.20 THERMAL FACTOR, C1</li> </ul> </li> </ol> <p>WIND: 90 MPH BASIC WIND SPEED 1.00 WIND IMPORTANCE FACTOR, Iw EXPOSURE CATEGORY B</p> <p>EARTHQUAKE: 1.00 SEISMIC IMPORTANCE FACTOR, Ie SEISMIC USE GROUP 1 SITE CLASS D 1.40g MAPPED SPECTRAL RESPONSE ACCELERATION, Ss 0.57g MAPPED SPECTRAL RESPONSE ACCELERATION, S1 0.931 SPECTRAL RESPONSE COEFFICIENT, Srs 0.380 SPECTRAL RESPONSE COEFFICIENT, Srs SEISMIC DESIGN CATEGORY D</p>	<p><b>CONCRETE:</b></p> <ol style="list-style-type: none"> <li>UNLESS NOTED OTHERWISE ON THE DRAWINGS OR SPECIFIED, ALL STRUCTURAL CONCRETE SHALL BE 4000 psi.</li> <li>CONCRETE FOR FOOTINGS AND FOUNDATIONS SHALL BE 4000 psi UNLESS OTHERWISE NOTED.</li> <li>MIXING AND PLACING OF ALL CONCRETE AND SELECTION OF MATERIALS SHALL BE IN ACCORDANCE WITH THE APPLICABLE CODE.</li> <li>COLD AND HOT WEATHER MIX DESIGNS SHALL BE SUBMITTED TO THE RESIDENT CONSTRUCTION INSPECTOR FOR REVIEW 30 DAYS PRIOR TO USE.</li> <li>UNLESS INDICATED OTHERWISE, CONCRETE SURFACES LEADING TO DRAINS SHALL BE SLOPED A MINIMUM OF 1 INCH PER FOOT TOWARD DRAIN, WITH ALL DECK AND THE ADJACENT SURFACES WARPED AS REQUIRED TO SATISFY AN ADEQUATE DRAINAGE FLOW.</li> <li>ALL EXPOSED CONCRETE EDGES AND CORNERS SHALL BE CHAMFERED WITH A 3/4 INCH BY 45 DEGREE CHAMFER, EXCLUDING THE PLATFORM EDGE AND TACTILE AREAS.</li> <li>PROVIDE 2-INCH CONCRETE COVER ON REINFORCEMENT BARS EXCEPT AS NOTED.</li> <li>ALL EXPOSED CONCRETE SURFACES SHALL HAVE A RUBBED FINISH AS DEFINED IN THE SPECIFICATION 03345 UNLESS OTHERWISE NOTED.</li> </ol> <p><b>REINFORCING STEEL:</b></p> <ol style="list-style-type: none"> <li>REINFORCING STEEL SHALL BE DEFORMED STEEL CONFORMING TO THE REQUIREMENTS OF ASTM A615, GRADE 40 FOR #4 AND SMALLER BARS, GRADE 60 FOR #5 AND LARGER BARS.</li> <li>WIRE MESH SHALL CONFORM TO ASTM A185.</li> <li>WIRE MESH SHALL BE LAPPED 1-1/2 SPACES (9" MINIMUM).</li> <li>REINFORCING BARS SHALL BE SPLICED AS SHOWN ON THE DRAWINGS, OR AS REQUIRED BY ACI 318-05 CHAPTER 12.</li> <li>REINFORCING STEEL EXCEPT FOR STIRRUPS AND TIES SHALL HAVE A MINIMUM PROTECTIVE COVERING OF CONCRETE AS FOLLOWS:             <ul style="list-style-type: none"> <li>A) CONCRETE PLACED DIRECTLY AGAINST EARTH: 3"</li> <li>B) CONCRETE PLACED AGAINST FORMS BUT EXPOSED TO EARTH OR WEATHER:                     <ul style="list-style-type: none"> <li>PRIMARY REINFORCEMENT: 2"</li> <li>STIRRUPS, TIES, &amp; SPIRALS: 2"</li> </ul> </li> <li>C) CONCRETE PLACED AGAINST FORMS BUT NOT EXPOSED DIRECTLY TO EARTH OR WEATHER:                     <ul style="list-style-type: none"> <li>PRIMARY REINFORCEMENT: 1"</li> <li>STIRRUPS, TIES, &amp; SPIRALS: 1"</li> </ul> </li> </ul> </li> <li>ALL BARS SHALL BE CLEAN OF RUST, GREASE AND OTHER MATERIALS LIKELY TO IMPAIR BOND. ALL BENDS SHALL BE MADE COLD.</li> <li>ALL REINFORCING STEEL, EXCEPT WELDED WIRE FABRIC (WWF), TO BE EPOXY COATED BY CRSI CERTIFIED APPLICATOR.</li> </ol> <p><b>FOUNDATION:</b></p> <ol style="list-style-type: none"> <li>ALL EXCAVATIONS SHALL BE INSPECTED AND ALL COMPACTIONS OF FILL SHALL BE INSPECTED BY THE CONTRACTORS THIRD PARTY INDEPENDENT INSPECTOR.</li> </ol> <p><b>STEEL DECKING:</b></p> <ol style="list-style-type: none"> <li>STEEL ROOF DECKING SHALL CONFORM TO THE REQUIREMENTS OF THE STEEL DECK INSTITUTE SPECIFICATIONS AND APPLICABLE MANUFACTURER'S REQUIREMENTS.</li> <li>STEEL CELLULAR DECKING FOR CANOPY SHALL BE 20/20 GAUGE, 1-1/2" DEEP GALVANIZED METAL FORM DECK WITH MINIMUM MOMENT OF INERTIA 0.30 IN/FT. DECKING SHALL BE CONTINUOUS OVER A MINIMUM OF TWO SPANS.</li> </ol>	<p><b>STRUCTURAL STEEL AND MISC. METAL:</b></p> <ol style="list-style-type: none"> <li>ALL STRUCTURAL STEEL SHALL CONFORM TO ASTM A36, UNLESS NOTED OTHERWISE.</li> <li>ALL STEEL PIPES SHALL BE STANDARD PIPE, SCHEDULE 40 (UNLESS NOTED OTHERWISE), CONFORMING TO ASTM A53, GRADE B.</li> <li>STRUCTURAL TUBING SHALL CONFORM TO ASTM A500, GRADE B, Fy=46 KSI.</li> <li>ALL BOLTS SHALL BE GALVANIZED UNLESS OTHERWISE NOTED.</li> <li>MACHINE BOLTS SHALL CONFORM TO ASTM A307.</li> <li>HIGH-STRENGTH BOLTS SHALL CONFORM TO ASTM A325 (FRICTION TYPE).</li> <li>ALL CONNECTIONS SHALL BE MADE WITH HIGH-STRENGTH BOLTS UNLESS NOTED OTHERWISE ON THE DRAWINGS.</li> <li>ALL ANCHOR BOLTS SHALL CONFORM TO ASTM A307 (UNLESS NOTED OTHERWISE).</li> <li>ALL STEEL SHALL BE FABRICATED IN ACCORDANCE WITH THE ASTM (2003) APPLICABLE STANDARDS BY A CERTIFIED FABRICATOR.</li> <li>WELDING SHALL BE PERFORMED BY CERTIFIED WELDERS.</li> <li>THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR REVIEW PRIOR TO FABRICATION.</li> <li>ALL STRUCTURAL STEEL EXCEPT THAT EMBEDDED IN CONCRETE OR GROUT SHALL HAVE ONE SHOP COAT OF APPROVED PRIMER PAINT.</li> <li>CONTRACTOR SHALL PROVIDE SHIMS WHERE REQUIRED.</li> <li>FOR ALL ROOF OPENINGS OVER 1'-0" PROVIDE SUPPORT, METAL ROOF DECK FABRICATOR TO REINFORCE ALL OPENINGS IN METAL DECK.</li> <li>ALL WELDING ELECTRODES SHALL BE E70XX OR EQUIVALENT.</li> <li>NO STRUCTURAL STEEL CONNECTION SHALL CONSIST OF LESS THAN 2 A325 BOLTS OR WELDS DEVELOPING A FACTORED LOAD OF 10 KIPS.</li> <li>ALL WELD SIZES NOT INDICATED SHALL COMPLY WITH SECTION J2 OF THE AISC SPECIFICATION LRFD THIRD EDITION.</li> <li>BEARING PLATE SHOULD CONTAIN HOLES FOR GROUTING, WHICH ARE IN ACCORDANCE WITH AISC SPECIFICATION LRFD THIRD EDITION.</li> </ol> <p><b>WELDING:</b></p> <ol style="list-style-type: none"> <li>WELD LENGTHS SHOWN ARE EFFECTIVE LENGTH PER CODE. WHERE LENGTHS ARE NOT SHOWN, THE WELD SHALL BE THE FULL LENGTH OF THE JOINT.</li> <li>ALL WELDING REQUIREMENTS SHOWN OR INDICATED ON THE DRAWINGS MAY BE FIELD OR SHOP WELDED AS REQUIRED FOR EFFICIENT ERECTION, SUBJECT TO THE APPROVAL OF THE STRUCTURAL ENGINEER.</li> <li>ALL WELDING SHALL BE AS PER THE LATEST EDITION OF AWS D1.1.</li> </ol> <p><b>TOLERANCES:</b></p> <ol style="list-style-type: none"> <li>FLOOR LEVEL SHALL NOT DEVIATE OVER 1/8" FROM THE THEORETICAL FLOOR LEVELS SHOWN ON THE DRAWINGS.</li> <li>EDGE OF EXTERIOR WALL SHALL BE ON A STRAIGHT OR A CURVED LINE AS SHOWN ON THE PLAN VIEW AND SHALL NOT DEVIATE MORE THAN 1/2" TOTAL FROM THEORETICAL STRAIGHT OF CURVED LINES AS SHOWN ON DRAWINGS.</li> <li>TOLERANCE LIMITS FOR CONCRETE CONSTRUCTION SHALL BE SPECIFIED IN THE LATEST EDITION OF ACI 117.</li> <li>TOLERANCE LIMITS FOR STEEL CONSTRUCTION SHALL BE AS SPECIFIED IN THE LATEST EDITION OF THE CODE OF STANDARD PRACTICE BY THE AMERICAN INSTITUTE OF STEEL CONSTRUCTION.</li> </ol>		
<p><b>STRUCTURAL SYMBOLS</b></p> <p>MOMENT CONNECTION</p> <p>STEEL DECK</p>		<p>Designed By: F. NANNENGA</p> <p>Drawn By: T. COVEYOU</p> <p>Checked By: A. DIGIROLAMO</p> <p>Approved By: F. NANNENGA</p>	<p>S-LINE EXTENSION PROJECT</p> <p>STATION</p> <p>STRUCTURAL SYMBOLS, ABBREVIATIONS, AND NOTES</p>	<p>State: NTS</p> <p>CADD Filename: SL-C-P-0-201.dwg</p> <p>Submitted Date: 01-16-26</p> <p>UTA Contract No: 24-03849</p> <p>Drawing No: P0201</p> <p>Sheet No:</p>

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			<p>Submitted By:</p>	<p>Approved By:</p>
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- NOTES:
1. SLABS ON GRADE SHALL BE THICKENED TO 12" MIN AT EXPANSION JOINT TO ALLOW SPACE FOR DOWELS.
  2. FOR WALLS, FORM ALL JOINT EDGES AT 1/2" CHAMFER.
  3. FOR SLABS, PROVIDE 1/2" RADIUS TOOLED EDGES AT TOP SURFACE.
  4. "W"=1" WIDE UNLESS OTHERWISE INDICATED ON THE DRAWINGS.
  5. USE DETAIL "B" AT UNDERSIDE OF SLABS ON GRADE ONLY. USE DETAIL "A" AT ALL OTHER LOCATIONS.
  6. AT EXPANSION JOINTS, GREENSTREAK "SPEED DOWEL" NO. PSD12#7TX (BASE CODE PSD#7BX) OR APPROVED EQUAL MAY BE USED IN LIEU OF SMOOTH DOWEL AND PVC ASSEMBLY SYSTEM SHOWN.
  7. SEE ARCHITECTURAL PLANS FOR LOCATIONS OF EJS.



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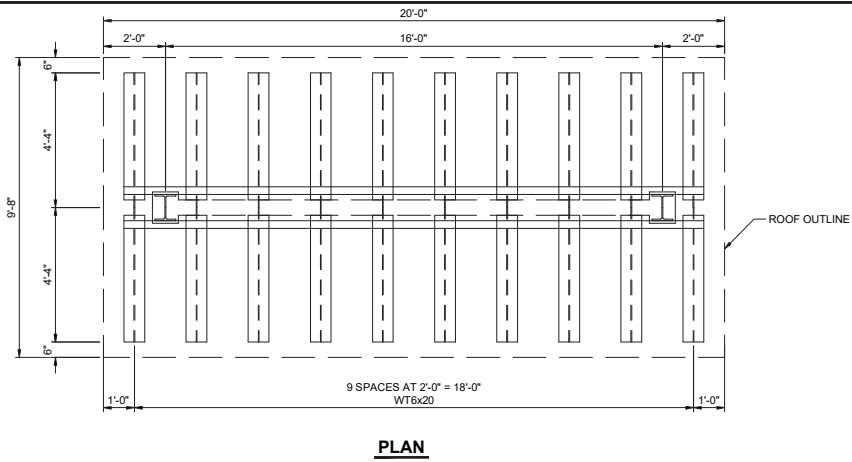


Designed By	F. NANNENGA
Drawn By	T. COVEYOU
Checked By	A. DIGIROLAMO
Approved By	F. NANNENGA

S-LINE EXTENSION PROJECT

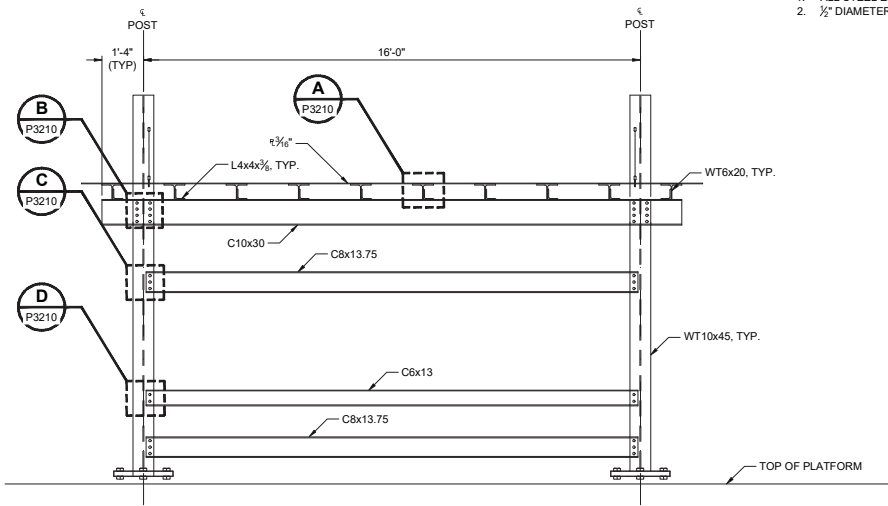
STATION  
TYPICAL STRUCTURAL DETAILS

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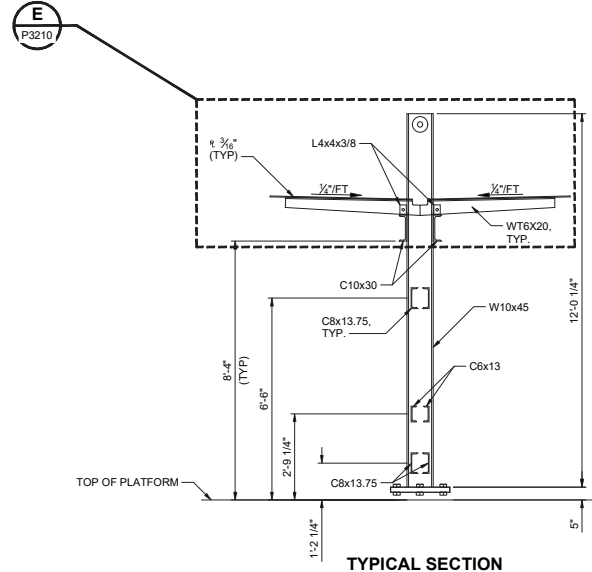


**PLAN**

- NOTES:  
 1. ALL STEEL ELEMENTS TO BE GALVANIZED.  
 2. 1/2" DIAMETER CABLE NOT SHOWN



**ELEVATION**



**TYPICAL SECTION**

12/21/2025			
REV	DATE	Description	



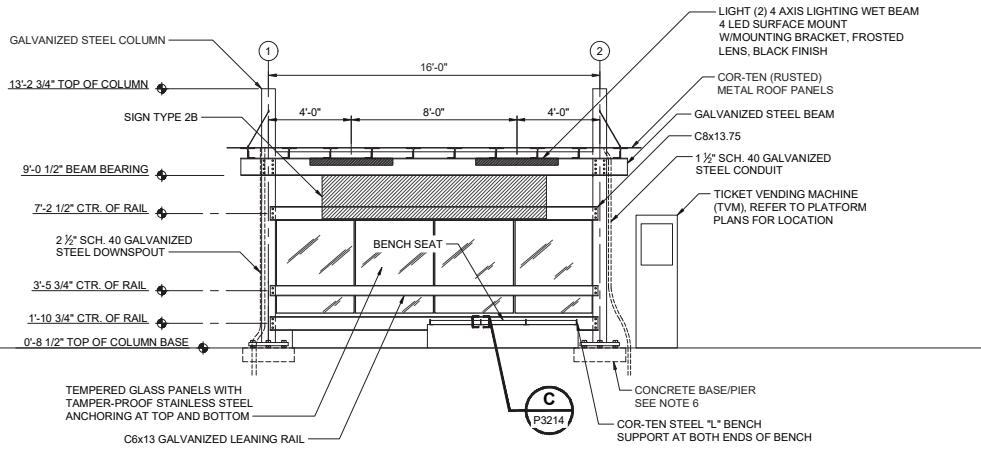
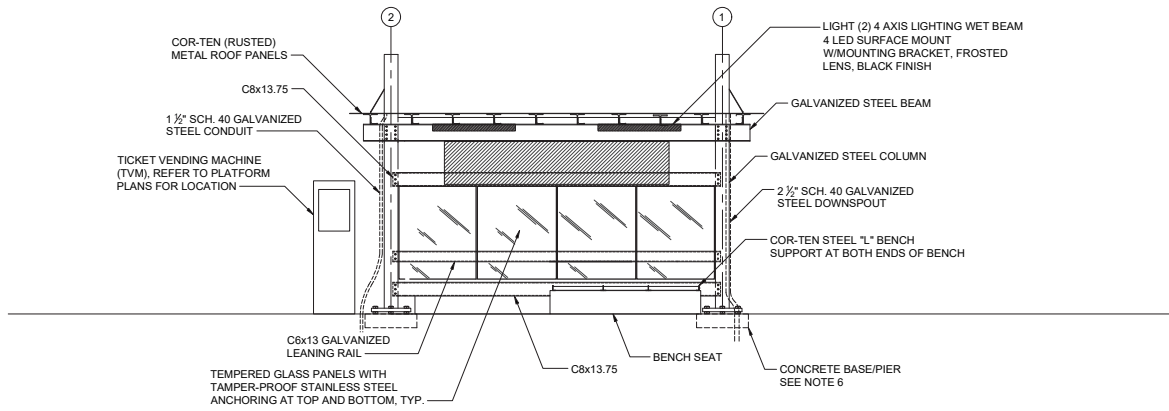
Designed By:  
F. NANNENGA  
 Drawn By:  
T. COVEYOU  
 Checked By:  
A. DIGIROLAMO  
 Approved By:  
F. NANNENGA

S-LINE EXTENSION PROJECT  
 STATION  
 STRUCTURAL DRAWINGS  
 20'-0" DOUBLE CANOPY

Scale	1/2"=1'-0"
CADD Filename	SL-C-P-3-202.dwg
Submital Date	01-16-26
UTA Contract No.	24-03849
Drawing No.	P3202
Sheet No.	







**20' DBL CANOPY FRONT ELEVATION**  
NTS

- GENERAL CANOPY NOTES:
1. EACH CANOPY SHALL HAVE ONE DATA CONDUIT AND ONE POWER CONDUIT. CONDUITS SHALL BE ONE AND ONE HALF INCH GALVANIZED.
  2. DATA CONDUIT SHALL BE PLACED ON PRIMARY VERTICAL POST, ON THE OUTSIDE FLANGE. PLACE CONDUIT ON SIDE OF CANOPY TO ENSURE SHORTEST DISTANCE BETWEEN CONDUIT AND THE PCC.
  3. POWER CONDUIT SHALL BE PLACED ON PRIMARY VERTICAL BEAM, ON THE OUTSIDE FLANGE. PLACE CONDUIT ON THE POST THAT DOES NOT HOUSE THE DATA CONDUIT.
  4. EACH CANOPY SHALL HAVE ONE DOWNSPOUT PLACED ON THE PRIMARY VERTICAL POST OPPOSITE OF THE CONDUIT, ON THE OUTSIDE FLANGE. DOWNSPOUT SHALL BE TWO AND ONE HALF INCH GALVANIZED.
  5. ELEVATIONS REFER TO DISTANCE ABOVE TOP OF RAIL.
  6. SEE DRAWING P3208 FOR CANOPY FOUNDATION DETAILS.

12/21/2025			
REV	DATE	Description	



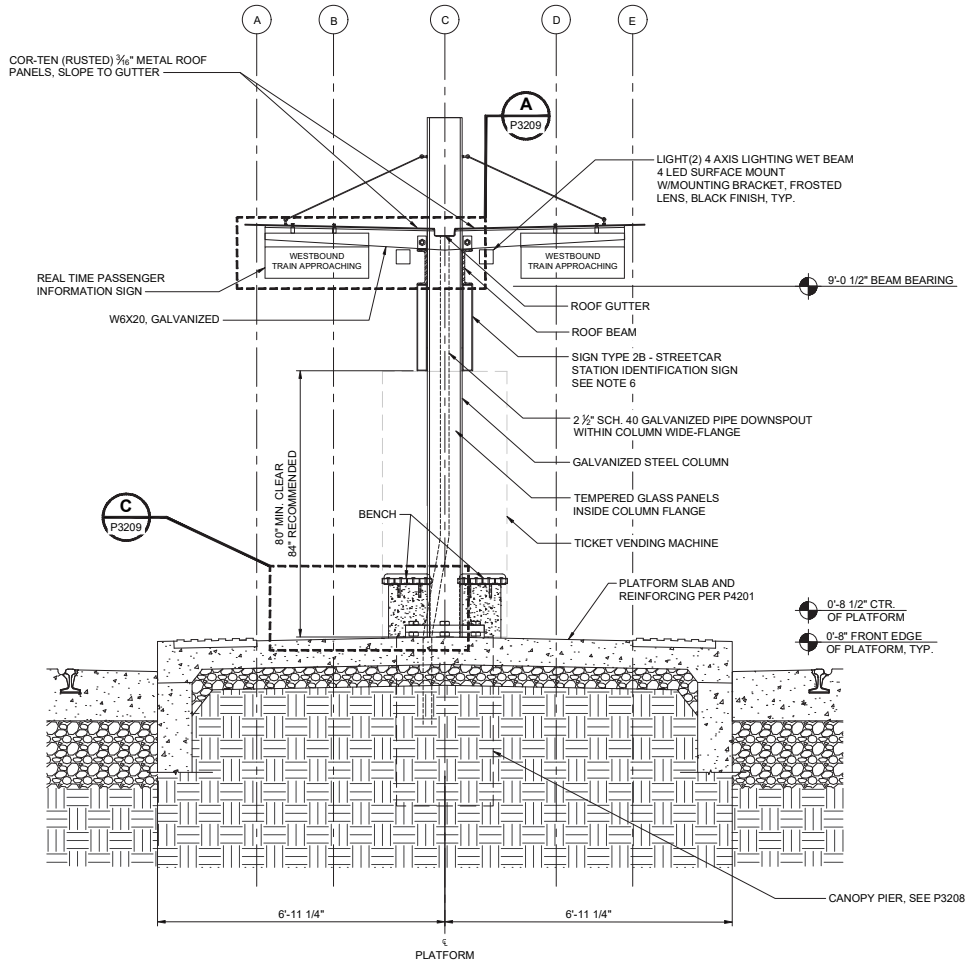
Designed By:	F. NANNENGA
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

S-LINE EXTENSION PROJECT

STATION  
20 FOOT DOUBLE CANOPY  
ELEVATIONS

State:	NTS
CADD Filename:	SL-C-P-3-205.dwg
Submital Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	P3205
Sheet No.:	

12/21/2025



- GENERAL CANOPY NOTES:
1. EACH CANOPY SHALL HAVE ONE DATA CONDUIT AND ONE POWER CONDUIT. CONDUITS SHALL BE ONE AND ONE HALF INCH GALVANIZED.
  2. DATA CONDUIT SHALL BE PLACED ON PRIMARY VERTICAL POST, ON THE OUTSIDE FLANGE. PLACE CONDUIT ON SIDE OF CANOPY TO ENSURE SHORTEST DISTANCE BETWEEN CONDUIT AND THE PCC.
  3. POWER CONDUIT SHALL BE PLACED ON PRIMARY VERTICAL BEAM, ON THE OUTSIDE FLANGE. PLACE CONDUIT ON THE POST THAT DOES NOT HOUSE THE DATA CONDUIT.
  4. EACH CANOPY SHALL HAVE ONE DOWNSPOUT PLACED ON THE PRIMARY VERTICAL POST OPPOSITE OF THE CONDUIT, ON THE OUTSIDE FLANGE. DOWNSPOUT SHALL BE TWO AND ONE HALF INCH GALVANIZED.
  5. ELEVATIONS REFER TO DISTANCE ABOVE TOP OF RAIL.
  6. SEE DRAWING P3219 FOR SIGN DETAILS.

**20' DBL SIDE CANOPY SECTION AT BENCH**  
NTS

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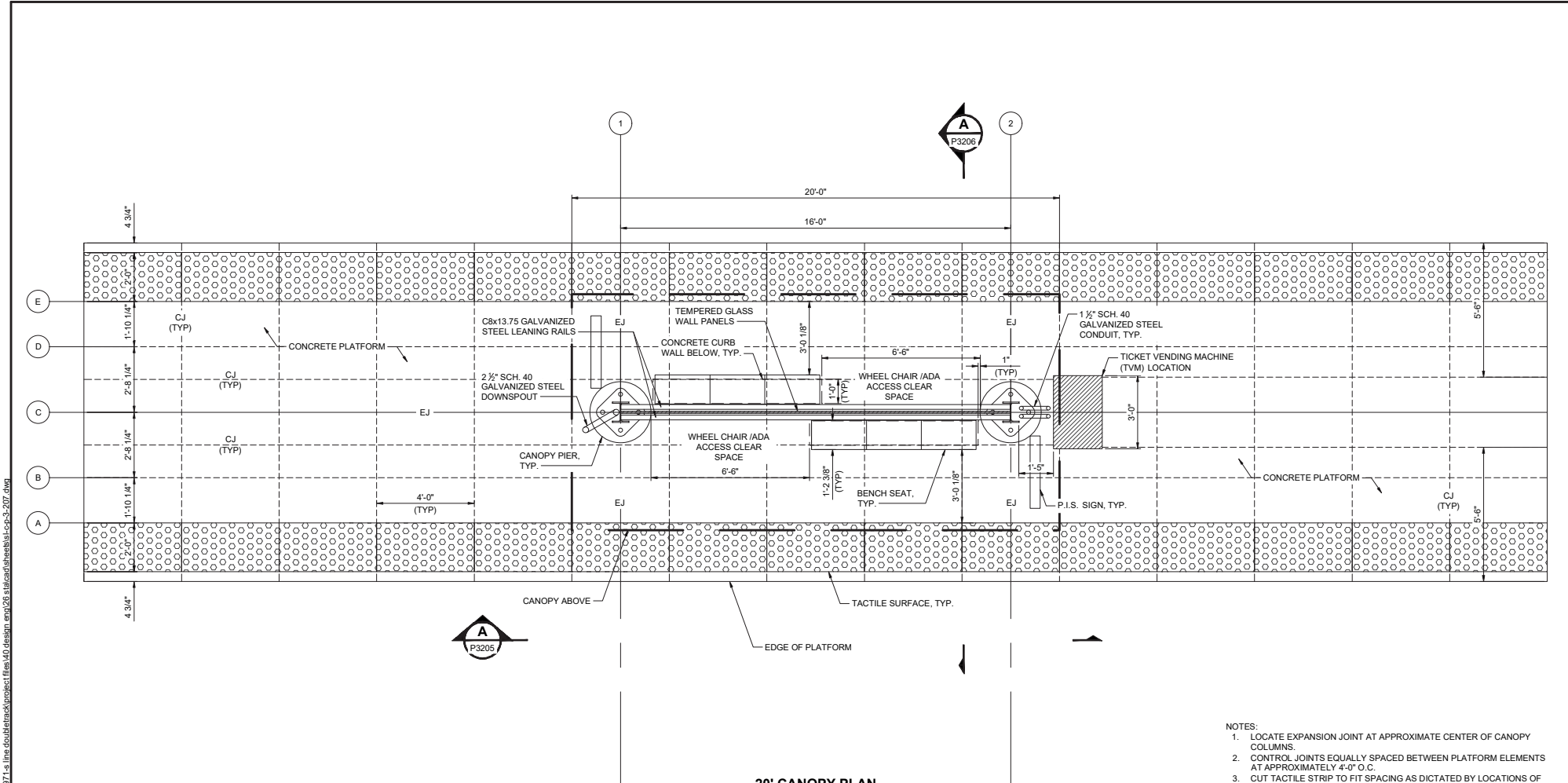


Designed By: F. NANNENGA
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT

STATION  
20 FOOT DOUBLE CANOPY  
SECTION

State:	NTS
CADD Filename:	SL-C-P-3-206.dwg
Submital Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	P3206
Sheet No.:	



**20' CANOPY PLAN**  
SCALE: 1" = 2'-0"

- NOTES:
1. LOCATE EXPANSION JOINT AT APPROXIMATE CENTER OF CANOPY COLUMNS.
  2. CONTROL JOINTS EQUALLY SPACED BETWEEN PLATFORM ELEMENTS AT APPROXIMATELY 4'-0" O.C.
  3. CUT TACTILE STRIP TO FIT SPACING AS DICTATED BY LOCATIONS OF CANOPY COLUMNS, TYP.

- TVM NOTES:
1. TICKET VENDING MACHINE (TVM) SHALL BE INSTALLED WITH REAR OF MACHINE SIX INCHES FROM FACE OF ADJACENT OBSTRUCTION TO ALLOW FOR MAINTENANCE ACCESS.

12/21/2025			
REV	DATE	Description	



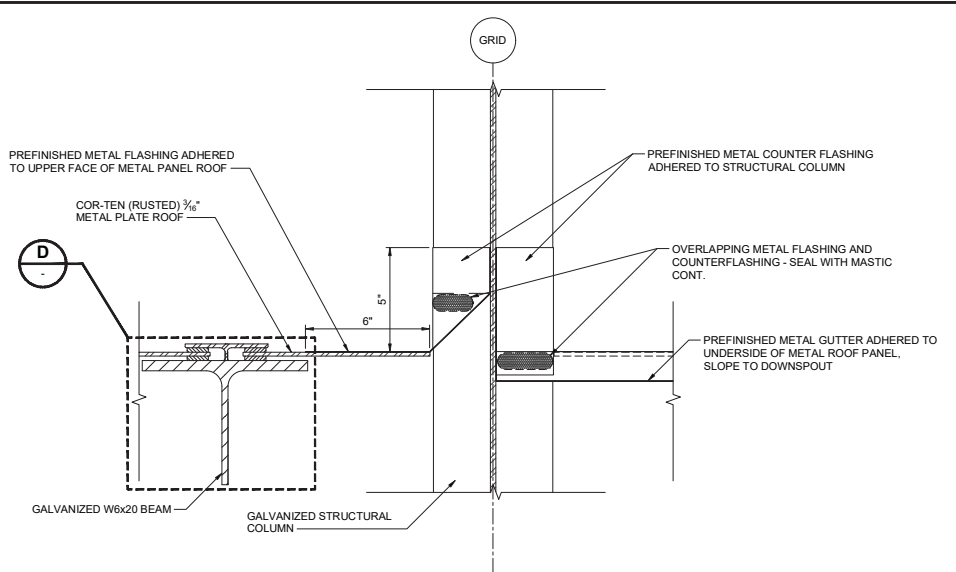
Designed By:	T. COVEYOU
Drawn By:	T. COVEYOU
Checked By:	A. DIGIROLAMO
Approved By:	F. NANNENGA

S-LINE EXTENSION PROJECT  
  
STATION  
20 FOOT STANDARD CANOPY  
FLOOR PLAN

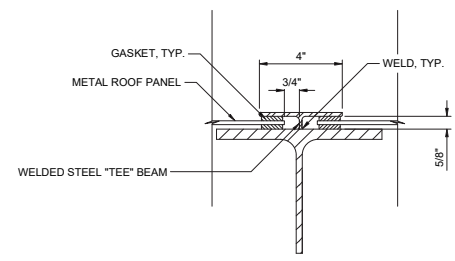
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Submitted Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	P3207
Sheet No.:	

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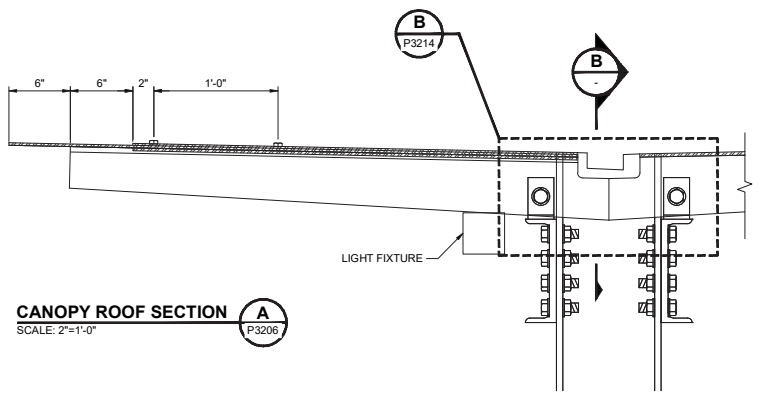




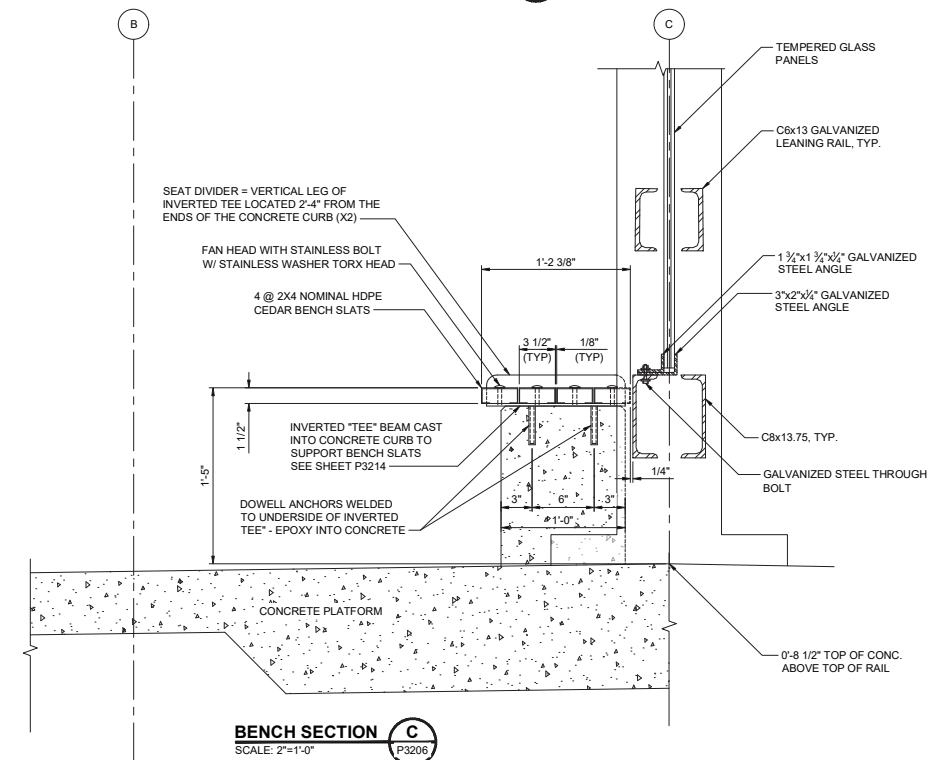
**ROOF FLASHING DETAIL B**  
SCALE: 4"=1'-0"



**PANEL ATTACHMENT DETAIL D**  
SCALE: 8"=1'-0"



**CANOPY ROOF SECTION A**  
SCALE: 2"=1'-0"



**BENCH SECTION C**  
SCALE: 2"=1'-0"

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REV	DATE	Description	



Designed By: F. NANNENGA
Drawn By: T. COVEYOU
Checked By: A. DIGIROLAMO
Approved By: F. NANNENGA

S-LINE EXTENSION PROJECT  
  
STATION  
20 FOOT DOUBLE CANOPY  
DETAILS

State:	AS NOTED
CADD Filename:	SL-C-P-3-209.dwg
Submital Date:	01-16-26
UTA Contract No.:	24-03849
Drawing No.:	P3209
Sheet No.:	