



**Wednesday, April 1, 2026  
Development Review Committee**

**DEVELOPMENT REVIEW COMMITTEE AGENDA**

**PUBLIC NOTICE** is hereby given that the Development Review Committee of Spanish Fork, Utah, will hold a regular meeting at the City Council Chambers at Library Hall, 80 South Main Street, Second Floor, Spanish Fork, Utah, commencing at 10:00 a.m. This meeting is not available to attend virtually.

**1. Approval of Minutes**

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A. MARCH 25, 2026.

**2. Amended Site Plan Approval**

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A. PATEY AVIATION TIE-DOWN PLAN. This proposal is to discuss a new layout for tie downs which is an amendment to the Patey Aviation Phase 1 Site Plan.

**3. Site Plan**

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A. GLH INDUSTRIAL STAGING AREA. This proposal involves the development of an outdoor staging area to be located at approximately 950 West 2140 North.

**4. Minor Plat Amendment**

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A. MAPLE MOUNTAIN PLAT N - PHASE 2 - REVISED. This proposal involves an adjustment to the road to realign 320 North.

**5. Zone Change**

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A. STATION 61 - ARGYLE SUBSTATION ZONE CHANGE. This proposal involves changing the zoning from R-O to P-F to accommodate the development of a new City Fire Station and substation at 137 West Center Street.

**6. Concept Review**

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A. SEVEN BREW COFFEE CONCEPT.

B. 7th NORTH BUILDING CONCEPT.

C. U-HAUL SELF STORAGE CONCEPT.

**7. Adjourn**

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End

Draft Minutes  
Spanish Fork City Development Review Committee  
80 South Main Street  
Spanish Fork, Utah  
March 25, 2026

Staff Members Present: Cory Pierce, Public Works Director Extraordinaire; Seth “I didn’t do it” Perrins, City Manager; Dave Anderson, Community Development Director; Brandon Snyder, Senior Planner and Future Yankees Fan; David I’m The Mann, Senior Planner; Kasey Woodard, Community Development Secretary, Diet Coke Drinker; Ian Bunker, Associate Planner and Principal, Run With the Bulls Consulting; His Honor Vaughn Pickell, City Attorney; Joshua Nielsen, Assistant City Attorney, Oscar nominee; John Not So Little, Chief Building Official; Byron “stop calling me assistant” Haslam, City Engineer; Marcie Clark, Engineering Department Secretary of the Year; Jered “make it rain” Johnson, Engineering Division Manager; Kevin “Sparky” Taylor, Senior Power Utility Planner; Jake The Snake Theurer, Power and Light Superintendent; Bart “you won’t like me when I’m angry” Morrill, Parks Maintenance Supervisor; Bryton Shepherd, Landscape Architect, Taste Tester; Jason Where There’s Smoke Turner, Fire Marshall.

Citizens Present: Burke Davis, Bryce Davis, Gavin West, Boyd Brown, Wes Bastian, Matt Brown, Nathan Chappell.

Cory Pierce called the meeting to order at 10:00 a.m.

## MINUTES

March 18, 2026

Jake Theurer moved to approve the minutes of March 18, 2026.

Dave Anderson seconded and the motion **passed** all in favor.

## MINOR SUBDIVISION

HALES 2-LOT SUBDIVISION

Brandon Snyder opened the presentation by noting that the first two agenda items would be discussed concurrently. He provided the location of the proposal and explained that the applicant is seeking to subdivide an existing residential lot into two separate parcels. He stated that the current lot contains a single-family home, which the property owner intends to demolish to facilitate the subdivision.

Mr. Snyder confirmed that both proposed lots meet the minimum lot size requirements for the R-1-6 zoning designation. He further explained that the applicant has submitted a request for a zone change incorporating the Infill Overlay, which would allow for reductions in the standard front, side, and rear setbacks. Each resulting lot is intended to be developed with a single-family home.

He referenced a previous proposal for the site that included townhomes, noting that it did not move forward. He expressed that the current proposal represents a significant improvement and is more compatible with the surrounding neighborhood. Mr. Snyder also clarified the approval process, indicating that the Minor Subdivision would be reviewed and approved by the Development Review Committee (DRC), while the Infill Overlay request would require approval from the City Council.

Dave Anderson inquired whether the requested setback reduction applied only to the rear property line. Mr. Snyder responded that the applicant is requesting reductions to the front, side, and rear setbacks.

Engineering staff noted minor redline comments related to the existing access.

Although Mr. Hales was not present, the applicant was represented by Gavin West of Atlas Engineering.

Seth Perrins asked whether the applicant intends to sell the property. Mr. West indicated that he was unsure of the plans for the property at this time.

Staff expressed a preference for the applicant to be present to provide additional details regarding the proposal. The discussion continued with staff reviewing potential uses of the two single-family homes, including considerations related to building orientation and floor plan design.

John Little asked about the size of the proposed lots, and Mr. Snyder confirmed that each lot would be slightly over 6,000 square feet. Mr. Little noted that, at that size, each home could potentially qualify for an accessory dwelling unit (ADU). He then asked whether each lot would be capable of meeting the associated parking requirements. Mr. Snyder stated that a detailed parking analysis had not been conducted but noted that each home is

proposed to include a two-car garage and driveway space, which may accommodate ADU parking requirements.

The discussion concluded with staff reviewing the building footprints and overall parking feasibility. It was also noted that the proposed homes would include basements.

Seth Perrins **moved** to approve the proposed Hales 2-Lot Subdivision Minor Subdivision based on the following finding and subject to the following conditions:

Finding:

1. That the proposal is consistent with the City's General Plan Designation of Medium Density Residential.

Conditions:

1. That the Applicant meets the City's development and construction standards, zoning requirements and other applicable City ordinances.
2. That the Applicant addresses any remaining staff review comments and pays all applicable fees prior to the plat being recorded.
3. That the standard setbacks of the R-1-6 Zone be followed unless the City Council approves of the corresponding zone map amendment to apply the I-F Overlay which would allow for modified setbacks.

Seth Perrins **seconded** and the motion **passed** all in favor.

## ZONE CHANGE

### HALES 2-LOT INFILL

There was no additional discussion.

Seth Perrins **moved** to recommend approval of the proposed Hales 2-Lot Infill Zone Change to City Council based on the following findings and subject to the following conditions:

Findings:

1. That the proposal is consistent with the City's General Plan Designation of Medium Density Residential.
2. That the proposal meets the intent of the Infill Overlay Zone.

Conditions:

1. That the Applicant meets the City's development and construction standards, zoning requirements and other applicable City ordinances.
2. That the Applicant addresses any staff review comments.
3. That the Applicant follows the approved building plans and exterior elevations.

John Little **seconded** and the motion **passed** all in favor.

## TITLE 15 AMENDMENTS

### TITLE 15 AMENDMENTS

Dave Anderson opened the discussion by presenting a proposed amendment to the City Code related to church height regulations in anticipation of a future project. He characterized the proposal as primarily administrative in nature, though he acknowledged that a zone change may also be required. He explained that the amendment would allow churches within residential zones to exceed the current 35-foot height limitation, provided specific criteria are met. These include a minimum building size of 70,000 square feet, a maximum building height of 80 feet, and allowance for steeples or towers up to 190 feet.

Vaughn Pickell advised that, based on recent communication with legal counsel representing the church and his own research, the City must carefully consider the implications of the Religious Land Use and Institutionalized Persons Act (RLUIPA). He explained that federal law prohibits local governments from imposing regulations that substantially burden religious exercise unless supported by a compelling governmental interest and implemented through narrowly tailored measures.

Mr. Pickell emphasized that any proposed conditions—such as height restrictions or construction standards—must be evaluated within this legal framework. He noted that fire safety is a recognized compelling governmental interest and suggested that requiring non-combustible materials for structures reaching heights of up to 190 feet could be a defensible, narrowly tailored condition tied to public safety concerns, including emergency response capabilities.

In response to a question from Seth Perrins regarding whether the City could impose height restrictions on religious buildings, Mr. Pickell clarified that such regulations are permissible if they meet the legal threshold of serving a compelling interest and are not overly broad.

Staff then discussed whether amending the code was necessary, considering potential legal limitations on regulating religious uses. The conversation included concerns about emergency response, particularly the reach of fire apparatus for taller structures, and whether those concerns could justify specific regulatory conditions. Staff also reviewed the proposed amendments in detail and made revisions, including modifications to Amendment D.

Brandon Snyder inquired whether discussions had occurred with representatives of the church and whether any concerns had been raised. Mr. Pickell indicated that a meeting with the property owner's legal counsel was scheduled for the following week and noted that the timing of the discussion was appropriate. He acknowledged uncertainty as to whether the church would accept the proposed regulations or assert protections under RLUIPA.

Seth Perrins recommended that Mr. Pickell proceed with discussions with the developer's legal counsel. Mr. Anderson concluded by noting that the Planning Department had provided the required public notice regarding the proposed amendment.

Seth Perrins **moved** to recommend approval of the proposed Title 15 Amendments to City Council to City Council for uses subject to conditions:

Conditions:

1. That the church be taller than 35 feet.
2. That the language from today's discussion be added.
3. That the City Attorney be charged with having additional conversations regarding the matter and bring back any information should that be needed.

Dave Anderson **seconded** and the motion **passed** all in favor.

## CONCEPT REVIEW

### DAVIS TOWNHOMES CONCEPT

The applicants, Burke and Bryce Davis, approached the podium and explained that they are requesting a zoning change to R-3 to accommodate a 16-unit townhome development. They noted that the property is currently zoned R-R and that the proposed rezone would allow for higher density. The applicants also indicated that the site layout, prepared by Atlas Engineering, has been reviewed by staff. The project site is located on the south side of Cal Pac Avenue.

Brandon Snyder stated that this area is currently under review as part of the General Plan update, with consideration being given to changing the designation from Mixed-Use to Low Density Residential. He indicated that staff is seeking input from the Planning Commission on this concept prior to moving forward. He further noted that at the March Planning Commission meeting, the Commission recommended approval of a draft General Plan that would designate the area for approximately 3.5 units per acre. He suggested that consideration should be given to the surrounding uses, particularly those to the north and east.

Dave Anderson commented that it would be beneficial to consider the adjacent River Meadows Townhomes development to the west, which consists of six-unit buildings. He acknowledged the presence of nearby half-acre residential properties that accommodate larger animals and expressed sensitivity to those neighboring uses. While he indicated general support for townhome development in the area, he emphasized that the design would be an important factor. He also noted his appreciation that the proposed design does not include driveways accessing Cal Pac Avenue.

Mr. Davis stated that the proposed plan exceeds minimum parking requirements and incorporates open space.

Staff and the applicants briefly discussed the proposed layout and clarified that the purpose of the Concept Review is to provide feedback to guide the project.

Mr. Davis further expressed their intent to utilize the parcel efficiently by maximizing the number of units while still providing a quality development with open space amenities.

Seth Perrins stated that he was not in favor of the current design, expressing concern that the proposal is too dense. While he did not specify a preferred number of units, he indicated that maximizing unit count should not be the primary objective.

In response, Mr. Davis noted that the current proposal had already been reduced from an earlier concept of 21 units and that the design reflects what they believe is appropriate under the R-3 zoning designation.

Dave Anderson asked if Mr. Perrins could provide more specific guidance. Mr. Perrins responded that he did not have a specific number in mind but reiterated that the development feels overly crowded.

Mr. Davis disagreed, stating that the project includes adequate open space and is less dense than a nearby development.

Cory Pierce commented that the site orientation could be improved, and Vaughn Pickell suggested reorienting the units on the southern portion of the site. Mr. Pierce added that the transition to surrounding uses should be softened.

The discussion concluded with staff expressing concerns regarding the proposed density and indicating that they were not supportive of the initial design as presented. Staff felt that the use would certainly be appropriate, but the design and layout need to be revised.

#### WESTONE INDUSTRIAL CONCEPT

Boyd Brown approached the podium and thanked staff for their review comments. He acknowledged the concerns raised regarding the proposed access but expressed that he believes the site is well-suited for his project. Mr. Brown stated that while he understands staff's indication that a new access point may be unlikely, he is hopeful the City would be willing to explore a shared access arrangement between the project and the City.

Dave Anderson asked Mr. Brown how he envisioned the proposal functioning given the presence of a median on US Highway 6. Mr. Brown acknowledged the challenges and suggested that the access could operate as a right-turn-out only, with limited use.

Brandon Snyder requested input from the Engineering Department regarding the feasibility of the proposed access. He inquired about any planned future improvements to Highway 6 and whether UDOT might consider modifying the existing median or the current "T" intersection configuration, or if the design is intended to remain long-term.

Jered Johnson provided insight into the challenges of the proposal, explaining that Highway 6 is anticipated to function more as a major corridor, similar to a freeway, which limits opportunities for additional access points.

Seth Perrins asked whether the applicant had consulted with UDOT. Mr. Brown responded that he had approached the City first. Mr. Perrins suggested that coordination with UDOT should occur early in the process for proposals of this nature.

The group then discussed general safety concerns associated with the proposed access.

Cory Pierce agreed with the previous comments, noting that Highway 6 is a high-priority roadway. He questioned whether UDOT would permit modifications to the existing design and expressed agreement with Mr. Johnson that such changes are unlikely to be approved by the State.

Mr. Perrins also noted that any required improvements would likely be costly and expressed concern that the applicant may not be prepared to undertake that level of financial commitment. Mr. Brown asked whether the City would be willing to assist with

such improvements; however, it was clarified that the City is not able to fund or undertake modifications to US Highway 6.

The discussion concluded with staff expressing very little support for the proposal due to the significant access and safety concerns.

## DISCUSSION

### NELSON MEADOWS PLAT DISCUSSION

John Little introduced the item and explained that he had invited Nathan Chappell and his engineer, Matt Brown, to present their proposal for a Minor Plat Amendment at 1700 East Canyon Road. He provided background, noting that the property was originally platted as a single lot and later subdivided into three lots, with access to Lot 3 located between Lots 1 and 2.

He explained that the applicants are now proposing modifications to the existing plat, which raised several questions and concerns. In particular, he noted that the revised proposal includes an access easement that runs along the south side of Lot 2 and wraps behind it to serve Lot 3. He pointed out that Lot 3 does not have direct access to Canyon Road, meaning access is limited to either the existing route between Lots 1 and 2 or the proposed route along the south side of Lot 2.

He expressed concern about how this access arrangement would function, especially in relation to both vehicular access and utilities. He noted that the original design anticipated access along the south side of Lot 1; however, utility boxes have since been installed within that driveway area. He also observed that additional utility boxes are located along the south side of Lot 3, further complicating access and potentially limiting the usability of the easement.

Mr. Chappell explained that there were three primary issues with the existing layout. First, the placement of utility boxes occupies approximately five feet on both sides of the access area, creating a constraint. Second, he noted that, as discussed previously during the house approval, the home includes an exterior entrance that opens directly toward the driveway, resulting in a potential conflict where residents would step into the drive aisle. He emphasized that these factors have created challenges with the current access configuration.

Brandon Snyder asked Mr. Little for clarification regarding the DRC approving a shared access agreement but that the power department put utility boxes in the middle of what was platted as a shared driveway access and Mr. Little confirmed that this was correct.

Staff deliberated the proposed layout for the new driveway, location of the utility box and the lot layout.

Dave Anderson confirmed that the developer is wishing to relocate the driveway to the able location of the existing utility boxes

The discussion continued, focused primarily on the configuration and access for the three proposed lots, particularly addressing driveway placement, garage location, and utility relocation. The existing garage on the home addressed as 881, which previously had a carport, was reviewed with consideration for attaching it to the house while maintaining appropriate access.

Mr. Little voiced concerns centered on safety and accessibility as the basement entrance and stairs on one property limit sightlines for vehicles.

Jason Turner raised concerns regarding fire access requirements restricting driveway length without a substantial turnaround area.

Staff emphasized the importance of adhering to previously approved plans, including shared access driveways and easements, while relocating the transformer and other utility boxes to prevent conflicts with driveways and stairwells.

Staff members discussed options including slightly shifting the driveway south to maintain fire access and avoiding window wells and stairway conflicts, retaining the current lot configuration while adjusting driveway placement, or removing the garage entirely if necessary to achieve proper access for Lot 3. It was confirmed that Lot 3 meets minimum zoning requirements and setbacks. UDOT access from Canyon Road was considered unlikely due to prior restrictions, though it could be explored.

Matt Brown noted his prior discussions with UDOT but stated he was denied access off Canyon Road.

Seth Perrins questioned whether the garage was worth saving as he does not feel that it was worth the effort. He expressed his frustration with how this plan was approved, whether the fault being on the city staff or applicant's engineer. He felt that this oversight should have been caught much sooner.

While preserving the existing garage was discussed, it was recognized that doing so should not compromise access or fire safety. Staff acknowledged that the transformer was placed on the wrong side of the property but relocation was discussed.

Next steps outlined in the discussion include Mr. Brown preparing revised drawings that reflect the driveway adjustments and utility relocations, and Mr. Little overseeing the resolution of access issues in coordination with the new property owner to ensure compliance with plat requirements and fire access standards. Utilities, including the transformer and communication boxes, will be relocated to align with approved easements and avoid conflicts with driveways. **The revised plan will maintain Lot 3's zoning requirements while accommodating shared access and easements, and the group agreed to review the updated drawings and report back within one week.**

Overall, the consensus was to follow the original approved lot and driveway plans while making minor adjustments for safety and access, and ensure utilities and garages are properly integrated without compromising fire code or easement requirements.

Seth Perrins moved to adjourn the meeting at 11:30 a.m.

Adopted:

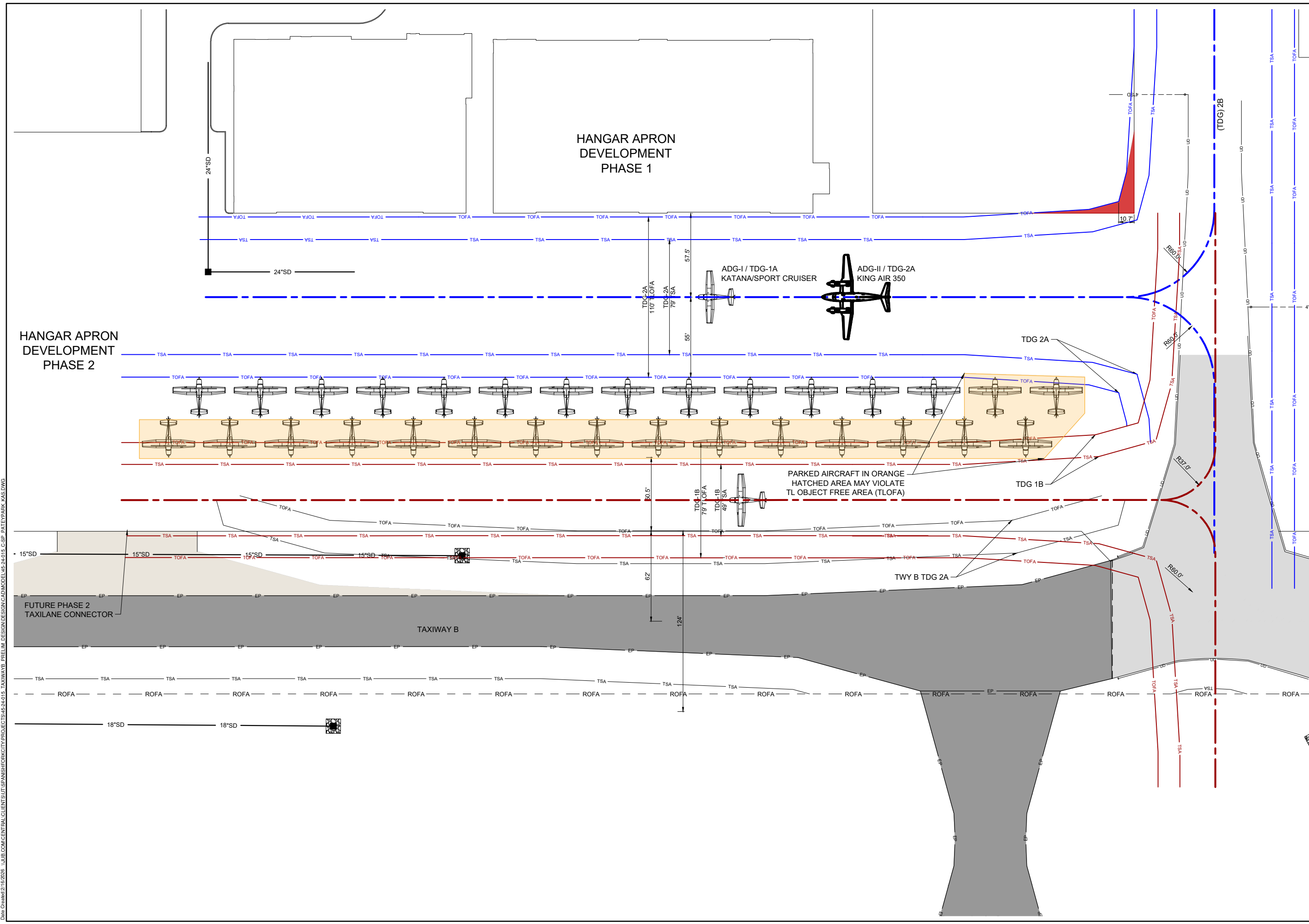
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**Kasey Woodard**  
Community Development Division  
Secretary

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NO.	REVISION	DESCRIPTION	BY	APPR.	DATE

**CONSTRUCT TAXIWAY B**  
**SPANISH FORK MUNICIPAL AIRPORT**  
**PROPOSED MEETING TDG SAFETY REQUIREMENTS**  
**PATEY AVIATION PARK - PHASE 1**  
**TIE DOWN MARKING PLAN**



Plot Date: 3/30/2026 11:50 AM Plotted By: Kimberly Silvester  
 Date Created: 2/16/26 JUB\CENTRAL\CLIENTS\UT\SPANISHFORK\PT\PROJECTS\45-24-015\_C-SP\_PATEYPARK\_KAS.DWG



J-U-B ENGINEERS, INC.

J-U-B ENGINEERS, INC.  
392 E. Winchester St.  
Suite 300  
Salt Lake City, UT 84107  
Phone: 801.886.9052  
www.jub.com

PRELIMINARY  
PLANS  
NOT FOR  
CONSTRUCTION

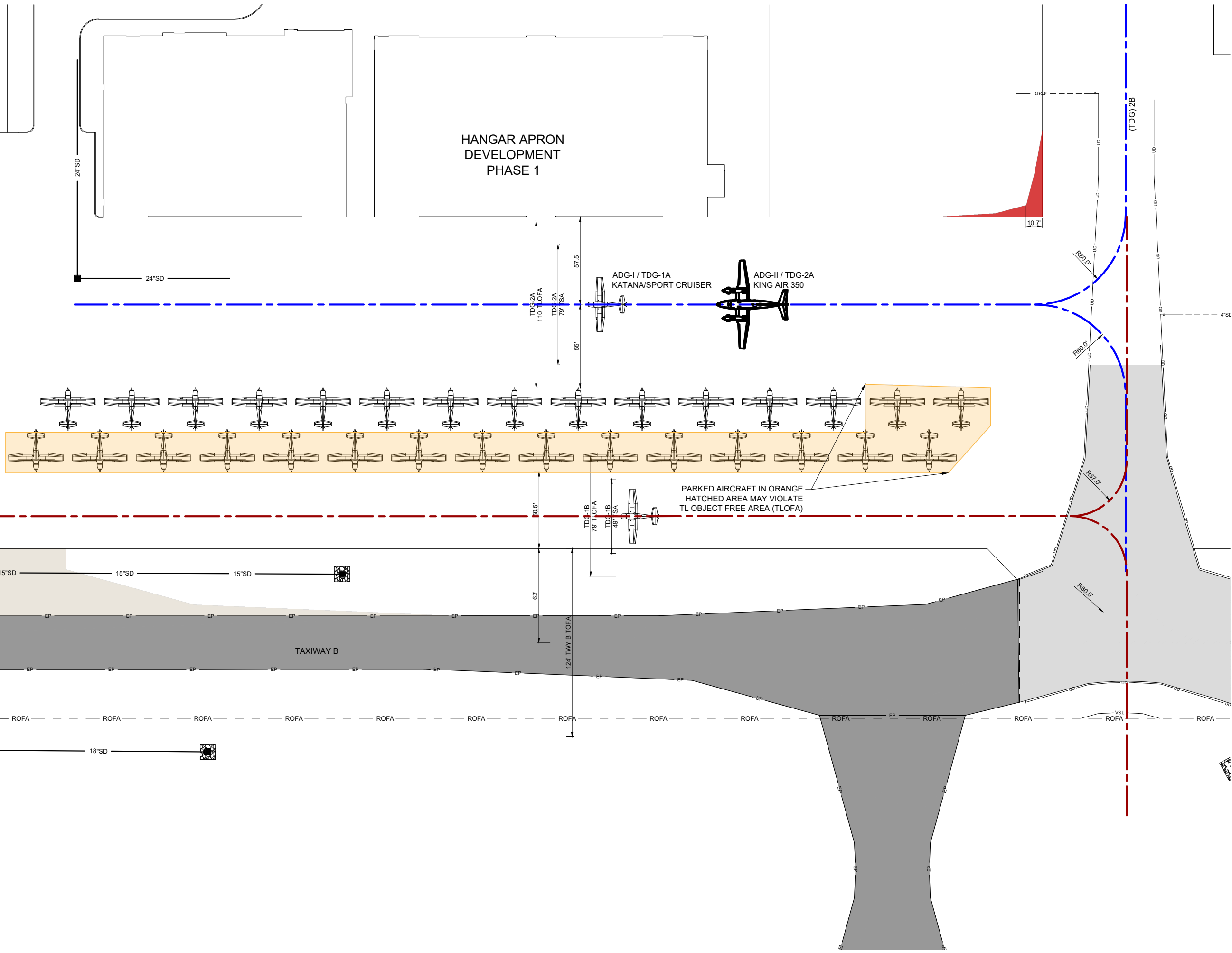
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CONSTRUCT TAXIWAY B  
SPANISH FORK MUNICIPAL AIRPORT  
PROPOSED MEETING TDG SAFETY REQUIREMENTS  
PATEY AVIATION PARK - PHASE 1  
TIE DOWN MARKING PLAN

FILE: 45-24-015 C-SP PATEYPARK KAS  
JUB PROJ. #: #####  
DRAWN BY: ###  
DESIGN BY: ###  
CHECKED BY: ###  
AT FULL SIZE, IF NOT ONE  
INCH, SCALE ACCORDINGLY  
LAST UPDATED: 2/18/2026  
SHEET NUMBER:  
**PTY-2**

### HANGAR APRON DEVELOPMENT PHASE 1

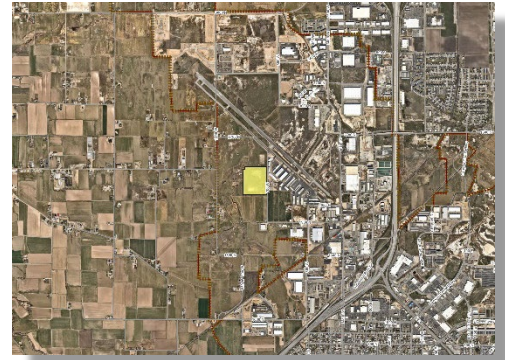
### HANGAR APRON DEVELOPMENT PHASE 2



Plot Date: 3/30/2026 11:54 AM Plotted By: Kimberly Silvester  
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GLH Industrial Staging Area  
Site Plan  
950 West 2140 North  
11.46 acres  
Industrial I-1 Zone  
Industrial General Plan  
Designation



**PROPOSAL**

The Applicant proposes to develop an outdoor staging area to be located at approximately 950 West 2140 North. The staging area would be located primarily to the west of the GLH Industrial Lot 1 existing building. The Applicant also proposes to use portions of the GLH Industrial Lot 1 site as staging area. Currently the request is planned to be temporary for a period of 3-6 years.

The Applicant proposes to encircle the staging area with 7' tall black chain link fencing. This type of fencing is an option because on February 17, 2026, the City Council approved the definition for Staging Areas as a distinct use separate from walled off Outdoor Storage Areas. The Applicant is proposing to not improve or dedicate 2140 North at this time.

Some of the key issues include: improvements and phasing.

**STAFF RECOMMENDATION**

That the proposed GLH Industrial Staging Area Site Plan be approved based on the following finding and subject to the following conditions:

**Finding**

- 1. That the proposal conforms to the City's General Plan Designation and Zoning Map.

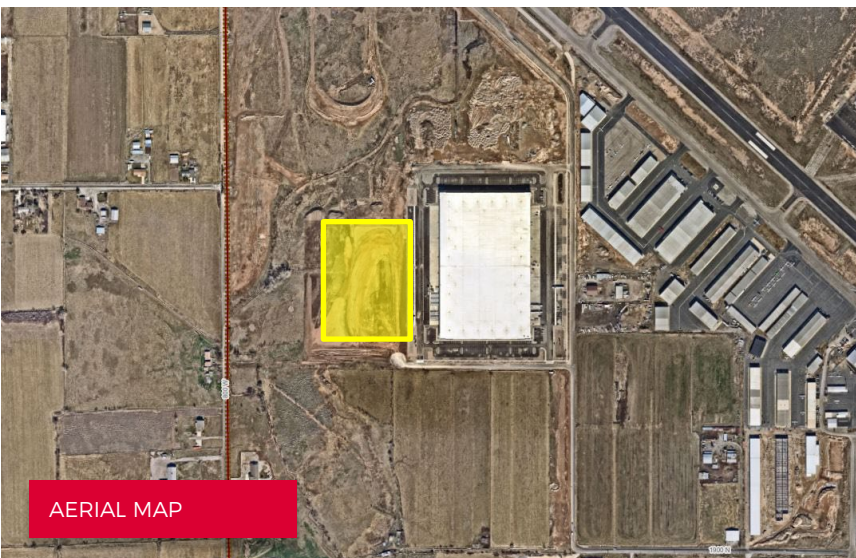
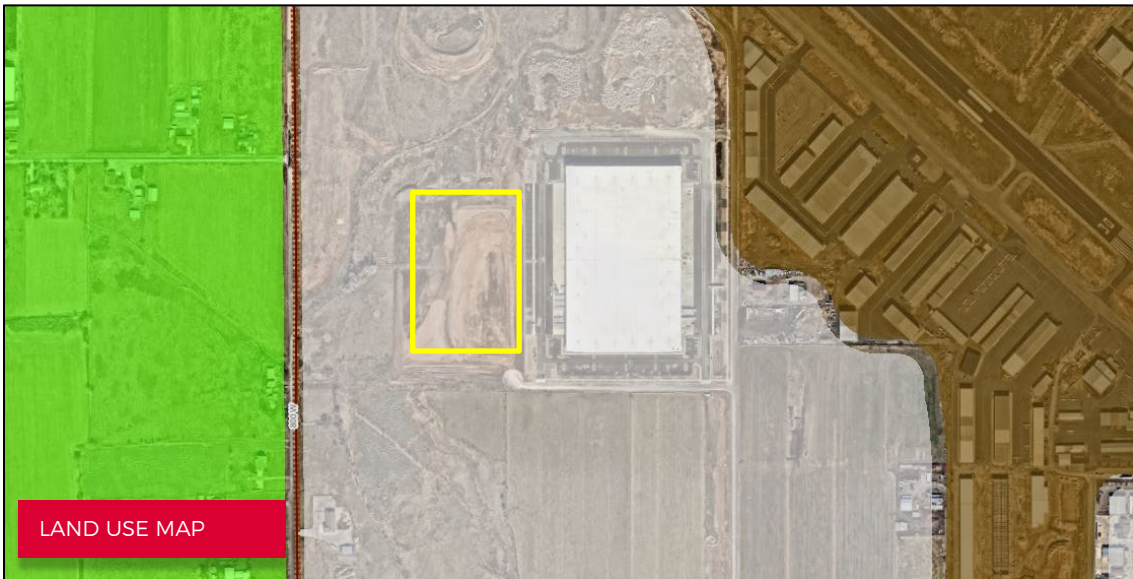
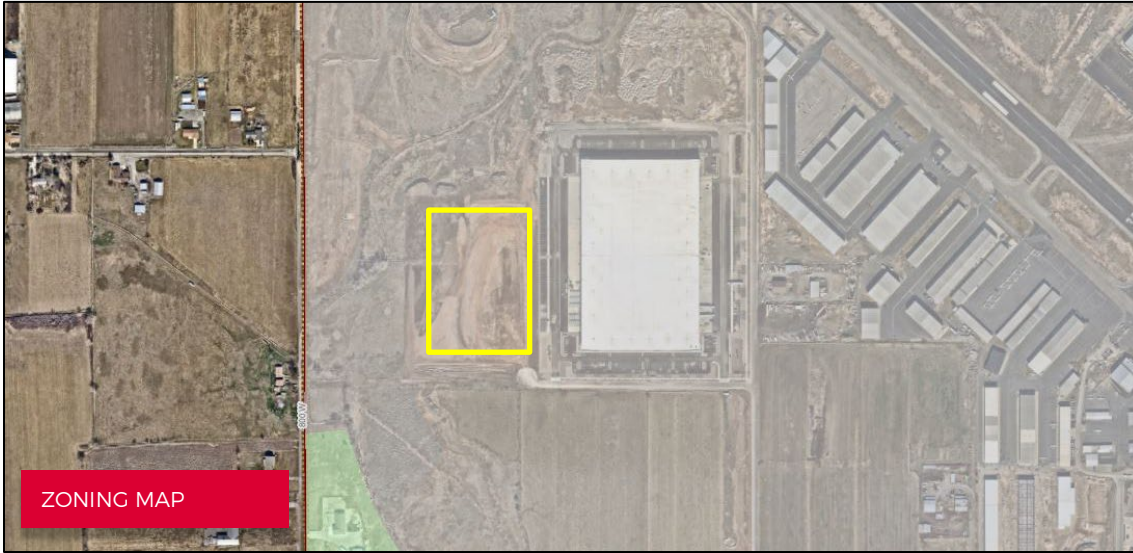
**Conditions**

- 1. That the Applicant meets the City's development and construction standards, zoning requirements and other applicable City ordinances.
- 2. That any remaining redlines are addressed.
- 3. That the Applicant obtains a fence permit prior to fence installation.
- 4. That the staging area be used for the storage, organization, loading and shipping of finished products and finished components only.
- 5. That the Applicant and City enter into an agreement to address 2140 North Street, including but not limited to: public improvements, easements, landscaping, and any other development obligations.

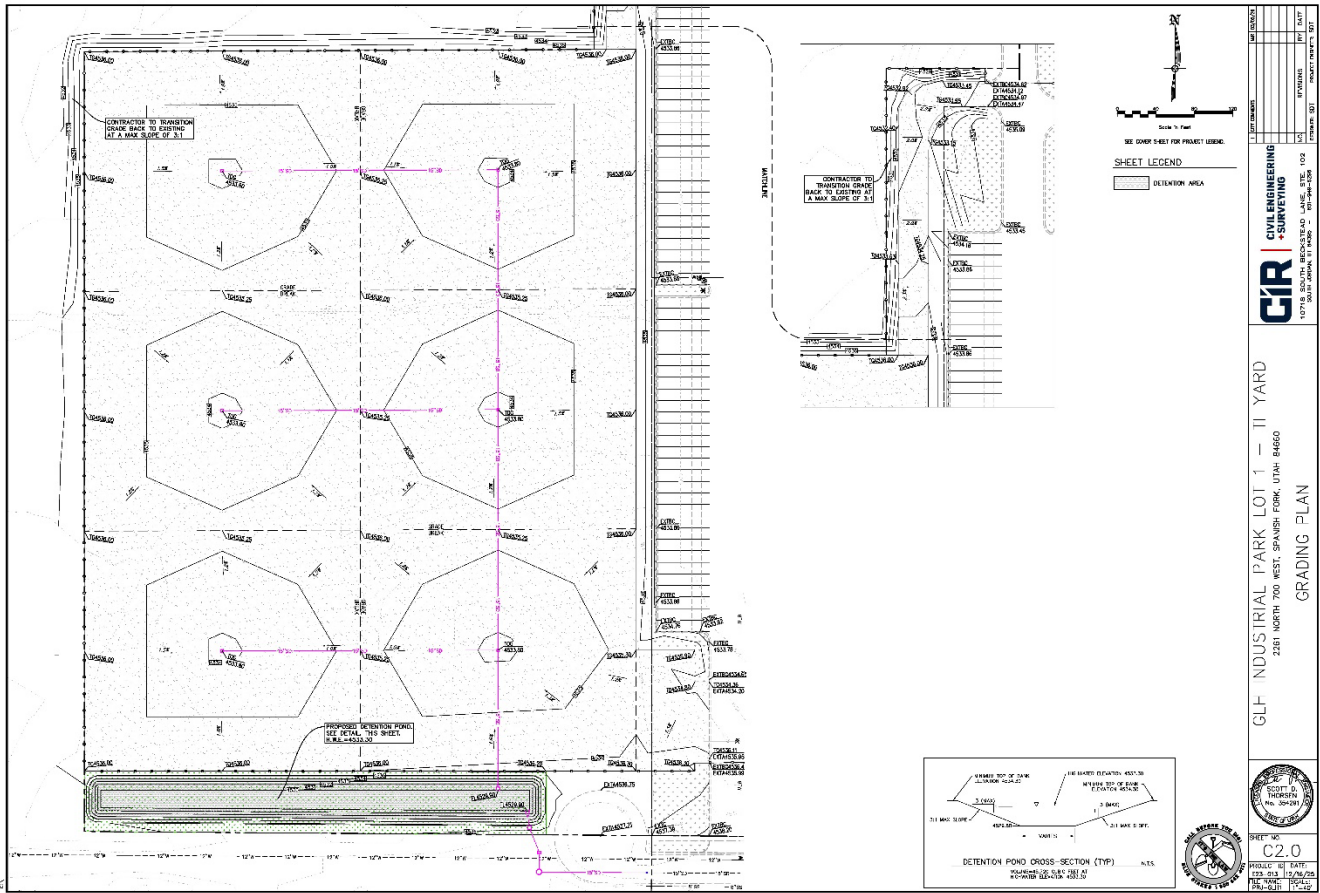
**EXHIBITS**

- 1. Area Maps
- 2. Site Plan
- 3. GLH Lot 1 Site Plan
- 4. GLH Preliminary Plat
- 5. Draft Agreement

EXHIBIT 1



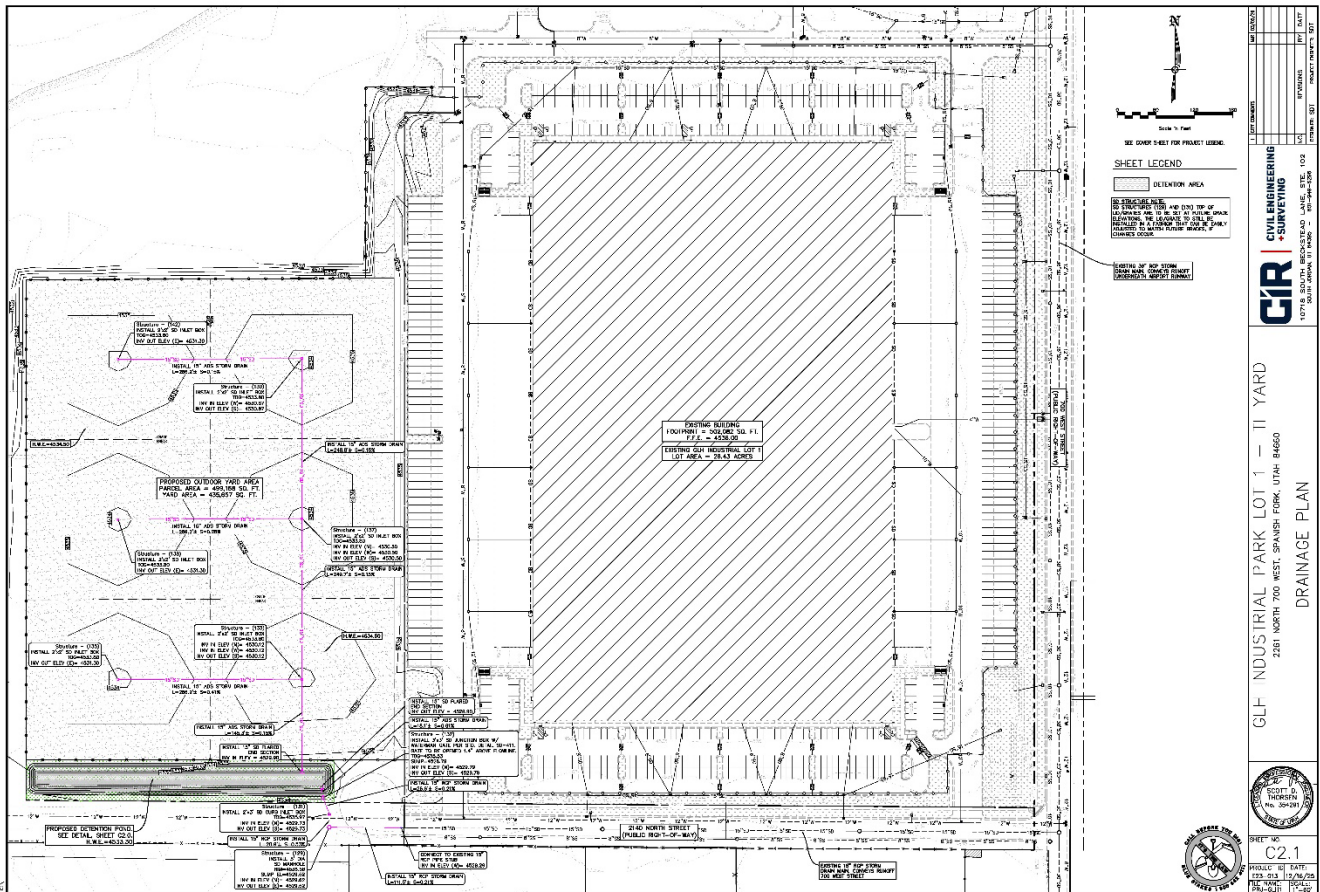




CIVIL ENGINEERING  
 + SURVEYING  
**GIR**  
 10718 SOUTH BECKSTEAD LANE, STE. 102  
 SPANISH FORK, UT 84660

GLF INDUSTRIAL PARK LOT 1 - II YARD  
 2301 NORTH 700 WEST, SPANISH FORK, UTAH 84660  
 GRADING PLAN

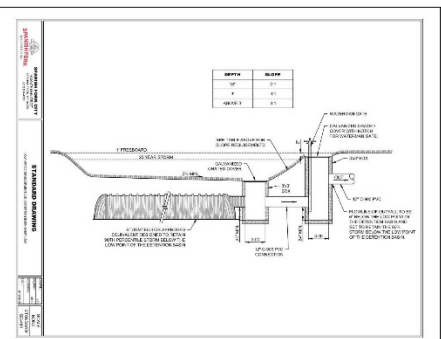
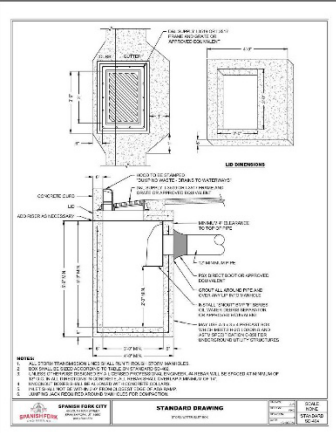
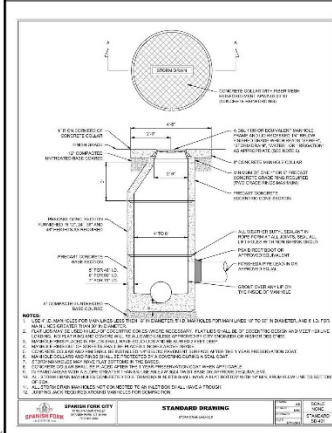
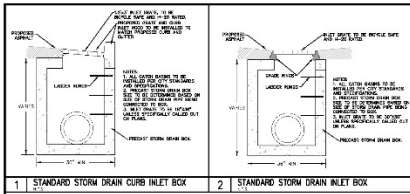
SHEET NO. C2.0  
 PROJECT NO. 17-013 (17-013) (17-013)  
 DATE: 12/15/2017  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]



CIVIL ENGINEERING  
 + SURVEYING  
**GIR**  
 10718 SOUTH BECKSTEAD LANE, STE. 102  
 SPANISH FORK, UT 84660

GLF INDUSTRIAL PARK LOT 1 - II YARD  
 2301 NORTH 700 WEST, SPANISH FORK, UTAH 84660  
 DRAINAGE PLAN

SHEET NO. C2.1  
 PROJECT NO. 17-013 (17-013) (17-013)  
 DATE: 12/15/2017  
 DRAWN BY: [Name]  
 CHECKED BY: [Name]



**CIVIL ENGINEERING + SURVEYING**

**CIR**

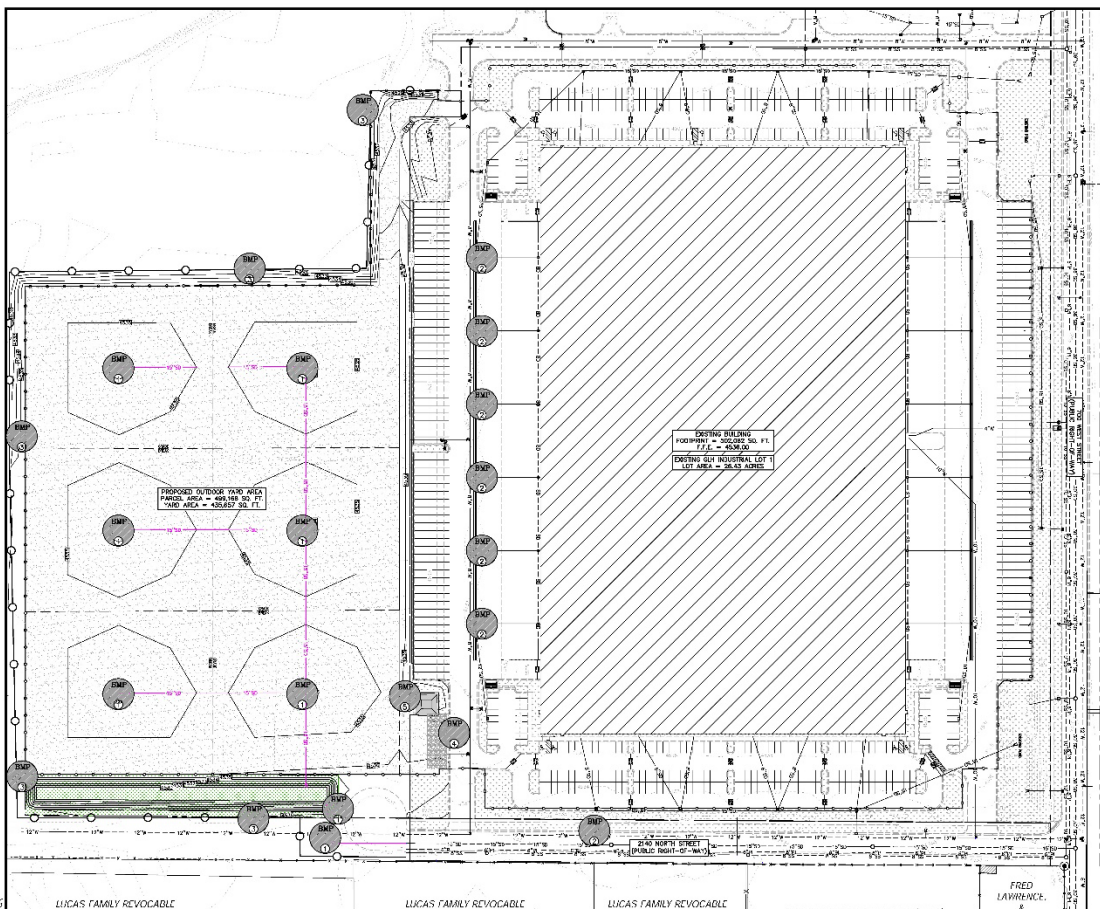
107 S. SOUTH REGISTRATION AVE., STE. 102  
SPANISH FORK, UT 84660

PROJECT NO. 22-011  
DATE: 12/20/23  
DRAWN BY: J. HARRIS  
CHECKED BY: J. HARRIS

GLH INDUSTRIAL PARK LOT 1 - TI YARD  
2261 NORTH 700 WEST, SPANISH FORK, UT 84660

**C3.0**

EROSION CONTROL PLAN (SWPPP)



**CIVIL ENGINEERING + SURVEYING**

**CIR**

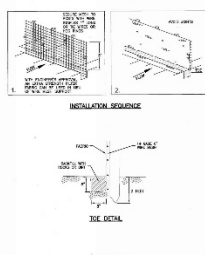
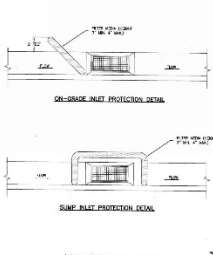
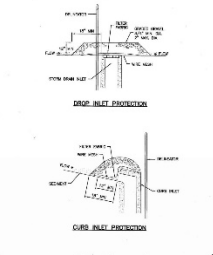
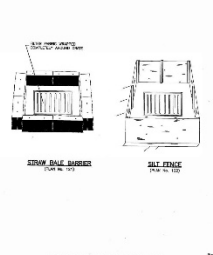
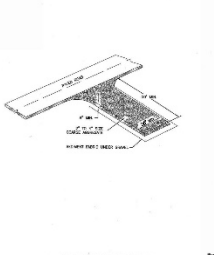
107 S. SOUTH REGISTRATION AVE., STE. 102  
SPANISH FORK, UT 84660

PROJECT NO. 22-011  
DATE: 12/20/23  
DRAWN BY: J. HARRIS  
CHECKED BY: J. HARRIS

GLH INDUSTRIAL PARK LOT 1 - TI YARD  
2261 NORTH 700 WEST, SPANISH FORK, UT 84660

**C4.0**

EROSION CONTROL PLAN (SWPPP)

122 Silt Fence	124 Inlet protection - gravel sack	124 Inlet protection - gravel	124 Inlet protection - fence or straw bale	126 Stabilized roadway entrance
<p><b>GENERAL</b></p> <ol style="list-style-type: none"> <li>Provide a silt fence to prevent sediment from leaving the construction site.</li> <li>Install silt fence in a trench 6 inches deep and 12 inches wide.</li> <li>Use a minimum of 100 lbs of silt fence per 100 lineal feet.</li> <li>Install silt fence in a trench 6 inches deep and 12 inches wide.</li> <li>Use a minimum of 100 lbs of silt fence per 100 lineal feet.</li> </ol> <p><b>PRODUCT</b> (Not listed)</p> <p><b>INSTALLATION</b></p> <ol style="list-style-type: none"> <li>Install silt fence in a trench 6 inches deep and 12 inches wide.</li> <li>Use a minimum of 100 lbs of silt fence per 100 lineal feet.</li> <li>Install silt fence in a trench 6 inches deep and 12 inches wide.</li> <li>Use a minimum of 100 lbs of silt fence per 100 lineal feet.</li> </ol> <p><b>MAINTENANCE</b></p> <ol style="list-style-type: none"> <li>Inspect silt fence daily for damage.</li> <li>Repair or replace damaged silt fence immediately.</li> <li>Remove 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 <p><b>INSTALLATION SEQUENCE</b></p> <p><b>DETAIL</b></p>	 <p><b>ON-ROAD INLET PROTECTION DETAIL</b></p> <p><b>SILT FENCE DETAIL</b></p>	 <p><b>INLET PROTECTION</b></p> <p><b>ON-ROAD INLET PROTECTION</b></p> <p><b>INLET PROTECTION</b></p>	 <p><b>INLET PROTECTION</b></p> <p><b>INLET PROTECTION</b></p>	 <p><b>STABILIZED ROADWAY ENTRANCE</b></p>

**CIVIL ENGINEERING & SURVEYING**  
1075 S. SOUTH MAIN ST. SUITE 100  
SPANISH FORK, UT 84660

**GLF INDUSTRIAL PARK LOT 1 - 11 YARD**  
2811 NORTH 700 WEST, SPANISH FORK, UTAH 84660

**EROSION CONTROL DETAIL SHEET**

**SCALE: 1" = 10'-0"**

**DATE: 10/20/2023**

**PROJECT NO: 23-001**

**CLIENT: GLF**

**DESIGNER: CIVIL ENGINEERING & SURVEYING**

**CHECKED: [Signature]**

**DATE: 10/20/2023**

**EXISTING BLDG - NOT IN SCOPE**

**EXISTING BUILDING BY AREA = 2,500 SQ. FT.**

**PROPOSED YARD BY AREA = 14,250 SQ. FT.**

**ELECTRICAL SITE PARCEL PHOTOMETRIC - FOR REFERENCE ONLY**

**SCALE: 1" = 40'-0"**

**LEGEND**

Symbol	Description
[Symbol]	EXISTING BUILDING
[Symbol]	PROPOSED YARD

**Table**

LINE	DATE	BY	DESCRIPTION
1	10/20/2023	[Name]	ISSUED FOR PERMIT

**Distances**

Location	Station	Area	Dist.	Vol.	Wt.	Cost
Point	1	1.00	1.00	1.00	1.00	1.00
Bottom Point	1	1.00	1.00	1.00	1.00	1.00

**Atlas Best Super Concrete**

**OMEGA MORGAN - TI**  
2811 NORTH 700 WEST  
SPANISH FORK, UT 84660

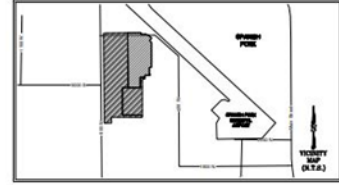
**BOCKY MOUNTAIN**  
1075 S. SOUTH MAIN ST. SUITE 100  
SPANISH FORK, UT 84660

**SALMON DESIGN GROUP**  
**SALMON ELECTRIC**

**ELECTRICAL SITE PARCEL PHOTOMETRIC**

**E801**

**RECORD OF SURVEY  
SIMPLE BOUNDARY ADJUSTMENT  
LOCATED IN THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 8 SOUTH, RANGE 2 EAST,  
SALT LAKE BASE AND MERIDIAN  
SPANISH FORK CITY, UTAH COUNTY, UTAH**



**Legend of Symbols & Abbreviations**

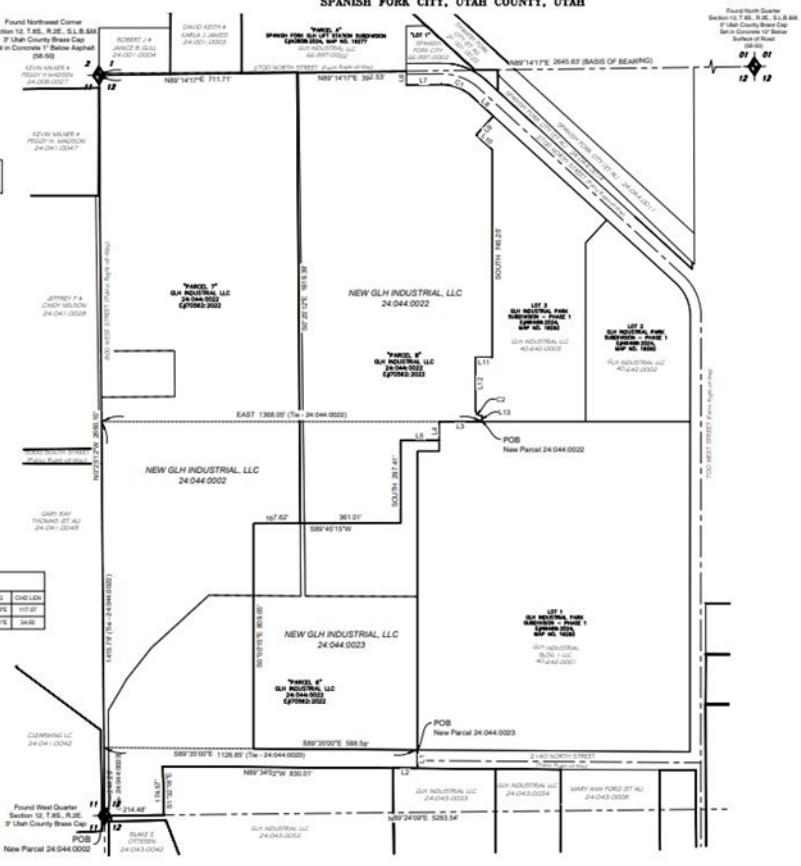
- Boundary Line
- Adjacent Parcel Line
- Road Center Line
- Road Right-of-Way
- Section Line
- Original Parcel Line

**LINE TABLE**

LINE #	LENGTH	BEARING
L1	86.50	S0° 50' 00" W
L2	82.07	N89° 50' 00" W
L3	154.48	N89° 50' 00" W
L4	88.84	S0° 50' 00" W
L5	138.16	N89° 50' 00" W
L6	48.40	S0° 50' 00" W
L7	128.43	N89° 54' 45" E
L8	102.62	S47° 32' 12" E
L9	82.09	S47° 32' 12" E
L10	79.32	S47° 32' 12" E
L11	81.40	N89° 50' 00" W
L12	102.62	S0° 50' 00" W
L13	20.30	S0° 50' 00" W

**CURVE TABLE**

CURVE #	LENGTH	RADIUS	DELTA	CHG BRG	CHG LEN
C1	118.87	158.52	43° 03' 02"	S89° 52' 00" W	110.87
C2	24.42	24.32	90° 00' 00"	S47° 32' 12" E	24.88



**As-Surveyed Legal Description**

The above-described tract of land contains 1,000.00 sq. ft. in area, in 20.00 acres, more or less.

**Survey Narrative**

The purpose of this survey is to adjust the boundaries of Parcel 24-044-0022, 24-044-0023, and 24-044-0024 owned by GLH Industrial LLC, located in the Northwest Quarter of Section 12, Township 8 South, Range 2 East, Salt Lake Base and Meridian.

**Surveyor's Certification**

I, Brian P. Wilcox, certify that I am a Licensed Professional Land Surveyor in the State of Utah, License No. 2202781, in accordance with the Utah Chapter and Professional Land Surveyors Licensing Act. Further, I certify that a survey of the land shown herein is a correct representation of the land surveyed and has been prepared in conformity with the minimum standards and requirements of the law.



**CR**  
CITY RECORDERS

Prepared for:  
**GLH INDUSTRIAL, LLC**

Parcel No.:  
**24-044-0022**

Parcel Location:  
**24-044-0022**

Prepared By:  
**Brian P. Wilcox**

Date:  
**12/22/2023**

Checked By:  
**Brian P. Wilcox**

Sheet No. 1 of 1

Project No. 24-044-0022

File Name:

# EXHIBIT 3

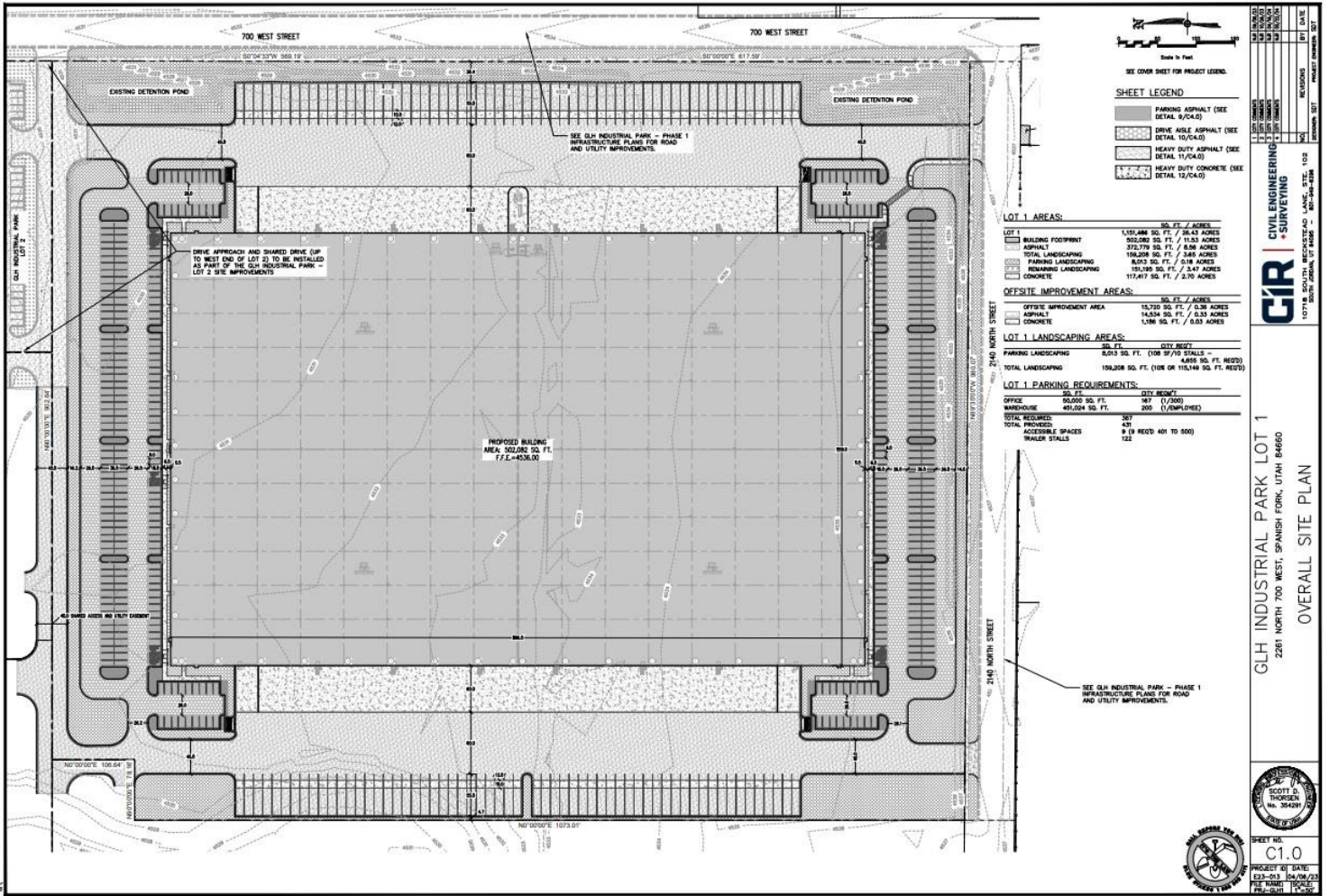
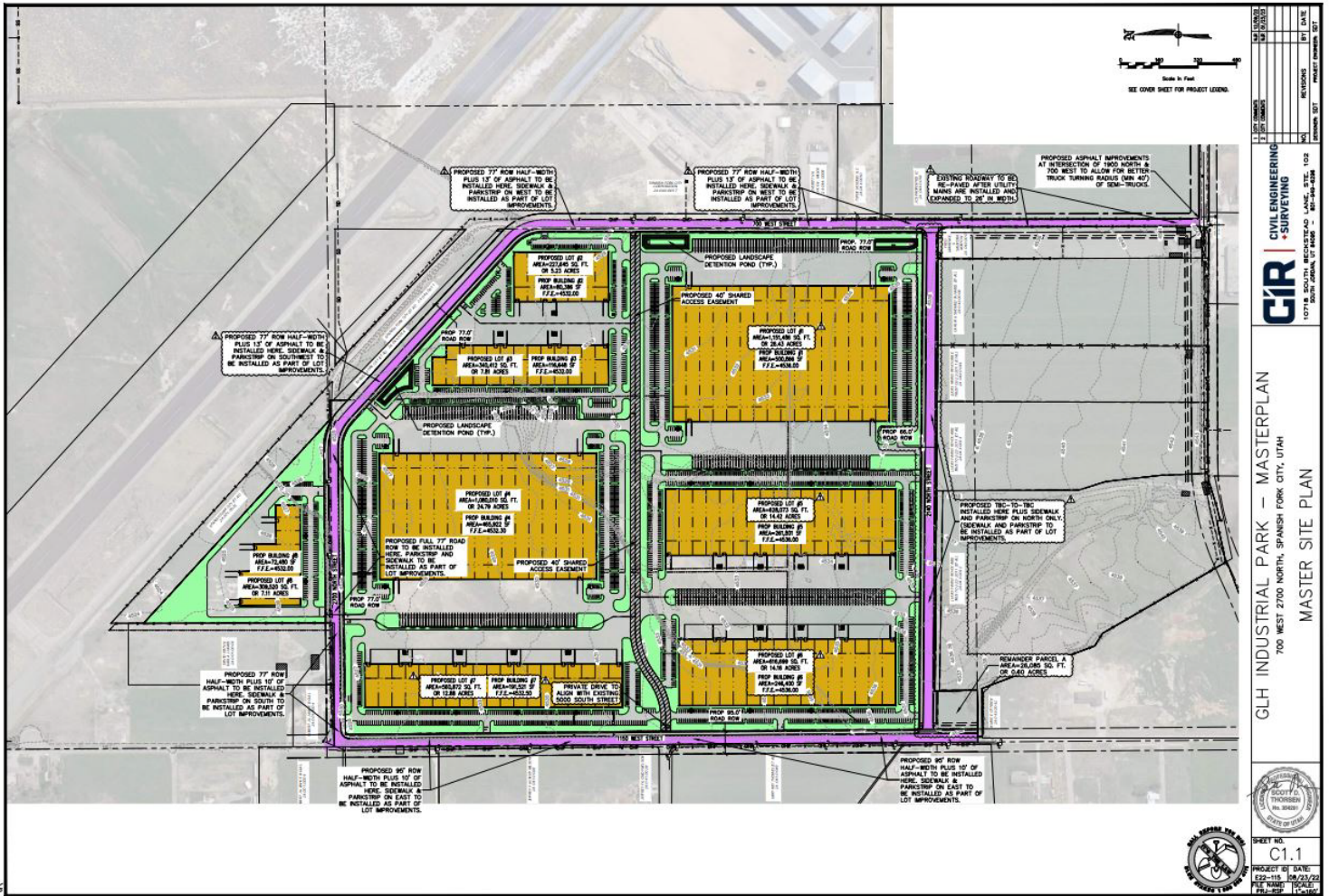


EXHIBIT 4



DATE	10/15/2024
PROJECT	GLH INDUSTRIAL PARK - MASTERPLAN
CLIENT	GLH INDUSTRIAL PARK
LOCATION	700 WEST 2700 NORTH, SPANISH FORK CITY, UTAH
SCALE	AS SHOWN
DRAWN BY	W. J. HARRIS
CHECKED BY	J. L. HARRIS
DATE	10/15/2024

**CIR** CIVIL ENGINEERING & SURVEYING  
 1071 S. SOUTH WEST BLVD. SUITE 102  
 SPANISH FORK, UT 84660

GLH INDUSTRIAL PARK - MASTERPLAN  
 700 WEST 2700 NORTH, SPANISH FORK CITY, UTAH  
 MASTER SITE PLAN

DOUGLAS HARRIS  
 STATE OF UTAH  
 LICENSE NO. 102

SHEET NO. C.1.1  
 PROJECT & DATE: GLH-118 10/15/2024  
 TITLE: MASTER SITE PLAN  
 SCALE: AS SHOWN

## EXHIBIT 5

When recorded, return to:  
Public Works Director  
Spanish Fork City  
40 S. Main Street  
Spanish Fork, Utah 84660

Affecting Parcel Nos. 22:044:023,  
22:044:0022, 22:044:0002, 24:043:0052, and  
24:043:0033

### **Development Restriction and Future Road Easement Agreement**

This Development Restriction and Future Road Easement Agreement, hereafter referred to as **The Agreement**, is entered into as of March 31, 2026, by and between the Spanish Fork City, Utah, a Utah municipal corporation, hereafter referred to as **The City**, and **GLH Industrial, LLC**, a Utah limited liability company, hereafter referred to as **The Developer**. It is acknowledged that **The Developer** and **The Owner** are the same.

#### **Recitals:**

- A.** The Owner is the fee title owner of certain real property known as the GLH Industrial Park, located at 2261 North and 700 West in Spanish Fork, Utah County, Utah, and is a planned industrial park, hereafter referred to as **The Property**, and as more particularly described in Exhibit A, attached hereto.
- B.** The Developer has built the first building in the planned industrial park, hereafter referred to as **The Existing Building**.
- C.** The City and The Developer have identified the need for a future public roadway, hereafter referred to as **The Future Road**, which is described as a road to be known as 2140 North and which will connect 700 West and 1150 West, Spanish Fork, Utah, and as depicted on Exhibit A.

- D. The parties desire to establish conditions governing the future development of The Property and to preserve the Future Road corridor for public use.

**Agreement:**

Now, Therefore, for good and valuable consideration, the receipt and sufficiency thereof is acknowledged, the City and the Developer agree as follows:

**1. Development Restrictions**

The Developer may not build any building west of The Existing Building (defined as Parcel Nos. 22:044:023, 22:044:0022, 22:044:0002, 24:043:0052, and 24:043:0033), and no vertical construction may commence for any portion of The Property located on those parcels unless and until The Future Road has been constructed or adequate provisions for its construction have been approved by The City, in its sole judgment. The Future Road shall be constructed to the then-current adopted City Construction Standards.

**2. Grant of Easement**

- a. The Developer hereby grants to the City a non-exclusive easement over, under, and across the Future Road corridor for the purpose of installing, maintaining, repairing, and replacing a public roadway, municipal utilities, and related improvements as The City deems necessary. The City may exercise its rights under this agreement at such times as it determines, in its sole judgement, that construction of The Future Road is necessary. Until The Future Road is constructed, The Developer/Owner may continue to use The Future Road corridor in a manner that does not interfere with the intended Future Road.
- b. Any construction of The Future Road will be the responsibility of The Developer upon the development of any additional building construction west of The Existing Building (Parcel Nos. 22:044:023, 22:044:0022, 22:044:0002, 24:043:0052, and 24:043:0033). Should the City determine, in its own judgement, that it wishes to build The Future Road, and this is not triggered by The Developer building an additional buildings West of The Existing Building, then the cost and responsibility of construction shall be with The City. The City shall be entitled to reimbursement, including from the Developer, through following its established pioneering agreement procedure.

- c. It is acknowledged and agreed between the parties that The Agreement shall run with the land and shall be binding upon and inure to the benefit of the parties and their respective heirs, successors, and assigns.

**3. Recording**

Upon execution of this agreement by the parties, The Developer shall immediately commence the process to have The Agreement recorded in the official records of Utah County, Utah, and shall constitute notice to all future owners of the property. In the event of a breach of this agreement, the parties shall have all remedies available at law or in equity, including but not limited to injunctive relief to enforce the provisions herein.

**4. Miscellaneous**

The Agreement shall be governed by the laws of the State of Utah. The Agreement constitutes the entire agreement between the parties. The Agreement may only be amended in writing signed by both parties.

In Witness Whereof, the parties have executed this agreement as of the date of the first written above.

**The City**

By: \_\_\_\_\_  
MIKE MENDENHALL, Mayor  
Spanish Fork City, Utah  
Date: \_\_\_\_\_

**The Developer**

By: \_\_\_\_\_  
Paul Ritchie, Manager  
GLH Industrial, LLC  
Date: \_\_\_\_\_

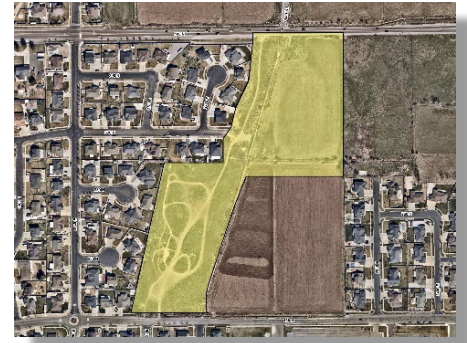
ATTEST:

\_\_\_\_\_  
Tara Silver, City Recorder





Maple Mountain Plat N Phase 2  
Minor Plat Amendment  
400 North Slant Road  
13.54 acres  
R-1-12 with MPD Overlay  
Low Density Residential  
General Plan Designation



**PROPOSAL**

This proposal involves an adjustment to the road alignment of 320 North Street by lots 38-43. The realignment will shift the road slightly south by approximately 12 feet to match up with the recently installed improvements for the Mellor Subdivision to the east.

The original proposal is for a Master Planned Development with 33 single-family residential lots located at 400 North Slant Road. The Final Plat was approved by the Development Review Committee on January 21, 2026.

The proposal will slightly decrease the lot sizes of lots 41, 42 and 43 while slightly increasing the lot sizes of lots 38, 39 and 40. (Proposed changes in lot square footage include: Lot 38 increasing from 11,758 to 11,837; Lot 39 increasing from 11,758 to 12,409; Lot 40 increasing from 11,758 to 12,758; Lot 41 decreasing from 8,800 to 7,800; Lot 42 decreasing from 8,800 to 8,125; and Lot 43 decreasing from 8,800 to 8,710.)

Some of the key issues to consider are: improvements, landscaping, street tree plan and utilities.

**STAFF RECOMMENDATION**

That the proposed Maple Mountain at Spanish Fork Subdivision Plat N Phase 2 Minor Plat Amendment be approved based on the following findings and subject to the following conditions:

**Findings**

1. That the proposal conforms to the City's General Plan Land Use Designation and Zoning Map.
2. That the proposal is consistent with the Preliminary Plat.

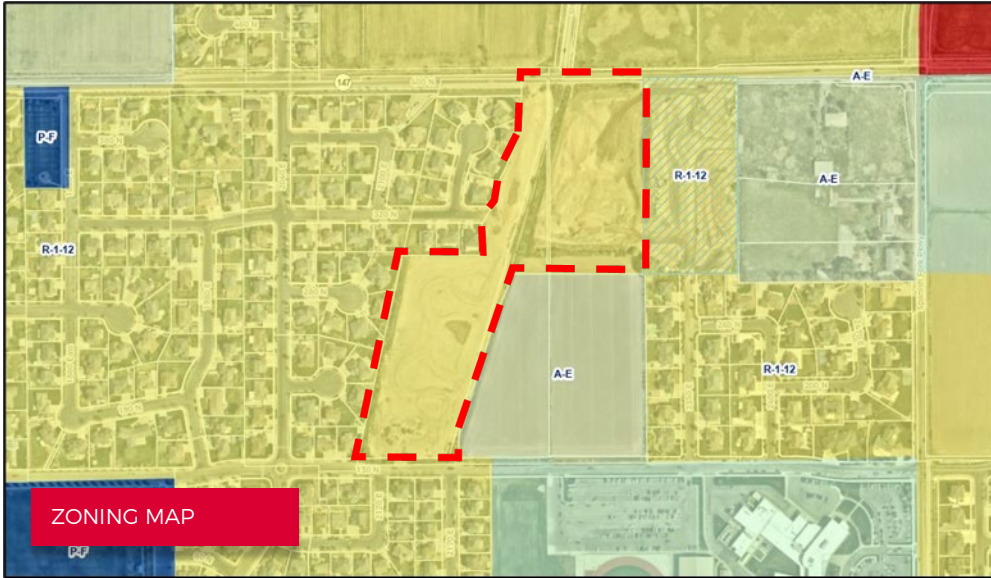
**Conditions**

1. That the Applicant meets the City's development and construction standards, zoning requirements and other applicable City ordinances.
2. That the Applicant addresses all red-line review comments.
3. That the Applicant follows the 400 North landscaping plans as previously approved with the Final Plat.

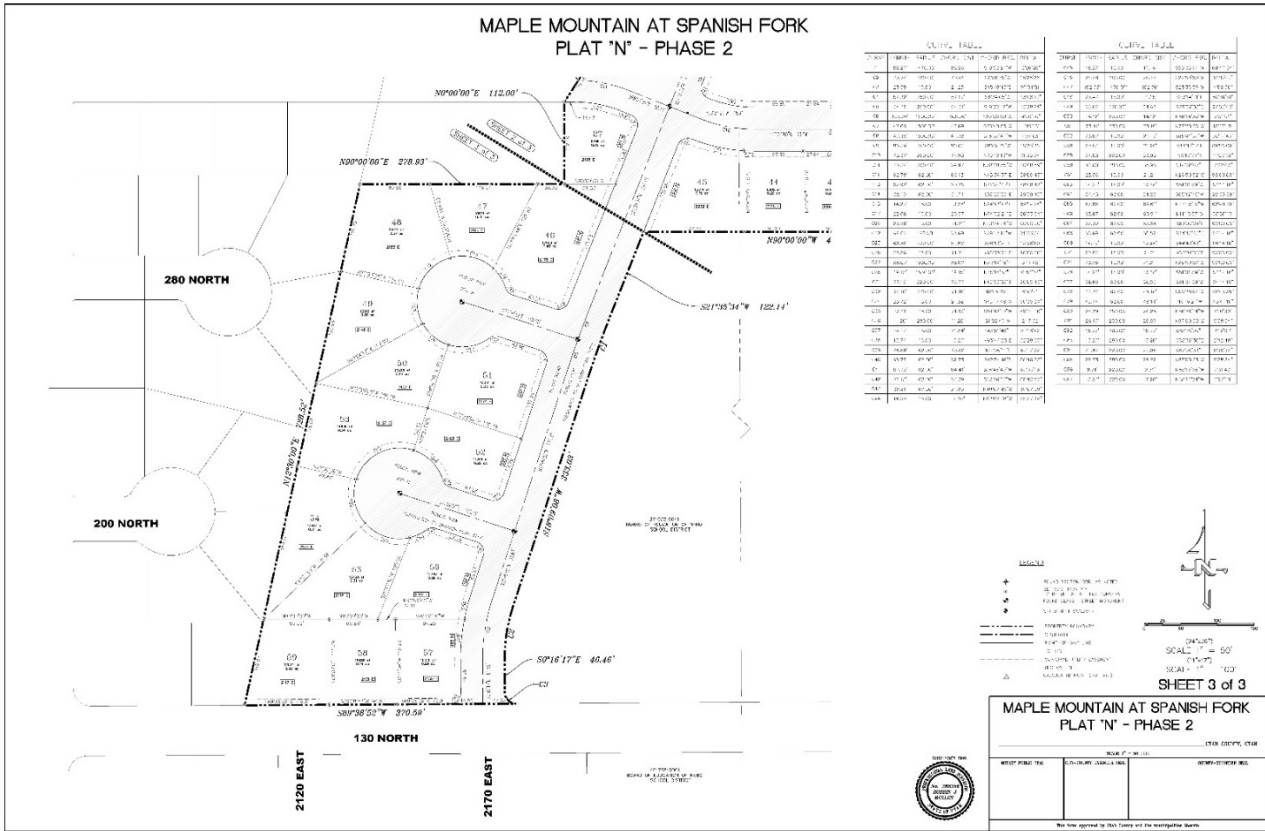
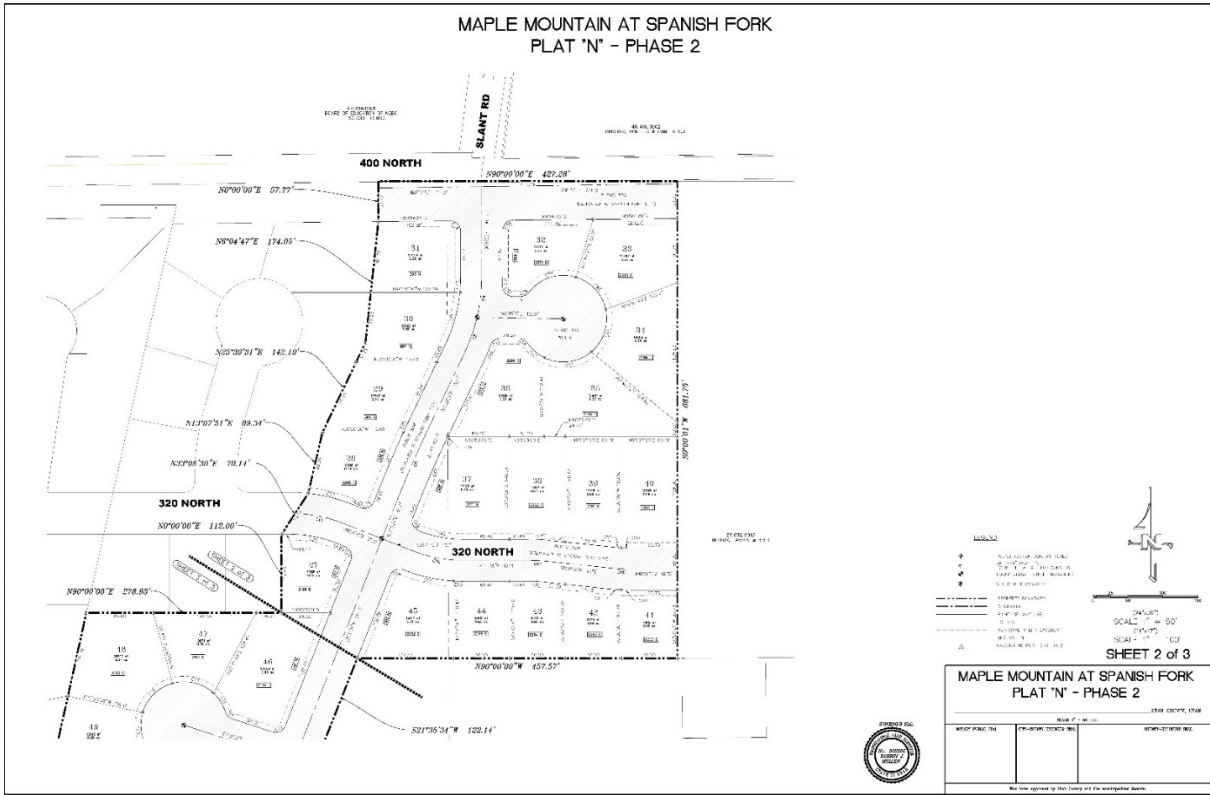
**EXHIBITS**

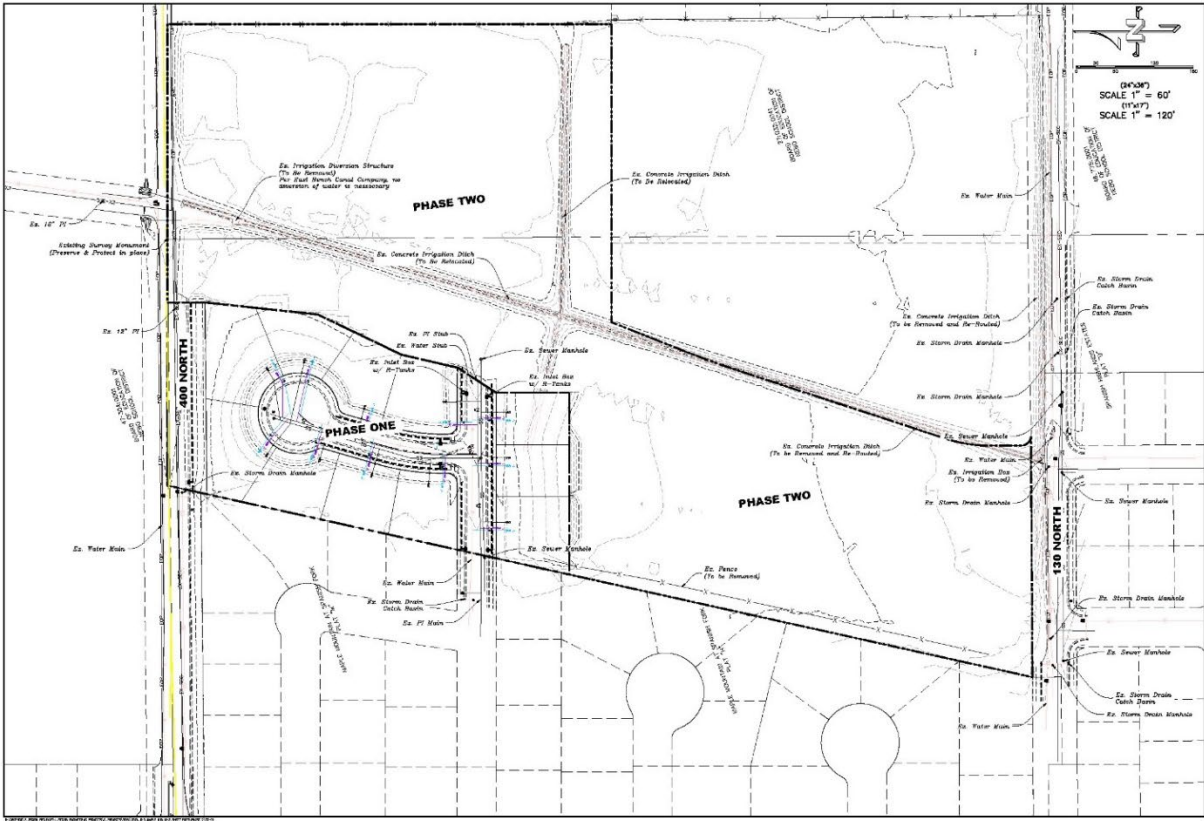
1. Area Maps
2. Civil Plans
3. Landscaping Plans

# EXHIBIT 1







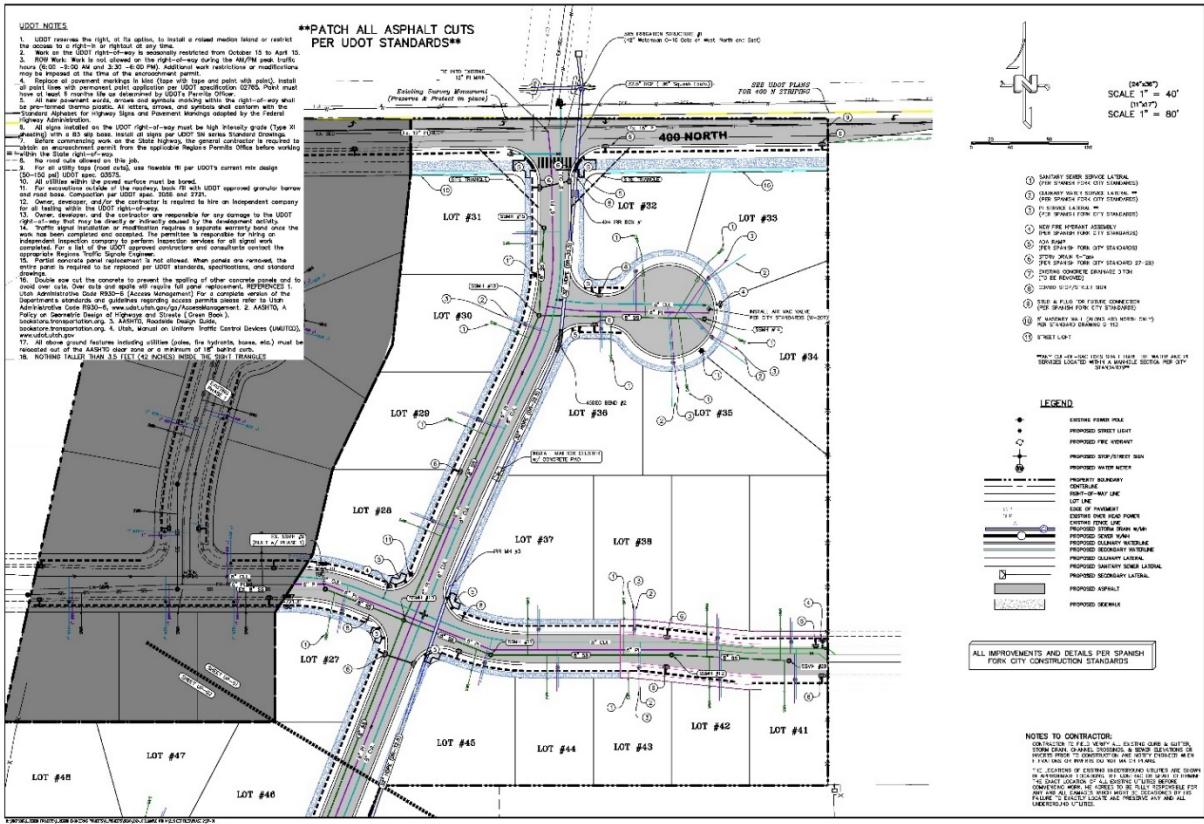


Engineering  
**TCGION** & Surveying  
 1776 N. State St. #110  
 Orem, UT 84407  
 P: 801.225.1215  
 F: 801.225.1216  
 www.tcgion.com

**MAPLE MOUNTAIN AT SPANISH FORK**  
 PHASE 1  
 1776 N. State St. #110  
 Orem, UT 84407  
 PROJECT # 19-206  
 DATE 12/2020

REVISIONS:

EX-01



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**MAPLE MOUNTAIN AT SPANISH FORK**  
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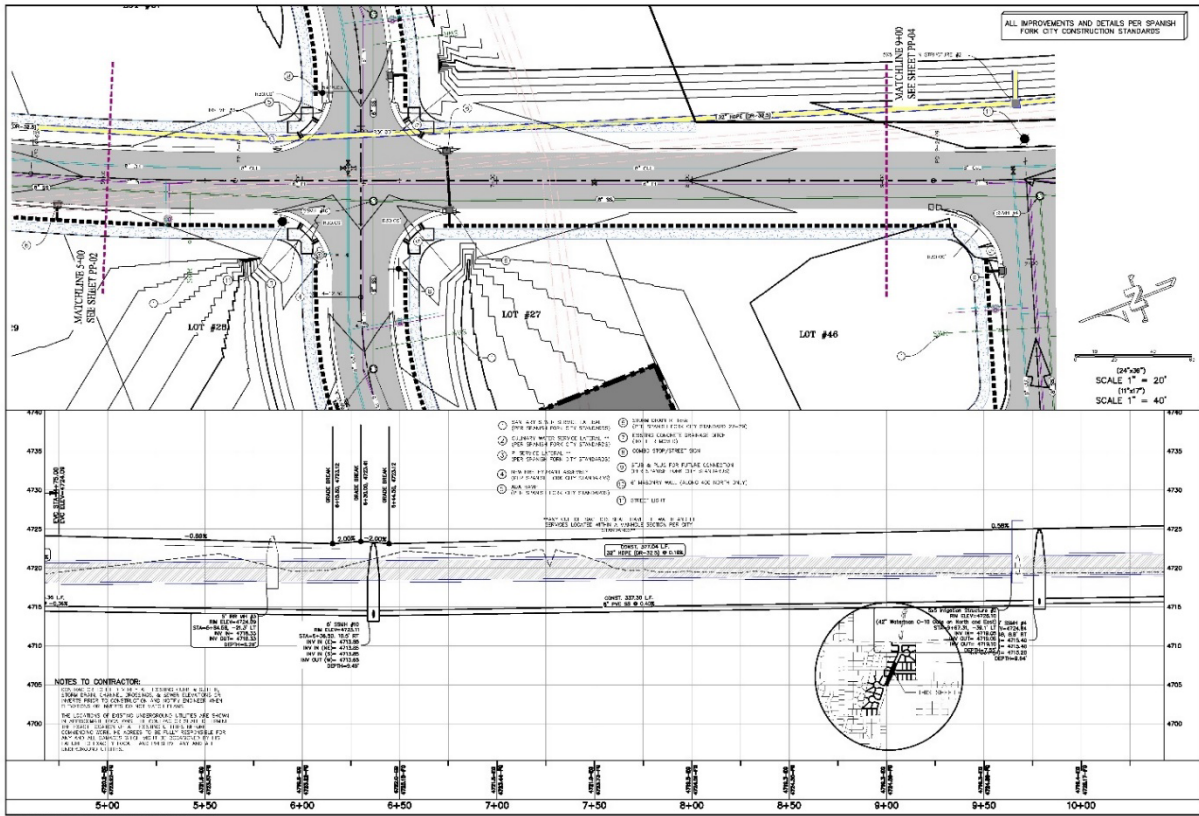
REVISIONS:

EX-01









Engineering  
**region**  
 1776 N. State St. #110  
 Orem, UT 84057  
 P: 801.225.8888  
 F: 801.225.8889

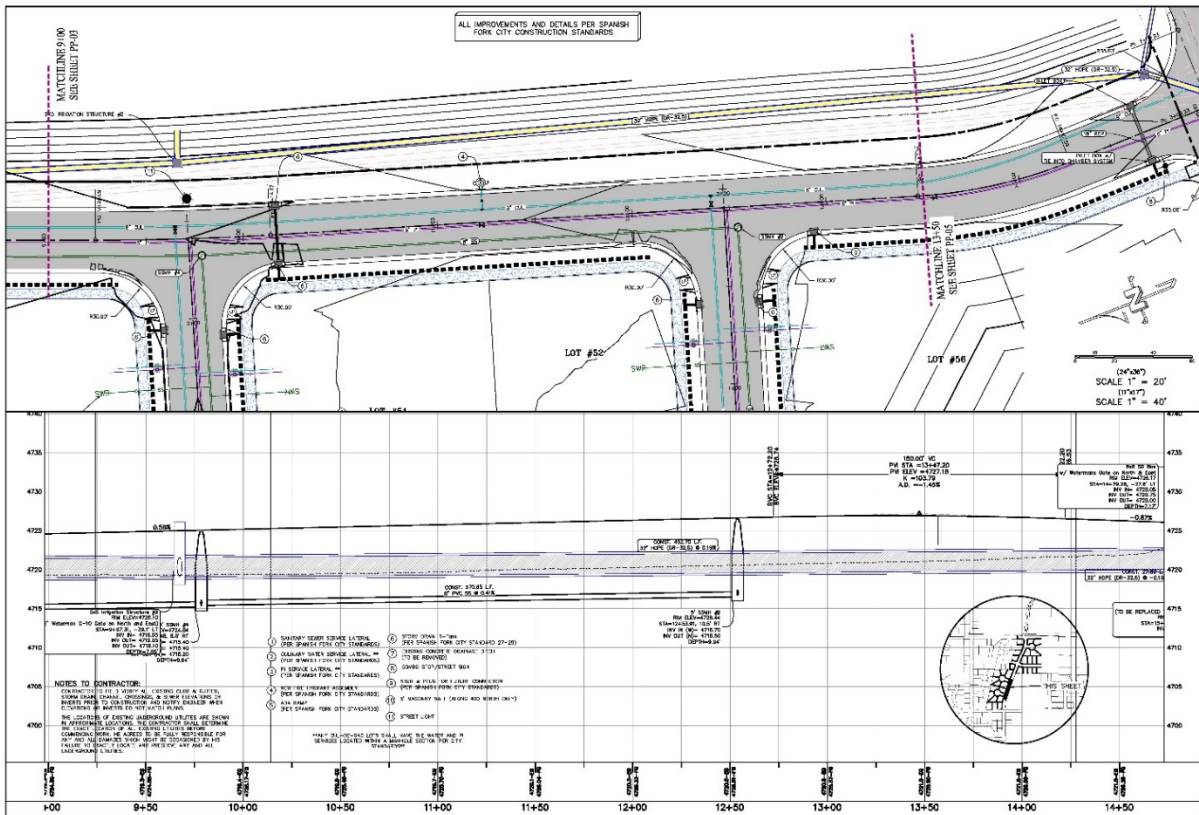
**MAPLE MOUNTAIN AT SPANISH FORK**  
 PLAT "N" - PHASE 2  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 8 SOUTH, T24N, R10E, 4TH MERIDIAN

DATE 3.19.2026  
 PROJECT #

REVISIONS

PLAN & PROFILE

PP-03



Engineering  
**region**  
 1776 N. State St. #110  
 Orem, UT 84057  
 P: 801.225.8888  
 F: 801.225.8889

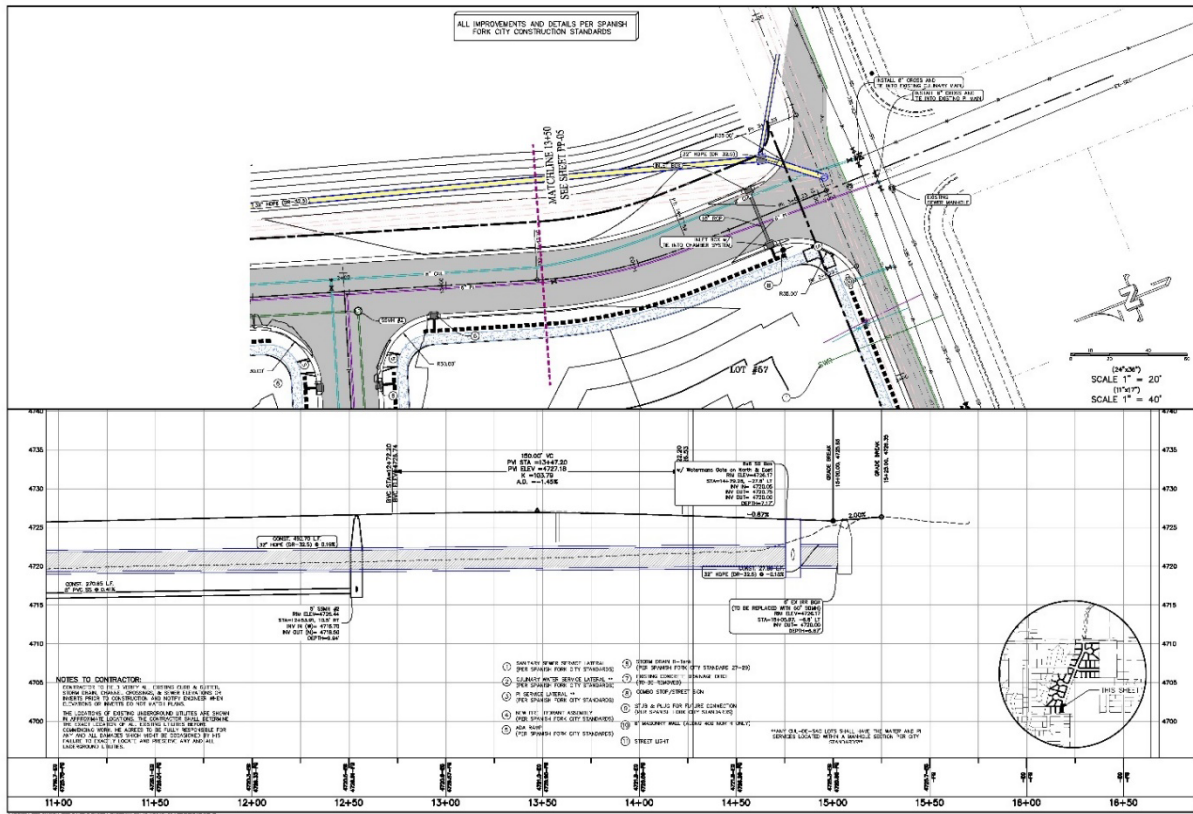
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DATE 3.19.2026  
 PROJECT #

REVISIONS

PLAN & PROFILE

PP-04



NOTES TO CONTRACTOR:  
 1. ALL IMPROVEMENTS AND DETAILS PER SPANISH FORK CITY CONSTRUCTION STANDARDS.  
 2. THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE SHOWN BY THE DASHED LINES. THE LOCATION OF NEW UNDERGROUND UTILITIES ARE SHOWN BY THE SOLID LINES. THE LOCATION OF NEW UNDERGROUND UTILITIES ARE SHOWN BY THE SOLID LINES. THE LOCATION OF NEW UNDERGROUND UTILITIES ARE SHOWN BY THE SOLID LINES.  
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Engineering & Surveying  
**region**  
 1776 N. STATE ST. #110  
 SPANISH FORK, UT 84660  
 P. 801.736.2345  
 www.region-engineering.com

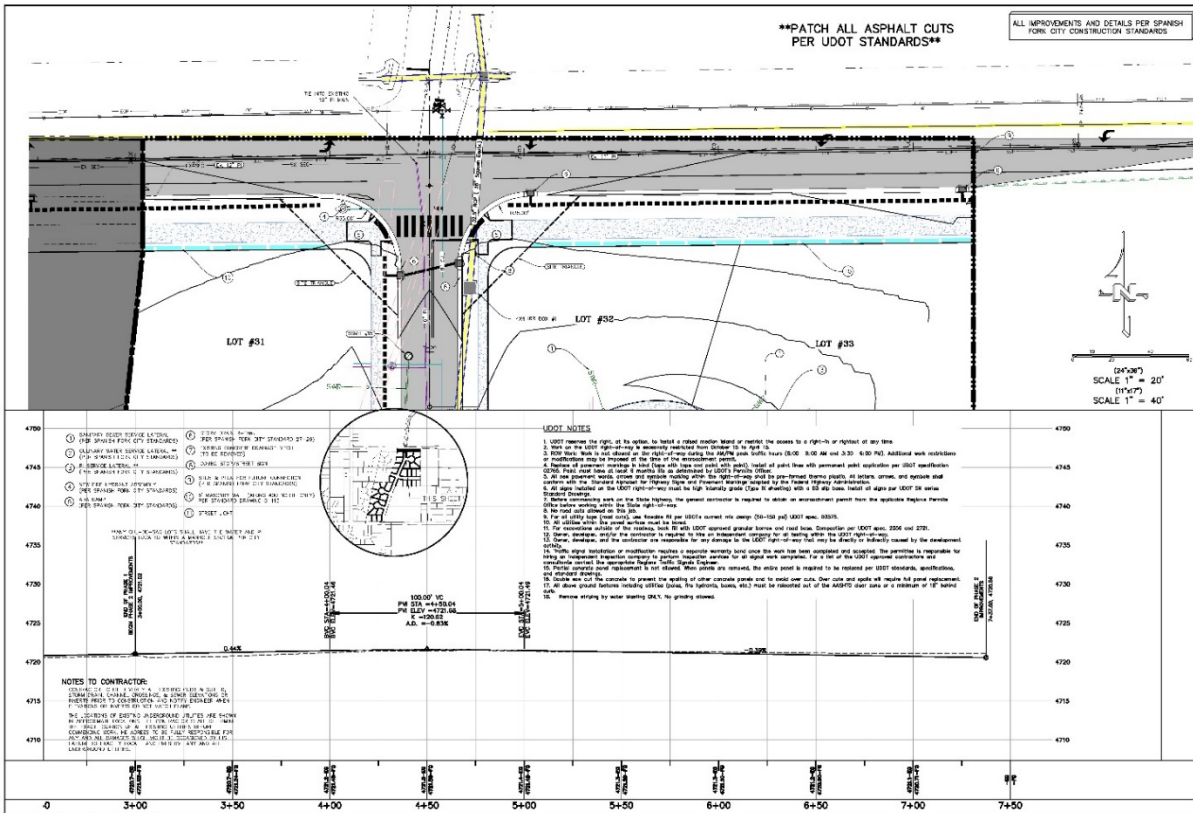
**MAPLE MOUNTAIN AT SPANISH FORK  
 PLAT "N" - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 3 EAST, T14N, R10E AND MERIDIAN

DATE: 3.19.2026  
 PROJECT #

REVISIONS

FOR THE PLAN & PROFILE

PP-05



NOTES TO CONTRACTOR:  
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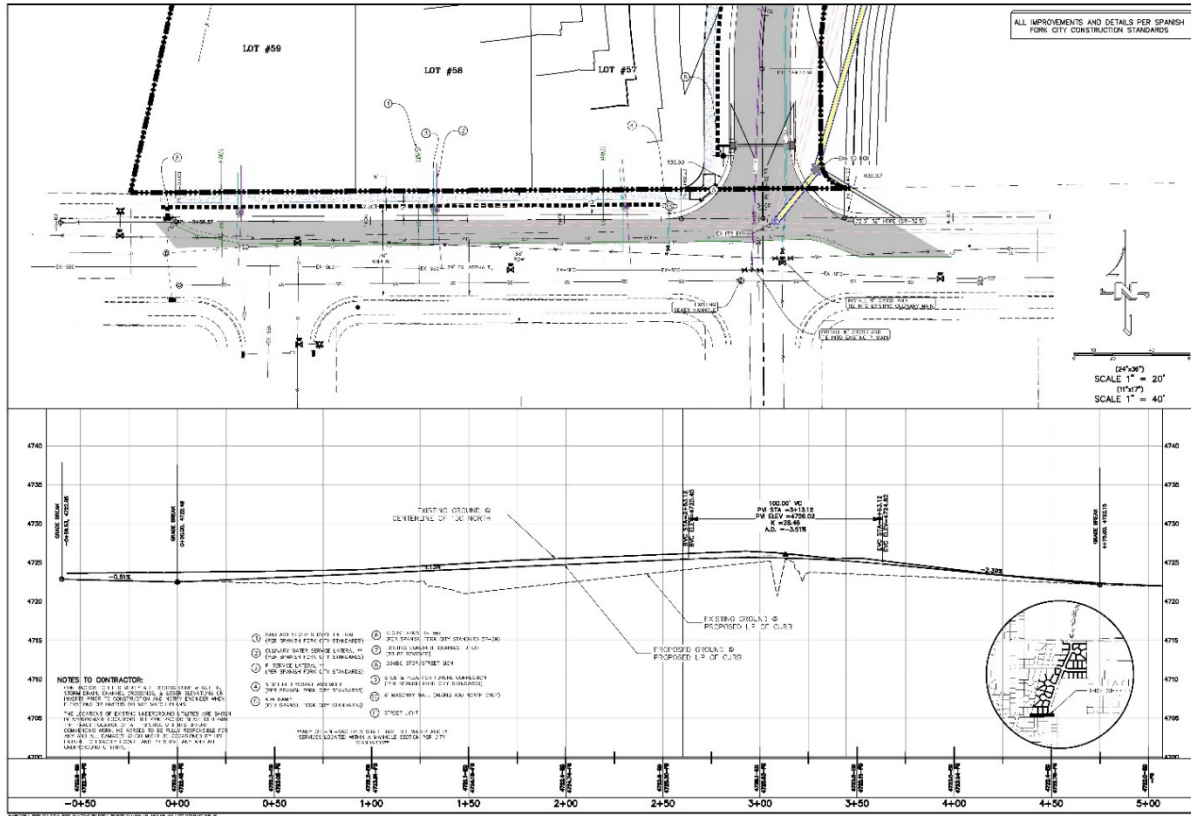
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 PROJECT #

REVISIONS

FOR THE PLAN & PROFILE

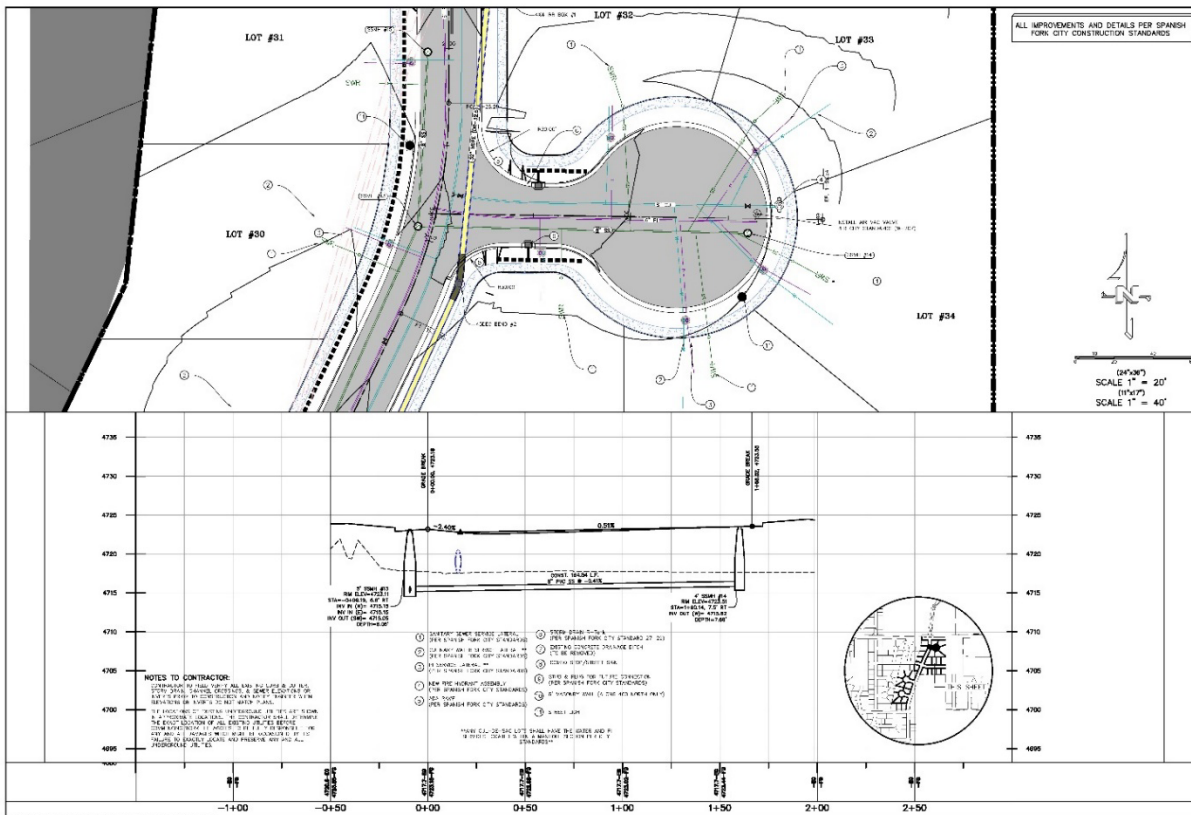
PP-06



region  
 Engineering & Surveying  
 1776 N. Sims St #110  
 P.O. Box 1776  
 Spanish Fork, UT 84603  
 P: 801.734.2245  
 F: 801.734.2245  
 www.regioneng.com

MAPLE MOUNTAIN AT SPANISH FORK  
 PLAT "M" - PHASE 2  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 3 EAST, T12N, R10E, S40N

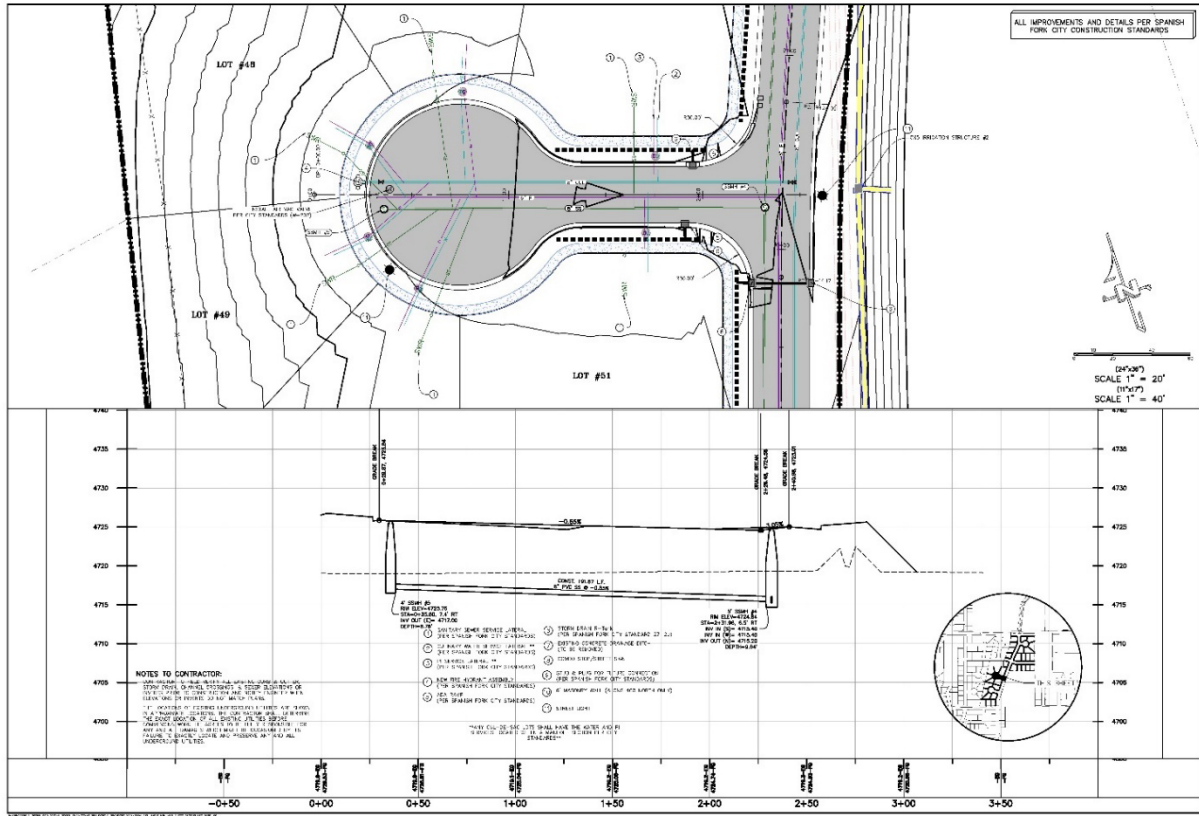
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 PROJECT #  
 REVISIONS  
 PLAN & PROFILE  
 PP-07



region  
 Engineering & Surveying  
 1776 N. Sims St #110  
 P.O. Box 1776  
 Spanish Fork, UT 84603  
 P: 801.734.2245  
 F: 801.734.2245  
 www.regioneng.com

MAPLE MOUNTAIN AT SPANISH FORK  
 PLAT "M" - PHASE 2  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 3 EAST, T12N, R10E, S40N

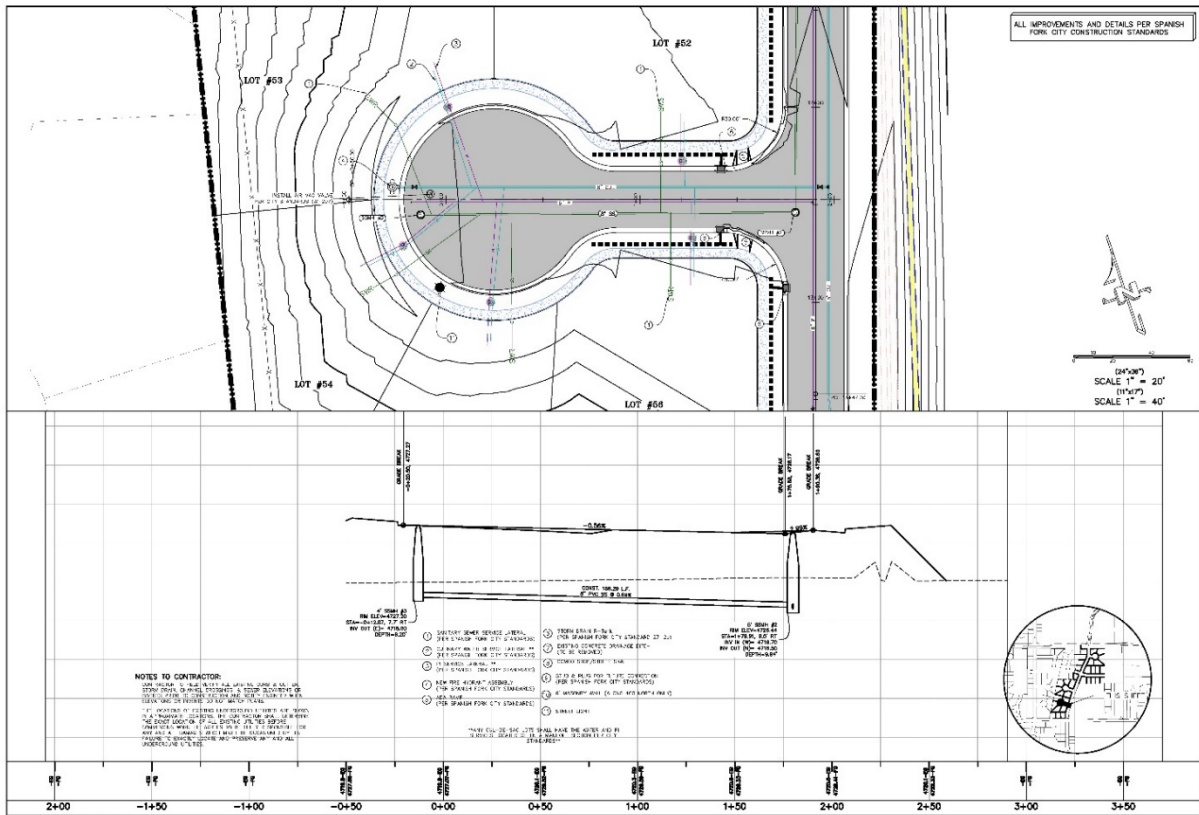
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 PROJECT #  
 REVISIONS  
 PLAN & PROFILE  
 PP-08



Engineering & Surveying  
**region**  
 1776 N. Sims St #410  
 Spanish Fork, UT 84660  
 P: 801.726.2245  
 r@region-engineering.com

**MAPLE MOUNTAIN AT SPANISH FORK PLAT "N" - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF SECTION 36, T12N, R10E, S12E, 4TH MAIN TIER AND MERRIAN RANGES

DATE: 10/20/2025  
 PROJECT #  
 REVISIONS  
 PLAN & PROFILE  
 PP-09

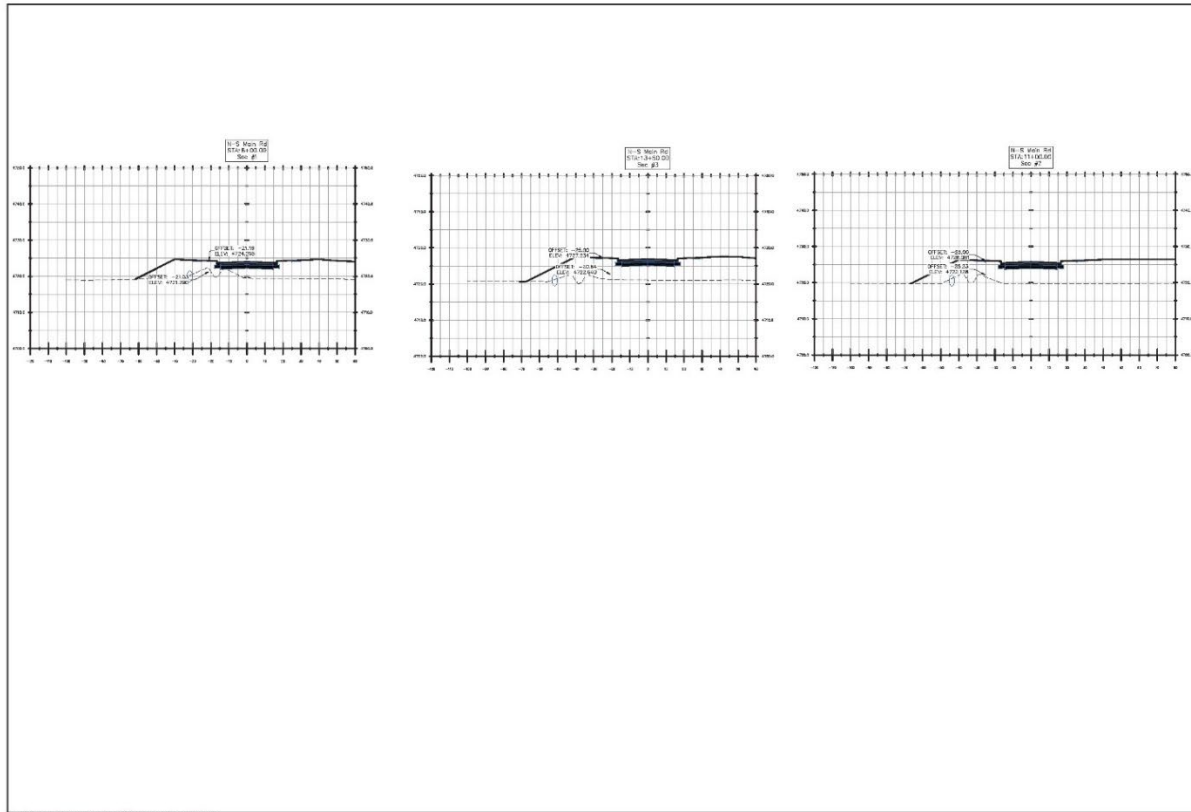


Engineering & Surveying  
**region**  
 1776 N. Sims St #410  
 Spanish Fork, UT 84660  
 P: 801.726.2245  
 r@region-engineering.com

**MAPLE MOUNTAIN AT SPANISH FORK PLAT "N" - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF SECTION 36, T12N, R10E, S12E, 4TH MAIN TIER AND MERRIAN RANGES

DATE: 10/20/2025  
 PROJECT #  
 REVISIONS  
 PLAN & PROFILE  
 PP-10



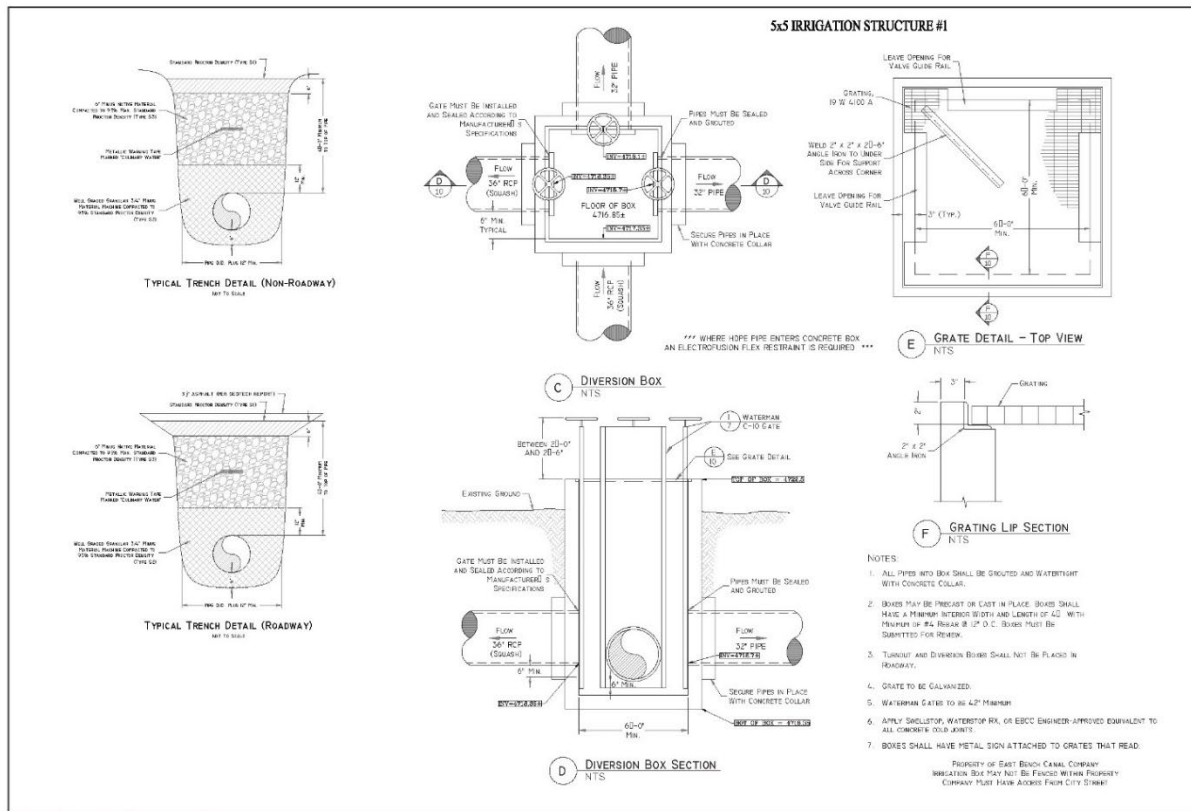


Engineering & Surveying  
**region**  
 1776 N. State St. #110  
 Spanish Fork, UT 84660  
 P: 801.376.2245  
 r@region-ut.com

**MAPLE MOUNTAIN AT SPANISH FORK  
 PLAT "N" - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 8 WEST, T14N, R10E, S22E

DATE: 3/19/2020  
 PROJECT #  
 REVISIONS  
 1  
 2  
 3

ISSUE  
**IRR-03**

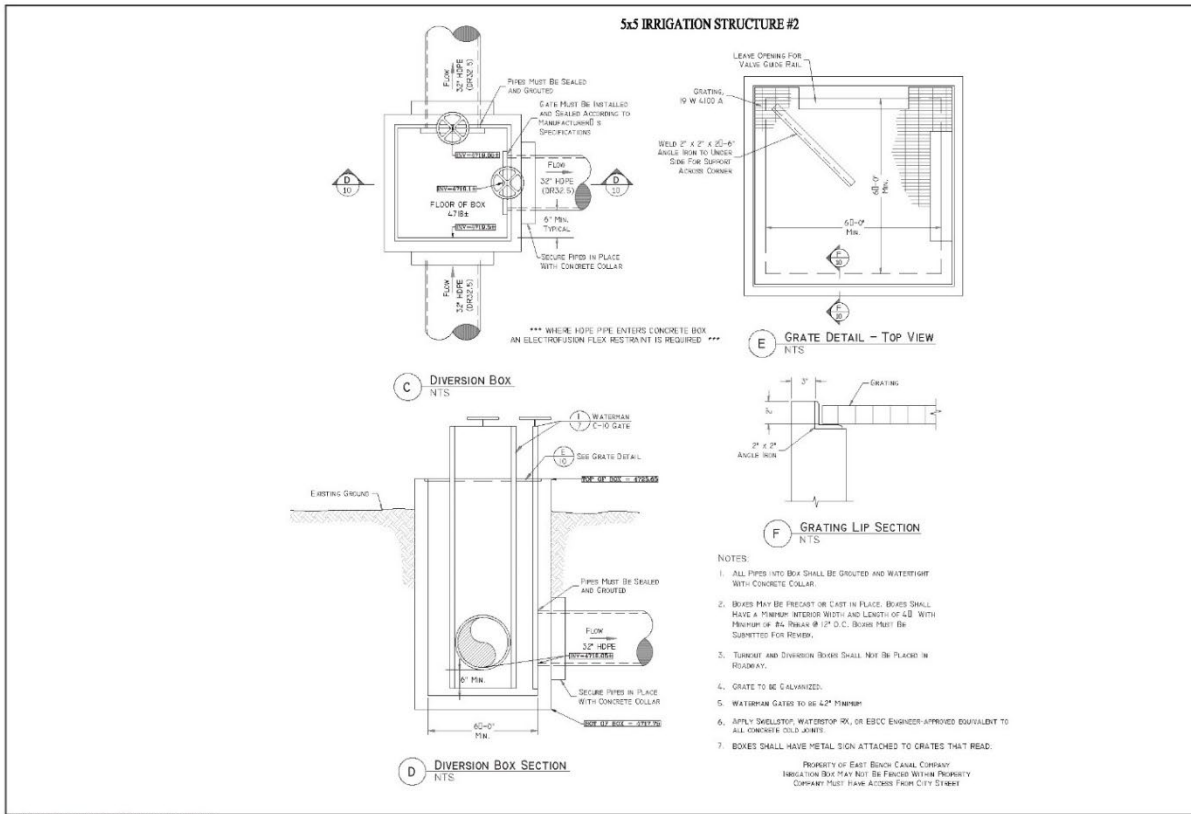


Engineering & Surveying  
**region**  
 1776 N. State St. #110  
 Spanish Fork, UT 84660  
 P: 801.376.2245  
 r@region-ut.com

**MAPLE MOUNTAIN AT SPANISH FORK  
 PLAT "N" - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 8 WEST, T14N, R10E, S22E

DATE: 3/19/2020  
 PROJECT #  
 REVISIONS  
 1  
 2  
 3

ISSUE  
**IRR-04**



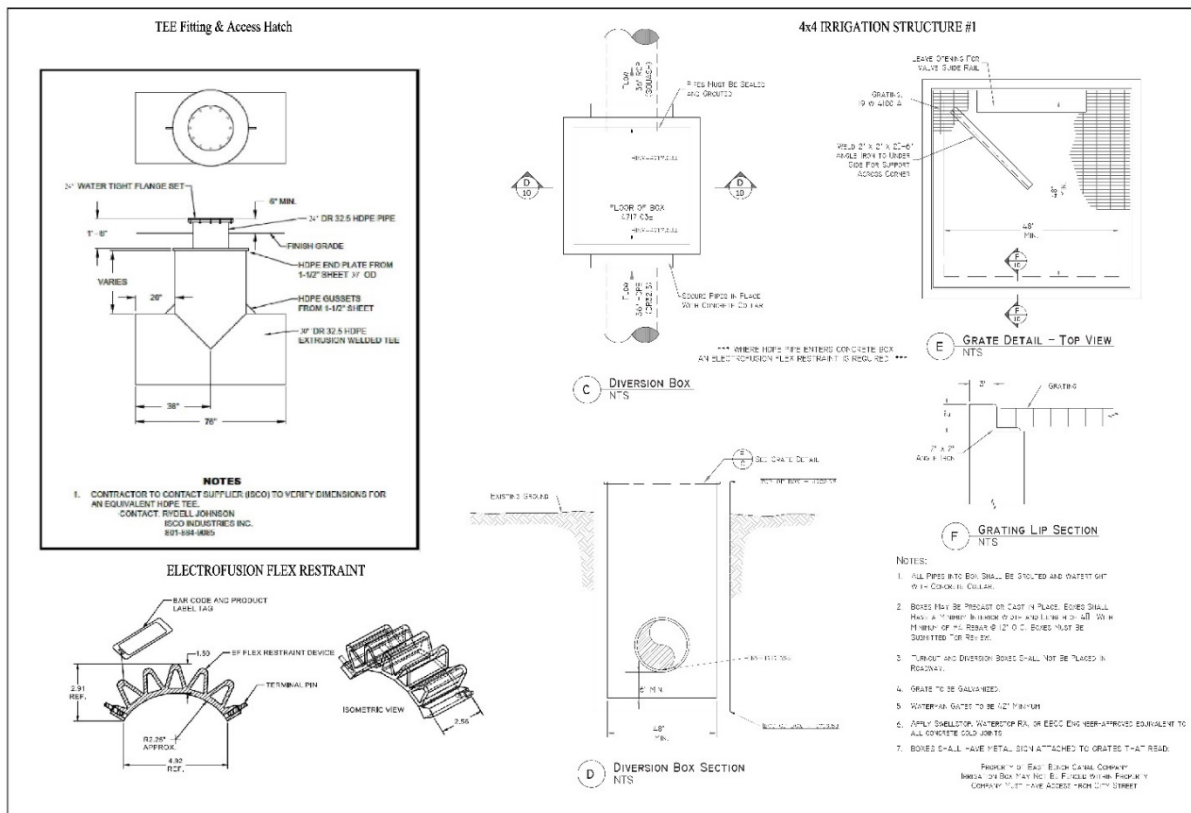
**region**  
Engineering & Surveying  
1776 N. State St. #110  
Orem, UT 84407  
P: 801.225.2245  
F: 801.225.2246  
www.region-engineering.com

**MAPLE MOUNTAIN AT SPANISH FORK  
PLAT "N" - PHASE 2**  
LOCATED IN THE NORTHEAST QUARTER OF  
RANGE 8 EAST, T12N, R12E, T12N AND T12N  
RANGE 8 EAST, T12N, R12E, T12N AND T12N

DATE: 3/19/2024  
PROJECT #  
REVISIONS:  
BY: [initials]  
DATE: [initials]  
DATE: [initials]

SCALE: [initials]  
DATE: [initials]  
DATE: [initials]

**IRR-05**



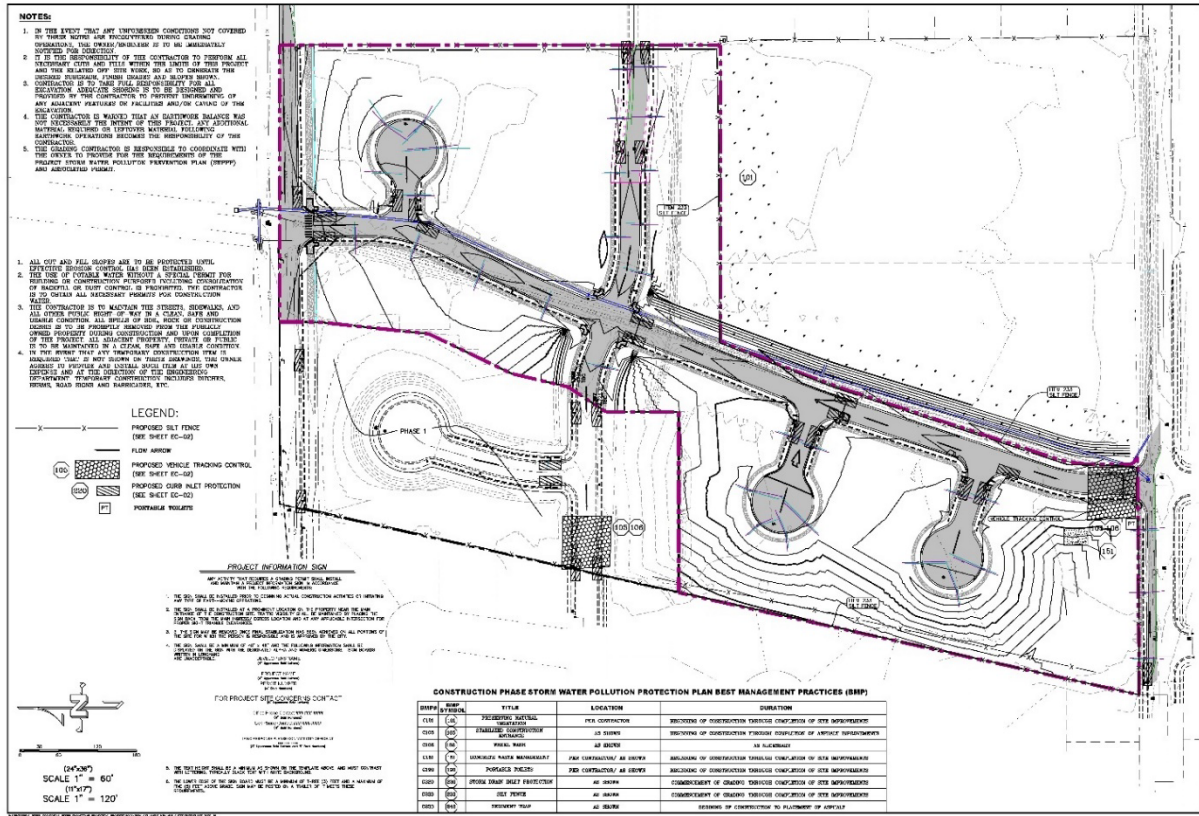
**region**  
Engineering & Surveying  
1776 N. State St. #110  
Orem, UT 84407  
P: 801.225.2245  
F: 801.225.2246  
www.region-engineering.com

**MAPLE MOUNTAIN AT SPANISH FORK  
PLAT "N" - PHASE 2**  
LOCATED IN THE NORTHEAST QUARTER OF  
RANGE 8 EAST, T12N, R12E, T12N AND T12N  
RANGE 8 EAST, T12N, R12E, T12N AND T12N

DATE: 3/19/2024  
PROJECT #  
REVISIONS:  
BY: [initials]  
DATE: [initials]  
DATE: [initials]

SCALE: [initials]  
DATE: [initials]  
DATE: [initials]

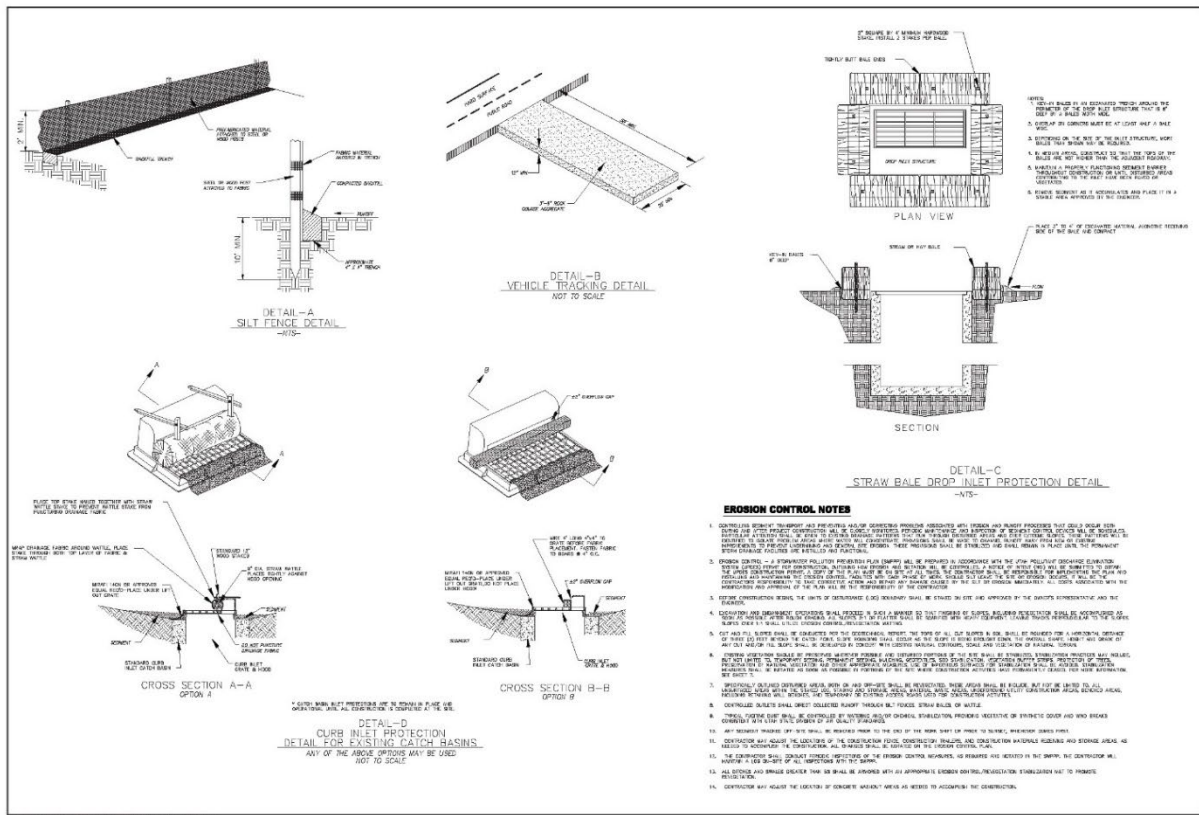
**IRR-06**



Engineering & Surveying  
**region**  
 1776 N. State St. #110  
 P.O. BOX 1276, 2245  
 SPANISH FORK, UT 84660

**MAPLE MOUNTAIN AT SPANISH FORK  
 PLAT 'N' - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 9 EAST, T14N, R12E AND NEARBY

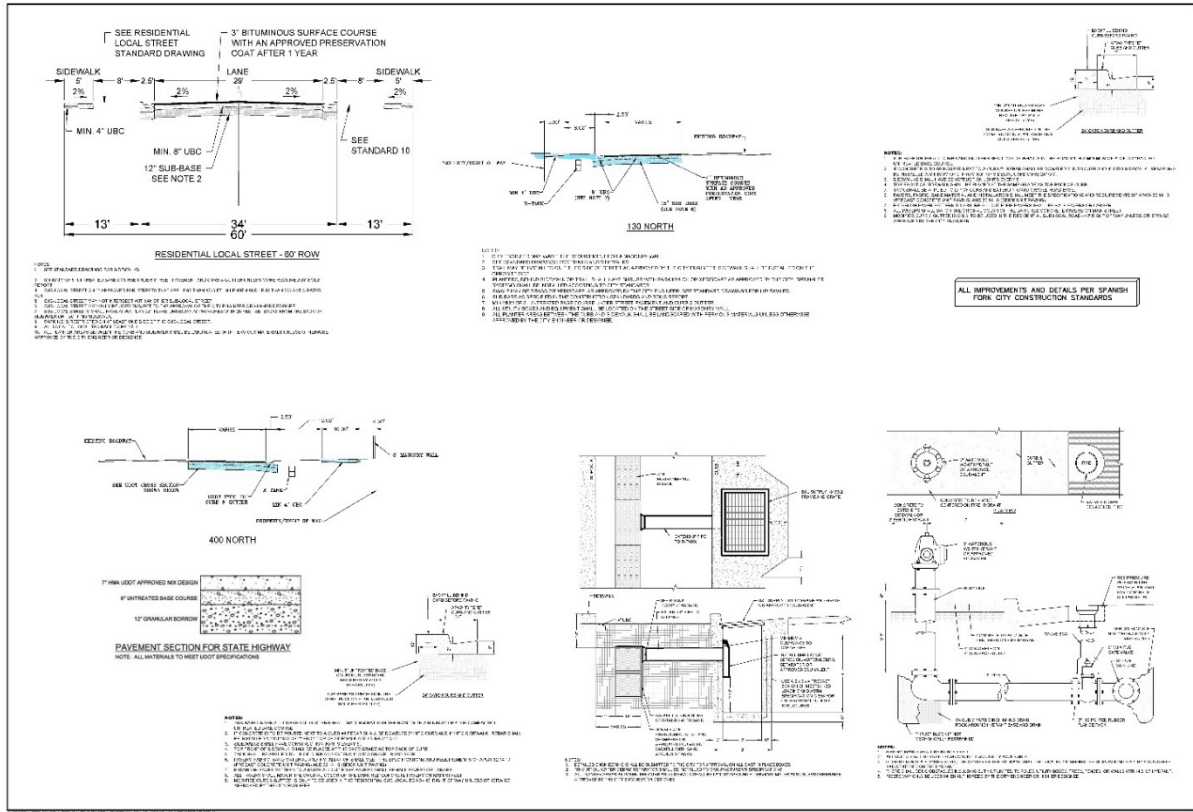
DATE: 3.19.2025  
 PROJECT #  
 REVISIONS  
 EROSION CONTROL PLAN  
 EC-01



Engineering & Surveying  
**region**  
 1776 N. State St. #110  
 P.O. BOX 1276, 2245  
 SPANISH FORK, UT 84660

**MAPLE MOUNTAIN AT SPANISH FORK  
 PLAT 'N' - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 9 EAST, T14N, R12E AND NEARBY

DATE: 3.19.2025  
 PROJECT #  
 REVISIONS  
 EROSION CONTROL PLAN  
 EC-02



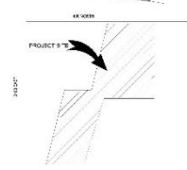
Engineering & Surveying  
**region**  
 1775 N. State St. #110  
 P. 801.776.2245  
 www.regioneng.com

**MAPLE MOUNTAIN AT SPANISH FORK**  
**PLAT "N" - PHASE 2**  
 LOCATED IN THE NORTHEAST QUARTER OF  
 RANGE 3 EAST, TOWN 14 NORTH AND  
 MERIDIAN

DATE 3.19.2026  
 PROJECT #  
 REVISIONS  
 TOTAL SHEETS  
 DT-01

# EXHIBIT 3

## VICINITY MAP



## PLANT LEGEND

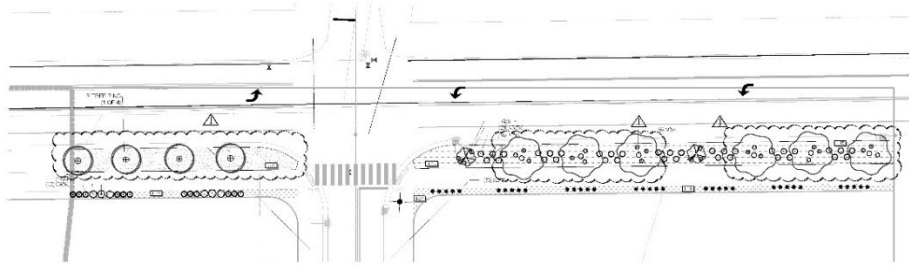
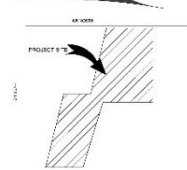
SYMBOL	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	CALL
<b>DECIDUOUS TREES</b>					
(Symbol)	ATL	1	Red-Twig Dogwood Cornus sericea	24.0	24.0
(Symbol)	RSB	1	Red-Flowering Dogwood Cornus florida	24.0	24.0
(Symbol)	ST	1	Star Magnolia Magnolia stellata	24.0	24.0
<b>EVERGREEN TREES</b>					
(Symbol)	SP	1	Spirea Spirea japonica	24.0	24.0
<b>DECIDUOUS SHRUBS</b>					
(Symbol)	DOX	10	Boxwood Buxus sempervirens	2.0	20.0
(Symbol)	STL	1	Star Magnolia Magnolia stellata	24.0	24.0
(Symbol)	PH	10	Phlox Phlox paniculata	2.0	20.0
(Symbol)	ST	1	Star Magnolia Magnolia stellata	24.0	24.0
<b>EVERGREEN SHRUBS</b>					
(Symbol)	SP	10	Spirea Spirea japonica	2.0	20.0
<b>GRASSES</b>					
(Symbol)	CLC	10	Claytonia Claytonia sibirica	1.0	10.0

## SITE MATERIALS LEGEND

SYMBOL	DESCRIPTION	SCALE
(Symbol)	Asphalt Paving	1:200
(Symbol)	Gravel Paving	1:200
(Symbol)	Concrete Paving	1:200
(Symbol)	Grass	1:200

DATE: 11/25/2025	PROJECT NUMBER: UT25014	PLANNING INFORMATION: 811 BILL OF MATERIALS	PROJECT INFORMATION: MAPLE MOUNTAIN PH. 2 400 N. 2400 E. SPANISH FORK, UTAH	DEVELOPER / PROPERTY OWNER / CLIENT: REGION ENGINEERING ATTN: SLAVEN HERRING SHERKING@REGIONENGINEERING.COM	LANDSCAPE ARCHITECT / PLANNER: PKJ DESIGN GROUP 3600 N. TRUMP BLVD, SUITE 102 LEHI, UTAH 84043 (801) 596-2217 www.pkjdesigngroup.com	LICENSE NUMBER: 153220	ISSUED BY: JTA
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## VICINITY MAP



## PLANT LEGEND

SYMBOL	CODE	QTY	BOTANICAL / COMMON NAME	CONT.	CALL
<b>DECIDUOUS TREES</b>					
(Symbol)	ATL	1	Red-Twig Dogwood Cornus sericea	24.0	24.0
(Symbol)	RSB	1	Red-Flowering Dogwood Cornus florida	24.0	24.0
(Symbol)	ST	1	Star Magnolia Magnolia stellata	24.0	24.0
<b>EVERGREEN TREES</b>					
(Symbol)	SP	1	Spirea Spirea japonica	24.0	24.0
<b>DECIDUOUS SHRUBS</b>					
(Symbol)	DOX	10	Boxwood Buxus sempervirens	2.0	20.0
(Symbol)	STL	1	Star Magnolia Magnolia stellata	24.0	24.0
(Symbol)	PH	10	Phlox Phlox paniculata	2.0	20.0
(Symbol)	ST	1	Star Magnolia Magnolia stellata	24.0	24.0
<b>EVERGREEN SHRUBS</b>					
(Symbol)	SP	10	Spirea Spirea japonica	2.0	20.0
<b>GRASSES</b>					
(Symbol)	CLC	10	Claytonia Claytonia sibirica	1.0	10.0

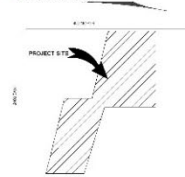
## SITE MATERIALS LEGEND

SYMBOL	DESCRIPTION	SCALE
(Symbol)	Asphalt Paving	1:200
(Symbol)	Gravel Paving	1:200
(Symbol)	Concrete Paving	1:200
(Symbol)	Grass	1:200

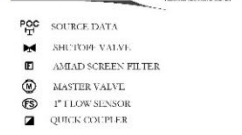
DATE: 11/25/2025	PROJECT NUMBER: UT25014	PLANNING INFORMATION: 811 BILL OF MATERIALS	PROJECT INFORMATION: MAPLE MOUNTAIN PH. 2 400 N. 2400 E. SPANISH FORK, UTAH	DEVELOPER / PROPERTY OWNER / CLIENT: REGION ENGINEERING ATTN: SLAVEN HERRING SHERKING@REGIONENGINEERING.COM	LANDSCAPE ARCHITECT / PLANNER: PKJ DESIGN GROUP 3600 N. TRUMP BLVD, SUITE 102 LEHI, UTAH 84043 (801) 596-2217 www.pkjdesigngroup.com	LICENSE NUMBER: 153220	ISSUED BY: JTA
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VICINITY MAP

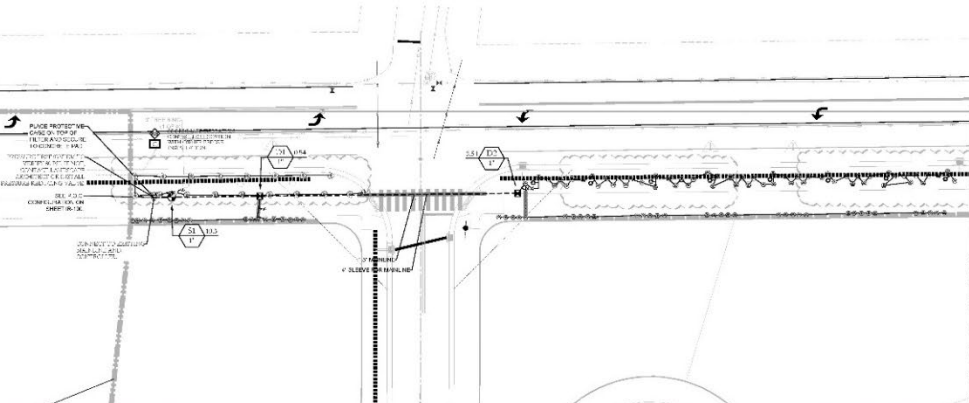


P.O.C. CONFIGURATION



IRRIGATION LEGEND

SYMBOL	MANUFACTURER/DESCRIPTION	QTY	UNIT
1	1" PVC 40' x 1/2" Schedule 40	1	FT
2	1" PVC 40' x 1/2" Schedule 40	1	FT
3	1" PVC 40' x 1/2" Schedule 40	1	FT
4	1" PVC 40' x 1/2" Schedule 40	1	FT
5	1" PVC 40' x 1/2" Schedule 40	1	FT
6	1" PVC 40' x 1/2" Schedule 40	1	FT
7	1" PVC 40' x 1/2" Schedule 40	1	FT
8	1" PVC 40' x 1/2" Schedule 40	1	FT
9	1" PVC 40' x 1/2" Schedule 40	1	FT
10	1" PVC 40' x 1/2" Schedule 40	1	FT
11	1" PVC 40' x 1/2" Schedule 40	1	FT
12	1" PVC 40' x 1/2" Schedule 40	1	FT
13	1" PVC 40' x 1/2" Schedule 40	1	FT
14	1" PVC 40' x 1/2" Schedule 40	1	FT
15	1" PVC 40' x 1/2" Schedule 40	1	FT
16	1" PVC 40' x 1/2" Schedule 40	1	FT
17	1" PVC 40' x 1/2" Schedule 40	1	FT
18	1" PVC 40' x 1/2" Schedule 40	1	FT
19	1" PVC 40' x 1/2" Schedule 40	1	FT
20	1" PVC 40' x 1/2" Schedule 40	1	FT
21	1" PVC 40' x 1/2" Schedule 40	1	FT
22	1" PVC 40' x 1/2" Schedule 40	1	FT
23	1" PVC 40' x 1/2" Schedule 40	1	FT
24	1" PVC 40' x 1/2" Schedule 40	1	FT
25	1" PVC 40' x 1/2" Schedule 40	1	FT
26	1" PVC 40' x 1/2" Schedule 40	1	FT
27	1" PVC 40' x 1/2" Schedule 40	1	FT
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29	1" PVC 40' x 1/2" Schedule 40	1	FT
30	1" PVC 40' x 1/2" Schedule 40	1	FT
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39	1" PVC 40' x 1/2" Schedule 40	1	FT
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41	1" PVC 40' x 1/2" Schedule 40	1	FT
42	1" PVC 40' x 1/2" Schedule 40	1	FT
43	1" PVC 40' x 1/2" Schedule 40	1	FT
44	1" PVC 40' x 1/2" Schedule 40	1	FT
45	1" PVC 40' x 1/2" Schedule 40	1	FT
46	1" PVC 40' x 1/2" Schedule 40	1	FT
47	1" PVC 40' x 1/2" Schedule 40	1	FT
48	1" PVC 40' x 1/2" Schedule 40	1	FT
49	1" PVC 40' x 1/2" Schedule 40	1	FT
50	1" PVC 40' x 1/2" Schedule 40	1	FT



NO.	REVISION	DATE	DESCRIPTION
1	1	11/23/2025UT25014	811 CALL SHEET UT/UT1 1-800-882-4111
2	2		
3	3		
4	4		
5	5		
6	6		
7	7		



**MAPLE MOUNTAIN PH. 2**  
400 N. 2400 E.  
SPANISH FORK, UTAH

811 CALL SHEET UT/UT1 1-800-882-4111

RIEHL ENGINEERING  
ATT: SHAWN HERRING  
SHAWN@RIEHL.COM

PKJ DESIGN GROUP  
IRRIGATION OVERALL PLAN  
CITY PERMIT SET  
IR-100

11/23/2025  
UT25014

811 CALL SHEET UT/UT1 1-800-882-4111

MAPLE MOUNTAIN PH. 2  
400 N. 2400 E.  
SPANISH FORK, UTAH

RIEHL ENGINEERING  
ATT: SHAWN HERRING  
SHAWN@RIEHL.COM

PKJ DESIGN GROUP  
IRRIGATION OVERALL PLAN  
CITY PERMIT SET  
IR-100

IRRIGATION PLAN SPECIFICATIONS

1. GENERAL NOTES: All work shall be in accordance with the Utah State Construction Code and the City of Spanish Fork specifications. The contractor shall be responsible for obtaining all necessary permits and utility markings.

2. MATERIALS: All materials shall be of standard quality and shall be approved by the City Engineer. Materials shall be delivered to the project site and stored in a secure location.

3. INSTALLATION: The irrigation system shall be installed in accordance with the manufacturer's instructions and the City of Spanish Fork specifications. The contractor shall be responsible for protecting all existing utilities and structures.

4. TESTING: The irrigation system shall be tested and adjusted prior to final acceptance. The contractor shall provide a written report of the test results.

5. MAINTENANCE: The contractor shall provide a written manual for the irrigation system, including instructions for operation and maintenance. The contractor shall also provide a list of local irrigation professionals for future reference.

6. SCHEDULING: The irrigation system shall be scheduled to operate during the hours of 6:00 AM to 6:00 PM, Monday through Friday. The contractor shall be responsible for adjusting the schedule as needed.

7. FINISHES: The irrigation system shall be finished in a professional and neat manner. The contractor shall be responsible for cleaning up all debris and restoring the site to its original condition.

8. WARRANTY: The contractor shall warrant the workmanship and materials for a period of one year from the date of completion. The contractor shall be responsible for any repairs or replacements during this period.

9. PAYMENT: Payment shall be made in three equal installments. The first payment shall be made upon completion of the mainline routing. The second payment shall be made upon completion of the controller installation. The final payment shall be made upon completion of the system and final acceptance.

10. CONTACT: The contractor shall maintain contact with the City Engineer throughout the project. The City Engineer shall be responsible for providing any necessary information and approvals.

WATERING SCHEDULE

ZONE	DAY	START TIME	END TIME	WATERING RATE
1	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
2	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
3	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
4	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
5	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
6	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
7	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
8	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
9	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
10	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
11	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
12	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
13	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
14	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
15	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
16	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
17	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
18	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
19	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
20	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
21	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
22	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
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25	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
26	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
27	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
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31	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
32	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
33	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
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47	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
48	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
49	MON-FRI	6:00 AM	6:00 PM	1.0 GPH
50	MON-FRI	6:00 AM	6:00 PM	1.0 GPH

IRRIGATION NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE UTAH STATE CONSTRUCTION CODE AND THE CITY OF SPANISH FORK SPECIFICATIONS.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND UTILITY MARKINGS.

3. ALL MATERIALS SHALL BE OF STANDARD QUALITY AND SHALL BE APPROVED BY THE CITY ENGINEER.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITIES AND STRUCTURES.

5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TESTING AND ADJUSTING THE IRRIGATION SYSTEM PRIOR TO FINAL ACCEPTANCE.

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A WRITTEN REPORT OF THE TEST RESULTS.

7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A WRITTEN MANUAL FOR THE IRRIGATION SYSTEM, INCLUDING INSTRUCTIONS FOR OPERATION AND MAINTENANCE.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING UP ALL DEBRIS AND RESTORING THE SITE TO ITS ORIGINAL CONDITION.

9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING CONTACT WITH THE CITY ENGINEER THROUGHOUT THE PROJECT.

10. THE CITY ENGINEER SHALL BE RESPONSIBLE FOR PROVIDING ANY NECESSARY INFORMATION AND APPROVALS.

NO.	REVISION	DATE	DESCRIPTION
1	1	11/23/2025UT25014	811 CALL SHEET UT/UT1 1-800-882-4111
2	2		
3	3		
4	4		
5	5		
6	6		
7	7		



**MAPLE MOUNTAIN PH. 2**  
400 N. 2400 E.  
SPANISH FORK, UTAH

811 CALL SHEET UT/UT1 1-800-882-4111

RIEHL ENGINEERING  
ATT: SHAWN HERRING  
SHAWN@RIEHL.COM

PKJ DESIGN GROUP  
IRRIGATION COVER  
CITY PERMIT SET  
IR-101

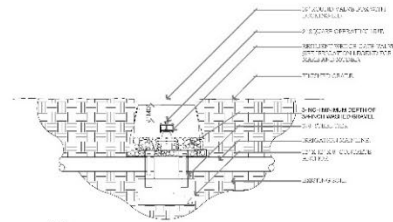
11/23/2025  
UT25014

811 CALL SHEET UT/UT1 1-800-882-4111

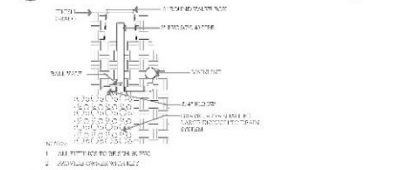
MAPLE MOUNTAIN PH. 2  
400 N. 2400 E.  
SPANISH FORK, UTAH

RIEHL ENGINEERING  
ATT: SHAWN HERRING  
SHAWN@RIEHL.COM

PKJ DESIGN GROUP  
IRRIGATION COVER  
CITY PERMIT SET  
IR-101



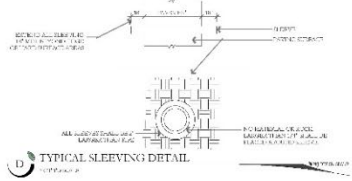
**C GATE VALVE AND ANCHOR DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2



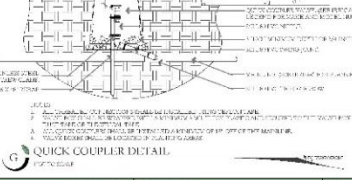
**I MANUAL DRAIN DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2



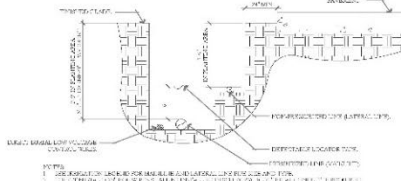
**J MASTER VALVE AND FLOW SENSOR DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2



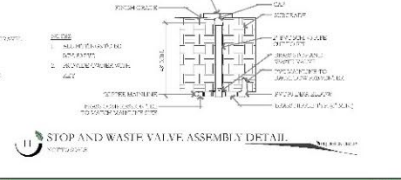
**D TYPICAL SLEEVE DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2



**G QUICK COUPLER DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2

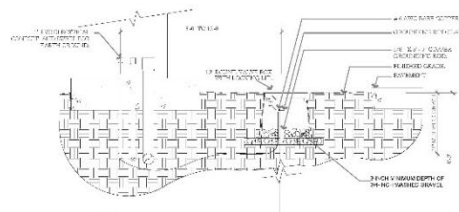


**E IRRIGATION TRENCHING DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2

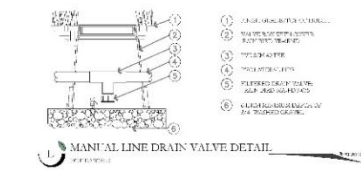


**H STOP AND WASTE VALVE ASSEMBLY DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2

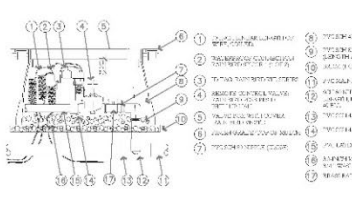
ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	PROJECT INFORMATION	DEVELOPER/PROPERTY OWNER - CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE NUMBER	DRAWING NO.
11/25/2025	UT25014		MAPLE MOUNTAIN PH. 2 400 N. 2400 E. SPANISH FORK, UTAH	REGION ENGINEERING ATT: SHAWN TIERRING 801.330.2411	PKJ DESIGN GROUP LANDSCAPE ARCHITECTURE & PLANNING 801.330.2411	UT 1500002	IR-501



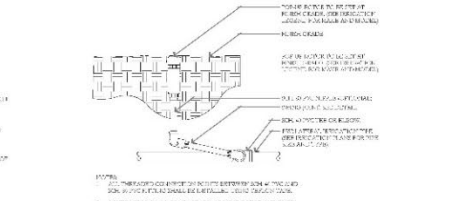
**K GROUNDING ROD DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2



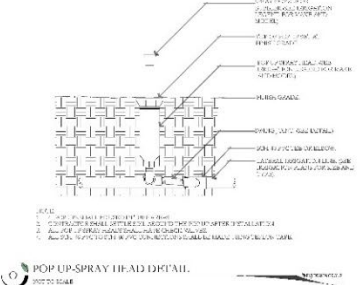
**N MANUAL LINE DRAIN VALVE DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2



**M REMOTE-CONTROL VALVE DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2

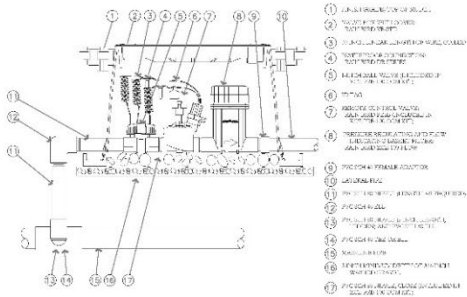


**O ROTOR HEAD DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2

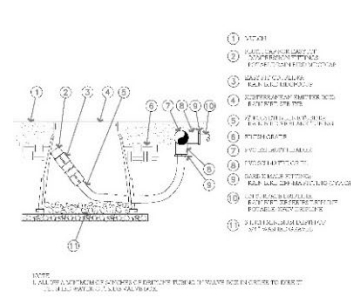


**P POP-UP-SPRAY HEAD DETAIL**  
 PROJECT: MAPLE MOUNTAIN PH. 2

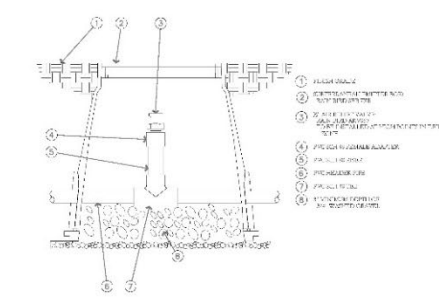
ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	PROJECT INFORMATION	DEVELOPER/PROPERTY OWNER - CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE NUMBER	DRAWING NO.
11/25/2025	UT25014		MAPLE MOUNTAIN PH. 2 400 N. 2400 E. SPANISH FORK, UTAH	REGION ENGINEERING ATT: SHAWN TIERRING 801.330.2411	PKJ DESIGN GROUP LANDSCAPE ARCHITECTURE & PLANNING 801.330.2411	UT 1500002	IR-502



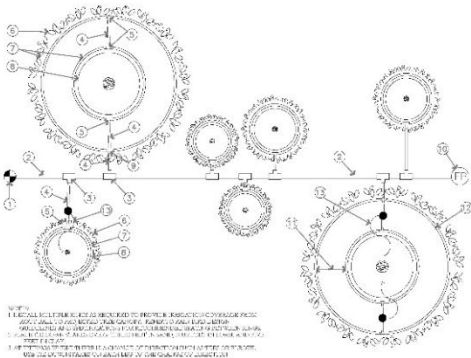
**P DRIP CONTROL ZONE KIT DETAIL**  
1/8" = 1'-0"



**Q ON-SURFACE DRIPLINE FLUSH POINT DETAIL**  
1/8" = 1'-0"



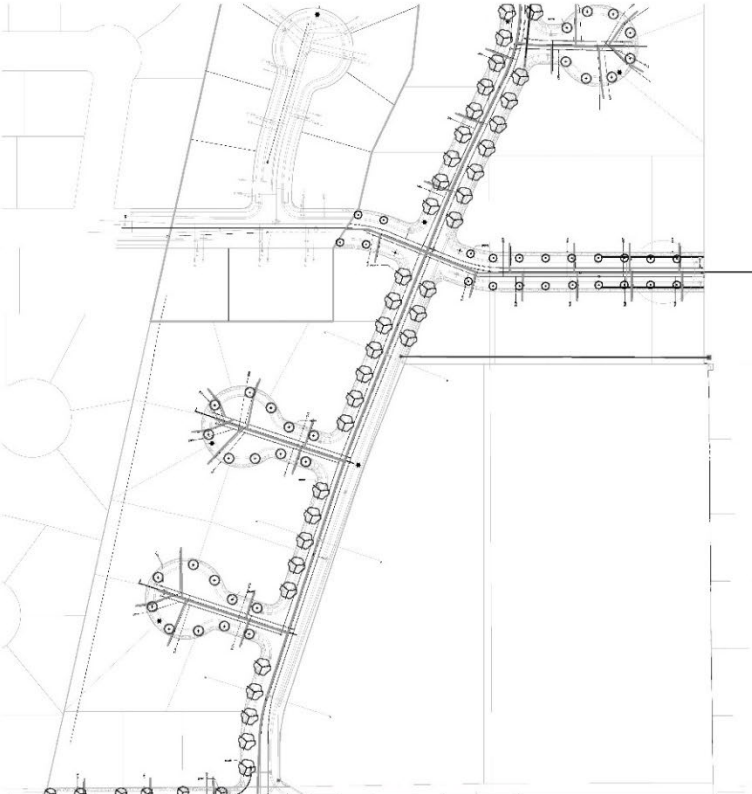
**R AIR RELIEF VALVE DETAIL**  
1/8" = 1'-0"



**S ON-SURFACE DRIPLINE TREE/SHRUB DETAIL**  
1/8" = 1'-0"

- 1. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 2. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 3. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 4. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 5. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 6. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 7. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 8. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 9. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 10. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 11. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 12. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 13. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 14. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 15. 1/2" POLYETHYLENE GLYCOL (PE) TUBING
- 16. 1/2" POLYETHYLENE GLYCOL (PE) TUBING

DATE: 11/25/2025	PROJECT NUMBER: UT25014	PROJECT INFORMATION: MAPLE MOUNTAIN PH. 2 400 N. 2400 E. SPANISH FORK, UTAH	DEVELOPER/PROPERTY OWNER/CLIENT: REGION ENGINEERING ATT: SHAWN HERRING SHERRING@REGIONDESIGNLLC.COM	LANDSCAPE ARCHITECT/PLANNER: PKJ DESIGN GROUP Landscape Architecture & Planning & Visualization 360 N. 700 SOUTH BLVD. SUITE 102 LEHI, UTAH 84043 (801) 395-2217 www.pkjdesigngroup.com	LICENSE NUMBER: JTA ACIP MAA 1599020	CITY PERMIT SET: IR-503
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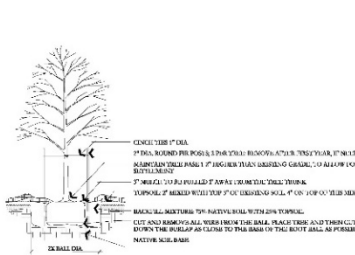


**PLANT LEGEND**

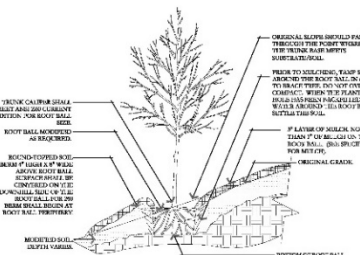
SYMBOL	CODE	QTY	BOTANICAL COMMON NAME	CONT	CAL
(Symbol)	ATX	21	Arctostaphylos Uva-ursi	3.0 B	2" GA
(Symbol)	POT	21	Prunella virginiana	3.0 B	2" GA
(Symbol)	LXY	39	Lonicera xylosteum	3.0 B	2" GA
(Symbol)	EXC	6	Excelsior	3.0 B	2" GA

- STREET TREE REQUIREMENTS**
- No tree may be planted within thirty (30) feet of intersecting sidewalk corners.
  - No tree may be planted within ten feet of any water meter, electric meter, junction box, transformer, other electrical facility, or utility pole.
  - Street trees in Master Planned Developments shall be spaced 30 feet apart (15.3.20.080 Master Planned Development Overlay District).
  - Trees need to be minimum two-inch caliper. (15.4.16.130 Landscaping, Buffering Walls, and Fences.)
  - Street trees in Master Planned Developments shall be installed prior to a Certificate of Occupancy being granted for the adjacent lot unless provisions are made for the developer to provide financial assurance to the City that the trees will be installed at a later time.
  - Automated irrigation systems are required for landscape plantings and street trees in the park strips. (Newly planted trees need additional water during the first years of planting in order to become established. In addition to properly designed irrigation systems, other methods such as drip hoses and "gator bags" should be used to provide more water for new trees.)

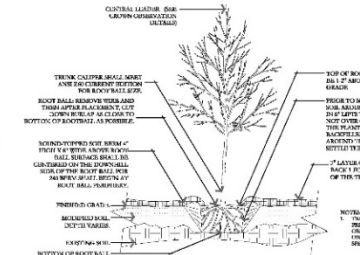
DATE: 4/23/2025	PROJECT NUMBER: UT25014	PROJECT INFORMATION: MAPLE MOUNTAIN PH. 2 400 N. 2400 E. SPANISH FORK, UTAH	DEVELOPER/PROPERTY OWNER/CLIENT: REGION ENGINEERING ATT: SHAWN HERRING SHERRING@REGIONDESIGNLLC.COM	LANDSCAPE ARCHITECT/PLANNER: PKJ DESIGN GROUP Landscape Architecture & Planning & Visualization 360 N. 700 SOUTH BLVD. SUITE 102 LEHI, UTAH 84043 (801) 395-2217 www.pkjdesigngroup.com	LICENSE NUMBER: JTA ACIP MAA 1599020	CITY PERMIT SET: LP-102
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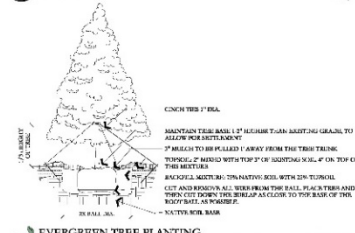
**A DECIDUOUS TREE PLANTING**  
NOT TO SCALE



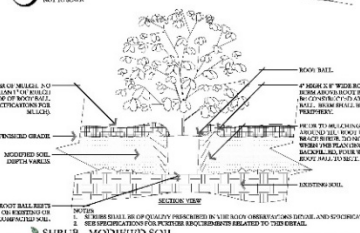
**B TREE ON SLOPE 5% (20:1) TO 50% (2:1)**  
NOT TO SCALE



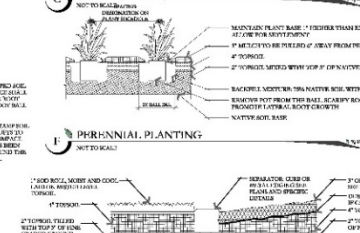
**C TRELLIS W/ BERM (EXISTING SOIL MODIFIED)**  
NOT TO SCALE



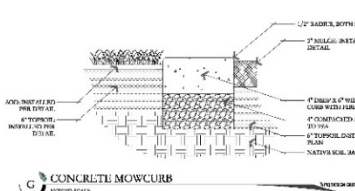
**D EVERGREEN TREE PLANTING**  
NOT TO SCALE



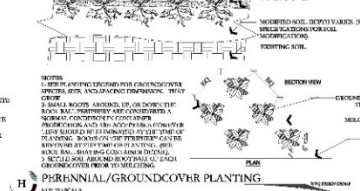
**E SHRUB - MODIFIED SOIL**  
NOT TO SCALE



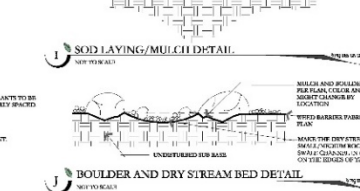
**F PERENNIAL PLANTING**  
NOT TO SCALE



**G CONCRETE MOWCURB**  
NOT TO SCALE



**H PERENNIAL/GROUNDCOVER PLANTING**  
NOT TO SCALE



**I SOIL LAYING/MULCH DETAIL**  
NOT TO SCALE



**J BOULDER AND DRY STREAM BED DETAIL**  
NOT TO SCALE

4/23/2025 NO. REVISION 1 2 3 4 5	PROJECT NUMBER <b>UT25014</b>	PLAN INFORMATION <b>811</b> BLUE STAKES OF UTAH 210 N. 1000 WEST, SUITE 100 SALT LAKE CITY, UT 84119 (801) 488-4111 www.bluestakes.com	PROJECT INFORMATION <b>MAPLE MOUNTAIN PH. 2</b> <b>400 N. 2400 E.</b> <b>SPANISH FORK, UTAH</b>	DEVELOPER/PROPERTY OWNER/CLIENT <b>RICHMON ENGINEERING</b> ATTN: SHAWN HERRING 801.378.2245 SHERRING@RICHMONDESIGNLLC.COM	LANDSCAPE ARCHITECT/PLANNER <b>PKJ DESIGN GROUP</b> LANDSCAPE ARCHITECTS # 19818 3450 N. TRIUMPH BLVD. SUITE 102 LEHI, UTAH 84040 (801) 733-8844 www.pkjdesigngroup.com	LICENSED PROFESSIONAL <b>JTA</b> <b>ACP</b> <b>JMA</b> LICENSE # 4299386 <b>CITY PERMIT SET</b> <b>LP-501</b>
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Station 61/Argyle Substation  
Zone Map Amendment  
197 West Center Street  
2.23 acres  
R-O/R-1-6 Zone  
Public Facilities General Plan Designation



**PROPOSAL**

The City applied for a Zone Map Amendment approval for the P-F Zone on the subject property.

The City has received Site Plan approval to construct a new fire station and substation adjacent to 200 West between Center Street and 100 South. The subject property includes two parcels, and the back third of two other parcels, in the R-1-6 Zone and one parcel in the R-O Zone. This Zone Map Amendment would bring the whole project area into the P-F Zone. A legal description will be created for the project area prior to approval by the City Council.

**STAFF RECOMMENDATION**

That the proposed Station 61/Argyle Substation Zone Map Amendment be recommended for approval based on the following finding:

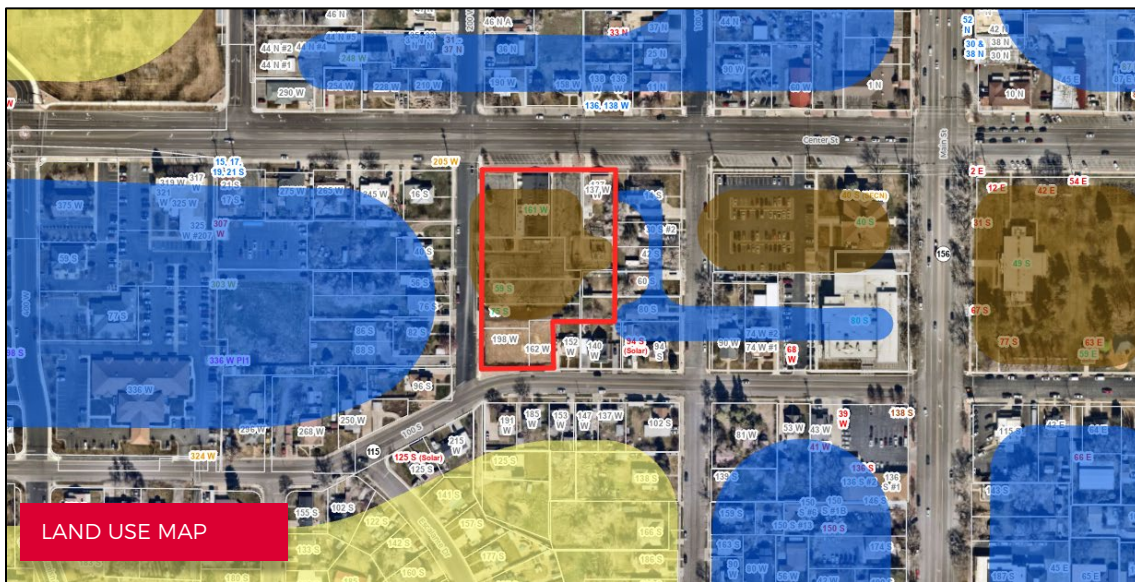
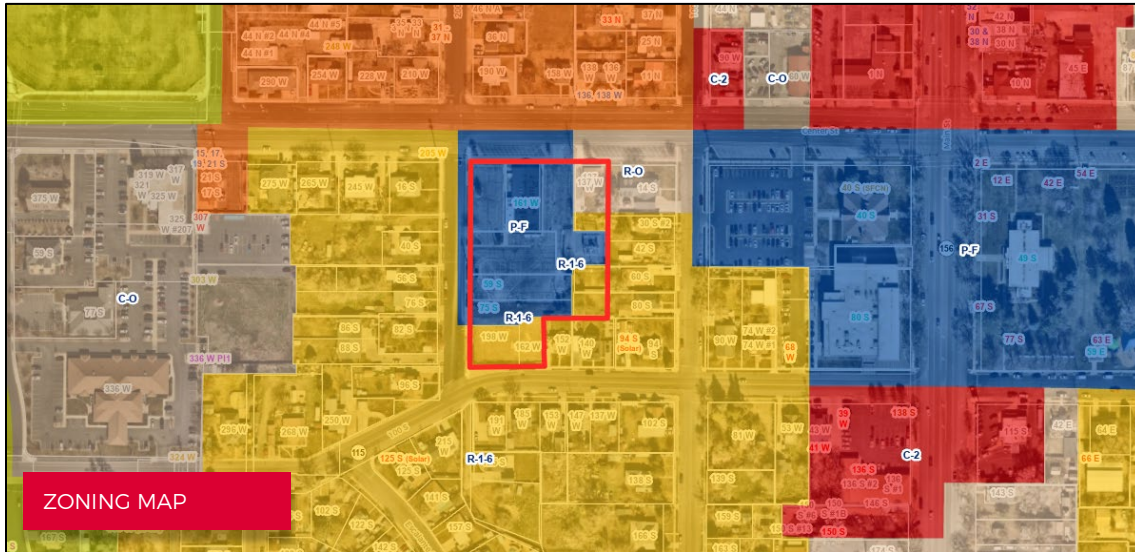
**Finding**

1. That the proposal is consistent with the City's General Plan Land Use Designation of Public Facilities.

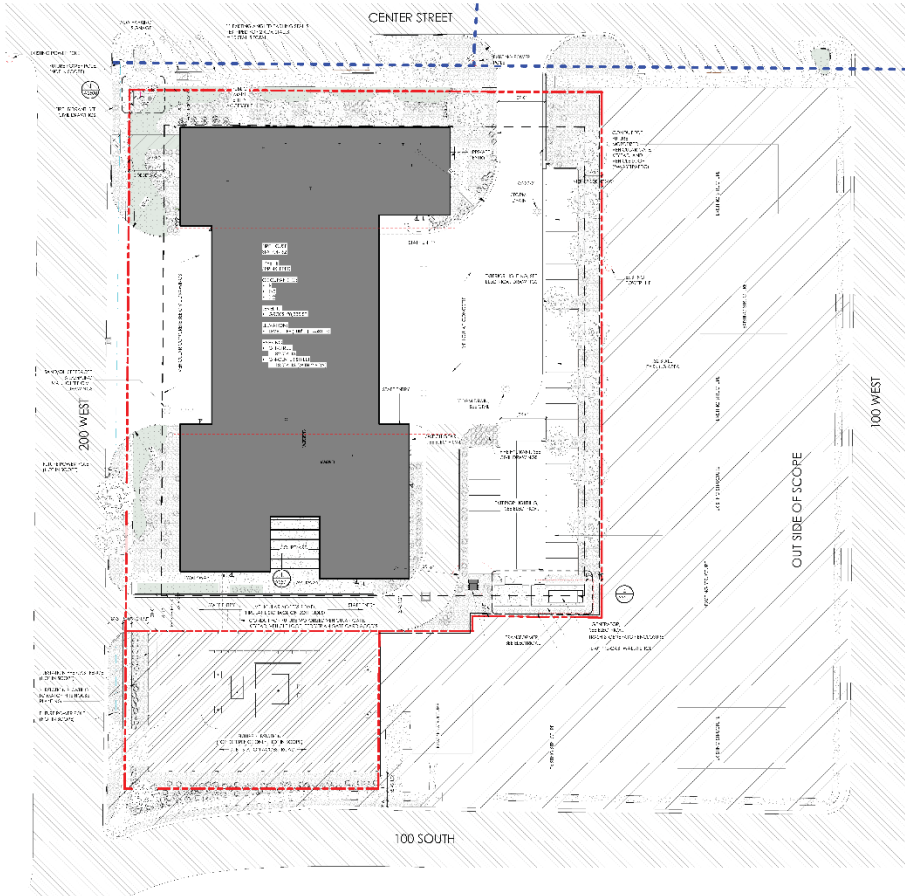
**EXHIBITS**

1. Area Maps
2. Site Plan

# EXHIBIT 1



# EXHIBIT 2



### GENERAL NOTES:

1. ALL DIMENSIONS ARE IN FEET AND INCHES UNLESS OTHERWISE NOTED.  
 2. THE ARCHITECT IS NOT RESPONSIBLE FOR THE ACCURACY OF THE SURVEY DATA OR THE EXISTING CONDITIONS SHOWN ON THIS PLAN.  
 3. THE ARCHITECT HAS CONDUCTED VISUAL GENERAL VERIFICATION OF THE SURVEY DATA AND EXISTING CONDITIONS SHOWN ON THIS PLAN.

### LEGEND:

- PROPOSED LOT
- PROPOSED DRIVE
- EXISTING DRIVE
- EXISTING SIDEWALK
- EXISTING CURB
- EXISTING UTILITY
- EXISTING TREE
- EXISTING TREE PROTECTION

### ZONING:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
 3. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.

### EXISTING CONDITIONS:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
 3. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.

### PROPOSED CONDITIONS:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
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### PROPOSED IMPROVEMENTS:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
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### PROPOSED UTILITIES:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
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### PROPOSED LANDSCAPE:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
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### PROPOSED SIGNAGE:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
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### PROPOSED PAVING:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
 3. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.

### PROPOSED LIGHTING:

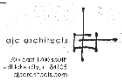
1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
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### PROPOSED FENCES:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
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### PROPOSED SECURITY:

1. THE PROJECT IS LOCATED WITHIN THE CITY OF SPANISH FORK, UT, ZONING DISTRICT OF SPANISH FORK INDUSTRIAL ZONING DISTRICT (SIF-IZ).  
 2. THE PROJECT IS SUBJECT TO THE SIF-IZ ZONING ORDINANCE, WHICH IS ADOPTED BY THE CITY OF SPANISH FORK, UT, AND IS AVAILABLE AT THE CITY CLERK'S OFFICE.  
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ARCHITECT ZONING DISTRICT

NOT FOR CONSTRUCTION

PROJECT SHEET  
**SPANISH FORK STATION 61**

DATE: 08/20/2024  
**OVERALL ARCHITECTURAL SITE PLAN**

PROJECT NO: 24001

DATE: 08/20/2024

SCALE: 1/8" = 1'-0"

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

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DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

DATE: 08/20/2024

1 ARCHITECTURAL SITE PLAN  
 DATE: 08/20/2024



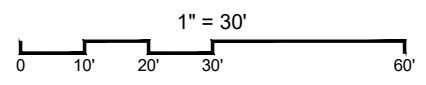
PRELIMINARY SITE PLAN  
7 BREW COFFEE

PROPERTY AREA: 24,760 SQ. FT. (.568 ACRES)  
BUILDING AREA: 510 SQ. FT.  
EMPLOYEE PARKS: 12  
STACKING: 23  
LAYOUT: 02  
DESIGNER: RST  
OWNER:



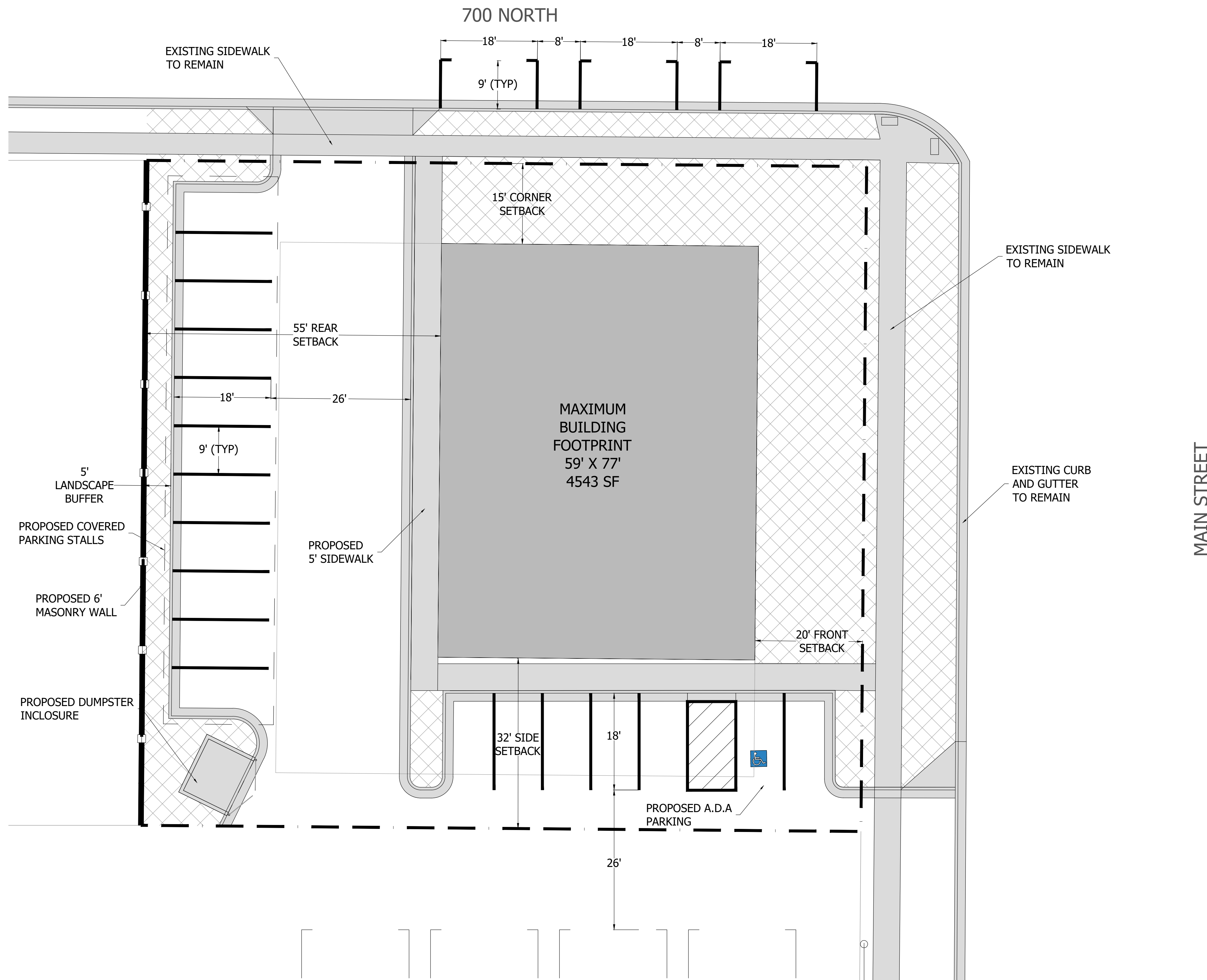
CONCEPTUAL SITE PLAN IS PREPARED FOR PRELIMINARY TEST FIT PURPOSES ONLY AND SHALL NOT BE USED FOR CONSTRUCTION.

EXPRESSWAY LN  
SPANISH FORK, UTAH



CONCEPTUAL SITE PLAN

MARCH 02, 2026



**SITE DATA SUMMARY TABLE-1**

ITEM	AREA
TOTAL AREA (ACRES)	0.38
TOTAL AREA (SF)	16,583
ZONE	R-O
GROSS OFFICE AREA (SF)	4543
RESIDENTIAL UNITS	8

**SETBACK SUMMARY TABLE-2**

ITEM	REQUIRED	PROVIDED
SIDE YARD SETBACK (15.3.16.170 TABLE 1)	5-10 FT	32 FT
FRONT YARD SETBACK (15.3.16.170 TABLE 1)	15-25 FT	20 FT
REAR YARD SETBACK (15.3.16.170 TABLE 1)	25 FT	60 FT
CORNER SETBACK (15.3.16.170 TABLE 1)	15-25 FT	15 FT

**PARKING SUMMARY TABLE-3**

ITEM	REQUIRED	PROVIDED
PARKING USE (15.4.16.120.C):		
- GENERAL/PROFESSIONAL OFFICE (1:300 FT)	15	21
- RESIDENTIAL MULTIFAMILY (2 SPACES PER UNIT)	16	
ADA PARKING STALLS	1	

**LANDSCAPE SUMMARY TABLE-4**

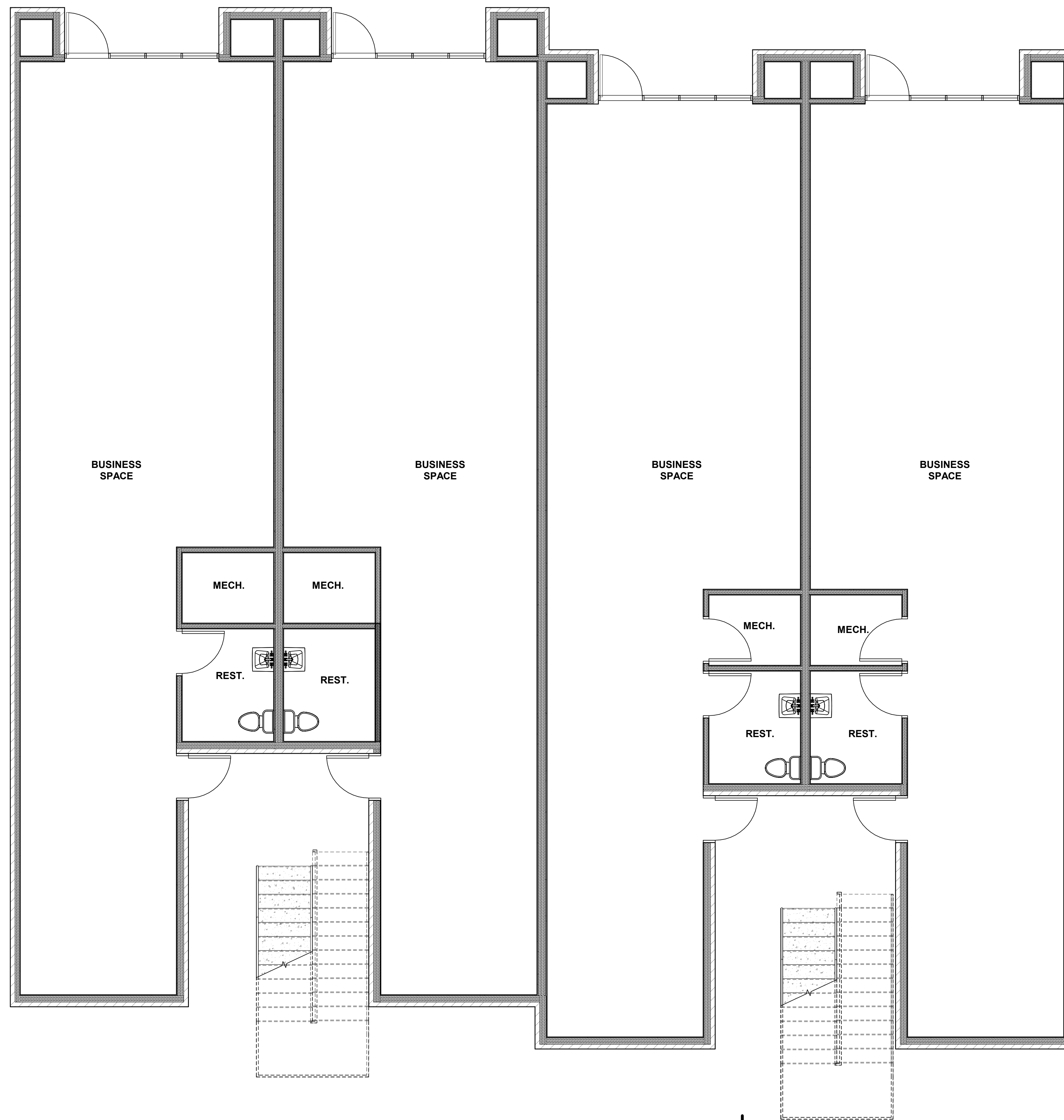
ITEM	REQUIRED	PROVIDED
LANDSCAPING AREA: 25% OF TOTAL SITE (15.4.16.130.D.5.a.(1))	4146 SF	5773 SF (34.8%)

**LEGEND**

- PROPERTY BOUNDARY LINE
- STREET CENTERLINE
- EASEMENT LINE
- SETBACK LINE
- EXISTING UTILITY LINE
- PROPOSED UTILITY LINE
- CONCRETE CURB AND GUTTER
- ADA PARKING SIGN
- PROPOSED CONCRETE
- LANDSCAPE AREA
- EXISTING CONTOURS
- PROPOSED CONTOURS

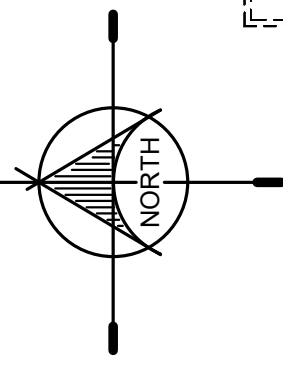
REVISIONS

NO.	DATE	BY	REMARKS



PRELIMINARY MAIN FLOOR PLAN

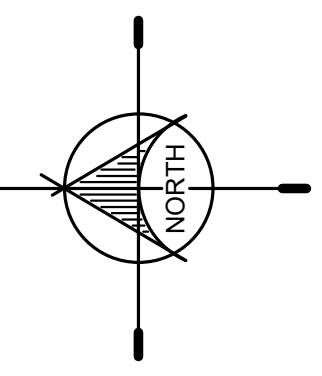
1/4" = 1'-0"





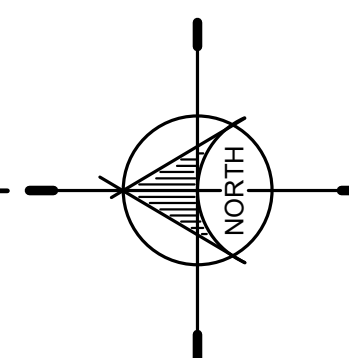
PRELIMINARY SECOND FLOOR PLAN

1/4" = 1'-0"





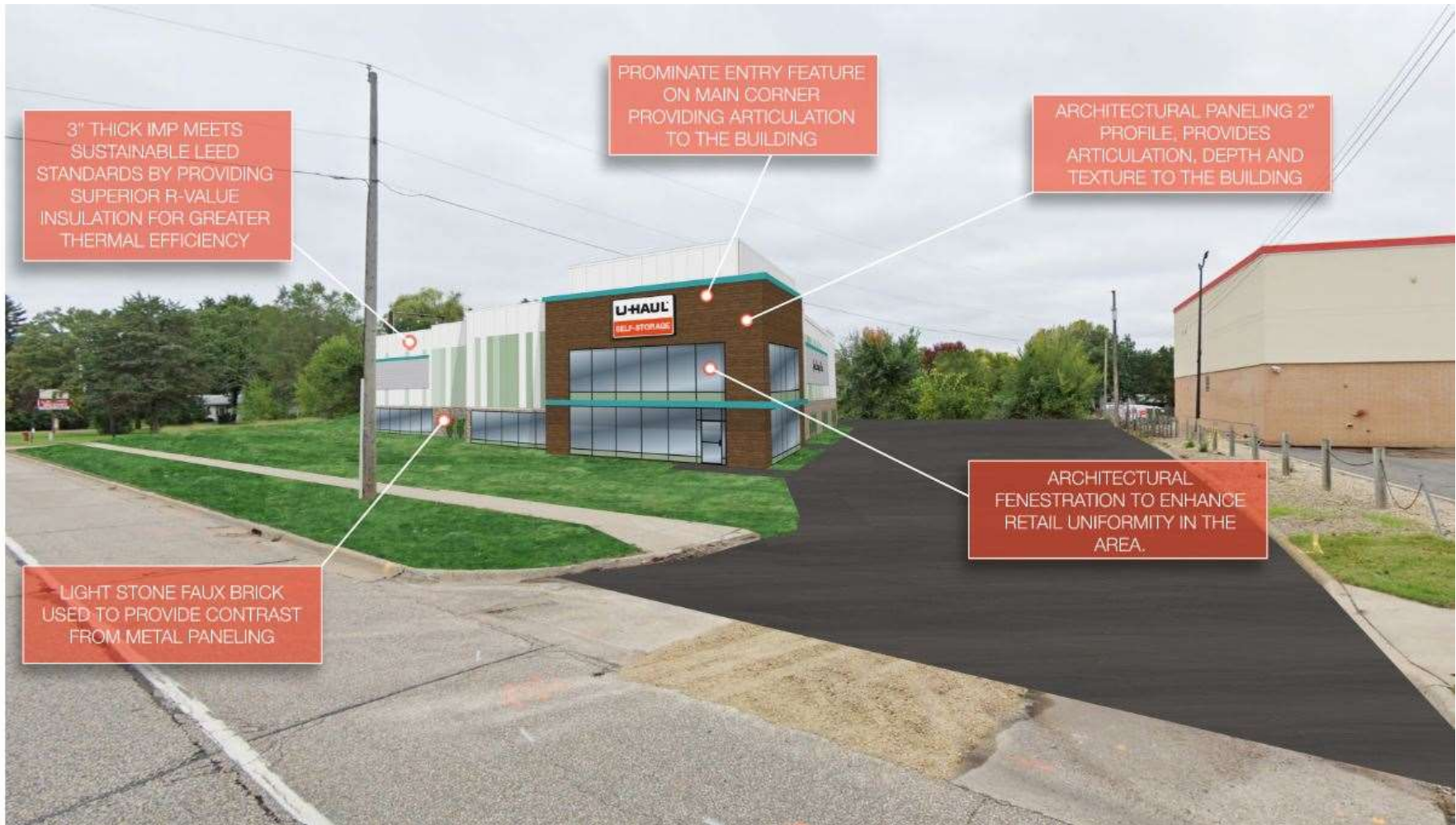
PRELIMINARY THIRD FLOOR  
 1/4" = 1'-0"



# U-Box self-storage



# U-Box self-storage



With your input, U-Haul ensures that it builds and operates centers that are aesthetically pleasing.



# U-Box self-storage



# U-Box self-storage



# U-Box self-storage



# U-Box self-storage



# U-Box self-storage



# U-Box self-storage



Colorado Springs, CO



# U-Box self-storage



# U+HAUL<sup>®</sup>

Self-Storage

Truck/Trailer Sharing

Retail Sales



Ground Ups

Adaptive Reuse

Historic Revitalization



# A sharing business since 1945



- Founded in 1945 out of need, by a Navy veteran and his wife, U-Haul has been serving the do-it-yourself movers and their households. The Company maintains the largest rental fleet which includes trucks, trailers and towing devices.
- U-Haul also offers self-storage throughout North America. The Company provides industry leading moving and storage boxes and an extended line of packing supplies to protect customer possessions.
- U-Haul is the consumer's number one choice as the largest installer of permanent trailer hitches in the automotive aftermarket. The Company supplies alternative-fuel for vehicles and backyard grills as one of the nation's largest retailers of propane.



# A sharing business since 1945



- U-Haul has 75 years of experience and expertise utilizing a "sharing" business model.
- The Company was founded as WWII was ending and raw materials were in short-supply and veterans and their families were moving towards a new life.
- U-Haul maintains the same premise since inception that, the "division of use and specialization of ownership" are good for the public.
- Sharing our equipment with as many customers as possible allows U-Haul to keep the customer's costs low, while reducing the need for more privately owned large capacity vehicles in the community.





Photo source: <http://housely.com/wp-content/uploads/2016/03/dfc1036812d14b675848157ec9f6.jpg>

The city of Grand Rapids, MI

## U-Haul Partners with Cities and Towns



### **U-Haul enhances local businesses and cities by:**

- Attracting customers who might not otherwise visit the location.
- Bringing new business to a city; ensuring a viable, expanding local economy.
- Improving the quality of life for residents needing work to support themselves and/or their families.
- Representing a responsible business model which is thoughtful towards the communities in which they do business.



# U-Haul Promotes Social and Economic Sustainability



- More than 18 million customers rented U-Haul trucks and trailers in 2016. **U-Haul provides positive economic support to a community** by attracting customers from different regions.
- This added customer base, increases awareness of other nearby merchants and fosters relationships that may **bring new business to the city, ensuring a viable, expanding local economy.**
- Increased business traffic may require hiring additional employees, improving the quality of life for residents needing work to support themselves and/or their families.



# U-Haul by the numbers



**176,000**  
Trucks



**126,000**  
Trailers



**46,000**  
Towing Devices



**1,800**  
Company Owned/  
Operated Stores



**20,000**  
Independent  
Dealers



**28,000**  
Team Members



**70.5 Million**  
Rentable Sq. Ft. of Storage



**1,100 Locations**  
Offering Alternative Fuel

**Largest**

Retail Network of Propane  
in the Country



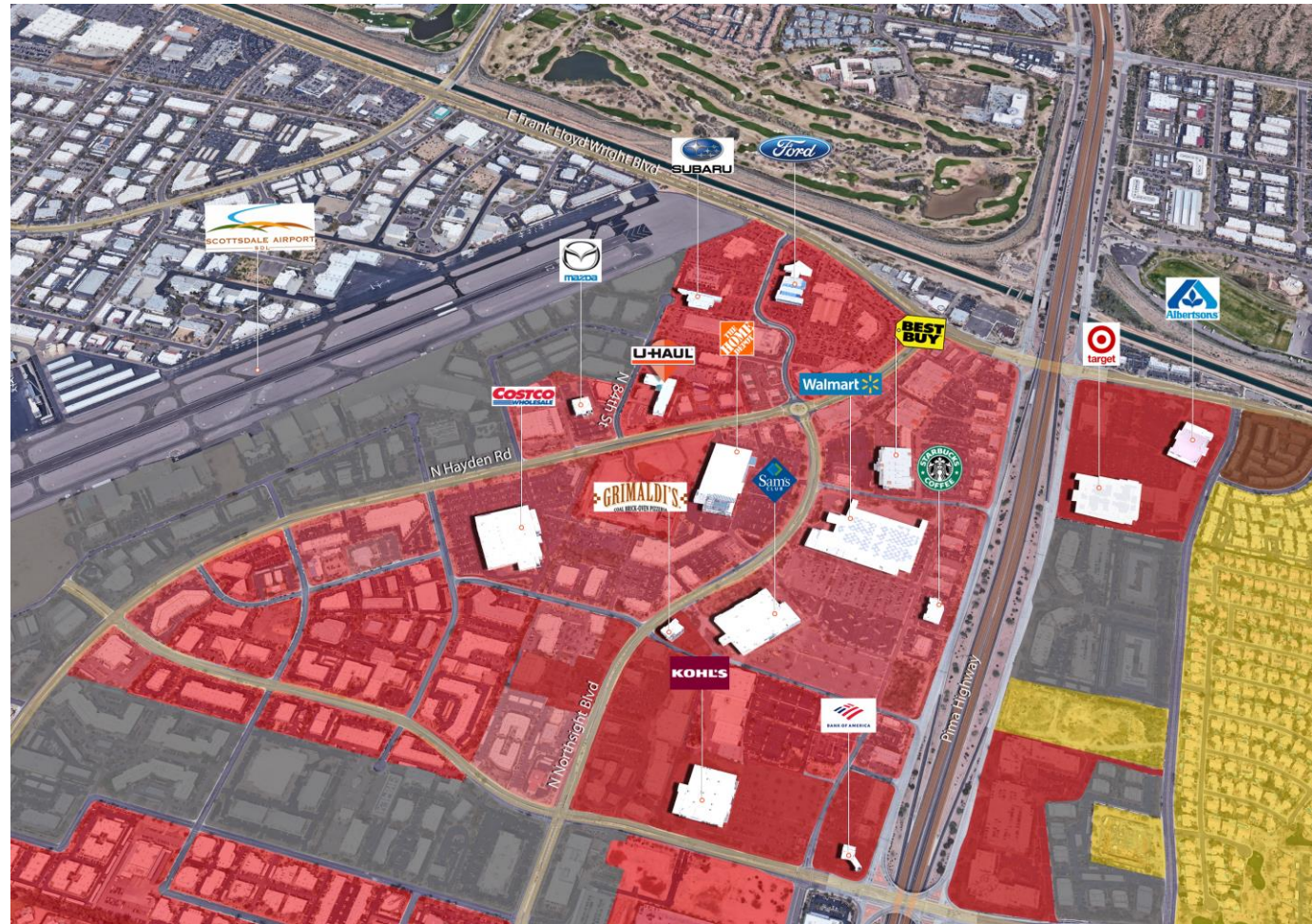
**1,300 of 1,800**  
U-Haul  
buildings are adaptive reuse projects

Over time, every U-Haul vehicle placed in a community serves as a potential substitute for 19 personally owned large-capacity vehicles. Trailer sharing reduces environmental impact because trailers do not emit greenhouse gases. Our aerodynamic design optimizes fuel efficiency of the tow vehicle.



# Partners for a Dynamic Community

U-Haul has a successful and reliable track record of stability and reliability and, is a devoted neighbor in over 1,000 communities in North America



U-Haul Moving and Storage at Hayden Road, 15455 N 84<sup>th</sup> Street, Scottsdale, Arizona 85260



# U-HAUL®

U-Haul Ground-Ups



# An Established Reputation Serving Your Community

## Ground Ups



U-Haul strives to raise standards and provide a desired and essential product to the communities in which it serves.

For over 50 years, U-Haul has been recognized as a symbol of quality and reliability throughout the United States and Canada.

Houston, TX



# Ground Ups



# An Asset To Your Community

## Ground Ups



With your input, U-Haul ensures that it builds and operates retail centers that are aesthetically pleasing.



# Ground Ups



# Ground Ups



# Ground Ups



Santa Fe Springs, CA



# Ground Ups



Somerville, MA



# Ground Ups



# Ground Ups



# U-Haul Security Features



- "State-of-the-Art" Burglar/Max Alarm System, includes 24-hour monitoring and interior motion detectors on all storage floors, stairwells and main showroom.
- Hands Free Intercom System, able to communicate to all Max Stations throughout all floors of storage; specifically designed for customers use.
- 24 Digital, HD Video surveillance, with remote & web base viewing.
- Individually Alarmed Rooms, armed & disarmed by a Keypad/Card swipe.
- Programmable, automatic security gates which only allow paying customers to access exterior storage.
- 16+ Color/HD, Day and Night Cameras, will display facilities interior, exterior and elevator.
- The Exclusive U-Haul patented latch contact, used in all storage units.





# U-Haul Sustainability Program



**Safe**



**Thriving**



**Sustainable**

**Working with you to support**

**Communities**



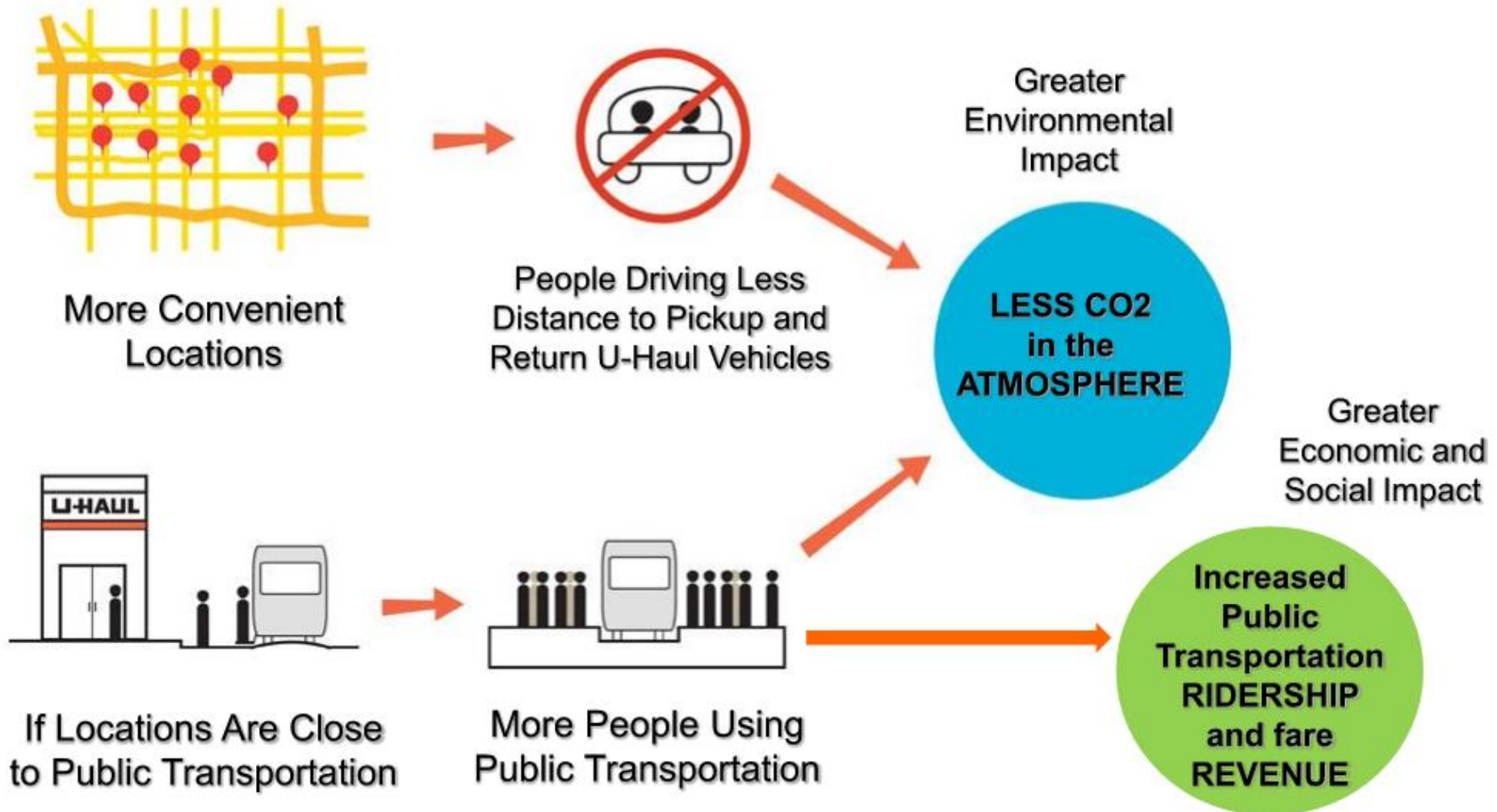
# U-Haul Sustainability Programs



- Box Exchange Program
- Facilitating Multiple Reuses
- Fuel Economy Gages
- U-Car Share
- 100% Biodegradable Packing Peanuts made of starch mixture, replacing Styrofoam and reducing landfill waste
- Moving Blankets Manufactured from Denim and cotton/polyester Re-use Centers at Storage Locations
- Telecommute Program
- The Conservation Fund - Go Zero
- Placing U-Haul Dealerships and equipment in neighborhoods, reducing customer fuel consumption and CO2 generation when obtaining and returning rental equipment.



# U-Haul Sustainability Programs- truck sharing



## U-Haul Sustainability Programs- truck sharing

Overtime, every U-Haul truck placed in a community eliminates 19 personally owned large-capacity vehicles (LCVs) off the roads.



Consider the homeowner who uses his own LCV to transport household goods back and forth until the move is complete. Using a properly sized U-Haul truck will allow that homeowner to move in one trip, saving time, money, fuel and reducing traffic and emissions in the city.



# U-Haul Rental Shunting

## **SHUNT (verb):**

*To travel back and forth, as in 'to shunt between the two towns'.*

The truck shunting practice presents a real-world example of efficient land use.

- ❑ Each U-Haul truck rental store serves as a dynamic shunting area instead of a static parking lot.
- ❑ In other words, it allows us to keep our rental vehicles in use, never parked in one spot for a long time. Once a customer returns the truck, it's cleaned and maintained and presented to the next rental customer without sitting parked in a lot for a long period of time.



# U-Haul Sustainability Programs



**Our centers are constructed with methods and products that reduce energy consumption:**

- Using Butler MR-24 Metal Roof System has a life cycle of 35+ years. It is Energy Star compliant and is recyclable.
- Mobile storage rooms made from 95% reused material.
- Motion Sensor Lighting in Storage Centers.
- Permeable ground cover for rental equipment parking and self-storage drives.
- Reducing development footprint, recharging the ground water, and lowering impact on municipal water management systems.
- Partnership with Arizona State University on incorporating sustainable architecture into U-Haul Centers.
- Locating U-Haul Centers near public transportation lines to reduce customer fuel consumption and CO2 emissions.



# Re-Use Centers

To reduce the amount of used goods being dumped, we provide centers in which our customers can leave their unwanted but reusable belongings, such as furniture, for others to take.

U-Haul has implemented more than 450 Storage Re-use Centers across the United States and Canada.

Annually, the existing re-use centers facilitate local residents reusing more than 135,000 household goods, which equates to avoiding:

- 4.6 million pounds of materials being dumped in landfills
- 720,000 pounds of GHG/CO<sub>2</sub> emissions from municipal waste trucks which would have been used to transport the materials to landfills.



# Take a Box, Leave a Box

We designate a place at our center for our customers where they can leave their used moving boxes for future customers to reuse. Our idea is that while recycling is great, reuse should occur first to ensure more efficient use of our resources

Box Exchange & Take A Box Leave A Box- 2007

- Both programs encourage reuse of U-Haul boxes, which are built to be reused again and again.
- Nearly 1 million boxes are reused every year.



# Biodegradable Packing Peanuts

**U-Haul uses packing peanuts made of starch mixture, replacing Styrofoam and reducing landfill waste.**

Biodegradable Packing Peanuts - Since 1993

Made primarily of corn and potato starch, the peanuts dissolve/degrade in water as an eco-friendly alternative to Styrofoam. Annual diversion of more than 407,000 cubic feet of materials formerly destined for landfills.



# Green Building

## U-Haul Sustainable Development

At U-Haul, we are committed to sustainable development through environmental protection, social responsibility, and economic efficiency.

For us, sustainability means embracing our responsibility to act as a good corporate citizen. We are continually refining our products, services, and activities to meet the needs of the present without compromising the ability of future generations to meet their own needs.



# Go Zero



**Our centers are constructed with methods and products that reduce energy consumption:**

To help protect and restore America's special places, U-Haul has partnered with **The Conservation Fund's Go Zero<sup>(SM)</sup>** program to plant trees to help "zero out" carbon footprint. We ask customers to donate to the fund, in which The Conservation Fund receives 100% of the donation.

Since 2007, U-Haul and more than 840,000 customers have chosen to offset moving emissions with a donation to The Conservation Fund's Go Zero program.

We've passed along 100% of those contributions, which have planted nearly 300,000 native trees. As they grow, these new forests restore habitat for wildlife, preserve natural ecosystems and clean the air we breathe.

The logo for The Conservation Fund, featuring the text 'THE CONSERVATION FUND' in a serif font, with a horizontal line underneath.

*America's Partner in Conservation*





ALLIANCE TO  
**SAVE ENERGY**  
*Creating an Energy-Efficient World*



## Awards

- 2009** U-Haul AAMVA's Environmental Leadership Award for Climate Change and Energy & Resource Conservation
- 2009** U-Haul earned Valley Forwards Environmental Stewardship Award for Merit
- 2010** U-Haul earned The Conservation Funds Heroes of Go Zero® Award for addressing climate change and restoring America's forest legacy
- 2010** U-Haul earned The Alliance to save Energy's Galaxy Star of Energy Efficiency Award
- 2011** U-Haul earned Valley Forwards Crescordia Award for Energy Efficiency Program of the Year
- 2011** U-Haul named finalist of International Platts *Global Energy Awards* for Energy Efficiency Program of the Year: Commercial End-User award
- 2012** U-Haul earned Valley Forwards Crescordia Award for Contributions to Environmental Education and Awareness
- 2012** U-Haul named one of seven finalists for the world-class Platts *Global Energy Awards*, nominated for prestigious Energy Efficiency Program of the Year: Commercial End-User award
- 2012** U-Haul named one of the twelve finalists for the world-class Platts 2014 *Global Energy Awards*, nominated for the prestigious Corporate Social Responsibility award
- 2014** U-Haul was named a finalist for the Platts 2014 Global Energy Awards "Efficiency Initiative - Commercial End-User".
- 2015** U-Haul earned Arizona forward's Award of Merit as a "Healthy Community, Sustainability Workplace."



# Traffic Studies & Commercial Uses

**Our truck and self-storage uses generate less traffic than most other commercial uses.**

Use Comparison					
Use	Square Feet	Traffic Volume		Typical Hours	Days
		Weekday	Weekend		
Fast Food Restaurant	3,000 sq ft	3,161 trips	3,430 trips	18 hours – 24 hours	7
Gas Station w/ Convenience Store	2,200 sq. ft.	1,200 trips	2,200 trips	18 hours – 24 hours	7
Hotel	50,000 sq. ft.	905 trips	901 trips	24 hours	7
Casual Dining	5,000 sq. ft.	1,075 trips	1,258 trips	11 am – 11 pm 12 hours	7
U-Haul Center	80,000 sq. ft.	31 trips	53 trips	7 am – 7 pm 12 hours	7



# Benefits to your city



- Promotes growth in your community.
- Promotes sustainability by reducing customer trip-length to obtain a shared truck – HUGE CO<sub>2</sub> savings. Shows leadership via us, public/private cooperation.
- The proposed U-Haul would add to the property value and increase the tax base, which strengthens the economic base of the community.
- Promotes reuse programs and reduces tree harvesting, pulp manufacturing and shows leadership via public/private.
- Promotes sustainability and generates less traffic than other retail or commercial uses, thus reducing congestion and pollution.
- The planned U-Haul expansion would adhere to City Code and standards, creating a new use for an existing site.



**U+HAUL®**



Thank you for your time and attention

