



# AGENDA – Planning Commission Meeting

Planning Commission Chair Rachel Sprosty Burns  
Planning Commission Vice Chair Doug Willden  
Planning Commissioner Scott A. Hill  
Planning Commissioner Virginia Rae Mann  
Planning Commissioner Colton Miles  
Planning Commissioner NH Rather  
Planning Commissioner Chris Roman

**CITY OF SARATOGA SPRINGS**  
**Thursday, March 26, 2026 @ 6:00 pm**  
City of Saratoga Springs Council Chambers  
319 S. Saratoga Road, Saratoga Springs, UT 84045

## CALL TO ORDER

1. Pledge of Allegiance.
2. Roll Call
3. Public Input – *This time has been set aside for the public to express ideas, concerns, and comments for subject matter not listed as public hearing on the agenda. Limit of 3 minutes per speaker, unused time may not be given to another. Time for Public Input is limited to no more than 15 minutes total.*

## BUSINESS ITEMS

*The Commission will discuss (without public comment) and may either make a recommendation to the City Council, or approve the following items as needed:*

1. North Lake Meadows Phase 2 Preliminary Plat, located at 145 North 1700 West. Keaton Morton as applicant. Planner II Sam Stout.
2. The Hub Lot 5 Site Plan, located at 2438 North Stagecoach Drive. Michael McIntire as applicant. Planner II Sam Stout.
3. Approval of Minutes: March 12, 2026.

## PUBLIC HEARINGS

*The Commission will accept public comment and may make a recommendation to the City Council for the following items:*

1. Lighthouse Cove Concept Plan and Rezone from Agricultural, Rural Residential to R1-10, located at approximately 4200 South Schooner Drive. Lynsi Neve as applicant. Planner II Sam Stout.
2. Amendments to Title 19 Land Development Code of the City of Saratoga Springs, Chapter 19.02 – Definitions. The amendment would add a definition for loading bays. Citywide. City-initiated. Senior Planner Austin Roy.
3. Amendments to Title 19 Land Development Code of the City of Saratoga Springs, Chapter 19.16 – Site and Architectural Design Standards. The amendment would allow an architecture and design exception to additional standards for Office Warehouse, Heavy Commercial, and Light Industrial/Industrial zoned buildings if they are no wider than 200 ft. in front and no larger than 15,000 square feet. Citywide. City-initiated. Planner II Kendal Black.

Supporting materials are available for inspection on the City Website [www.saratogasprings-ut.gov](http://www.saratogasprings-ut.gov) Questions and comments to Staff and/or Commissioners may be submitted to [comments@saratogasprings-ut.gov](mailto:comments@saratogasprings-ut.gov) Meetings are streamed live at <https://www.youtube.com/c/CityofSaratogaSprings>

PLEASE NOTE: The order of items may be subject to change with the order of the planning commission chair. One or more members of the Commission may participate electronically via video or telephonic conferencing in this meeting.

In compliance with the Americans with Disabilities Act, individuals needing special accommodations (including auxiliary communicative aids and services) during this meeting should notify the City Recorder at 801.766.9793 at least two days prior to the meeting.

## **REPORTS**

1. Commissioner's Comments.
2. Director's Report.

## **CLOSED SESSION**

Possible motion to enter into closed session for the purchase, exchange, or lease of property; pending or reasonably imminent litigation; the character, professional competence, or the physical or mental health of an individual; or the deployment of security personnel, devices, or systems.

## **ADJOURNMENT**



**Preliminary Plat  
Meadows at North Lake 2  
March 26, 2026  
PUBLIC MEETING**

Applicant:	Keaton Morton
Owner:	Courtney M Lye
Location:	145 N 1700 W
Project Acreage:	5.962 acres
Current Use:	Vacant Land
Land Use Designation:	Low Density Residential
Parcel Zoning:	R1-9
Adjacent Zoning:	R1-9 & Agricultural
Adjacent Uses:	Low Density Residential and Vacant Land
Type of Action:	Administrative
Land Use Authority:	Planning Commission
Planner:	Sam Stout, Planner II

**A. Executive Summary and Request:**

The applicant is requesting approval of a preliminary plat for the Meadows at North Lake 2 Subdivision, located at 145 N 1700 W. The total area that is being platted is 5.962 acres. The plat will consist of 19 lots with a minimum lot size of 9000 square feet.

**Recommendation:**

**Staff recommends that the Planning Commission conduct a public meeting on the application, review and discuss the proposal, and choose from the options in the Recommendation and Alternatives Section of this report.** Options include approval with or without conditions, denial, or continuation.

**B. Background: October 7, 2025** - the City Council approved the Annexation Petition application.

The land that makes up the project area was recently annexed to the City of Saratoga Springs from Lehi City – Meadows at North Lake Annexation Plat was recorded on December 23, 2025.

**C. Process:** City Code Chapter 19.13 outlines the process for a preliminary plat and specifies that the Planning Commission is the approval authority for preliminary plats with single-family lots.

**D. Community Review:**

Public Meeting: This has been noticed as a public meeting pursuant to City and State statutes, which requires posting notice of the meeting and the agenda not less than 24 hours before the meeting.

As of the date of this report, no public input has been received.

**E. General Plan:** The General Plan Land Use Map shows t the project area as Low-Density Residential.

**Staff conclusion: Consistent.**

The proposed subdivision plat will is consistent with Low Density Residential.

**F. Code Criteria:**

For full analysis please see the attached Planning Review Checklist.

**List all related sections from the checklist**

- 19.04, Land Use Zones: **Complies.**
- 19.06, Landscaping and Fencing: **Complies.**
- 19.11, Lighting: **Complies.**
- 19.13, Process: **Complies.**
- 19.14, Site Plans: **Complies.**
- 19.16, Site and Architectural Design Standards: **Complies.**
- 19.18, Sign Regulations: **Complies {separate permit and approval required}.**

**Open Space Evaluation:**

The applicant desires to apply these credits to the open space and amenity requirements for the Meadows at North Lake 2 Subdivision Plat:

Required Open Space Credits

- 0.48 equivalent acres
- 0.60 actual acres
- 47.5 amenity points

Proposed Open Space Credits

- 0.67 equivalent acres
- 0.60 actual acres
- 59.35 amenity points

One of the amenity items exceeds 25% of the total required points. The current code does not allow for one amenity item to exceed 25% of the required points. A Code Amendment is currently being taken to Planning Commission on March 12, 2026 and City Council on April 14, 2026 – removing the 25% requirement (19.09.04.04) to allow for more flexibility in applying

required points. If the Code Amendment is approved, all Open Space amenity requirements will be met.

**G. Recommendation and Alternatives:**

Staff recommends that the Planning Commission conduct a public meeting, discuss the application and choose from the following options.

**Option 1 – Approval**

“I move that the Planning Commission approve the request for Meadows at North Lake 2 Preliminary Plat located at 145 N 1700 W,, with the Findings and Conditions in the Staff Report.”

**Findings**

1. The application is consistent with the General Plan, as articulated in Section E of the staff report, which section is incorporated by reference herein.
2. The application complies with the criteria in the Land Development Code, as articulated in Section F of the staff report, which section is incorporated by reference herein.

**Conditions:**

1. All conditions of the City Engineer shall be met, including but not limited to those in the attached Engineering Staff Report.
2. All requirements of the Fire Chief shall be met.
3. Once approved by the Planning Commission, all remaining redlines on plans, Engineering staff report, and the redlines in the Application Review Checklist shall be corrected before the construction drawings are approved by staff.
4. The Code Amendment 19.09.04.04 of the City Code will need to be approved by City Council for the open space amenities to meet the code.
5. All other Code requirements shall be met.
6. Any other conditions or changes as articulated by the Planning Commission:

\_\_\_\_\_.

**Option 2 – Continuance**

“I move to **continue** the Preliminary Plat for Meadows at North Lake **Meadows at North Lake 2** 2 to another meeting on [DATE], with direction to the applicant and Staff on information and/or changes needed to render a decision, as follows:

1. \_\_\_\_\_
2. \_\_\_\_\_

**Option 3 – Denial**

“I move that the Planning Commission deny the requested Preliminary Plat for Meadows at North Lake 2, located at 145 N 1700 W based upon the Findings below:

1. The application is not consistent with the General Plan:
  - a. \_\_\_\_\_, and/or,
2. The application is not consistent with Section {XX.XX} of the Code:
  - a. \_\_\_\_\_, and/or
3. The application does not comply with the DA,MDA,DAP,CP,VP,NP:  
\_\_\_\_\_.

**H. Exhibits:**

1. Location Map
2. City Engineer's Report
3. Preliminary Plat
4. Application Review Checklist
5. Landscape Plan

Exhibit 1



CLARK DR

PONY EXPRESS PKWY

MIA CV

LIVI LN

1650 EAST (1700  
W Lehi COOR.)

## Staff Report

**Author:** Scott Petrik, Engineer I  
**Subject:** Meadows at North Lake—Final Plat  
**Date:** March 26, 2026  
**Type of Item:** Final Plat Approval



SARATOGA  
SPRINGS

### Description:

**A. Topic:** The Applicant has submitted a Final Plat Application. Staff has reviewed the submittal and provides the following recommendations.

**B. Background:**

*Applicant:* Keaton Morton—Anderson Development Services  
*Request:* Final Plat Approval  
*Location:* Northwest of the Pony Express Pkwy / 1600 East  
*Acreage:* 5.94 Acres - 19 Lots

**C. Recommendation:** Staff recommends the approval of final plat subject to the following conditions:

1. All review comments and redlines provided by the City Engineer are to be complied with and implemented into the Final plat and construction drawings.
2. Provide a Storm Water Pollution Prevention Plan (SWPPP) following the State template prior to the pre-construction meeting.
3. All property shall be removed from the Special Flood Hazard Area (SFHA) identified by FEMA mapping, including an approved Letter of Map Revision Based on Fill (LOMR-F) prior to plat recordation.
4. Developer shall install frontage improvements along Pony Express Parkway, meeting the City's arterial road standards.
5. Developer shall install and improve the trail along Pony Express Parkway.
6. Developer shall construct 1600 East to Saratoga Springs City collector roadway standards and include a raised median at the proposed intersection restricting traffic to right-in-right-out to meet the City's access Management standards.
7. Developer shall install the master planned storm drain line PE4C along the project frontage of Pony Express Parkway. The master planned drain line shall be designed to convey a flow of 12 cfs.

8. Developer shall install the master planned 16" secondary waterline along the project's frontage for Pony Express Parkway.
9. Owner to record a Long-Term Storm Water Management Agreement and provide a Long-Term Storm Water Management Plan for the project.
10. Review and inspection fees must be paid as indicated by the City prior to any construction being performed on the project.
11. Developer must secure water rights as required by the City Engineer, City Attorney, and development code.
12. Developer shall provide easements for all off-site utilities not located in the public right-of-way.
13. Developer is required to ensure that there are no adverse effects to future homeowners due to the grading practices employed during construction of these plats.
14. Project bonding must be completed as approved by the City Engineer prior to recordation of plats.
15. Developer may be required by the Saratoga Springs Fire Chief to perform fire flow tests prior to final plat approval and prior to the commencement of the warranty period.
16. Submittal of an electronic version of the as-built drawings in pdf format to the City Engineer is required prior acceptance of site improvements and the commencement of the warranty period.
17. Developer shall bury the power lines less than 46 kV and underbuilt that are within this plat.
18. Please be advised that, only a signed reimbursement agreement approved by the City Council, as detailed in the City Engineering Standard 00500/2.3/E, can guarantee that improvements in this project are eligible for reimbursement by the City. If the applicant believes that some of the improvements in this project are eligible for reimbursement from the City, it is strongly recommended the applicant obtain a reimbursement agreement from the City before they are constructed. Constructing them without a signed reimbursement agreement in place, even if they are approved/accepted in the construction drawings, inspected by the City as they are constructed, and/or contained in the City's Capital Facilities Plans and Impact Fees Facilities Plans, is at the sole risk of the developer without any guarantee of reimbursement or any particular timing of reimbursement.
19. Traffic study summary of key findings & recommendations:

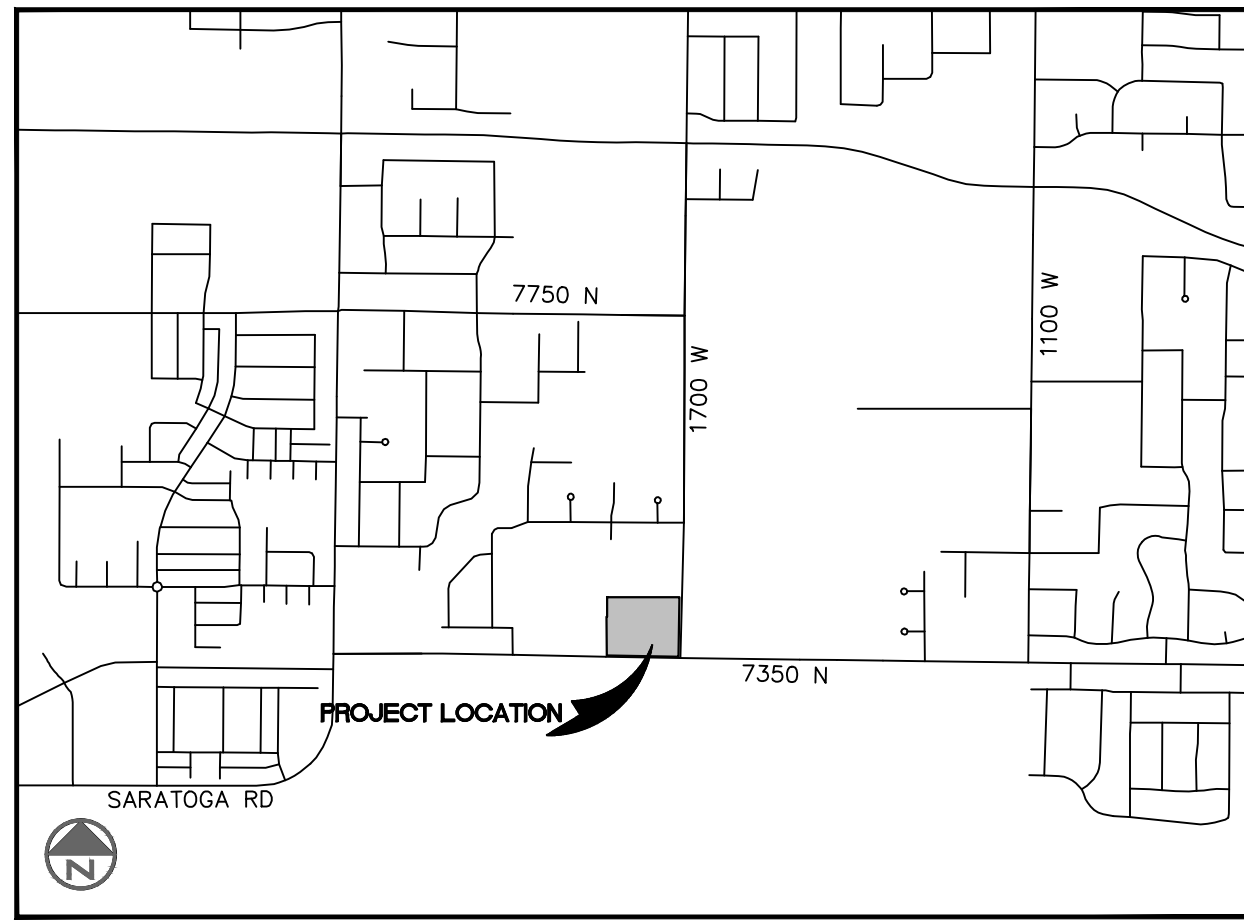
Project Conditions		
<ul style="list-style-type: none"> <li>• The development will consist of a subdivision with 19 single-family homes</li> <li>• The project is anticipated to generate approximately 420 weekday daily trips, including 20 trips in the morning peak hour, and 22 trips in the evening peak hour</li> <li>• Based on the anticipated project traffic, no auxiliary lanes are recommended</li> <li>• Based on standard practice in Utah, a median of 150 feet in each direction (from the center) would be recommended at the South Access / 1600 East intersection</li> </ul>		
2025	Background	Plus Project
Assumptions	<ul style="list-style-type: none"> <li>• North Lake Meadows phase 1 traffic added</li> </ul>	<ul style="list-style-type: none"> <li>• Median on 1600 East installed making South Access RIRO</li> </ul>
Findings	<ul style="list-style-type: none"> <li>• Acceptable LOS</li> </ul>	<ul style="list-style-type: none"> <li>• Acceptable LOS</li> </ul>

# Exhibit 3

## MEADOWS AT NORTHLAKE SUBDIVISION

LOCATED IN THE SOUTH HALF OF SECTION 19,  
TOWNSHIP 5 SOUTH, RANGE 1 EAST,  
SALT LAKE BASE AND MERIDIAN  
SARATOGA SPRINGS CITY, UTAH COUNTY, UTAH

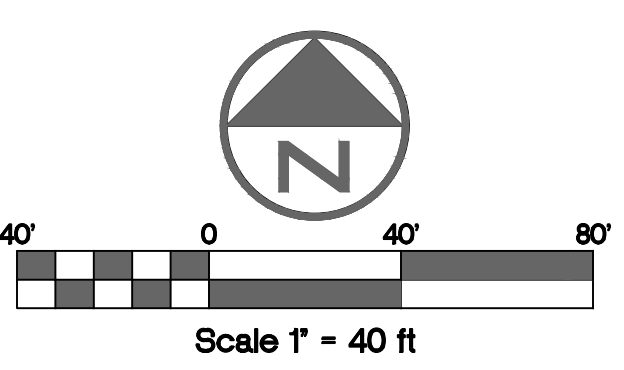
Curve #	Length	Radius	Delta	Chord Bearing	Chord Distance
C1	23.65'	15.00'	90°20'02"	N44°47'55"W	21.27'
C2	23.47'	15.00'	89°39'58"	N45°12'05"E	21.15'
C3	43.20'	77.00'	32°08'42"	S15°42'15"E	42.64'
C4	28.59'	77.00'	21°16'34"	S42°24'53"E	28.43'
C5	49.61'	77.00'	36°54'46"	S71°30'33"E	48.75'
C6	121.40'	77.00'	90°20'02"	S44°47'55"E	109.21'
C7	72.52'	46.00'	90°20'13"	S44°48'00"E	65.24'
C8	23.65'	15.00'	90°21'21"	N44°48'11"W	21.27'
C9	17.17'	11.00'	89°25'28"	S45°19'12"W	15.48'
C10	17.39'	11.00'	90°34'23"	N44°40'44"W	15.63'
C11	17.22'	11.00'	89°42'39"	N45°28'05"E	15.52'
C12	17.39'	11.00'	90°34'23"	S44°40'44"E	15.63'
C14	36.26'	46.00'	45°10'06"	S22°12'57"E	35.33'
C15	36.26'	46.00'	45°10'06"	S67°23'04"E	35.33'



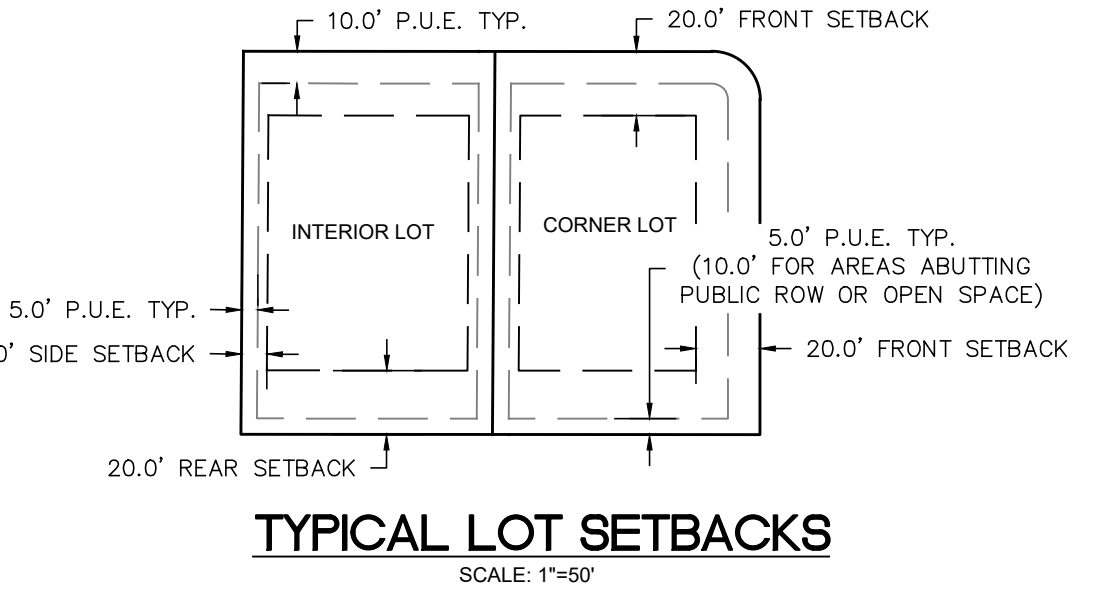
### VICINITY MAP

- PLAT NOTES:
- 1-PLAT MUST BE RECORDED WITHIN 24 MONTHS OF FINAL PLAT APPROVAL, OR FOR PHASED DEVELOPMENTS, WITHIN 24 MONTHS OF RECORDATION OF MOST RECENT PHASE. THE FIRST FINAL PLAT APPROVAL WAS GRANTED ON THE \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.
  - 2-PRIOR TO BUILDING PERMITS BEING ISSUED, SOIL AND/OR GEOTECHNICAL TESTING STUDIES MAY BE REQUIRED ON EACH LOT AS DETERMINED BY THE CITY BUILDING OFFICIAL.
  - 3-THIS PLAT MAY BE SUBJECT TO A DEVELOPMENT AGREEMENT THAT SPECIFIES THE CONDITIONS OF DEVELOPING, BUILDING, AND USING PROPERTY WITHIN THIS PLAT. SEE CITY RECORDER FOR DETAILS.
  - 4-PLAT IS SUBJECT TO "INSTALLATION OF IMPROVEMENTS AND BOND AGREEMENT NO. \_\_\_\_\_," WHICH REQUIRES THE CONSTRUCTION AND WARRANTY OF IMPROVEMENTS IN THIS SUBDIVISION. THESE OBLIGATIONS RUN WITH THE LAND AND ARE BINDING ON SUCCESSORS, AGENTS, AND ASSIGNS OF DEVELOPER. THERE ARE NO THIRD-PARTY RIGHTS OR BENEFICIARIES UNDER THIS AGREEMENT.
  - 5-BUILDING PERMITS WILL NOT BE ISSUED UNTIL ALL REQUIRED IMPROVEMENTS HAVE BEEN INSTALLED PER CITY STANDARDS AND ALL FEES INCLUDING IMPACT AND CONNECTION FEES ARE PAID.
  - 6-REFERENCES HEREIN TO DEVELOPER OR OWNER SHALL APPLY TO BOTH, AND ANY SUCH REFERENCE SHALL ALSO APPLY TO SUCCESSORS, AGENTS, AND ASSIGNS.
  - 7-NO CITY MAINTENANCE SHALL BE PROVIDED FOR STREETS AND IMPROVEMENTS DESIGNATED AS "PRIVATE" ON THIS PLAT.
  - 8-A GEOTECHNICAL REPORT HAS BEEN COMPLETED BY \_\_\_\_\_ WHICH ADDRESSES SOIL AND GROUNDWATER CONDITIONS, PROVIDES ENGINEERING DESIGN CRITERIA, AND RECOMMENDS MITIGATION MEASURES IF PROBLEMATIC CONDITIONS WERE ENCOUNTERED. THE CITY ASSUMES NO LIABILITY OR RESPONSIBILITY FOR ANY RELIANCE OR LACK THEREOF IN THE REPORT.
  - 9-AGRICULTURAL USES, OPERATIONS, AND RIGHTS ARE ADJACENT TO OR NEAR THE PLAT AND LOTS. THE LOTS IN THIS PLAT ARE SUBJECT TO THE SIGHTS, SOUNDS, ODORS, NUISANCES, AND ASPECTS ASSOCIATED WITH AGRICULTURAL OPERATIONS, USES, AND RIGHTS. THESE USES AND OPERATIONS MAY OCCUR AT ALL TIMES OF THE DAY AND NIGHT INCLUDING WEEKENDS AND HOLIDAYS. THE CITY IS NOT RESPONSIBLE OR LIABLE FOR THESE USES AND IMPACTS AND WILL NOT RESTRICT ANY GRANDFATHERED AGRICULTURAL USE FROM CONTINUING TO OCCUR LAWFULLY.
  - 10-DRAINAGE ACROSS PROPERTY LINES SHALL NOT EXCEED THAT WHICH EXISTED PRIOR TO GRADING. EXCESS OR CONCENTRATED DRAINAGE SHALL BE CONTAINED ON SITE OR DIRECTED TO AN APPROVED DRAINAGE FACILITY. LOTS SHALL BE GRADED TO DRAIN SURFACE WATER AWAY FROM FOUNDATION WALLS. THE GRADE SHALL FALL A MINIMUM OF 6 INCHES WITHIN THE FIRST 10 FEET.
  - 11-PARKSTRIPS SHALL BE LANDSCAPED AND MAINTAINED BY THE PROPERTY OWNER WHO ABUTS THE PARKSTRIP, UNLESS OTHERWISE NOTED ON AN APPROVED AND RECORDED SUBDIVISION PLAT.
  - 12-ANY UTILITIES LOCATED OUTSIDE THE PUBLIC RIGHT-OF-WAY SHALL BE PRIVATELY OWNED AND MAINTAINED, UNLESS OTHERWISE SPECIFIED ON THE PLAT AND ACCOMPANIED BY RECORDED MAINTENANCE EASEMENTS IN FAVOR OF THE CITY.
  - 13-ALL OPEN SPACE, COMMON SPACE, AND TRAIL IMPROVEMENTS LOCATED HEREIN ARE TO BE INSTALLED BY OWNER AND MAINTAINED BY A HOMEOWNERS ASSOCIATION UNLESS SPECIFIED OTHERWISE ON EACH IMPROVEMENT.
  - 14-TRAILS WITHIN PRIVATE OPEN SPACE OR COMMON SPACE ARE PUBLIC ACCESS EASEMENTS AND TRAIL SURFACE SHALL BE MAINTAINED BY NORTHLAKE MEADOWS HOA.
  - 15-LOTS/UNITS MAY BE SUBJECT TO ASSOCIATION BYLAWS, ARTICLES OF INCORPORATION AND COAR'S."
  - 16-COMMON AREAS ARE CONVEYED TO NORTHLAKE MEADOWS HOA.
  - 17-COMMON AREAS AND LIMITED COMMON AREAS ARE BLANKET UTILITY EASEMENTS FOR THE CITY OF SARATOGA SPRINGS AND ARE ASSIGNABLE AT THE CITY'S DISCRETION TO OTHER PUBLIC UTILITIES.
  - 18-FENCES SURROUNDING OPEN SPACES SHALL BE INSTALLED, OWNED, AND MAINTAINED BY THE HOME OWNERS ASSOCIATION. THE FENCE SHALL BE SEMI-PRIVATE AND ARE TO CONFORM TO SARATOGA SPRINGS CODE 19.06.09(B).
  - 19-CAUTION: SHALLOW SEWER DEPTHS: CONTRACTOR TO VERIFY SEWER LATERAL DEPTH AND SET FOUNDATION ELEVATION TO PROVIDE ADEQUATE FALL INTO SEWER MAIN. BUILDINGS WITH A BASEMENT MAY NOT HAVE SEWER SERVICE AVAILABLE TO BASEMENT.
  - 20-ALL BUILDINGS AND STRUCTURES INTENDED FOR HUMAN OCCUPANCY SHALL BE CONSTRUCTED AT LEAST ONE (1) FOOT ABOVE THE BASE FLOOD ELEVATION OF ZONE AE AS DEFINED ON THE FEMA FLOOD INSURANCE RATE MAP WITH MAP NUMBER 49049C0285F, DATED JUNE 19, 2020.

INFORMATION	ACRES	SQUARE FEET	% TOTAL	#
TOTAL AREA	5.962	259,723	100	--
TOTAL LOTS	--	--	--	19
BUILDABLE LAND	4.112	179,101	68.97	--
SENSITIVE LANDS	0	0	0	--
OPEN SPACE	0.667	29,055	11.18	2
RIGHT-OF-WAY	1.184	51,568	19.85	1
NET DENSITY (DWELLING/ACRE)	3.187			

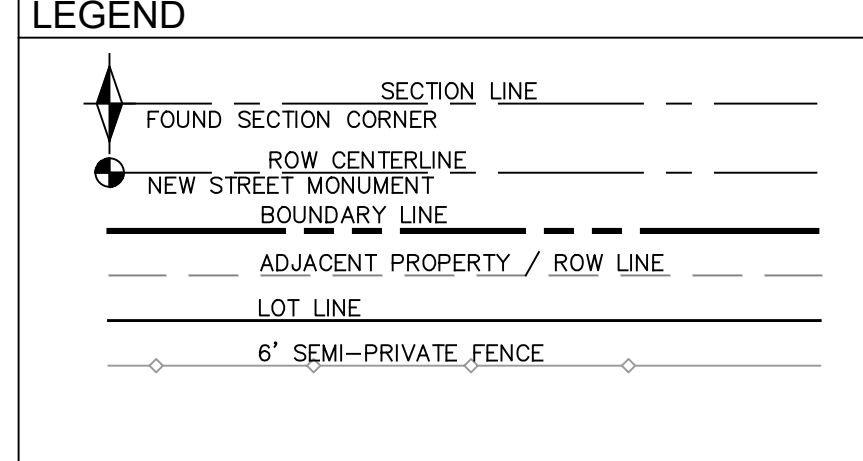


C:\DATA\25176 North Lake Meadows PH 2\dwg\25176 Plat.dwg  
PLOT DATE: Feb 13, 2026



DEVELOPER:  
ANDERSON DEVELOPMENT SERVICES  
CONTACT: KEATON MORTON  
801-856-2478  
KEATON@AND-DEV.COM

OWNER:  
ALPINE HOMES  
CONTACT: AUSTIN RICHARDS  
801-838-9990  
ARICHARDS@ALPINEHOMES.COM



### SURVEYOR'S CERTIFICATE

I, KAGAN M. DIXON, DO HEREBY CERTIFY THAT I AM A PROFESSIONAL LAND SURVEYOR AND THAT I HOLD LICENSE NO. 9061091 IN ACCORDANCE WITH THE PROFESSIONAL ENGINEERS AND LAND SURVEYORS LICENSING ACT FOUND IN TITLE 58, CHAPTER 22 OF THE UTAH CODE. I FURTHER CERTIFY THAT BY AUTHORITY OF THE OWNERS, I HAVE MADE A SURVEY OF THE TRACT OF LAND SHOWN ON THIS PLAT AND DESCRIBED BELOW, HAVE SUBDIVIDED SAID TRACT OF LAND INTO LOTS, STREETS, AND EASEMENTS, HAVE COMPLETED A SURVEY OF THE PROPERTY DESCRIBED ON THIS PLAT IN ACCORDANCE WITH UTAH CODE SECTION 17-79-703, HAVE VERIFIED ALL MEASUREMENTS, AND HAVE PLACED MONUMENTS AS REPRESENTED ON THE PLAT. I FURTHER CERTIFY THAT EVERY EXISTING RIGHT-OF-WAY AND EASEMENT GRANT OF RECORD FOR UNDERGROUND FACILITIES, AS DEFINED IN UTAH CODE SECTION 54-8A-2, AND FOR OTHER UTILITY FACILITIES, IS ACCURATELY DESCRIBED ON THIS PLAT, AND THAT THIS PLAT IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE. I ALSO CERTIFY THAT I HAVE FILED, OR WILL FILE WITHIN 90 DAYS OF THE RECORDATION OF THIS PLAT, A MAP OF THE SURVEY I HAVE COMPLETED WITH THE UTAH COUNTY SURVEYOR.

### BOUNDARY DESCRIPTION

A TRACT OF LAND BEING SITUATE IN THE SOUTH HALF OF SECTION 19, TOWNSHIP 5 SOUTH, RANGE 1 EAST, SALT LAKE BASE AND MERIDIAN, SAID TRACT OF LAND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT ALONG THE EAST BOUNDARY LINE OF NORTH LAKE MEADOWS PHASE 1 WHICH IS SOUTH 89°30'39" WEST ALONG THE SECTION LINE A DISTANCE OF 297.81 AND NORTH 102.60 FEET FROM THE SOUTH QUARTER CORNER OF SAID SECTION 19, AND RUNNING THENCE ALONG SAID BOUNDARY LINE OF NORTH LAKE MEADOWS PHASE 1 THE FOLLOWING (4) COURSES: (1) NORTH 01°17'51" WEST 302.68 FEET; (2) EAST 3.49 FEET; (3) NORTH 02°20'6" EAST 152.83 FEET; (4) SOUTH 89°57'56" EAST 565.32 FEET; THENCE SOUTH 03°32'28" WEST 464.96 FEET; THENCE NORTH 89°00'18" WEST 561.08 FEET TO THE POINT OF BEGINNING.

CONTAINS 259,723 SQUARE FEET  
5.962 ACRES, MORE OR LESS.  
19 LOTS

### OWNERS' DEDICATION

KNOW ALL MEN BY THESE PRESENTS THAT THE UNDERSIGNED OWNER(S) OF THE ABOVE DESCRIBED TRACT OF LAND HAVE CAUSED THE SAME TO BE SUBDIVIDED INTO LOTS, PARCELS, AND STREETS, TOGETHER WITH EASEMENTS AND RIGHTS-OF-WAY, TO BE HEREAFTER KNOWN AS

### MEADOWS AT NORTHLAKE SUBDIVISION

AND DO HEREBY DEDICATE FOR PERPETUAL USE OF THE PUBLIC AND CITY ALL PARCELS, LOTS, STREETS, EASEMENTS, RIGHTS-OF-WAY, AND PUBLIC AMENITIES SHOWN ON THIS PLAT AS INTENDED FOR PUBLIC OR CITY USE. THE OWNER(S) VOLUNTARILY DEFEND, INDEMNIFY, AND SAVE HARMLESS THE CITY AGAINST ANY EASEMENTS OR OTHER ENCUMBRANCE ON A DEDICATED STREET WHICH WILL INTERFERE WITH THE CITY'S USE, MAINTENANCE, AND OPERATION OF THE STREET. THE OWNER(S) AND ALL AGENTS, SUCCESSORS, AND ASSIGNS VOLUNTARILY DEFEND, INDEMNIFY, AND HOLD HARMLESS THE CITY FROM ANY CLAIM ARISING FROM THE OWNER'S CREATION OF THIS SUBDIVISION, THE ALTERATION OF THE GROUND SURFACE, VEGETATION, DRAINAGE, OR SURFACE OR SUB-SURFACE WATER FLOWS WITHIN THIS SUBDIVISION, AND THE DEVELOPMENT ACTIVITY WITHIN THIS SUBDIVISION BY THE OWNERS AND ALL AGENTS, SUCCESSORS, AND ASSIGNS. THE OWNER HEREBY CONVEYS THE COMMON AREA, AS INDICATED HEREON, TO THE ADDRESS OF \_\_\_\_\_ A UTAH NONPROFIT CORPORATION, WITH A REGISTERED ADDRESS OF \_\_\_\_\_

IN WITNESS WHEREOF I HAVE SET FORTH MY HAND THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D. 20\_\_\_\_

SIGNATURE \_\_\_\_\_ PRINT NAME \_\_\_\_\_ MANAGER \_\_\_\_\_  
ALPINE HOMES

### OWNER'S ACKNOWLEDGEMENT

STATE OF UTAH \_\_\_\_\_ } S.S.  
COUNTY OF \_\_\_\_\_ } S.S.

ON THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D. 20\_\_\_\_, PERSONALLY APPEARED BEFORE ME \_\_\_\_\_, WHO BEING BY ME DULY SWORN, DID PROVIDE TO ME ON THE BASIS OF SATISFACTORY EVIDENCE TO BE THE PERSON(S) WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE WITHIN INSTRUMENT, AND ACKNOWLEDGED TO ME THAT HE/SHE/HEY EXECUTED THE SAME IN HIS/HER/THEIR AUTHORIZED CAPACITY(IES), AND THAT BY HIS/HER/THEIR SIGNATURE(S) ON THE INSTRUMENT THE PERSON(S), OR THE ENTITY UPON BEHALF OF WHOM THE PERSON(S) ACTED, EXECUTED THIS PLAT WITH FULL AUTHORITY OF THE OWNER(S).

WITNESS MY HAND OFFICIAL SEAL.

I CERTIFY UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE STATE OF UTAH THAT THE FOREGOING PARAGRAPH IS TRUE AND CORRECT.

SIGNATURE: \_\_\_\_\_  
NOTARY PUBLIC FULL NAME: \_\_\_\_\_  
COMMISSION NUMBER: \_\_\_\_\_  
MY COMMISSION EXPIRES: \_\_\_\_\_  
A NOTARY PUBLIC COMMISSIONED IN UTAH

### APPROVAL BY LEGISLATIVE BODY

THE CITY COUNCIL OF THE CITY OF SARATOGA SPRINGS, COUNTY OF UTAH, APPROVES THIS SUBDIVISION SUBJECT TO THE CONDITIONS AND RESTRICTIONS STATED HEREON, AND HEREBY ACCEPTS THE DEDICATION OF ALL STREETS, EASEMENTS, AND OTHER PARCELS OF LAND INTENDED FOR THE PUBLIC PURPOSE OF THE PERPETUAL USE OF THE PUBLIC.

THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D. 20\_\_\_\_,

CITY MAYOR \_\_\_\_\_ ATTEST \_\_\_\_\_ CITY RECORDER (SEE SEAL BELOW)

<h4>PLANNING DIRECTOR</h4> <p>APPROVED BY THE PLANNING DIRECTOR ON THIS _____ DAY OF _____, A.D. 20____</p> <p>PLANNING DIRECTOR _____</p>	<h4>LAND USE AUTHORITY</h4> <p>APPROVED BY THE LAND USE AUTHORITY ON THIS _____ DAY OF _____, A.D. 20____</p> <p>LAND USE AUTHORITY _____</p>	<h4>SARATOGA SPRINGS ATTORNEY</h4> <p>APPROVED BY SARATOGA SPRINGS ATTORNEY ON THIS _____ DAY OF _____, A.D. 20____</p> <p>SARATOGA SPRINGS ATTORNEY _____</p>
<h4>CITY ENGINEER</h4> <p>APPROVED BY THE CITY ENGINEER ON THIS _____ DAY OF _____, A.D. 20____</p> <p>CITY ENGINEER _____</p>	<h4>FIRE CHIEF</h4> <p>APPROVED BY THE FIRE CHIEF ON THIS _____ DAY OF _____, A.D. 20____</p> <p>CITY FIRE CHIEF _____</p>	<h4>LEHI CITY POST OFFICE</h4> <p>APPROVED BY POST OFFICE REPRESENTATIVE ON THIS _____ DAY OF _____, A.D. 20____</p> <p>LEHI CITY POST OFFICE REPRESENTATIVE _____</p>

### MEADOWS AT NORTHLAKE SUBDIVISION

LOCATED IN THE SOUTH HALF OF SECTION 19, TOWNSHIP 5 SOUTH, RANGE 1 EAST, SALT LAKE BASE AND MERIDIAN, CITY OF SARATOGA SPRINGS, UTAH COUNTY, UTAH

SURVEYOR'S SEAL	CITY ENGINEER SEAL	CLERK-RECORDER SEAL
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## **APPLICATION REVIEW CHECKLIST**

### **Application Information**

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#### **Preliminary & Final Plat Meadows at North Lake**

Applicant:	Keaton Morton
Owner:	Barbara M. Jones Trust
Location:	9150 W 7350 N
Project Acreage:	5.962 acres
Residential Units/Lots:	19 Lots
Current Use:	Agricultural
Requested Land Use:	Low Density Residential
Requested Zoning:	R1-9
Adjacent Zoning:	R1-9 & A
Adjacent Uses:	Low Density Residential & Natural Open Space
Past Action and Date:	Annexation & Rezone Must Be Approved
Type of Action:	Administrative
Land Use Authority:	Planning Commission
Planner:	Sam Stout, Planner II
Floodplain:	Yes

<b>Date Received:</b>	<b>1<sup>st</sup> submittal:</b> November 17, 2025
	<b>2<sup>nd</sup> submittal:</b> December 23, 2025
	<b>3<sup>rd</sup> submittal:</b> January 28, 2026
	<b>4<sup>th</sup> submittal:</b> Click or tap to enter a date.
<b>Date of Review:</b>	<b>1<sup>st</sup> submittal:</b> December 10, 2025
	<b>2<sup>nd</sup> submittal:</b> January 15, 2026
	<b>3<sup>rd</sup> submittal:</b> February 11, 2026
	<b>4<sup>th</sup> submittal:</b> Click or tap to enter a date.
<b>Parcel Number(s) and size:</b>	13-028-0036/6.01 acres

### **Section 19.13 – Application Submittal**

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- Application Complete (Date of Payment): November 17, 2025
- Rezone Required: Yes
- General Plan Amendment required: Yes

### **Section 19.13.04 – Process**

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- Required Meetings: Planning Commission

- Planning Director Approval: Yes
- Public Hearing Required – check 19.13.04 process table: No
- Neighborhood Meeting Required: No

## DRC Review

### DRC Review Comments:

#### North Lake Meadows Phase 2 Prelim/Final – (10-minute discussion)

- Need a new identifying name per the Recorder’s office
- Private Open Space needs to be labeled

## Code Review

- 19.04, Land Use Zones
  - Zone: R1-9
  - General Plan Land Use: Low Density Residential
  - Density: 19 Units / 6.01 Acres, 3.16 units per acre

19.04.010 Requirements		R1-9	
Category To Be Reviewed	Regulation	Compliance	Findings
Maximum Units per Acre	4 unit/acre	<b>Complies</b>	<i>3.16 units/acre</i>
Lot Size, Residential (Minimum)	9,000 sq. ft.	<b>Complies</b>	<i>&gt;9,000 sq. ft.</i>
Lot Size, Non-Residential (Minimum)	30,000 sq. ft.	<b>N/A</b>	
Footprint Development	allowed	<b>N/A</b>	<i>None proposed.</i>
Building Separation - Footprint (Minimum)	10'	<b>Can Comply</b>	<i>TBD at building permit</i>
Project Size - Footprint (Minimum)	5 acres	<b>Complies</b>	<i>6.01 acres</i>
Lot Coverage (Maximum)	50%	<b>Can Comply</b>	<i>TBD at building permit</i>
Structure Height (Minimum)	35'	<b>Can Comply</b>	<i>TBD at building permit</i>
Dwelling Size (Minimum)	1,000 sq. ft.	<b>Can Comply</b>	<i>TBD at building permit</i>
Lot Width (Minimum)	70'	<b>Complies</b>	<i>&gt;70'</i>
Lot Frontage*** (Minimum)	35'	<b>Complies</b>	<i>&gt;35'</i>
Open Space Frontage (Minimum)	35'	<b>Complies</b>	<i>&gt;35'</i>
Front Setback* (Minimum)	25', 20' for enclosed entry or porches	<b>Complies</b>	<i>Shown on plat</i>
Street Side Setback (Minimum)	20'	<b>Complies</b>	<i>Shown on plat</i>
Interior Side Setback, Residential(Minimum)	8'/16'(min/combined)	<b>Complies</b>	<i>Shown on plat</i>
Interior Side Setback, Non-Residential(Minimum)	25'	<b>N/A</b>	<i>Not Applicable</i>
Rear* Setback, Residential(Minimum)	20'	<b>Complies</b>	<i>Shown on plat</i>
Rear Setback, Non-Residential(Minimum)	20'	<b>N/A</b>	<i>Not Applicable</i>

No density shall be calculated on sensitive lands.	<b>Complies.</b>	<i>Shown in file</i>
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## 19.05 Supplemental Regulations

Regulation	Compliance	Findings
<b>Flood Plain:</b> All buildings and structures intended for human occupancy shall be constructed at least one (1) foot above the base flood elevation of Zone A as defined on the FEMA Flood Insurance Map.	<b>Complies.</b>	<i>Meets Engineering Standards</i>
<b>Water &amp; Sewage:</b> Each lot shall be connected to City water and sewer.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<b>Transportation Master Plan:</b> No building lot shall be created and no structure shall be erected within the location of a proposed street, road, highway, or right-of-way as shown on the City's currently-approved Transportation Master Plan.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<b>Property Access -</b> All lots shall abut a dedicated public street or highway or a private roadway.	<b>Complies.</b>	<i>Shown in file</i>
<b>19.05.16. Special Standards and Considerations Governing Particular Uses. See Code for details</b>	N/A.	<i>Not Applicable</i>
Automobile refueling stations and car wash operations.	N/A.	<i>Not Applicable</i>
Automobile Repair, Minor:	N/A.	<i>Not Applicable</i>
Car Wash (full service).	N/A.	<i>Not Applicable</i>
Hotels.	N/A.	<i>Not Applicable</i>
Kennel, Private.	N/A.	<i>Not Applicable</i>
Storage, Self-Storage, or Mini-Storage Units.	N/A.	<i>Not Applicable</i>
Vehicle Storage.	N/A.	<i>Not Applicable</i>
Public and Private Utility Building or Facility and Public Building Sites.	N/A.	<i>Not Applicable</i>
Bars.	N/A.	<i>Not Applicable</i>

## 19.06 Landscaping and Fencing

### General Provisions

Drainage across property lines shall follow International Residential Code (IRC) and International Building Code (IBC), as of 2024 (shall drain onto own property).	<b>Shall Comply.</b>	
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### Landscape Plans

Regulation	Compliance	Findings
<b>Landscape Architect:</b> Landscaped plans shall be prepared by a licensed landscape architect.	N/A.	<i>Not applicable to this application</i>
<b>Existing Conditions:</b> Show the location and dimension of all existing and proposed structures, property lines, easements, parking lots, power lines, rights-of-way, ground signs, refuse areas, and lighting.	N/A.	<i>Not applicable to this application</i>
<b>Planting Plan:</b> Show location and planting details for all proposed vegetation and materials. Indicate the size of the plant material at maturation. All existing vegetation that will be removed or remain must be identified.	N/A.	<i>Not applicable to this application</i>

<b>Plants:</b> The name (both botanical and common name), quantity, and size of all proposed plants.	N/A.	<i>Not applicable to this application</i>
<b>Topography:</b> Existing and proposed grading of the site indicating contours at two-foot intervals.	N/A.	<i>Not applicable to this application</i>
<b>Irrigation:</b> Irrigation plans showing the system layout and details.	N/A.	<i>Not applicable to this application</i>
<b>Fencing:</b> Location, style, and details for proposed and existing fences and identification of the fencing materials.	N/A.	<i>Not applicable to this application</i>
<b>Data Table:</b> Table including the total number of each plant type, and total square footage and percentage of landscaped areas, domestic turf grasses, decorative rock, mulch, bark, and drought tolerant plant species.	N/A.	<i>Not applicable to this application</i>
<b>Planting Standards</b>		
<b>Deciduous Trees:</b> Minimum 2" in caliper.	N/A.	<i>Not applicable to this application</i>
<b>Evergreen Trees:</b> Minimum 6' in height.	N/A.	<i>Not applicable to this application</i>
<b>Tree Base Clearance:</b> 3' diameter around every tree must be kept clear of turf and rock mulch. In parking lot islands and other narrow strips where turf two feet or less in width would otherwise occur, this clear area may be reduced to 2'.	N/A.	<i>Not applicable to this application</i>
<b>Shrubs:</b> 25% of required shrubs must be a minimum of 5 gallons in size at time of installation; all other required shrubs shall be a minimum of 1 gallon in size.	N/A.	<i>Not applicable to this application</i>
<b>Turf:</b> No landscaping shall be comprised of more than seventy percent turf, except within landscaped parks.	N/A.	<i>Not applicable to this application</i>
<b>Artificial Turf :</b> Shall require a building permit shall meet the following requirements with documentation provided by home owner or contractor: <ul style="list-style-type: none"> <li>i. Artificial turf shall have a minimum eight-year “no-fade” warranty;</li> <li>ii. Multi-color blend needed to mimic real grass</li> <li>iii. Must be installed according to manufacturer’s instructions;</li> <li>iv. Minimum pile height shall be 1.5 inches, minimum face/pile weight shall be at least 50 oz. per square yard, and water permeability shall be at least 19 inches per hour;</li> <li>v. Landscape plan shall provide for a minimum of 25 percent live vegetation coverage to comply with residential front and/or street side yard landscape standards;</li> <li>vi. Landscaping shall maintain a five-foot diameter around trees free from roc, debris, or artificial turf;</li> <li>vii. Rubber or inorganic infill is prohibited while organic infill is permitted;</li> <li>viii. The term “artificial turf” shall not be construed to include artificial trees, shrubs or bushes; and</li> <li>ix. Storm water runoff shall be maintained on-site.</li> </ul>	N/A.	<i>Not applicable to this application</i>
<b>Drought Tolerant Plants:</b> 50% of all trees and shrubs shall be drought tolerant.	N/A.	<i>Not applicable to this application</i>
<b>Rock Mulch:</b> Rock mulch shall be two separate colors and separate sizes and must be contrasting in color from the pavement and other hard surfaces. All colors used must be earth tones.	N/A.	<i>Not applicable to this application</i>
<b>Design Requirements</b>		
<b>Evergreens:</b> Evergreens shall be incorporated into landscaped treatment of sites where screening and buffering are required.	N/A.	<i>Not applicable to this application</i>

<b>Softening of Walls and Fences:</b> Plants shall be placed intermittently against long expanses of building walls, fences, and barriers to create a softening effect.	N/A.	<i>Not applicable to this application</i>
<b>Planting and Shrub Beds:</b> Planting and shrub beds are encouraged to be used in order to conserve water.	N/A.	<i>Not applicable to this application</i>
<b>Water Conservation:</b> Water-conserving sprinkler heads and rain sensors are required. Drip lines should be used for shrubs and trees.	N/A.	<i>Not applicable to this application</i>
<b>Energy Conservation:</b> Placement of plants shall be designed to reduce energy consumption. Deciduous trees are encouraged to be planted on the south and west sides of structures. Evergreens are encouraged to be planted on the north side of structures.	N/A.	<i>Not applicable to this application</i>
<b>Placement:</b> Whenever possible, landscaping shall be placed immediately adjacent to structures, particularly where proposed structures have large empty walls.	N/A.	<i>Not applicable to this application</i>
<b>Trees and Power Poles:</b> No trees shall be planted directly under or within 10' of power lines, poles, or utility structures unless: <ul style="list-style-type: none"> <li>a. The Land Use Authority gives its approval.</li> <li>b. The Power Company or owner of the power line gives written consent.</li> <li>c. The maximum height or width at maturity of the tree species planted is less than 5' to any pole, line, or structure.</li> </ul>	N/A.	<i>Not applicable to this application</i>
<b>Preservation of Existing Vegetation</b>		
Where possible and appropriate, existing native vegetation must be incorporated into the landscape treatment of the proposed site.	N/A.	<i>Not applicable to this application</i>
<b>Tree Preservation:</b> Existing mature evergreen trees of 16' in height or greater, and existing mature deciduous or decorative trees of more than 4" in caliper, shall be identified on the landscape plan and preserved if possible. If a mature tree is preserved, an area around the roots as wide as the existing canopy shall not be disturbed.	N/A.	<i>Not applicable to this application</i>
If preservation is not possible, the required number of trees shall be increased by double the number of such trees removed.	N/A.	<i>Not applicable to this application</i>
The replacement trees for evergreen trees shall be evergreens, and for deciduous shall be deciduous.	N/A.	<i>Not applicable to this application</i>
Deciduous trees smaller than 4" inches in caliper, or mature ornamental trees, that are removed shall be replaced on a 1-to-1 ratio.	N/A.	<i>Not applicable to this application</i>
Replacement trees shall be in addition to the minimum tree requirements of this Chapter, and shall comply with minimum sizes as outlined in the Chapter.	N/A.	<i>Not applicable to this application</i>
<b>Planter Beds</b>		
<b>Weed Barrier:</b> A high quality weed barrier or pre-emergent shall be used.	N/A.	<i>Not applicable to this application</i>
<b>Materials:</b> High quality materials such as wood chips, wood mulch, ground cover, decorative rock, landscaping rocks, or similar materials shall be used, and materials must be heavy enough to not blow away in the wind.	N/A.	<i>Not applicable to this application</i>
<b>Edging:</b> Concrete edging must be used to separate planter and turf areas in all non-residential zones.	N/A.	<i>Not applicable to this application</i>
<b>Drip Lines:</b> Drip lines must be used in planter beds.	N/A.	<i>Not applicable to this application</i>
<b>Fencing and Screening</b>		
<b>Front Yards:</b> Fences exceeding 3' in height shall not be erected in any front yard space of any residential lot.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>

<p><b>Clear Sight Triangle:</b> All landscaping and fencing shall be limited to a height of not more than 3’ and the grade at such intersections shall not be bermed or raised and comply with AASHTO Standards.</p>	<p><b>Shall Comply.</b></p>	<p><i>Each lot shall comply</i></p>
<p><b>Street side yards:</b> fencing in street side yards adjacent to a driveway shall not exceed three feet for a distance of 15 feet back from the intersection of driveway and sidewalk, or driveway and property line where no sidewalk exists as shown in the drawing below. Fencing shall also comply with all other clear sight triangle requirements as stated in 19.06.</p>	<p><b>Shall Comply.</b></p>	<p><i>Each lot shall comply</i></p>
<p><b>Retaining walls:</b> for construction of retaining walls four feet or taller in height of unbalanced fill or for any wall supporting surcharge loads, a building permit must be obtained. Prior to construction of retaining walls, Chapter 18 of the City Code shall be consulted to determine if a grading permit is also required. Where there is a difference in elevation on opposite sides of the fence, the height of the fence shall be measured from the ground level on the highest side of the wall when the fence is placed on top of the wall. If the fence is placed at the bottom of the wall, the fence height shall be measured from the ground it is placed upon and there shall be at least 2 feet to access and maintain the retaining wall. <b>Retaining walls shall follow all applicable regulations outlined in Chapter 19.10 of the City Code, regardless of slope.</b></p>	<p><b>Shall Comply.</b></p>	<p><i>Each lot shall comply</i></p>
<p><b>Height:</b> Approval of fences over 6 feet in height will be determined on a case-by-case basis by the City Council for all new developments if fencing is proposed during the subdivision review process, or by the Planning Director for all developments that have received final approval; however, in no case will a fence be allowed to exceed eight feet in height. The following criteria shall be applied in making this determination:</p> <ul style="list-style-type: none"> <li>a. compatibility with fences of surrounding uses;</li> <li>b. quality of proposed materials;</li> <li>c. aesthetics of proposed materials;</li> <li>d. requirements of applicable development agreements;</li> <li>e. intensity of existing surrounding uses; and</li> <li>f. applicable conditions of approval.</li> </ul>	<p><b>Shall Comply.</b></p>	<p><i>Each lot shall comply</i></p>
<p><b>Prohibited fencing:</b></p> <ul style="list-style-type: none"> <li>a. No barbed wire, chain link, razor, or wire (agricultural, electric, chicken wire, mesh wire, hog fencing, etc.) fences shall be allowed. This does not apply to chain link or wire fences if the fence: (1) is not being used to delineate lot boundaries; and (2) is being used for Agricultural uses or otherwise for the keeping of animals; and (3) does not occupy more than 50% of any residential yard; or (4) is for back stops, sports fields, or sport court fencing within a public or private park.</li> <li>b. No fencing that parallels existing fencing shall be permitted within an existing fenced yard. Exceptions: interior fencing to enclose chickens, bees, or other livestock as otherwise specifically permitted under this Code, and any fencing of three feet or less in height within an existing fenced yard.</li> </ul>	<p><b>Shall Comply.</b></p>	<p><i>Each lot shall comply</i></p>
<p><b>Double frontages:</b> where lots have frontages onto more than one street, that area designated by the property owner as the rear yard may have a solid or view obstructing fence, wall, or hedge not exceeding 6 feet in height. Where the double frontage lot is also a corner lot (3 frontages), clear sight across corner property shall be required and enforced. See Section 19.06.11, Clear Sight Triangles.</p>	<p><b>Shall Comply.</b></p>	<p><i>Each applicable lot shall comply</i></p>
<p><b>Non-residential and Multi-family:</b> fencing and other screening materials for multifamily, residential, commercial, or industrial projects</p>	<p><b>N/A.</b></p>	<p><i>Not applicable to this application</i></p>

must receive approval by the Land Use Authority through the Site Plan review process. See Chapter 19.13 for Site Plan review requirements. In addition, the following criteria shall be applied: a. compatibility with fences of surrounding uses; b. quality of proposed materials; c. aesthetics of proposed materials; d. requirements of applicable development agreements; e. intensity of existing surrounding uses; and f. applicable conditions of approval.		
<b>Required Residential Fencing:</b> Fencing in residential development shall be placed along property lines abutting open space, parks, canals, and trails. In addition, fencing may also be required adjacent to undeveloped properties.	<b>Complies.</b>	<i>Shown in file</i>
Fences along open space, parks, canals, and trails shall be semi-private. Exception: privacy fencing is permitted for property lines abutting trail corridors that are not City maintained and are both adjacent to and visible from an arterial.	<b>Complies.</b>	<i>Shown in file</i>
Fencing along arterial roads shall be of a consistent material and color within each development.	<b>Complies.</b>	<i>Shown in Construction Details.</i>
Fencing along open space, parks, and trails may be less than 6' in height but shall not be less than 3' in height, at the discretion of the property owner or HOA as applicable.	<b>Complies.</b>	<i>Shown in Construction Details.</i>
<b>Screening at Boundaries of Residential Zones:</b> For residential developments, abutting active agricultural property or operations, a solid fence or wall shall be installed and maintained along the abutting property line.	<b>Complies.</b>	<i>Shown in Construction Details.</i>
<b>Amount of Required Landscaping</b>		
Portions of the property that are not developed with structures, rights of ways, or parking areas shall be required to be landscaped per the definition of Landscaping in Section 19.02 in all land use zones.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
Multi-family, common space not including parks and nonresidential development in all zones shall be required to adhere to the minimum landscaped standards in 19.06.07 of the Land Development Code.	N/A.	<i>Not applicable to this application</i>
At least 50% of the landscaped area shall be covered with live vegetation at maturity, including shrubs, grasses, flowers, tree and shrub canopies and other live vegetation. The percentage may be reduced to 40% in areas where bark mulch, wood or plant fiber mulch, or rubber mulch is used instead of rock mulch.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>

<b>19.11 Lighting</b>		
<b>General Standards</b>		
<b>Regulation</b>	<b>Compliance</b>	<b>Findings</b>
<b>Material:</b> All Lighting Fixtures and assemblies shall be metal.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Base:</b> All lighting poles shall have a 16" decorative base.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Type:</b> All lighting fixtures shall be of the full cutoff variety. Shoebox fixtures are prohibited.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Angle:</b> Shall be directed downward.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Lamp:</b> Bulbs may not exceed 4000k.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Drawings:</b> Design and location of fixtures shall be specified on the plans.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>

<b>Flags:</b> The United States flag and the state flag shall be permitted to be illuminated from dusk till dawn. All other flags shall not be illuminated past 11:00 p.m. Flag lighting sources shall not exceed 10,000 lumens per flagpole. The light source shall have a beam spread no greater than necessary to illuminate the flag.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Prohibited Lighting:</b> Searchlights, strobe lights and any laser source light or any similar high intensity light.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Descriptions:</b> Descriptions of the illuminating devices, fixtures, lamp supports, and other devices. This description may include, but is not limited to, manufacturers' specifications, drawings, and sections.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Residential Lighting</b>		
<b>Floodlights:</b> Floodlights are prohibited.	Item.	
<b>Street Lighting:</b> All street lighting fixtures shall be metal and black and also include an arm and bell shade or a pole and lantern configuration and meet the City Standards.	<b>Complies.</b>	<i>Shown in file</i>
<b>Building Lighting:</b> Building lighting shall be full cutoff and downward directed, and only for the illumination of vertical surfaces such as building facades and signs, and shall not cast illumination beyond the surface being illuminated.	<b>Shall Comply.</b>	<i>Each lot shall comply</i>
<b>Single Family:</b> Homes located less than one half mile from Camp Williams shall have full cutoff lighting that does not exceed 90, and no upward lighting is permitted.	<b>N/A.</b>	<i>Not Applicable</i>
<b>Walkway Lighting</b>		
Lighting of all pedestrian pathways is recommended.	<b>N/A.</b>	<i>Not Applicable</i>
All pathway, walkway, and sidewalk lighting fixtures shall be mounted at a height not to exceed 10'. i. Themed walkway lighting within Regional Parks shall not exceed a height of 25'. Such lighting within 200' of residential development shall not exceed 16'.	<b>N/A.</b>	<i>Not Applicable</i>
Bollard lighting shall be limited to a height of 4'.	<b>N/A.</b>	<i>Not Applicable</i>
<b>Lighting Plan</b>		
Plans indicating the location and types of illuminating devices on the premises.	<b>N/A.</b>	<i>Not Applicable</i>
Descriptions of the illuminating devices, fixtures, lamp supports, and other devices. This description may include, but is not limited to, manufacturers' specifications, drawings, and sections.	<b>N/A.</b>	<i>Not Applicable</i>
Photometric sheet showing measurement of light intensity across the site and onto adjacent property in terms of candela, lumens, and foot-candles.	<b>N/A.</b>	<i>Not Applicable</i>

<b>19.12 Subdivision</b>		
<b>Preliminary Plat Requirements</b>		
<b>Regulation</b>	<b>Compliance</b>	<b>Findings</b>
Standard Plat Format followed.	<b>Complies.</b>	<i>Shown on plat</i>
Name and address of property owner and developer.	<b>Complies.</b>	<i>Shown on plat</i>
Name of land surveyor.	<b>Complies.</b>	<i>Shown on plat</i>
The location of proposed subdivision with respect to surrounding property and street.	<b>Complies.</b>	<i>Shown on plat</i>
The name of all adjoining property owners of record, or names of adjoining developments.	<b>Complies.</b>	<i>Shown on plat</i>
The names and location of ROW widths of adjoining streets and all facilities within 100' of the platted property.	<b>Complies.</b>	<i>Shown on plat</i>

Street and road layout with centerline bearing and distance labels, dimensions, and names of existing and future streets and roads, (with all new names cleared through the City GIS Department).	<b>Shall Comply.</b>	<i>Needs to meet requirements from GIS Department</i>
Subdivision name cleared with Utah County.	<b>Shall Comply.</b>	<i>Needs to comply with County Recorder Office</i>
North arrow.	<b>Complies.</b>	<i>Shown on plat</i>
A tie to a permanent survey monument at a section corner.	<b>Complies.</b>	<i>Shown on plat</i>
The boundary lines of the project with bearings and distances and a legal description.	<b>Complies.</b>	<i>Shown on plat</i>
Layout and dimensions of proposed lots with lot area in square feet.	<b>Complies.</b>	<i>Shown on plat</i>
Location, dimensions, and labeling of roads, structures, irrigation features, drainage, parks, open space, trails, and recreational amenities.	<b>Complies.</b>	<i>Shown on plat</i>
Location of prominent natural features such as rock outcroppings, woodlands, steep slopes, etc.	<b>N/A.</b>	
Proposed road cross sections.	<b>Complies.</b>	<i>Shown on plat</i>
Proposed fencing.	<b>Complies.</b>	<i>Shown on plat</i>
Vicinity map.	<b>Complies.</b>	<i>Shown on plat</i>
All required signature blocks are on the plat.	<b>Complies.</b>	<i>Shown on plat</i>
Prepared by a professional engineer licensed in Utah.	<b>Complies.</b>	<i>Shown on plat</i>
Proposed methods for the protection or preservation of sensitive lands.	<b>Complies.</b>	<i>Shown on plat</i>
Location of any flood plains, wetlands, and other sensitive lands.	<b>Complies.</b>	<i>Shown on plat</i>
Location of 100-year high water marks of all lakes, rivers, and streams.	<b>N/A.</b>	<i>Not applicable to this application</i>
Projected Established Grade of all building lots.	<b>Complies.</b>	<i>Shown on plat</i>
A data table. 1. total project area; 2. total number of lots, dwellings, and buildings; 3. where buildings are included, square footage of proposed building footprints and, if multiple stories, square footage by floor; 4. for multi-family developments, the number of proposed garage parking spaces and number of proposed total parking spaces; 5. percentage of buildable land; 6. acreage of sensitive lands and percentage sensitive lands comprise of total project area and open space area; 7. area and percentage of open space or landscaping; 8. area to be dedicated as right-of-way (public and private); 9. net density of dwellings by acre (sensitive lands must be subtracted from base acreage).	<b>Complies.</b>	<i>Shown on plat</i>
<b>Phasing Plan:</b> Including a data table with the following Information for each phase: i. Subtotal area in square feet and acres; ii. number of lots or dwelling units; iii. open space area and percentage; iv. utility phasing plan; v. number of parking spaces; vi. recreational facilities to be provided; vii. overall plan showing existing, proposed, and remaining phases.	<b>N/A.</b>	<i>Not Applicable</i>
<b>Final Plat Requirements</b>		
Subdivision name and location.	<b>Complies.</b>	<i>Shown on plat</i>
Standard Plat Format followed.	<b>Complies.</b>	<i>Shown on plat</i>
Name and address of property owner and developer.	<b>Complies.</b>	<i>Shown on plat</i>
Name of land surveyor.	<b>Complies.</b>	<i>Shown on plat</i>
The location of proposed subdivision with respect to surrounding property and street.	<b>Complies.</b>	<i>Shown on plat</i>

The name of all adjoining property owners of record, or names of adjoining developments.	<b>Complies.</b>	<i>Shown on plat</i>
The names and location of ROW widths of adjoining streets and all facilities within 100' of the platted property.	<b>Complies.</b>	<i>Shown on plat</i>
North arrow.	<b>Complies.</b>	<i>Shown on plat</i>
A tie to a permanent survey monument at a section corner.	<b>Complies.</b>	<i>Shown on plat</i>
The boundary lines of the project with bearings and distances and a legal description with total project area in SF and acres.. -	<b>Complies.</b>	<i>Shown on plat</i>
Layout and dimensions of proposed lots with lot area in square feet and acres. Lot boundaries shall include dimensions and bearings.	<b>Complies.</b>	<i>Shown on plat</i>
Lot Numbers.	<b>Complies.</b>	<i>Shown on plat</i>
Location, dimensions, and labeling of roads, structures, irrigation features, drainage, parks, open space, trails, and recreational amenities.	<b>Complies.</b>	<i>Shown on plat</i>
Location of prominent natural features such as rock outcroppings, woodlands, steep slopes, etc.	<b>N/A.</b>	
Proposed road ROW widths.	<b>Complies.</b>	<i>Shown on plat</i>
Vicinity map.	<b>Complies.</b>	<i>Shown on plat</i>
All required signature blocks are on the plat.	<b>Complies.</b>	<i>Shown on plat</i>
Prepared by a professional engineer licensed in Utah.	<b>Complies.</b>	<i>Shown on plat</i>
Proposed methods for the protection or preservation of sensitive lands.	<b>N/A.</b>	<i>Not applicable to this application</i>
Fencing plans.	<b>Complies.</b>	<i>Shown on plat</i>
Location of any flood plains, wetlands, and other sensitive lands.	<b>N/A.</b>	<i>Not applicable to this application</i>
Flood plain boundaries as indicated by the Federal Emergency Management Agency as well as the location of 100-year high water marks of all lakes, rivers, and streams.	<b>N/A.</b>	<i>Not applicable to this application</i>
Existing and Proposed easements.	<b>Does Not Comply.</b>	<i>See Engineering Note on third page of red lines.</i>
Street monument locations.	<b>Complies.</b>	<i>Shown on plat</i>
Lot and road addresses and addresses for each intersection. Road names shall be approved by the City GIS department before being added to the subdivision plat.	<b>Complies.</b>	<i>Shown on plat</i>
A data table. 1. total project area; 2. total number of lots, dwellings, and buildings; 3. where buildings are included, square footage of proposed building footprints and, if multiple stories, square footage by floor; 4. for multi-family developments, the number of proposed garage parking spaces and number of proposed total parking spaces; 5. percentage of buildable land; 6. acreage of sensitive lands and percentage sensitive lands comprise of total project area and open space area; 7. area and percentage of open space or landscaping; 8. area to be dedicated as right-of-way (public and private); 9. net density of dwellings by acre (sensitive lands must be subtracted from base acreage).	<b>Complies.</b>	<i>Shown on plat</i>
<b>Phasing Plan:</b> Including a data table with the following Information for each phase: viii. Subtotal area in square feet and acres; ix. number of lots or dwelling units; x. open space area and percentage; xi. utility phasing plan; xii. number of parking spaces; xiii. recreational facilities to be provided;  overall plan showing existing, proposed, and remaining phases.	<b>N/A.</b>	

Check that all open space areas note who owners it, who improves it, who maintains it, and who maintains any regional trail surfaces. And specifies public access easements over regional trails.	<b>Complies.</b>	<i>Shown in file</i>
Check that all living space pop-outs are in the building footprint area on the plat (applies to footprint development and multi-family) (example: a townhome with an upper floor pop-out that extends out over a garage needs to be in the lot area on the plat)	<b>N/A.</b>	
<b>Developer shall ensure all rollback taxes are paid prior to recordation.</b>	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<b>Subdivision Layout</b>		
<b>Layout:</b> The subdivision layout should be generally consistent with the City's adopted Land Use Element of the General Plan, and shall conform to any land use ordinance, any capital facilities plan, any impact fee facilities plan, and the transportation master plan.	<b>Complies.</b>	<i>Shown in file</i>
<b>Trails Master Plan:</b> Shows required trails	<b>Complies.</b>	<i>Shown in file</i>
<b>Block Length:</b> The maximum length of blocks shall be 1,000'. In blocks over 800' in length, a dedicated public walkway through the block at approximately the center of the block will be required.	<b>Complies.</b>	<i>Shown in file</i>
Such a walkway shall not be less than 15' in width unless otherwise approved by the City in accordance with other applicable standards approved by the City Council.	<b>N/A.</b>	
<b>Connectivity:</b> The City shall require the use of connecting streets, pedestrian walkways, trails, and other methods for providing logical connections and linkages between neighborhoods.	<b>Complies.</b>	<i>Shown in file</i>
<b>Local Mailboxes:</b> Group mailboxes shall be accessed only from a local street, and shall not be placed on a collector or arterial street, unless a bulbout is provided with space for a minimum of 3 vehicles to park outside the lane of travel and shoulder.	<b>Complies.</b>	<i>Shown in C202</i>
<b>Private Roads:</b> Private roads may be constructed as approved as part of the Preliminary Plat approval and so long as such roads meet the same standards identified in the Saratoga Springs Standard Street Improvement Details.	<b>N/A.</b>	
<b>Access:</b> Where the vehicular access into a subdivision intersects an arterial road as defined in the Transportation Master Plan, driveways shall not be placed on the intersecting road within 100' of the arterial connection.	<b>N/A.</b>	
<b>Two separate means</b> of vehicular access onto a collector or arterial road shall be required to be constructed to City road standards when the total number of equivalent residential units (including adjacent developments and neighborhoods) served by a single means of access will exceed thirty. <b>Exception:</b> Where no point of second access is available within 500' and where all units are provided with an approved sprinkler system, a second access shall not be required until the number of units reaches double the above limits.	<b>Complies.</b>	<i>Shown in construction drawings</i>
Where two means of access are required, the points of access shall be placed a minimum of 500' apart, measured along the center of the driving lane from center of right-of-way to center of right-of-way. The City Fire Chief may require a greater distance than 500 feet if: 1. an essential link exists between a legitimate governmental interest and the requirement; and 2. the requirement is roughly proportionate, both in nature and extent, to the impact of the proposed development.	<b>N/A.</b>	
<b>Driveway and Driveway Approaches:</b> <b>Construction:</b> single driveways in the A, RA, and RR zones that are constructed of road base or gravel shall include a concrete apron at the	<b>Shall Comply.</b>	<i>Each driveway shall comply</i>

<p>entrance to the garage and at the intersection with the street, each a minimum of 5 feet in depth. Single driveways in all other zones, and shared driveways in all zones, shall be constructed of concrete or asphalt.</p>		
<p><b>Shared Driveways:</b> Shared driveways shall be a minimum of 26' in width and shall direct all runoff to a public or private drainage system. All dwellings on shared driveways shall provide enclosed garages or other covered parking. Shared driveways accessing more than four dwellings shall also provide a minimum of 25' of parking space between the garage and shared driveway. Shared driveways with four or fewer dwellings, if not providing a minimum of 20' of parking space, shall install a remote garage door opener prior to issuance of Certificate of Occupancy. All requirements of the Fire Code shall also be met.</p>	<p>N/A.</p>	
<p>1. Shared Driveways may be used for clustered single-family lots.</p> <ul style="list-style-type: none"> <li>a. When there are more than two units sharing a driveway, the maximum length shall be 150 feet.</li> <li>b. The maximum amount of units accessing a shared driveway shall be six.</li> </ul> <p>2. Shared driveways may be used for two-family, three-family, and multi-family units.</p> <ul style="list-style-type: none"> <li>a. The maximum length shall be 150 feet.</li> <li>b. The maximum amount of units accessing a shared driveway shall be 10.</li> </ul> <p>3. Number of Driveway Approaches: Only one driveway approach is allowed per property frontage. A circular driveway is considered one driveway approach if installed in accordance with the provisions of this section.</p> <ul style="list-style-type: none"> <li>a. Corner lots may have one driveway approach per street frontage.</li> <li>b. Circular driveways must have at least 15' between the two closest edges of the driveway approach at the property line and meet all other conditions of this code.</li> </ul> <p>4. Driveway Approach widths: Residential driveway aprons shall not exceed 30 feet in width, except as follows.</p> <ul style="list-style-type: none"> <li>a. Driveway aprons may be increased up to 45-ft in width if the home has a 3 car garage and/or parking pad and the combined width of all drive approaches on a single frontage does not consume more than 60% of the total property frontage and all other conditions of the section are met.</li> </ul>	<p><b>Shall Comply.</b></p>	<p><i>Each driveway shall comply</i></p>
<p>iii. Driveway Widths:</p> <ul style="list-style-type: none"> <li>1. Driveways shall be a minimum of 20 feet deep and eight feet wide for a single-wide driveway and sixteen feet wide for a double-wide driveway.</li> <li>2. Rear-load alley product, as approved as part of a Community Plan, Village Plan, or Neighborhood Plan, may have an apron in lieu of a driveway approach for access to the garage from the alley. The apron shall be a minimum of five feet deep and shall not count towards required parking.</li> </ul>	<p><b>Shall Comply.</b></p>	<p><i>Each driveway shall comply</i></p>
<p>iv. Driveway Approach Locations: Driveway approaches shall be a minimum of 5' from any property line where water meters are located, except for corner lots as specified below.</p> <p>1. For corner lots, or where the vehicular access into a subdivision intersects a collector or arterial road as defined in the Transportation Master Plan, the minimum driveway approach distance, as measured from the edge of the right of way to the nearest edge or driveway</p>	<p><b>Shall Comply.</b></p>	<p><i>Each driveway shall comply</i></p>

<p>surface shall be as follows: Local = 15 feet; Collector = 40 feet, Arterials = 100 feet.</p> <p>2. Residential lots or parcels shall not be allowed to have access ways onto arterial roads such as Redwood Road, Crossroads Boulevard, Pioneer Crossing, and Pony Express. Exceptions may be made for large lots (at least 1 acre in size) or for lots where the home is set back over 150 feet from the arterial roadway. Approval by UDOT may be required.</p>		
<p>v. The City may reject any permit or proposal for a driveway approach where staff has determined the proposed location would be dangerous or where it conflicts with any permanent improvements, existing or master planned utilities, or waterways.</p>	<b>Shall Comply.</b>	<i>Each driveway shall comply</i>
<b>Lot Design</b>		
<p>All subdivisions shall result in the creation of lots that are developable and capable of being built upon. A subdivision shall not create lots that would make improvement impractical due to size, shape, steepness of terrain, location of watercourses, sanitary sewer problems, driveway grades, or other physical constraints and considerations.</p>	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<p>All lots or parcels created by the subdivision shall have frontage on a street or road that meets the City's ordinances, regulations, and standards for public roads.</p>	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<p>Flag lots may be approved with less frontage when the Planning Commission determines that the creation of such a lot would result in an improved design or better physical layout for the lot based on the following criteria:</p> <ul style="list-style-type: none"> <li>i. For subdivisions with 20 or less lots: no more than 10% (rounding down) of the total lots are allowed to be flag lots;</li> <li>ii. For subdivisions with 50 or less lots: no more than 7.5% (rounding down) of the total lots are allowed to be flag lots; and</li> <li>iii. For subdivision with more than 50 lots: no more than 5% (rounding down) of the total lots are allowed to be flag lots.</li> </ul>	N/A.	
<p>Land dedicated as public roads and rights-of-way shall be separate and distinct from land included in lots adjacent to public roads and rights-of-way. In no case may land dedicated for public roads and rights-of-way be included in the area calculation of any lots, except for non-conforming lots.</p>	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<p>Side property lines shall be at approximately right angles to the street line or radial to the street line.</p>	<b>Complies.</b>	<i>Shown in files</i>
<p>Corner lots for residential use shall be 10% larger than the required minimum lot. Corner lots in the R1-10, R1-9, R2-8, R3-6, MR-10, MF-14, and MF-18 zones shall be 10% larger than the minimum required of the zone.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p>No lot shall be created that is divided by a municipal or county boundary line.</p>	<b>Shall Comply.</b>	<i>Area being annexed to Saratoga Springs</i>
<p>Remnants of property shall not be left in the subdivision that do not conform to lot requirements or are not required or suitable for common open space, private utilities, public purposes, or other purpose approved by the Land Use Authority.</p>	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<p>Double access lots are not permitted with the exception of corner lots.</p>	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<p>Driveways for residential lots or parcels shall not be allowed to have access on major arterials.</p>	<b>Shall Comply.</b>	<i>Applicant shall comply</i>

<b>Exception:</b> Exceptions may be made for large lots (at least 1 acre in size) or for lots where the home is set back over 150' from the arterial roadway. Approval by UDOT may be required.		
All subdivisions along arterial roadways shall conform to the City's requirements and adopted street cross-section including pedestrian walkways, park strips, landscaping, and fencing.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<b>Street Connectivity</b>		
<b>Connectivity Standards.</b> All new subdivisions shall provide connectivity with adjacent developed and undeveloped properties and with adjacent open space, amenities, parks, and natural areas. All new subdivisions are required to: Extend streets, sidewalks, and trails at least once in each direction to adjacent properties; and	<b>Complies.</b>	<i>Shown in file</i>
Connect to all existing vehicular and pedestrian access points on adjacent developed properties; and	<b>Complies.</b>	<i>Shown in file</i>
Install and connect public trails into all adjacent public open space, parks, and trails, which includes but is not limited to connections to trail corridors with public access easements; and	<b>Complies.</b>	<i>Shown in file</i>
Stub public streets at least every 1,000 feet into all adjacent sides of undeveloped properties; and	<b>Complies.</b>	<i>Shown in file</i>
Connect or stub into all adjacent master-planned rights-of-way, sidewalks, trails, and public transportation stops, stations, and facilities.	<b>Complies.</b>	<i>Shown in file</i>
<b>Exceptions:</b> Connectivity Standards may be reduced by the Land Use Authority for Preliminary Plats, Final Plats, or Site Plans, as applicable, if the applicant provides clear and convincing evidence that it is impracticable to achieve due to the following:	<b>N/A.</b>	
a. Right-of-way, intersection, or access spacing cannot meet the Standard Technical Specifications and Drawings for City of Saratoga Springs; or		
ii. The property is adjacent to the Jordan River, Utah Lake, delineated wetlands, slopes exceeding 30%, drainage channels, natural features, open space, or waterways that do not allow for a crossing or an access; or	<b>N/A.</b>	
iii. The property is adjacent to fully developed property that does not have any vehicular or pedestrian access points.	<b>N/A.</b>	
iv. Exceptions shall be construed narrowly by the Land Use Authority.	<b>N/A.</b>	
v. These exceptions shall not apply to trail connections.	<b>N/A.</b>	
<b>Arrangement of Streets.</b> The arrangement of streets in new developments shall make provision for the continuation of the existing streets in adjoining areas (or their proper projection where adjoining land is not subdivided) at the same or greater width (but in no case less than the required minimum width per the City's Standard Technical Specifications and Drawings). Exceptions below shall be construed narrowly and only granted if the developer provides clear and convincing evidence to the Land Use Authority that the exception is met.	<b>Complies.</b>	<i>Shown in file</i>
In order to develop a public street grid throughout the City, public right-of-way connections through and between developments shall be made at a minimum of every 1000 feet and in a manner that will provide safe and convenient access to existing or planned arterial/collector streets, schools, public parks, public trails, private parks or trails with public access easements, employment centers, commercial areas, or similar neighborhood activity centers. The connections may be completed over time in phases as part of a circulation plan. This requirement does not apply to commercial	<b>Complies.</b>	<i>Shown in file</i>

development; however, all streets and trails on the Transportation Master Plan and Parks, Recreation, Trails, and Open Space Master Plan shall be included and shall be public streets.		
A public street connection shall be provided to any existing or approved public street right-of-way stub abutting the development, unless it is demonstrated that a connection cannot be made because of the existence of one of the following conditions: <ul style="list-style-type: none"> <li>i. Physical conditions that preclude development of a public street meeting the City’s Standard Technical Specifications and Drawings. Such conditions may include, but are not limited to, topography, natural resource areas, such as wetlands, ponds, streams, channels, rivers or lakes, or slopes exceeding 30%.</li> </ul>	<b>Complies.</b>	<i>Shown in file</i>
<ul style="list-style-type: none"> <li>ii. Buildings or other existing development on adjacent lands, including previously subdivided but vacant lots or parcels, which obstruct a connection now and in the future.</li> </ul>	<b>Complies.</b>	<i>Shown in file</i>
<b>Circulation Plan.</b>	<b>Complies.</b>	<i>Shown in file</i>
The circulation plan shall include connections to adjacent parcels and shall address street, sidewalk, and trail connectivity and shall include facilities shown on the City’s adopted Master Plans.	<b>Complies.</b>	<i>Shown in file</i>
The circulation plan shall show the connectivity, block length dimensions, cul-de-sac length dimensions, bike and pedestrian trails, and any proposed traffic calming features.	<b>Complies.</b>	<i>Shown in file</i>
The circulation plan shall include proposed connections to and across adjacent properties.	<b>Complies.</b>	<i>Shown in file</i>

<b>19.13 Process</b>	
<b>Regulation</b>	<b>Findings</b>
<b>Neighborhood Meeting.</b> Required before Planning Commission for any multi-family or non-residential development proposal adjacent to developed property in a residential zone. <b>Inform the applicant when this is required.</b>	<i>N/A</i>
Notice/Land Use Authority.	<i>N/A</i>
<b>Master Development Agreement.</b> A Master Development Agreement shall be required of any development that is in excess of 20 acres in size if non-residential or mixed-use or developments in excess of 160 acres in size if residential. A Master Development Agreement may also be required pursuant to this Title 19 including Chapter 19.26 or may be desirable or necessary pursuant to the exercise of the City Council’s legislative discretion in the fact scenarios listed in Section 19.13.08.	<i>N/A</i>
Phasing Improvements.	<i>N/A</i>
Payment of Lieu of Open Space.	Amount of \$:
Piping of Canals	For residential projects, piping of canals per canal company specifications if a canal or canal easement that area is adjacent to or within the area of the proposed residential project, unless the canal company or Bureau of Reclamation does not allow piping. Non-residential projects shall install secure fencing adjacent to canal easements or canals per canal company specifications to prevent entry from the non-residential project onto the canal or canal easement.

## 19.16 Site and Architectural Design Standards

### General Site Design Standards

Regulation	Compliance	Findings
<p><b>Submittal Requirements.</b> Scaled building elevations and perspectives (3D renderings) shall be submitted that indicate all colors, styles, materials, and other proposed building treatments. Photorealistic material and color boards shall also be submitted with the items required accompanying development application. An RYB Hexadecimal Color Code number shall be provided for all applicable elevation colors; however, natural materials shall be exempt and shall instead provide a photographic image of the proposed material.</p>	N/A.	
<p><b>Pedestrian Connectivity:</b> All buildings and sites shall be designed to be pedestrian friendly by the use of connecting walkways.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p>Safe pedestrian connections shall be provided to link between buildings with the public rights-of-way, as well as links within a development. Sidewalks shall also be provided to link the building to pedestrian facilities adjacent to the property, and when feasible between developments.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p>All developments shall provide pedestrian connections that lead to the building entrances of adjacent non-residential and residential sites and shall stub to adjacent undeveloped property to allow for future connections.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p>All pedestrian connections shall be shown on the related site plan or plat.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>Parking Areas:</b> On-site parking shall be located primarily to the sides or rear of the building. Variations may be approved by the Land Use Authority, subject to the following criteria:</p> <ul style="list-style-type: none"> <li>i. The use is a big box with outparcels or smaller commercial buildings helping to screen parking, or</li> <li>ii. At least 50% of the parking is located to the side or rear of the building, or</li> <li>iii. A safety issue is created by locating parking to the side or rear as verified and documented by the Saratoga Springs Police Department. For example, the parking will be entirely concealed from view by existing walls or buildings, or</li> <li>iv. That portion of development that lies within the Waterfront Buffer Overlay, or</li> <li>v. The development is Office, Warehouse/Flex space and when loading docks are not adjacent to a public right-of-way. Exception: when a lot with Office, Warehouse/Flex space is adjacent to more than one public street, loading docks may face the lower classification of the streets.</li> </ul>	N/A.	
<p>Parking lots shall be designed with a hierarchy of circulation: major access drives with no parking; major circulation drives with little or no parking; and then parking aisles for direct access to parking spaces.</p>	N/A.	
<p>Parking lots adjacent to, and visible from, public streets shall be screened from view through the use of earth berms, screen walls, landscape hedges or combinations thereof with a minimum height of 3' as measured from the parking surface. Within regional parks this requirement may be met through the use of intermittent planter beds rather than a berm, hedge, or screen wall; trees and shrubs may be clustered in the planter beds where necessary to avoid light spillage.</p>	N/A.	

<b>Acceleration and Deceleration Lanes:</b> Acceleration and deceleration lanes shall be required on major arterials when deemed necessary by the City Engineer.	N/A.	
<b>Parking Structures:</b> Parking structures shall be architecturally consistent with the project buildings, including the use of the same primary finish materials as the exterior of the adjacent or adjoining buildings.	N/A.	
When placement of parking structures along site frontages is necessary, visual mitigation such as landscape buffers, upper-level setbacks, or additional architectural treatment shall be used.	N/A.	
The view of a parking structure from a public street shall be minimized by placing its shortest dimension along the street edge.	N/A.	
The top deck of parking structures shall include screen walls, periphery landscape islands, or similar features where visible from public view in order to soften the appearance of the top of the parking structure and screen the view of cars on the top deck of the structure.	N/A.	
All top deck lighting shall be screened and directed downward to avoid light spill onto the street below and shall consist of only bollards or building lighting. Light poles are prohibited.	N/A.	
<b>Trash Enclosures, Storage Areas, and External Structures:</b> Landscaping, fencing, berms, or other devices integral to overall site and building design shall screen trash enclosures, storage areas, and other external structures.		
Service yards, refuse and waste-removal areas, loading docks, truck parking areas and other utility areas shall be screened from view by the use of a combination of walls, fences, and dense planting. Screening shall block views to these areas from on-site as well as from public rights of way and adjacent properties. This does not apply to Municipal Buildings.	N/A.	
All trash dumpsters shall be provided with solid enclosures. b. Enclosures shall be composed of 6' high solid masonry or decorative precast concrete walls, with opaque gates and self-latching mechanisms to keep gates closed when not in use. Bollards are required at the front of the masonry walls to protect the enclosure from trash collection vehicles. Gates shall be made of opaque metal for durability. Chain-link gates with or without opaque slats are not acceptable. Colors and materials shall be consistent with the main building or use.	N/A.	
Where trash enclosures, storage areas, or other external structures are adjacent to parking areas, a 3' landscaped buffer shall be provided that does not impede access into and out of vehicles.	N/A.	
These areas shall be well maintained and oriented away from public view. The consolidation of trash areas between buildings is encouraged. The use of modern disposal and recycling techniques is encouraged. This section shall not apply to community or public recycling bins or drop boxes; however, the location shall be determined by city Staff in accordance with the standards herein.	N/A.	
<b>Utility Boxes:</b> Dense vegetative buffers which include an evergreen variety of plant materials shall be placed where appropriate to screen all utility boxes and pedestals in order to remain attractive during the winter months.	N/A.	
<b>Interconnection.</b> Interconnection shall occur via pedestrian and vehicular connections. All parking and other vehicular use areas shall be interconnected with, or stubbed to, all adjacent non-residential developed and undeveloped properties, as designated on the land use map or the zoning map, in order to allow maximum off-street vehicular	N/A.	

<p>circulation. Walkways and trails shall be connected to adjacent sites and stubbed for future development. All residential site plans shall meet 19.12 Connectivity Standards.</p>		
<b>General Architectural Design Standards</b>		
<b>General standards do not apply one-family and two-family dwellings unless governed under a DA.</b>		
<p><b>Building Articulation for Buildings Under 20,000 Square Feet (footprint):</b> Building elevations exceeding 40' in length shall incorporate a minimum of one horizontal elevation shift or combination of vertical and horizontal elevation shifts that together equal at least five feet, stepping portions of the elevation to create shadow lines and changes in volumetric spaces, and a minimum of two of the following, all spaced at intervals of 20' of horizontal width:</p> <ul style="list-style-type: none"> <li>i. Addition of horizontal and vertical divisions by use of textures or materials.</li> <li>ii. Primary material change (i.e. change in material type, size, or color).</li> <li>iii. Addition of projections such as balconies, cornices, covered entrances, porte-cocheres, trellis', pergolas, arcades, and colonnades. Such trellis' and awnings extend outward from the underlying wall surface at least 36".</li> <li>iv. Variation in the rooflines by use of dormer windows, overhangs, arches, stepped roofs at a minimum of 12", gables or other similar devices.</li> </ul>	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<p><b>Building Articulation for Buildings 20,000 Square Feet (footprint) and Over:</b> Building elevations exceeding 60' in length shall incorporate a minimum of one horizontal elevation shift or combination of vertical and horizontal elevation shifts that together equal at least 20 feet, stepping portions of the elevation to create shadow lines and changes in volumetric spaces, and a minimum of two of the following, all spaced at intervals of 50' of horizontal width:</p> <ul style="list-style-type: none"> <li>v. Addition of horizontal and vertical divisions by use of textures or materials.</li> <li>vi. Primary material change (i.e. change in material type, size, or color).</li> <li>vii. Addition of projections such as balconies, cornices, covered entrances, porte-cocheres, trellis', pergolas, arcades, and colonnades. Such trellis' and awnings extend outward from the underlying wall surface at least 36".</li> </ul> <p>Variation in the rooflines by use of dormer windows, overhangs, arches, stepped roofs at a minimum of 36", gables or other similar devices.</p>	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<p><b>Exception:</b> In the I/C zone, Section 19.16.05(2) shall not apply to building elevations that are 50 percent or more obscured by natural topography, from adjacent or future developments, as measured at the property line, and are not fronting public or private streets.</p>	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<p><b>Roof Treatment:</b> Sloped roofs shall provide articulation and variations in order to break up the massiveness of the roof. Sloped roofs shall include eaves which are proportional to the roofs slope and scale of the building.</p>	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<p>Flat roofs shall be screened with parapets on all sides of the building. If no roof top equipment exists or is proposed, the parapet shall be a minimum of 12" in height above the roof.</p>	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<p>All roofs on three-family and multi-family dwellings shall have a minimum pitch of 3/12 (25% slope). To provide architectural enhancement, residential structures are encouraged to have multiple pitch variations.</p>	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>

Roof mounted equipment shall not be visible from adjacent public and private streets as well as from adjacent properties, unless grade differences make visibility unavoidable.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
Screening shall be solid and shall be consistent with the material and color of exterior finishes of the building through the use of at least two out of three of the exterior finishes of the building.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<b>Windows:</b> Windows, other than rectangular windows, may be used as accents and trim. Untreated aluminum or metal window frames are prohibited.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<b>Awnings, Canopies, Trellises, Pergolas, and Similar Features:</b> All such features must be attached to a vertical wall.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
All such features shall project at least 4' from the building when located over a pedestrian traffic area and no less than 2' otherwise.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
All such features shall maintain a minimum clearance above sidewalk grade of 8' to the bottom of the framework when located over a pedestrian traffic area.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
Backlighting is not permitted.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<b>Mechanical Equipment:</b> All mechanical equipment shall be located or screened and other measures shall be taken so as to shield visibility of such equipment from any public or private streets.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
Wing walls, screens, or other enclosures shall be shall be integrated into the building and landscaping of the site, whether located on the ground or roof.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
Rooftops of buildings shall be free of any mechanical equipment unless completely screened from all horizontal points of view.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
Screening materials shall conform to the color scheme and materials of the primary building.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<b>Residential Architectural Design Standards</b>		
<b>General standards do not apply one-family and two-family dwellings unless governed under a DA.</b>		
<b>Architectural Wrap:</b> Stone, brick, wainscot, and other front elevation materials shall be wrapped a minimum of 2' into interior side elevations and shall extend the full width of any street side yard, or façade abutting common open space.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<b>Materials and Colors:</b> A minimum of three materials and three colors shall be utilized on front or street side elevations, building façades abutting common open space, or building facades abutting an arterial or collector road. A minimum of two materials and two colors shall be utilized on interior side and rear elevations. The roof shall not be considered a material or color.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
No more than 75% of any building elevation shall consist of any one material or color.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
Carports and similar structures shall be compatible in architectural design and details with the main residential structure through the use of at least 2 out of 3 of the same materials and colors.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
For each new three-family and multi-family residential building, all building facades abutting an arterial or collector road shall include either a porch pop out of at least 36 inches on every other residential unit or shall include a dormer gable on every other residential unit.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>
<b>Color Variation:</b> Three-family, and multi-family developments containing more than five buildings shall be designed with at least 2 color palettes.	N/A.	<i>Not mentioned in Annexation and Development Agreement</i>

## 19.18 Signs

Regulation	Compliance	Findings
Site Plans require signage information (if tenant known, show location potential location size)	<b>Shall Comply.</b>	<i>Applicant shall comply with residential sign standards if applicable.</i>
Monument sign location meets code and sign details provided	<b>Shall Comply.</b>	<i>Applicant shall comply with residential sign standards if applicable.</i>
Wall signs and potential locations and details	<b>Shall Comply.</b>	<i>Applicant shall comply with residential sign standards if applicable.</i>

## 19.19 Open Space

### Minimum Required Open Space

Regulation	Compliance	Findings
<b>Open Space Required:</b> A minimum of one Equivalent Acre of park space is required for every 40 residential units in a development, or fraction thereof.	<b>Complies.</b>	<i>Shown in Open Space Worksheet Excel</i>
<b>Minimum Percentage by Development:</b> In addition to the minimum Equivalent Acres, to ensure a livable community, in no case shall the percentage of total open space acreage provided in the following types of development be less than the listed percentage: <ul style="list-style-type: none"> <li>a. Single family developments: 10% of overall development acreage</li> <li>b. Multi-family developments: 15% of overall development acreage</li> <li>c. Mixed-use and Mixed Waterfront developments: 25% of overall development acreage</li> <li>d. Developments with both single family and multi-family: combination of the requirement based on the acreage for each type of housing.</li> </ul>	<b>Complies.</b>	<i>Shown in Open Space Worksheet Excel</i>
<b>Other Limitations:</b> In no case may the cumulative total of the following categories qualify for more than 50% of a development's Equivalent Acre requirement. <ul style="list-style-type: none"> <li>i. Unimproved, not Sensitive Lands</li> <li>ii. Open space with no access</li> </ul>	<b>Complies.</b>	<i>Shown in Open Space Worksheet Excel</i>

### Minimum Required Amenities

<b>Minimum Points:</b> Minimum required points are based on the number of required Equivalent Acres provided in a contiguous park and are outlined in the table in 19.19.05(2).	<b>Complies.</b>	<i>Shown in Open Space Worksheet Excel</i>
<b>Mixture of Amenities and Required Amenities:</b> All parks are required to provide a mixture of amenities, including at least 1 separate item each from Categories C, D, and E.	<b>Complies.</b>	<i>Meets Requirement</i>
No more than 25% of the points may be met by one specific item type in any one category.	<b>Shall Comply.</b>	<i>Code Amendment will resolve issue.</i>
All parks over 5 Equivalent Acres are also required to provide at least one item from Category A or B, and three items from category P, and a minimum one toilet restroom per each 5 acres or fraction thereof.	<b>N/A.</b>	<i>Not applicable to this application</i>
Items in Categories A and B may qualify for impact fee credits, if identified in the Parks and Trails Master Plan.	<b>N/A.</b>	<i>Not applicable to this application</i>

### Payment in Lieu of Open Space

<b>Applicability:</b> the City's Payment in Lieu of Open Space Program may be utilized for all or a portion of the requirement for developments that meet one or more of the following criteria:	<b>N/A.</b>	<i>Not applicable to this application</i>
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<ul style="list-style-type: none"> <li>i. any single-family development where a minimum of 75% of the lots are 10,000 square feet or larger, or</li> <li>ii. any single-family development with a park requirement of less than 1 acre, or</li> <li>iii. any development in the MW zone where such fee will aid in the creation of large clustered open spaces near the waterfront, or</li> <li>iv. that portion of a development that is located within ¼-mile of an existing improved regional public park.</li> </ul>		
<p><b>Total Cost:</b></p> <ul style="list-style-type: none"> <li>1. The City shall maintain an annually updated list of land values for the cost of a non-sensitive developable acre, and apply the land value to the required Equivalent Acres for which the applicant desires to pay a fee in lieu.</li> <li>2. The City shall calculate the minimum required points per this chapter, and multiply the points by \$2,000 to determine the cost of the minimum required open space improvements, including landscaping, parks, trails, and other amenities.</li> </ul>	N/A.	<i>Not applicable to this application</i>

<b>19.27 Addressing and Street Naming</b>		
<b>Double Check all Addresses after GIS Assigns Them</b>		
Regulation	Compliance	Findings
<p><b>WebApp to Review Addresses assigned by GIS:</b>  <a href="https://sngis.maps.arcgis.com/apps/webappviewer/index.html?id=b4786008c6dd420ab06445cd8d5e5fe0">https://sngis.maps.arcgis.com/apps/webappviewer/index.html?id=b4786008c6dd420ab06445cd8d5e5fe0</a></p>		
<p><b>Lot Numbers:</b> Are they correct and sequential. Lot numbers shall not be duplicated in separate phases/plats in the same development. i.e. Phase 1 and Phase 2 shall not have Lot 101.</p>	<b>Complies.</b>	<i>Shown in plat.</i>
<p><b>Address Numbers:</b> is it accurate and sequential and does it fit between the intersection coordinates</p>	<b>Complies.</b>	<i>Shown in plat.</i>
<p><b>Compare Surrounding Addresses:</b> do the addresses work with surrounding addresses? Are there any repetitions in address numbers? Do the street names match and line up with streets in other plats and across intersections?</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>Double Access Lots and Corner Lots.</b> These types of lots should have an address on each road frontage, unless front door access is not allowed. If a front door is allowed, but not a driveway, this should be clear on the plat.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>No Access on Double Frontage of Lots:</b> The plat should state what this means. No driveway and no front door, or just no driveway</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>Alternate Addresses:</b> are they in the right location on the lot and do they make sense concerning lots that have no access. What does “no access mean”?</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>Rear Load:</b> Meet with Brian to review every time there is a rear-load product.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>Rear Load:</b> label “FRONT” to identify which side is the front, but check grading to see if the units can be accessed from the front.</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>Grading/Slope:</b> Does the grade in front of rear-load product allow for front door access for EMT and deliveries?</p>	<b>Complies.</b>	<i>Shown in file</i>
<p><b>New Address:</b> street names shall be proposed by the developer and approved by City staff. Street coordinates and house numbers shall be approved by City staff and placed on the final subdivision plat by the developer before plat recordation.</p>	<b>Complies.</b>	<i>Shown in file</i>

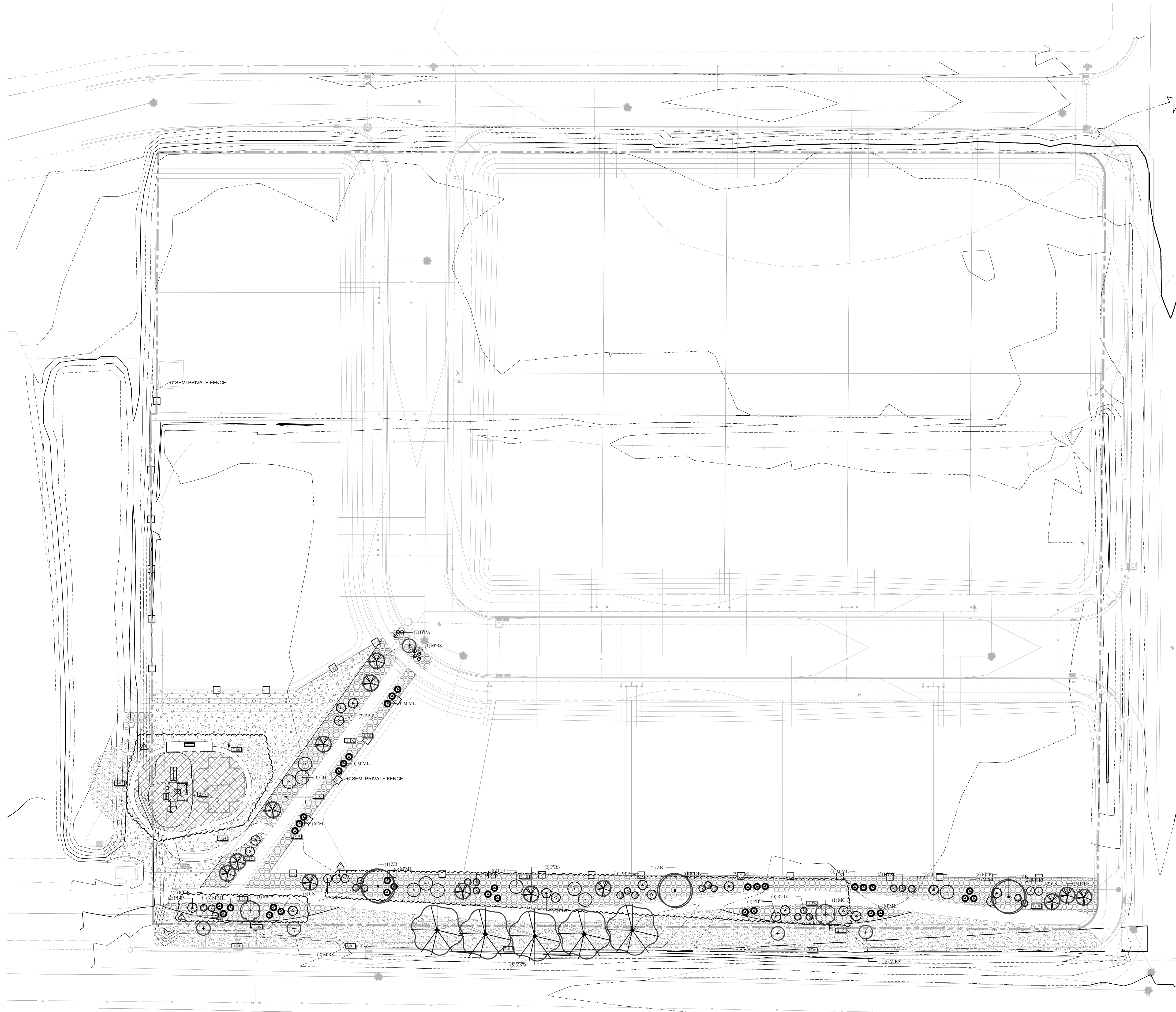
<p><b>Duplicate Street Names:</b> No duplication of street names or numbers shall be used. No phonetic duplications shall be used (same name, spelled differently). If a street is terminated by unimproved land and continues on the same bearing beyond the unimproved land, it should continue with the same name.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p><b>Continuity.</b> Proposed street names are encouraged to have the following characteristics: Historic significance, local sense of places, overall theme, compatibility with adjacent streets</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p><b>Compass Directions.</b> The 4 compass directions shall not be used as part of the street name.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p><b>Name Length.</b> Street names shall not be longer than 13 letters, including spaces between letters. (this does not include the street type designator)</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>Street Type Designations. Proposed street names and types shall be matched as follows:</p> <ol style="list-style-type: none"> <li>a. <b>Boulevard (Blvd), Parkway (Pkwy):</b> This name may only be used for arterials and collectors..</li> <li>b. <b>Drive (Dr), Way (Wy):</b> Meandering, curvilinear, or diagonal street usually longer than 1000 feet.</li> <li>c. <b>Road (Rd):</b> Limited streets that may run in a direction and are most always longer than 1000 feet.</li> <li>d. <b>Streets (St), Avenues (Ave):</b> Straight directional streets.</li> <li>e. <b>Lanes (Ln), Row:</b> Short collector or minor streets which are usually less than 1000 feet in length. May be used in names for private rights-of way.</li> <li>f. <b>Circle (Cir), Court (Ct), Place (Pl), Cove (Cv):</b> Permanent dead end streets or cul-de-sac streets usually less than 600 feet in length and containing three or more lots or separate dwelling structures.</li> <li>g. <b>Center (Cntr) and Mall:</b> Designations reserved for high density commercial developments with multiple structures and occupancies that can be substituted for the street type designator in an assigned address.</li> </ol>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p><b>Street and Property Numbering.</b> Each street, lot, unit, dwellings, or parcel in the site plan or plat shall be addressed according to the following standards:</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>1. All street intersections shall be addressed numerically. Street intersection numbers shall end with a “0” or a “5” and with “North,” “South,” “East,” or “West”.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>2. Where possible, property addresses shall be odd on the south and west sides of a street.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>3. Where possible, property addresses shall be even on the north and east sides of a street.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>4. Property addresses for IADUs (see Chapter 19.20), ADUs, stacked multi-family, dwelling above commercial, commercial/retail, or office building that have multiple users shall be identified by a single address, and the individual units shall have a unique identifier such as suite, apartment, dwelling, or unit number.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>5. Dual addresses on corner lots will be assigned at plat recordation. At Building Permit issuance, one address shall be permanently assigned.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>6. Buildings that are hidden behind other buildings or do not have public frontage, shall be numbered from the centerline of the principal access or driveway.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>

7. City Staff shall assign numbers to site plans before final site plan approval, based on the above standards.	<b>Complies.</b>	<i>Shown in file</i>
8. If a unit faces a street, it shall be numbered on that street. If a unit faces open space, it shall be numbered to the alley way.	<b>Complies.</b>	<i>Shown in file</i>
<b>Building Identification.</b> All buildings shall have approved address numbers, and such numbers shall be identified using the following standards:	<b>Complies.</b>	<i>Shown in file</i>
1. <b>Background.</b> Numbers shall be set on a background of a contrasting color.	<b>Complies.</b>	<i>Shown in file</i>
2. <b>Size.</b> Residential building numbers shall be at least 4-inch tall block letters. All building numbers shall be of sufficient size so as to be legible from the street or private road. Non-residential buildings shall use Western Arabic numbers or alphabetic letters that are a minimum 4 inches high with a minimum stroke width of 0.5 inch, installed on a contrasting background and be plainly visible from the street or road fronting the property.	<b>Complies.</b>	<i>Shown in file</i>
3. <b>Visibility.</b> Numbers shall be placed in a position that is plainly legible and visible from the street or road fronting the property. When a building is at a distance greater than the required setback from a street or when view of the building is blocked by trees or shrubs, numbers shall be displayed on a sign attached to a fence, gate, street mailbox, or lawn stake, in addition to being placed on the building. a. Stacked multi-family structures, condominiums, commercial, office, or institutional building numbers shall be placed in a prominent location at the main entrance of such buildings. b. Interior suites, apartments, units, or offices shall be clearly identified in a logical numeric or alphabetic sequence. c. Attached side-by-side town homes, commercial, office, or institutional buildings adjacent to a public or private street shall have individual addresses for each unit building numbers or letter with associated unit numbers. Developments with buildings that do not have direct access to a public or private street shall use one address from the public or private street, and each unit shall have a number or letter, such as 1, 2, 3 or A, B, C and so on. with building numbers or letters and associated unit numbers. d. Stacked multi-family structures, commercial, office, or institutional buildings shall have 1 address facing a public or private street and each unit shall have a number or letter such as 1, 2, d, or A, B, C and so on. with an individual building number or letter with associated unit numbers. e. Townhomes, three-family dwellings, two-family dwellings, commercial, office, or institutional buildings that have a front door that faces a public or private road and a driveway or parking access from a rear alley shall number only attach address numbers to the front of each unit and not the rear. f. Townhomes, three-family dwellings, two-family dwellings, commercial, office, or institutional buildings that have a front door that faces open space or another building and have a driveway or parking access from a rear alley shall number both the front and back of each unit. The applicant shall verify that address numbers are not duplicated in units that are across the green space or across the alley.	<b>Complies.</b>	<i>Shown in file</i>

## Fiscal Impact

Regulation	Findings
Is there any City maintained open space?	<i>None</i>
What is the anticipated cost to the City?	<i>None</i>
When will City maintenance begin?	<i>None</i>

SHEET LP 102
SHEET LP 103



ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	PROJECT INFORMATION	DEVELOPER / PROPERTY OWNER / CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE STAMP	DRAWING INFO
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2/17/2026	UT25182	THIS PRINT FROM PKJ DESIGN GROUP IS BASED ON CIVIL AND ARCHITECT INFORMATION ISSUED ON 02/17/2026		ANDERSON DEVELOPMENT KEATON MORTON 801-856-2478 KEATON@AND-DEV.COM			PM: JTA DRAWN: ACP CHECKED: KBA PLOT DATE: 2/17/2026
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NO.	REVISION	DATE
1	CITY COMMENTS	01-20-2026
2	CITY COMMENTS	01-22-2026
3		
4		

# MEADOWS AT NORTH LAKE PH2

## SARATOGA SPRINGS, UTAH

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**LANDSCAPE OVERALL PLAN**  
**CITY PERMIT SET**  
**LP-100**

# LANDSCAPE PLAN SPECIFICATIONS

## PART 1 - GENERAL

### 1.1 SUMMARY

A. THIS SECTION INCLUDES LANDSCAPE PROCEDURES FOR THE PROJECT INCLUDING ALL LABOR, MATERIALS, AND INSTALLATION NECESSARY, BUT NOT LIMITED TO, THE FOLLOWING:

1. SITE CONDITIONS
2. GUARANTEES
3. MAINTENANCE
4. SOIL AMENDMENTS
5. FINE GRADING
6. LANDSCAPE EDGING
7. FURNISH AND INSTALLING PLANT
8. TURF PLANTING
9. WEED BARRIER
12. SITE CONDITIONS

A. EXAMINATION: BEFORE SUBMITTING A BID, EACH CONTRACTOR SHALL CAREFULLY EXAMINE THE CONTRACT DOCUMENTS, SHALL VISIT THE SITE OF THE WORK, SHALL FULLY INFORM THEMSELVES AS TO ALL EXISTING CONDITIONS AND LIMITATIONS, AND SHALL INCLUDE IN THE BID THE COST OF ALL ITEMS REQUIRED BY THE CONTRACT DOCUMENTS ARE AT A VARIANCE WITH THE APPLICABLE LAWS, BUILDING CODES, RULES, REGULATIONS, OR CONTAIN OBVIOUS ERRONEOUS OR UNCOORDINATED INFORMATION, THE CONTRACTOR SHALL PROMPTLY NOTIFY THE PROJECT REPRESENTATIVE AND THE NECESSARY CHANGES SHALL BE ACCOMPLISHED BY ADDENDUM.

B. PROTECTION: CONTRACTOR TO CONDUCT THE WORK IN SUCH A MANNER TO PROTECT ALL EXISTING UNDERGROUND UTILITIES OR STRUCTURES. CONTRACTOR TO REPAIR OR REPLACE ANY DAMAGED UTILITY OR STRUCTURE USING IDENTICAL MATERIALS TO MATCH EXISTING AT NO EXPENSE TO THE OWNER.

C. IRRIGATION SYSTEM: DO NOT BEGIN PLANTING UNTIL THE IRRIGATION SYSTEM IS COMPLETELY INSTALLED, IS ADJUSTED FOR FULL COVERAGE AND IS COMPLETELY OPERATIONAL.

### 1.3 PERMITS

A. BLUE STAKE/ DIG LINE: WHEN DIGGING IS REQUIRED, "BLUE STAKE" OR "DIG LINE" THE WORK SITE AND IDENTIFY THE APPROXIMATE LOCATION OF ALL KNOWN UNDERGROUND UTILITIES OR STRUCTURES.

### 1.4 PLANT DELIVERY, QUALITY, AND AVAILABILITY

A. UNAUTHORIZED SUBSTITUTIONS WILL NOT BE ACCEPTED. IF PROOF IS SUBMITTED THAT SPECIFIC PLANTS OR PLANT SIZES ARE UNOBTAINABLE, WRITTEN SUBSTITUTION REQUESTS WILL BE CONSIDERED FOR THE NEAREST EQUIVALENT PLANT OR SIZE. ALL SUBSTITUTION REQUESTS MUST BE MADE IN WRITING AND PREFERABLY BEFORE THE BID DUE DATE.

### 1.5 FINAL INSPECTION

A. ALL PLANTS WILL BE INSPECTED AT THE TIME OF FINAL INSPECTION PRIOR TO RECEIVING A LANDSCAPE SUBSTANTIAL COMPLETION FOR CONFORMANCE TO SPECIFIED PLANTING PROCEDURES, AND FOR GENERAL APPEARANCE AND VITALITY. ANY PLANT NOT APPROVED BY THE PROJECT REPRESENTATIVE WILL BE REJECTED AND REPLACED IMMEDIATELY.

### 1.6 LANDSCAPE SUBSTANTIAL COMPLETION

A. A SUBSTANTIAL COMPLETION CERTIFICATE WILL ONLY BE ISSUED BY THE PROJECT REPRESENTATIVE FOR "LANDSCAPE AND IRRIGATION" IN THEIR ENTIRETY. SUBSTANTIAL COMPLETION WILL NOT BE PROPORTIONED TO BE DESIGNATED AREAS OF A PROJECT.

### 1.7 MAINTENANCE

A. PLANT MATERIAL: THE CONTRACTOR IS RESPONSIBLE TO MAINTAIN ALL PLANTED MATERIALS IN A HEALTHY AND GROWING CONDITION FOR 90 DAYS AFTER RECEIVING A LANDSCAPE SUBSTANTIAL COMPLETION AT WHICH TIME THE GUARANTEE PERIOD COMMENCES. THIS MAINTENANCE IS TO INCLUDE MOWING, WEEDING, CULTIVATING, FERTILIZING, MONITORING WATER SCHEDULES, CONTROLLING INSECTS AND DISEASES, RE-GUYSING AND STAKING, AND ALL OTHER OPERATIONS OF CARE NECESSARY FOR THE PROMOTION OF ROOT GROWTH AND PLANT LIFE SO THAT ALL PLANTS ARE IN A CONDITION SATISFACTORY AT THE END OF THE GUARANTEE PERIOD. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR FAILURE TO MONITOR WATERING OPERATIONS AND SHALL REPLACE ANY AND ALL PLANT MATERIAL THAT IS LOST DUE TO IMPROPER APPLICATION OF WATER.

### 1.8 GUARANTEE

A. GUARANTEES: A GUARANTEE PERIOD OF ONE YEAR SHALL BEGIN FROM END OF MAINTENANCE PERIOD AND FINAL ACCEPTANCE FOR TREES, SHRUBS, AND GROUND COVERS. ALL PLANTS SHALL GROW AND BE HEALTHY FOR THE GUARANTEE PERIOD AND TREES SHALL LIVE AND GROW IN ACCEPTABLE UPRIGHT POSITION. ANY PLANT NOT ALIVE, IN POOR HEALTH, OR IN POOR CONDITION AT THE END OF THE GUARANTEE PERIOD WILL BE REPLACED IMMEDIATELY. ANY PLANT WILL ONLY NEED TO BE REPLACED ONCE DURING THE GUARANTEE PERIOD. CONTRACTOR TO PROVIDE DOCUMENTATION SHOWING WHERE EACH PLANT TO BE REPLACED IS LOCATED. ANY OUTSIDE FACTORS, SUCH AS VANDALISM OR LACK OF MAINTENANCE ON THE PART OF THE OWNER, SHALL NOT BE PART OF THE GUARANTEE.

## PART 2 - PRODUCTS

# LANDSCAPE PLAN NOTES

### GRADING AND DRAINAGE REQUIREMENTS

- AS PER CODE, ALL GRADING IS TO SLOPE AWAY FROM ANY STRUCTURE. SURFACE OF THE GROUND WITHIN 10 FEET OF THE FOUNDATION SHOULD DRAIN AWAY FROM THE STRUCTURE WITH A MINIMUM FALL OF 6"
- AS PER CODE, FINISHED GRADE WILL NOT DRAIN ON NEIGHBORING PROPERTIES
- A MINIMUM OF 6" OF FOUNDATION WILL BE LEFT EXPOSED AT ALL CONDITIONS
- LANDSCAPE CONTRACTOR TO MAINTAIN OR IMPROVE FINAL GRADE AND PROPER DRAINAGE ESTABLISHED BY EXCAVATOR, INCLUDING BUT NOT LIMITED TO ANY MAINTENANCE, PRESERVATION, OR EXAGGERATION OF SLOPES, BERMS, AND SWALES
- LANDSCAPE CONTRACTOR IS RESPONSIBLE TO CORRECT ANY DAMAGED OR IMPROPER WATERFLOW OF ALL SWALES, BERMS, OR GRADE
- DEVICES FOR CHANNELING ROOF RUN-OFF SHOULD BE INSTALLED FOR COLLECTION AND DISCHARGE OF RAINWATER AT A MINIMUM OF 10' FROM THE FOUNDATION, OR BEYOND THE LIMITS OF FOUNDATION WALL BACKFILL WITH GREATER DISTANCE

### GENERAL LANDSCAPE NOTES

- LANDSCAPE CONTRACTOR SHALL HAVE ALL UTILITIES BLUE STAKED PRIOR TO DIGGING. ANY DAMAGE TO UTILITIES SHALL BE REPAIRED AT CONTRACTORS EXPENSE WITH NO ADDITIONAL COST TO THE OWNER
- DURING THE BIDDING AND INSTALLATION PROCESS, THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR VERIFYING QUANTITIES OF ALL MATERIALS. IF DISCREPANCIES EXIST, THE PLAN SHALL DICTATE QUANTITIES TO BE USED.
- ALL PLANT MATERIAL SHALL BE PLANTED ACCORDING TO ANSI STANDARDS WITH CONSIDERATION TO INDIVIDUAL SOIL AND SITE CONDITIONS, AND NURSERY CARE AND INSTALLATION INSTRUCTIONS.
- SELECTED PLANTS WILL BE ACCORDING TO THE PLANT LEGEND. IF SUBSTITUTIONS ARE NECESSARY, PROPOSED LANDSCAPE CHANGES MUST BE SUBMITTED TO THE LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO LAYING SOD.
- SHOULD THE SITE REQUIRE ADDITIONAL TOPSOIL, REFER TO SOIL TEST WHEN MATCHING EXISTING SOIL. IF A MATCHING SOIL IS NOT LOCATABLE, A 6" DEPTH OF SANDY LOAM TOPSOIL MIXED PRIOR TO SPREADING WITH 1% ORGANIC MATTER, CAN BE INCORPORATED INTO THE EXISTING SOIL USING THE FOLLOWING DIRECTIONS: SCARIFY TOP 6" OF EXISTING SUBSOIL AND INCORPORATE 3" OF NEW COMPOST ENRICHED TOPSOIL. SPREAD AND INCORPORATE TO REACH FINISHED GRADE.
- EDGING, AS INDICATED ON PLAN, IS TO BE INSTALLED BETWEEN ALL LAWN AND PLANTER AREAS. ANY TREES LOCATED IN LAWN MUST HAVE A 4' TREE RING OF THE SAME EDGING.
- LAWN/GRASS AREA
  - SOD
    - ON ALL LAWN AREAS TO RECEIVE MIN. 6" DEPTH OF QUALITY TOPSOIL. IF TOPSOIL IS PRESENT ON SITE, PROVIDE SOIL TEST TO DETERMINE SOIL QUALITY FOR PROPOSED HYDROSEEDING. FINE LEVEL ALL AREAS PRIOR TO LAYING SOD. ALL LAWN AREAS SHALL BE IRRIGATED WITH 100% COVERAGE BY POP-UP SPRAY HEADS AND GEAR-DRIVEN ROTORS. ALL DECIDUOUS AND CONIFER TREES PLANTED WITHIN SOD AREAS SHALL HAVE A FOUR FOOT(4') DIAMETER TREE RING COVERED WITH CHOCOLATE BROWN BARK MULCH. NO SHRUBS OR PERENNIALS SHALL BE APPROVED BY LANDSCAPE ARCHITECT AND OWNER BEFORE INSTALLATION.
    - SEED
      - SOIL TEST SOIL FOR ADEQUATE FERTILITY. ANY WEEDS CURRENTLY ON THE SITE SHALL BE REMOVED BY LITTER MECHANICAL MEANS SUCH AS HAND PULLING OR SPRAYING WITH AN HERBICIDE SUCH AS GLYPHOSATE MIXED WITH A SURFACTANT. HERBICIDES SHOULD BE APPLIED BY A CERTIFIED PESTICIDE APPLICATOR. COMPACTED SOIL SHALL BE SCARIFIED TO A DEPTH OF 18

## 2.1 LANDSCAPE MATERIALS

- A. TREE STAKING: ALL TREES SHALL BE STAKED FOR ONE YEAR WARRANTY PERIOD. ALL TREES NOT PLUMB SHALL BE REPLACED. STAKED TREES SHALL USE VINYL TREE TIES AND TREE STAKES TWO (2) INCH BY TWO (2) BY EIGHT (8) FOOT COMMON PINE STAKES USED AS SHOWN ON THE DETAILS.
- B. TREE WRAP: TREE WRAP IS NOT TO BE USED.
- C. MULCH/ROCK: SEE PLANS. ALL PLANTER BEDS TO RECEIVE A MINIMUM 3" LAYER FOR TREES, SHRUBS, AND PERENNIALS AND 1" FOR GROUND COVERS.
- D. WEED BARRIER: DEWITT 5 OZ. WEED BARRIER FABRIC, MANUFACTURED BY DEWITT COMPANY, DEWITT COMPANY COM OR APPROVED EQUAL.
- E. TREE, SHRUB, AND GRASS BACKFILL MIXTURE: BACKFILL MIXTURE TO BE 80% NATIVE SOIL AND 20% TOPSOIL, THOROUGHLY MIXED TOGETHER PRIOR TO PLACEMENT.
- F. TOPSOIL: REQUIRED FOR TURF AREAS, PLANTER BEDS AND BACKFILL MIXTURE. ACCEPTABLE TOPSOIL SHALL MEET THE FOLLOWING STANDARDS:
  - PH: 5.5-7.5
  - EC (ELECTRICAL CONDUCTIVITY): < 20 MMHOS PER CENTIMETER
  - SAR (SODIUM ABSORPTION RATION): < 30
  - OM (PERCENT ORGANIC MATTER): > 1%
  - TEXTURE (PARTICLE SIZE PER USDA SOIL CLASSIFICATION): SAND < 70%, CLAY < 30%, SILT < 70%, STONE FRAGMENTS (GRAVEL OR ANY SOIL PARTICLE GREATER THAN TWO (2) MM IN SIZE) < 5% BY VOLUME.
- G. TURF SOD: ALL SOD SHALL BE 18 MONTH OLD AS SPECIFIED ON PLANS (OR APPROVED EQUAL) THAT HAS BEEN CUT FRESH THE MORNING OF INSTALLATION. ONLY SOD THAT HAS BEEN GROWN ON A COMMERCIAL SOD FARM SHALL BE USED. ONLY USE SOD FROM A SINGLE SOURCE.
- H. LANDSCAPE CURB EDGING: SIX (6) INCHES BY FOUR (4) INCHES EXTRUDED CONCRETE CURB MADE UP OF THE FOLLOWING MATERIALS:
  - WASHED MORTAR SAND FREE OF ORGANIC MATTER.
  - PORTLAND CEMENT (SEE CONCRETE SPEC. BELOW FOR TYPE)
  - REINFORCED FIBER - SPECIFICALLY PRODUCED FOR COMPATIBILITY WITH AGGRESSIVE ALKALINE ENVIRONMENT OF PORTLAND CEMENT-BASED COMPOSITES.
  - ONLY POTABLE WATER FOR MIXING.
- I. LANDSCAPE METAL EDGING: 5.5" ALUMINUM EDGING WITH 15" DOWELS INTO THE GROUND FOR STABILIZATION.

## PART 3 - EXECUTION

### 3.1 GRADING

- A. TOPSOIL PREPARATION: GRADE PLANTING AREAS ACCORDING TO THE GRADING PLAN. ELIMINATE UNPLANTED AREAS AND LOW SPOTS. PROVIDE FOR PROPER GRADING AND DRAINAGE.
- B. TOPSOIL PLACEMENT: SLOPE SURFACED AWAY FROM BUILDING AT TWO (2) PERCENT SLOPE WITH NO POCKETS OF STANDING WATER. ESTABLISH FINISH GRADES OF ONE (1) INCHES FOR PLANTERS AND BELOW GRADE OF ADJACENT PAVED SURFACE. PROVIDE NEAT, SMOOTH, AND UNIFORM FINISH GRADES. REMOVE SURPLUS SUB-SOIL AND TOPSOIL FROM THE SITE.
- C. COMPACTION: COMPACTION UNDER HARD SURFACE AREAS (ASPHALT PATHS AND CONCRETE SURFACES) SHALL BE NINETY-FIVE (95) PERCENT. COMPACTION UNDER PLANTING AREAS SHALL BE BETWEEN EIGHTY-FIVE (85) AND NINETY (90) PERCENT.

### 3.2 TURF GRADING

- A. THE SURFACE ON WHICH THE SOD IS TO BE LAID SHALL BE FIRM AND FREE FROM FOOTPRINTS, DEPRESSIONS, OR UNDULATIONS OF ANY KIND. THE SURFACE SHALL BE FREE OF ALL MATERIALS LARGER THAN 1/2" IN DIAMETER.
- B. THE FINISH GRADE OF THE TOPSOIL ADJACENT TO ALL SIDEWALKS, MOW-STRIPS, ETC. PRIOR TO THE LAYING OF SOD, SHALL BE SET SUCH THAT THE CROWN OF THE GRASS SHALL BE AT THE SAME LEVEL AS THE ADJACENT CONCRETE OR HARD SURFACE, NO EXCEPTIONS.

### 3.3 PLANTING OPERATIONS

- A. REVIEW THE EXACT LOCATIONS OF ALL TREES AND SHRUBS WITH THE PROJECT REPRESENTATIVE FOR APPROVAL PRIOR TO THE DIGGING OF ANY HOLES. PREPARE ALL HOLES ACCORDING TO THE DETAILS ON THE DRAWINGS.
- B. WATER PLANTS IMMEDIATELY UPON ARRIVAL AT THE SITE. MAINTAIN IN MOIST CONDITION UNTIL PLANTED.
- C. BEFORE PLANTING, LOCATE ALL UNDERGROUND UTILITIES PRIOR TO DIGGING. DO NOT PLACE PLANTS ON OR NEAR UTILITY LINES.
- D. THE TREE PLANTING HOLE SHOULD BE THE SAME DEPTH AS THE ROOT BALL, AND TWO TIMES THE DIAMETER OF THE ROOT BALL.
- E. TREES MUST BE PLACED ON UNDISTURBED SOIL AT THE BOTTOM OF THE PLANTING HOLE.
- F. THE TREE HOLE DEPTH SHALL BE DETERMINED SO THAT THE TREE MAY BE SET SLIGHTLY HIGH OF FINISH GRADE, 1" TO 2" ABOVE THE BASE OF THE TRUNK FUR, USING THE TOP OF THE ROOT BALL AS A GUIDE.
- G. PLANT IMMEDIATELY AFTER REMOVAL OF CONTAINER FOR CONTAINER PLANTS.
- H. SET TREE ON SOIL AND REMOVE ALL BURLAP, WIRE BASKETS, TWINE, WRAPPINGS, ETC. BEFORE

## SITE MATERIALS

BEGINNING AND BACKFILLING OPERATIONS. DO NOT USE PLANTING STOCK IF THE BALL IS CRACKED OR BROKEN BEFORE OR DURING PLANTING OPERATION.

1. APPLY VITAMIN B-1 ROOT STIMULATOR AT THE RATE OF ONE (1) TABLESPOON PER GALLON.

J. UPON COMPLETION OF BACKFILLING OPERATION, THOROUGHLY WATER TREE TO COMPLETELY SETTLE THE SOIL, AND FILL ANY VOIDS THAT MAY HAVE OCCURRED. USE A WATERING HOSE. NOTE THE AREA IRRIGATION SYSTEM. IF ADDITIONAL PREPARED TOPSOIL MIXTURE NEEDS TO BE ADDED, IT SHOULD BE A COURSER MIX AS REQUIRED TO ESTABLISH FINISH GRADE AS INDICATED ON THE DRAWINGS.

K. THE AMOUNT OF PRUNING SHALL BE LIMITED TO THE MINIMUM NECESSARY TO REMOVE DEAD OR INJURED TWIGS AND BRANCHES. ALL CUTS, SCARS, AND BRUISES SHALL BE PROPERLY TREATED ACCORDING TO THE DIRECTION OF THE PROJECT REPRESENTATIVE. PROPER PRUNING OPERATIONS SHALL BE USED. DO NOT LEAVE CUTS AND DO NOT CUT THE LEADER BRANCH. IMPROPER PRUNING SHALL BE CAUSE FOR REJECTION OF THE PLANT MATERIAL.

L. PREPARE A WATERING CIRCLE OF 2" DIAMETER AROUND THE TRUNK. FOR CONIFERS, EXTEND THE WATERING WELL TO THE DRIP LINE OF THE TREE CANOPY. PLACE MULCH AROUND THE PLANTED TREES.

4. TURF - SOD LAYING

A. TOP SOIL AMENDMENTS: PRIOR TO LAYING SOD, COMMERCIAL FERTILIZER SHALL BE APPLIED AND INCORPORATED INTO THE UPPER FOUR (4) INCHES OF THE TOPSOIL. AT A RATE OF FOUR POUNDS OF NITROGEN PER ONE THOUSAND (1,000) SQUARE FEET. ADJUST FERTILIZATION MIXTURE AND RATE OF APPLICATION AS NEEDED TO MEET RECOMMENDATIONS GIVEN BY TOPSOIL ANALYSIS. INCLUDE OTHER AMENDMENTS AS REQUIRED.

B. FERTILIZATION: THREE WEEKS AFTER SOD PLACEMENT FERTILIZE THE TURF AT A RATE OF 1/2 POUND OF NITROGEN PER 1000 SQUARE FEET. USE FERTILIZER SPECIFIED ABOVE. ADJUST FERTILIZATION MIXTURE AND RATES TO MEET RECOMMENDATIONS GIVEN BY TOPSOIL ANALYSIS.

C. SOD AVAILABILITY AND CONDITION: SOD IS TO BE DELIVERED TO THE SITE IN GOOD CONDITION. IT IS TO BE INSPECTED UPON ARRIVAL AND INSTALLED WITHIN 24 HOURS. SOD IS TO BE MOIST AND COOL. DO NOT BEGIN SOD INSTALLATION UNTIL THE SOIL IS FREE OF PESTS, DISEASES, OR WEEDS. THE CONTRACTOR SHALL SATISFY HIMSELF AS TO THE EXISTING CONDITIONS PRIOR TO ANY CONSTRUCTION. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR FURNISHING AND LAYING ALL SOD REQUIRED ON THE PLANS. HE SHALL FURNISH NEW SOD AS SPECIFIED ABOVE AND LAY IT SO AS TO MOST COMPLETELY SATISFY THE INTENT AND MEANING OF THE PLANS AND SPECIFICATION AT NO EXTRA COST TO THE OWNER. IN THE CASE OF ANY DISCREPANCY IN THE AMOUNT OF SOD TO BE RECEIVED OR AMOUNT TO BE USED, IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO REPORT SUCH TO THE PROJECT REPRESENTATIVE PRIOR TO COMMENCING THE WORK.

D. SOD LAYING: THE SURFACE UPON WHICH THE NEW SOD TO BE LAID WILL BE PREPARED AS SPECIFIED IN THE DETAIL AND BE LIGHTLY WATERED BEFORE LAYING. AREAS WHERE SOD IS TO BE LAID SHALL BE CUT, TRIMMED, OR SHAPED TO RECEIVE FULL WIDTH SOD (MINIMUM TWELVE (12) INCHES). NO PARTIAL STRIP OR PIECES WILL BE ACCEPTED.

E. SOD SHALL BE TAMPED LIGHTLY AS EACH PIECE IS SET TO ENSURE THAT GOOD CONTACT IS MADE BETWEEN EDGES AND ALSO THE GROUND. IF VOIDS OR HOLES ARE DISCOVERED, THE SOD PIECE(S) IS (ARE) TO BE RAISED AND TOPSOIL IS TO BE USED TO FILL IN THE AREAS UNTIL LEVEL. SOD LAID ON ANY SLOPED AREAS SHALL BE ANCHORED WITH WOODEN DOWELS OR OTHER MATERIALS WHICH ARE ACCEPTED BY THE GRASS SOD INDUSTRY.

F. SOD SHALL BE ROLLED WITH A ROLLER THAT IS AT LEAST 50% FULL IMMEDIATELY AFTER INSTALLATION TO ENSURE THE FULL CONTACT WITH SOIL IS MADE.

G. APPLY WATER DIRECTLY AFTER LAYING SOD. RAINFALL IS NOT ACCEPTABLE.

H. WATERING OF THE SOD SHALL BE THE COMPLETE RESPONSIBILITY OF THE CONTRACTOR BY WHATEVER MEANS NECESSARY TO ESTABLISH THE SOD IN AN ACCEPTABLE MANNER TO THE END OF THE MAINTENANCE PERIOD. IF AN IRRIGATION SYSTEM IS IN PLACE ON THE SITE, BUT FOR WHATEVER REASON, WATER IS NOT AVAILABLE IN THE SYSTEM, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO WATER THE SOD BY WHATEVER MEANS, UNTIL THE SOD IS ACCEPTED BY THE PROJECT REPRESENTATIVE.

I. PROTECTION OF THE NEWLY LAID SOD SHALL BE THE COMPLETE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL PROVIDE ACCEPTABLE VISUAL BARRIERS, TO INCLUDE BARRICADES SET AT APPROPRIATE SPACINGS WITH STRINGS OR TAPES BETWEEN BARRIERS, AS AN INDICATION OF NEW WORK. THE CONTRACTOR IS TO RESTORE ANY DAMAGED AREAS CAUSED BY OTHERS (INCLUDING VEHICULAR TRAFFIC, EROSION, ETC. UNTIL SUCH TIME AS THE LAWN IS ACCEPTED BY THE OWNER.

J. ALL SOD THAT HAS NOT BEEN LAID WITHIN 24 HOURS SHALL BE DEEMED UNACCEPTABLE AND WILL BE REMOVED FROM THE SITE.

### 3.5 WEED BARRIER

- A. FOR THE HEALTH OF THE SOIL AND THE MICROORGANISMS, WEED BARRIER IS NOT RECOMMENDED. IF USE IS REQUIRED OR REQUESTED, DO NOT PLACE IN ANNUAL OR GRASS AREAS.
- B. CUT WEED BARRIER BACK TO THE EDGE OF THE PLANT FOOTBALL.
- C. OVERLAP ROWS OF FABRIC MIN. 6"
- D. STABLE FABRIC EDGES AND OVERLAPS TO GROUND.

## END OF SECTION

## SYMBOL CODE DESCRIPTION

SYMBOL	CODE	DESCRIPTION
	B	BENCH
	1	1 LANDSCAPE
	1-101	SODDED LAWN AREA LAWN AREAS SHALL BE SOD. NEW TURF AREAS TO BE SODDED WITH DROUGHT TOLERANT KENTUCKY BLUE GRASS OR APPROVED EQUAL. SEE SOD LANDSCAPE NOTES FOR FURTHER INFORMATION. SHEET LP-101.
	1-102	2-4" TAN CRUSHED ROCK. SUBMIT SAMPLES FOR LANDSCAPE ARCHITECT AND OWNER APPROVAL. PROVIDE 6" DEPTH OF ROCK MULCH TOP DRESSING. SEE INORGANIC MULCH LANDSCAPE NOTES FOR ADDITIONAL INFORMATION. SHEET LP-101.
	1-124	4-6" BROWN CRUSHED ROCK. SUBMIT SAMPLES FOR LANDSCAPE ARCHITECT AND OWNER APPROVAL. PROVIDE 6" DEPTH OF ROCK MULCH TOP DRESSING. SEE INORGANIC MULCH LANDSCAPE NOTES FOR ADDITIONAL INFORMATION. SHEET LP-101.
	2	2 HARDSCAPE
	2-106	CUT EDGE
	2-108	PLAYGROUND SURFACE. SOF'FALL ENGINEERED WOOD FIBER. INSTALL AS PER MANUFACTURERS SPECIFICATIONS.

# PLANT MATERIALS

QTY	SYMBOL	CODE	QTY	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	DROUGHT TOLERANT
1		PBS	13	Picea omarika 'Bruns' Bruns Serbian Spruce moderate; 25x12; sun; z4	B & B		6'	Yes
11,383			sf	SUBTOTAL:				
12,021		MCC	2	Malus x 'Coralburst' Coralburst Crabapple low; 10x12; sun; z3; Utah Lake water tolerant	20 gal.	2"	Cal	Yes
6,166		MRS	5	Malus x 'Raspberry Spear' Raspberry Spear Crabapple low; 20x8; sun; z4; Utah Lake water tolerant	B & B	2"	Cal	
		ZSW	5	Zelkova serrata 'Wireless' Wireless Zelkova Td4; 24x36; AV 490; sun; z5; Utah Lake water tolerant	B & B	2"	Cal	Yes
604		ZB	3	Zelkova serrata 'Burgundy Vase' Burgundy Vase Japanese Zelkova	B & B	2"	Cal	Yes
91.8			cy	SUBTOTAL:				

## SYMBOL CODE QTY BOTANICAL / COMMON NAME CONT

### DECIDUOUS SHRUBS

		BPA	7	Buddleja x 'SMNBDL' Pugster Amethyst Dwarf Butterfly Bush Sd3; 2x2; AV 12.5; sun; z5; Utah Lake water tolerant	5 gal			Yes
		CH	10	Cotoneaster horizontalis Rockspray Cotoneaster	5 gal			Yes
		CS	5	Cytisus scoparius 'Moonlight' Moonlight Scotch Broom	5 gal			Yes
		PBP	19	Prunus besseyi 'P011S' 'Pawnee Buttes' Pawnee Buttes Sand Cherry Sd1; 1.5 x 6; AV19.5; sun; z4;	5 gal			Yes
			41	SUBTOTAL:				

### GRASSES

		MML	34	Miscanthus sinensis 'Morning Light' Morning Light Maiden Grass Tw2; 5x4; AV 32; sun to light shade; z5; Utah Lake water tolerant	2 gal			Yes
			34	SUBTOTAL:				

### ROSES

		RDK	23	Rosa x 'Radiko' Double Knock Out Rose moderate; 3-4 x 3-4; sun; z5; Utah Lake water tolerant	5 gal			Yes
			23	SUBTOTAL:				

# LAKE MEADOWS PHASE 2-SITE REQUIREMENT CALCULATIONS

TOTAL PLANT QUANTITY:	REQUIRED:	PROVIDED:
DECIDUOUS TREES:	15	32
EVERGREEN TREES:	13	13
SHRUBS:	33	98 for coverage

PLANTED OPEN SPACE AREA: 42,246 SQ FT  
RIGHT OF WAY OPEN SPACE AREA: 7,879 SQ FT  
LANDSCAPING IN OPEN SPACE: 31,889 SQ FT  
PLAYGROUND AREA: 2,478 SQ FT

SIDEWALK AREA: 11,780 SQ FT  
DEFENTION BASIN: 10,787 SQ FT  
TURF: 28% (11,304 SQ FT)  
ROCK: 60%  
PLANT COVERAGE OF ROCK (19,939 SQ FT):  
TYPES OF ROCKS

50% (9,969 SQ FT)	50% (9,971 SQ FT)
2	2

DROUGHT TOLERANT TREES: 75%  
DROUGHT TOLERANT SHRUBS: 100%

LINEAR FEET OF TRAIL: 1,051'

AREA VALUE POINTS: 47.5 59.064

ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	DEVELOPER / PROPERTY OWNER / CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE STAMP	DRAWING INFO
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2/17/2026 UT25182

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# MEADOWS AT NORTH LAKE PH2 SARATOGA SPRINGS, UTAH

ANDERSON DEVELOPMENT  
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KEATON@AND-DEV.COM

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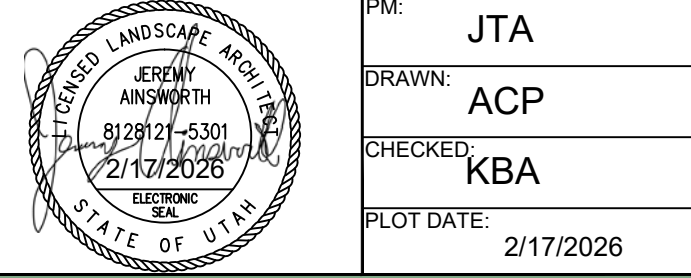
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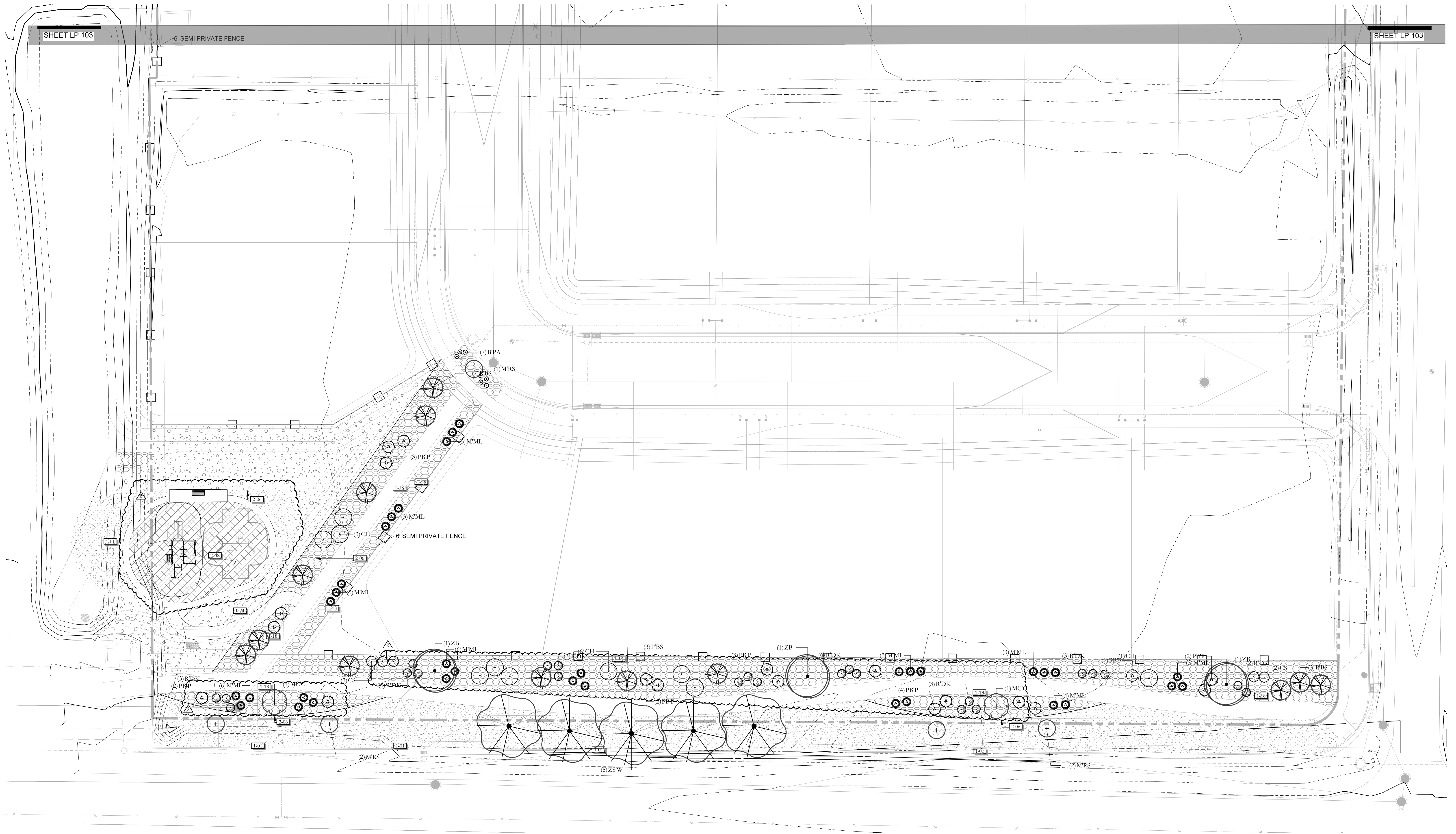
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LANDSCAPE COVER  
**CITY PERMIT SET**  
LP-101

NO.	REVISION	DATE
1	CITY COMMENTS	01-20-2026
2	CITY COMMENTS	01-22-2026
3		
4		

811 BLUE STAKES OF UTAH  
UTILITY LOCATION CENTER, INC  
1-800-662-4111  
www.bluestakes.org





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NO.	REVISION	DATE
1	CITY COMMENTS	01-20-2026
2	CITY COMMENTS	01-22-2026
3		
4		

**811** BLUE STAKES OF UTAH  
 UTILITY NOTIFICATION CENTER, INC.  
 1-800-662-4111  
 www.bluestakes.org

GRAPHIC SCALE: 1" = 20'

# MEADOWS AT NORTH LAKE PH2

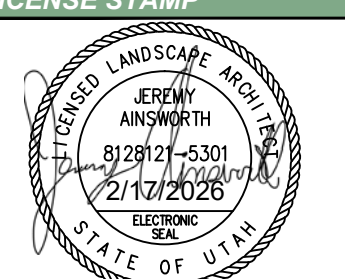
## SARATOGA SPRINGS, UTAH

SHEET LP 103  
 SHEET LP 102

DEVELOPER / PROPERTY OWNER / CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE STAMP	DRAWING INFO
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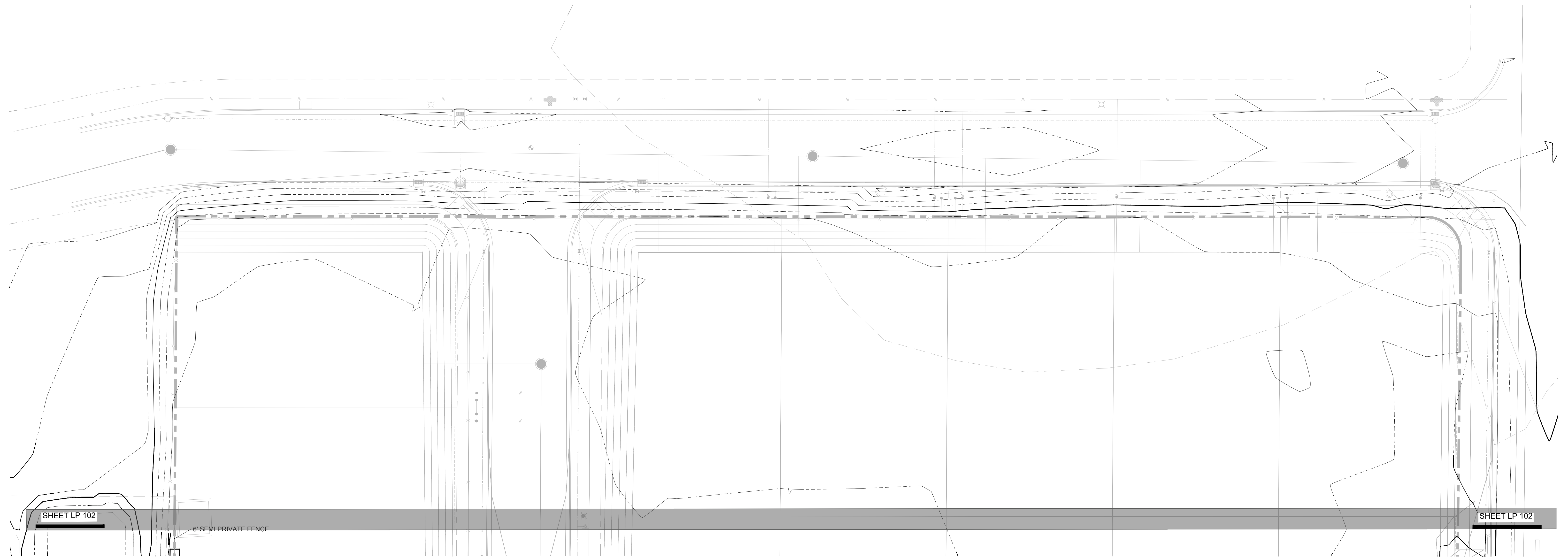
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 LEHI, UTAH 84043 (801) 995-2217  
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PM: JTA  
 DRAWN: ACP  
 CHECKED: KBA  
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**LANDSCAPE PLAN**  
 CITY PERMIT SET  
 LP-102



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2	CITY COMMENTS	01-22-2026
3		
4		

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 UTILITY NOTIFICATION CENTER, INC  
 1-800-662-4111  
 www.bluestakes.org

GRAPHIC SCALE: 1" = 20'

# MEADOWS AT NORTH LAKE PH2

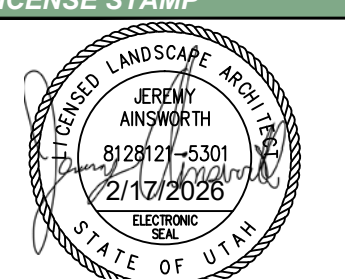
## SARATOGA SPRINGS, UTAH

SHEET LP 103  
 SHEET LP 102

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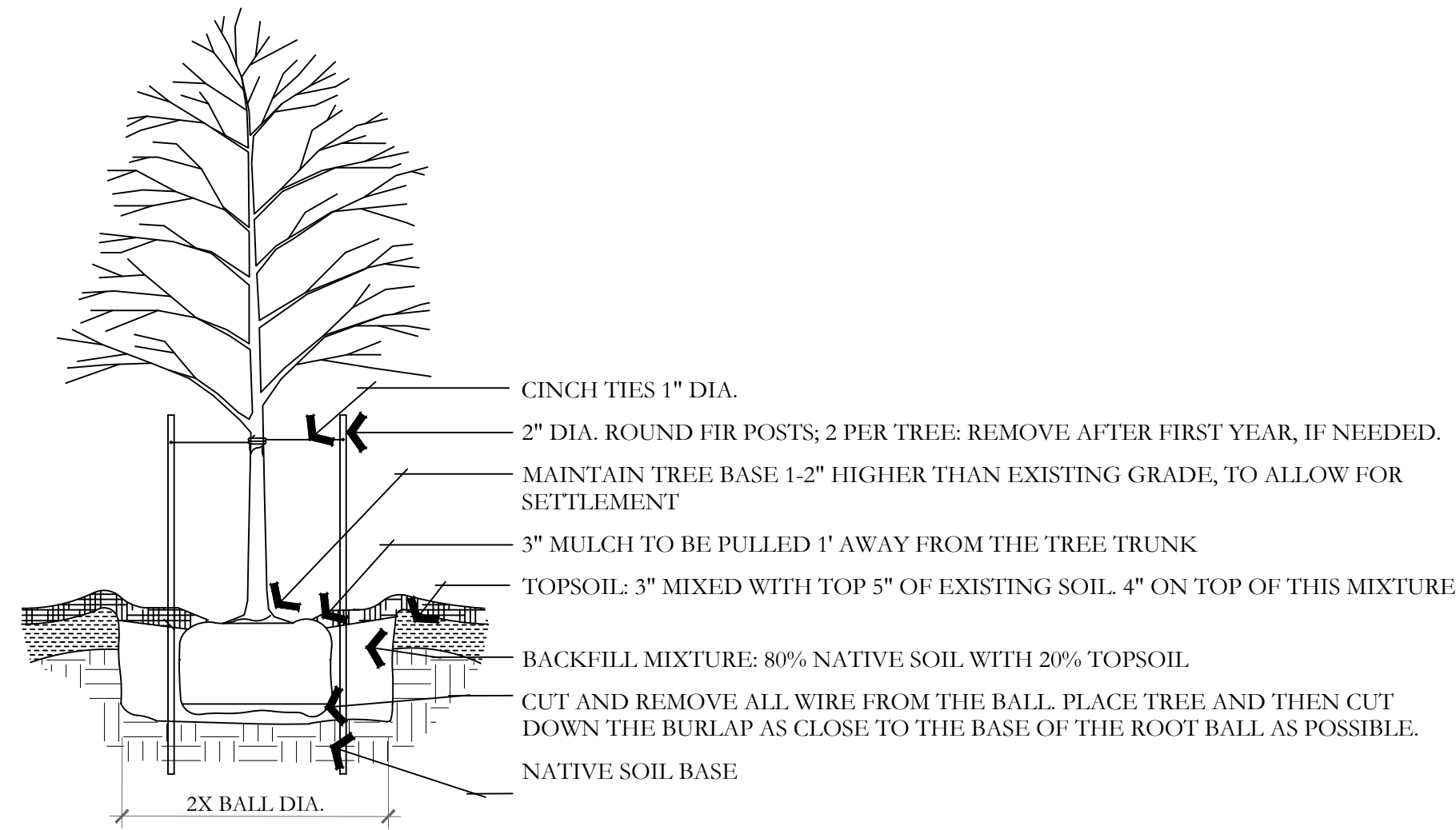
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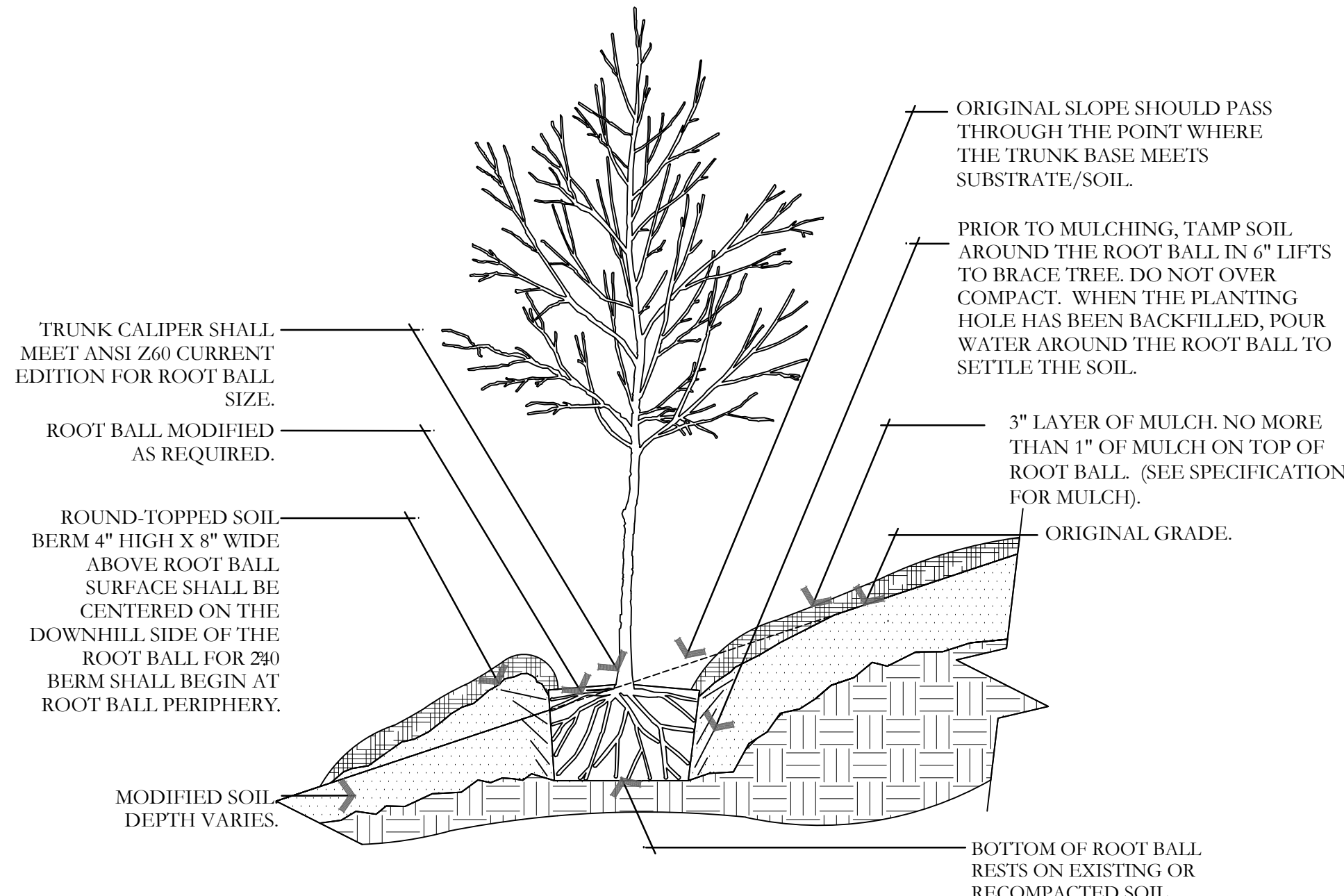
**LANDSCAPE PLAN**  
**CITY PERMIT SET**  
**LP-103**

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PLOT DATE:	2/17/2026



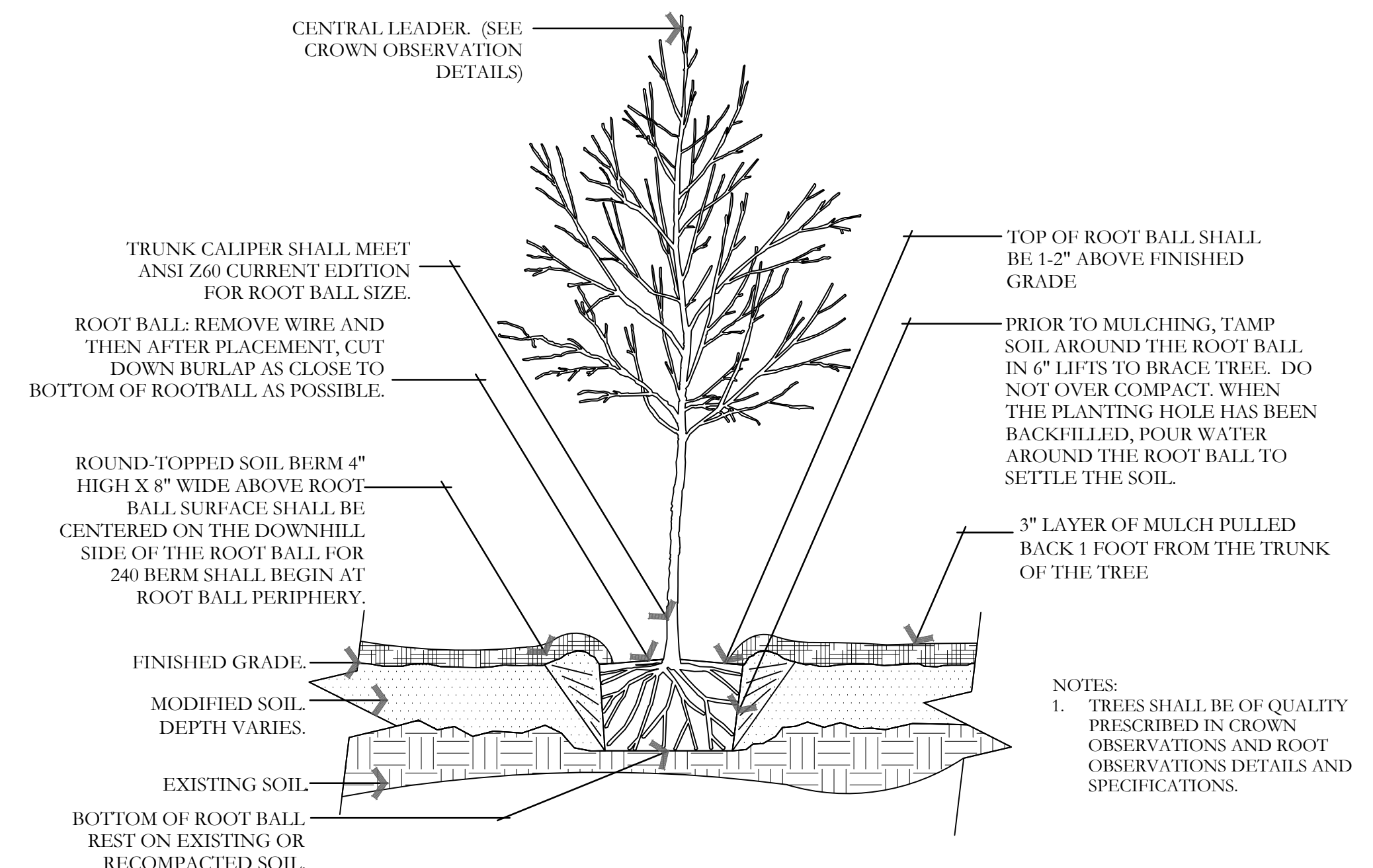
**A DECIDUOUS TREE PLANTING**

NOT TO SCALE



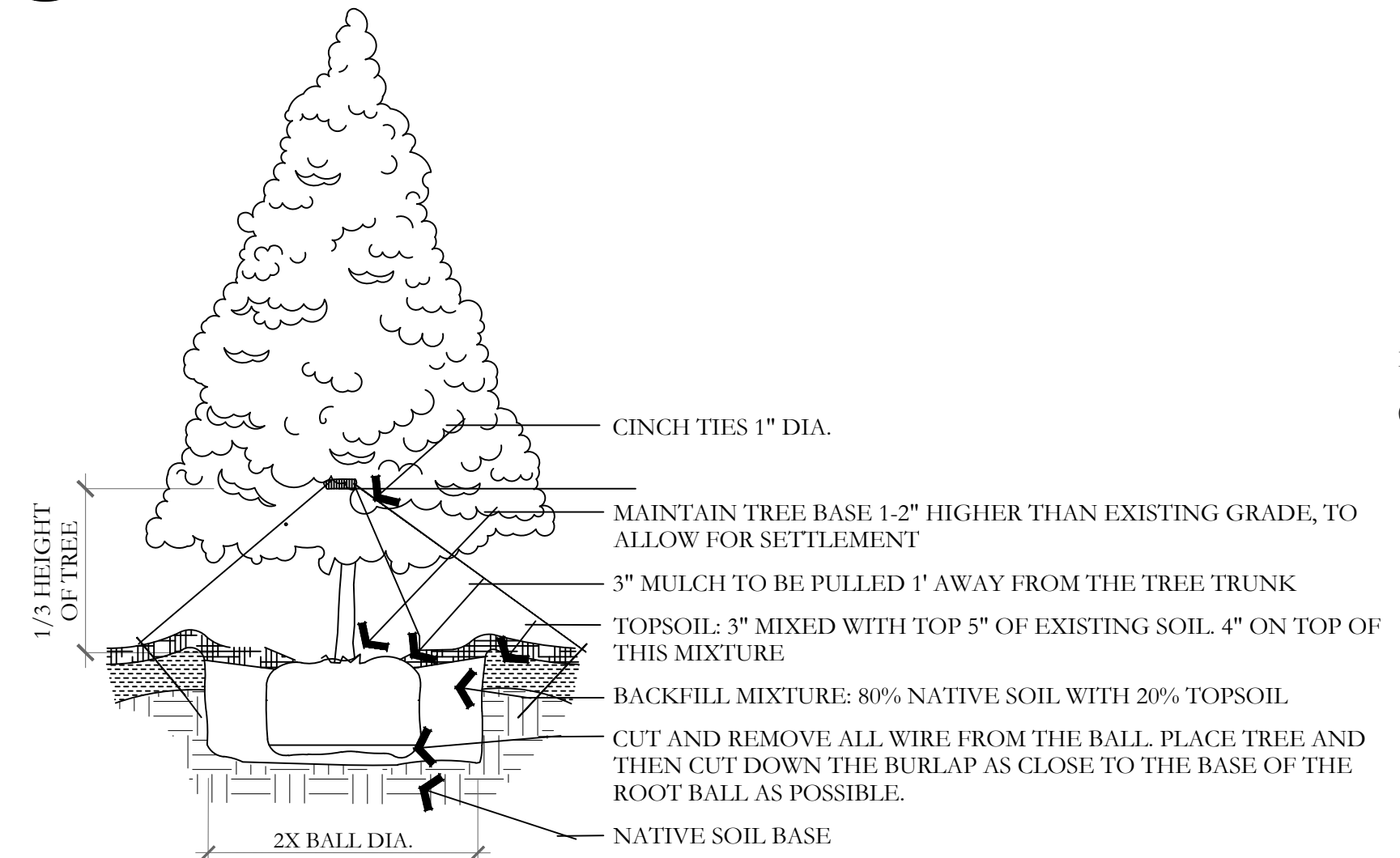
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NOT TO SCALE



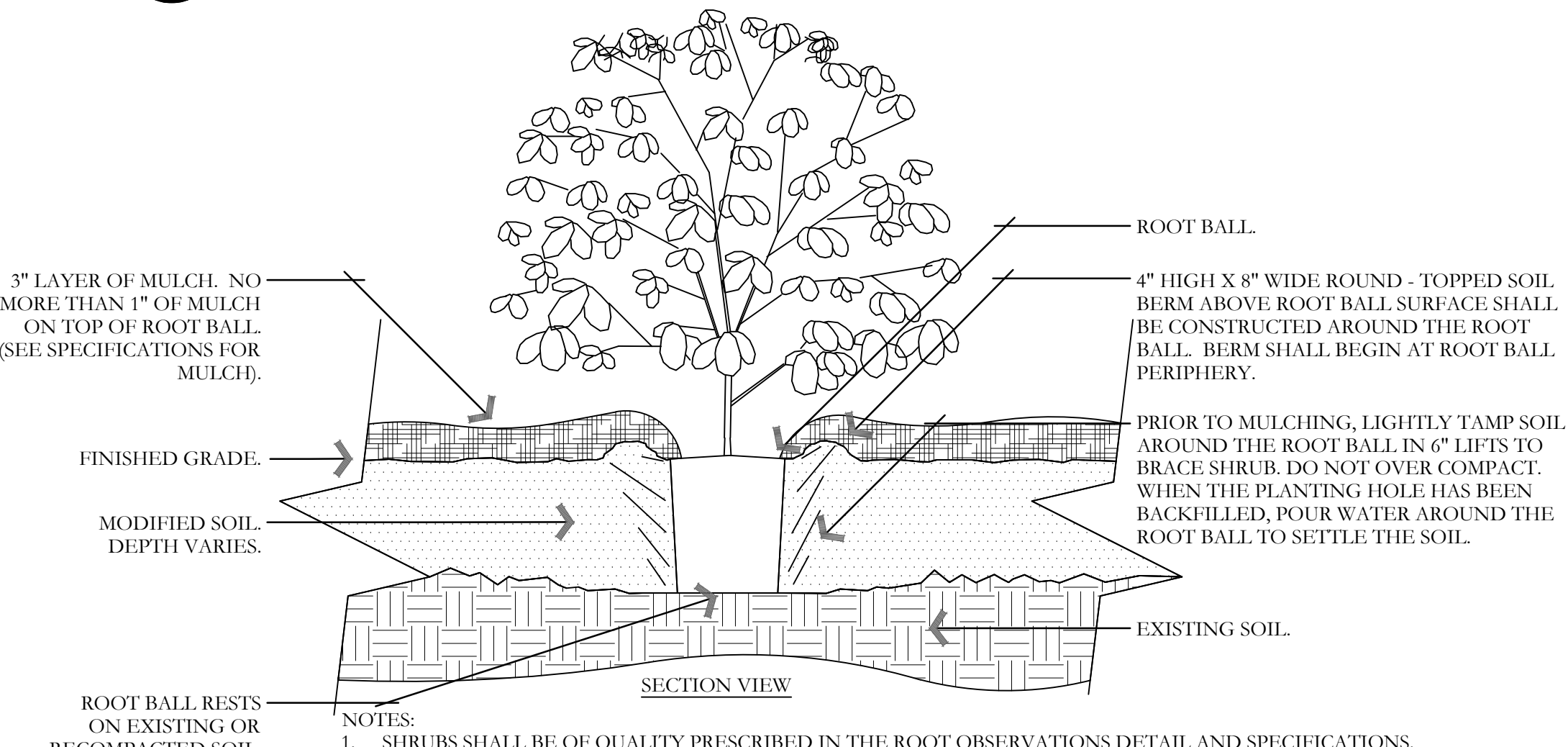
**C TREE W/ BERM (EXISTING SOIL MODIFIED)**

NOT TO SCALE



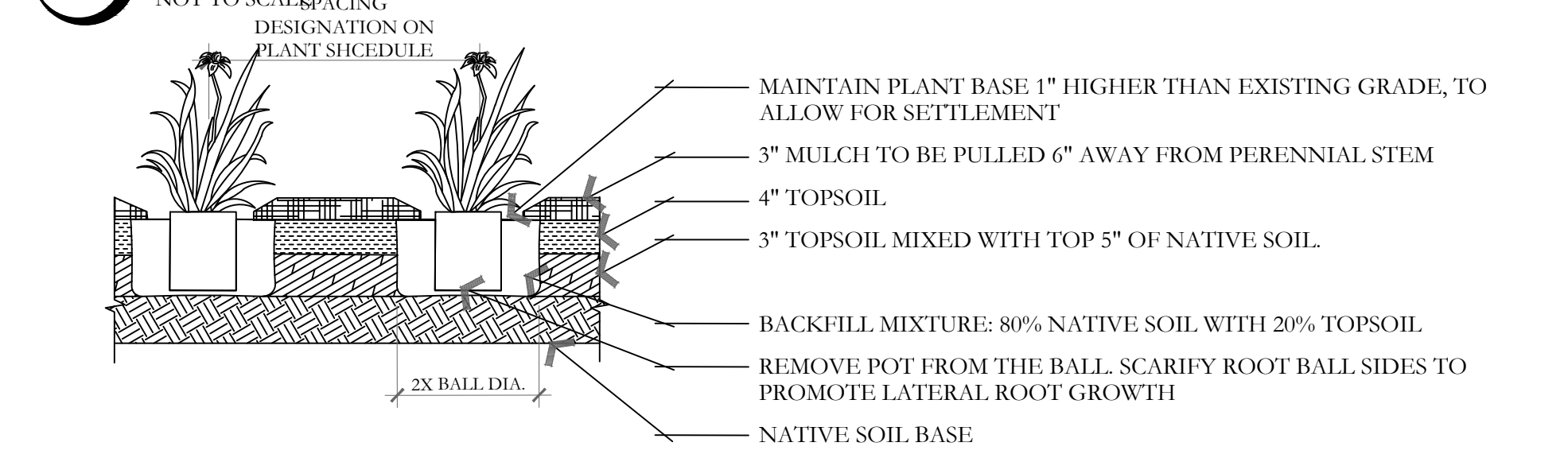
**D EVERGREEN TREE PLANTING**

NOT TO SCALE



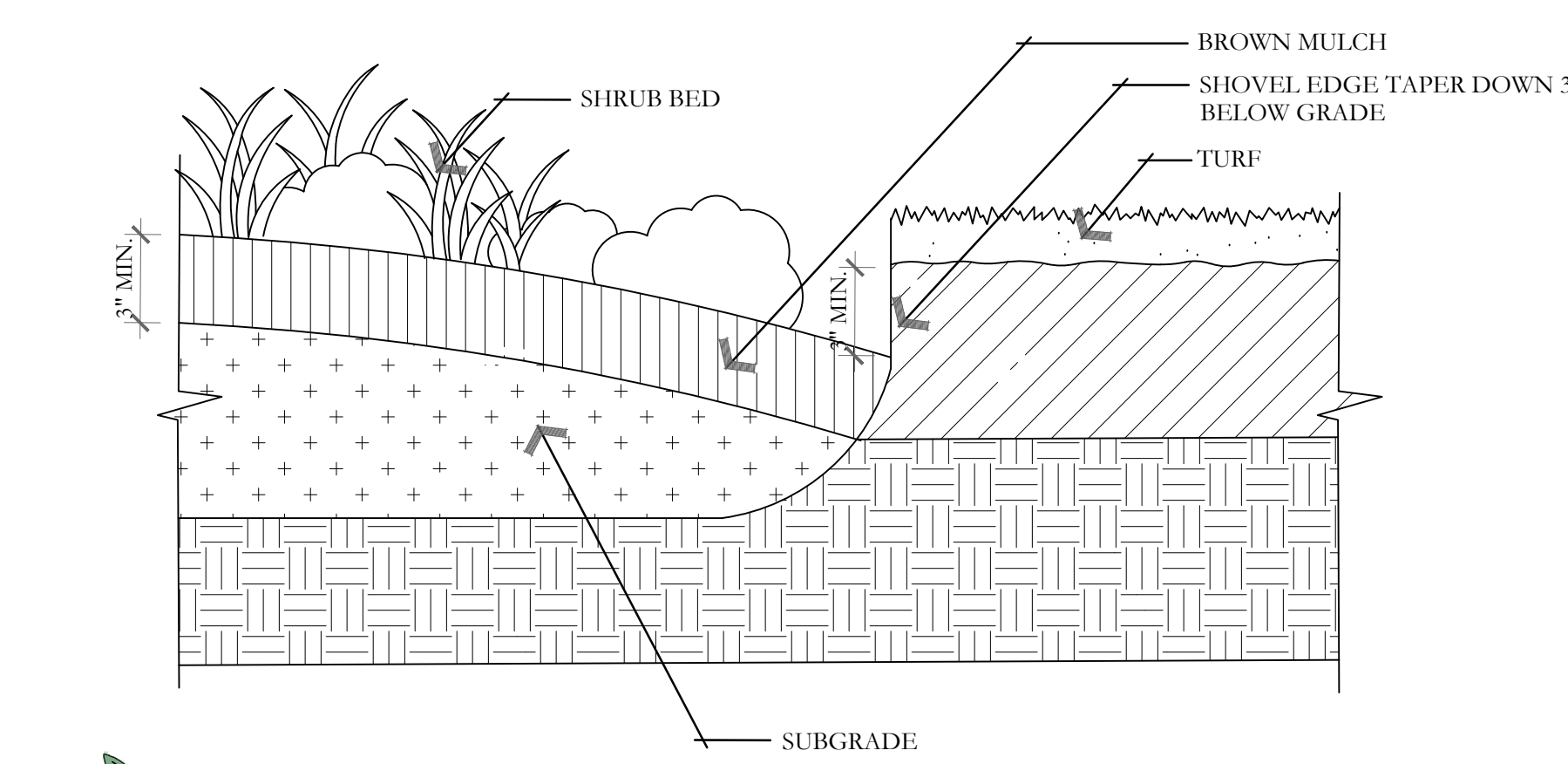
**E SHRUB - MODIFIED SOIL**

NOT TO SCALE



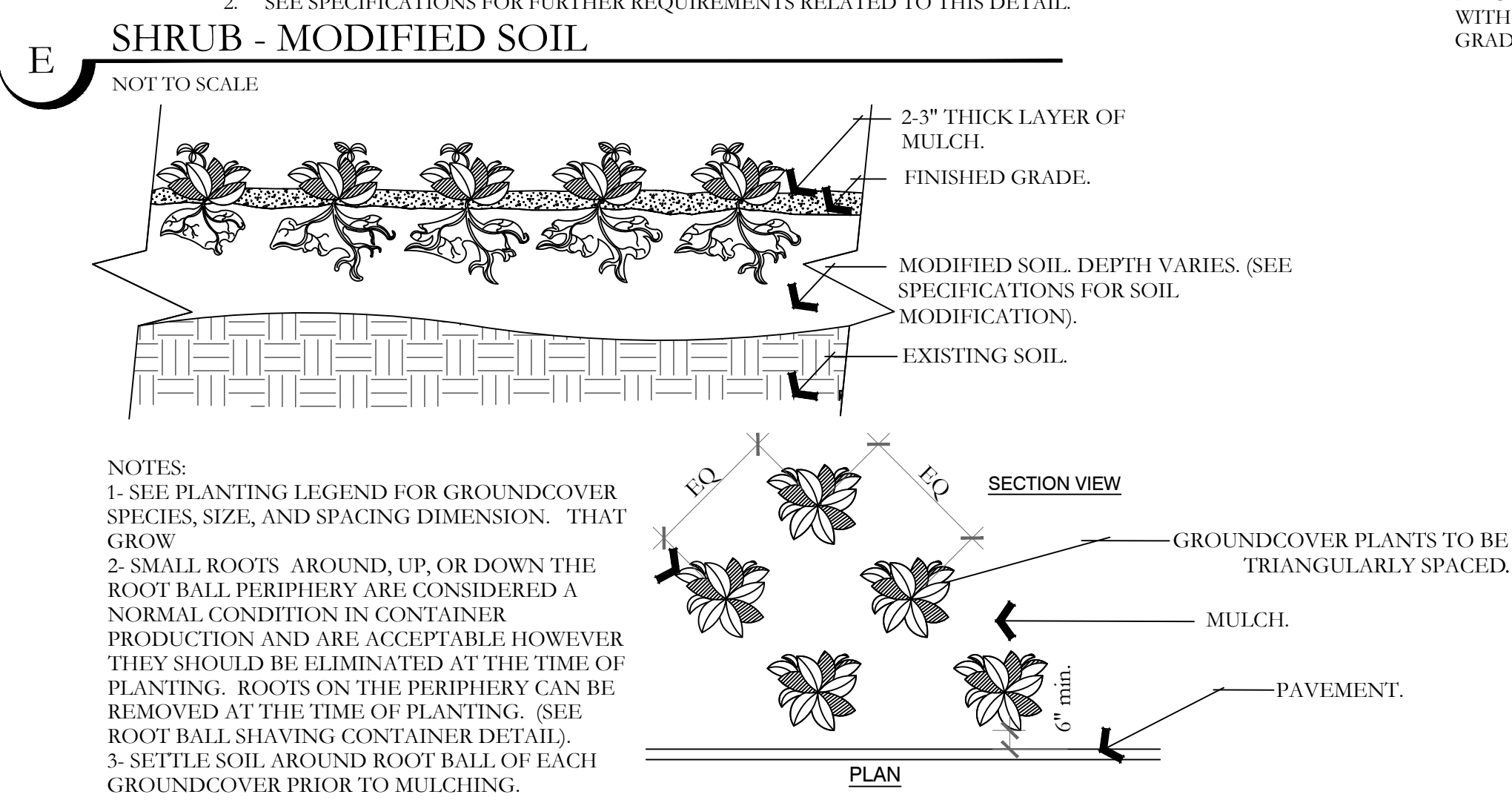
**F PERENNIAL PLANTING**

NOT TO SCALE



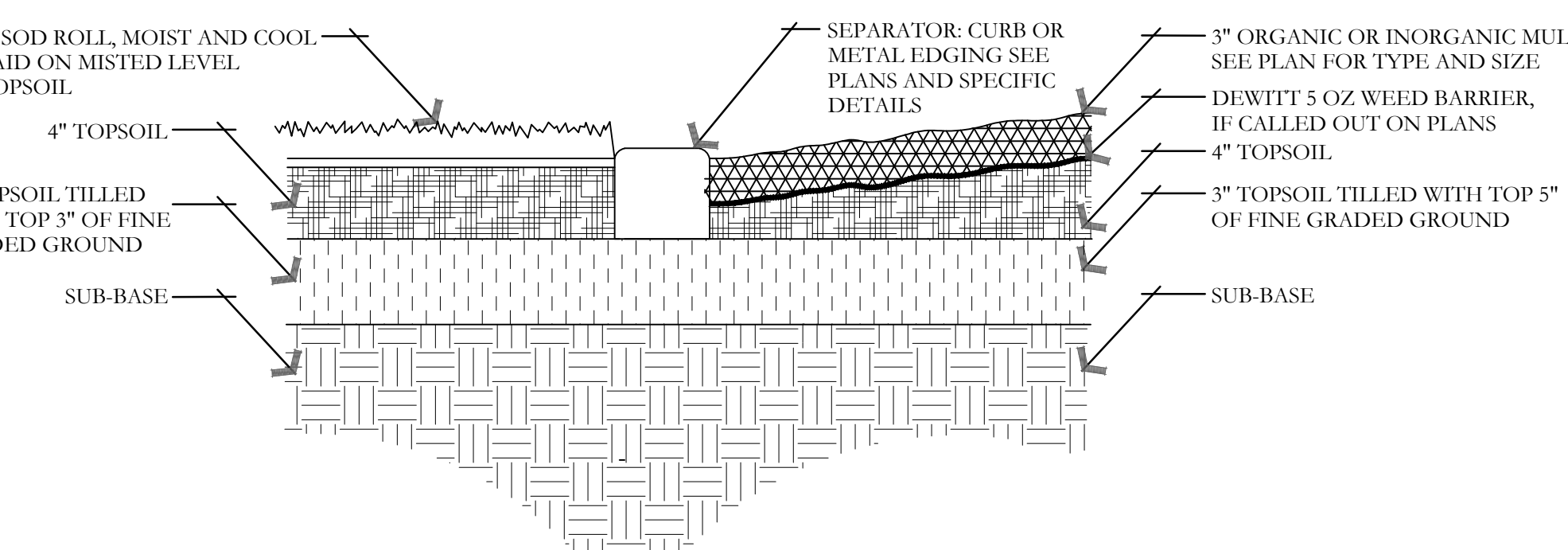
**G CUT EDGE DETAIL**

NOT TO SCALE



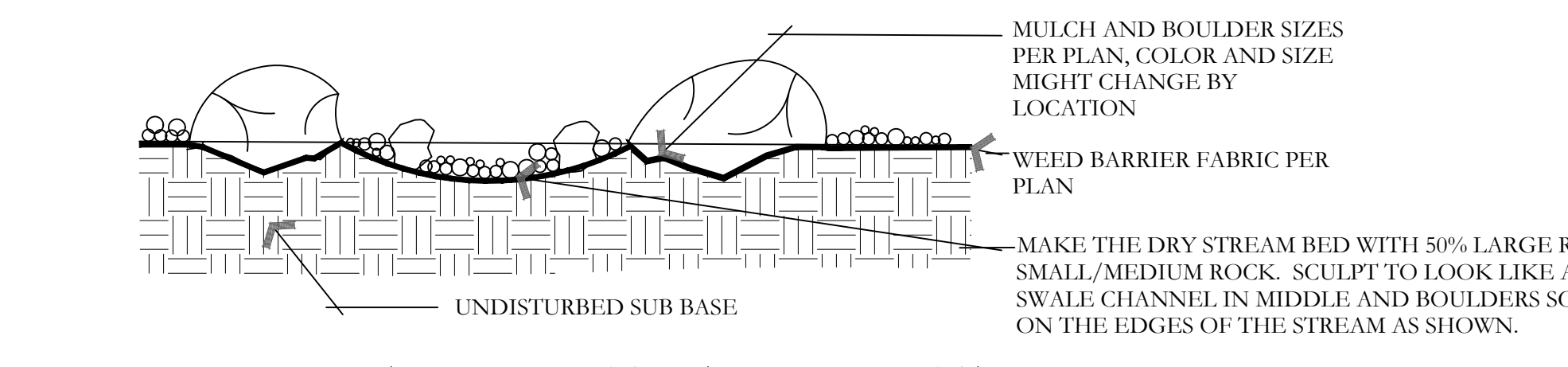
**H PERENNIAL/GROUNDCOVER PLANTING**

NOT TO SCALE



**I SOD LAYING/MULCH DETAIL**

NOT TO SCALE



**J BOULDER AND DRY STREAM BED DETAIL**

NOT TO SCALE

ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	PROJECT INFORMATION	DEVELOPER / PROPERTY OWNER / CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE STAMP	DRAWING INFO
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2/17/2026 UT25182 THIS PRINT FROM PKJ DESIGN GROUP IS BASED ON CIVIL AND ARCHITECT INFORMATION ISSUED ON 02/17/2026

NO.	REVISION	DATE
1	CITY COMMENTS	01-20-2026
2	CITY COMMENTS	01-22-2026
3		
4		

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# MEADOWS AT NORTH LAKE PH2

## SARATOGA SPRINGS, UTAH

ANDERSON DEVELOPMENT  
KEATON MORTON  
801-856-2478  
KEATON@AND-DEV.COM

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DATE: 2/17/2026  
PLOT DATE: 2/17/2026

**LANDSCAPE DETAILS**  
CITY PERMIT SET  
LP-501

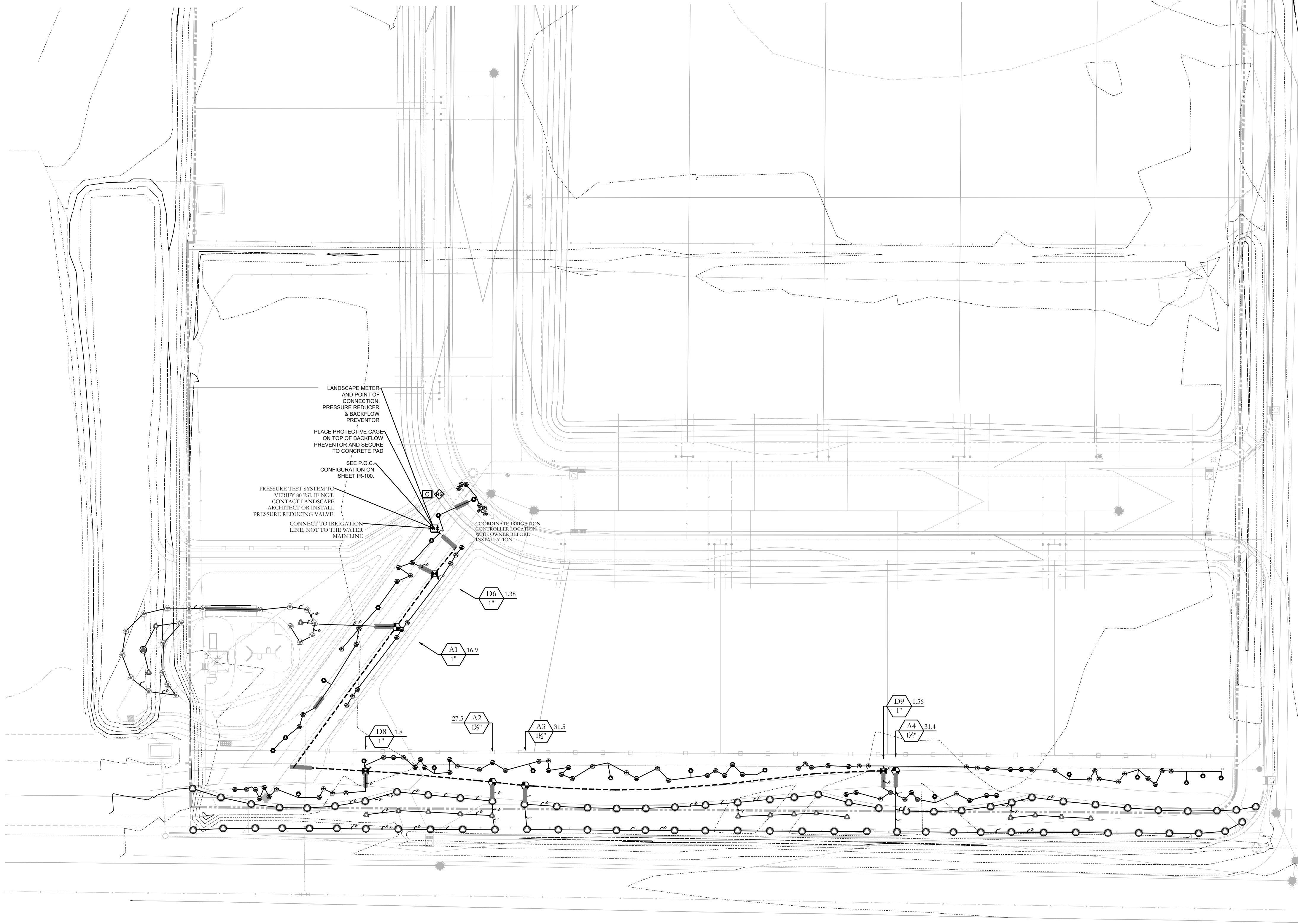
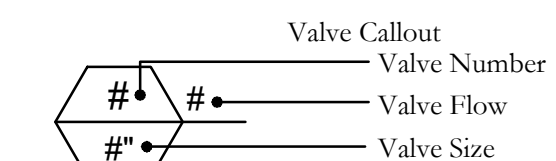
**P.O.C. CONFIGURATION**

(NOTE: PRESSURE TEST SYSTEM TO VERIFY 80 PSI. IF NOT, CONTACT LANDSCAPE ARCHITECT OR INSTALL PRESSURE REDUCING VALVE.)

- SOURCE DATA
- BACKFLOW PREVENTOR
- SHUTOFF VALVE
- MASTER VALVE
- 1" FLOW SENSOR
- QUICK COUPLER

**IRRIGATION LEGEND** (NOTE: PLANT QUANTITIES ARE PROVIDED FOR CONVENIENCE ONLY. IN CASE OF DISCREPANCY, THE DRAWING SHALL TAKE PRECEDENCE. IF YOU NEED HELP WITH A WATER BUDGET CONSULTATION, OR A PLANT MAINTENANCE SCHEDULE, CONTACT PKJ@PKJDESIGNGROUP.COM)

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	PSI
	Rain Bird R-VAN14 1806-SAM-P45 Turf Rotary, 8ft.-14ft. 45-270 degrees and 360 degrees. Hand Adjustable Multi-Stream Rotary w/1800 turf spray body on 6in. pop-up, with check valve and 45 psi in-stem pressure regulator. 1/2in. NPT Female Threaded Inlet.	37	45
	Rain Bird R-VAN18 1806-SAM-P45 Turf Rotary, 13ft.-18ft. 45-270 degrees and 360 degrees. Hand Adjustable Multi-Stream Rotary w/1800 turf spray body on 6in. pop-up, with check valve and 45 psi in-stem pressure regulator. 1/2in. NPT Female Threaded Inlet.	75	45
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	
	Rain Bird XCZ-100-IVMQ (2) 1" Wide Flow IVM Drip Control Kit for Commercial Applications. 1in. Ball Valve with 1in. PESBIVM Smart Valve w/ factory installed IVM-SOL 0.3-20 gpm and 1in. Pressure Regulating 40psi Quick-Check Basket Filter 0.3-20 gpm	3	
	Rain Bird XFS-09-18 Drip Ring(SHRUB)	98	
	Rain Bird XFS-09-18 Drip Ring(TREE)	20	
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	
	Rain Bird PESB-IVM 1-1/2" 1in., 1-1/2in., 2in. Plastic Industrial Smart Valves w/ Factory Installed IVM-SOL. Low Flow Operating Capability, Globe Configuration. With Scrubber Technology for Reliable Performance in Dirty Water Irrigation Applications.	3	
	Rain Bird PESB-IVM 1" 1in., 1-1/2in., 2in. Plastic Industrial Smart Valves w/ Factory Installed IVM-SOL. Low Flow Operating Capability, Globe Configuration. With Scrubber Technology for Reliable Performance in Dirty Water Irrigation Applications.	1	
	Rain Bird 44-RC 1" 1in. Brass Quick-Coupling Valve, with Corrosion-Resistant Stainless Steel Spring, Thermoplastic Rubber Cover, and 2-Piece Body.	1	
	Shut Off Valve	1	
	Rain Bird EFB-CP-PRS-D 1-1/2" 1in., 1-1/4", 1-1/2in., 2in. Brass Master Valve, that is Contamination Proof w/Self-Flushing Filter Screen. Globe Configuration, Reclaimed Water Compatible, and Purple Handle Cover Designates Non-Potable Water Use. With Pressure Regulator.	1	
	Zurn 475 2-1/2" Reduced Pressure Backflow Preventer.	1	
	Rain Bird ESPLXIVM 60 Station, 2-Wire Controller w/ Smart Valve Technology. (1) ESPLXIVM 60-Station, Indoor/Outdoor, Plastic Wall-Mount Cabinet. System Requirements: Rain Bird LXIVM-XXX Integrated Valve Modules & 2-Wire Devices. Use Paige Electric Cable P7072D & Rain Bird WC20 Dry Splices ONLY. Ground System w/ (X) LXIVMSD Surge Device in Rain Bird Round Valve Boxes. Install Per Manufacturers Recommendations.	1	
	Rain Bird WR2-RFS Wireless Rain/Freeze Sensor.	1	
	Rain Bird FS-200-B 2in. Flow Sensor, Brass Model. Suggested Operating Range 10 GPM to 100 GPM. Size for Flow Not According to Pipe Size. Rain Bird Compatible Controllers: ESP-LXIVM(P)   LXD   LXME2(P)   ME3, or Controllers Accepting Custom K-Factor and Offset. Install in Rain Bird Valve Box.	1	
	Water Meter 1"	1	
	Irrigation Lateral Line: PVC Schedule 40 3/4"	2,118 lf	
	Irrigation Lateral Line: PVC Schedule 40 1"	401.3 lf	
	Irrigation Lateral Line: PVC Schedule 40 1 1/4"	216.6 lf	
	Irrigation Lateral Line: PVC Schedule 40 1 1/2"	8.2 lf	
	Irrigation Lateral Line: PVC Schedule 40 2"	28.4 lf	
	Irrigation Mainline: PVC Schedule 40	472.5 lf	
	Pipe Sleeve: PVC Class 200 SDR 21	110.0 lf	



LANDSCAPE METER AND POINT OF CONNECTION. PRESSURE REDUCER & BACKFLOW PREVENTOR. PLACE PROTECTIVE CAGE ON TOP OF BACKFLOW PREVENTOR AND SECURE TO CONCRETE PAD. SEE P.O.C. CONFIGURATION ON SHEET IR-100. PRESSURE TEST SYSTEM TO VERIFY 80 PSI. IF NOT, CONTACT LANDSCAPE ARCHITECT OR INSTALL PRESSURE REDUCING VALVE. CONNECT TO IRRIGATION LINE, NOT TO THE WATER MAIN LINE. COORDINATE IRRIGATION CONTROLLER LOCATION WITH OWNER BEFORE INSTALLATION.

ISSUE DATE PROJECT NUMBER PLAN INFORMATION PROJECT INFORMATION DEVELOPER / PROPERTY OWNER / CLIENT LANDSCAPE ARCHITECT / PLANNER LICENSE STAMP DRAWING INFO

2/17/2026 UT25182 THIS PRINT FROM PKJ DESIGN GROUP IS BASED ON UTILITY NOTIFICATION CENTER, INC. CIVIL AND ARCHITECT INFORMATION ISSUED ON 02/17/2026 ANDERSON DEVELOPMENT KEATON MORTON 801-856-2478 KEATON@AND-DEV.COM JTA

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www.pkjdesigngroup.com

**IRRIGATION OVERALL PLAN**  
CITY PERMIT SET  
IR-100

PKJ DESIGN GROUP LICENSED LANDSCAPE ARCHITECT JTA ANS WORTH 2/17/2026 STATE OF UTAH

PKJ DRAWN: ACP CHECKED: KBA PLOT DATE: 2/17/2026

# IRRIGATION PLAN SPECIFICATIONS

## IRRIGATION SPECIFICATIONS

### PART 1 - GENERAL

#### 1. SUMMARY

WORK TO BE DONE INCLUDES ALL LABOR, MATERIALS, EQUIPMENT AND SERVICES REQUIRED TO COMPLETE THE PROJECT. IRRIGATION SYSTEM AS INDICATED ON THE CONSTRUCTION DRAWINGS, AND AS SPECIFIED HEREIN, INCLUDES BUT IS NOT LIMITED TO: FURNISHING AND INSTALLING UNDERGROUND AND ABOVE-GROUND SPRINKLER SYSTEM COMPLETE WITH ANY ACCESSORIES NECESSARY FOR PROPER FUNCTION AND OPERATION OF THE SYSTEM. ALL PLANT MATERIAL ON THE PROJECT SHALL BE IRRIGATED. REMOVE AND DISPOSE OF ANY EXISTING SPRINKLER SYSTEM COMPONENTS WHICH ARE DISTURBED DURING THE CONSTRUCTION PROCESS AND ARE NOT TO BE SAVED. RESTORATION OF ANY ALTERED OR DAMAGED EXISTING LANDSCAPE TO ORIGINAL STATE AND CONDITION.

#### 1.2 SYSTEM DESCRIPTION

A. DESIGN OF IRRIGATION COMPONENTS: LOCATIONS OF IRRIGATION COMPONENTS ON CONSTRUCTION DRAWINGS MAY BE APPROXIMATE. PIPING, SLEEVING AND/OR OTHER COMPONENTS SHOWN ON CONSTRUCTION DRAWINGS MAY BE SHOWN SCHEMATICALLY FOR GRAPHIC CLARITY AND DEMONSTRATION OF COMPONENT GROUPINGS AND SEPARATIONS. ALL IRRIGATION COMPONENTS SHALL BE PLACED IN LANDSCAPED AREAS, WITH THE EXCEPTION OF POPE AND WIRE IN SLEEVING UNDER HARDCAPES. ACTUAL ROUTING OF PIPE, WIRE OR OTHER COMPONENTS MAY BE ALTERED DUE TO SITE CONDITIONS NOT ACCOUNTED FOR IN THE DESIGN PROCESS.

B. CONSTRUCTION REQUIREMENTS: ACTUAL PLACEMENT MAY VARY AS REQUIRED TO ACHIEVE A MINIMUM OF 100% COVERAGE WITHOUT OVERSPRAY ONTO HARDCAPE, BUILDINGS OR OTHER FEATURES.

C. LAYOUT OF IRRIGATION COMPONENTS: DURING LAFFOUR AND STAKING, CONSULT WITH OWNER APPROVED REPRESENTATIVE (HEREAFTER REFERRED TO AS OAR) TO VERIFY PROPER PLACEMENT OF IRRIGATION COMPONENTS, AND TO PROVIDE CONTRACTOR RECOMMENDATIONS FOR CHANGES WHERE NECESSARY. SMALL OR MINOR ADJUSTMENTS TO SYSTEM LAYOUT ARE PERMISSIBLE TO AVOID EXISTING FIELD OBSTRUCTIONS SUCH AS UTILITY BOXES OR STREET LIGHT POLES. CONTRACTOR SHALL PLACE REMOTE CONTROL VALVES IN GROUPS AS PRACTICAL TO ECONOMIZE ON QUANTITY OF MANIFOLD ISOLATION VALVES. QUICK COUPLER VALVES SHALL BE PLACED WITH MANIFOLD GROUPS AND PROTECTED BY MANIFOLD ISOLATION VALVES. QUICK COUPLER VALVES ARE SHOWN ON CONSTRUCTION DOCUMENTS IN APPROXIMATE LOCATIONS.

#### 1.3 DEFINITIONS

A. WATER SUPPLY: CULINARY WATER PIPING AND COMPONENTS, FURNISHED AND INSTALLED BY OTHERS TO PROVIDE IRRIGATION WATER TO THIS PROJECT, INCLUDING BUT NOT LIMITED TO BACKFLOW PREVENTOR, SADDLES, NIPPLES, SPOOLS, SHUT OFF VALVES, CORPORATION STOP VALVES, WATER METERS, PRESSURE REGULATION VALVES, AND PIPING UPSTREAM OF (OR PRIOR TO) THE POINT OF CONNECTION.

B. POINT OF CONNECTION: LOCATION WHERE THE CONTRACTOR SHALL TAKE INTO THE WATER SUPPLY. MAY REQUIRE BACKFLOW PREVENTOR, SADDLE, NIPPLES, SPOOLS, ISOLATION VALVES OR STOP AND WASTE VALVE FOR LANDSCAPE IRRIGATION NEEDS AND USE.

C. MAIN LINE PIPING: PRESSURIZED PIPING DOWNSTREAM OF THE POINT OF CONNECTION TO PROVIDE WATER TO REMOTE CONTROL VALVES AND QUICK COUPLERS, NORMALLY UNDER CONSTANT PRESSURE.

D. LATERAL LINE PIPING: CIRCUIT PIPING DOWNSTREAM OR REMOTE CONTROL VALVES TO PROVIDE WATER TO SPRINKLER HEADS, DRIP SYSTEMS OR BUBBLERS.

#### 1.4 REFERENCES

A. THE FOLLOWING STANDARDS WILL APPLY TO THE WORK OF THIS SECTION:

- a. ASTM-AMERICAN SOCIETY FOR TESTING AND MATERIALS
- b. IA - THE IRRIGATION ASSOCIATION: MAIN BMP DOCUMENT, LANDSCAPE IRRIGATION SCHEDULING AND WATER MANAGEMENT DOCUMENT.

#### 1.5 SUBMITTALS

A. AT LEAST THIRTY (30) DAYS PRIOR TO ORDERING OF ANY MATERIALS, THE CONTRACTOR SHALL PROVIDE MANUFACTURER CATALOG CUT SHEET AND CURRENT PRINTED SPECIFICATIONS FOR EACH ELEMENT OR COMPONENT OF THE IRRIGATION SYSTEM. SUBMITTALS SHALL BE IN THREE RING BINDERS OR OTHER SIMILAR BOUND FORM, PROVIDE FIVE COPIES OF SUBMITTALS TO OAR FOR DISTRIBUTION. PLACE COVER OR INDEX SHEET INDICATING ORDER IN SUBMITTAL DOCUMENT. NO MATERIAL SHALL BE ORDERED, DELIVERED OR ANY WORK PERFORMED IN THE FIELD UNTIL THE REQUIRED SUBMITTALS HAVE BEEN REVIEWED IN ITS ENTIRETY AND STAMPED APPROVED. DELIVERED MATERIAL SHALL MATCH THE APPROVED SAMPLES.

B. OPERATION AND MAINTENANCE MANUAL:

- a. AT LEAST THIRTY (30) DAYS PRIOR TO FINAL INSPECTION, THE CONTRACTOR SHALL PROVIDE OPERATION AND MAINTENANCE MANUAL TO OAR, CONTAINING:
  - i. MANUFACTURER CATALOG CUT SHEET AND CURRENT PRINTED SPECIFICATIONS FOR EACH ELEMENT OR COMPONENT OF THE IRRIGATION SYSTEM.
  - ii. PARTS LIST FOR EACH OPERATING ELEMENT OF THE SYSTEM
  - iii. MANUFACTURER PRINTED LITERATURE ON OPERATION AND MAINTENANCE OF OPERATING ELEMENTS OF THE SYSTEM.

c. SECTION LISTING INSTRUCTIONS FOR OVERALL SYSTEM OPERATION AND MAINTENANCE. INCLUDE DIRECTION FOR SPRING START-UP AND WINTERIZATION.

d. PROJECT RECORD COPY

e. MAINTAIN AT PROJECT SITE ONE COPY OF ALL PROJECT DOCUMENTS CLEARLY MARKED "PROJECT RECORD COPY". MARK ANY DEVIATION IN MATERIAL INSTALLATION ON CONSTRUCTION DRAWINGS, MAINTAIN AND UPDATE DRAWING AT LEAST WEEKLY. PROJECT RECORD COPY TO BE AVAILABLE TO OAR ON DEMAND.

f. COMPLETED PROJECT AS-BUILT DRAWINGS

1. PRIOR TO FINAL INSPECTION, PREPARE AND SUBMIT TO OAR ACCURATE AS-BUILT DRAWINGS INCLUDING 2" WIRE PATH AND JUNCTION BOX LOCATIONS.

2. SHOW DETAIL AND DIMENSION CHANGES MADE DURING INSTALLATION. SHOW SIGNIFICANT DETAILS AND DIMENSIONS THAT WERE NOT SHOWN IN ORIGINAL CONTRACT DOCUMENTS.

3. FIELD DIMENSION LOCATIONS OF SLEEVING, POINTS OF CONNECTION, MAIN LINE PIPING, WIRING RUNS NOT CONTAINED IN MAIN LINE PIPE TRENCHES, VALVES AND VALVE BOXES, QUICK COUPLER VALVES.

4. DIMENSIONS ARE TO BE TAKEN FROM PERMANENT CONSTRUCTED SURFACES, FEATURES, OR FINISHED EDGES LOCATED AT OR ABOVE FINISHED GRADE.

5. CONTROLLER MAP: UPON COMPLETION OF SYSTEM, PLACE IN EACH CONTROLLER A COLOR CODED COPY OF THE AREA THAT CONTROLLER SERVICES, INDICATING ZONE NUMBER, TYPE OF PLANT MATERIAL AND LOCATION ON PROJECT THAT ZONE SERVES. LAMINATE MAP WITH HEAT SHRINK CLEAR PLASTIC.

#### 1.6 QUALITY ASSURANCE

A. ACCEPTANCE: DO NOT INSTALL WORK IN THIS SECTION PRIOR TO ACCEPTANCE BY OAR.

B. REGULATORY REQUIREMENTS: ALL WORK AND MATERIALS SHALL BE ACCORDING TO ANY AND ALL RULES, REGULATIONS OR CODES, WHETHER THEY ARE STATE OR LOCAL LAWS AND ORDINANCES. CONTRACT DOCUMENTS, DRAWINGS OR SPECIFICATIONS MAY NOT BE CONSTRUED OR INTERPRETED TO PERMIT WORK OR MATERIALS NOT CONFORMING TO THE ABOVE CODES.

C. ADEQUATE WATER SUPPLY: WATER SUPPLY TO THIS PROJECT EXISTS, INSTALLED BY OTHERS. CONNECTIONS TO THESE SUPPLY LINES SHALL BE BY THIS CONTRACTOR. VERIFY THAT PROPER CONNECTION IS AVAILABLE TO SUPPLY LINE AND IS OF ADEQUATE SIZE. VERIFY THAT SECONDARY CONNECTION COMPONENTS MAY BE INSTALLED IF NECESSARY. PERFORM STATIC PRESSURE TEST PRIOR TO COMMENCEMENT OF WORK AT SUPPLIED POC. NOTIFY OAR IN WRITING OF PROBLEMS ENCOUNTERED AND PRESSURE READING PRIOR TO PROCEEDING.

D. WORKMANSHIP AND MATERIALS:

- a. IT IS THE INTENT OF THIS SPECIFICATION THAT ALL MATERIAL HEREIN SPECIFIED AND SHOWN ON THE CONSTRUCTION DOCUMENTS SHALL BE OF THE HIGHEST QUALITY AVAILABLE AND MEETING THE REQUIREMENTS SPECIFIED.
- b. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE BEST STANDARDS OF PRACTICE RELATING TO THE TRADE.

E. CONTRACTOR QUALIFICATIONS:

a. CONTRACTOR SHALL PROVIDE DOCUMENT OR RESUME INCLUDING AT LEAST THE FOLLOWING ITEMS:

i. THAT CONTRACTOR HAS BEEN INSTALLING SPRINKLERS ON COMMERCIAL PROJECTS FOR FIVE PREVIOUS CONSECUTIVE YEARS.

ii. CONTRACTOR IS LICENSED TO PERFORM LANDSCAPE AND IRRIGATION CONSTRUCTION IN THE STATE OF THIS PROJECT.

iii. CONTRACTOR IS BONDFORMER FOR THE WORK TO BE PERFORMED.

iv. REFERENCES OF FIVE PROJECTS OF SIMILAR SIZE AND SCOPE COMPLETED WITHIN THE LAST FIVE YEARS. THREE OF THE PROJECTS LISTED SHALL BE LOCAL.

v. LISTING OF SUPPLIERS WHERE MATERIALS WILL BE OBTAINED FOR USE ON THIS PROJECT.

vi. PROJECT SITE FOREMAN OR SUPERVISOR HAS AT LEAST FIVE CONSECUTIVE YEARS OF COMMERCIAL IRRIGATION INSTALLATION EXPERIENCE. THIS PERSON SHALL BE A CURRENT CERTIFIED IRRIGATION CONTRACTOR IN GOOD STANDING AS SET FORTH BY THE IRRIGATION ASSOCIATION. THIS PERSON SHALL BE ON PROJECT SITE AT LEAST 75% OF EACH WORKING DAY.

vii. EVIDENCE THAT CONTRACTOR CURRENTLY EMPLOYS WORKERS IN SUFFICIENT QUANTITIES TO COMPLETE PROJECT WITHIN TIME LIMITS THAT ARE ESTABLISHED BY THE CONTRACT.

viii. ALL GENERAL LABORERS OR WORKERS ON THE PROJECT SHALL BE PREVIOUSLY TRAINED AND FAMILIAR WITH SPRINKLER INSTALLATION AND HAVE A MINIMUM OF ONE-YEAR EXPERIENCE. THOSE WORKERS PERFORMING TASKS RELATED TO PVC PIPE SHALL HAVE CERTIFICATES DESIGNATED BELOW.

1.7 DELIVERY-STORAGE-HANDLING

A. DURING DELIVERY, INSTALLATION AND STORAGE OF MATERIALS FOR PROJECT, ALL MATERIALS SHALL BE PROTECTED FROM CONTAMINATION, DAMAGE, VANDALISM, AND PROLONGED EXPOSURE TO SUNLIGHT. ALL MATERIAL STORED AT PROJECT SITE SHALL BE NEATLY ORGANIZED IN A COMPACT ARRANGEMENT AND STORAGE SHALL NOT DISRUPT PROJECT OWNER OR OTHER TRADES ON PROJECT SITE. ALL MATERIAL TO BE INSTALLED SHALL BE HANDLED BY CONTRACTOR WITH CARE TO AVOID BREAKAGE OR DAMAGE. DAMAGED MATERIALS ATTRIBUTED TO CONTRACTOR SHALL BE REPLACED WITH NEW AT CONTRACTOR'S EXPENSE.

1.8 SEQUENCING

A. BEFORE SITE SURVEY, RESEARCH UTILITY RECORDS, CONTACT UTILITY LOCATION SERVICES. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH ALL HAZARDS AND UTILITIES PRIOR TO WORK COMMENCEMENT. INSTALL SLEEVING PRIOR TO INSTALLATION OF CONCRETE, PAVING OR OTHER PERMANENT SITE ELEMENTS. IRRIGATION SYSTEM POINT OF CONNECTION COMPONENTS, BACKFLOW PREVENTION AND PRESSURE REGULATION DEVICES SHALL BE INSTALLED AND OPERATIONAL PRIOR TO ALL DOWNSTREAM COMPONENTS. ALL MAIN LINES SHALL BE THOROUGHLY FLUSHED OF ALL DEBRIS PRIOR TO INSTALLATION OF ANY SPRINKLER HEADS.

1.9 WARRANTY

A. CONTRACTOR SHALL PROVIDE ONE YEAR WARRANTY. WARRANTY SHALL COVER ALL MATERIALS, WORKMANSHIP AND LABOR. WARRANTY SHALL INCLUDE FILLING AND OR REPAIRING DEPRESSIONS OR REPLACING TURF OR OTHER PLANTINGS DUE TO SETTLEMENT OF IRRIGATION TRENCHES OR IRRIGATION SYSTEM ELEMENTS. VALVE BOXES, SPRINKLERS OR OTHER COMPONENTS SETTLED FROM ORIGINAL FINISH GRADE SHALL BE RESTORED TO PROPER GRADE. IRRIGATION SYSTEM SHALL HAVE BEEN ADJUSTED TO PROVIDE PROPER, ADEQUATE COVERAGE OF IRRIGATED AREAS.

1.10 OWNER'S INSTRUCTION

A. AFTER SYSTEM IS INSTALLED, INSPECTED, AND APPROVED, INSTRUCT OWNER'S REPRESENTATIVES IN COMPLETE OPERATION AND MAINTENANCE PROCEDURES. COORDINATE INSTRUCTION WITH REFERENCES TO PREVIOUSLY SUBMITTED OPERATION AND MAINTENANCE MANUAL.

1.11 MAINTENANCE

A. FURNISH THE FOLLOWING ITEMS TO OWNERS REPRESENTATIVE:

- a. TWO QUICK COUPLER KEYS WITH HOSE SWIVELS.
- b. ONE OF EACH TYPE OR SIZE OF QUICK COUPLER VALVE AND REMOTE CONTROL VALVE. FIVE PERCENT OF TOTAL QUANTITIES USED OF EACH SPRINKLER AND SPRINKLER NOZZLE.

B. PROVIDE THE FOLLOWING SERVICES:

- a. WINTERIZE: ENTIRE IRRIGATION SYSTEM INSTALLED UNDER THIS CONTRACT. WINTERIZE BY "BLOW-OUT" METHOD USING COMPRESSED AIR. COMPRESSOR SHALL BE CAPABLE OF MINIMUM OF 175 CFM. THIS OPERATION SHALL OCCUR AT THE END OF FIRST GROWING SEASON AFTER NEED FOR PLANT IRRIGATION BUT PRIOR TO FREEZING. COMPRESSOR SHALL BE CAPABLE OF EVACUATING SYSTEM OF ALL WATER. PRESSURE REGULATION DEVICES, COMPRESSOR SHALL BE REGULATED TO NOT MORE THAN 60 PSI. START UP SYSTEM THE FOLLOWING SPRING AFTER DANGER OF FREEZING HAS PASSED. CONTRACTOR SHALL TRAIN OWNERS REPRESENTATIVE IN PROPER START-UP AND WINTERIZATION PROCEDURE.

1.12 LATERAL LINE PIPE

A. ALL LATERAL PIPING SHALL BE SCHEDULE 40 PVC, SOLVENT WELD, AND BELL END. LATERAL PIPE SHALL BE BURIED WITH 12" OF COVER TYPICALLY. LATERAL PIPE SHALL BE 3/4", 1", 1 1/2", 1 3/4" OR 2" IN SIZE, AS INDICATED ON CONSTRUCTION DRAWINGS.

1.13 LATERAL LINE FITTINGS

A. ALL LATERAL LINE FITTINGS SHALL BE S/40 PVC

1.14 SPRAY SPRINKLERS

A. SPRAY HEAD SPRINKLERS SHALL BE AS SPECIFIED ON THE DRAWINGS. NOZZLES SHALL BE AS SPECIFIED ON THE DRAWINGS.

1.15 RAIN BIRD VALVE BOXES

A. CARSON VALVE BOXES SHALL BE USED ON THIS PROJECT. SIZES ARE AS DIRECTED IN THESE SPECIFICATIONS. DETAIL SHEETS OR PLAN SHEETS. VALVE BOXES SHALL BE CENTERED OVER THE CONTROL VALVE OR ELEMENT THEY COVER. VALVE BOX SHALL BE SIZED LARGE ENOUGH TO ALLOW AMPLE ROOM FOR SERVICES ACCESS, REMOVAL OR REPLACEMENT OF VALVE OR ELEMENT. VALVE BOX SHALL BE SET TO FINISH GRADE OF TOPSOIL OR BARKED AREAS.

CONTRACTOR SHALL PROVIDE EXTENSIONS OR STACK ADDITIONAL VALVE BOXES AS NECESSARY TO BRING VALVE BOX PIT TO PROPER GRADE.

1.16 IMPORT BACKFILL

A. ALL MAIN LINE PIPE, LATERAL LINE PIPE AND OTHER IRRIGATION ELEMENTS SHALL BE BEDDED AND BACKFILLED WITH CLEAN SOIL, FREE OF ROCKS 1" AND LARGER. CONTRACTOR SHALL FURNISH AND INSTALL ADDITIONAL BACKFILL MATERIAL AS NECESSARY DUE TO ROCKY CONDITIONS. TRENCHES AND OTHER ELEMENTS SHALL BE COMPACTED AND/OR WATER SETTLED TO ELIMINATE SETTLING. DEBRIS FROM TRENCHING OPERATIONS UNUSABLE FOR FILL SHALL BE REMOVED FROM PROJECT AND DISPOSED OF PROPERLY BY CONTRACTOR.

1.17 OTHER PRODUCTS

A. SUBSTITUTION OF EQUIVALENT PRODUCTS IS SUBJECT TO THE LANDSCAPE ARCHITECT OR OARS APPROVAL AND MUST BE DESIGNATED AS ACCEPTED IN WRITING.

- a. THE CONTRACTOR SHALL PROVIDE MATERIALS TO MAKE THE SYSTEM COMPLETE AND OPERATIONAL.

1.18 PREPARATION

A. CONTRACTOR SHALL REPAIR OR REPLACE WORK DAMAGED BY IRRIGATION SYSTEM INSTALLATION. IF DAMAGED WORK IS NEW, REPAIR OR REPLACEMENT SHALL BE PERFORMED BY THE ORIGINAL INSTALLER OF THAT WORK. THE EXISTING LANDSCAPE OF THIS PROJECT SHALL REMAIN IN PLACE. CONTRACTOR SHALL PROTECT AND WORK AROUND EXISTING PLANT MATERIAL. COORDINATION OF TRENCH AND VALVE LOCATIONS SHALL BE Laid OUT FOR THE OAR PRIOR TO ANY EXCAVATION OCCURRING. PLANT MATERIAL DEEMED DAMAGED BY THE OAR SHALL BE REPLACED WITH NEW PLANT MATERIAL AT CONTRACTOR'S EXPENSE. CONTRACTOR SHALL NOT CUT EXISTING TREE ROOTS LARGER THAN 2" TO INSTALL THIS PROJECT. ROUTE PIPE, WIRE AND IRRIGATION ELEMENTS AROUND TREE CANOPY DRIP LINE TO MINIMIZE DAMAGE TO TREE ROOTS. CONTRACTOR SHALL HAVE NO PART OF EXISTING SYSTEM USED BY OTHER PORTIONS OF SITE LANDSCAPE WITHOUT WATER FOR MORE THAN 24 HOURS AT A TIME.

1.19 TRENCHING AND BACKFILLING

A. PULLING OF PIPE SHALL NOT BE PERMITTED ON THIS PROJECT. OPEN EXCAVATE TRENCHES BOTH IN WIDTH AND DEPTH ENSURE BASE OF TRENCH IS ROCK OR DEBRIS FREE TO PROTECT PIPE AND WIRE. GRADE TRENCH BASE TO ENSURE FLAT, EVEN SUPPORT OF PIPING. BACKFILL WITH CLEAN SOIL OR IMPORT MATERIAL. CONTRACTOR SHALL BACKFILL, NO LESS THAN 2" AROUND ENTIRE PIPE WITH CLEAN, ROCK FREE FILL. MAIN LINE PIPING AND FITTINGS SHALL NOT BE BACKFILLED UNTIL OAR HAS INSPECTED AND PIPE HAS PASSED PRESSURE TESTING. PERFORM BALANCE OF BACKFILL OPERATION TO ELIMINATE ANY SETTLING.

1.20 SLEEVING

A. CONTRACTOR SHALL BE RESPONSIBLE TO PROTECT EXISTING UNDERGROUND UTILITIES AND

COMPONENTS. SLEEVING MINIMUM SIZE SHALL BE 2". SLEEVING 2" THROUGH 4" IN SIZE SHALL BE SCH40 PVC SOLVENT WELD, SLEEVING 6" AND LARGER SHALL BE CL 200 PVC GASKETED. SLEEVE DIAMETER SHALL BE AT LEAST TWO TIMES THE DIAMETER OF THE PIPE WITHIN THE SLEEVE. SLEEVES SHALL BE EXTENDED 6" MINIMUM BEYOND WALK OR EDGE OF PAVEMENT. WIRE OR CABLE SHALL NOT BE INSTALLED IN THE SAME SLEEVE AS PIPING, BUT SHALL BE INSTALLED IN SEPARATE SLEEVES. SLEEVE ENDS ON SLEEVE SIZES 4" AND LARGER SHALL BE CAPPED WITH INTEGRAL CORRESPONDING SIZED PVC SLIP CAP, PRESSURE FIT, UNTIL USED, TO PREVENT CONTAMINATION. SLEEVES SHALL BE INSTALLED AT APPROPRIATE DEPTHS FOR MAIN LINE PIPE OR LATERAL PIPE.

2.6 MAIN LINE PIPE

A. ALL MAIN LINE PIPE 4" AND LARGER SHALL BE CLASS 200 GASKETED BELL END. ALL MAIN LINE PIPE 3" IN SIZE AND SMALLER SHALL BE SCHEDULE 40 PVC SOLVENT WELD BELL END.

- a. MAXIMUM FLOWS ALLOWED THROUGH MAIN LINE PIPE SHALL BE:
 

3/4"	8 GPM
1"	12 GPM
1 1/2"	30 GPM
2"	53 GPM
2 1/2"	75 GPM
3"	110 GPM
4"	180 GPM

b. MAIN LINE PIPE SHALL BE BURIED WITH 24" COVER

2.7 MAIN LINE FITTINGS

A. ALL MAIN LINE FITTINGS 3" AND LARGER SHALL BE GASKETED DUCTILE IRON MATERIAL. ALL DUCTILE IRON FITTINGS HAVING CHANGE OF DIRECTION SHALL HAVE PROPER CONCRETE THRUST BLOCK INSTALLED. ALL MAIN LINE FITTINGS SMALLER THAN 3" IN SIZE SHALL BE SCHEDULE 80 PVC.

2.8 ISOLATION VALVES

A. ISOLATION VALVES 3" AND LARGER SHALL BE WATERLOUS BRAND MODEL 2500 CAST IRON GATE VALVE, RESILIENT WEDEGE, PUSH ON TYPE, WITH 2" SQUARE OPERATING NUT. PLACE SLEEVE OF 6" OR LARGER PIPE OVER TOP OF VALVE VERTICALLY AND THEN EXTEND TO GRADE. PLACE 10" ROUND VALVE BOX OVER SLEEVE AT GRADE.

B. ISOLATION VALVES 2 1/2" AND SMALLER SHALL BE APOLLO BRAND 70 SERIES BRASS BALL VALVES, CONTAINED IN A CARSON STANDARD SIZE VALVE BOX. VALVES SHALL BE INSTALLED WITH SCH80 PVC TIE NIPPLES ON BOTH SIDES OF THE VALVE. VALVE SHALL BE PLACED SO THAT THE HANDLE IS VERTICAL TOWARD THE TOP OF THE VALVE BOX IN THE 'OFF' POSITION.

2.9 MANIFOLDS

A. ACTION MANHOLD FITTINGS SHALL BE USED TO CREATE UNIONS ON BOTH SIDES OF EACH CONTROL VALVE, ALLOWING THE VALVE TO BE REMOVED FROM THE BOX WITHOUT CUTTING PIPING. VALVES SHALL BE LOCATED IN BOXES WITH AMPLE SPACE SURROUNDING THEM TO ALLOW ACCESS FOR MAINTENANCE AND REPAIR. WHERE PRACTICAL, GROUP REMOTE CONTROL VALVES IN CLOSE PROXIMITY, AND PROTECT EACH GROUPING WITH A MANHOLD ISOLATION VALVE AS SHOWN IN DETAILS. MANHOLD MAIN LINE (OR SUB-MAN LINE) AND ALL MANHOLD COMPONENTS AND ISOLATION VALVES SHALL BE AT LEAST AS LARGE AS THE LARGEST DIAMETER LATERAL SERVED BY THE RESPECTIVE MANHOLD.

2.10 REMOTE CONTROL VALVES

A. REMOTE CONTROL VALVES SHALL BE AS SPECIFIED ON THE DRAWINGS. REMOTE CONTROL VALVES SHALL BE LOCATED SEPARATELY AND INDIVIDUALLY IN SEPARATE CONTROL BOXES.

2.11 MANUAL CONTROL VALVES

A. QUICK COUPLER VALVE SHALL BE ATTACHED TO THE MANIFOLD SUB-MAN LINE USING A LASCO G17812 SWING JOINT ASSEMBLY WITH SNAP-LOCK OUTLET AND BRASS STABILIZER ELBOW. QUICK COUPLER VALVE SHALL BE PLACED WITHIN A CARSON 10" ROUND VALVE BOX. TOP OF QUICK COUPLER VALVE COVER SHALL ALLOW FOR COMPLETE INSTALLATION OF VALVE BOX LID, BUT ALSO ALLOW FOR INSERTION AND OPERATION OF KEY. BASE OF QUICK COUPLER VALVE AND TOP OF QUICK COUPLER SWING JOINT SHALL BE ENCASED IN 3/4" GRAVEL. CONTRACTOR SHALL NOT PLACE QUICK COUPLER VALVES FURTHER THAN 200 FEET APART, TO ALLOW FOR SPOT WATERING OR SUPPLEMENTAL IRRIGATION OF NEW PLANT MATERIAL. QUICK COUPLER VALVE AT POC SHALL NOT BE ELIMINATED OR RELOCATED.

2.12 LATERAL LINE PIPE

A. ALL LATERAL PIPING SHALL BE SCHEDULE 40 PVC, SOLVENT WELD, AND BELL END. LATERAL PIPE SHALL BE BURIED WITH 12" OF COVER TYPICALLY. LATERAL PIPE SHALL BE 3/4", 1", 1 1/2", 1 3/4" OR 2" IN SIZE, AS INDICATED ON CONSTRUCTION DRAWINGS.

2.13 LATERAL LINE FITTINGS

A. ALL LATERAL LINE FITTINGS SHALL BE S/40 PVC

2.14 SPRAY SPRINKLERS

A. SPRAY HEAD SPRINKLERS SHALL BE AS SPECIFIED ON THE DRAWINGS. NOZZLES SHALL BE AS SPECIFIED ON THE DRAWINGS.

2.15 RAIN BIRD VALVE BOXES

A. CARSON VALVE BOXES SHALL BE USED ON THIS PROJECT. SIZES ARE AS DIRECTED IN THESE SPECIFICATIONS. DETAIL SHEETS OR PLAN SHEETS. VALVE BOXES SHALL BE CENTERED OVER THE CONTROL VALVE OR ELEMENT THEY COVER. VALVE BOX SHALL BE SIZED LARGE ENOUGH TO ALLOW AMPLE ROOM FOR SERVICES ACCESS, REMOVAL OR REPLACEMENT OF VALVE OR ELEMENT. VALVE BOX SHALL BE SET TO FINISH GRADE OF TOPSOIL OR BARKED AREAS.

CONTRACTOR SHALL PROVIDE EXTENSIONS OR STACK ADDITIONAL VALVE BOXES AS NECESSARY TO BRING VALVE BOX PIT TO PROPER GRADE.

2.16 IMPORT BACKFILL

A. ALL MAIN LINE PIPE, LATERAL LINE PIPE AND OTHER IRRIGATION ELEMENTS SHALL BE BEDDED AND BACKFILLED WITH CLEAN SOIL, FREE OF ROCKS 1" AND LARGER. CONTRACTOR SHALL FURNISH AND INSTALL ADDITIONAL BACKFILL MATERIAL AS NECESSARY DUE TO ROCKY CONDITIONS. TRENCHES AND OTHER ELEMENTS SHALL BE COMPACTED AND/OR WATER SETTLED TO ELIMINATE SETTLING. DEBRIS FROM TRENCHING OPERATIONS UNUSABLE FOR FILL SHALL BE REMOVED FROM PROJECT AND DISPOSED OF PROPERLY BY CONTRACTOR.

2.17 OTHER PRODUCTS

A. SUBSTITUTION OF EQUIVALENT PRODUCTS IS SUBJECT TO THE LANDSCAPE ARCHITECT OR OARS APPROVAL AND MUST BE DESIGNATED AS ACCEPTED IN WRITING.

- a. THE CONTRACTOR SHALL PROVIDE MATERIALS TO MAKE THE SYSTEM COMPLETE AND OPERATIONAL.

2.18 PREPARATION

A. CONTRACTOR SHALL REPAIR OR REPLACE WORK DAMAGED BY IRRIGATION SYSTEM INSTALLATION. IF DAMAGED WORK IS NEW, REPAIR OR REPLACEMENT SHALL BE PERFORMED BY THE ORIGINAL INSTALLER OF THAT WORK. THE EXISTING LANDSCAPE OF THIS PROJECT SHALL REMAIN IN PLACE. CONTRACTOR SHALL PROTECT AND WORK AROUND EXISTING PLANT MATERIAL. COORDINATION OF TRENCH AND VALVE LOCATIONS SHALL BE Laid OUT FOR THE OAR PRIOR TO ANY EXCAVATION OCCURRING. PLANT MATERIAL DEEMED DAMAGED BY THE OAR SHALL BE REPLACED WITH NEW PLANT MATERIAL AT CONTRACTOR'S EXPENSE. CONTRACTOR SHALL NOT CUT EXISTING TREE ROOTS LARGER THAN 2" TO INSTALL THIS PROJECT. ROUTE PIPE, WIRE AND IRRIGATION ELEMENTS AROUND TREE CANOPY DRIP LINE TO MINIMIZE DAMAGE TO TREE ROOTS. CONTRACTOR SHALL HAVE NO PART OF EXISTING SYSTEM USED BY OTHER PORTIONS OF SITE LANDSCAPE WITHOUT WATER FOR MORE THAN 24 HOURS AT A TIME.

2.19 TRENCHING AND BACKFILLING

A. PULLING OF PIPE SHALL NOT BE PERMITTED ON THIS PROJECT. OPEN EXCAVATE TRENCHES BOTH IN WIDTH AND DEPTH ENSURE BASE OF TRENCH IS ROCK OR DEBRIS FREE TO PROTECT PIPE AND WIRE. GRADE TRENCH BASE TO ENSURE FLAT, EVEN SUPPORT OF PIPING. BACKFILL WITH CLEAN SOIL OR IMPORT MATERIAL. CONTRACTOR SHALL BACKFILL, NO LESS THAN 2" AROUND ENTIRE PIPE WITH CLEAN, ROCK FREE FILL. MAIN LINE PIPING AND FITTINGS SHALL NOT BE BACKFILLED UNTIL OAR HAS INSPECTED AND PIPE HAS PASSED PRESSURE TESTING. PERFORM BALANCE OF BACKFILL OPERATION TO ELIMINATE ANY SETTLING.

2.20 SLEEVING

A. CONTRACTOR SHALL BE RESPONSIBLE TO PROTECT EXISTING UNDERGROUND UTILITIES AND

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3.4 GRADES AND DRAINAGE

A. PLACE IRRIGATION PIPE AND OTHER ELEMENTS AT UNIFORM GRADES. WINTERIZATION SHALL BE BY EVACUATION WITH COMPRESSED AIR. AUTOMATIC DRAINS SHALL NOT BE INSTALLED ON THIS PROJECT. MANUAL DRAINS SHALL ONLY BE INSTALLED AT POC WHERE DESIGNATED ON CONSTRUCTION DRAWINGS.

3.5 PVC PIPE

A. INSTALL PIPE TO ALLOW FOR EXPANSION AND CONTRACTION AS RECOMMENDED BY PIPE MANUFACTURER

B. INSTALL MAIN LINE PIPES WITH 18" OF COVER, LATERAL LINE PIPES WITH 12" OF COVER.

C. DRAWINGS SHOW DIAGNOSTIC OR CONCEPTUAL LOCATION OF PIPING - CONTRACTOR SHALL INSTALL PIPING TO MINIMIZE CHANGE OF DIRECTION, AVOID PLACEMENT UNDER LARGE TREES OR LARGE SHRUBS, AVOID PLACEMENT UNDER HARDCAPE FEATURES.

D. PLASTIC PIPE SHALL BE CUT SQUARELY. BURRS SHALL BE REMOVED. SPIGOT ENDS OF PIPES 3" AND LARGER SHALL BE BEVELED.

E. PIPE SHALL NOT BE GLUED UNLESS AMBIENT TEMPERATURE IS AT LEAST 50 DEGREES F. PIPE SHALL NOT BE GLUED IN RAINY CONDITIONS UNLESS PROPERLY TENDED. ALL SOLVENT WELD JOINTS SHALL BE ASSEMBLED USING IP5 711 GLUE AND P70 PRIMER ACCORDING TO MANUFACTURER'S SPECIFICATION, NO EXCEPTIONS. ALL WORKERS PERFORMING GLUE OPERATIONS SHALL PROVIDE EVIDENCE OF CERTIFICATION. GLUED MAIN LINE PIPE SHALL CURE A MINIMUM OF 24 HOURS PRIOR TO BEING ENERGIZED. LATERAL LINES SHALL CURE A MINIMUM OF 2 HOURS PRIOR TO BEING ENERGIZED AND SHALL NOT REMAIN UNDER CONSTANT PRESSURE UNLESS CURED FOR 24 HOURS.

F. APPROPRIATE THRUST BLOCKING SHALL BE PERFORMED ON FITTINGS 3" AND LARGER. ALL THREADED JOINTS SHALL BE WRAPPED WITH TEFLON TAPE OR PASTE UNLESS DIRECTED BY PRODUCT MANUFACTURER OR SEALING BY O-RING.

3.6 CONTROLLERS

A. ALL GROUNDING FOR PEDESTAL CONTROLLERS SHALL BE AS DIRECTED BY CONTROLLER MANUFACTURER AND ASIC GUIDELINES, NOT TO EXCEED A RESISTANCE READING OF 5 OHMS.

B. LOCATE CONTROLLERS IN PROTECTED, INCONSPICUOUS PLACES, WHEN POSSIBLE. COORDINATE LOCATION OF PEDESTAL CONTROLLERS WITH LANDSCAPE ARCHITECT TO MINIMIZE VISIBILITY.

C. COORDINATE LOCATION OF WALL MOUNT CONTROLLERS WITH BUILDING OR ELECTRICAL CONTRACTOR TO FACILITATE ELECTRICAL SERVICE AND FUTURE MAINTENANCE NEEDS. WALL MOUNT SHALL BE SECURELY FASTENED TO SURFACE. IF EXTERIOR MOUNTED, WALL MOUNT CONTROLLERS SHALL HAVE ELECTRICAL SERVICE WIRE AND FIELD CONTROL WIRE IN SEPARATE, APPROPRIATE SIZED WEATHERPROOF ELECTRICAL CONDUIT, PVC PIPE SHALL NOT BE USED.

D. WIRING UNDER HARDCAPE SURFACES SHALL BE PLACED CONTINUOUSLY IN CONDUIT. CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE SLEEVING NEEDS FOR CONDUIT OR SURFACE ELBOWS FROM EXTERIOR TO INTERIOR OF BUILDING.

E. PEDESTAL CONTROLLERS SHALL BE PLACED UPON STRONG BOX QUICK PAD AS PER MANUFACTURER'S RECOMMENDATIONS. CONTROLLERS SHALL BE ORIENTED SUCH THAT OWNERS REPRESENTATIVE MAINTENANCE PERSONNEL MAY ACCESS EASILY AND PERFORM FIELD SYSTEM TESTS EFFICIENTLY.

F. PLACE STANDARD VALVE BOX AT BASE OF CONTROLLER OR NEARBY TO ALLOW FOR THREE TO FIVE FEET OF SLACK FIELD CONTROL WIRE TO BE PLACED AT EACH CONTROLLER. THIS CONTRACTOR SHALL PROVIDE CONDUIT ACCESS IF NEEDED FOR ELECTRICAL CONTRACTOR. ELECTRICAL SUPPLY AND INSTALLATION, AS WELL AS HOOK-UP TO CONTROLLER SHALL BE BY THIS CONTRACTOR.

G. ELECTRICAL CONTRACTOR IS IN CHARGE OF PROVIDING 1.5" CONDUIT FROM CONTROLLER TO OUTSIDE LANDSCAPE AREA. PROVIDE POWER AND ROOM FOR CONTROLLER. PROVIDE ETHERNET TO HARDWARE POWER INTO THE CONTROLLER.

VALVES

A. ISOLATION VALVES, REMOTE CONTROL VALVES, AND QUICK COUPLER VALVES SHALL BE INSTALLED ACCORDING TO MANUFACTURER RECOMMENDATION AND CONTRACT SPECIFICATIONS AND DETAILS.

B. VALVE BOXES SHALL BE SET OVER VALVES SO THAT ALL PARTS OF THE VALVE CAN BE REACHED FOR SERVICE.

C. VALVE BOX AND LID SHALL BE SET TO BE FLUSH WITH FINISHED GRADE. ONLY ONE REMOTE CONTROL VALVE MAY BE INSTALLED IN A CARSON 149124 BOX. PLACE A MINIMUM OF 4" OF 3/4" WASHED GRAVEL BENEATH VALVE BOX FOR DRAINAGE. BOTTOM OF REMOTE CONTROL VALVE SHALL BE A MINIMUM OF 2" ABOVE GRAVEL.

3.8 SPRINKLER HEADS

A. NO SPRINKLER SHALL BE LOCATED CLOSER THAN 6" TO WALLS, FENCES, OR BUILDINGS.

B. HEADS ADJACENT TO WALKS, CURBS, OR PATHS SHALL BE LOCATED AT GRADE AND 2" AWAY FROM HARDCAPE.

C. CONTROL VALVES SHALL BE OPENED, THEN FULLY FLUSH LATERAL LINE PIPE AND SWING JOINTS PRIOR TO INSTALLATION OF SPRINKLERS.

D. SPRAY HEADS SHALL BE INSTALLED AND FLUSHED AGAIN PRIOR TO INSTALLATION OF NOZZLES.

E. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTMENT IF NECESSARY DUE TO GRADE CHANGES DURING LANDSCAPE CONSTRUCTION.

F. FIELD QUALITY CONTROL

A. MAIN LINE PIPES SHALL NOT BE BACKFILLED OR ACCEPTED UNTIL THE SYSTEM HAS BEEN TESTED FOR 2 HOURS AT 100 PSI.

B. MAIN LINE PRESSURE TEST SHALL INCLUDE ALL PIPE AND COMPONENTS FROM THE POINT OF CONNECTION TO THE UPSTREAM SIDE OF REMOTE CONTROL VALVES. TEST SHALL INCLUDE ALL MANIFOLD COMPONENTS UNDER CONSTANT PRESSURE. PIPING MAY BE TESTED IN SECTIONS THAT CAN BE ISOLATED.

C. CONTRACTOR SHALL PROVIDE PRESSURIZED WATER PUMP TO INCREASE OR BOOST PRESSURE WHERE EXISTING STATIC PRESSURE IS LESS THAN 100 PSI.

D. SCHEDULE TESTING WITH OAR 48 HOURS IN ADVANCE FOR APPROVAL.

E. LEAKS OR DEFECTS SHALL PROMPTLY BE REPAIRED OR RECTIFIED BY THE CONTRACTORS EXPENSE AND RETESTED UNTIL ABLE TO PASS TESTING.

F. GROUNDING RESISTANCE AT PEDESTAL CONTROLLER SHALL ALSO BE TESTED AND SHALL NOT EXCEED 5 OHMS.

3.10 ADJUSTMENT

A. SPRINKLER HEADS SHALL BE ADJUSTED TO PROPER HEIGHT WHEN INSTALLED. CHANGES IN GRADE OR ADJUSTMENT OF HEAD HEIGHT AFTER INSTALLATION SHALL BE CONSIDERED A PART OF THE ORIGINAL CONTRACT AND AT CONTRACTORS EXPENSE.

B. ADJUST ALL SPRINKLER HEADS FOR ARC, RADIUS, PROPER TRIM AND DISTRIBUTION TO COVER ALL LANDSCAPED AREAS THAT ARE TO BE IRRIGATED.

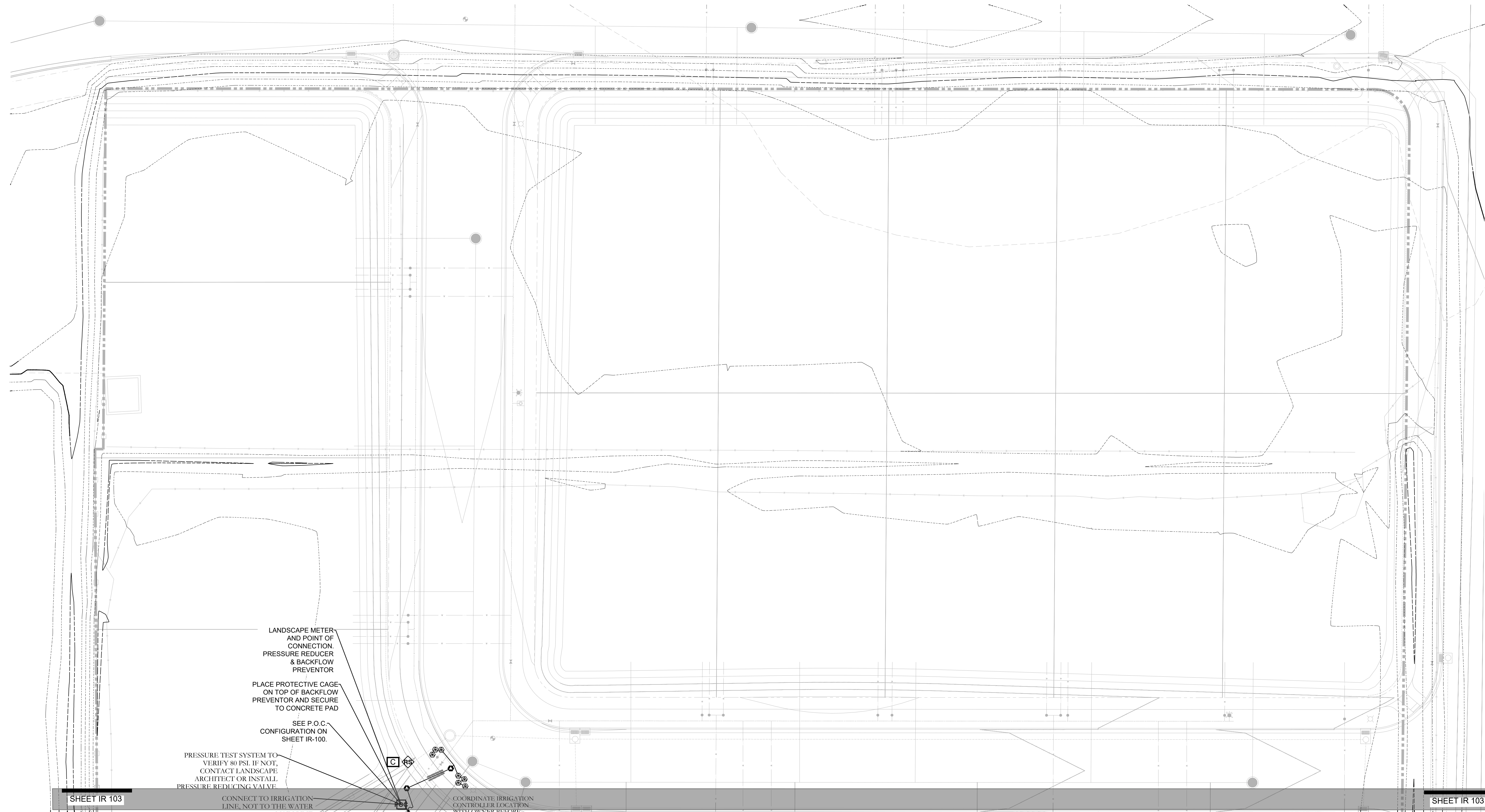
C. ADJUST SPRINKLERS SO THEY DO NOT WATER BUILDINGS, STRUCTURES, OR OTHER HARDCAPE FEATURES.

D. ADJUST RUN TIMES OF STATION TO MEET NEEDS OF PLANT MATERIAL. THE STATION SERVICES.

3.11 CLEANING

A. CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANLINESS OF JOBSITE. WORK AREAS SHALL BE SWEEP CLEANLY AND PICKED UP DAILY.

B



LANDSCAPE METER AND POINT OF CONNECTION, PRESSURE REDUCER & BACKFLOW PREVENTOR

PLACE PROTECTIVE CAGE ON TOP OF BACKFLOW PREVENTOR AND SECURE TO CONCRETE PAD

SEE P.O.C. CONFIGURATION ON SHEET IR-100.

PRESSURE TEST SYSTEM TO VERIFY 80 PSI. IF NOT, CONTACT LANDSCAPE ARCHITECT OR INSTALL PRESSURE REDUCING VALVE.

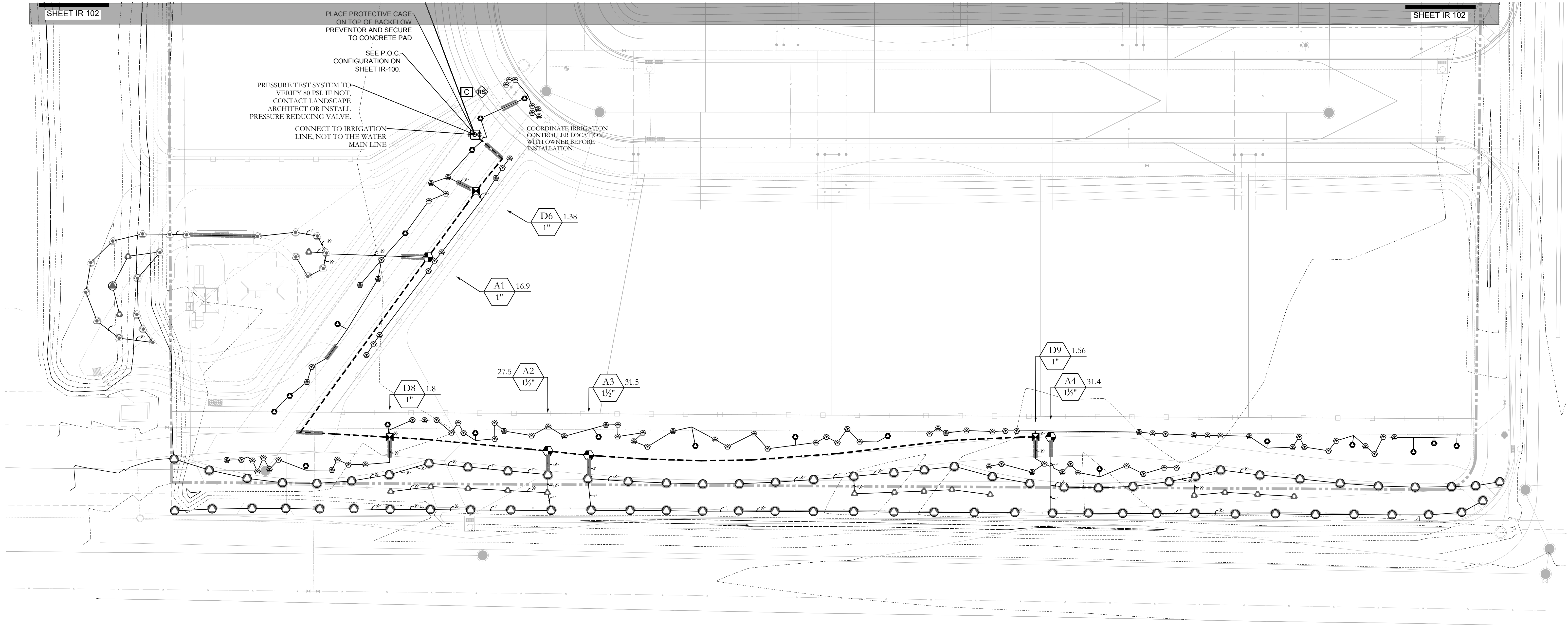
CONNECT TO IRRIGATION LINE, NOT TO THE WATER

COORDINATE IRRIGATION CONTROLLER LOCATION WITH OWNER BEFORE

SHEET IR 103

SHEET IR 103

ISSUE DATE 2/17/2026		PROJECT NUMBER UT25182		PLAN INFORMATION ** THIS PRINT FROM PKJ DESIGN GROUP IS BASED ON CIVIL AND ARCHITECT INFORMATION ISSUED ON 02/17/2026		PROJECT INFORMATION		DEVELOPER / PROPERTY OWNER / CLIENT		LANDSCAPE ARCHITECT / PLANNER		LICENSE STAMP		DRAWING INFO	
NO. REVISION 1 CITY COMMENTS 2 CITY COMMENTS 3 4		DATE 01-20-2026 01-22-2026		BLUE STAKES OF UTAH UTILITY NOTIFICATION CENTER, INC. 1-800-662-4111 www.bluestakes.org		<h1>MEADOWS AT NORTH LAKE PH2</h1> <h2>SARATOGA SPRINGS, UTAH</h2>		ANDERSON DEVELOPMENT KEATON MORTON 801-856-2478 KEATON@AND-DEV.COM		PKJ DESIGN GROUP Landscape Architecture • Planning & Visualization 3450 N. TRIUMPH BLVD. SUITE 102 LEHI, UTAH 84043 (801) 995-2217 www.pkjdesigngroup.com				PM: JTA DRAWN: ACP CHECKED: KBA PLOT DATE: 2/17/2026	
GRAPHIC SCALE: 1" = 20' 				SHEET IR 102 SHEET IR 103		COPYRIGHT: PKJ DESIGN GROUP THIS DOCUMENT AND THE IDEAS AND DESIGNS INCORPORATED HEREIN AS AN INSTRUMENT OF PROFESSIONAL SERVICE IS PROPERTY OF PKJ DESIGN GROUP. IT IS NOT TO BE USED IN WHOLE OR IN PART FOR ANY OTHER PROJECT WITHOUT THE WRITTEN AUTHORIZATION OF PKJ DESIGN GROUP.		IRRIGATION PLAN CITY PERMIT SET IR-102							



ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	PROJECT INFORMATION	DEVELOPER / PROPERTY OWNER / CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE STAMP	DRAWING INFO
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2/17/2026      UT25182

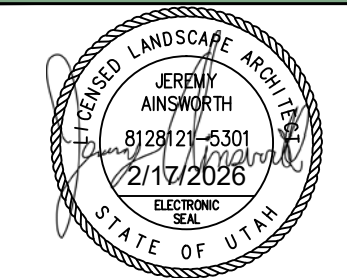
THIS PRINT FROM PKJ DESIGN GROUP IS BASED ON CIVIL AND ARCHITECT INFORMATION ISSUED ON 02/17/2026

# MEADOWS AT NORTH LAKE PH2

## SARATOGA SPRINGS, UTAH

SHEET IR 102  
SHEET IR 103

ANDERSON DEVELOPMENT  
KEATON MORTON  
801-856-2478  
KEATON@AND-DEV.COM



PM: JTA  
DRAWN: ACP  
CHECKED: KBA  
PLOT DATE: 2/17/2026

NO.	REVISION	DATE
1	CITY COMMENTS	01-20-2026
2	CITY COMMENTS	01-22-2026
3		
4		

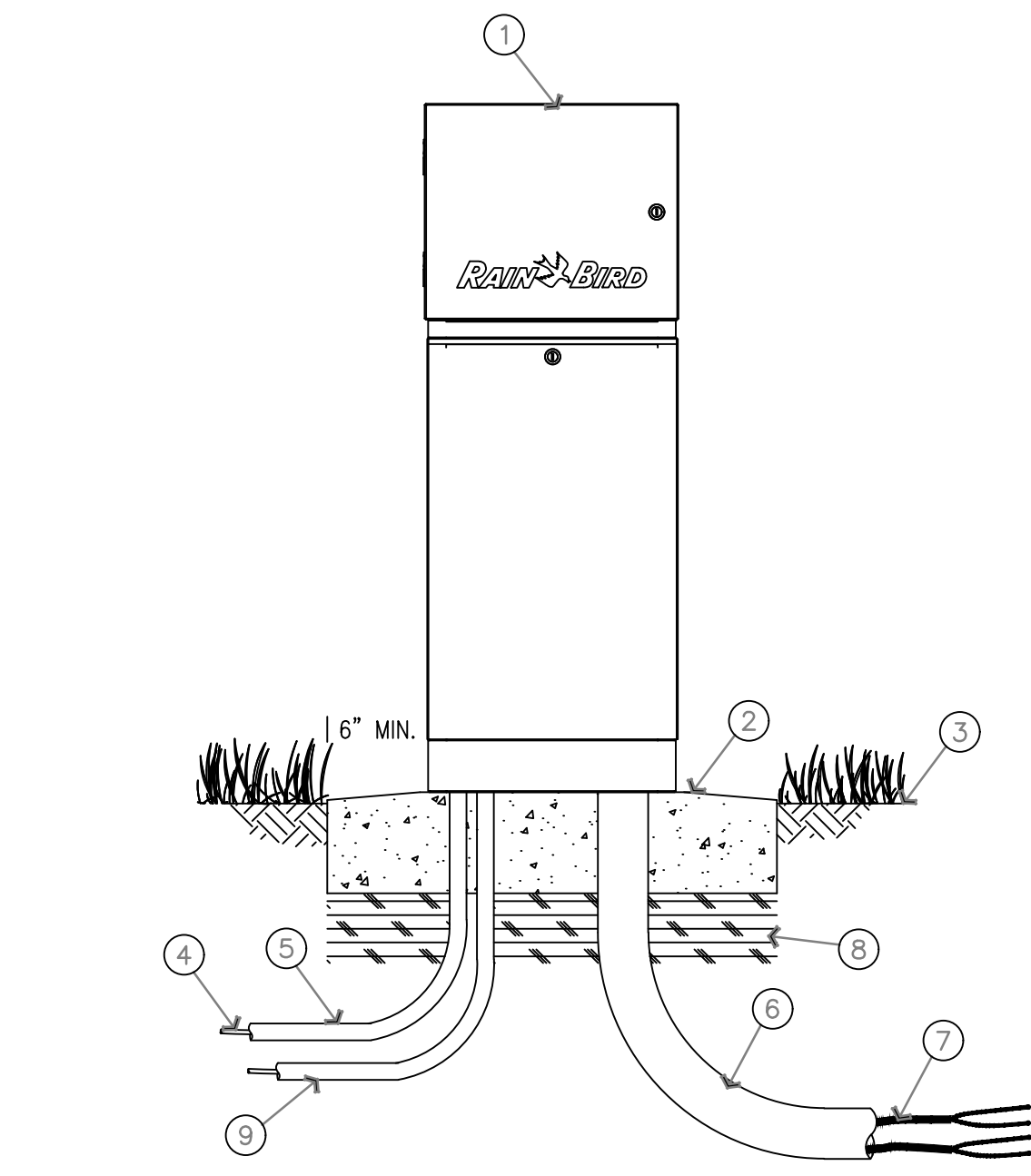
**811** BLUE STAKES OF UTAH  
UTILITY NOTIFICATION CENTER, INC.  
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**IRRIGATION PLAN**  
**CITY PERMIT SET**  
**IR-103**



- 1 TWO-WIRE CONTROLLER: RAIN BIRD ESP-LXIVM/PRO WITH LXMM METAL CABINET AND LXMMPED METAL PEDESTAL. INSTALL CONTROLLER, CABINET AND PEDESTAL PER MANUFACTURER'S RECOMMENDATIONS.
- 2 CONCRETE PAD: 6-INCH MINIMUM THICKNESS, 6-INCHES LARGER ON ALL SIDES THAN PEDESTAL FOOTPRINT.
- 3 FINISH GRADE.
- 4 POWER SUPPLY WIRE.
- 5 1-INCH SCH 40 PVC CONDUIT, FITTINGS AND SWEEP ELL FOR POWER SUPPLY.
- 6 2-INCH SCH 40 PVC CONDUIT, FITTINGS AND SWEEP ELL FOR TWO-WIRE CABLE.
- 7 MAXI-CABLE TWO-WIRE PATH TO FIELD DEVICES. USE A DIFFERENT CABLE JACKET COLOR FOR EACH PATH.
- 8 COMPACTED SUBGRADE.
- 9 1-INCH SCH 40 PVC CONDUIT, FITTINGS AND SWEEP ELL FOR GROUND.

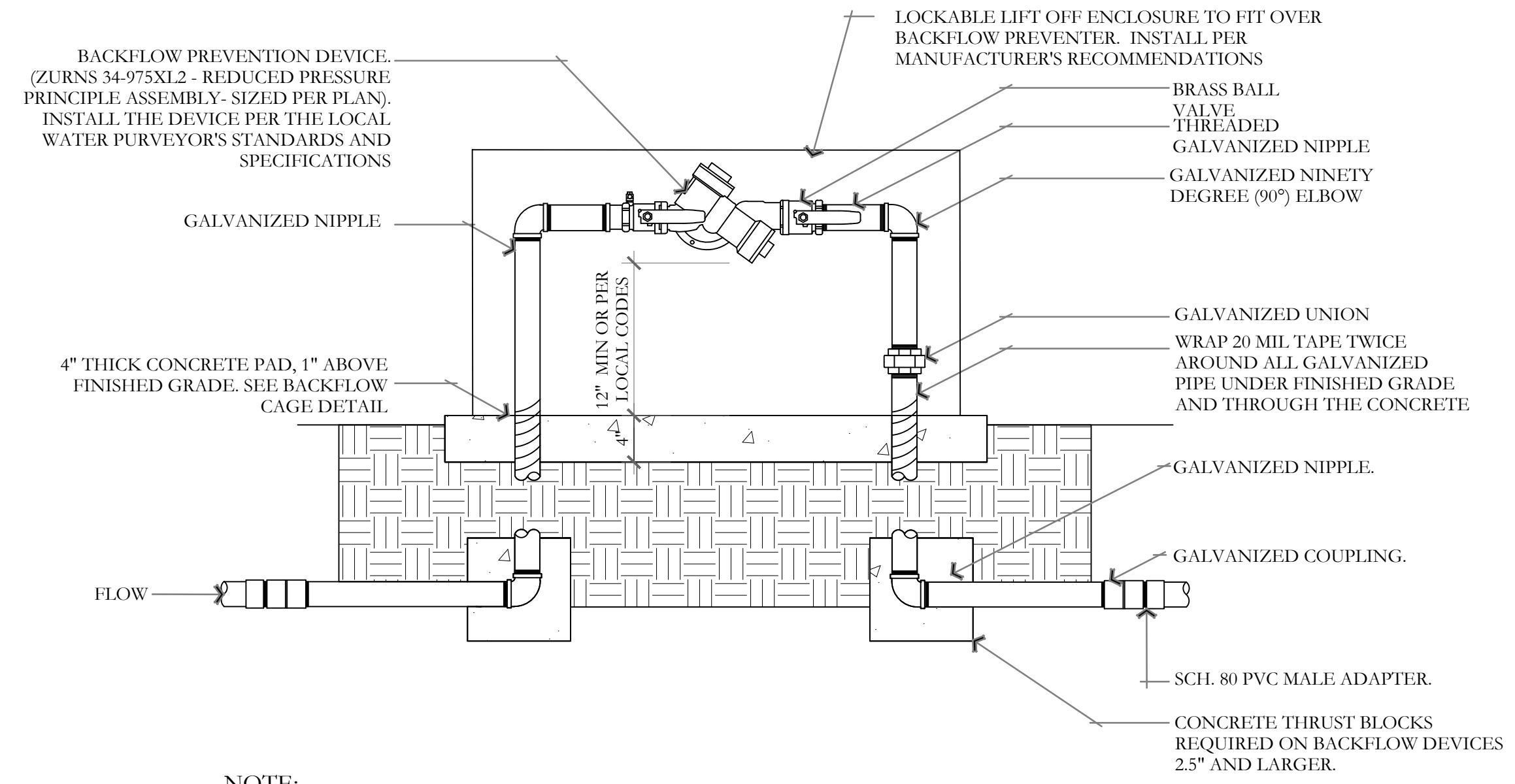
NOTES:  
 1. ESP-LXIVM CONTROLLER IS AVAILABLE IN TWO MODELS: THE LXIVM WITH 60 STATIONS AND THE LXIVM-PRO WITH 240 STATIONS. REFER TO THE CHART BELOW FOR DIFFERENCES BETWEEN THE TWO MODELS.  
 2. PROVIDE PROPER GROUNDING COMPONENTS TO ACHIEVE GROUND RESISTANCE OF 10 OHMS OR LESS.

**KEY SPECIFICATIONS**

FEATURE	MODEL	MAX PROGRAMS	MAX STATIONS	MAX SIMULSTATIONS	MASTER VALVES	FLOW SENSORS	WEATHER SENSORS
	LX-IVM	10	60	8	5	5	4
	LX-IVM PRO	40	240	16	10	10	8

**ESP-LXIVM/PRO TWO-WIRE CONTROLLER IN METAL PEDESTAL**

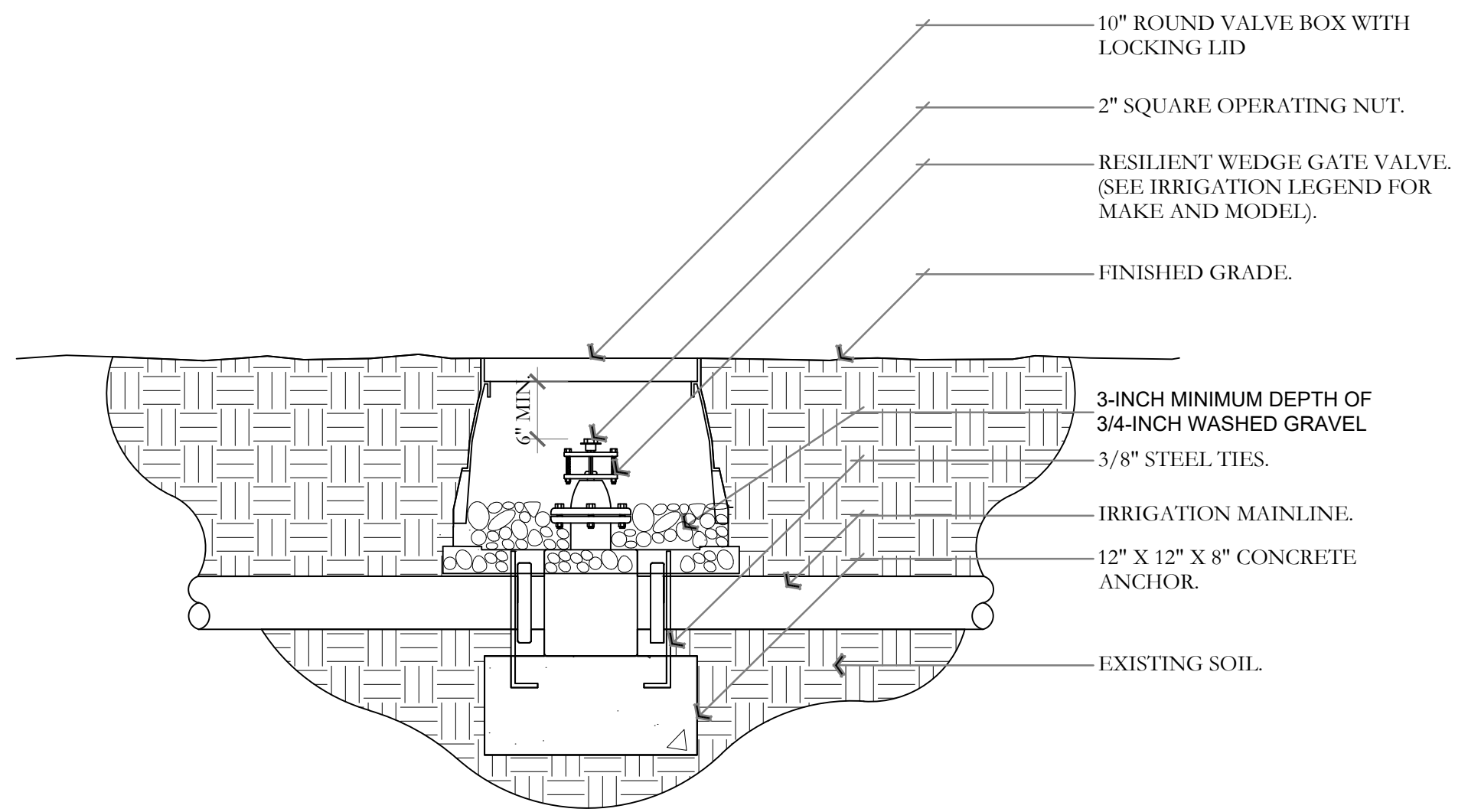
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- BACKFLOW PREVENTION DEVICE (ZURNS 34-975XL2 - REDUCED PRESSURE PRINCIPLE ASSEMBLY - SIZED PER PLAN). INSTALL THE DEVICE PER THE LOCAL WATER PURVEYOR'S STANDARDS AND SPECIFICATIONS.
- LOCKABLE LIFT OFF ENCLOSURE TO FIT OVER BACKFLOW PREVENTER. INSTALL PER MANUFACTURER'S RECOMMENDATIONS.
- BRASS BALL VALVE
- THREADED GALVANIZED NIPPLE
- GALVANIZED NINETY DEGREE (90°) ELBOW
- GALVANIZED UNION
- WRAP 20 MIL TAPE TWICE AROUND ALL GALVANIZED PIPE UNDER FINISHED GRADE AND THROUGH THE CONCRETE.
- GALVANIZED NIPPLE
- GALVANIZED COUPLING.
- SCH. 80 PVC MALE ADAPTER.
- CONCRETE THRUST BLOCKS REQUIRED ON BACKFLOW DEVICES 25" AND LARGER.
- NOTE:  
 1. GALVANIZED NIPPLE SHALL EXTEND 12" PAST THE EDGE OF THE CONCRETE FOOTING.  
 2. SCH. 80 PVC MALE ADAPTER SHALL BE USED IN CONNECTION FROM GALVANIZE TO THE MAINLINE.  
 3. BACKFLOW PREVENTION DEVICE SHALL BE LOCATED AS CLOSE AS POSSIBLE TO THE LANDSCAPE METER.  
 4. BACKFLOW PREVENTION DEVICE SHALL BE LOCATED IN PLANTING AREA UNLESS APPROVED BY OWNER'S REPRESENTATIVE.  
 5. SEE DETAIL FOR BACKFLOW CAGE INSTALLATION.  
 6. ALL ASSEMBLY PARTS (THREADED NIPPLES, FITTINGS, ETC.) SHALL BE GALVANIZED OR BRASS PER LOCAL CODES AND REQUIREMENTS.  
 7. ALL BACKFLOW PREVENTION DEVICES SHALL HAVE FREEZE BLANKET INCLUDED UPON INSTALLATION.  
 8. ALL GALVANIZED CONNECTIONS SHALL TO BE MADE USING PIPE THREAD SEALANT. ALL SCH. 80 PVC TO GALVANIZED CONNECTIONS TO BE MADE USING TEFLON TAPE.

**BACKFLOW PREVENTION DETAIL**

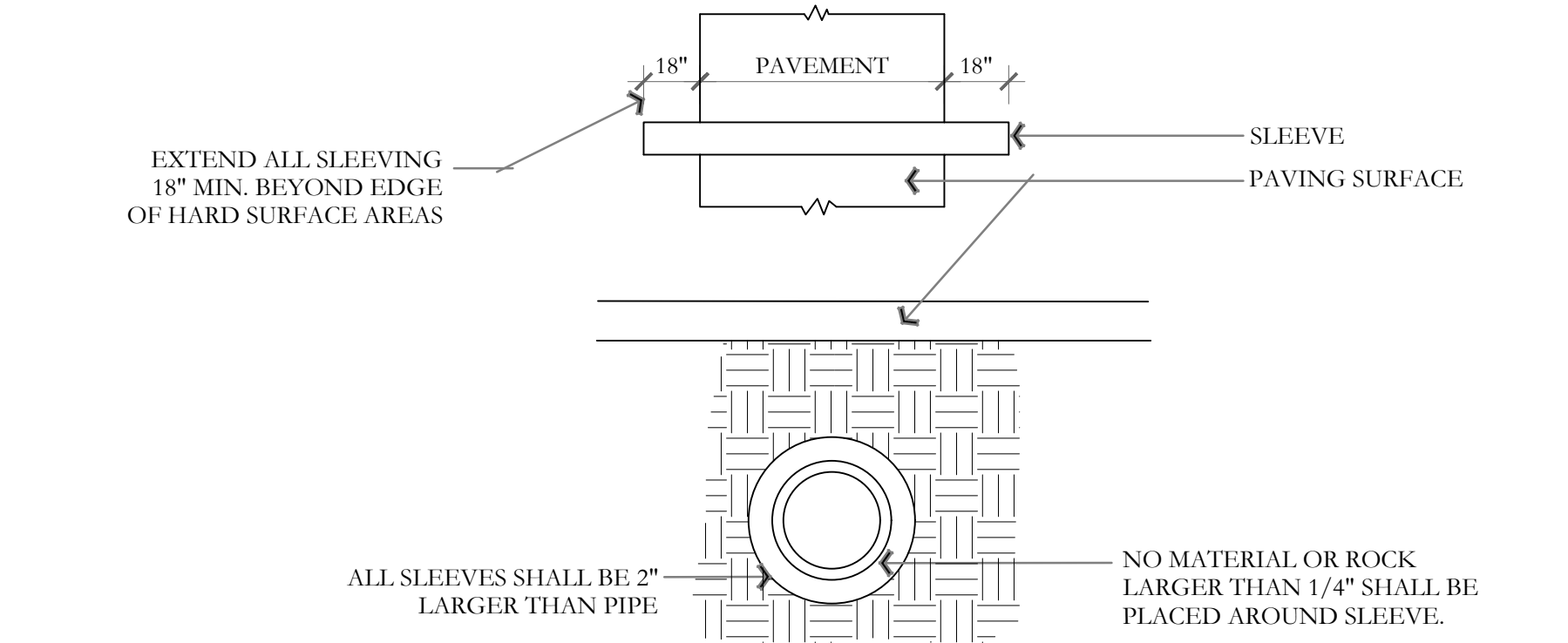
NOT TO SCALE



- 10" ROUND VALVE BOX WITH LOCKING LID
- 2" SQUARE OPERATING NUT.
- RESILIENT WEDGE GATE VALVE (SEE IRRIGATION LEGEND FOR MAKE AND MODEL).
- FINISHED GRADE.
- 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL
- 3/8" STEEL TIES.
- IRRIGATION MAINLINE.
- 12" X 12" X 8" CONCRETE ANCHOR.
- EXISTING SOIL.
- NOTES:  
 1. INSTALL GATE VALVE PER MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.  
 2. VALVE BOX SHALL BE WRAPPED WITH MINIMUM 3 MIL THICK PLASTIC AND SECURE IT TO VALVE BOX USING DUCT TAPE OR ELECTRICAL TAPE.  
 3. VALVE BOX SHALL BE LOCATED IN PLANTING AREA.

**GATE VALVE AND ANCHOR DETAIL**

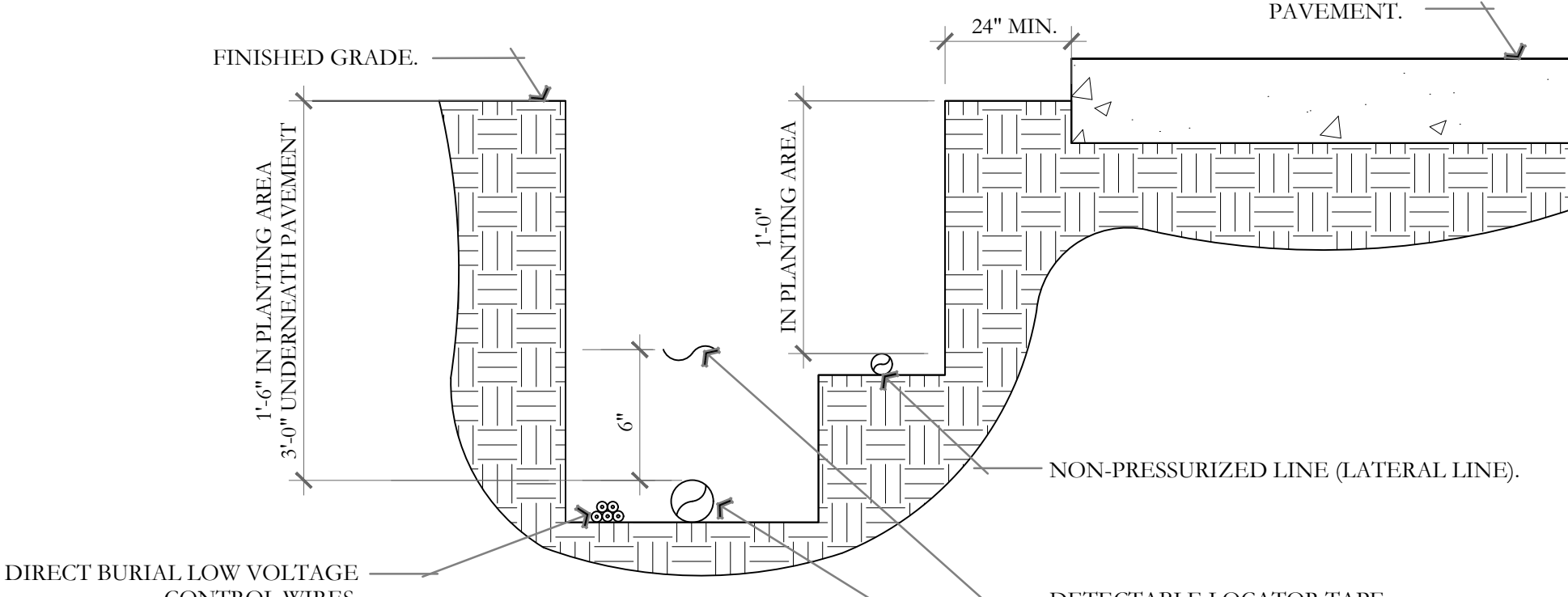
NOT TO SCALE



- EXTEND ALL SLEEVING 18" MIN. BEYOND EDGE OF HARD SURFACE AREAS
- PAVEMENT
- 18"
- SLEEVE
- PAVING SURFACE
- NO MATERIAL OR ROCK LARGER THAN 1/4" SHALL BE PLACED AROUND SLEEVE.
- ALL SLEEVES SHALL BE 2" LARGER THAN PIPE

**TYPICAL SLEEVING DETAIL**

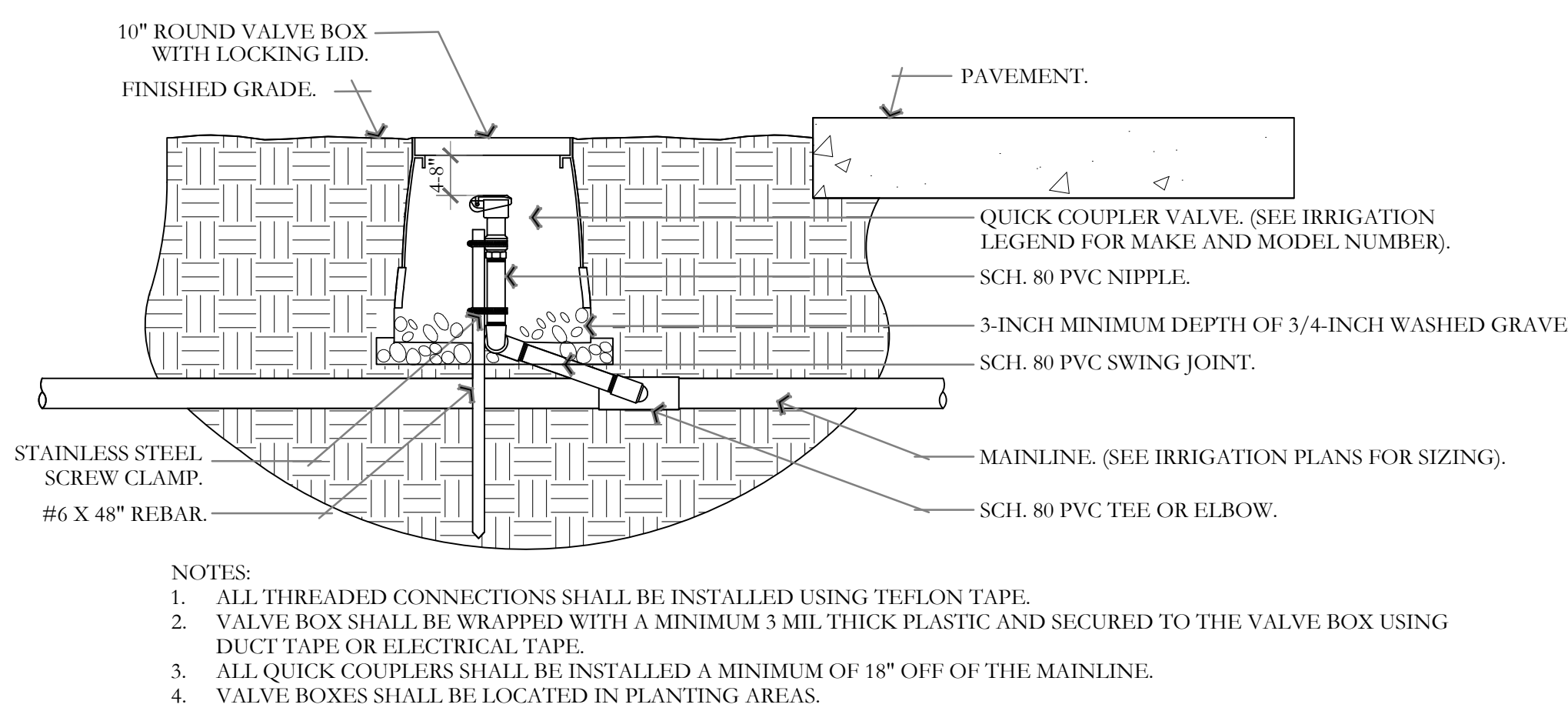
NOT TO SCALE



- FINISHED GRADE.
- 24" MIN.
- PAVEMENT.
- 1"-6" IN PLANTING AREA
- 3'-0" UNDERNEATH PAVEMENT
- 1"-0" IN PLANTING AREA
- NON-PRESSURIZED LINE (LATERAL LINE).
- DIRECT BURIAL LOW VOLTAGE CONTROL WIRES.
- DETECTABLE LOCATOR TAPE.
- PRESSURIZED LINE (MAINLINE).
- NOTES:  
 1. SEE IRRIGATION LEGEND FOR MAINLINE AND LATERAL LINE PIPE SIZE AND TYPE.  
 2. DIRECT BURIAL CONTROL WIRES SHALL BE INSTALLED IN SCH. 40 PVC ELECTRICAL CONDUIT IF REQUIRED.  
 3. 2 WIRE IRRIGATION WIRE SHALL BE INSTALLED IN SCH. 40 PVC ELECTRICAL CONDUIT.  
 4. DETECTABLE LOCATOR TAPE SHALL BE LOCATED SIX INCHES (6") ABOVE THE ENTIRE MAINLINE RUN.

**IRRIGATION TRENCHING DETAIL**

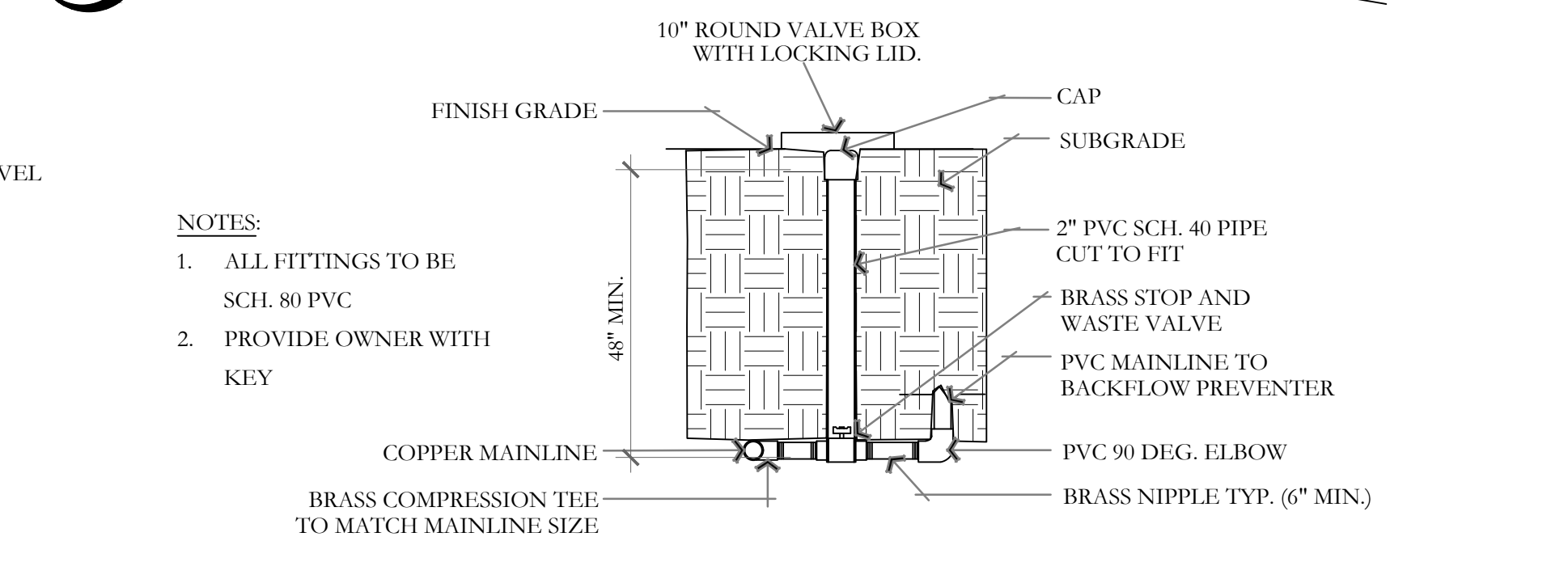
NOT TO SCALE



- 10" ROUND VALVE BOX WITH LOCKING LID.
- FINISHED GRADE.
- PAVEMENT.
- QUICK COUPLER VALVE (SEE IRRIGATION LEGEND FOR MAKE AND MODEL NUMBER).
- SCH. 80 PVC NIPPLE.
- 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL
- SCH. 80 PVC SWING JOINT.
- MAINLINE (SEE IRRIGATION PLANS FOR SIZING).
- SCH. 80 PVC TEE OR ELBOW.
- STAINLESS STEEL SCREW CLAMP.
- #6 X 48" REBAR.
- NOTES:  
 1. ALL THREADED CONNECTIONS SHALL BE INSTALLED USING TEFLON TAPE.  
 2. VALVE BOX SHALL BE WRAPPED WITH A MINIMUM 3 MIL THICK PLASTIC AND SECURED TO THE VALVE BOX USING DUCT TAPE OR ELECTRICAL TAPE.  
 3. ALL QUICK COUPLERS SHALL BE INSTALLED A MINIMUM OF 18" OFF OF THE MAINLINE.  
 4. VALVE BOXES SHALL BE LOCATED IN PLANTING AREAS.

**QUICK COUPLER DETAIL**

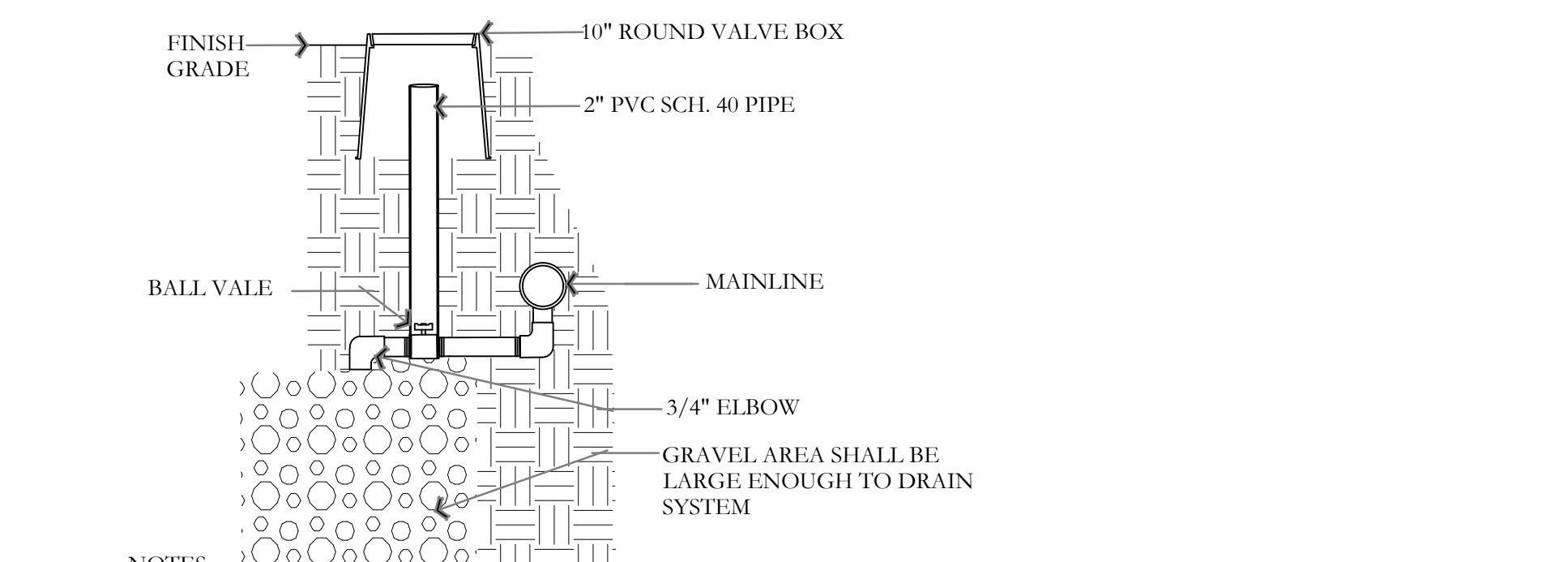
NOT TO SCALE



- 10" ROUND VALVE BOX WITH LOCKING LID.
- FINISHED GRADE.
- CAP
- SUBGRADE
- 2" PVC SCH. 40 PIPE CUT TO FIT
- BRASS STOP AND WASTE VALVE
- PVC MAINLINE TO BACKFLOW PREVENTER
- PVC 90 DEG. ELBOW
- BRASS NIPPLE TYP. (6" MIN.)
- 48" MIN.
- COPPER MAINLINE
- BRASS COMPRESSION TEE TO MATCH MAINLINE SIZE
- NOTES:  
 1. ALL FITTINGS TO BE SCH. 80 PVC  
 2. PROVIDE OWNER WITH KEY

**STOP AND WASTE VALVE ASSEMBLY DETAIL**

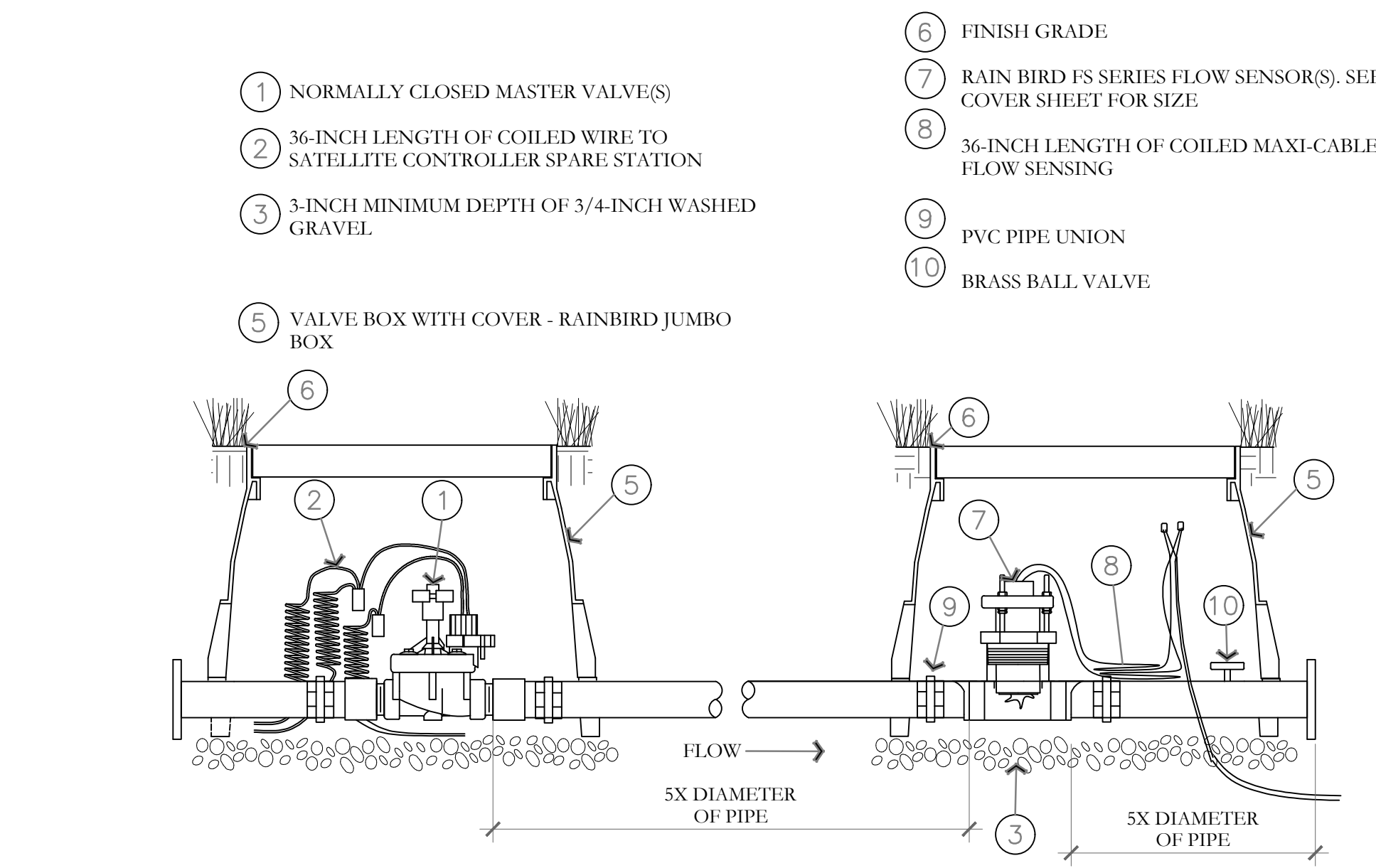
NOT TO SCALE



- FINISH GRADE
- 10" ROUND VALVE BOX
- 2" PVC SCH. 40 PIPE
- BALL VALVE
- MAINLINE
- 3/4" ELBOW
- GRAVEL AREA SHALL BE LARGE ENOUGH TO DRAIN SYSTEM
- NOTES:  
 1. ALL FITTINGS TO BE SCH. 80 PVC  
 2. PROVIDE OWNER WITH KEY

**MANUAL DRAIN DETAIL**

NOT TO SCALE

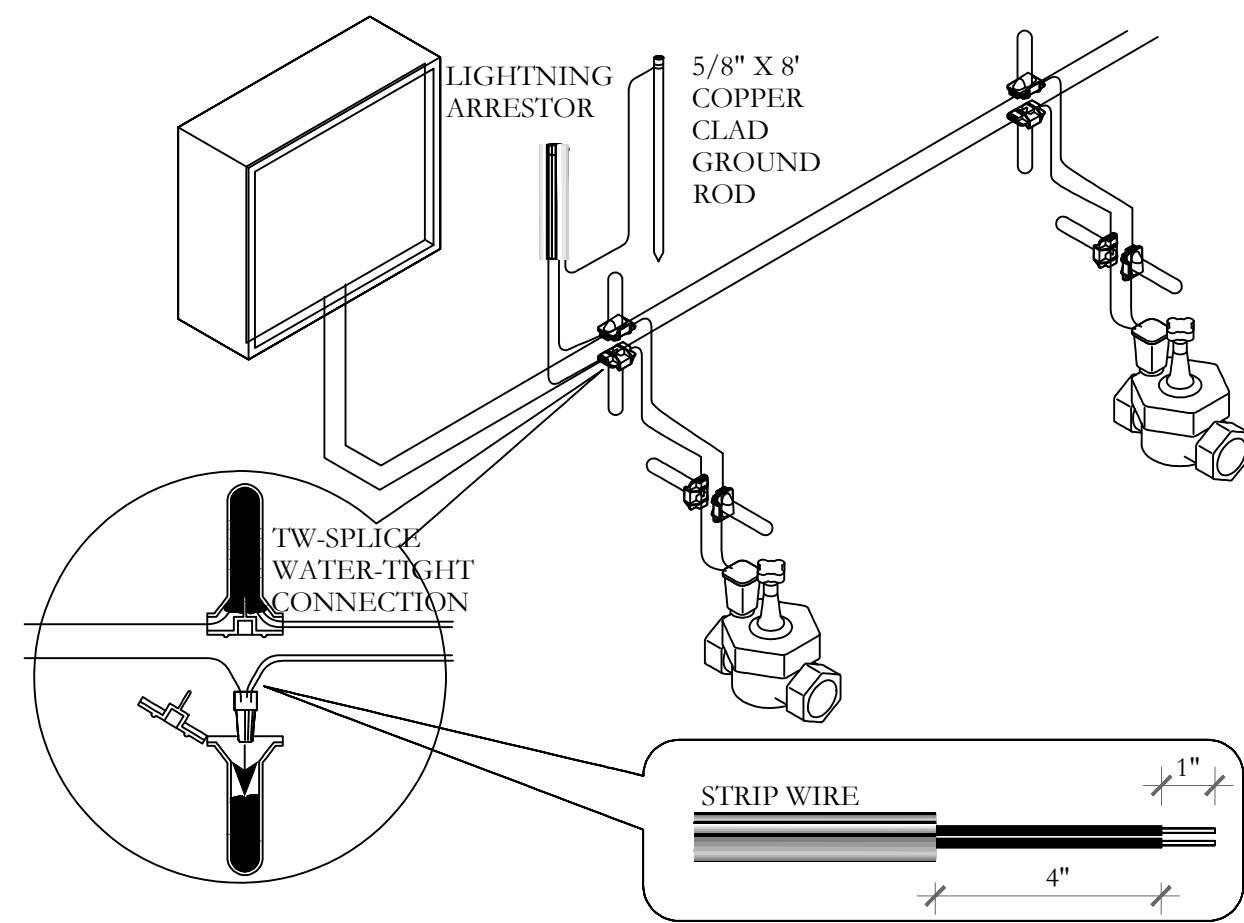


- 6 FINISH GRADE
- 7 RAIN BIRD FS SERIES FLOW SENSOR(S). SEE COVER SHEET FOR SIZE
- 8 36-INCH LENGTH OF COILED WIRE TO SATELLITE CONTROLLER SPARE STATION
- 9 36-INCH LENGTH OF COILED MAXI-CABLE TO FLOW SENSING
- 10 PVC PIPE UNION
- BRASS BALL VALVE
- 5 VALVE BOX WITH COVER - RAINBIRD JUMBO BOX
- 5X DIAMETER OF PIPE
- 5X DIAMETER OF PIPE
- NOTES:  
 1. NORMALLY CLOSED MASTER VALVE(S)  
 2. 36-INCH LENGTH OF COILED WIRE TO SATELLITE CONTROLLER SPARE STATION  
 3. 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL  
 4. VALVE BOX WITH COVER - RAINBIRD JUMBO BOX

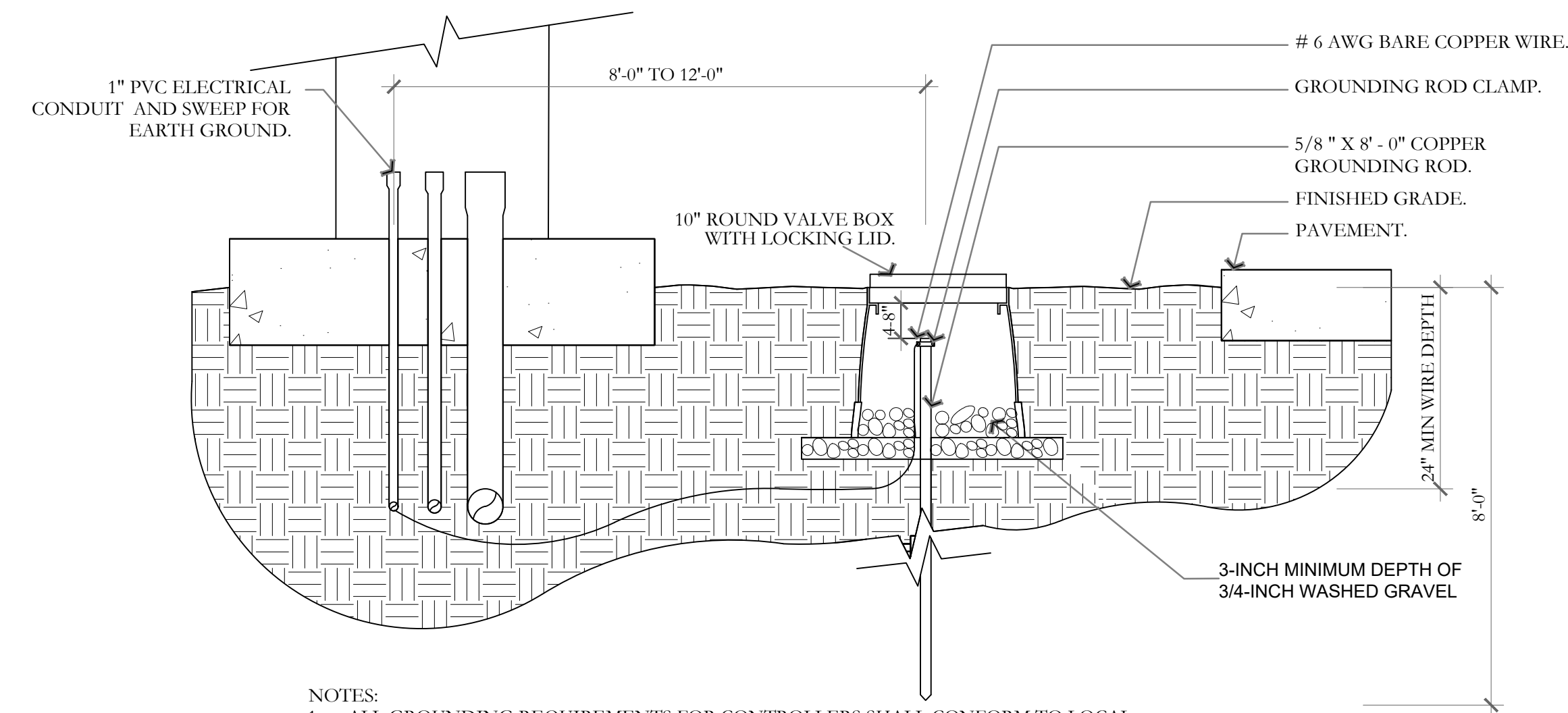
**MASTER VALVE AND FLOW SENSOR DETAIL**

NOT TO SCALE

ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	PROJECT INFORMATION	DEVELOPER / PROPERTY OWNER / CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE STAMP	DRAWING INFO
2/17/2026	UT25182	THIS PRINT FROM PKJ DESIGN GROUP IS BASED ON CIVIL AND ARCHITECT INFORMATION ISSUED ON 02/17/2026		ANDERSON DEVELOPMENT KEATON MORTON 801-856-2478 KEATON@AND-DEV.COM	PKJ DESIGN GROUP		PM: JTA DRAWN: ACP CHECKED: KBA PLOT DATE: 2/17/2026
NO. REVISION 1 CITY COMMENTS 2 CITY COMMENTS 3 4		DATE 01-20-2026 01-22-2026	BLUE STAKES OF UTAH UTILITY NOTIFICATION CENTER, INC 1-800-662-4111 www.bluestakes.org	<h1>MEADOWS AT NORTH LAKE PH2</h1> <h2>SARATOGA SPRINGS, UTAH</h2>		PKJ DESIGN GROUP Landscape Architecture • Planning & Visualization 3450 N. TRIUMPH BLVD. SUITE 102 LEHI, UTAH 84043 (801) 995-2217 www.pkjdesigngroup.com	
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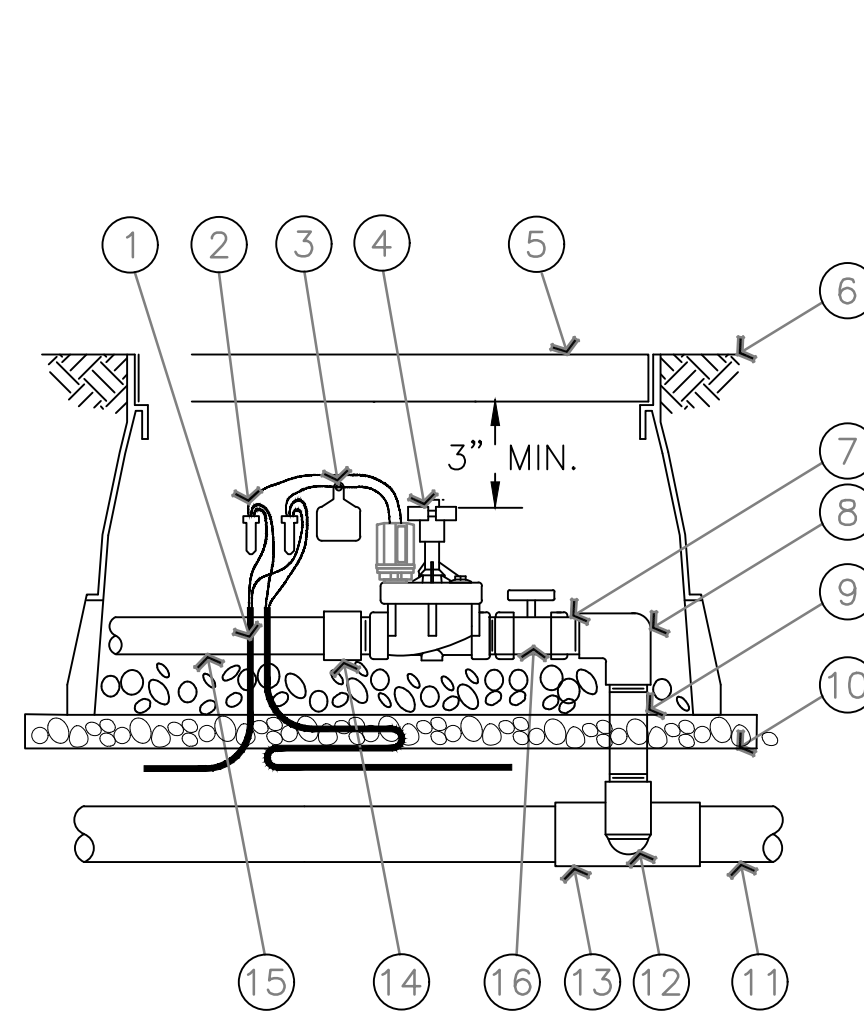


**J 2-WIRE CONNECTION DETAIL**  
NOT TO SCALE



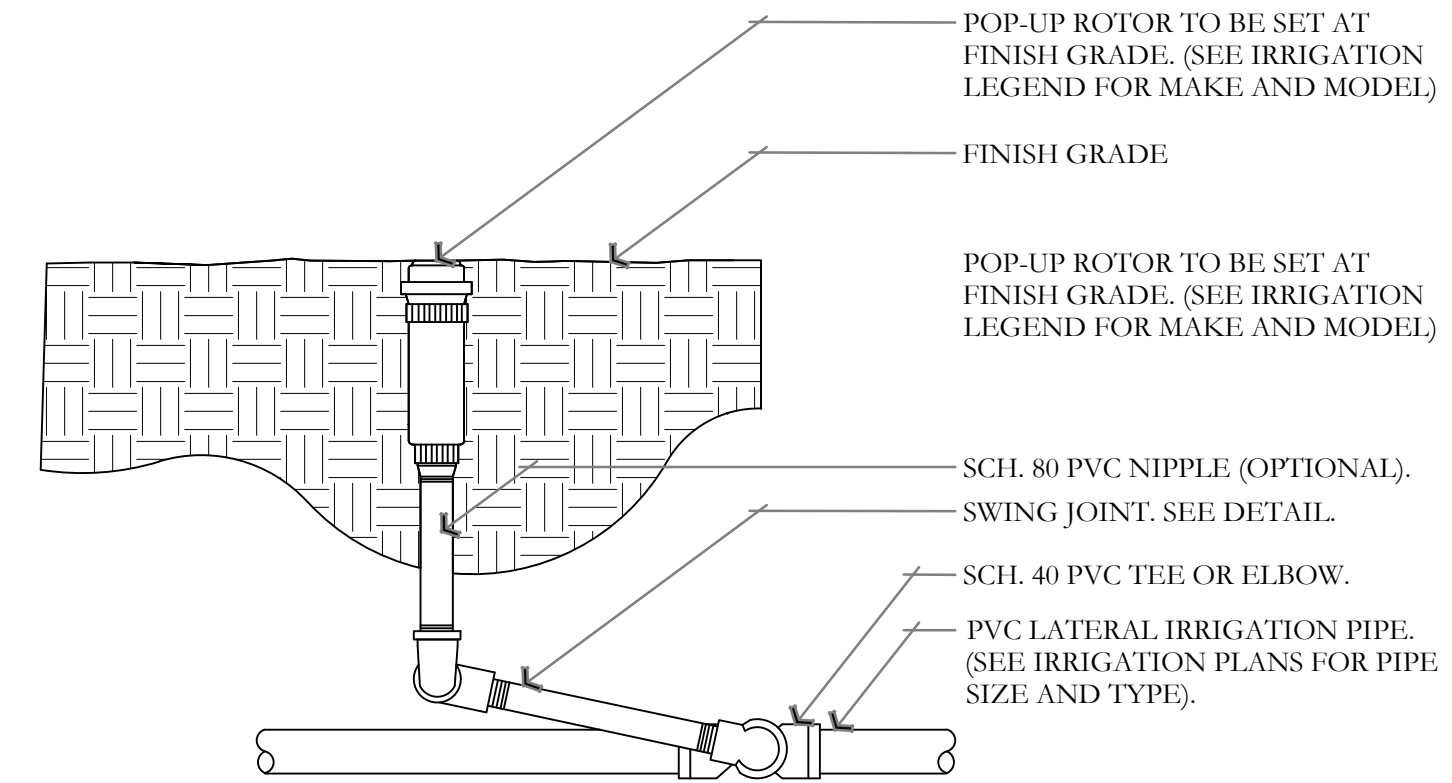
- NOTES:
1. ALL GROUNDING REQUIREMENTS FOR CONTROLLERS SHALL CONFORM TO LOCAL ELECTRIC CODES.
  2. GROUNDING ROD SHALL NOT BE LOCATED IN THE SAME TRENCH AS THE IRRIGATION MAINLINES OR LATERAL LINES.
  3. VALVE BOX SHALL BE WRAPPED WITH A MINIMUM 3 MIL THICK PLASTIC AND SECURED TO THE VALVE BOX USING DUCT TAPE OR ELECTRICAL TAPE.
  4. INSTALL GROUNDING ROD PER THE CONTROLLER MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.

**K GROUNDING ROD DETAIL**  
NOT TO SCALE



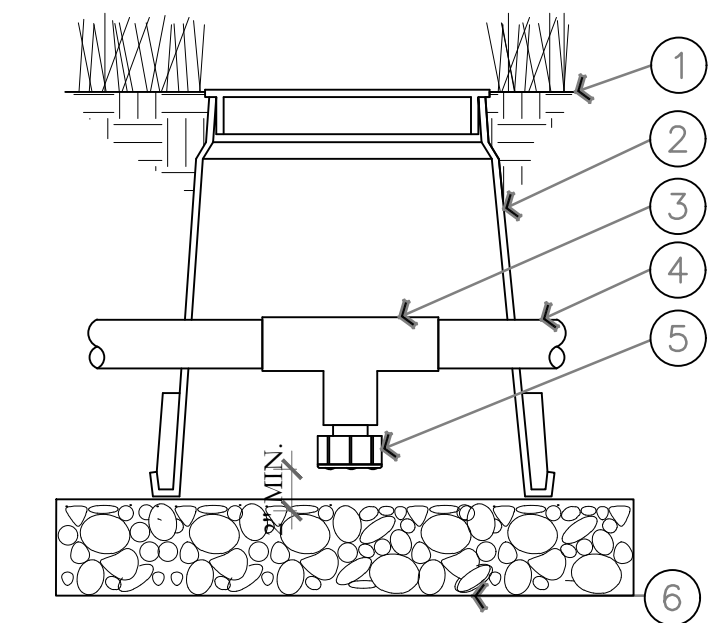
- 1 3/6-INCH LENGTH OF 2-WIRE CABLE
- 2 WATERPROOF CONNECTION RAIN BIRD WC20 (TWO REQUIRED)
- 3 ID TAG: RAIN BIRD VID SERIES
- 4 REMOTE CONTROL VALVE: RAIN BIRD PEB/PESB/PESB-R WITH IVM-SOL
- 5 VALVE BOX WITH COVER: RAIN BIRD VB-STD
- 6 FINISH GRADE/TOP OF MULCH
- 7 PVC SCH 80 NIPPLE (CLOSE)
- 8 PVC SCH 40 ELL
- 9 PVC SCH 80 NIPPLE (LENGTH AS REQUIRED)
- 10 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL
- 11 PVC MAINLINE PIPE SCH 80 NIPPLE (2-INCH LENGTH, HIDDEN) AND SCH 40 ELL
- 12 PVC SCH 40 TEE OR ELL
- 13 PVC SCH 40 MALE ADAPTER
- 14 PVC LATERAL PIPE
- 15 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL
- 16 BRASS BALL VALVE

**M ELECTRIC REMOTE-CONTROL VALVE PEB OR PESB SERIES WITH IVM-SOL**  
NOT TO SCALE



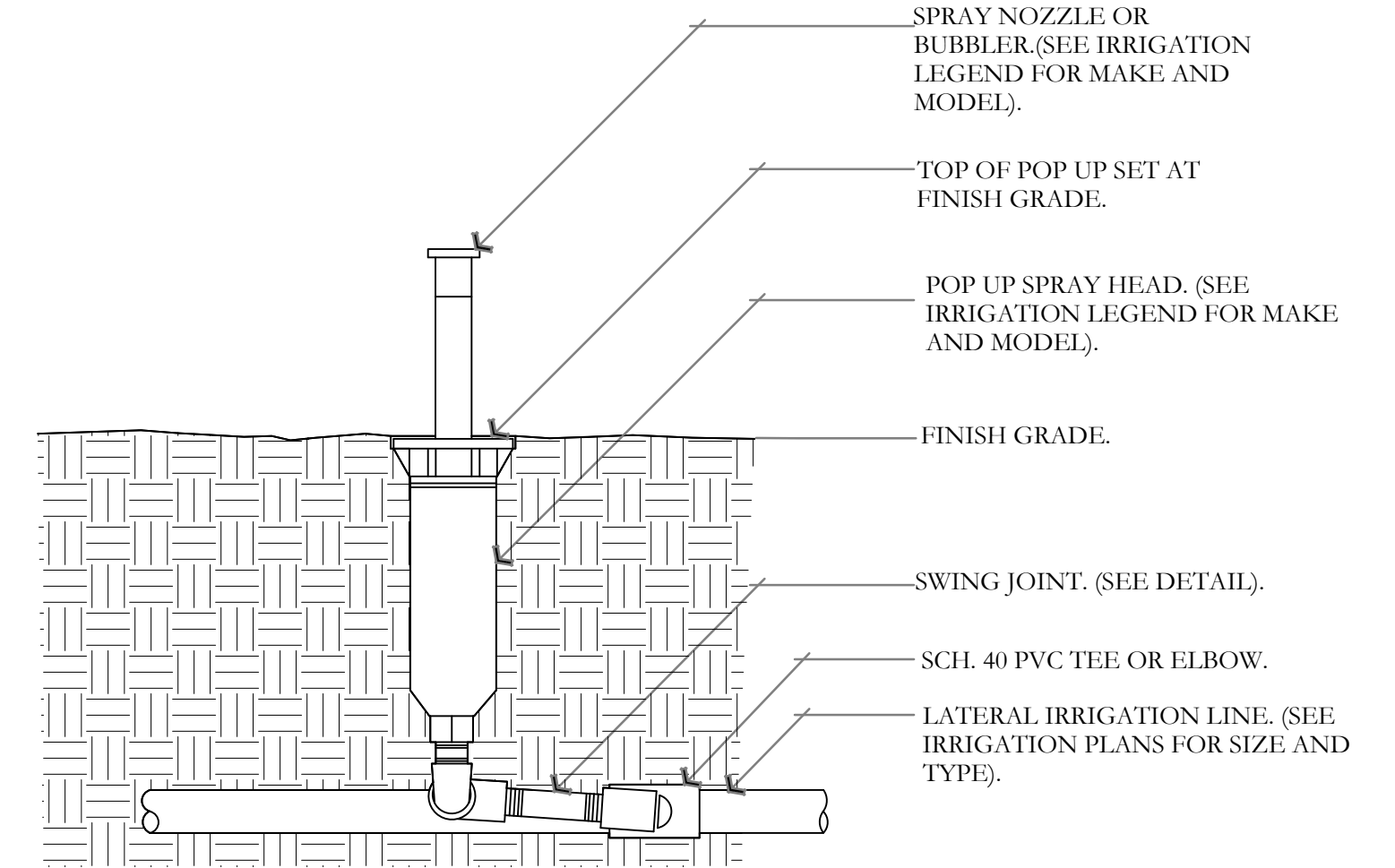
- NOTES:
1. ALL THREADED CONNECTION POINTS BETWEEN SCH 40 PVC AND SCH 80 PVC FITTING SHALL BE INSTALLED USING TEFLON TAPE.
  2. CONTRACTOR SHALL COMPACT SOIL AROUND ROTOR AND RISER PRIOR TO PLANTING, PLUGGING, SEEDING, OR LAYING OF SOD.

**N ROTOR HEAD DETAIL**  
NOT TO SCALE



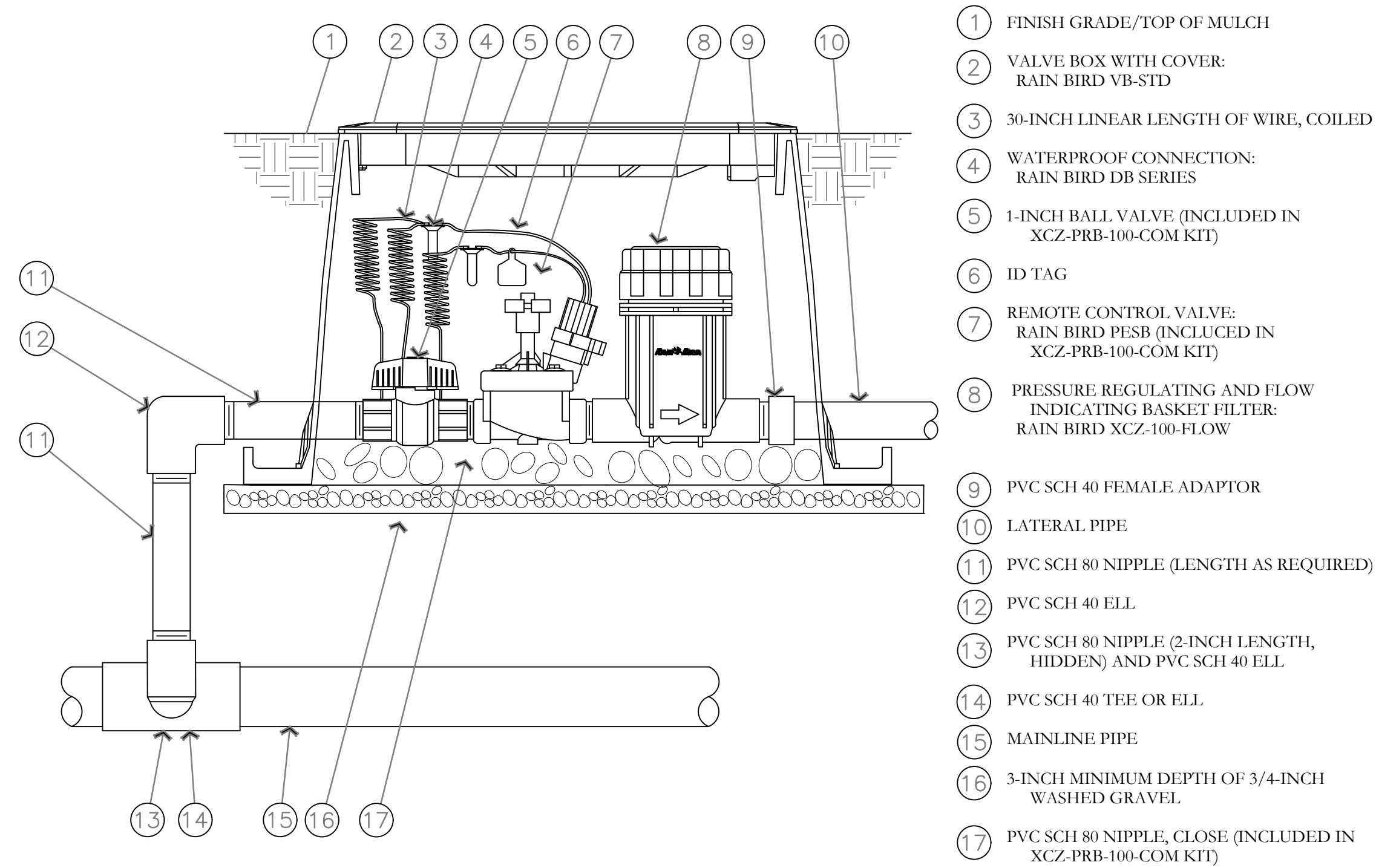
- 1 FINISH GRADE/TOP OF MULCH
- 2 VALVE BOX WITH COVER: RAIN BIRD VB-GRND
- 3 PVC SCH 40 TEE
- 4 PVC LATERAL PIPE
- 5 FILTERED DRAIN VALVE: RAIN BIRD 16A-FDV-075
- 6 6-INCH MINIMUM DEPTH OF 3/4\"/>

**L MANUAL LINE DRAIN VALVE DETAIL**  
NOT TO SCALE



- NOTE:
1. 4\"/>

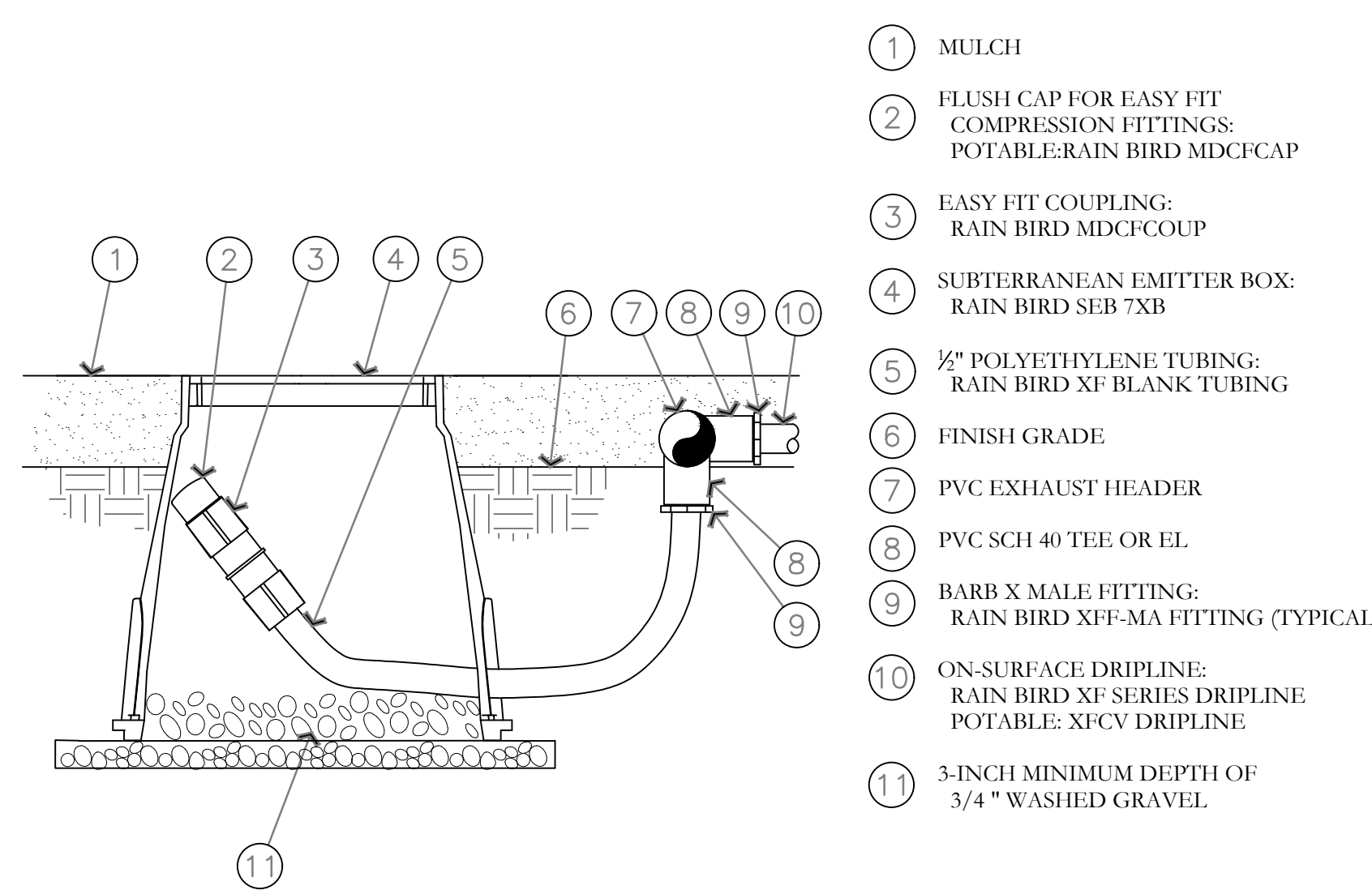
**O POP UP-SPRAY HEAD DETAIL**  
NOT TO SCALE



- 1 FINISH GRADE/TOP OF MULCH
- 2 VALVE BOX WITH COVER: RAIN BIRD VB-STD
- 3 30-INCH LINEAR LENGTH OF WIRE, COILED
- 4 WATERPROOF CONNECTION: RAIN BIRD DB SERIES
- 5 1-INCH BALL VALVE (INCLUDED IN XCZ-PRB-100-COM KIT)
- 6 ID TAG
- 7 REMOTE CONTROL VALVE: RAIN BIRD PMSB (INCLUDED IN XCZ-PRB-100-COM KIT)
- 8 PRESSURE REGULATING AND FLOW INDICATING BASKET FILTER: RAIN BIRD XCZ-100-FLOW
- 9 PVC SCH 40 FEMALE ADAPTOR
- 10 LATERAL PIPE
- 11 PVC SCH 80 NIPPLE (LENGTH AS REQUIRED)
- 12 PVC SCH 40 ELL
- 13 PVC SCH 80 NIPPLE (2-INCH LENGTH, HIDDEN) AND PVC SCH 40 ELL
- 14 PVC SCH 40 TEE OR ELL
- 15 MAINLINE PIPE
- 16 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL
- 17 PVC SCH 80 NIPPLE, CLOSE (INCLUDED IN XCZ-PRB-100-COM KIT)

**P DRIP CONTROL ZONE KIT DETAIL**  
NOT TO SCALE

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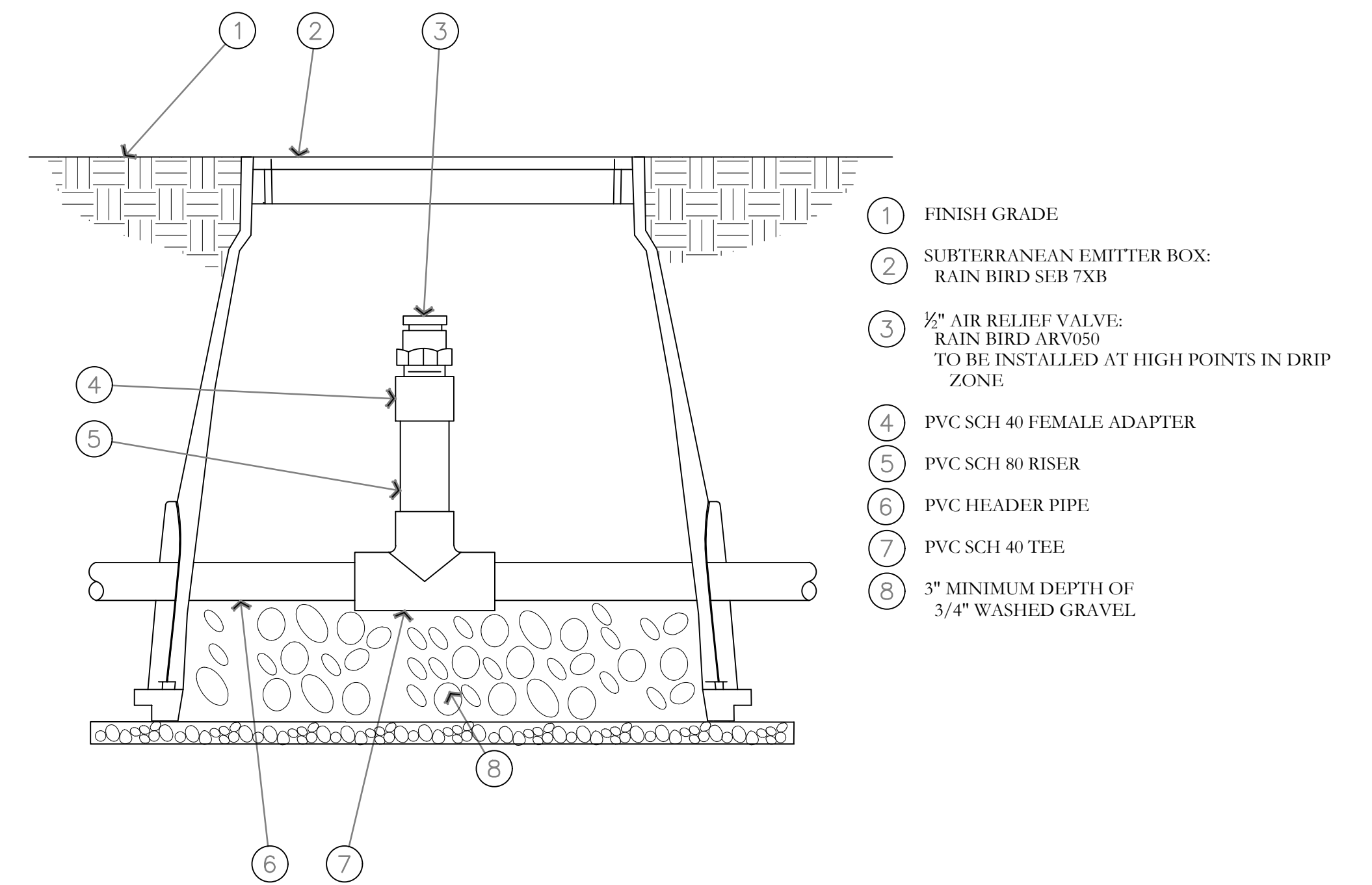


- 1 MULCH
- 2 FLUSH CAP FOR EASY FIT COMPRESSION FITTINGS: POTABLE:RAIN BIRD MDCFCAP
- 3 EASY FIT COUPLING: RAIN BIRD MDCFCOUP
- 4 SUBTERRANEAN EMITTER BOX: RAIN BIRD SEB 7XB
- 5 1/2" POLYETHYLENE TUBING: RAIN BIRD XF-BLANK TUBING
- 6 FINISH GRADE
- 7 PVC EXHAUST HEADER
- 8 PVC SCH 40 TEE OR ELL
- 9 BARB X MALE FITTING: RAIN BIRD XFF-MA FITTING (TYPICAL)
- 10 ON-SURFACE DRIPLINE: RAIN BIRD XF SERIES DRIPLINE POTABLE: XFCV DRIPLINE
- 11 3-INCH MINIMUM DEPTH OF 3/4" WASHED GRAVEL

NOTE:  
1. ALLOW A MINIMUM OF 6-INCHES OF DRIPLINE TUBING IN VALVE BOX IN ORDER TO DIRECT FLUSHED WATER OUTSIDE VALVE BOX.

**Q ON-SURFACE DRIPLINE FLUSH POINT DETAIL**  
NOT TO SCALE

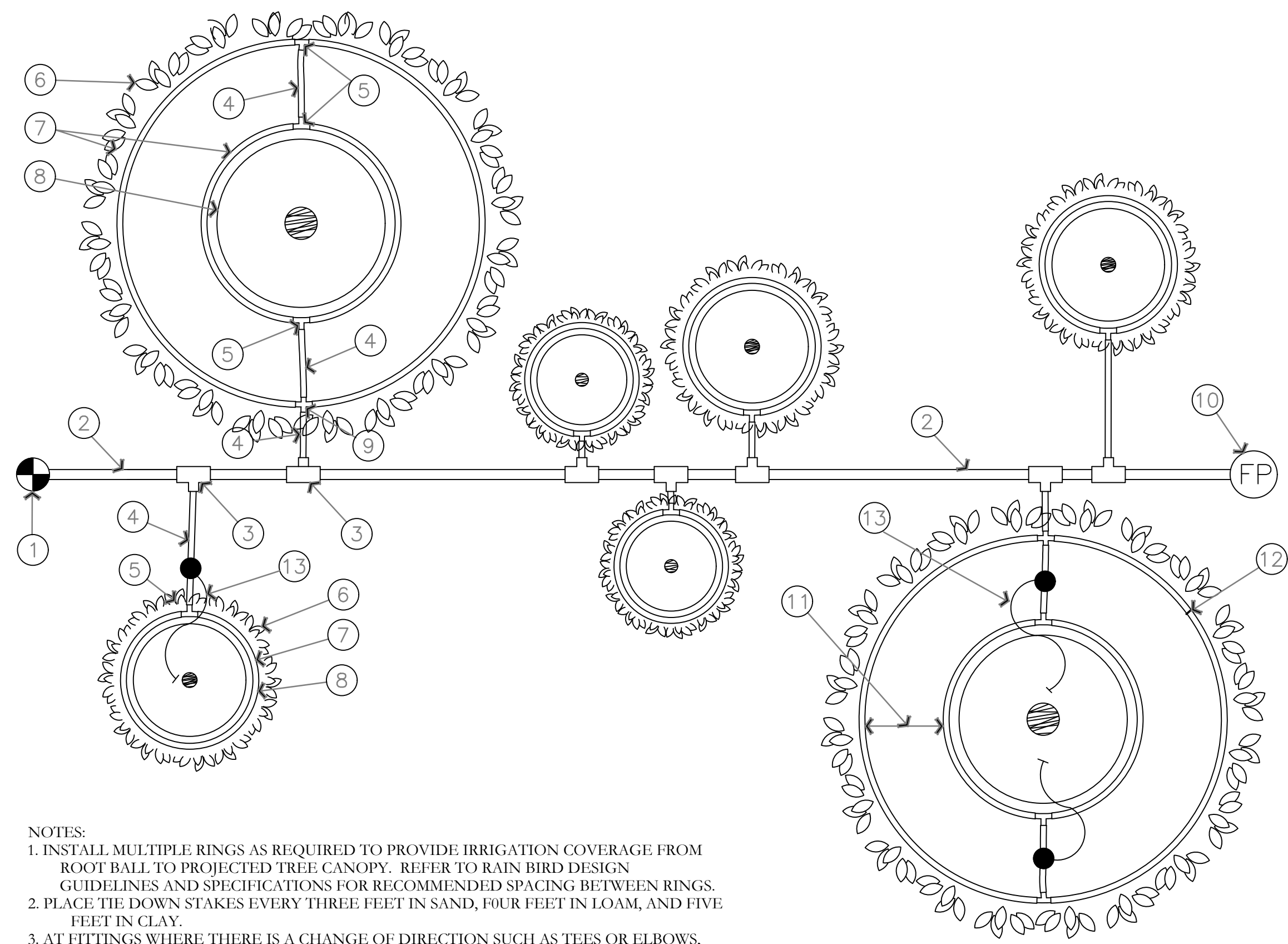
PKJ DESIGN GROUP



- 1 FINISH GRADE
- 2 SUBTERRANEAN EMITTER BOX: RAIN BIRD SEB 7XB
- 3 1/2" AIR RELIEF VALVE: RAIN BIRD ARV050 TO BE INSTALLED AT HIGH POINTS IN DRIP ZONE
- 4 PVC SCH 40 FEMALE ADAPTER
- 5 PVC SCH 80 RISER
- 6 PVC HEADER PIPE
- 7 PVC SCH 40 TEE
- 8 3" MINIMUM DEPTH OF 3/4" WASHED GRAVEL

**R AIR RELIEF VALVE DETAIL**  
NOT TO SCALE

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NOTES:  
1. INSTALL MULTIPLE RINGS AS REQUIRED TO PROVIDE IRRIGATION COVERAGE FROM ROOT BALL TO PROJECTED TREE CANOPY. REFER TO RAIN BIRD DESIGN GUIDELINES AND SPECIFICATIONS FOR RECOMMENDED SPACING BETWEEN RINGS.  
2. PLACE TIE DOWN STAKES EVERY THREE FEET IN SAND, FOUR FEET IN LOAM, AND FIVE FEET IN CLAY.  
3. AT FITTINGS WHERE THERE IS A CHANGE OF DIRECTION SUCH AS TEES OR ELBOWS, USE TIE-DOWN STAKES ON EACH LEG OF THE CHANGE OF DIRECTION.

- 1 RAIN BIRD CONTROL ZONE KIT (SIZED TO ACCOMMODATE LATERAL FLOW DEMAND)
- 2 PVC DRIP LATERAL PIPE
- 3 PVC SCH 40 TEE OR ELL (TYPICAL)
- 4 1/2" POLYETHYLENE TUBING: RAIN BIRD XF SERIES-S FOR COPPER SHEILD (TYPICAL)
- 5 BARB X BARB INSERT TEE: RAIN BIRD XFF-TEE (TYPICAL)
- 6 PROJECTED CANOPY LINE OF TREE OR SHRUB (TYPICAL)
- 7 ON-SURFACE DRIPLINE: RAIN BIRD XF SERIES DRIPLINE POTABLE: XFCV SERIES PLACE AS SHOWN (LENGTH AS REQUIRED, TYPICAL)
- 8 ROOT BALL (TYPICAL)
- 9 BARB X BARB INSERT CROSS: RAIN BIRD XFD-CROSS (TYPICAL)
- 10 DRIPLINE FLUSH POINT (SEE RAIN BIRD DETAIL: "XFCV DRIPLINE FLUSH POINT WITH BALL VALVE")
- 11 SPACING PER SPECIFICATION
- 12 TIE DOWN STAKE: RAIN BIRD TDS-050 WITH BEND (QUANTITY AS REQUIRED, SEE NOTES 2-3 BELOW)
- 13 POINT SOURCE EMITTERS FOR ESTABLISHMENT PERIOD. REMOVE AFTER ESTABLISHMENT PERIOD.

**S ON-SURFACE DRIPLINE TREE/SHRUB DETAIL**  
NOT TO SCALE

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ISSUE DATE	PROJECT NUMBER	PLAN INFORMATION	PROJECT INFORMATION	DEVELOPER / PROPERTY OWNER / CLIENT	LANDSCAPE ARCHITECT / PLANNER	LICENSE STAMP	DRAWING INFO
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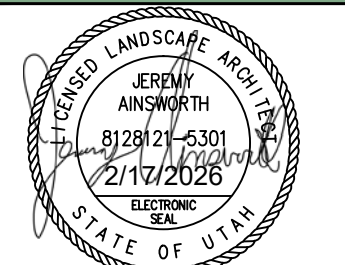
2/17/2026 UT25182

THIS PRINT FROM PKJ DESIGN GROUP IS BASED ON CIVIL AND ARCHITECT INFORMATION ISSUED ON 02/17/2026

# MEADOWS AT NORTH LAKE PH2 SARATOGA SPRINGS, UTAH

ANDERSON DEVELOPMENT  
KEATON MORTON  
801-856-2478  
KEATON@AND-DEV.COM

**PKJ DESIGN GROUP**  
Landscape Architecture Planning & Visualization  
3450 N. TRIUMPH BLVD. SUITE 102  
LEHI, UTAH 84043 (801) 995-2217  
www.pkjdesigngroup.com



PM:	JTA
DRAWN:	ACP
CHECKED:	KBA
PLOT DATE:	2/17/2026

NO.	REVISION	DATE
1	CITY COMMENTS	01-20-2026
2	CITY COMMENTS	01-22-2026
3		
4		

**811** BLUE STAKES OF UTAH  
UTILITY NOTIFICATION CENTER, INC  
1-800-662-4111  
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**IRRIGATION DETAILS**  
CITY PERMIT SET  
IR-503



**Site Plan**

**The Hub Lot 5**

**March 26, 2026**

**PUBLIC MEETING**

Applicant:	Michael McIntire
Owner:	Middle Mile Infastructure LLC
Location:	2438 N Stagecoach Drive
Project Acreage:	0.69 acres
Residential Units/Lots:	0
Current Use:	Vacant Land
Land Use Designation:	Regional Commercial
Parcel Zoning:	Regional Commercial
Adjacent Zoning:	Regional Commercial and Office Warehouse
Adjacent Uses:	Vacant Land
Type of Action:	Administrative
Land Use Authority:	City Council
Planner:	Sam Stout, Planner II

**A. Executive Summary and Request:**

The applicant is requesting approval of a Site Plan for Lot 5 of The Hub at Saratoga Phase 1 Subdivision, located at 2438 N Stagecoach Drive. The total project area is 0.69 acres. An unmanned fiber utility, with two buildings – 852 square feet each, are proposed on the site. The site will be visited occasionally for normal maintenance

**Recommendation:**

**Staff recommends that the Planning Commission conduct a public meeting on the application, review and discuss the proposal, and choose from the options in the Recommendation and Alternatives Section of this report.** Options include a positive recommendation with or without conditions, a negative recommendation, or continuation.

**B. Background:**

**May 6, 2021** – the City approved the Final Plat for The Hub at Saratoga Phase 1.

**April 2, 2025** - the Planning Director approved the Concept Plan review letter.

**C. Process:** City Code Chapter 19.13 outlines the process for a site plan and specifies that the City Council is the approval authority for non-residential site plans.

## Site Plan/Site Plan Amendment

City Code Chapter 19.14.05.8 outlines the process for a site plan review.

### Site Plan Application and Approval Process.

- a. All persons seeking Site Plan approval shall submit an application to the Planning Department for review by the City's Development Review Committee (DRC).  
**Complete.**
- b. Complete engineering drawings for all on-site and off-site improvements must be provided prior to the Site Plan application being scheduled for any public meeting or hearing. The Engineering Department and Development Review Committee shall review the drawings for compliance with City ordinances, regulations, and standards. **Construction Drawings approved.**
- c. New site plans shall follow the process below:
  - i. Prior to being scheduled for any public meeting or hearing, the developer shall provide a soils report for the development. **Provided.**
  - ii. Upon compliance with the Development Review Committee's recommendations, the revised application shall be forwarded to the Planning Commission for possible recommendation. **Completed.**
  - iii. Upon recommendation by the Planning Commission, the application shall be forwarded to the City Council. **TBD.**
  - iv. The City Council shall review and take action to table, approve, deny, or to modify the same. **TBD.**
  - v. Upon action by the City Council on the Site Plan application, the City Recorder shall prepare written minutes of the decision. **TBD.**

### D. Community Review:

Public Meeting: This has been noticed as a public meeting pursuant to City and State statutes, which requires posting notice of the meeting and the agenda not less than 24 hours before the meeting.

As of the date of this report, no public input has been received.

- E. **General Plan:** The General Plan Land Use Map shows the property of the proposed Site Plan as RC – Regional Commercial.

### Staff conclusion: Consistent.

The proposed use for this site is Private Utility – which is permitted in the Regional Commercial Zone

### F. Code Criteria:

For full analysis please see the attached Planning Review Checklist.

- 19.04, Land Use Zones: **Complies.**
- 19.05, Supplementary Regulations: **Complies**

**G. Recommendation and Alternatives:**

Staff recommends that the Planning Commission discuss the application and choose from the following options.

**Option 1 – Positive Recommendation** “I move that the Planning Commission forward a recommendation for approval of the requested Site Plan for The Hub Lot 5 located at 2438 N Stagecoach Drive, with the Findings and Conditions in the Staff Report.”

**Findings**

1. The application is consistent with the General Plan, as articulated in Section E of the staff report, which section is incorporated by reference herein.
2. The application complies with the criteria in the Land Development Code, as articulated in Section F of the staff report, which section is incorporated by reference herein.

**Conditions:**

1. All conditions of the City Engineer shall be met, including but not limited to those in the attached Engineering Staff Report.
2. All requirements of the Fire Chief shall be met.
3. Once approved by the City Council, all remaining redlines on plans, Engineering staff report, and the redlines in the Application Review Checklist shall be corrected before the construction drawings are approved by staff.
4. All other Code requirements shall be met.
5. Any other conditions or changes as articulated by the Planning Commission:

\_\_\_\_\_.

**Option 2 – Continuance**

“I move to **continue** the Site Plan for The Hub Lot 5 **The Hub Lot 5** to another meeting on [DATE], with direction to the applicant and Staff on information and/or changes needed to render a decision, as follows:

1. \_\_\_\_\_
2. \_\_\_\_\_

**Option 3 – Negative Recommendation**

“I move that the Planning Commission forward a recommendation of denial for the requested Site Plan located at 2438 N Stagecoach Drive with the Findings below:

1. The application is not consistent with the General Plan:
  - a. \_\_\_\_\_, and/or,
2. The application is not consistent with Section {XX.XX} of the Code:
  - a. \_\_\_\_\_, and/or

**H. Exhibits:**

1. City Engineer’s Report

2. Location & Zone Map
3. Planning Review Checklist
4. Proposed Building Elevations
5. Construction Drawings

## Staff Report

**Author:** Scott Petrik, Engineer I  
**Subject:** MMI – ILA Shelter, Lot 5 the Hub – Site Plan  
**Date:** March 26, 2026  
**Type of Item:** Site Plan Approval



SARATOGA  
SPRINGS

### Description:

**A. Topic:** The Applicant has submitted a Site Plan application. Staff has reviewed the submittal and provides the following recommendations.

**B. Background:**

*Applicant:* Alec Geist—Middle Mile Infrastructure  
*Request:* Site Plan Approval  
*Location:* 2438 N Stagecoach Dr  
*Acreage:* 0.69 Acres - 1 Lots

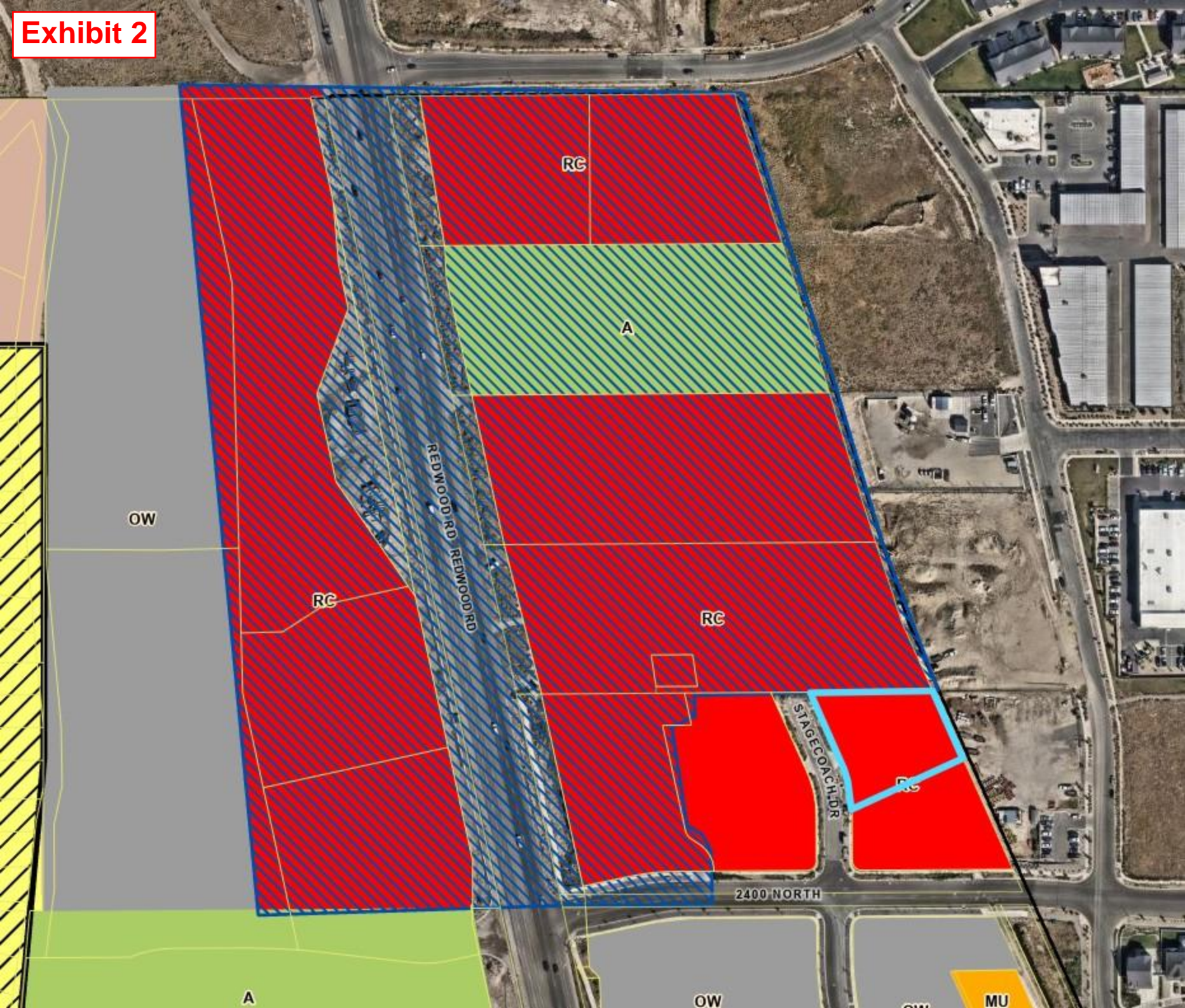
**C. Recommendation:** Staff recommends the approval of Site Plan subject to the following conditions:

1. All review comments and redlines provided by the City Engineer are to be complied with and implemented with the approved construction drawings.
2. Provide a Storm Water Pollution Prevention Plan (SWPPP) following the State template, prior to the pre-construction meeting.
3. Owner to record a Long-Term Storm Water Management Agreement and provide a Long-Term Storm Water Management Plan (LTSWMP) for the project. The plan portion will be required prior to scheduling a pre-construction meeting.
4. Project shall comply with all ADA standards and requirements.
5. Developer must secure water rights as required by the City Engineer, City Attorney, and development code.
6. Developer shall provide easements for all public utilities not located in the public right-of-way.
7. Developer is required to ensure that there are no adverse effects to adjacent properties due to the grading practices employed during construction.
8. Developer may be required by the Saratoga Springs Fire Chief to perform fire flow tests prior to the issuance of certificate of occupancy or prior to commencement of

the warranty period.

9. Submittal of as-built drawings in pdf format to the City Engineer is required prior to acceptance of site improvements and the commencement of the warranty period.
10. Project bonding for the site plan must be completed as approved by the City Engineer prior to the preconstruction meeting.
11. Developer shall bury the power lines less than 46 kV and underbuilt that are within this plat.
12. Please be advised that, only a signed reimbursement agreement approved by the City Council, as detailed in the City Engineering Standard 00500/2.3/E, can guarantee that improvements in this project are eligible for reimbursement by the City. If the applicant believes that some of the improvements in this project are eligible for reimbursement from the City, it is strongly recommended the applicant obtain a reimbursement agreement from the City before they are constructed. Constructing them without a signed reimbursement agreement in place, even if they are approved/accepted in the construction drawings, inspected by the City as they are constructed, and/or contained in the City's Capital Facilities Plans and Impact Fees Facilities Plans, is at the sole risk of the developer without any guarantee of reimbursement or any particular timing of reimbursement.
13. The developer shall install a control orifice, snout, and sump for stormwater outlet control prior to discharge to the city's stormwater system.
14. Traffic impacts from this site will be minimal with an expected average of 4 visits per month.

**Exhibit 2**





2100 NORTH (Lehi coord.)

MVC/SR 85

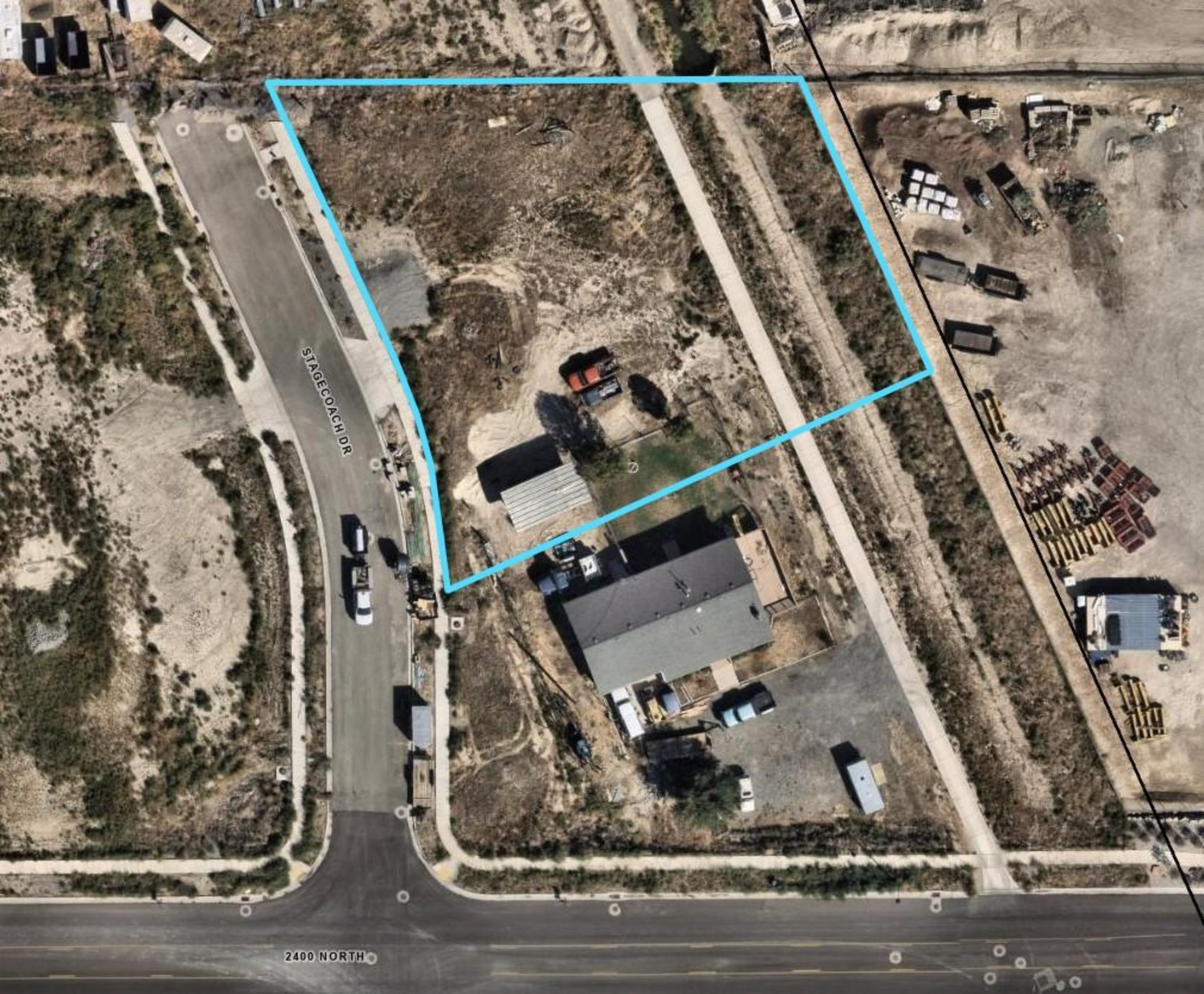
MVC/SR 85

REDWOOD RD

HARVEST HILLS BLVD

HARVEST HILLS BLVD

WILD BLOSSOM BLVD



STAGECOACH DR

2400 NORTH



**APPLICATION REVIEW CHECKLIST**  
Updated 8.8.25

**Application Information**

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**SITE PLAN APPLICATION**  
**The Hub Lot 5 Site Plan**

Applicant:	Alec Geist – Middle Mile Infrastructure
Owner:	Boyd Brown – BB Land Opps LLC
Location:	2438 N Stagecoach Drive
Project Acreage:	0.69 acres
Current Use:	Regional Commercial
Land Use Designation:	Regional Commercial
Requested Land Use:	Regional Commercial
Parcel Zoning:	Regional Commercial
Requested Zoning:	Regional Commercial
Adjacent Zoning:	Regional Commercial & Office Warehouse
Adjacent Uses:	Vacant Land & Commercial
Past Action and Date:	Not Applicable
Type of Action:	Administrative
Land Use Authority:	City Council
Planner:	Sam Stout, Planner II
Date Received:	2 <sup>nd</sup> Submittal: February 6, 2026
Date of Review:	2 <sup>nd</sup> Submittal: March 3, 2026
Parcel Number(s) and size:	68-054-0012

**Section 19.13 – Application Submittal**

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- Application Complete (Date of Payment): July 14, 2025
- Rezone Required: No
- General Plan Amendment required: No

**Section 19.13.04 – Process**

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- Required Meetings: **Planning Commission and City Council**
- Planning Director Approval: **No**
- Public Hearing Required – check 19.13.04 process table: **No**
- Neighborhood Meeting Required: **No**

**DRC Review**

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**DRC Review Comments:**

**The Hub Site Plan (10-minute discussion)**

- Further architectural details needed for review
- Canal trail already installed
- Roads not aligned with existing infrastructure

**Code Review**

- 19.04, Land Use Zones
  - Zone: Regional Commercial
  - General Plan Land Use: Regional Commercial

19.04.01 Requirements		Regional Commercial	
Category To Be Reviewed	Regulation	Compliance	Findings
Development Size (Minimum)	N/A		
Lot Size (Minimum)	30,000 sq. ft.	N/A	<i>Exempt due to 19.05.16(8)(a)</i>
Front/Corner Side Setback (Minimum)	15'	<b>Complies</b>	10'
Interior Side Setback (Minimum)	10' or height of building, whichever is greater, when adjacent to a residential zone	<b>Complies</b>	10'
Rear Setback (Minimum)	30' or height of building, whichever is greater, when adjacent to a residential zone	<b>Complies</b>	10'
Building Separation (Minimum)	N/A		
Lot Width (Minimum)	N/A		
Lot Frontage (Minimum)	N/A		
Building Height (Maximum)	50'	<b>Complies</b>	<i>Less than 17'</i>
Lot coverage (Maximum)	50%	N/A	<i>Exempt due to 19.05.16(8)(a)</i>
Building Size (Minimum)	1,000 sq. ft.	N/A	<i>Exempt due to 19.05.16(8)(a)</i>
Building Size (Maximum)	N/A		
Arterial Street Setback	105' off the arterial roadway centerline, or, if applicable, 15' from the back of the 30' trail corridor - whichever is greater.	N/A	
<p>1. One building of 80,000 sq. ft. maximum is permitted for every 20 contiguous acres within the same development. i.e. 40 contiguous acres is required for two buildings that are each 80,000 sq. ft. maximum.</p>			
<p>2. These limits shall not be applied to “Retail, Big Box,” “Office, Professional,” or “Office, Medical and Health Care” which may be any size in the HC zone.</p>			

3. Zero (0) foot side setback is permitted where the adjacent property is RC, CC, or HC and where the use of the building is limited exclusively to non-residential use. The wall along the interior side property line shall be unperced and constructed in accordance with the International Building Code, requiring a building permit and building inspection.

### 19.05 Supplemental Regulations

Regulation	Compliance	Findings
<b>Flood Plain:</b> All buildings and structures intended for human occupancy shall be constructed at least (1) one foot above the base flood elevation of Zone A as defined on the FEMA Flood Insurance Map.	N/A.	<i>Not Applicable</i>
<b>Water &amp; Sewage:</b> Each lot shall be connected to City water and sewer.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<b>Transportation Master Plan:</b> No building lot shall be created and no structure shall be erected within the location of a proposed street, road, highway, or right-of-way as shown on the City's currently-approved Transportation Master Plan.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<b>Property Access -</b> All lots shall abut a dedicated public street or highway or a private roadway.	<b>Complies.</b>	<i>Shown in file.</i>
<b>19.05.16. Special Standards and Considerations Governing Particular Uses. See Code for details</b>	N/A.	<i>Not Applicable</i>
Automobile refueling stations and car wash operations.	N/A.	<i>Not Applicable</i>
Automobile Repair, Minor:	N/A.	<i>Not Applicable</i>
Car Wash (full service).	N/A.	<i>Not Applicable</i>
Hotels.	N/A.	<i>Not Applicable</i>
Kennel, Private.	N/A.	<i>Not Applicable</i>
Storage, Self-Storage, or Mini-Storage Units.	N/A.	<i>Not Applicable</i>
Vehicle Storage.	N/A.	<i>Not Applicable</i>
Public and Private Utility Building or Facility and Public Building Sites.	<b>Complies.</b>	<i>Shown in file.</i>
Bars.	N/A.	<i>Not Applicable</i>

### 19.05.16 Special Standards and Considerations Governing Particular Uses

#### Public and Private Utility Building or Facility and Public Building Sites

Regulation	Compliance	Findings
Minimum lot size, lot width, lot frontage, lot coverage, and building size requirements do not apply to Public Utility Buildings or Facilities and Public Building Sites that are not intended for occupancy and are owned by a governmental entity or public utility company in the State of Utah.	<b>Complies.</b>	<i>Public utility building</i>
Utility structures and Public Buildings shall not encroach onto a public utility easement. However, this does not preclude the use of the public utility easement for service delivery.	<b>Complies.</b>	<i>Shown in file</i>
<b>Setbacks:</b> The following setbacks shall apply to buildings and structures that are not intended for occupancy. All buildings that are intended for occupancy shall comply with the setback requirements within the underlying zone. <ul style="list-style-type: none"> <li>i. In residential zones, above-grade buildings and structures over 200 square feet in size and/or 15 feet in height, shall comply with the minimum front setback within the underlying zone. The side and rear setbacks shall be ten feet minimum.</li> <li>ii. For all other buildings and structures (including those in non-residential zones), including below-grade structures, the minimum setbacks shall be equal to the required public utility easements</li> </ul>	<b>Complies.</b>	<i>Shown in file</i>

<p><b>Fencing:</b> Because of security concerns or specific site or facility design, the Public Utility site or Facility or Public Building site shall not be required to have fencing or walls.</p> <p>1. Vinyl-coated chain link is allowed for facilities more than 200 feet from an existing residential dwelling that is in a residential zone. Chain link fence shall be setback five feet from the right of way.</p> <p>Notwithstanding fencing requirements contained in Chapter 19.06, barbed wire may be used in conjunction with a chain-link fence if warranted because of a legitimate security concern related to the health, safety, or general welfare of the public.</p>	<b>Complies.</b>	<i>Shown in file.</i>
Installation of fencing or walls or type of fencing or walls, if used, shall be determined at the discretion of the property owner, and subject to final approval through the site plan process. Fencing shall consist of one or more of the following: masonry, wrought iron style, or steel reinforced pre-panelized polyethylene. Vinyl-coated chain link may be used subject to subsection (i).	<b>Complies.</b>	<i>Shown in file.</i>
Fencing or walls taller than three feet may be permitted in the front yard after review and recommendation by the Development Review Committee and subject to final approval through the site plan review process.	<b>N/A.</b>	<i>Not Applicable</i>
Fences and walls that require a building permit shall not encroach onto a public utility easement, but may cross it if needed.	<b>Complies.</b>	<i>Shown in file.</i>
The maximum height of a utility fence or wall shall be eight feet.	<b>Complies.</b>	<i>Eight feet.</i>
<b>Landscaping:</b> All structures intended for occupancy shall meet the landscaping requirements in Chapter 19.06. All other structures shall meet the requirements below.	<b>Complies.</b>	<i>Not intended for occupancy.</i>
The park strip adjacent to Public Utility Building or Facility sites and Public Building Sites shall be landscaped with trees spaced no more than 30 feet on-center, rock mulch, and/or additional vegetation.	<b>Complies.</b>	<i>Shown in file</i>
All tree requirements contained in Chapter 19.06 with respect to size and clear sight triangle shall apply.	<b>Complies.</b>	<i>Shown in file.</i>
Additional landscaping may be installed on-site at the discretion of the property owner.	<b>Complies.</b>	<i>Shown in file.</i>
If landscaping is not used internally to the site, the ground shall be prepared to prevent weed growth.	<b>N/A.</b>	
Native vegetation may be used to meet the landscaping requirements in this subsection. Irrigation systems are not required if there are no reasonably available water sources and the developer successfully establishes the non-native vegetation with other means of watering	<b>N/A.</b>	
If water sources are not reasonably available, the tree requirement in this subsection shall not apply.	<b>N/A.</b>	
<b>Access:</b> Access to the site is required via easements or driveways.	<b>Complies.</b>	<i>Shown in file.</i>
<b>Site and Architectural Design:</b> Public utility buildings and facilities and Public Buildings that are not intended for occupancy shall be exempt from Chapter 19.16, Site and Architectural design standards, and shall comply with the following requirements:	<b>Complies.</b>	<i>Public utility building.</i>
Buildings shall be constructed of masonry with a standing seam metal roof and include an anti-graffiti coating.	<b>Complies.</b>	<i>Shown in file.</i>
Buildings shall include a minimum of a 4:12 gabled pitched roof	<b>Complies.</b>	<i>Shown in file.</i>
Building colors shall be earth-tones.	<b>Complies.</b>	<i>Shown in file.</i>
Notwithstanding, nothing in this section shall preclude imposing additional conditions in order to mitigate detrimental effects to the health, safety, and general welfare of the public.	<b>Shall Comply.</b>	<i>Applicant shall comply.</i>

Temporary/portable structures are prohibited, except for those used by municipalities, and shall not be required to meet the above architectural requirements in this subsection (g).	N/A.	
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## 19.14 Site Plan Review

Regulation	Compliance	Findings
<p><b>Approval Required:</b> Site Plan approval shall be required for all developments which contain the following uses, together with any others for which it is required elsewhere in these Ordinances:</p> <ol style="list-style-type: none"> <li>1. Any industrial use;</li> <li>2. Any commercial use;</li> <li>3. Any institutional use;</li> <li>4. Two-Family Structures and Three-Family Structures; and</li> <li>5. A multi-family residential development.</li> </ol>	<b>Complies.</b>	
<p><b>Site Plan Standards:</b> The entire parcel area shall be built upon, landscaped, or paved in accordance with the zone's open space and parking requirements.</p>	<b>Complies.</b>	
<p><b>Piping of Irrigation Ditches:</b> All existing irrigation canals and ditches which are located on the site or straddle a site property line shall be piped with a sufficient size pipe and shall be approved by the City Engineer.</p>	<b>Shall Comply.</b>	<i>Shall meet City Engineering Standards</i>
<p><b>Preliminary Plat With Attached Units:</b> When the proposed Site Plan includes attached units, the Site Plan submittal shall include a preliminary subdivision plat. Said plat shall include an ALTA survey of the property recorded with the Utah County Surveyor pursuant to the Utah Code, the proposed building locations, proposed floor plans and proposed elevations identifying each building in the development. Approval of the proposed Site Plan may occur simultaneously with the approval of the proposed preliminary plat.</p>	<b>N/A.</b>	<i>Not applicable</i>
<p><b>Nuisances:</b> All commercial uses shall be free from objectionable odors, noises, hazards, or other nuisances.</p>	<b>Shall Comply.</b>	<i>Applicant shall comply.</i>
<p><b>Residential Conversions:</b> No existing residential dwelling or residential lot in any commercial or residential zone may be used or converted into a commercial use unless all of the standards set forth herein are met, including parking regulations, setbacks, landscaping, and architectural design.</p>	<b>N/A.</b>	<i>Not applicable</i>
<p><b>Ownership Affidavit:</b> A statement of ownership and control of the subject property and a statement describing the nature of the intended use.</p>	<b>Complies.</b>	<i>Shown in file.</i>
<p><b>Vicinity Map:</b> A general location map indicating the approximate location of the subject parcel.</p>	<b>Complies.</b>	<i>Shown in file.</i>
<p><b>Context plan:</b> A context plan shall include the existing features within 200 feet of the proposed Site Plan property line. Existing features include, but are not limited to, buildings, ingress and egress points, landscaping areas, pedestrian paths, and property names.</p>	<b>Complies.</b>	<i>Shown in file.</i>
<p><b>Site Analysis:</b> A site analysis is a plan view drawing demonstrating land constraints and existing features. Existing features may consist of the presence of boulders, existing man-made features, significant trees, canals or ditches, access points or public rights-of-way, and existing conditions within 200 feet of the property line.</p>	<b>Complies.</b>	<i>Shown in file.</i>
<p><b>Survey:</b> A survey prepared and stamped by a Utah registered land surveyor listing the metes and bounds legal description and the gross acreage within the subject parcel.</p>	<b>Complies.</b>	<i>Shown in file</i>

<p><b>Compliance statement:</b> A statement indicating how the proposed development complies with the City’s adopted Land Use Element of the General Plan.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file.</i></p>
<p>Final Construction Drawings containing, at a minimum, all items specified in the City’s “Standard Technical Specification and Drawings” manual. Applicant shall provide three full-size 24” x 36” copies and five 11 x 17 inch reductions as required on the application form, along with digital copies as outlined below.</p> <p>Additional copies may be required prior to adding the application to the Planning Commission agenda. Final Construction Drawings for a Site Plan is hereby required and shall be prepared and stamped by licensed or certified professionals including architects, landscape architects, land planners, engineers, surveyors, transportation engineers, or other professionals deemed necessary by the Planning Director. The City may require plans prepared by any or all of the above-noted professionals. A Site Plan application shall also contain the following :</p> <ul style="list-style-type: none"> <li>i. locations, dimensions, floor plans, uses and heights of all proposed buildings and structures, including overhangs, porches, stairwells, and balconies, and the locations of all structures on adjoining properties;</li> <li>ii. access points, provisions for vehicular and pedestrian circulation on and off site, interconnection to adjacent sites, dimensions of such access and circulation, and pedestrian paths within 200 feet of the property boundary;</li> <li>iii. acceleration and deceleration lanes, and dimensions thereof, if required;</li> <li>iv. off-street parking and loading areas complying with the City’s off-street parking requirements contained in Chapter 19.09 of this Title;</li> <li>v. proposed outdoor display areas;</li> <li>vi. screening and buffering provisions, including types and heights of existing and proposed buffering and fencing elements;</li> <li>vii. location and treatment of refuse collection areas, storage areas, mechanical equipment, and external structures;</li> <li>viii. location, type, and size of all business and on-site circulation signage;</li> <li>ix. tabulation of square footage devoted to various land uses, ground coverage by structures, and other impervious surfaces;</li> <li>x. type of construction of all structures, presence or absence of fire sprinkling, and location of existing and proposed fire hydrants;</li> <li>xi. Established Grade of building area.</li> </ul>	<p><b>Shall Comply.</b></p>	<p><i>Applicant shall comply.</i></p>
<p><b>Final Hydraulic and Hydrological storm drainage report and calculations.</b> Location of all existing and proposed secondary irrigation systems, both on site and on adjacent properties, including ditches, pipes, and culverts;</p>	<p><b>Shall Comply.</b></p>	<p><i>Needs to be approved by City Engineer – Scott Petrik.</i></p>
<p><b>Final Traffic report:</b> Said report shall comply with the standards outlined in the City’s adopted Transportation Master Plan and shall include, but not be limited to, the following:</p> <ul style="list-style-type: none"> <li>i. an analysis of the average daily trips generated by the proposed project;</li> <li>ii. an analysis of the distribution of trips on City street systems;</li> <li>iii. a description of the type of traffic generated; and</li> </ul>	<p><b>Shall Comply.</b></p>	<p><i>Needs to be approved by City Engineer – Scott Petrik.</i></p>

<p>iv. recommendations on what mitigation measures should be implemented with the project to maintain a level of service for existing and proposed residents acceptable to the City.</p>		
<p><b>Data table including:</b></p> <ul style="list-style-type: none"> <li>i. total project area</li> <li>ii. total number of lots, dwellings, and buildings</li> <li>iii. square footage of proposed building footprints and, if multiple stories, square footage by floor</li> <li>iv. number of proposed garage parking spaces</li> <li>v. number of proposed surface parking spaces</li> <li>vi. percentage of buildable land</li> <li>vii. acreage of sensitive lands and what percent sensitive lands comprise of total project area and of open space area</li> <li>viii. area and percentage of open space or landscaping</li> <li>ix. area to be dedicated as right-of-way (public and private)</li> <li>x. net density of dwellings by acre (sensitive lands must be subtracted from base acreage).</li> <li>xi. number of off-street parking spaces (e.g., number of proposed garage parking spaces, number of proposed surface parking spaces, etc.)</li> </ul>	<p><b>Complies.</b></p>	<p><i>Shown in file.</i></p>
<p><b>Landscaping Plan:</b> A landscaping plan, prepared and stamped by a licensed landscape architect, indicating the location, spacing, types, and sizes of landscaping elements and existing trees, and showing compliance with the City’s off-street parking requirements, the City’s design guidelines and policies, and the requirements of the appropriate zone.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file.</i></p>
<p><b>Lighting Plan:</b> A lighting plan indicating the illumination of all interior areas and immediately adjoining streets showing the location, candle power, and type of lighting proposed, and in conformance with the City’s lighting standards. An individual photometric plan is also required.</p>	<p><b>N/A.</b></p>	<p><i>Not applicable to this application.</i></p>
<p><b>Elevations:</b> The elevations of all proposed buildings, fences, and other structures viewed from all sides indicating height of structures, the average finished grade of the site at the foundation area of all structures, percentage of building materials proposed, and color of all materials. A board showing building colors and materials is required.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p><b>Signage Plan:</b> An overall signage plan shall be approved during the Site Plan approval process. All information to be provided for the sign approval shall be submitted concurrent with Site Plan application materials, consistent with the requirements in Section 19.18.</p>	<p><b>Shall Comply.</b></p>	<p><i>Applicant shall comply.</i></p>
<p><b>Fee:</b> A fee set by resolution of the City Council shall accompany the application for any Site Plan review.</p>	<p><b>Shall Comply.</b></p>	<p><i>Application shall comply</i></p>
<p><b>Considerations Relating to Traffic Safety and Traffic Congestion:</b></p> <ul style="list-style-type: none"> <li>i. the effect of the site development plan on traffic conditions on adjacent street systems;</li> <li>ii. the layout of site with respect to location and dimensions of vehicular and pedestrian entrances, exits, driveways, and walkways;</li> <li>iii. the arrangement and adequacy of off-street parking facilities to prevent traffic congestion and compliance with the provisions of Chapter 19.09, off-street parking requirements;</li> <li>iv. the location, arrangement, and dimensions of truck loading and unloading facilities;</li> <li>v. the circulation patterns within the boundaries of the development; and</li> <li>vi. the surfacing and lighting of off-street parking facilities.</li> </ul>	<p><b>Shall Comply.</b></p>	<p><i>Applicant shall comply with Engineering Standards regarding traffic.</i></p>

<b>Considerations Relating to Outdoor Advertising:</b> Outdoor advertising shall comply with the provisions of Chapter 19.18.	<b>Shall Comply.</b>	<i>Applicant shall comply</i>
<b>Consideration Relating to Landscaping:</b> i. the location, height, and materials of walls, fences, hedges, and screen plantings to ensure harmony with adjacent development, to provide buffer areas, or to conceal storage areas, utility installations, or other unsightly development; ii. the requirements of Chapter 19.06; iii. the planting of ground cover or other surfaces to prevent dust and erosion; and iv. the unnecessary destruction of existing healthy trees.	<b>Does Not Comply.</b>	<i>See comments on Landscaping Plans.</i>
<b>Considerations Relating to Buildings and Site Layout:</b> i. the general silhouette and mass, including location on the site, elevations, and relation to natural plan coverage, all in relationship to the character of the neighborhood; ii. the exterior design in relation to adjoining structures in height, bulk, and area openings, breaks in facade facing the street, line and pitch of roofs, and the arrangement of structures on the parcel; iii. compliance with the City's Architectural design standards.	<b>Shall Comply.</b>	<i>Applicant shall comply.</i>
The proposed project shall comply with the City's adopted Land Use Element of the General Plan, Land Use Ordinance, land development regulations, architectural guidelines, and all other adopted ordinances, regulations, policies, and standards.	<b>Shall Comply.</b>	<i>Applicant shall comply.</i>
<b>Trails Master Plan:</b> Shows required trails	<b>Complies.</b>	<i>Shown in file</i>

## 19.18 Signs

Regulation	Compliance	Findings
Site Plans require signage information (if tenant known, show location potential location size)	N/A.	<i>None proposed</i>
Monument sign location meets code and sign details provided	N/A.	<i>None proposed</i>
Wall signs and potential locations and details	N/A.	<i>None proposed</i>

## 19.27 Addressing and Street Naming

### Double Check all Addresses after GIS Assigns Them

Regulation	Compliance	Findings
<b>WebApp to Review Addresses assigned by GIS:</b> <a href="https://ssgis.maps.arcgis.com/apps/webappviewer/index.html?id=b4786008c6dd420ab06445cd8d5e5fe0">https://ssgis.maps.arcgis.com/apps/webappviewer/index.html?id=b4786008c6dd420ab06445cd8d5e5fe0</a>		
<b>Lot Numbers:</b> Are they correct and sequential. Lot numbers shall not be duplicated in separate phases/plats in the same development. i.e. Phase 1 and Phase 2 shall not have Lot 101.	N/A.	<i>Not Applicable</i>
<b>Address Numbers:</b> is it accurate and sequential and does it fit between the intersection coordinates	N/A.	<i>Not Applicable</i>
<b>Compare Surrounding Addresses:</b> do the addresses work with surrounding addresses? Are there any repetitions in address numbers? Do the street names match and line up with streets in other plats and across intersections?	N/A.	<i>Not Applicable</i>
<b>Double Access Lots and Corner Lots.</b> These types of lots should have an address on each road frontage, unless front door access is not allowed. If a front door is allowed, but not a driveway, this should be clear on the plat.	N/A.	<i>Not Applicable</i>

<b>No Access on Double Frontage of Lots:</b> The plat should state what this means. No driveway and no front door, or just no driveway	N/A.	<i>Not Applicable</i>
<b>Alternate Addresses:</b> are they in the right location on the lot and do they make sense concerning lots that have no access. What does “no access mean”?	N/A.	<i>Not Applicable</i>
<b>Rear Load:</b> Meet with Brian to review every time there is a rear-load product.	N/A.	<i>Not Applicable</i>
<b>Rear Load:</b> label “FRONT” to identify which side is the front, but check grading to see if the units can be accessed from the front.	N/A.	<i>Not Applicable</i>
<b>Grading/Slope:</b> Does the grade in front of rear-load product allow for front door access for EMT and deliveries?	N/A.	<i>Not Applicable</i>
<b>New Address:</b> street names shall be proposed by the developer and approved by City staff. Street coordinates and house numbers shall be approved by City staff and placed on the final subdivision plat by the developer before plat recordation.	N/A.	<i>Not Applicable</i>
<b>Duplicate Street Names:</b> No duplication of street names or numbers shall be used. No phonetic duplications shall be used (same name, spelled differently). If a street is terminated by unimproved land and continues on the same bearing beyond the unimproved land, it should continue with the same name.	N/A.	<i>Not Applicable</i>
<b>Continuity.</b> Proposed street names are encouraged to have the following characteristics: Historic significance, local sense of places, overall theme, compatibility with adjacent streets	N/A.	<i>Not Applicable</i>
<b>Compass Directions.</b> The four compass directions shall not be used as part of the street name.	N/A.	<i>Not Applicable</i>
<b>Name Length.</b> Street names shall not be longer than thirteen letters, including spaces between letters. (this does not include the street type designator)	N/A.	<i>Not Applicable</i>
Street Type Designations. Proposed street names and types shall be matched as follows: a. <b>Boulevard (Blvd), Parkway (Pkwy):</b> This name may only be used for arterials and collectors.. b. <b>Drive (Dr), Way (Wy):</b> Meandering, curvilinear, or diagonal street usually longer than 1000 feet. c. <b>Road (Rd):</b> Limited streets that may run in a direction and are most always longer than 1000 feet. d. <b>Streets (St), Avenues (Ave):</b> Straight directional streets. e. <b>Lanes (Ln), Row:</b> Short collector or minor streets which are usually less than 1000 feet in length. May be used in names for private rights-of way. f. <b>Circle (Cir), Court (Ct), Place (Pl), Cove (Cv):</b> Permanent dead end streets or cul-de-sac streets usually less than 600 feet in length and containing three or more lots or separate dwelling structures. g. <b>Center (Cntr) and Mall:</b> Designations reserved for high density commercial developments with multiple structures and occupancies that can be substituted for the street type designator in an assigned address.	N/A.	<i>Not Applicable</i>
<b>Street and Property Numbering.</b> Each street, lot, unit, dwellings, or parcel in the site plan or plat shall be addressed according to the following standards:	N/A.	<i>Not Applicable</i>
1. All street intersections shall be addressed numerically. Street intersection numbers shall end with a “0” or a “5” and with “North,” “South,” “East,” or “West”.	N/A.	<i>Not Applicable</i>

2. Where possible, property addresses shall be odd on the south and west sides of a street.	N/A.	<i>Not Applicable</i>
3. Where possible, property addresses shall be even on the north and east sides of a street.	N/A.	<i>Not Applicable</i>
4. Property addresses for IADUs (see Chapter 19.20), ADUs, stacked multi-family, dwelling above commercial, commercial/retail, or office building that have multiple users shall be identified by a single address, and the individual units shall have a unique identifier such as suite, apartment, dwelling, or unit number.	N/A.	<i>Not Applicable</i>
5. Dual addresses on corner lots will be assigned at plat recordation. At Building Permit issuance, one address shall be permanently assigned.	N/A.	<i>Not Applicable</i>
6. Buildings that are hidden behind other buildings or do not have public frontage, shall be numbered from the centerline of the principal access or driveway.	N/A.	<i>Not Applicable</i>
7. City Staff shall assign numbers to site plans before final site plan approval, based on the above standards.	N/A.	<i>Not Applicable</i>
8. If a unit faces a street, it shall be numbered on that street. If a unit faces open space, it shall be numbered to the alley way.	N/A.	<i>Not Applicable</i>
<b>Building Identification.</b> All buildings shall have approved address numbers, and such numbers shall be identified using the following standards:	N/A.	<i>Not Applicable</i>
1. <b>Background.</b> Numbers shall be set on a background of a contrasting color.	N/A.	<i>Not Applicable</i>
2. <b>Size.</b> Residential building numbers shall be at least four inch tall block letters. All building numbers shall be of sufficient size so as to be legible from the street or private road. Non-residential buildings shall use Western Arabic numbers or alphabetic letters that are a minimum four inches high with a minimum stroke width of 0.5 inch, installed on a contrasting background and be plainly visible from the street or road fronting the property.	N/A.	<i>Not Applicable</i>
3. <b>Visibility.</b> Numbers shall be placed in a position that is plainly legible and visible from the street or road fronting the property. When a building is at a distance greater than the required setback from a street or when view of the building is blocked by trees or shrubs, numbers shall be displayed on a sign attached to a fence, gate, street mailbox, or lawn stake, in addition to being placed on the building. a. Stacked multi-family structures, condominiums, commercial, office, or institutional building numbers shall be placed in a prominent location at the main entrance of such buildings. b. Interior suites, apartments, units, or offices shall be clearly identified in a logical numeric or alphabetic sequence. c. Attached side-by-side town homes, commercial, office, or institutional buildings adjacent to a public or private street shall have individual addresses for each unit building numbers or letter with associated unit numbers. Developments with buildings that do not have direct access to a public or private street shall use one address from the public or private street, and each unit shall have a number or letter, such as 1, 2, 3 or A, B, C and so on. with building numbers or letters and associated unit numbers. d. Stacked multi-family structures, commercial, office, or institutional buildings shall have one address facing a public or private street and each unit shall have a number or letter such as	N/A.	<i>Not Applicable</i>

<p>1, 2, d, or A, B, C and so on. with an individual building number or letter with associated unit numbers.</p> <p>e. Townhomes, three-family dwellings, two-family dwellings, commercial, office, or institutional buildings that have a front door that faces a public or private road and a driveway or parking access from a rear alley shall number only attach address numbers to the front of each unit and not the rear.</p> <p>f. Townhomes, three-family dwellings, two-family dwellings, commercial, office, or institutional buildings that have a front door that faces open space or another building and have a driveway or parking access from a rear alley shall number both the front and back of each unit. The applicant shall verify that address numbers are not duplicated in units that are across the green space or across the alley.</p>		
--	--	--

<b>Fiscal Impact</b>	
<b>Regulation</b>	<b>Findings</b>
Is there any City maintained open space?	<b>No</b>
What is the anticipated cost to the City?	<b>N/A</b>
When will City maintenance begin?	<b>N/A</b>





# MMI - ILA SHELTER SLC - 3.1 SARATOGA SPRINGS, UT



**PROJECT**

MMI - ILA SHELTER  
SLC3.1  
SARATOGA SPRINGS,  
UT

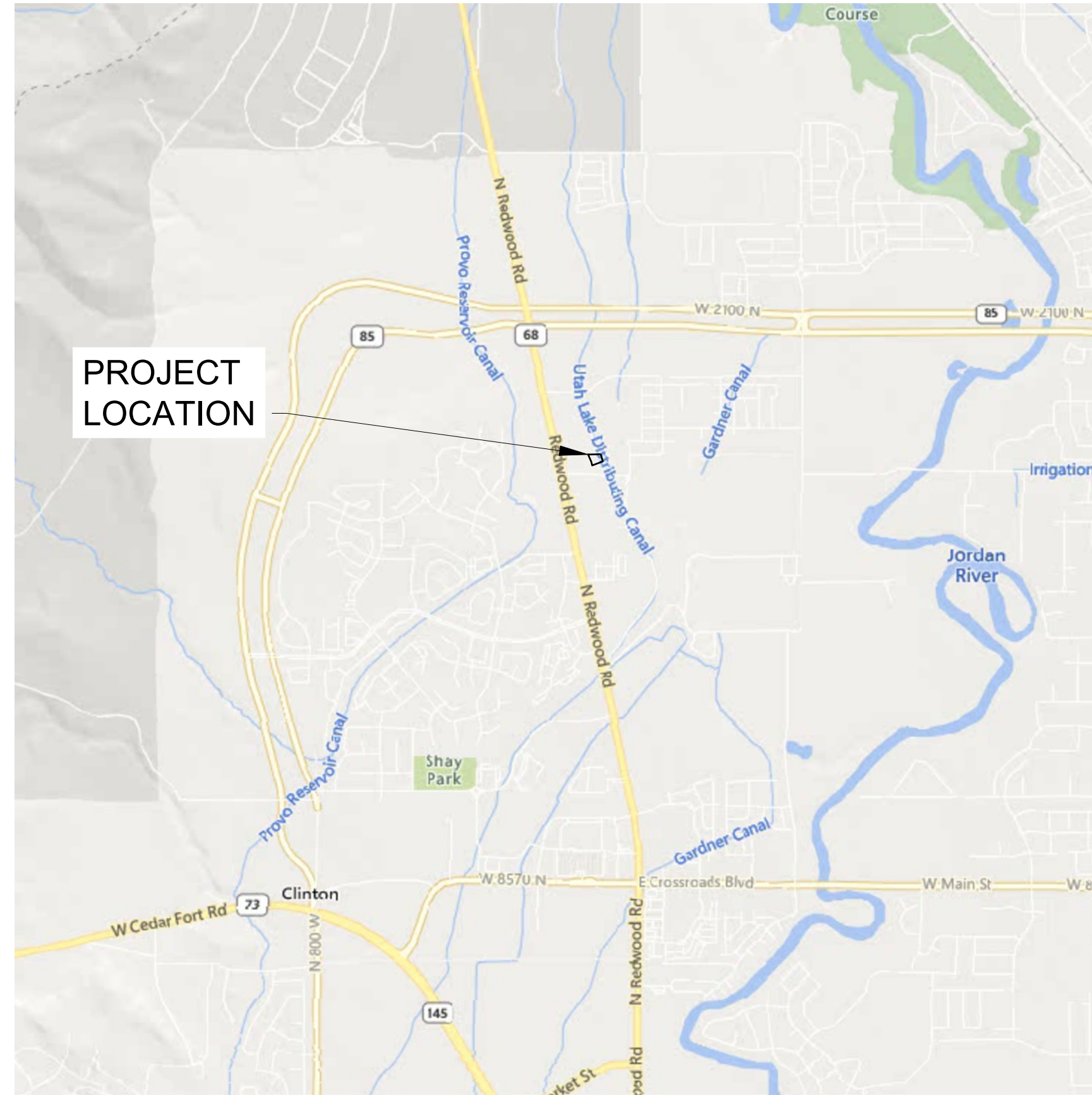
PARCEL ID: 68-054-0012  
2438 N Stagecoach Dr  
Saratoga Springs, UT 84045

**CLIENT**

Middle Mile Infrastructure  
14080 Blue Wing Lane  
Parker, CO 80134  
(605) 505-4018

**CONSULTANT**

AECOM Technical Services of South Carolina, Inc.  
10 Patewood Suite 500  
Greenville, SC 29615  
Telephone 1-864-234-3069  
www.aecom.com



**VICINITY MAP**  
SCALE: 1" = 2000'



**AERIAL MAP**  
SCALE: 1" = 100'

Sheet List Table	
Sheet Number	Sheet Title
1	COVER SHEET
2	GENERAL NOTES
3	EXISTING CONDITIONS AND DEMOLITION PLAN
4	GRADING PLAN
5	GRADING POINTS
6	PROPOSED PURCHASE EXHIBIT
7	SITE LAYOUT
8	PROFILES
9	SITE DETAILS-1
10	SITE DETAILS-2
11	SITE DETAILS-3
12	SITE DETAILS-4
13	SITE DETAILS-5
14	SITE DETAILS-6
15	STRUCTURAL DETAILS-1
16	STRUCTURAL DETAILS-2
17	STRUCTURAL DETAILS-3
18	LANDSCAPE GENERAL NOTES AND SCHEDULE
19	LANDSCAPE PLAN
20	LANDSCAPE DETAILS-1
21	LANDSCAPE DETAILS-2
22	IRRIGATION GENERAL NOTES AND LEGEND
23	IRRIGATION PLAN
24	IRRIGATION DETAILS-1
25	IRRIGATION DETAILS-2
26	ELECTRICAL GENERAL NOTES, SYMBOL LEGENDS AND ABBREVIATIONS
27	ELECTRICAL SITE PLAN
28	ELECTRICAL SITE LIGHTING PLAN
29	ELECTRICAL SITE GROUNDING PLAN
30	ELECTRICAL SITE CONDUIT TRACING PLAN
31	ELECTRICAL SPECIFICATIONS
32	ELECTRICAL ONE-LINE DIAGRAM
33	SITE PHOTOMETRIC PLAN
34	UNDERGROUND UTILITY PLAN

PROJECT INFORMATION	
TOTAL PROJECT AREA	0.69 AC (30,056 SF) (100%)
BUILDABLE LAND %	0.36 AC (15,682 SF) (52%)
TOTAL DISTURBED AREA	0.43 AC (18,731 SF) (62%)
OPEN SPACE AREA	0.26 AC (11,326 SF) (38%)
PROJECT DEDICATED R/W	0.00 AC (0 SF) (0%)
SENSITIVE LAND % (GREATER THAN 30% SLOPE, LOCATED WEST AND NORTH OF GRAVEL LOT)	20.2%
# OF LOTS, DWELLINGS, AND BUILDINGS	1
SQUARE FOOTAGE OF PROPOSED BUILDING	852 SF
PROPOSED PARKING SPACES	1
REQUIRED/PROPOSED ADA SPACES	0 / 0
NET DENSITY OF DWELLING BY ACRE	0



**ISSUE/REVISION**

I/R	DATE	DESCRIPTION



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**ISSUE FOR BID**  
DATE OF ISSUE: FEBRUARY 2, 2026

**PROJECT NUMBER**

60713679

**SHEET TITLE**

COVER SHEET

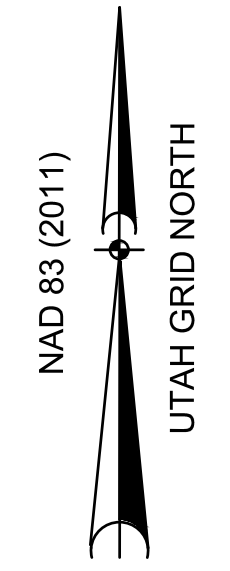
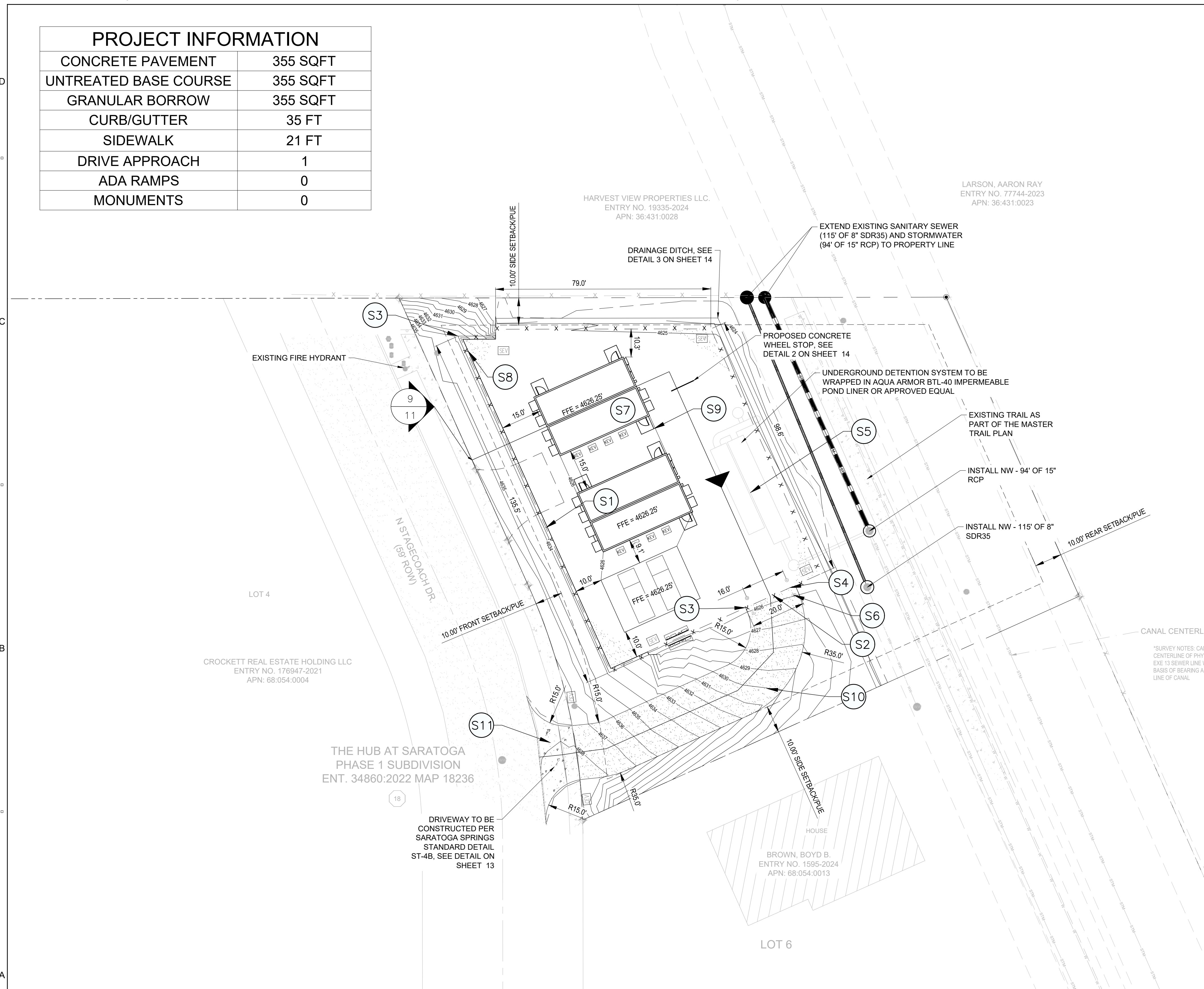
**SHEET NUMBER**

1

ANS I D 22" x 34" Approved: \_\_\_\_\_ Checked: \_\_\_\_\_ Designer: \_\_\_\_\_ Project Management Initials: \_\_\_\_\_  
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ANS I D 22" x 34" Approved: \_\_\_\_\_ Checked: \_\_\_\_\_ Designer: \_\_\_\_\_ Project Management Initials: \_\_\_\_\_  
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PROJECT INFORMATION	
CONCRETE PAVEMENT	355 SQFT
UNTREATED BASE COURSE	355 SQFT
GRANULAR BORROW	355 SQFT
CURB/GUTTER	35 FT
SIDEWALK	21 FT
DRIVE APPROACH	1
ADA RAMPS	0
MONUMENTS	0



LEGEND	
---	WORK LIMIT LINE
- - - -	PROPERTY LINE
---	RIGHT OF WAY LINE
---	PROPOSED EASEMENT LINE
---	LANDSCAPE BUFFER
---	EDGE OF GRAVEL
---	SETBACK LINE
U-FOC	PROPOSED FIBER
x - x	FENCE LINE
[Pattern]	GRAVEL
[Pattern]	CONCRETE
[Pattern]	GRADING LINE

**SITE NOTES**

- DIMENSIONS AND RADII ARE TO EDGE OF PAVEMENT, CONCRETE & GRAVEL PAD UNLESS SHOWN OTHERWISE.
- ALL NEW PAVEMENT ABUTTING EXISTING PAVEMENTS SHALL MATCH THE ELEVATION OF THE EXISTING.
- SEE GENERAL NOTES, SHEET 2.
- SITE DETAILS, SEE SHEET 9-14.
- PROPOSED EASEMENT EXHIBIT, SEE SHEET 6.
- THE OPEN SPACE WILL BE MAINTAINED BY THE DEVELOPER.
- A RIGHT-OF-WAY ENCROACHMENT PERMIT MUST BE OBTAINED FROM THE CITY OF SARATOGA SPRINGS PRIOR TO DOING ANY WORK IN THE EXISTING RIGHT-OF-WAY. APPLY FOR AN ENCROACHMENT PERMIT AT [HTTPS://CITYWORKS.SARATOGASPRINGS.COM/PUBLICACCESS/TEMPLATE/LOGIN.ASPX](https://cityworks.saratogasprings.com/publicaccess/template/login.aspx)
- UNDOCUMENTED FILL ON SITE HAS BEEN OBSERVED. OVER EXCAVATION DEPTH SHALL BE 5' FOR THE ILA FOUNDATIONS AS SPECIFIED BY THE GEOTECHNICAL REPORT
- THE AREA NORTH OF THE EXISTING TRAIL IS NOT DEVELOPED DUE TO THERE BEING INSUFFICIENT ROOM TO ADD CONCRETE WITHOUT ENCROACHING ON THE ADJACENT PROPERTY.

**SITE DATA:**

SITE ADDRESS:	2438 N STAGECOACH DRIVE, SARATOGA SPRINGS, UT 84045
PIN/MAP #:	68-054-0012
LEGAL DESCRIPTION:	58029-2023
ZONING:	RC (REGIONAL COMMERCIAL)
LAND USE:	N/A
PROPERTY PURCHASE	
ACREAGE:	0.69 AC / 30,037 SF
PROPOSED DISTURBED AREA:	0.43 AC / 18,856 SF
PROPOSED LAND USE:	TELECOMMUNICATIONS SUPPORT STRUCTURE (UTILITY)
SETBACKS:	FRONT: 10' SIDE: 10' REAR: 10'

**PROJECT**  
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 SARATOGA SPRINGS, UT  
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**CONSULTANT**  
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**REGISTRATION**

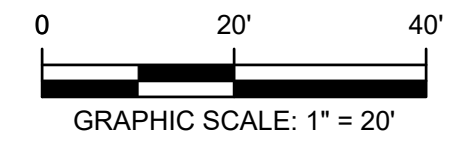
**ISSUE/REVISION**

IR	DATE	DESCRIPTION



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 DATE OF ISSUE: FEBRUARY 2, 2026



**CONSTRUCTION NOTES**

- |  |        |   |        |   |        |
|--|--------|---|--------|---|--------|
| (S1) CONSTRUCT 8' HIGH WROUGHT IRON FENCE, SEE DETAIL              | (1/9)  | (S4) CONSTRUCT KNOX BOX   | (1/16) | (S10) CONSTRUCT GRAVEL DRIVEWAY, SEE DETAIL   | (8/11) |
| (S2) CONSTRUCT MANUAL WROUGHT IRON GATE, SEE DETAIL                | (2/9)  | (S5) CONSTRUCT UNDERGROUND DETENTION SYSTEM, SEE DETAIL                           | (1/12) | (S11) CONSTRUCT CONCRETE DRIVEWAY, SEE DETAIL | (6/11) |
| (S3) CONSTRUCT RETAINING WALL, SEE RETAINING WALL STRUCTURAL PLANS | (6/10) | (S6) CONSTRUCT BOLLARDS AROUND GATE CONTROLLER AND GATE, 36IN SPACING, SEE DETAIL | (7/10) |   |        |
|  |        | (S7) CONSTRUCT CONCRETE SHELTER FOUNDATION, SEE DETAIL                            | (7/11) |   |        |
|  |        | (S8) CONSTRUCT GRAVEL PAVEMENT THROUGHOUT SITE, SEE DETAIL                        |        |   |        |
|  |        | (S9) CONCRETE APRON, SEE DETAIL   |        |   |        |

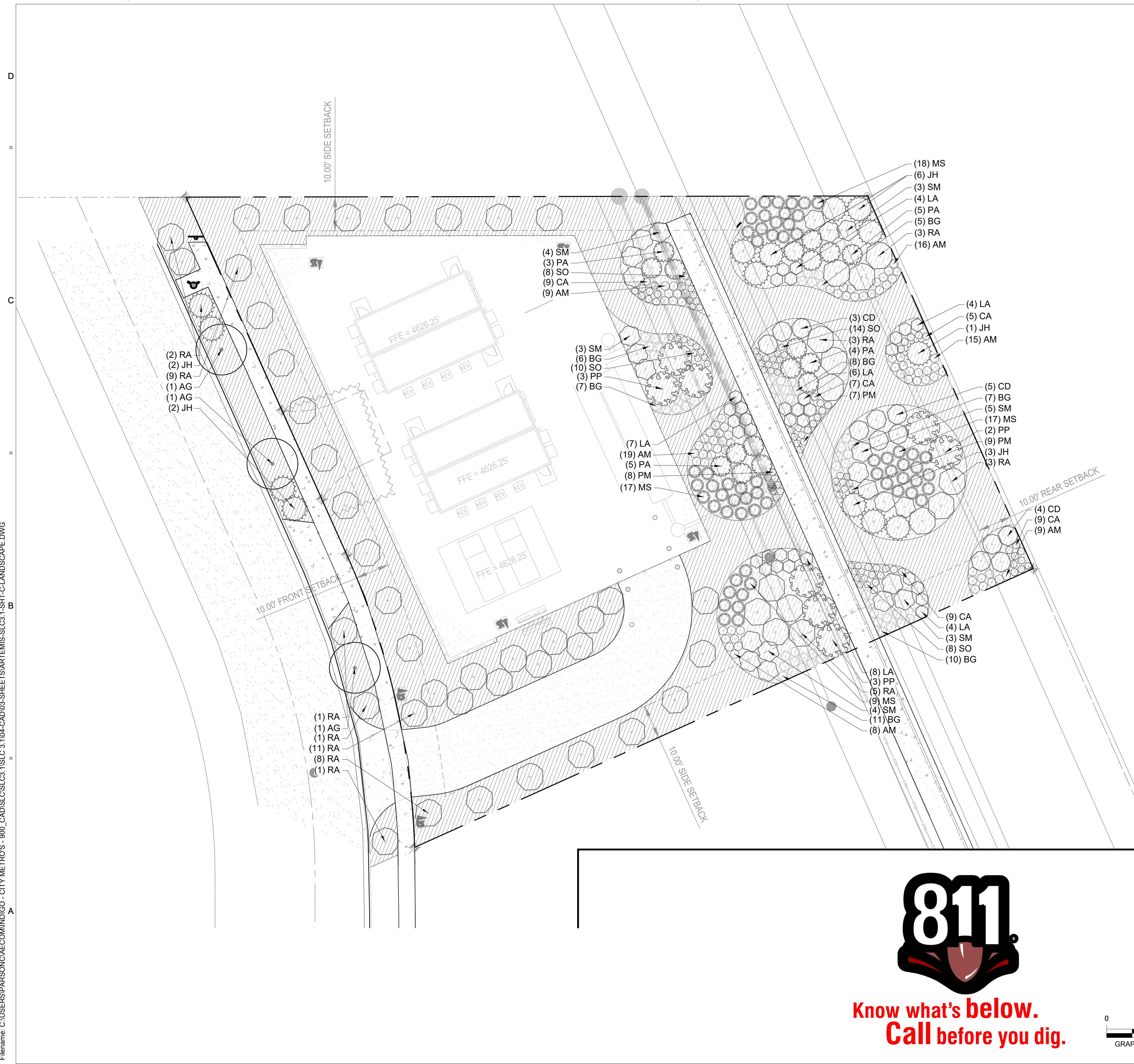
**PROJECT NUMBER**  
60713679

**SHEET TITLE**  
SITE LAYOUT

**SHEET NUMBER**  
7



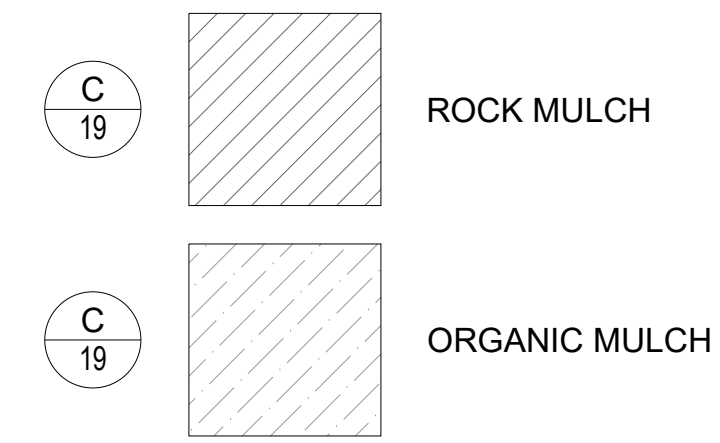
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 Project Management Initials: \_\_\_\_\_ Designer: \_\_\_\_\_ Checked: \_\_\_\_\_ Approved: \_\_\_\_\_  
 ANS I D 22" x 34"



**LANDSCAPE NOTES**

1. THE FOLLOWING LANDSCAPING REQUIREMENTS ARE BASE ON SECTION 19.06 OF THE CITY OF SARATOGA SPRINGS LAND DEVELOPMENT CODE..
2. LANDSCAPING MAINTENANCE WILL BE PROVIDED QUARTERLY AND AS NEEDED TO MAINTAIN SURVIVABILITY AND AESTHETIC VALUE.
3. MAINTENANCE OF REQUIRED LANDSCAPED AREAS IS THE RESPONSIBILITY OF THE PROPERTY OWNER. ALL SUCH AREAS SHALL BE PROPERLY MAINTAINED SO AS TO ASSURE THEIR SURVIVAL AND AESTHETIC VALUE AND SHALL BE PROVIDED WITH AN IRRIGATION SYSTEM OR A READILY AVAILABLE WATER SUPPLY. FAILURE TO MONITOR SUCH AREAS IS A VIOLATION OF THIS ORDINANCE AND MAY BE REMEDIED IN THE MANNER PRESCRIBED FOR OTHER VIOLATIONS.
4. NO SIGNIFICANT TREES WILL BE REMOVED AS PART OF THIS PROJECT

**LANDSCAPE AREA REQUIREMENTS**  
 TOTAL LANDSCAPE AREA: 16,420 SF  
 LIVE VEGETATION COVERAGE REQUIREMENT (50%): 8,210 SF  
 DESIGNED LIVE VEGETATION COVERAGE: 8,502 SF



**PLANT SCHEDULE**

SYMBOL	CODE	QTY	BOTANICAL N/A
<b>TREES</b>			
	AG	3	ACER GRANDIDENTA
		3	SUBTOTAL:
<b>SHRUBS</b>			
	AM	82	ACHILLEA MILLEFOLI
	BG	54	BOUTELOUA GRACILI
	CA	39	CALAMAGROSTIS X A
	CD	12	COTONEASTER DAM
	JH	15	JUNIPERUS HORIZON
	LA	33	LAVANDULA ANGUST
	MS	70	MISCANTHUS SINENS
	PA	17	PICEA ABIES 'PUMILA
	PP	8	PICEA PUNGENS 'GLA
	PM	24	PINIUS MUGO 'CARSTI



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 MMI - ILA SHELTER  
 SLC3.1  
 SARATOGA SPRINGS,  
 UT

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**SHEET TITLE**

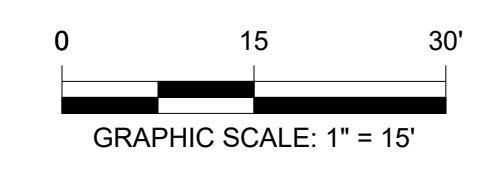
LANDSCAPE PLAN

**SHEET NUMBER**

19



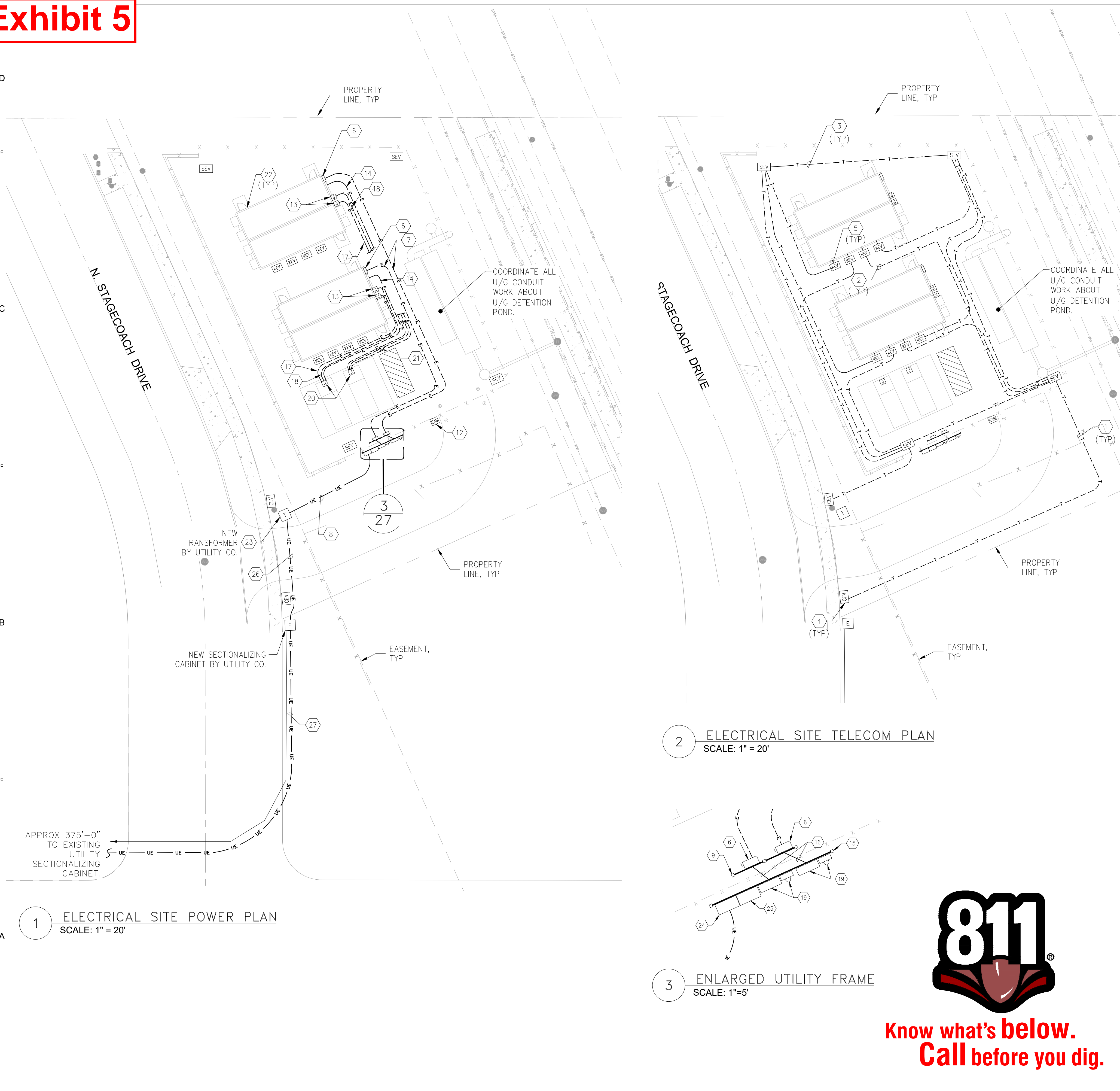
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 DATE OF ISSUE: JANUARY 15, 2026

# Exhibit 5

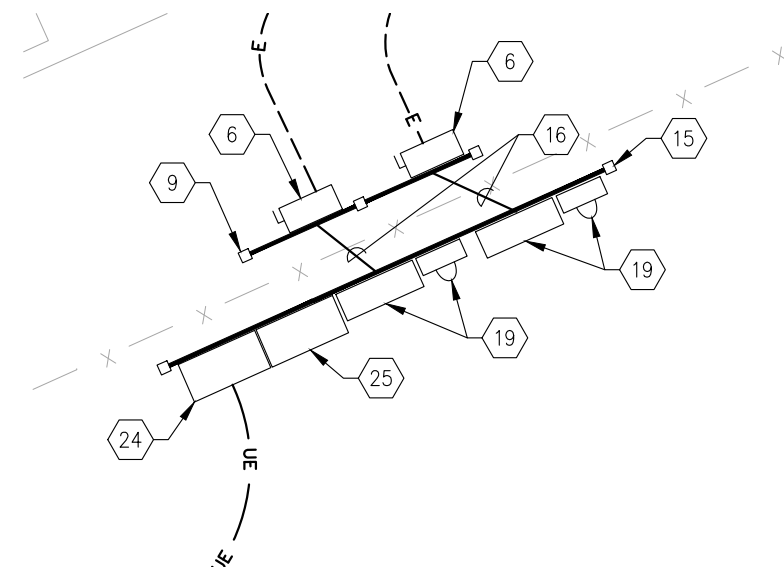
ANS  
 Approved:  
 Checked:  
 Designer:  
 Project Management Initials:  
 C  
 B  
 A  
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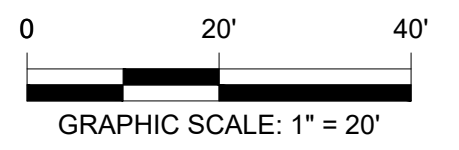
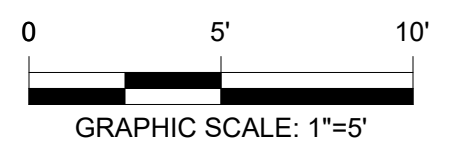
**1 ELECTRICAL SITE POWER PLAN**  
SCALE: 1" = 20'

**2 ELECTRICAL SITE TELECOM PLAN**  
SCALE: 1" = 20'

**3 ENLARGED UTILITY FRAME**  
SCALE: 1" = 5'



**Know what's below.  
Call before you dig.**



**GENERAL NOTES**

- A. REFER TO ELECTRICAL GENERAL NOTES, SYMBOLS LEGENDS, AND ABBREVIATIONS ON SHEET 26.
- B. REFER TO ELECTRICAL SPECIFICATIONS ON SHEET 31.
- C. REFER TO ELECTRICAL ONE-LINE DIAGRAM ON SHEET 32 FOR ADDITIONAL INFORMATION.
- D. REFER TO HUT DESIGN PACKAGE FOR LOW-VOLTAGE CABLING FEEDER SCHEDULES AND ADDITIONAL INFORMATION.
- E. THE GC SHALL PROVIDE ALL BACKFILL OF ELECTRICAL AND FIBER TRENCHES. ALL BACKFILL SHALL BE FREE OF PEAT, MARL, HIGHLY PLASTIC CLAY, OR OTHER UNSUITABLE MATERIALS SUCH AS TRASH, DEBRIS, BRUSH, OR ICE.
- F. ALL UNDERGROUND CONDUITS SHALL HAVE #6 SOLID, HMWPE INSULATION 0.045MIL ORANGE TRACER WIRE INSTALLED EXTERIOR TO THE CONDUIT. THIS TRACER WIRE IS USED FOR CONDUIT LOCATING.
- G. THE GC IS RESPONSIBLE FOR FURNISHING AND INSTALLING ALL SITE ENTRY VAULTS (SEV), KNOCK-OUT ENTRY VAULTS (KEV), AND INSTALLING ALL FIBER CONDUIT BETWEEN VAULTS. CHANNEL # BULKU366048 (SEV), CHANNEL # BULKU366048 (KEV); NO EXCEPTIONS.
- H. THE CUSTOMER ENTRY VAULTS (CEV) AND FIBER CONDUIT BETWEEN, ALONG WITH CEV CONNECTIONS TO, THE OSP LONG HAUL FIBER CONDUITS SHALL BE BY OTHERS.
- I. ALL FIBER VAULTS SHALL BE INSTALLED FLUSH WITH FINISHED GRADE AND 6" DEPTH OF COMPACTED AGGREGATE BASE AT THE BOTTOM OF THE VAULT. REFER TO VAULT BOX CONDUIT ENTRY DETAIL ON SHEET 28 FOR ADDITIONAL INFORMATION.
- J. VAULTS LOCATED OUTSIDE THE PERIMETER FENCE SHALL HAVE PENTA-BOLTS FOR LID LOCKING OPTION. VAULTS WITHIN THE PERIMETER FENCE SHALL NOT REQUIRE ANY SECURITY LOCKING OPTIONS.
- K. ALL FIBER CONDUITS SHALL BE INSTALLED WITH 1/2" MULETAPE AND SHALL BE CAPPED AFTER ENTERING EVERY VAULT.
- L. THE GC SHALL PROVIDE A MINIMUM OF 4" DEPTH OF BEDDING SAND ABOVE AND BELOW ALL UNDERGROUND FIBER CONDUITS.

**KEYNOTES**

- 1. FURNISH AND INSTALL (2) 4" SCHD 80 PVC EMPTY CONDUITS WITH (3) THREE-CELL INNERDUCT (MXE86383) AND PULLWIRE AT LEAST 24" BFG BETWEEN CEV AND SEV, TYPICAL. COORDINATE EXACT LOCATION OF CEV IN FIELD PRIOR TO ROUGH-IN AND INSTALLATION.
- 2. FURNISH AND INSTALL (2) 4" SCHD 80 PVC EMPTY CONDUITS WITH (3) THREE-CELL INNERDUCT (MXE86383) AND PULLWIRE AT LEAST 24" BFG BETWEEN SEV AND KEV, TYPICAL.
- 3. FURNISH AND INSTALL (2) 4" SCHD 80 PVC EMPTY CONDUITS WITH (3) THREE-CELL INNERDUCT (MXE86383) AND PULLWIRE AT LEAST 24" BFG BETWEEN SEV AND SEV, TYPICAL.
- 4. CEV AT FIBER LONG HAUL LINES BY OTHERS, REFER TO GENERAL NOTES ON THIS SHEET.
- 5. FURNISH AND INSTALL (4) 4" SCHD 80 PVC EMPTY CONDUITS WITH PULLWIRE AT 24" BFG BETWEEN KEV AND HUT SHELTER CAST-IN-PLACE CONDUIT OPENINGS, TYPICAL. COORDINATE EXACT OPENING LOCATIONS WITH HUT SHELTER CIVIL AND FLOOR PLANS.
- 6. 600A 480/277V, 3PH ELECTRIC UTILITY SERVICE DISCONNECT SWITCH WITH CLASS RK5 FUSES AND CLASS R<sup>1</sup> REJECTION CLIPS, IN NEMA 3R ENCLOSURE. GC IS RESPONSIBLE FOR COORDINATING MOUNTING STRUCTURE REQUIREMENTS WITH THE LOCAL UTILITY AND PROVIDE ACCORDINGLY. REFER TO ELECTRICAL SPECIFICATIONS ON SHEET 31 FOR ADDITIONAL INFORMATION.
- 7. FURNISH AND INSTALL (2) 4" SCHD 80 PVC CONDUITS AT 36" BFG FOR ELECTRICAL SERVICE CONDUCTORS TO HUT SHELTER DISCONNECT SWITCH. REFER TO ELECTRICAL ONE-LINE DIAGRAM ON SHEET 32 FOR CONDUCTOR SIZES.
- 8. FURNISH AND INSTALL (5) 4" SCHD 80 PVC CONDUITS WITH PULLWIRE AT 36" BFG FOR ELECTRICAL UTILITY CONDUCTORS. STUB INTO THE TRANSFORMER SECONDARY WINDOW, CAP AND MARK FOR FUTURE IDENTIFICATION. COORDINATE EXACT TERMINATION LOCATION AND REQUIREMENTS WITH UTILITY CO. PRIOR TO ROUGH-IN AND INSTALLATION, AND PROVIDE ACCORDINGLY.
- 9. FURNISH AND INSTALL 3-POST ELECTRICAL H-FRAME FOR RACK MOUNTED SERVICE DISCONNECT SWITCH. H-FRAME SHALL BE GALVANIZED STEEL POSTS WITH U-CHANNEL SUPPORTS. REFER TO DETAIL 2 ON SHEET 34 FOR ADDITIONAL INFORMATION.
- 10. NOT USED.
- 11. NOT USED.
- 12. EMERGENCY ACCESS BOX (EAB) WITH GATE CODE FOR EMERGENCY CONTROL OF ENTRY GATE BY THE FIRE DEPARTMENT. GC IS RESPONSIBLE FOR REGISTERING THE EAB, AND COORDINATING ALL REQUIREMENTS FOR EAB WITH THE FIRE MARSHAL AND PROVIDE ACCORDINGLY. REFER TO GENERAL EMERGENCY ACCESS BOX NOTES ON SHEET 26 FOR ADDITIONAL REQUIREMENTS AND INFORMATION.
- 13. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING LOOSE J-BOXES SHIPPED WITH HUT SHELTER FOR CONDUIT TERMINATIONS: (3) 16x4 J-BOX FOR POWER, AND (1) 10x10x5 J-BOX FOR LOW-VOLTAGE CONDUITS. COORDINATE EXACT MH FOR SURFACE MOUNTED J-BOXES PRIOR TO ROUGH-IN. CONDUITS SHALL BE INSTALLED WITH WEATHERPROOF FITTINGS. WALL PENETRATIONS SHALL BE SEALED WITH DUCT SEAL. SPRAY FOAM IS NOT PERMITTED FOR CONDUIT SEALING.
- 14. FURNISH AND INSTALL (2) 4" SCHD 80 PVC CONDUITS AT 36" BFG FOR ELECTRICAL SERVICE CONDUCTORS TO HUT SHELTER GENERATOR DOCKING STATION. REFER TO ELECTRICAL ONE-LINE DIAGRAM ON SHEET 32 FOR CONDUCTOR SIZES.
- 15. FURNISH AND INSTALL 3-POST ELECTRICAL H-FRAME FOR RACK MOUNTED ELECTRIC UTILITY METER / CT CABINET. H-FRAME SHALL BE GALVANIZED STEEL POSTS WITH U-CHANNEL SUPPORTS. REFER TO DETAIL 1 ON SHEET 34 FOR ADDITIONAL INFORMATION.
- 16. FURNISH AND INSTALL (2) 4" SCHD 80 PVC EMPTY CONDUIT WITH PULLWIRE AT 36" BFG BETWEEN UTILITY CT CABINET AND SERVICE DISCONNECT. COORDINATE WITH UTILITY CO. FOR TERMINATIONS.
- 17. FURNISH AND INSTALL (1) 1" SCHD 80 PVC CONDUITS AT 36" BFG FOR GENERATOR AUX LOAD CONDUCTORS TO HUT SHELTER 120V PANELBOARD. REFER TO ELECTRICAL ONE-LINE DIAGRAM ON SHEET 32 FOR CONDUCTOR SIZES.
- 18. FURNISH AND INSTALL (6) 1" SCHD 80 PVC CONDUITS AT 36" BFG FOR ELECTRICAL CONDUCTORS FROM HUT SHELTER TERMINAL BLOCK TB-08.
- 19. 600A 480/277V, 3PH ELECTRIC UTILITY METER AND CT CABINET. PROVIDE METER ENCLOSURE, CT CABINET, AND MOUNTING STRUCTURE IN ACCORDANCE WITH LOCAL UTILITY REQUIREMENTS. ELECTRIC METER FURNISHED AND INSTALLED BY THE LOCAL UTILITY. FURNISH AND INSTALL (1) 1" RGS CONDUIT WITH PULLWIRE AT 48" AFG BETWEEN UTILITY METER AND CT CABINET. COORDINATE WITH UTILITY CO. FOR TERMINATIONS.
- 20. GENERATOR POWER / LOW-VOLTAGE J-BOX. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF GENERATOR AND TERMINATION OF ALL POWER AND CONTROL CONDUITS AND CONDUCTORS.
- 21. ALLOTTED SPACE FOR FUTURE TRAILER MOUNTED PORTABLE GENERATOR.
- 22. HUT SHELTER FURNISHED WITH EXTERIOR WALL-MOUNTED LIGHT FIXTURES AT DOORS.
- 23. PAD-MOUNTED TRANSFORMER WILL BE INSTALLED BY UTILITY CO. COORDINATE FINAL PLACEMENT OF TRANSFORMER IN FIELD WITH UTILITY CO. CONTRACTOR SHALL PROVIDE CONCRETE PADVAULT PER UTILITY CO. REQUIREMENTS.
- 24. H-FRAME MOUNTED PULLBOX. PROVIDE NEMA 3R PULLBOX SIZED IN ACCORDANCE WITH UTILITY REQUIREMENTS, MINIMUM SIZE SHALL BE 30"W x 11"D x 48"H. PROVIDE RIGID BUS FOR LOAD SIDE CONNECTIONS. COORDINATE WITH UTILITY CO. FOR TERMINATIONS.
- 25. H-FRAME MOUNTED TAP BOX. PROVIDE NEMA 3R SEALABLE TAP BOX WITH RIGID BUS CONNECTIONS ON LINE SIDE TO FEEDER WIRE CONNECTIONS ON LOAD SIDE IN ACCORDANCE WITH UTILITY REQUIREMENTS.
- 26. FURNISH AND INSTALL (1) 4" SCHD 40 PVC CONDUIT AT 36" BFG FOR UTILITY SERVICE CONDUCTORS. COORDINATE ROUTING WITH UTILITY CO. AND ALL EXISTING UNDERGROUND UTILITIES.
- 27. FURNISH AND INSTALL (1) 4" HDPE CONDUIT WITH PULLWIRE AT 36" BFG FOR UTILITY SERVICE CONDUCTORS. CONTRACTOR SHALL DIRECTIONAL BORE UNDER EXISTING ROADWAY FROM NEW SECTIONALIZING CABINET TO EXISTING SECTIONALIZING CABINET. DETERMINE EXACT LOCATION OF SECTIONALIZING CABINETS IN FIELD WITH UTILITY CO. COORDINATE ROUTING AND PLACEMENT OF PILOT AND RECEIVING PITS WITH THE UTILITY CO. AND ALL EXISTING UNDERGROUND UTILITIES PRIOR TO INSTALLATION.



**PROJECT**

MMI - ILA SHELTER  
 SLC3.1  
 SARATOGA SPRINGS,  
 UT  
 PARCEL ID: 68-054-0012  
 2438 N Stagecoach Dr  
 Saratoga Springs, UT 84045

**CLIENT**

Middle Mile Infrastructure  
 14080 Blue Wing Lane  
 Parker, CO 80134  
 (605) 505-4018

**CONSULTANT**

AECOM Technical Services of South Carolina, Inc.  
 10 Patewood Suite 500  
 Greenville, SC 29615  
 Telephone 1-864-234-3069  
 www.aecom.com

**REGISTRATION**



**ISSUE/REVISION**

I/R	DATE	DESCRIPTION

**PROJECT NUMBER**

60713679

**SHEET TITLE**

ELECTRICAL SITE PLAN

**SHEET NUMBER**

27

**ISSUE FOR BID**  
 DATE OF ISSUE: FEBRUARY 2, 2026



# MINUTES – Planning Commission

Thursday, March 12, 2026

City of Saratoga Springs City Offices

319 S. Saratoga Road, Saratoga Springs, Utah 84045

## PLANNING COMMISSION MEETING MINUTES

**CALL TO ORDER - 6:00 p.m.** by Chair Rachel Sprosty Burns.

- 5 1. **Pledge of Allegiance** - led by Commissioner Willden.
2. **Roll Call** – A quorum was present

**Present:**

10 Commission Members: Rachel Sprosty Burns, Scott A. Hill, Virginia Rae Mann, NH Rather, Chris Roman, Doug Willden.  
Staff: Sarah Carroll, Planning Director; Rulon Hopkins, Assistant City Attorney; Owen Jackson, Assistant City Manager; Gina Grandpre, Senior Planner; Austin Roy, Senior Planner; Jeff Pearson, Engineer; Wendy Wells, Deputy Recorder.  
Others: Ryan Bull, Michael Wells.

15 **Excused:** Colton Miles.

3. **Public Input** - Public input was opened by Chair Rachel Sprosty Burns. Receiving no public comment, the Public Hearing was closed by the Chair.

20 **PUBLIC HEARINGS**

1. **Beacon Pointe Village Plan 8 – Porter Haven. Located approximately at Ensign Drive and Mountain View Corridor. Ryan Bull as applicant.**

25 Senior Planner Gina Grandpre presented the item. This is a 45.9-acre development within the Beacon Pointe Community Plan in Saratoga Springs. As outlined in the Community Plan, it includes 187 residential units over 45.9 acres, yielding a density of approximately 4.1 dwelling units per acre. The development incorporates a mix of traditional single-family detached homes and attached townhomes. Approximately 2.4 acres (5.23%) is designated as open space.

30 Senior Planner Gina Grandpre noted the applicant had proposed architectural standards that would modify the requirement in Title 19.16, including reducing the number of required materials and colors on building elevations from three materials and colors to two colors and materials respectively. She advised that Staff would be recommending a condition requiring the applicant to make revisions to comply with Title 19, or to add more prescriptive language to ensure the architectural intent illustrated in the village plan booklet would be clearly implemented in the final development.

35 **Public Hearing Open** by Chair Rachel Sprosty Burns. Receiving no public comment, the Public Hearing was closed by the Chair.

40 Commissioner Willden received clarification that the developer may not want to lock builders in to specific requirements, and the applicant had requested every elevation be allowed to use only two colors and two building materials, but City code only allowed that on elevations that did not face streets or open space.

45 Senior Planner Gina Grandpre informed Commissioners that the applicant had used language that stated “may be considered” or “may count”, and she explained they had a right to ask for what they wanted, but Staff was advising stronger, more prescriptive and specific language.

50 Applicant Ryan Bull of Land Reserve in Salt Lake City joined the discussion at 6:18 pm. He shared that they had completed Village 1 and Village 2, and expected to continue with the same builder group. He said there was an architectural review committee that reviewed every plan that came in, and explained the concern with the requirement for three colors and materials was that they felt it may look like a 3-layered cake-type of structure. He thought utilizing only two colors and materials would lead to a more enhanced streetscape. He presented a few examples of elevations with two colors and materials, and explained their main reason for the request was to allow for some flexibility.

55 Commissioner Hill liked the brick and stone buildings the applicant presented, but felt there had not been a lot of those types of materials used in Village 2. He was concerned Village 8 could end up with mostly stucco or siding materials like he had seen in Village 2. He suggested that perhaps only two colors and materials could be allowed if the main material were brick or stone, but felt the City should require three colors and materials if siding or stucco were used as the main material.

60 Planning Director Sarah Carroll clarified that the issue was not with the images themselves, but with the absence of language ensuring that those specific types of designs would be constructed. She advised that the solution would be to propose language guaranteeing that the final construction would fall in line with the images presented.

65 Senior Planner Gina Grandpre offered an example of how the language could be strengthened, proposing revisions such as replacing “may” with more definitive wording and incorporating specific requirements. She suggested the addition of wording such as, “choose two of the enhanced building articulations” to provide greater clarity and accountability for the builders. She advised that the City’s intent was to ensure the completed construction looked like the styles in the applicant’s proposed plan.

70 Mr. Bull explained that when a builder came into Beacon Pointe, they participated in a product line review where their plans were submitted to the architectural review committee who then worked with them on adjusting the plans to a higher standard. He said the intent was to have some flexibility so the architectural review committee could have a diverse streetscape.

75 Commissioner Sprosty Burns was concerned that condition 5 in the Staff Report was not strong enough to be in compliance with City code.

80 Planning Director Sarah Carroll advised it would be preferable for the applicant to draft the proposed language, rather than having Staff prepare it on their behalf. She explained that this approach would allow the applicant to better reflect their intent, after which Staff could review the proposal for compliance with Title 19. She noted that Staff had previously requested this language, but it had not yet been provided. She emphasized the City’s desire to avoid vague or noncommittal wording, and suggested that a condition could be added directing Staff to work with the applicant to develop the language.

85 Commissioner Mann felt the applicant should work with city to make the language precise. She explained that she was familiar with several of the builders involved, and she believed those builders would only want to put their name on a good quality product. She didn’t have a concern about using only two colors and two materials and felt it should be looked at on a case-by-case basis.

90 Commissioner Rather received clarification that the architectural review committee was an internal committee with the developer and that there would need to be a site plan for the townhomes.

95 Senior Planner Gina Grandpre expounded briefly on some of the history regarding the construction of Village Plan 2, and expressed that there was a desire to fix some of the ambiguous language going forward. She noted that some process changes since Village 2 had been tightened up to help make things more efficient.

100 Commissioner Rather felt that flexibility was a good thing, but he thought the language in condition 5 needed to be very clear.

Commissioner Sprosty Burns received clarification about terminology used in the Staff report and the way that condition 5 had been written.

105

Commissioner Hill referenced Village 2, noting that lessons had been learned through that process. He felt it was important to apply those insights to strengthen and refine the approach for Village 8.

Mr. Bull also felt it would be good to have language that was enforceable and avoid ambiguity.

110

**Motion made by Commissioner Hill that the Planning Commission forward a positive recommendation to the City Council for Beacon Pointe Village Plan 8, with the Findings and Conditions in the Staff Report, and modifying condition 5 to say: “Prior to going to City Council for approval and prior to issuance of the approval letter, the applicant shall revise the architectural design standards to have stronger language, including required materials and colors on street- and open space-facing elevations”. Seconded by Commissioner Rather.**

115

**Yes: Rachel Sprosty Burns, Scott A. Hill, Virginia Rae Mann, NH Rather, Chris Roman, Doug Willden.**

**No: None.**

120

**Absent: Colton Miles.**

**Motion passed 6 - 0.**

2. **Amendments to Title 19 Land Development Code of the City of Saratoga Springs, Chapter 19.19 – Open Space. This amendment will allow a broader range of amenities within open space. Citywide. City-Initiated.**

125

Senior Planner Gina Grandpre presented the item. Chapter 19.19 establishes open space and amenity requirements for residential development to help ensure livable neighborhoods with usable recreational spaces. The proposed amendment updates the code by expanding the range of amenities that may be used to meet the required point system, providing more flexibility in how developments satisfy open space standards. This change is intended to better align the code with modern development types and resident needs while maintaining the City’s open space goals. Staff will propose a separate amendment in the future to update the table of amenity points in this section.

130

**Public Hearing Open** by Chair Rachel Sprosty Burns. Receiving no public comment, the Public Hearing was closed by the Chair.

135

Commissioner Hill received clarification from Senior Planner Gina Grandpre that the City would not accept a public park unless it was 5 acres or more, and that public parks and trailheads were required to provide a minimum of a 1-toilet restroom. It was also noted that it had been based on equivalent acres before and had become really restrictive.

140

Planning Director Sarah Carrol further explained that the wording had been changed from “all parks” to “public parks” because City Council was concerned that requiring restrooms in private parks could potentially add a big maintenance and access burden on an HOA.

145

Commissioner Mann received clarification that the planning director would evaluate amenities and assign a point value to them to use towards credit for the amenities.

**Motion made by Commissioner Mann that the Planning Commission forward a recommendation for approval of the proposed Code Amendment to Chapter 19.19, with the Findings and Conditions in the Staff Report. Seconded by Commissioner Willden.**

150

**Yes: Rachel Sprosty Burns, Scott A. Hill, Virginia Rae Mann, NH Rather, Chris Roman, Doug Willden.**

**No: None.**

155

**Absent: Colton Miles.**

**Motion passed 6 - 0.**

160 3. **Amendments to Title 19 Land Development Code of the City of Saratoga Springs, Chapter 19.16 – Site and Architectural Design Standards. This amendment would allow an exception for building buffer requirements at the corners of buildings and loading bays. Citywide. City-Initiated.**

165 Senior Planner Austin Roy presented the item. This is a staff-initiated code amendment related to the building buffer for commercial buildings. Currently the code requires a minimum of 5 feet between the building and the drive thru lane. However, this requirement can be difficult to meet on building corners adjacent to a drive thru lane, and does not work on a building with a loading bay. Staff is proposing to allow exceptions for these specific circumstances.

**Public Hearing Open** by Chair Rachel Sprosty Burns. Receiving no public comment, the Public Hearing was closed by the Chair.

170 Commissioner Hill received clarification regarding the installation of bollards to protect both the building and pedestrians; and would additionally allow for design flexibility.

175 **Motion made by Commissioner Willden that the Planning Commission forward a recommendation for approval of the proposed code amendments to Chapter 19.16, with the Findings and Conditions in the Staff Report. Seconded by Commissioner Mann.**

180 **Yes: Rachel Sprosty Burns, Scott A. Hill, Virginia Rae Mann, NH Rather, Chris Roman, Doug Willden.**

**No: None.**

**Absent: Colton Miles.**

**Motion passed 6 - 0.**

#### BUSINESS ITEMS

185 1. **Approval of Minutes: February 26, 2026.**

**Motion made by Commissioner Willden to approve the minutes of February 26, 2026. Seconded by Commissioner Hill.**

190 **Yes: Rachel Sprosty Burns, Scott A. Hill, Virginia Rae Mann, NH Rather, Chris Roman, Doug Willden.**

**No: None.**

**Absent: Colton Miles.**

**Motion passed 6 - 0.**

#### REPORTS

195 1. **Commission Comments.** Commissioner Hill reminded Planning Commissioners of the APA conference at Bryce Canyon on April 8-10.

200 2. **Director's Report.** – Planning Director Sarah Carroll advised of upcoming agenda items and recent City Council actions.

#### CLOSED SESSION

Possible motion to enter into closed session – No closed session was held.

#### 205 ADJOURNMENT

Meeting Adjourned Without Objection 7:07 p.m. by Chair Rachel Sprosty Burns.

210 \_\_\_\_\_  
Date of Approval

\_\_\_\_\_  
Planning Commission Chair

\_\_\_\_\_  
Deputy City Recorder



**Rezone & Concept Plan  
Lighthouse Cove  
March 26, 2026  
PUBLIC HEARING**

Applicant:	Lynsi Neve
Owner:	Perry Land Investments LLC
Location:	~ 4200 South Schooner Drive
Project Acreage:	109 acres
Current Use:	Vacant Land Investments LLC
Land Use Designation:	Low Density Residential
Parcel Zoning:	Agricultural, R1-10, and Rural Residential
Adjacent Zoning:	R1-10, R1-9, Planned Community, & Agricultural
Adjacent Uses:	Low Density Residential and Vacant Land
Type of Action:	Legislative
Land Use Authority:	City Council
Planner:	Sam Stout, Planner II

**A. Executive Summary and Request:**

Perry Investments LLC owns 109 acres of undeveloped property that falls within three different zones; R1-10, RR, and A. The property consists of six adjoining parcels of land, parcel numbers: 16-003-0025, 16-003-0041, 16-003-0048, 16-003-0050, 16-003-0051, & 16-004-0003, located at roughly 4200 South Schooner Drive, The applicant would like all of the property to be zoned R1-10. Parcels 16-003-0041 & 16-004-0003 are currently zoned Agricultural and parcels 16-003-0025 is zoned Rural Residential. The request is consistent with the Land Use Map of the General Plan which designates this area for Low Density Residential.

A Concept Plan was submitted along with the rezone request – proposing 197 lots. Red Lines were provided to the applicant addressing items that will need to comply with Land Development Code when Preliminary and Final Plat applications are submitted. Red Lines related to the Concept Plan are available upon request from the Planning Commission.

**Recommendation:**

**Staff recommends that the Planning Commission conduct a public hearing on the application, take public comment, review and discuss the proposal, and choose from the options in the Recommendation and Alternatives Section of this report.** Options include a positive recommendation with or without conditions, a negative recommendation, or continuation.

**B. Process:**

**Rezone**

Code Section 19.13.04 outlines the process for a Rezone. A public hearing is required with the Planning Commission who then make a recommendation to the City Council. The City Council makes the final decision to either approve with or without conditions, continue, or deny the request.

Section 19.17.03 outlines the process criteria for Planning Commission and City Council Review:

1. The Planning Commission shall review the petition and make its recommendations to the City Council within thirty days of the receipt of the petition.  
*Complies. The application will be reviewed by the Planning Commission and receive a recommendation prior to review by the City council. The application was received on February 3rd, 2026 and upon completion of review, March 26th, 2026 was the soonest available Planning Commission meeting.*
  
2. The Planning Commission shall recommend adoption of proposed amendments only where it finds the proposed amendment furthers the purpose of the Saratoga Springs Land Use Element of the General Plan and this Title.  
*Complies. Please see Sections D & E of this report.*
  
3. The Planning Commission shall provide the notice and hold a public hearing as required by the Utah Code and Chapter 19.13. For an application which concerns a specific parcel of property, the City shall provide the notice required by the Utah Code and Chapter 19.13 for a public hearing.  
*Complies. Please see Section C of this report.*

**Concept Plan**

Per Chapter 19.13.05.2 of the Land Development Code, “The review of a Concept Plan accompanying a rezone application also involves an informal review of the Concept Plan by the Planning Commission and City Council.” The review shall be for comment only and is non-binding.

A concept plan has been submitted with the request for a rezone and is included with this staff report. The concept plan is meant to help understand the intent of the applicant’s request and their future development plans. The attached concept plan review is non-binding and does not address all concerns or requirements of the Land Development Code. The items marked “can comply” or “does not comply” shall be incorporated into the Preliminary and Final Plat applications.

**C. Community Review:**

Public Hearing: This has been noticed as a public hearing pursuant to City and State statutes, which requires posting notice on the Utah public notice website and the City’s website and in

City Hall, and, except for code amendments, mailing notices to property owners whose land is directly affected by the request and property owners within 300 feet of the subject property at least 10 calendar days prior to the public hearing.

Public Comment: As of the date of this report, no public input has been received.

- D. General Plan:** The General Plan shows the land use as Low Density Residential within the project area.  
**Staff conclusion: Consistent.** *The requested rezone of the project area to R1-10 matches the General Plan.*

- E. Code Criteria:**  
Development Agreement is currently being reviewed by staff and will be finalized before the Rezone request is taken to the City Council.

**Land Use**

The requested rezone would change the land use regulations for the subject property as follows:

From Agricultural & Rural Residential to R1-10.

	<b>Agricultural</b>	<b>Rural Residential</b>
• Lot Minimum Size	From: 5 acres	From: 1 acre
• Interior Side Setback	From: 12’	From: 12’
• Maximum Height	From: 35’	From: 35’
• Maximum Bldg. Footprint	From: N/A	From: NA

	<b>R1-10</b>
• Lot Minimum Size	To: 10,000 sf
• Interior Side Setback	To: 8’/20’
• Maximum Height	To: 35’
• Maximum Bldg. Footprint	To: N/A

**Concept Plan**

A concept plan has been submitted with the request for a rezone and General Plan Amendment and is included with this staff report. The concept plan is meant to help understand the intent of the applicant’s request and their future development plans. The attached concept plan review is non-binding and does not address all concerns or requirements of the Land Development Code. The items marked “can comply” or “does not comply” shall be incorporated into the preliminary plat application. It’s common for the concept plan review to have a significant amount of redlines as this is a non-binding informal review and staff typically completes only one review, rather than multiple reviews at this stage of the process.

- F. Recommendation and Alternatives:**

Staff recommends that the Planning Commission conduct a public hearing, take public input, discuss the application, and choose from the following options.

**Option 1 – Positive Recommendation** “I move that the Planning Commission forward a recommendation for approval of the requested Rezone for Lighthouse Cove located at 4200 South Schooner Drive, with the Findings and Conditions in the Staff Report.”

**Findings**

1. The application is consistent with the General Plan, as articulated in Section D of the staff report, which section is incorporated by reference herein.
2. The application can comply with the criteria in the Land Development Code, as articulated in Section E of the staff report, which section is incorporated by reference herein.

**Conditions:**

1. All conditions of the City Engineer shall be met, including but not limited to those in the attached Engineering Staff Report.
2. All requirements of the Fire Chief shall be met.
3. All other Code requirements shall be met.
4. Any other conditions or changes as articulated by the Planning Commission:

\_\_\_\_\_.

**Option 2 – Continuance**

“I move to **continue** the Rezone for Lighthouse Cove **Lighthouse Cove** to another meeting on [DATE], with direction to the applicant and Staff on information and/or changes needed to render a decision, as follows:

1. \_\_\_\_\_
2. \_\_\_\_\_

**Option 3 – Negative Recommendation**

“I move that the Planning Commission forward a recommendation for denial of the requested Rezone for Lighthouse Cove located at 4200 South Schooner Drive with the Findings below:

1. The application is not consistent with the General Plan:
  - a. \_\_\_\_\_, and/or,
2. The application is not consistent with Section {XX.XX} of the Code:
  - a. \_\_\_\_\_, and/or

**G. Exhibits:**

1. City Engineer's Report
2. Current and Proposed Zoning Map
3. Planning Review Checklist
4. Concept Plan
5. Compatibility Statement from Applicant

## Staff Report

**Author:** Ken Knight, Staff Engineer  
**Subject:** Lighthouse Cove – Concept Plan  
**Date:** March 26, 2026  
**Type of Item:** Concept Plan Review



SARATOGA  
SPRINGS

### Description:

**A. Topic:** The applicant has submitted a concept plan application. Staff has reviewed the submittal and provides the following recommendations.

**B. Background:**

*Applicant:* Lynsi Neve  
*Request:* Concept Plan  
*Location:* East 4200 South Schooner Dr.  
*Acreage:* 66.71 acres - 197 lot

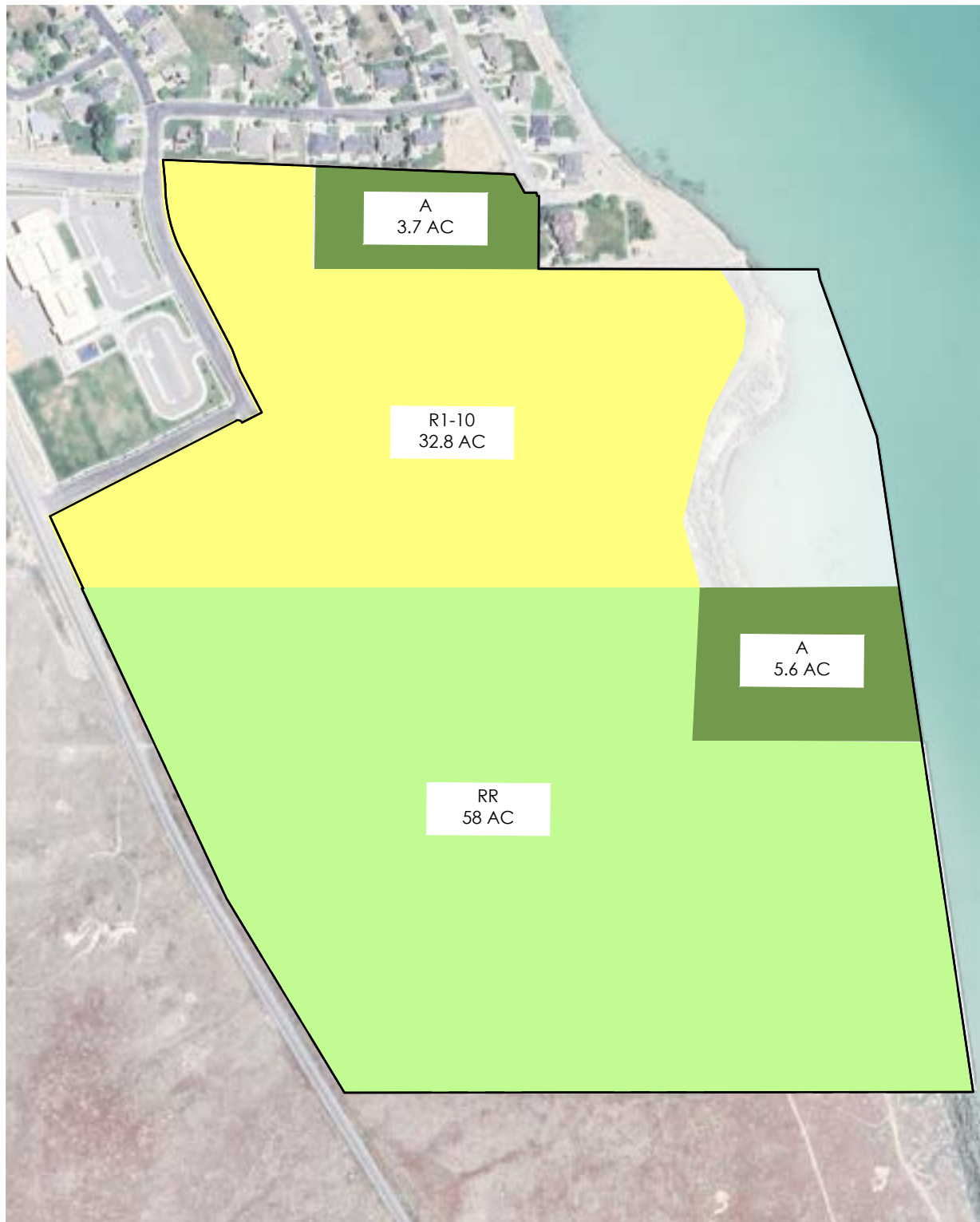
**C. Recommendation:** Staff recommend the applicant address and incorporate the following items into the development of their project and construction drawings.

1. The City has insufficient information at this time to determine what project and system improvements will be necessary to service the developer's property. As a result, this review does not reserve utility system capacity. Prior to, concurrent with, or subsequent to Final Plat Approval, the developer will be required to install all required infrastructure to service the property while mitigating negative impacts to the existing system. In addition to all required project improvements, the developer may also be required to install any and all system improvements, subject to required impact fee credits.
2. These are concept-level plans which are not ready for construction. The applicant understands that full engineering review will need to occur on final-level engineering plans to comply with City Standards. The applicant also understands that concept plans do not entitle the applicant to any approvals, including lot yields, and that approvals are not granted until final-level engineering plans are accepted by the City for construction.
3. The developer is required to adhere to the horizontal and vertical sewer separations standards established by the State of Utah.
4. Developer shall be responsible for installing any offsite infrastructure required to service the property. It shall be noted that the Zone 1 South pressurized irrigation system lacks sufficient source water to support this development. The Master planned PI Well 9 or Marina Pump Station upgrades are required to provide source.
5. As part of the future preliminary plat application the Developer shall follow the City's Transportation Masterplan and design roadway intersection access onto Redwood Rd. This intersection access onto Redwood Rd is a future signalized

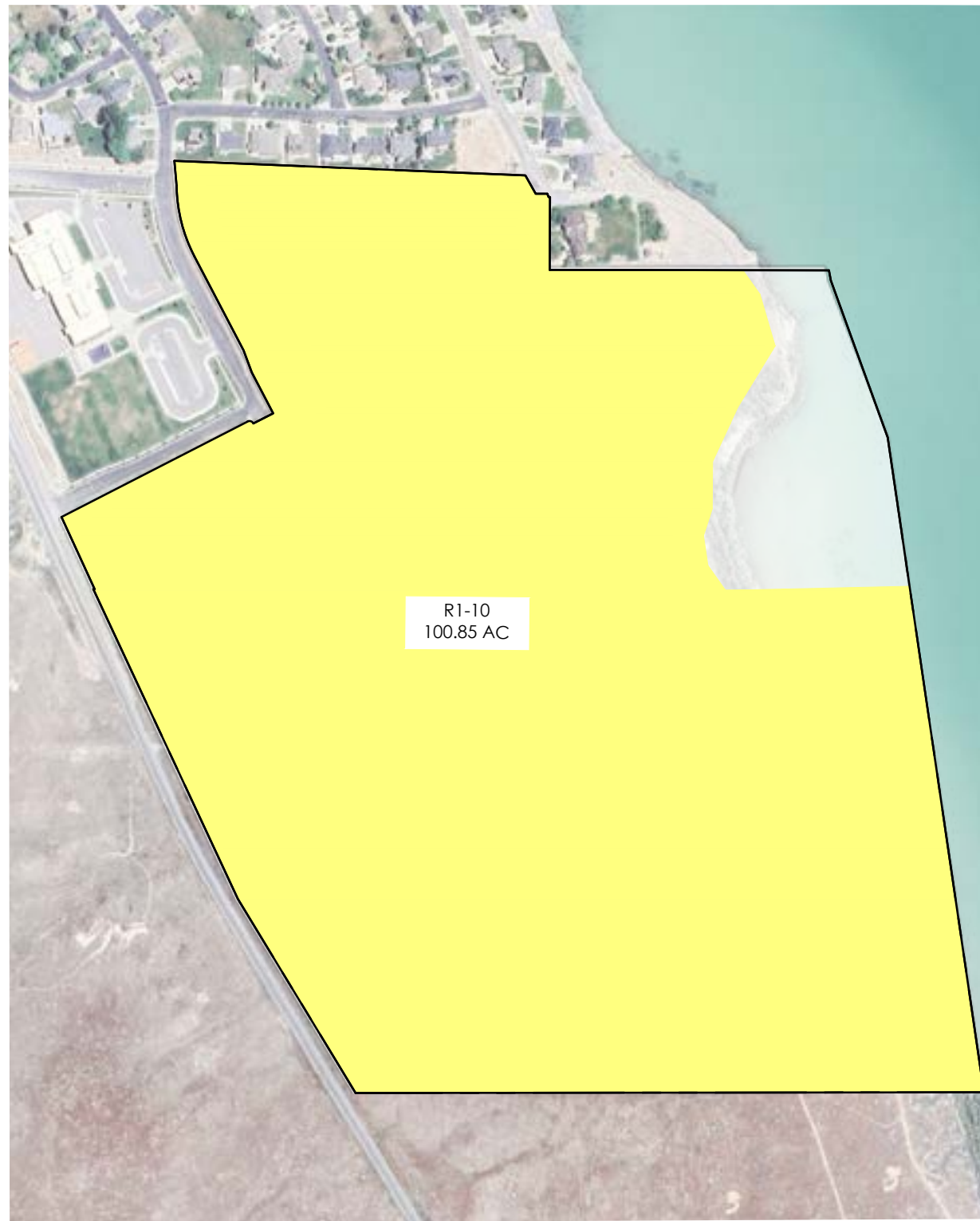
intersection and its location shall be coordinated with UDOT through their conditional access and encroachment permits. Coordination with the landowner across the street westerly (currently Ivory Homes) is advised to ensure the proposed intersection location aligns with all stakeholders' future plans for this key intersection.




6. Developer shall be required to install and improve the trail improvements along Redwood Rd and the Utah Lake trail according to the City's Trails Masterplan.
7. Developer shall also be responsible for installing landscaping and maintaining any frontage areas between Developer's property and the pavement surface of Redwood Rd.

**EXISTING ZONING**

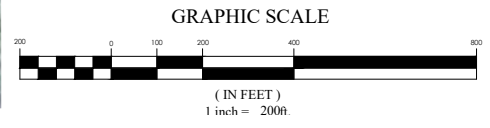


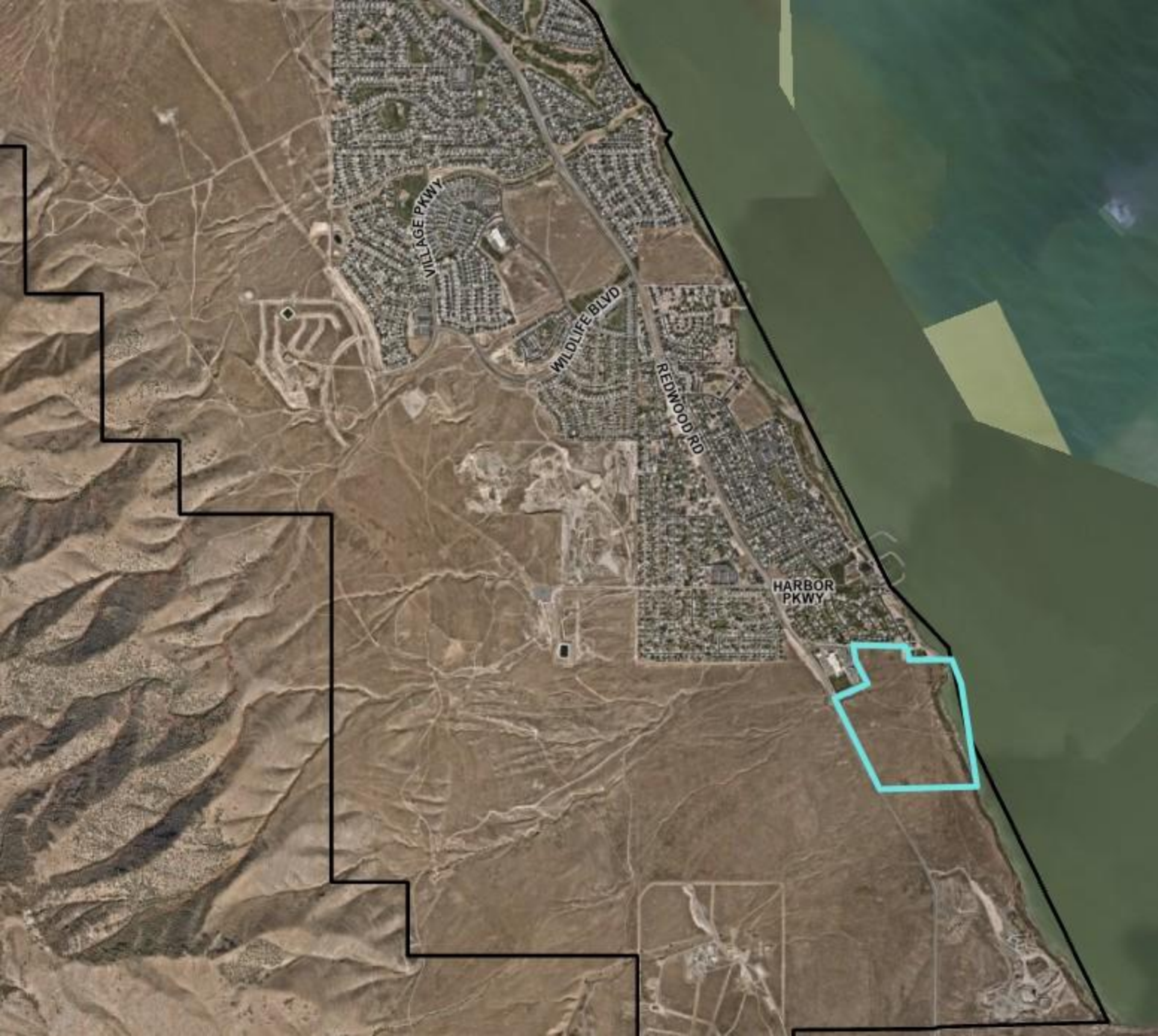
**PROPOSED ZONING**



- LEGEND**
-  R1-10: LOW DENSITY RESIDENTIAL
  -  RR: RURAL RESIDENTIAL
  -  A: AGRICULTURAL ZONE

TOTAL ACREAGE ±109.0 ACRES





VILLAGE PKWY

WILDLIFE BLVD

REDWOOD RD

HARBOR PKWY



**APPLICATION REVIEW CHECKLIST**  
Updated 1.12.26

**Application Information**

---

**Rezone and Concept Plan**  
**Lighthouse Cove**

Applicant:	Lynsi Neve
Owner:	Perry Land Investments LLC
Location:	Redwood Road and East Ranken Lane
Project Acreage:	Rezone 66.71 acres; Concept Plan -
Residential Units/Lots:	197 units
Current Use:	Vacant land
Land Use Designation:	Low Density Residential
Parcel Zoning:	Agricultural and RR
Requested Zoning:	R1-10
Adjacent Zoning:	West: R1-9,PC; North: R1-10; South: A
Adjacent Uses:	Residential and vacant land
Past Action and Date:	No past actions
Type of Action:	Legislative
Land Use Authority:	City Council
Planner:	Gina Grandpre, Senior Planner
Land Use Overlay:	Not Applicable
In Gateway Overlay:	No
Wildland Urban Interface:	Yes
In Hillside Areas:	No
In Waterfront Buffer:	Yes
Wetlands:	Yes
Floodplain:	Yes
Agricultural Protection:	No

<b>Date Received:</b>	<b>1<sup>st</sup> submittal:</b> February 3, 2026
	<b>2<sup>nd</sup> submittal:</b> <a href="#">Click here to enter a date.</a>
	<b>3<sup>rd</sup> submittal:</b> <a href="#">Click here to enter a date.</a>
	<b>4<sup>th</sup> submittal:</b> <a href="#">Click or tap to enter a date.</a>
<b>Date of Review:</b>	<b>1<sup>st</sup> submittal:</b> February 26, 2026
	<b>2<sup>nd</sup> submittal:</b> <a href="#">Click here to enter a date.</a>
	<b>3<sup>rd</sup> submittal:</b> <a href="#">Click here to enter a date.</a>
	<b>4<sup>th</sup> submittal:</b> <a href="#">Click or tap to enter a date.</a>

**Parcel Number(s) and size:** Parcel 16:003:0025 – 57.95 acres  
Parcel 16:004:0003 – 6.05 acres  
Parcel 16:003:0051 – 37.64 acres  
Parcel 16:003:0048 – 3.34 acres  
Parcel 16:003:0041 – 2.84 acres  
Parcel 16:003:0050 – 0.82 acres  
Total Acres:

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### Section 19.13 – Application Submittal

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- Application Complete (Date of Payment): February 3, 2026
- Rezone Required: Yes
- General Plan Amendment required: Yes

---

#### Section 19.13.04 – Process

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- Required Meetings: Planning Commission and City Council
- Planning Director Approval: No
- Public Hearing Required – check 19.13.04 process table: Yes - Planning Commission
- Neighborhood Meeting Required: No
  - Required for any multi-family or non-residential development proposal adjacent to developed property in a residential zone.
  - **Inform the applicant via email with the steps in the code;** this should be completed prior to approval by the Land Use Authority - preferably before PC.
    - **Email Sent:** N/A **Date:** N/A

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#### DRC Review

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##### DRC Review Comments:

February 10, 2026

##### Lighthouse Cove Site Plan (15-minute discussion)

- Subdivision may need to acquire an additional property to the north in order to allow for needed radii of roads
  - Property may be a remnant of an illegally subdivided lot
- 59' road is an old standard, should it be rededicated as 62' of ROW?
- Are new drainage facilities being proposed?

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#### Code Review

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- 19.04, Land Use Zones
  - Zone: R1-10
  - General Plan Land Use: Low Density Residential
  - Density: 197 units/66.71 acres

19.04.010 Requirements		R1-10	
Category To Be Reviewed	Regulation	Compliance	Findings
Maximum Units per Acre	3 unit/acre	Can Comply	4.83 u/a subject to CC approval
Lot Size, Residential (Minimum)	10,000 sq. ft.	Can Comply	4,700 sq. ft. subject to CC approval
Lot Size, Non-Residential (Minimum)	30,000 sq. ft.	N/A	
Footprint Development	N/A		
Building Separation - Footprint (Minimum)	N/A		
Project Size - Footprint (Minimum)	N/A		
Lot Coverage (Maximum)	50%	Can Comply	TBD at building permit
Structure Height (Minimum)	35'	Can Comply	TBD at building permit
Dwelling Size (Minimum)	1,000 sq. ft.	Can Comply	TBD at building permit
Lot Width (Minimum)	70'	Can Comply	Exception required per CP/VP
Lot Frontage*** (Minimum)	35'	Complies	>35'
Open Space Frontage (Minimum)	35'	Complies	See VP page 16
Front Setback* (Minimum)	25', 20' for enclosed entry or porch	Can Comply	TBD at prelim plat
Street Side Setback (Minimum)	20'	Can Comply	TBD at building permit
Interior Side Setback, Residential(Minimum)	8'/20'(min/combined)	Can Comply	TBD at building permit
Interior Side Setback, Non-Residential(Minimum)	25'	Can Comply	TBD at building permit
Rear* Setback, Residential(Minimum)	25'	Can Comply	TBD at building permit
Rear Setback, Non-Residential(Minimum)	25'	Can Comply	TBD at building permit

- **Don't Forget to check the exceptions and \*\*\* at the bottom of 19.04.07 table**
  - Lot Size Reductions: Choose an item. If "Yes," add criteria from 19.04.07.2
  - Footprint Development: Choose an item. If "Yes," add criteria from 19.04.07.3

No density shall be calculated on sensitive lands.	<b>Shall Comply.</b>	<i>Show on Preliminary Plat</i>
A Neighborhood Plan is required in the Mixed Residential Zone.	N/A.	
Open Space and Landscaping Requirements: For non-residential and non-agricultural uses open space is not required; however a minimum of 20% of the total project shall be used for landscaping, including improvements consistent with the Parks and Trails Master Plan, General Plan, Bicycle and Pedestrian Master Plan, or other applicable plan.	N/A.	
Prohibition of Creating New Residential Units. No existing Dwelling, Two-family; Dwelling, Three-family; or Dwelling, Multi-family, shall be further subdivided or otherwise approved to contain an additional dwelling(s).	<b>Complies.</b>	<i>None proposed</i>
19.04.08 & 19.04.11 Proposed Uses Allowed - Permitted Uses:	<b>Complies.</b>	<i>Single-family</i>

A neighborhood meeting is required for all public parks, public playgrounds, public recreation areas, or other public park improvements prior to new construction. City staff will notify residents within the subdivision or neighborhood area prior to any meeting. Any proposal for a regional park within the City will also be required to go Through a Site Plan review according to the requirements within the Land Development Code.	N/A.	<i>Neighborhood meeting not required</i>
19.04.09 (9) and 19.04.11 (superscript 2): Ancillary uses and edge uses may not exceed 20% of the building area within a Master Development Plan contained in a Master Development Agreement	N/A.	

<b>19.11 Lighting</b>		
<b>General Standards</b>		
<b>Regulation</b>	<b>Compliance</b>	<b>Findings</b>
<b>Material:</b> All Lighting Fixtures and assemblies shall be metal.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Base:</b> All lighting poles shall have a 16” decorative base.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Type:</b> All lighting fixtures shall be of the full cutoff variety. Shoebox fixtures are prohibited.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Angle:</b> Shall be directed downward.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Lamp:</b> Bulbs may not exceed 4000k.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Drawings:</b> Design and location of fixtures shall be specified on the plans.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Flags:</b> The Unites States flag and the state flag shall be permitted to be illuminated from dusk till dawn. All other flags shall not be illuminated past 11:00 p.m. Flag lighting sources shall not exceed 10,000 lumens per flagpole. The light source shall have a beam spread no greater than necessary to illuminate the flag.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Prohibited Lighting:</b> Searchlights, strobe lights and any laser source light or any similar high intensity light.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Descriptions:</b> Descriptions of the illuminating devices, fixtures, lamp supports, and other devices. This description may include, but is not limited to, manufacturers’ specifications, drawings, and sections.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Residential Lighting</b>		
<b>Floodlights:</b> Floodlights are prohibited.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Street Lighting:</b> All street lighting fixtures shall be metal and black and also include an arm and bell shade or a pole and lantern configuration and meet the City Standards.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat.</i>
<b>Building Lighting:</b> Building lighting shall be full cutoff and downward directed, and only for the illumination of vertical surfaces such as building facades and signs, and shall not cast illumination beyond the surface being illuminated.	N/A.	
<b>Single Family:</b> Homes located less than one half mile from Camp Williams shall have full cutoff lighting that does not exceed 90, and no upward lighting is permitted.	N/A.	

## 19.12 Subdivision

### Subdivision Layout

<p><b>Layout:</b> The subdivision layout should be generally consistent with the City's adopted Land Use Element of the General Plan, and shall conform to any land use ordinance, any capital facilities plan, any impact fee facilities plan, and the transportation master plan.</p>	<p><b>Shall Comply.</b></p>	<p><i>Generally, complies with the proposed rezone from parts of land zoned as Agricultural and Rural Residential to R1-10 – Low Density Residential.</i></p>
<p><b>Trails Master Plan:</b> Shows required trails</p>	<p><b>Complies.</b></p>	<p><i>Shown in plans</i></p>
<p><b>Block Length:</b> The maximum length of blocks shall be 1,000'. In blocks over 800' in length, a dedicated public walkway through the block at approximately the center of the block will be required.</p>	<p><b>Complies.</b></p>	<p><i>Shown in file</i></p>
<p>Such a walkway shall not be less than 15' in width unless otherwise approved by the City in accordance with other applicable standards approved by the City Council.</p>	<p><b>N/A.</b></p>	<p><i>Not applicable to this application</i></p>
<p><b>Connectivity:</b> The City shall require the use of connecting streets, pedestrian walkways, trails, and other methods for providing logical connections and linkages between neighborhoods.</p>	<p><b>Shall Comply.</b></p>	<p><i>Submit with Preliminary Plat</i></p>
<p><b>Mailboxes:</b> Group mailboxes shall be accessed only from a local street, and shall not be placed on a collector or arterial street, unless a bulbout is provided with space for a minimum of 3 vehicles to park outside the lane of travel and shoulder.</p>	<p><b>Shall Comply.</b></p>	<p><i>Submit with Preliminary Plat.</i></p>
<p><b>Private Roads:</b> Private roads may be constructed as approved as part of the Preliminary Plat approval and so long as such roads meet the same standards identified in the Saratoga Springs Standard Street Improvement Details.</p>	<p><b>N/A.</b></p>	<p><i>Not applicable to this application</i></p>
<p><b>Access:</b> Where the vehicular access into a subdivision intersects an arterial road as defined in the Transportation Master Plan, driveways shall not be placed on the intersecting road within 100' of the arterial connection.</p>	<p><b>N/A.</b></p>	<p><i>Not applicable to this application</i></p>
<p><b>Two separate means</b> of vehicular access onto a collector or arterial road shall be required to be constructed to City road standards when the total number of equivalent residential units (including adjacent developments and neighborhoods) served by a single means of access will exceed thirty.  <b>Exception:</b> Where no point of second access is available within 500' and where all units are provided with an approved sprinkler system, a second access shall not be required until the number of units reaches double the above limits.</p>	<p><b>Shall Comply.</b></p>	<p><i>Will comply with Engineering Standards</i></p>
<p>Where two means of access are required, the points of access shall be placed a minimum of 500' apart, measured along the center of the driving lane from center of right-of-way to center of right-of-way. The City Fire Chief may require a greater distance than 500 feet if:</p> <ol style="list-style-type: none"> <li>1. an essential link exists between a legitimate governmental interest and the requirement; and</li> <li>2. the requirement is roughly proportionate, both in nature and extent, to the impact of the proposed development.</li> </ol>	<p><b>Shall Comply.</b></p>	<p><i>Will comply with Engineering Standards</i></p>
<p><b>Driveway and Driveway Approaches:</b>  <b>Construction:</b> single driveways in the A, RA, and RR zones that are constructed of road base or gravel shall include a concrete apron at the entrance to the garage and at the intersection with the street, each a minimum of 5 feet in depth. Single driveways in all other zones, and shared driveways in all zones, shall be constructed of concrete or asphalt.</p>	<p><b>N/A.</b></p>	
<p><b>Shared Driveways:</b> Shared driveways shall be a minimum of 26' in width and shall direct all runoff to a public or private drainage system.</p>	<p><b>N/A.</b></p>	

<p>All dwellings on shared driveways shall provide enclosed garages or other covered parking. Shared driveways accessing more than four dwellings shall also provide a minimum of 25' of parking space between the garage and shared driveway. Shared driveways with four or fewer dwellings, if not providing a minimum of 20' of parking space, shall install a remote garage door opener prior to issuance of Certificate of Occupancy. All requirements of the Fire Code shall also be met.</p>		
<ol style="list-style-type: none"> <li>1. Shared Driveways may be used for clustered single-family lots. <ol style="list-style-type: none"> <li>a. When there are more than two units sharing a driveway, the maximum length shall be 150 feet.</li> <li>b. The maximum amount of units accessing a shared driveway shall be six.</li> </ol> </li> <li>2. Shared driveways may be used for two-family, three-family, and multi-family units. <ol style="list-style-type: none"> <li>a. The maximum length shall be 150 feet.</li> <li>b. The maximum amount of units accessing a shared driveway shall be 10.</li> </ol> </li> <li>3. Number of Driveway Approaches: Only one driveway approach is allowed per property frontage. A circular driveway is considered one driveway approach if installed in accordance with the provisions of this section. <ol style="list-style-type: none"> <li>a. Corner lots may have one driveway approach per street frontage.</li> <li>b. Circular driveways must have at least 15' between the two closest edges of the driveway approach at the property line and meet all other conditions of this code.</li> </ol> </li> <li>4. Driveway Approach widths: Residential driveway aprons shall not exceed 30 feet in width, except as follows. <ol style="list-style-type: none"> <li>a. Driveway aprons may be increased up to 45-ft in width if the home has a 3 car garage and/or parking pad and the combined width of all drive approaches on a single frontage does not consume more than 60% of the total property frontage and all other conditions of the section are met.</li> </ol> </li> </ol>	<p>N/A.</p>	
<p><b>Driveway Widths:</b></p> <ol style="list-style-type: none"> <li>1. Driveways shall be a minimum of 20 feet deep and eight feet wide for a single-wide driveway and sixteen feet wide for a double-wide driveway.</li> <li>2. Rear-load alley product, as approved as part of a Community Plan, Village Plan, or Neighborhood Plan, may have an apron in lieu of a driveway approach for access to the garage from the alley. The apron shall be a minimum of five feet deep and shall not count towards required parking.</li> </ol>	<p><b>Shall Comply.</b></p>	<p><i>TBD at building permit.</i></p>
<p><b>Driveway Approach Locations:</b> Driveway approaches shall be a minimum of 5' from any property line where water meters are located, except for corner lots as specified below.</p> <ol style="list-style-type: none"> <li>1. For corner lots, or where the vehicular access into a subdivision intersects a collector or arterial road as defined in the Transportation Master Plan, the minimum driveway approach distance, as measured from the edge of the right of way to the nearest edge or driveway surface shall be as follows: Local = 15 feet; Collector = 40 feet, Arterials = 100 feet.</li> <li>2. Residential lots or parcels shall not be allowed to have access ways onto arterial roads such as Redwood Road, Crossroads Boulevard, Pioneer Crossing, and Pony Express. Exceptions may be made for large lots (at least 1 acre in size) or for lots where the</li> </ol>	<p><b>Shall Comply.</b></p>	<p><i>TBD at building permit.</i></p>

home is set back over 150 feet from the arterial roadway. Approval by UDOT may be required.		
The City may reject any permit or proposal for a driveway approach where staff has determined the proposed location would be dangerous or where it conflicts with any permanent improvements, existing or master planned utilities, or waterways.	<b>Shall Comply.</b>	<i>TBD at building permit.</i>
<b>Lot Design</b>		
All subdivisions shall result in the creation of lots that are developable and capable of being built upon. A subdivision shall not create lots that would make improvement impractical due to size, shape, steepness of terrain, location of watercourses, sanitary sewer problems, driveway grades, or other physical constraints and considerations.	<b>Complies.</b>	<i>Shown in Concept Plan</i>
All lots or parcels created by the subdivision shall have frontage on a street or road that meets the City's ordinances, regulations, and standards for public roads.	<b>Complies.</b>	<i>Shown in Concept Plan</i>
Flag lots may be approved with less frontage when the Planning Commission determines that the creation of such a lot would result in an improved design or better physical layout for the lot based on the following criteria: <ul style="list-style-type: none"> <li>i. For subdivisions with 20 or less lots: no more than 10% (rounding down) of the total lots are allowed to be flag lots;</li> <li>ii. For subdivisions with 50 or less lots: no more than 7.5% (rounding down) of the total lots are allowed to be flag lots; and</li> <li>iii. For subdivision with more than 50 lots: no more than 5% (rounding down) of the total lots are allowed to be flag lots.</li> </ul>	<b>N/A.</b>	<i>No flag lots proposed</i>
Land dedicated as public roads and rights-of-way shall be separate and distinct from land included in lots adjacent to public roads and rights-of-way. In no case may land dedicated for public roads and rights-of-way be included in the area calculation of any lots, except for non-conforming lots.	<b>Complies.</b>	<i>ROW separated from lots.</i>
Side property lines shall be at approximately right angles to the street line or radial to the street line.	<b>Does Not Comply.</b>	<i>See note on Concept Plan</i>
Corner lots for residential use shall be 10% larger than the required minimum lot. Corner lots in the R1-10, R1-9, R2-8, R3-6, MR-10, MF-14, and MF-18 zones shall be 10% larger than the minimum required of the zone.	<b>Complies.</b>	<i>Shown in file</i>
No lot shall be created that is divided by a municipal or county boundary line.	<b>N/A.</b>	
Remnants of property shall not be left in the subdivision that do not conform to lot requirements or are not required or suitable for common open space, private utilities, public purposes, or other purpose approved by the Land Use Authority.	<b>Complies.</b>	<i>No remnants</i>
Double access lots are not permitted with the exception of corner lots.	<b>Shall Comply.</b>	<i>Submit with Preliminary Plat</i>
Driveways for residential lots or parcels shall not be allowed to have access on major arterials. <b>Exception:</b> Exceptions may be made for large lots (at least 1 acre in size) or for lots where the home is set back over 150' from the arterial roadway. Approval by UDOT may be required.	<b>Complies.</b>	<i>No arterial driveways</i>
All subdivisions along arterial roadways shall conform to the City's requirements and adopted street cross-section including pedestrian walkways, park strips, landscaping, and fencing.	<b>Shall Comply.</b>	<i>Shall comply if applicable</i>
<b>Street Connectivity</b>		

<p><b>Connectivity Standards.</b> All new subdivisions shall provide connectivity with adjacent developed and undeveloped properties and with adjacent open space, amenities, parks, and natural areas. All new subdivisions are required to:</p> <p>Extend streets, sidewalks, and trails at least once in each direction to adjacent properties; and</p>	<p><b>Does Not Comply.</b></p>	<p>See Engineering and Planning Notes in Concept Plan regarding the northeast corner ROW of the proposed subdivision.</p>
<p>Connect to all existing vehicular and pedestrian access points on adjacent developed properties; and</p>	<p><b>Complies.</b></p>	<p>Shown in Concept Plan</p>
<p>Install and connect public trails into all adjacent public open space, parks, and trails, which includes but is not limited to connections to trail corridors with public access easements; and</p>	<p><b>Complies.</b></p>	<p>Shown in Concept Plan</p>
<p>Stub public streets at least every 1,000 feet into all adjacent sides of undeveloped properties; and</p>	<p><b>Complies.</b></p>	<p>Shown in Concept Plan</p>
<p>Connect or stub into all adjacent master-planned rights-of-way, sidewalks, trails, and public transportation stops, stations, and facilities.</p>	<p><b>Complies.</b></p>	
<p><b>Exceptions:</b> Connectivity Standards may be reduced by the Land Use Authority for Preliminary Plats, Final Plats, or Site Plans, as applicable, if the applicant provides clear and convincing evidence that it is impracticable to achieve due to the following:</p> <p>a. Right-of-way, intersection, or access spacing cannot meet the Standard Technical Specifications and Drawings for City of Saratoga Springs; or</p>	<p>N/A.</p>	
<p>ii. The property is adjacent to the Jordan River, Utah Lake, delineated wetlands, slopes exceeding 30%, drainage channels, natural features, open space, or waterways that do not allow for a crossing or an access; or</p>	<p>N/A.</p>	
<p>iii. The property is adjacent to fully developed property that does not have any vehicular or pedestrian access points.</p>	<p>N/A.</p>	
<p>iv. Exceptions shall be construed narrowly by the Land Use Authority.</p>	<p>N/A.</p>	
<p>v. These exceptions shall not apply to trail connections.</p>	<p>N/A.</p>	
<p><b>Arrangement of Streets.</b> The arrangement of streets in new developments shall make provision for the continuation of the existing streets in adjoining areas (or their proper projection where adjoining land is not subdivided) at the same or greater width (but in no case less than the required minimum width per the City’s Standard Technical Specifications and Drawings). Exceptions below shall be construed narrowly and only granted if the developer provides clear and convincing evidence to the Land Use Authority that the exception is met.</p>	<p><b>Shall Comply.</b></p>	<p>Submit with preliminary.</p>
<p>a. In order to develop a public street grid throughout the City, public right-of-way connections through and between developments shall be made at a minimum of every 1000 feet and in a manner that will provide safe and convenient access to existing or planned arterial/collector streets, schools, public parks, public trails, private parks or trails with public access easements, employment centers, commercial areas, or similar neighborhood activity centers. The connections may be completed over time in phases as part of a circulation plan. This requirement does not apply to commercial development; however, all streets and trails on the Transportation Master Plan and Parks, Recreation, Trails, and Open Space Master Plan shall be included and shall be public streets.</p>	<p><b>Shall Comply.</b></p>	<p>Submit with preliminary.</p>
<p>b. A public street connection shall be provided to any existing or approved public street right-of-way stub abutting the development,</p>	<p><b>Shall Comply.</b></p>	<p>Submit with preliminary.</p>

unless it is demonstrated that a connection cannot be made because of the existence of one of the following conditions: i. Physical conditions that preclude development of a public street meeting the City’s Standard Technical Specifications and Drawings. Such conditions may include, but are not limited to, topography, natural resource areas, such as wetlands, ponds, streams, channels, rivers or lakes, or slopes exceeding 30%.		
ii. Buildings or other existing development on adjacent lands, including previously subdivided but vacant lots or parcels, which obstruct a connection now and in the future.	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>
a. Public rights-of-way shall be extended/stubbed to adjacent undeveloped or partially developed contiguous land (i.e. land that can be further subdivided by provisions of this Title) in locations which will not prevent the adjoining property from developing consistent with applicable standards, unless it is demonstrated that a connection cannot be made because of one of the following conditions:	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>
i. Physical conditions that preclude development of a public street meeting the City’s Standard Technical Specifications and Drawings. Such conditions may include, but are not limited to, topography, natural resource areas, such as wetlands, ponds, streams, channels, rivers or lakes, or slopes exceeding 30%.	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>
ii. Buildings or other existing development on adjacent lands, including previously subdivided but vacant lots or parcels, which obstruct a connection now and in the future.	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>
<b>Circulation Plan.</b> A circulation plan shall be provided as part of a preliminary subdivision plat or site plan application.	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>
The circulation plan shall include connections to adjacent parcels and shall address street, sidewalk, and trail connectivity and shall include facilities shown on the City’s adopted Master Plans.	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>
The circulation plan shall show the connectivity, block length dimensions, cul-de-sac length dimensions, bike and pedestrian trails, and any proposed traffic calming features.	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>
The circulation plan shall include proposed connections to and across adjacent properties.	<b>Shall Comply.</b>	<i>Submit with preliminary.</i>

<b>19.13 Process</b>	
Regulation	Findings
<b>Neighborhood Meeting.</b> Required before Planning Commission for any multi-family or non-residential development proposal adjacent to developed property in a residential zone. <b>Inform the applicant when this is required.</b>	<i>Not required</i>
Notice/Land Use Authority.	<i>To be noticed at PC and CC</i>
<b>Master Development Agreement.</b> A Master Development Agreement shall be required of any development that is in excess of 20 acres in size if non-residential or mixed-use or developments in excess of 160 acres in size if residential. A Master Development Agreement may also be required pursuant to this Title 19 including Chapter 19.26 or may be desirable or necessary pursuant to the exercise of the City	<i>N/A</i>

Council's legislative discretion in the fact scenarios listed in Section 19.13.08.	
Phasing Improvements.	N/A
Payment of Lieu of Open Space.	Amount of \$:
Piping of Canals	For residential projects, piping of canals per canal company specifications if a canal or canal easement that area is adjacent to or within the area of the proposed residential project, unless the canal company or Bureau of Reclamation does not allow piping. Non-residential projects shall install secure fencing adjacent to canal easements or canals per canal company specifications to prevent entry from the non-residential project onto the canal or canal easement.
Burial of Overhead Utility Lines	See Section 19.13.10

<b>Concept Plan Application Requirements</b>		
<b>Regulation</b>	<b>Compliance</b>	<b>Findings</b>
A completed application and affidavit, form, and application fee.	<b>Complies.</b>	
Plat/Parcel Map of the area available at the Utah County Surveyor's Office.	<b>Complies.</b>	
Proposed changes to existing zone boundaries, if such will be needed	<b>Complies.</b>	
Conceptual elevations and floor plans, if available	<b>Complies.</b>	
Drawn to a scale of not more than 1" = 100'	<b>Complies.</b>	
Proposed name of subdivision, cleared with the County Recorder to ensure the name is not already in use	<b>Complies.</b>	
Name of property if no subdivision name has been chosen. This is commonly the name in which the property is locally known	<b>Complies.</b>	
Locations and widths of existing and proposed streets and right-of-ways	<b>Complies.</b>	
Road centerline data including bearing, distance, and curve radius	<b>Complies.</b>	
Configuration of proposed lots with minimum and average lot sizes	<b>Complies.</b>	
Approximate locations, dimensions, and area of all parcels of land proposed to be set aside for park or playground use or other public use, including acreages, locations, and percentages of each and conceptual plan of proposed recreational amenities	<b>Complies.</b>	
Those portions of property that qualify as sensitive lands per Section 19.02.02., including acreages, locations, types, and percentages of total project area and of open space	<b>Complies.</b>	
Total acreage of the entire tract proposed for subdivision	<b>Complies.</b>	
<b>General topography shown with 1' or 2' contours and slope arrows with labels</b>	<b>Shall Comply.</b>	<i>Provide with Preliminary Plat</i>
<b>North arrow, scale, and date of drawing</b>	<b>Shall Comply.</b>	<i>Provide with Preliminary Plat</i>
Property boundary with dimensions	<b>Complies.</b>	
Data table including total number of lots, dwellings, and buildings, square footage of proposed buildings by floor, number of proposed garage parking spaces, number of proposed surface parking spaces, number of required and proposed ADA compliant parking spaces, percentage of buildable land, percentage and amount of open space or landscaping, and net density of dwellings by acre	<b>Complies.</b>	<i>Shown in Concept Plan</i>
Existing conditions and features within and adjacent to the project area including roads, structures, drainages, wells, septic systems, buildings, and utilities	<b>Complies.</b>	<i>Shown in Concept Plan</i>
Conceptual utility schematic with existing and proposed utility alignments and sizes sufficient to show how property will be served including drainage, sewer, culinary and secondary water connections and any other existing or proposed utilities needed to service the	<b>Complies.</b>	<i>Shown in Concept Plan</i>

proposed development or that will need to be removed or relocated as part of the project		
A schematic drawing of the proposed project that depicts the existing proposed transportation corridors within 2 miles, and the general relationship of the proposed project to the Transportation and Land Use Element of the General Plan and the surrounding area	<b>Complies.</b>	<i>Shown in Concept Plan</i>
<b>Trails Master Plan:</b> Shows required trails	<b>Complies.</b>	<i>Shown in Concept Plan</i>

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<b>19.18 Signs</b>		
<b>Regulation</b>	<b>Compliance</b>	<b>Findings</b>
Site Plans require signage information (if tenant known, show location potential location size)	<b>Shall Comply.</b>	<i>Provide information during Preliminary Plat</i>
Monument sign location meets code and sign details provided	<b>Shall Comply.</b>	<i>Provide information during Preliminary Plat</i>
Wall signs and potential locations and details	<b>Shall Comply.</b>	<i>Provide information during Preliminary Plat</i>

<b>19.19 Open Space</b>		
<b>Minimum Required Open Space</b>		
<b>Regulation</b>	<b>Compliance</b>	<b>Findings</b>
<b>Open Space Required:</b> A minimum of one Equivalent Acre of park space is required for every 40 residential units in a development, or fraction thereof.	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>
<b>Minimum Percentage by Development:</b> In addition to the minimum Equivalent Acres, to ensure a livable community, in no case shall the percentage of total open space acreage provided in the following types of development be less than the listed percentage: a. Single family developments: 10% of overall development acreage b. Multi-family developments: 15% of overall development acreage c. Mixed-use and Mixed Waterfront developments: 25% of overall development acreage d. Developments with both single family and multi-family: combination of the requirement based on the acreage for each type of housing.	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>
<b>Other Limitations:</b> In no case may the cumulative total of the following categories qualify for more than 50% of a development’s Equivalent Acre requirement. i. Unimproved, not Sensitive Lands ii. Open space with no access	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>

<b>Minimum Required Amenities</b>		
<b>Minimum Points:</b> Minimum required points are based on the number of required Equivalent Acres provided in a contiguous park and are outlined in the table in 19.19.05(2).	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>
<b>Mixture of Amenities and Required Amenities:</b> All parks are required to provide a mixture of amenities, including at least 1 separate item each from Categories C, D, and E.	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>
No more than 25% of the points may be met by one specific item type in any one category.	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>

All parks over 5 Equivalent Acres are also required to provide at least one item from Category A or B, and three items from category P, and a minimum one toilet restroom per each 5 acres or fraction thereof.	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>
Items in Categories A and B may qualify for impact fee credits, if identified in the Parks and Trails Master Plan.	<b>Shall Comply.</b>	<i>The proposed amenity points should comply, but please provide more information – see notes in Red Lines.</i>
<b>Payment in Lieu of Open Space</b>		
<p><b>Applicability:</b> the City’s Payment in Lieu of Open Space Program may be utilized for all or a portion of the requirement for developments that meet one or more of the following criteria:</p> <ul style="list-style-type: none"> <li>i. any single-family development where a minimum of 75% of the lots are 10,000 square feet or larger, or</li> <li>ii. any single-family development with a park requirement of less than 1 acre, or</li> <li>iii. any development in the MW zone where such fee will aid in the creation of large clustered open spaces near the waterfront, or</li> <li>iv. that portion of a development that is located within ¼-mile of an existing improved regional public park.</li> </ul>	<b>Shall Comply.</b>	<i>If applicable, submit with preliminary plat</i>
<p><b>Total Cost:</b></p> <ol style="list-style-type: none"> <li>1. The City shall maintain an annually updated list of land values for the cost of a non-sensitive developable acre, and apply the land value to the required Equivalent Acres for which the applicant desires to pay a fee in lieu.</li> <li>2. The City shall calculate the minimum required points per this chapter, and multiply the points by \$2,000 to determine the cost of the minimum required open space improvements, including landscaping, parks, trails, and other amenities.</li> </ol>	<b>Shall Comply.</b>	<i>If applicable, submit with preliminary plat</i>

<b>19.25 Waterfront Buffer Requirements</b>		
<b>Waterway Setbacks</b>		
<b>Regulation</b>	<b>Compliance</b>	<b>Findings</b>
<p><b>Riparian Setback:</b> No disturbance shall occur within 50’ of the Jordan River ordinary high-water mark or the Utah Lake Compromise line.  <b>Exception:</b> Boardwalks, patios, decks, and associated seating areas may be placed within the riparian setback. Such areas shall not exceed 300 square feet.</p>	<b>Shall Comply.</b>	<i>Please provide information at Preliminary Plat</i>
<p><b>Shoreline Trails:</b> Jordan River and Utah Lake shoreline trail corridors shall be a minimum of 20’ in width measured landward from the edge of the riparian setback.</p>	<b>Shall Comply.</b>	<i>Please provide information at Preliminary Plat</i>
<p><b>Patio/Lawn Area:</b> A minimum of 15’ shall be maintained between the Shoreline or Jordan River trail corridor and structures/parking areas to allow for lawns, patios, restaurant eating areas, and similar low-impact uses. Roads shall not be located between the first row of buildings and the waterway.</p>	<b>Shall Comply.</b>	<i>Please provide information at Preliminary Plat</i>
<p><b>Landscaping in Riparian and Trail Setbacks:</b> Native and naturalized plants allowed. Manicured landscaping and lawns are prohibited.</p>	<b>Shall Comply.</b>	<i>Please provide information at Preliminary Plat</i>
<p><b>Bank Stabilization:</b> Industry-accepted bioengineering designs shall be used to meld structural features with the environment and may include the use of features such as live plants, willow cuttings, logs, woody debris, coir fabrics, straw bales etc. The use of riprap or other hard armoring techniques is discouraged and may only be approved by the State of Utah.</p>	<b>Shall Comply.</b>	<i>Please provide information at Preliminary Plat</i>

## Utah Lake and Jordan River Shoreline Trails

<p>All developments whose projects are next to, adjacent to, or abutting, or include Utah Lake or the Jordan River shall provide an improved pedestrian shoreline trail throughout the length of the project.</p> <ol style="list-style-type: none"> <li>i. The shoreline trail shall at all times be accessible to the public.</li> <li>ii. The developer will work with the City and any other agency whose approval is necessary, to determine the exact location of the trail.</li> <li>iii. The developer shall construct the trail as close to the waterway as reasonably possible.</li> <li>iv. The trail shall be hard surfaced and shall be a minimum of 12' in width.</li> </ol>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>In the event that a portion of the planned shoreline trail throughout a developer's project is located on property not in the possession of or under control of the developer, the developer will work with the City to identify a proper location through the developer's project on which to construct the trail. In all cases, the developer shall construct the shoreline trail as close to the waterway as reasonable possible.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>The developer will work with the City to determine which party will construct the shoreline trail throughout the developer's project.</p> <ol style="list-style-type: none"> <li>a. If the developer constructs the shoreline trail throughout the developer's project, the developer may receive credit for the trail against the open space requirements of this Title. However, if the developer is granted a credit, the developer shall maintain the trail in perpetuity and grant the City a perpetual public access easement.</li> <li>b. The shoreline trail shall be constructed in accordance with the City's design guidelines, standards, and regulations.</li> </ol>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>If the City specifically agrees in writing, the shoreline trail will be maintained by the City after the City issues a Certificate of Final Acceptance.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>

## Public Access and Parking

<p>Developments shall provide a public access to the Shoreline or Jordan River trail.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>Developments shall provide public parking for bicycles and shall permit a portion of their motor vehicle parking to be available to the public in non-residential areas.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>Public pedestrian access shall include clearly marked travel pathways from the public street through parking areas to primary building entries.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>Parking areas visible from the Shoreline or Jordan River trail or the waterway shall be screened from view by landscaping or decorative fencing at least 3' in height.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>Appropriate landscaping should be utilized to screen habitat areas within the riparian setback from new development.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>

## Site Coverage and Building Permeability

<p>On lots or sites with a width greater than 100' that are generally parallel to the river corridor, structures shall not exceed 70% of the width of the site.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>On lots or sites with a 50' to 80' width that are generally parallel to the river, structures shall not exceed 50' in width or 70% of the width of the site, whichever is greater.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>
<p>At a maximum interval of 300' that is generally parallel to the river, there shall be a clear visual and pedestrian connection at the ground level from a public street to the river corridor. Such connections may consist of pathways, landscaping, transparent entrances and lobbies that provide the ability to see through a building, and other features with a similar effect.</p>	<p><b>Shall Comply.</b></p>	<p><i>Please provide information at Preliminary Plat</i></p>

The visual and pedestrian connection shall not be less than 30' wide.	<b>Shall Comply.</b>	<i>Please provide information at Preliminary Plat</i>
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<b>Fiscal Impact</b>	
<b>Regulation</b>	<b>Findings</b>
Is there any City maintained open space?	<i>Please provide information on Preliminary Plat</i>
What is the anticipated cost to the City?	<i>Please provide information on Preliminary Plat</i>
When will City maintenance begin?	<i>Please provide information on Preliminary Plat</i>

# Exhibit 4



## CONCEPT TABULATIONS

TOTAL ACREAGE	± 109.0 ACRES
CURRENT ZONE	R1-10, RR, A
PROPOSED ZONE	R1-10

<b>LOT/UNIT COUNT</b>	<b>197</b>
TOTAL DENSITY	1.8 UNITS/ACRE

AVERAGE LOT SIZE	11,218 SQFT
------------------	-------------

## OPEN SPACE

REQUIRED OPEN SPACE	10.9 ACRES (10%)
REQUIRED EQUIVALENT ACRES	4.93 ACRES (1 ACRE PER 40 LOTS)

<b>TOTAL PROPOSED O.S.</b>	<b>31.8 ACRES (29.0%)</b>
<b>TOTAL PROPOSED EQ. O.S.</b>	<b>16.9 ACRES</b>
SENSITIVE LAND (DRAINAGE CHANNELS & WATER FEATURE)	22.3 ACRES 20% OF TOTAL 70% OF OS

## ZONE & LOT REQUIREMENTS

PROPOSED ZONE	R1-10
MAX. DENSITY	3 UNITS/ACRE
MIN. LOT SIZE	10,000 SQFT
MIN. LOT WIDTH	70'
MIN. FRONTAGE	35'
MIN. OPEN SPACE FRONTAGE	35'
FRONT YARD SETBACK	25', PORCHES 20'
REAR YARD SETBACK	20'
INTERIOR SIDE SETBACK	8' MIN., 20' COMBINED
CORNER SIDE SETBACK	25'

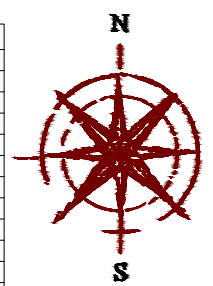
## CITY STANDARDS

LOCAL STREET R.O.W. WIDTH	62'
CUL-DE-SAC & KNUCKLE RADIUS	61.5'
MAX CUL-DE-SAC LENGTH	400' FROM CENTERLINE OF CROSS STREET

## LEGEND

- SINGLE FAMILY LOTS (197)
- PARK & AMENITY OPEN SPACE/DETENTION
- OPEN SPACE - SENSITIVE LAND
- ZONE AE FLOODZONE/OPEN SPACE
- CHURCH PARCEL
- PROPOSED TRAIL
- APPROXIMATE DRAINAGE LOCATIONS
- 20' BUFFER AROUND DRAINAGE

Required Information	Quantity
Total Lots	197
Total Dwellings	197
Net Density of Dwellings by Acre	1.8
Total Buildings	198
Proposed SF by Floor	1000-3500
Proposed Garage Spaces	394
Proposed Surface Parking Spaces	3
Required ADA Parking Stalls	N/A
Proposed ADA Parking Stalls	1
% Buildable Land	80%
Open Space (Acres)	31.8
Open Space (%)	29%



REZONE AMENDMENT APPLICATION  
COMPATIBILITY STATEMENTS

November 5<sup>th</sup>, 2025

The property located at approximately 40.284008, -111.870307, referred to as Lighthouse Cove, is being considered for development. The overall area consists of six parcels making up a total of 108.68 acres. Three of the six parcels, totaling 66.71 acres are under consideration for a zoning amendment.

Shown below are the three parcels to be rezoned to R1-10:

**PARCEL NUMBERS:**

**16:003:0041**

**16:004:0003**

**16:003:0025**

The following statements address the reasons that the proposed amendment is consistent with the Saratoga Springs Land Use Element of the General Plan:

The intention in proposing this amendment is to establish consistent zoning for the parcels listed above, of which their surrounding parcels (#16:003:0048 #16:003:0050 & #16:003:0051) are already zoned R1-10. This amendment is necessary to create more consistency with Title 19 & the Future Land Use Map in the General Plan, which designates the entire proposed area "Low Density Residential". The R1-10 zone is classified as a "Low Density Residential" Land Use Zone per Saratoga Springs Municipal Code 19.04.02.

**1. Conformance with the General Plan**

The proposed rezone from Agricultural and Rural Residential to R1-10 is consistent with and supportive of the goals and policies outlined in the Saratoga Springs General Plan. It reflects the City's long-term vision for managing rapid growth and ensuring adequate housing supply. The surrounding parcels are already R1-10, making this a logical and compatible transition. This rezone helps implement the City's plan to guide growth in a balanced and responsible manner while preserving the character and quality of life in Saratoga Springs.

**2. Impact on Public Health, Safety, and Welfare**

The proposed rezone from A & RR to R1-10 will not adversely impact the public health, safety, or general welfare of the community. Rather, it supports the City's long-term planning goals by allowing for additional single-family housing that meets the needs of

future residents. The R1-10 zone maintains appropriate standards for lot size, setbacks, and access, ensuring safe and livable neighborhoods. Infrastructure such as roads, utilities, and public services can be efficiently extended to serve the site, promoting orderly development without overburdening existing systems. By offering new housing opportunities in a planned and coordinated manner, this rezone contributes positively to the overall welfare of Saratoga Springs and supports the community's continued growth in a safe and responsible way.

### **3. Consistency with City Ordinances**

The proposed rezone from RR & A to Residential Single Family 10,000 (R1-10) is consistent with the stated purposes of the Saratoga Springs Land Development Code. This change supports the orderly growth and expansion of the City by transitioning underutilized agricultural land into a residential use that aligns with the surrounding development pattern and anticipated growth in the area. It contributes to fiscal and economic well-being by encouraging more efficient use of land and infrastructure. The rezone also supports the development of a cohesive, attractive neighborhood while ensuring compliance with the Land Use Element of the General Plan. Overall, the change promotes the general welfare of the City and reflects the City's goals for responsible and sustainable development.

### **4. Balancing Public and Petitioner Interests**

While the rezone enables the petitioner to move forward with a residential development, it also delivers clear public benefits, including housing opportunities to meet the City's growing demand, thoughtfully designed neighborhoods, and community open space and amenities. The R1-10 zoning supports a suburban scale of development that aligns with community expectations and the City's General Plan. By facilitating responsible growth in a location suited for residential expansion, the rezone helps ensure that private investment contributes meaningfully to the public good and enhances the overall quality of life in Saratoga Springs.

### **5. Advancement of General Welfare**

The proposed rezone to R1-10 directly supports the advancement of the general welfare by facilitating well-planned residential growth that meets the evolving needs of Saratoga Springs. By allowing for a greater amount of single-family housing on appropriately sized lots, this rezone contributes to housing availability and community stability. The development will be designed to complement the surrounding area. In this way, the project supports long-term community health, prosperity, and cohesion, aligning with the City's broader goals for sustainable and inclusive growth.



**Code Amendments**  
**Amending Section 19.02**  
**March 26, 2026**  
**Public Meeting**

Report Date:	March 19, 2026
Applicant:	City Initiated
Previous Meetings:	N/A
Type of Action:	Legislative
Land Use Authority:	City Council
Planner:	Austin Roy, Senior Planner

**A. Executive Summary:**

Staff recently proposed code amendments for an exception for building buffer and Loading bays and the Planning Commission recommended approval of that code amendment and it is now scheduled for City Council on March 24. Upon further review, it was determined that having a definition for the term “Loading Bay” will help clarify when the exception applies.

**Recommendation:**

**Staff recommends that the Planning Commission conduct a public meeting on the proposed code amendments, review and discuss the proposal, and choose from the options in Section H of this report.** Options include a positive recommendation with or without conditions, a negative recommendation, or continuation.

**B. Background:**

Recent site plans in Jacob Ranch Marketplace and North Shore Commercial have identified a need to be able to grant exceptions to the Building Buffer requirement. Title 19 does not currently allow exceptions for building corners or Loading Bays. An exception would allow better designs and also be consistent with how the City has treated Loading Bays. This code amendment will provide the ability for the City to grant exceptions for building corners and for Loading Bays.

**C. Specific Request:**

This is a request for approval of proposed code amendments to Section 19.02 of the Land Development Code, as summarized below and as attached.

<b>19.02, Definitions</b>	Adding a definition for “Loading Bays”.
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**D. Process:**

Section 19.17.03 outlines the process criteria for Planning Commission and City Council Review:

1. The Planning Commission shall review the petition and make its recommendations to the City Council within thirty days of the receipt of the petition.  
*Complies. This is a staff-initiated proposal. The application will be reviewed by the Planning Commission and receive a recommendation prior to review by the City Council.*
2. The Planning Commission shall recommend adoption of proposed amendments only where it finds the proposed amendment furthers the purpose of the Saratoga Springs Land Use Element of the General Plan and this Title.  
*Complies. Please see Sections F and G of this report.*
3. The Planning Commission shall provide the notice and hold a public hearing as required by the Utah Code and Chapter 19.13. For an application which concerns a specific parcel of property, the City shall provide the notice required by the Utah Code and Chapter 19.13 for a public hearing.  
*Complies. Please see Section E of this report.*

**E. Community Review:**

This has been noticed as a public hearing pursuant to City and State statutes, which requires posting notice on the Utah public notice website and the City’s website and in City Hall at least 10 calendar days prior to the public hearing.

Public Comment: As of the date of this report, no public input has been received.

**F. General Plan:**

The proposed amendments, as they relate to the 2022-2042 General Plan Vision, Goals and Strategies for Land Use & Neighborhoods, are evaluated below.

**Land Use and Neighborhoods, The Vision**

“Land Use and Neighborhoods supports the general plan vision by preserving existing neighborhoods and requiring new attractive, healthy, and family-friendly neighborhoods. Neighborhoods will have a variety of housing types and amenities. As new development occurs, it will be supported by appropriate services and amenities, ensuring a high quality of life for existing and future residents.”

**Land Use Goal**

Future development in Saratoga Springs reflects the community’s preferred vision.

**Staff conclusion: Consistent.** *The proposed amendments will allow better design for site plans by providing necessary exceptions for important elements of commercial site and building design.*

**G. Code Criteria:**

**Code amendments are a legislative decision and grant the City Council significant discretion when considering changes to the Code.**

The criteria for an ordinance are outlined below and act as guidance to the Council and to the Commission in making a recommendation. Note that the criteria are not binding.

**19.17.05 Consideration of General Plan, Ordinance, or Zoning Map Amendment**

The Planning Commission and City Council shall consider, but not be bound by, the following criteria when deciding whether to recommend or grant a general plan, ordinance, or zoning map amendment:

1. The proposed change will conform to the Land Use Element and other provisions of the General Plan;  
**Consistent.** *See Section F of this report.*
2. the proposed change will not decrease nor otherwise adversely affect the health, safety, convenience, morals, or general welfare of the public;  
**Consistent.** *The amendments will not adversely affect the health and welfare of the general public and will allow for exceptions in the design of commercial sites and buildings.*
3. the proposed change will more fully carry out the general purposes and intent of this Title and any other ordinance of the City; and

**19.01.04. Purpose.** This section identifies the purpose of Title 19.

1. The purpose of this Title, and for which reason it is deemed necessary, and for which it is designed and enacted, is to preserve and promote the health, safety, morals, convenience, order, fiscal welfare, and the general welfare of the City, its present and future inhabitants, and the public generally, and in particular to:
  - a. encourage and facilitate the orderly growth and expansion of the City;
  - b. secure economy in governmental expenditures;
  - c. provide adequate light, air, and privacy to meet the ordinary or common requirements of happy, convenient, and comfortable living of the municipality's inhabitants, and to foster a wholesome social environment;
  - d. enhance the economic well-being of the municipality and its inhabitants;
  - e. facilitate adequate provisions for transportation, water, sewer, schools, parks, recreation, storm drains, and other public requirements;

- f. prevent the overcrowding of land, the undue concentration of population, and promote environmentally friendly open space;
- g. stabilize and conserve property values;
- h. encourage the development of an attractive and beautiful community; and
- i. promote the development of the City of Saratoga Springs in accordance with the Land Use Element of the General Plan.

**Consistent.** *The proposed amendments will improve clarity in the code and contribute to a streamlined development review process both benefiting the City, developers, and the public.*

- 4. in balancing the interest of the petitioner with the interest of the public, community interests will be better served by making the proposed change.

**Consistent.** *The amendments will provide additional clarity and effectiveness of the Code and better enhance the consistency in development review.*

- 5. any other reason that, subject to the legislative discretion of the City Council, could advance the general welfare.

**H. Recommendation and Alternatives:**

Staff recommends that the Planning Commission conduct a public hearing, take public input, discuss the application, and choose from the following options.

**Option 1 – Positive Recommendation**

“I move that the Planning Commission forward a recommendation for approval of the proposed code amendments to Chapter 19.02, with the Findings and Conditions in the Staff Report.”

**Findings**

- 1. The application is consistent with the General Plan, as articulated in Section F of the staff report, which section is incorporated by reference herein.
- 2. The application complies with the criteria in section 19.17.05 of the Land Development Code, as articulated in Section G of the staff report, which section is incorporated by reference herein.

**Conditions:**

- 1. Any other conditions or changes as articulated by the Planning Commission:

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**Option 2 – Continuance**

“I move to **continue** the proposed code amendments to Chapter 19.02 to another meeting on [DATE], with direction to the applicant and Staff on information and/or changes needed to render a decision, as follows:

1. \_\_\_\_\_
2. \_\_\_\_\_

**Option 3 – Negative Recommendation**

“I move that the Planning Commission forward a recommendation for denial of the proposed code amendments to Chapter 19.02, with the Findings below:

1. The application is not consistent with the General Plan:
  - a. \_\_\_\_\_, and/or,
2. The application is not consistent with Section {XX.XX} of the Code:
  - a. \_\_\_\_\_.

**I. Exhibits:**

1. Proposed Code Amendments

**Chapter 19.02. Definitions**

**19.02.02. Definitions.**

As used in this Title:

178. “**Loading Bay**” means any part of a building where access is provided via a roll up door, overhead door, sectional door, or garage door. Primarily used to load and unload vehicles, however may also be used for other purposes.



**Code Amendment**  
**Office Warehouse Additional Standards Exemption**  
**March 26, 2026**  
**PUBLIC HEARING**

Applicant:	City Initiated
Location:	City wide
Type of Action:	Legislative
Land Use Authority:	City Council
Planner:	Kendal Black, Planner II

**A. Executive Summary and Request:**

This is a staff-initiated code amendment related to the Additional Standards for Office Warehouse (OW), Heavy Commercial (HC), Light Industrial (LI), and Industrial (I) zones (19.16.08). Currently the code requires that all buildings, regardless of size, are required to adhere to the Additional Standards section that are meant to “reduce the perceived mass of the building.” This makes sense for wide and large warehouse buildings but may not make as much sense for smaller commercial buildings that are allowed in some of these zones. Staff is proposing to allow exceptions under specific circumstances

**Recommendation:**

**Staff recommends that the Planning Commission conduct a public hearing on the application, take public comment, review and discuss the proposal, and choose from the options in the Recommendation and Alternatives Section of this report.** Options include a positive recommendation with or without conditions, a negative recommendation, or continuation.

**B. Background:**

Recent proposals for smaller buildings in the OW zone have identified a need to be able to grant exceptions to the additional standards requirement in 19.16.08. Title 19 does not currently allow exceptions to the additional standards requirement for smaller buildings. Chapter 19.16 already provides the architectural standards that all non-residential buildings are required to follow. This exception would allow smaller buildings (15,000 square feet in area or smaller and 200 feet in width or less) to build as a normal commercial building, with those architectural standards. Currently, the OW, HC, and LI zones allow most of the uses also allowed in the Regional Commercial zone (we do not currently have any properties zoned LI). This means restaurants, retail stores, and many of the other businesses usually located in the RC zone can be built in the OW, HC, and LI zones as well. An exception would allow consistent

design standards for similar uses and not require additional requirements for buildings that are a fraction of the building size this code was intended for.

**C. Specific Request:**

This is a request for approval of proposed code amendments to Section 19.16 of the Land Development Code, as summarized below and as attached.

<b>19.16.08, Site and Architectural Design Standards</b>	Adding an exception that buildings 15,000 square feet or less AND 200 feet wide or less at the front elevation are not required to comply with the Office Warehouse, Heavy Commercial, Light Industrial, and Industrial Additional Standards in 19.16.08 (2).
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**D. Process:**

Section 19.17.03 outlines the process criteria for Planning Commission and City Council Review:

1. The Planning Commission shall review the petition and make its recommendations to the City Council within thirty days of the receipt of the petition.  
*Complies. This is a staff-initiated proposal. The application will be reviewed by the Planning Commission and receive a recommendation prior to review by the City Council.*
2. The Planning Commission shall recommend adoption of proposed amendments only where it finds the proposed amendment furthers the purpose of the Saratoga Springs Land Use Element of the General Plan and this Title.  
*Complies. Please see Sections F and G of this report.*
3. The Planning Commission shall provide the notice and hold a public hearing as required by the Utah Code and Chapter 19.13. For an application which concerns a specific parcel of property, the City shall provide the notice required by the Utah Code and Chapter 19.13 for a public hearing.  
*Complies. Please see Section E of this report.*

**E. Community Review:**

This has been noticed as a public hearing pursuant to City and State statutes, which requires posting notice on the Utah public notice website and the City’s website and in City Hall, and, except for code amendments, mailing notices to property owners whose land is directly affected by the request and property owners within 300 feet of the subject property at least 10 calendar days prior to the public hearing.

Public Comment: As of the date of this report, no public input has been received.

**F. General Plan:**

The proposed amendments, as they relate to the 2022-2042 General Plan Vision, Goals and Strategies for Land Use & Neighborhoods, are evaluated below.

**Land Use and Neighborhoods, The Vision**

“Land Use and Neighborhoods supports the general plan vision by preserving existing neighborhoods and requiring new attractive, healthy, and family-friendly neighborhoods. Neighborhoods will have a variety of housing types and amenities. As new development occurs, it will be supported by appropriate services and amenities, ensuring a high quality of life for existing and future residents.”

**Land Use Goal**

Future development in Saratoga Springs reflects the community’s preferred vision.

***Staff conclusion: Consistent.** The amendments ensure consistent design standards for buildings of similar scale and use in site plans, by incorporating a necessary exception for commercial design features.*

**G. Code Criteria:**

**Code amendments are a legislative decision and grant the City Council significant discretion when considering changes to the Code.**

The criteria for an ordinance are outlined below and act as guidance to the Council and to the Commission in making a recommendation. Note that the criteria are not binding.

**19.17.05 Consideration of General Plan, Ordinance, or Zoning Map Amendment**

The Planning Commission and City Council shall consider, but not be bound by, the following criteria when deciding whether to recommend or grant a general plan, ordinance, or zoning map amendment:

1. The proposed change will conform to the Land Use Element and other provisions of the General Plan;  
***Consistent.** See Section F of this report.*
  
2. the proposed change will not decrease nor otherwise adversely affect the health, safety, convenience, morals, or general welfare of the public;  
***Consistent.** The amendments will not adversely affect the health and welfare of the general public and will allow for exceptions in the design of commercial sites and buildings.*
  
3. the proposed change will more fully carry out the general purposes and intent of this Title and any other ordinance of the City; and

**19.01.04. Purpose.** This section identifies the purpose of Title 19.

1. The purpose of this Title, and for which reason it is deemed necessary, and for which it is designed and enacted, is to preserve and promote the health, safety, morals, convenience, order, fiscal welfare, and the general welfare of the City, its present and future inhabitants, and the public generally, and in particular to:
  - a. encourage and facilitate the orderly growth and expansion of the City;
  - b. secure economy in governmental expenditures;
  - c. provide adequate light, air, and privacy to meet the ordinary or common requirements of happy, convenient, and comfortable living of the municipality's inhabitants, and to foster a wholesome social environment;
  - d. enhance the economic well-being of the municipality and its inhabitants;
  - e. facilitate adequate provisions for transportation, water, sewer, schools, parks, recreation, storm drains, and other public requirements;
  - f. prevent the overcrowding of land, the undue concentration of population, and promote environmentally friendly open space;
  - g. stabilize and conserve property values;
  - h. encourage the development of an attractive and beautiful community; and
  - i. promote the development of the City of Saratoga Springs in accordance with the Land Use Element of the General Plan.

***Consistent.** The proposed amendments will improve clarity in the code and contribute to a streamlined development review process both benefiting the City, developers, and the public.*

4. in balancing the interest of the petitioner with the interest of the public, community interests will be better served by making the proposed change.

***Consistent.** The amendments will provide additional clarity and effectiveness of the Code and better enhance the consistency in development review.*

5. any other reason that, subject to the legislative discretion of the City Council, could advance the general welfare.

**H. Recommendation and Alternatives:**

Staff recommends that the Planning Commission conduct a public hearing, take public input, discuss the application, and choose from the following options.

**Option 1 – Positive Recommendation**

“I move that the Planning Commission forward a recommendation for approval of the requested Code Amendment to Chapter 19.16, with the Findings and Conditions in the Staff Report.”

**Findings**

1. The application is consistent with the General Plan, as articulated in Section F of the staff report, which section is incorporated by reference herein.
2. The application complies with the criteria in section 19.17.05 of the Land Development Code, as articulated in Section G of the staff report, which section is incorporated by reference herein.

**Conditions:**

1. Any other conditions or changes as articulated by the Planning Commission:

\_\_\_\_\_.

**Option 2 – Continuance**

“I move to **continue** the Code Amendment to Chapter 19.16 to another meeting on [DATE], with direction to the applicant and Staff on information and/or changes needed to render a decision, as follows:

1. \_\_\_\_\_
2. \_\_\_\_\_

**Option 3 – Negative Recommendation**

“I move that the Planning Commission forward a recommendation for denial of the requested Code Amendment to Chapter 19.16, with the Findings below:

1. The application is not consistent with the General Plan:
  - a. \_\_\_\_\_, and/or,
2. The application is not consistent with Section {XX.XX} of the Code:
  - a. \_\_\_\_\_, and/or

**I. Exhibits:**

1. Proposed Code Amendment

## Chapter 19.16. Site and Architectural Design Standards

### 19.16.08. Office Warehouse, Heavy Commercial, Light Industrial, and Industrial Additional Standards.

#### **19.16.08. Office Warehouse, Heavy Commercial, Light Industrial, and Industrial Additional Standards.**

In addition to the non-residential requirements of this Chapter, developments in the Office Warehouse, Heavy Commercial, Light Industrial and Industrial Zones and all buildings that are larger than 15,000 square feet in area and wider than 200' on the front elevation shall be subject to the additional standards below:

1. “Warehouse/Flex” Use Standards. a. Each tenant shall designate at least 10 percent of the floor area for office and/or retail use. The required office and/or retail area may be divided among multiple levels. For example, if 10 percent of the floor area equals 10,000 square feet this area may be split over multiple stories.
2. Additional Architectural Standards.
  - a. All buildings shall have architectural elements to provide visual interest and reduce the perceived mass of the building. Both horizontal and vertical articulation is required on all sides of the building.