



NOTICE AND AGENDA BRIGHAM CITY PLANNING COMMISSION

Tuesday, March 17, 2026, 6:00 PM
City Council Chambers, 20 North Main

- 6:00 Pledge of Allegiance
- 6:02 Approval of Minutes
- 6:05 Application # 26-014 / Preliminary Plat / Heron Ridge Subdivision – Avian Shores Planned District / Approximately 650 South and 1200 West / Garth Day, Heritage Land Development
- 6:25 Application # 26-015 / Preliminary Plat / Sandpiper Meadows Subdivision – Avian Shores Planned District / Approximately 250 South and 1200 West / Garth Day, Heritage Land Development
- 6:40 Discussion Items
 - A. Private Property and the Development of Affordable Single-Family Housing / Randy David Navarro

In compliance with the Americans with Disabilities Act, persons needing auxiliary communicative aids or services for this meeting should contact Destry Larsen (435-734-6604) at least 48-hours in advance of the meeting.

CERTIFICATE OF POSTING

The above notice and agenda were posted in three places within Brigham City Limits. A copy was also provided to the Box Elder News Journal and posted on the City Website and the State Public Meeting Notice website on March 5, 2026

**BRIGHAM CITY, UTAH
PLANNING COMMISSION
STAFF EVALUATION**

APPLICANT: Garth Day
OWNER'S NAME: Heritage Land Development
PROPERTY ADDRESS: Approx. 660 S 1225 W
PARCEL NO.: 03-112-0038

APPLICATION NO.: 26-014
PARCEL AREA: 9.9 acres
ZONING DISTRICT: P-R-M-7
DATE: March 12, 2026

PLANNING COMMISSION MEETING: March 17, 2026
APPLICATION TYPE: Legislative
PLANNING COMMISSION ROLE: Recommending Body to City Council

NATURE OF REQUEST:
Subdivision Preliminary Plat – Heron Ridge

OVERVIEW:

The City has received a preliminary plat application (#26-014) for a residential subdivision within the Avian Shores Planned District. The proposed subdivision will consist of 63 residential units, including 23 single-family dwelling lots and 40 townhome units.

The proposed subdivision includes a combination of public streets and private drives. The single-family dwelling lots will be accessed from publicly dedicated roads, while the townhome units will be served by private drives extending from the proposed 660 South street. Primary access to the subdivision will be provided from 1200 West.

This subdivision represents one phase of the overall Avian Shores Planned District development. The Planned District allows for a mixture of housing types and flexibility in lot sizes while maintaining an overall density of seven (7) dwelling units per acre across the development area.

STAFF COMMENTS:

Community and Economic Development Department:

1. Standards for density, height regulations, and parking, shall be governed by the standards of the R-M-7 District. The Planned District provides flexibility for lot sizes and yard requirements (setbacks) to allow a variety of lot sizes and housing type, and for compatible design with mixed-uses, and street scale. Design flexibility must provide a variation of lot sizes. The overall density of the Planned District area, excluding the City properties, is limited to a maximum of 7 dwelling units per acre.
 - A. Recommend setbacks be established for the single-family dwelling lots. Developer is proposing a 20 ft front, 5 ft side, and 15 ft rear.
 - B. Recommend a minimum setback along 1200 West Street corridor be 25 feet for both townhomes and single-family lots.
 - C. Concerned with the proposed lot sizes backing up against 1200 West Street. Would like to see a footprint plan with the proposed lots to show how setbacks can be met.
2. Landscape and screening plan shall be submitted with the final design and construction plans for Planning Commission review and approval.

3. Fencing Plan. The fencing plan shall be submitted with the final design and construction plans for Planning Commission review and approval.
 - A. Recommend concrete curbing with wrought iron fence along with aesthetic structural columns spaced uniformly along 1200 West where townhome units face 1200 West.
 - B. Recommend other fencing through out the development be discussed. Focus is primarily on the 1200 West Street Corridor, but there are other areas of consideration (i.e. along trails, open space, etc.)
4. 1200 West Street Corridor. Developer shall install a low maintenance safety barrier fence or wall to ensure privacy and safety. Said fence or wall design is to be coordinated and approved by the Brigham City Staff DRT prior to installation
 - A. A privacy wall along 1200 West Street is required for single-family lots. Developer would like to use concrete panel like installed at Beeton Path Subdivision by the railroad. The wall design is to be coordinated and approved by the Brigham City Staff DRT prior to installation.
 - B. Recommend a different concrete panel for this corridor to be used. Requires additional discussion with the Staff DRT.
5. Recommend increasing the setback along 1200 West from ten (10) feet to a minimum of twenty-five (25) feet.
6. Recommend updating the Avian Shores Master Plan color coordination to reflect the proposed housing types within this phase.
7. Support the Fire Department recommendation to provide full road width along the west side of the townhomes to ensure adequate emergency access.
8. Support the Fire Department recommendation for a secondary access east of 1200 West connecting to the Spring Creek development for emergency access and egress.
9. Street numbers. Street numbers must be a whole, half, three-quarters, or one-quarter numbers.
 - A. 660 South and 730 South must be revised.
 - B. Provide street number for private drive.
10. Trail and Open Space.
 - A. Include the Regional Trail detail along the south properties (Lots 11-15).
 - B. Show trail will interface with lots.
11. Drawing No. 2. Typical Drive Aisle Cross-Section.
 - A. Label as Private Drive.
 - B. Please include the section widths of the roadway and driveways.

Engineering Department:

1. The setback of the townhomes along 1200 W. needs to be greater than 10 feet. The PUE along 1200 W. is 15 feet. Please coordinate with Mark Bradley in determining the setback.
2. Please note that the proposed townhomes are only wide enough for single vehicle parking within the garage. Please ensure that your parking calculations include one vehicle in the garage per unit.
3. We will review the geotechnical report and provide any necessary feedback.
4. Please ensure that the half-width roadway includes 26' of paved surface. The gutter pan can be included within that distance.
5. Please update the 1200 W. cross section you provided to match the City Standard 1200 W. cross section on SHT R3 of the City Standard Drawings.
6. We will provide a thorough review of the utilities when construction drawings are provided.

Fire Department:

1. We need to have a full road width on the west side of the townhomes to ensure adequate emergency access and egress.
2. Additionally, we will need to have the second access, east of twelve hundred, that will tie into the Spring Creek development

Police Department:

1. No comments

Public Power Department:

1. If the power pole needs to be moved for the road placement, there will be an additional cost.
2. Power Dept needs CAD drawings for the power plan.

Public Works Department:**(Culinary Water, Street, and Wastewater Treatment/ Storm Drain Divisions)**

1. Public Works supports the application with no additional comments.
2. See Engineering Department comments.

PLANNING COMMISSION RESPONSIBILITY:

1. The Planning Commission considers the application and determines whether the proposed subdivision plat is in harmony with the Avian Shores Planned District. *The purpose and intent of establishing the Avian Shores Planned District is to create a dynamic and vibrant area that fosters the orderly development of residential uses along with potential neighborhood commercial uses that would service the residential area. ... The district's development plan provides general guidance on the location and density of residential units and land uses compatible with adjacent properties, ensuring adequate infrastructure and services. City Code 156.285 Purpose and Intent*
2. The Planning Commission is the recommending body to the City Council for this application. Process established with the Planned District.
3. The Planning Commission may recommend approval, disapproval, or request additional information to make a recommendation to the City Council.

STAFF RECOMMENDATIONS:

1. Recommend the Planning Commission review the application against the Avian Shores Planned District.
2. Recommend the applicant present the application, discuss the project, timing, and details.
3. Recommend the application be continued to allow the applicant time to provide a more complete application to advance the project in accordance with the Avian Shores Planned District.

Clean up application

Show updated details

Determine setbacks

Timing for amending the Planned District

Recommended reviewing Avian Shores Planned District against plans ...

STIPULATIONS:

1. Update plans to address Staff Comments
2. Provide details to address items note in application submission.
3. The landscaping and screening plan, and the fencing plan shall be submitted with the final design and construction plans for Planning Commission review and approval.

FINDINGS OF FACT:

1. The proposed subdivision is in the Avian Shores Planned District.
 2. The Avian Shores Planned District was adopted by the City Council on November 6, 2026 as a P-R-M-7 zoning district.
 3. The Avian Shores Planned District allows for townhomes and variation of single-family dwelling lots.
 4. The Avian Shores Planned District allows for an overall density of 7 dwelling units per acre.
 5. Additional details on certain design elements must be worked out and finalized
 6. The Heron Ridge subdivision proposes sixty-three (63) residential units, including twenty-three (23) single-family dwelling lots and forty (40) townhome units.
 7. The subdivision includes a combination of public streets and private drives, with townhomes accessed from private drives extending from 660 South.
 8. Primary access to the subdivision will be from 1200 West.
 9. The preliminary plat establishes the general layout of lots, streets, and access, with additional details to be addressed during final plat review.
 10. The development includes design elements along the 1200 West corridor, including proposed fencing treatments and setback considerations intended to address the interface between the subdivision and the arterial roadway.
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ATTACHMENTS:

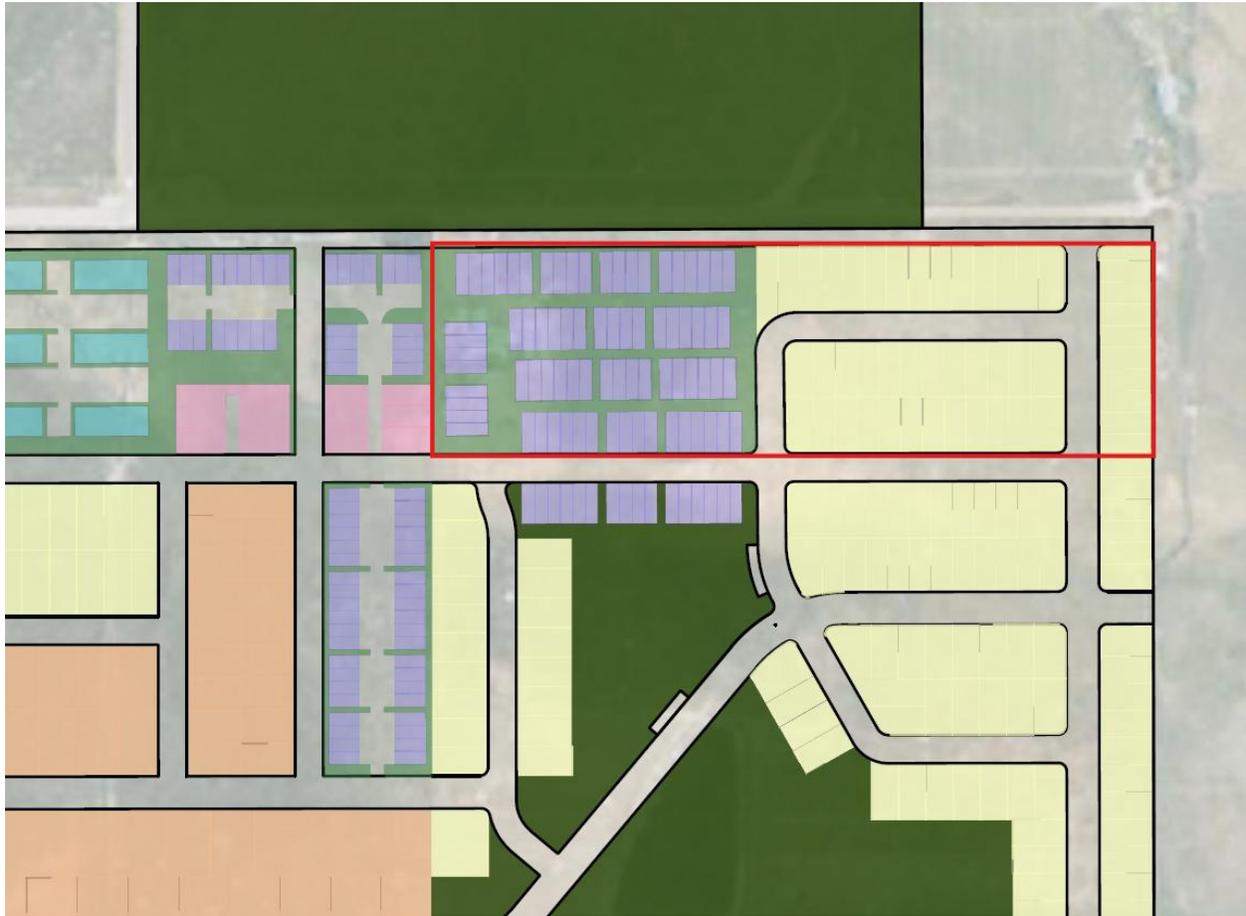
1. Locator Map
2. Avian Shores Planned District
3. Fence Detail
4. Heron Ridge Preliminary Plat and Drawings
5. Elevations (Emailed Electronically)
6. Landscape Architect Illustrations (Emailed Electronically)

LOCATOR MAP



AVIAN SHORES MASTER PLAN – SITE LOCATION

Heron Ridge Subdivision Area



Ordinance Exhibit "A"

AVIAN SHORES PLANNED DISTRICT

§ 156.285 PURPOSE AND INTENT.

The purpose and intent of establishing the Avian Shores Planned District along 1200 West Street and 400 South Street is to create a dynamic and vibrant area that fosters the orderly development of residential uses along with potential neighborhood commercial uses that would service the residential area. This district is intended to support Brigham City and Box Elder County's economic growth by promoting residential density and job creation. The district's development plan provides general guidance on the location and density of residential units and land uses compatible with adjacent properties, ensuring adequate infrastructure and services.

§ 156.286 PLANNED DISTRICT.

The Avian Shores Planned District shall apply to specific properties within Brigham City, located in the general area of 1200 West Street and 400 South Street as shown in Exhibit "A."

(A) Zoning District established. P-R-M-7 (Planned – Multiple Residential).

(B) Uses. The following uses are allowed within the P-R-M-7 District, which otherwise may not be listed in the underlying R-M-7 zoning district:

- (1) The underlying zone is Multiple Residential District R-M-7. All uses allowed under the same permitted or conditional use designation as in other Multiple Residential District R-M-7 districts are hereby allowed in this Planned District, except for limitations in subsection (D).
- (2) Additional permitted uses allowed and incorporated in the P-R-M-7 District, as defined, as applicable, in subsection (C) of this section.
 - (a) Assisted Living Facility;
 - (b) Day Care Center (Non-Residential);
 - (c) Church; Religious Institution;
 - (d) Condominiums;
 - (e) Community Recreation Facility;
 - (f) Educational Institution;
 - (g) Live-Work Units;

- (h) Neighborhood Commercial (Not a vested use. Requires additional planning, establishment of standards and regulations, and amendment to this Planned District prior to submission of any application);
- (i) Townhomes.

(C) Definitions. The following terms used in subsection (B) and (D) of this section, and not defined in Brigham City Code Chapter 156 Zoning, have these specific meanings within the Avian Shores Planned District area:

(1) COMMUNITY RECREATION FACILITY. A public or semi-private space designed to support recreational, social, educational, and physical activities for residents within the community. The facility is operated either by the City or an established Homeowner's Association (HOA), aimed to improve the well-being, health, and social connection of residents, and typically accessible to residents of all ages and abilities, promoting inclusion, healthy lifestyles, and community engagement.

(2) LIVE-WORK UNITS. A type of building or space designed to combine residential and commercial uses within the same unit. These spaces allow individuals to live and work in the same location, typically with a layout that accommodates both living quarters and workspace, such as an office, studio, or retail area. A live-work unit enables an owner to use the commercial space as an office, studio, or retail, or rent it to another business. The commercial space must be designed on ground floor with the residential space above. Live-work units can play an integral role in creating vibrant, diverse, walkable neighborhoods.

(D) Limitations (uses not allowed within the Planned District).

(1) Any type of dwelling not platted and recorded with the Box Elder County Recorder's office for home ownership.

(E) General Development Plan. The submitted site master plan and open space and trail master plan incorporated as Exhibits "A" and "B" respectively, shall serve as the general development plan for the Avian Shores Planned District. The development shall include a variety of housing types, including single-family lots, townhomes, and condominiums. A balanced housing mix shall be maintained to ensure compatibility with adjacent land uses and to support a diverse neighborhood. Minor modifications of the housing mix (excluding apartments) based on market demand are permissible provided all changes remain consistent with the Planned District standards and regulations. Significant revision to the development plan requires an amendment to this Planned District. The applicable land use application for final design and construction plans shall be submitted for Planning Commission review and approval as outlined in the Brigham City land use code. The exact layout and number of units may differ from the submitted site plan based upon site design adjustments, parking requirements, open space and trails. Any school (educational institution) or church (religious institution) sites, which are encouraged to be located in residential neighborhoods, does not constitute an amendment to the Planned District, but does require the plans to be submitted for Planning Commission review and approval as outlined in the Brigham City land use code.

§ 156.286 STANDARDS AND REGULATIONS.

The following provisions apply specifically to this Planned District. While the district is also subject to the standards and regulations outlined in the Brigham City Code, if there is any conflict between those standards and regulations the provisions stated within this section or on the general development plan, the Planned District standards and regulations or the approved general development plan shall apply.

(A) Standards for density, height regulations, and parking, shall be governed by the standards of the R-M-7 District. The Planned District provides flexibility for lot sizes and yard requirements (setbacks) to allow a variety of lot sizes and housing type, and for compatible design with mixed-uses, and street scale. Design flexibility must provide a variation of lot sizes. The overall density of the Planned District area, excluding the City properties, is limited to a maximum of 7 dwelling units per acre.

(B) Standards for public improvements shall be governed by applicable adopted city ordinances and laws, including the current adopted edition of the Brigham City Public Work Standards.

(C) Landscaping and screening.

(1) Landscaping standards shall be governed by the standards of the R-M-7 District.

(2) Plantings shall include drought tolerant designs where appropriate.

(3) The landscape and screening plan shall be submitted with the final design and construction plans for Planning Commission review and approval.

(4) Trees adequately spaced and separated throughout the residential areas of the Planned District, according to a final landscaping plan approved by the City Forester.

(D) Architectural controls. New building construction is intentionally allowed to be flexible, creative, and evolve with modern architectural designs. The architecture shall substantially follow the thematic elevations in Exhibit "C". Modifications, especially with residential industrial building standards, can change over time. In that event, a variation to the thematic elevations provided in Exhibit "C" is allowed. Where there are significant and uncharacteristic changes, new elevations are to be submitted for Planning Commission consideration for approval.

(E) Fencing plan. The fencing plan shall be submitted with the final design and construction plans for Planning Commission review and approval. See 1200 West Street Corridor subsection (H), for specific fence and/or wall requirements.

(F) Phasing plan. Phasing shall be permitted. Phasing plans shall be submitted with the final design and construction plans for Planning Commission review and approval.

(G) Subdivision Platting. All phases of the development shall be processed and recorded as standard subdivisions. Townhome and condominium phases shall be platted and recorded to support individual ownership and utility service.

(H) 1200 West Street Corridor.

(1) Direct access onto or from 1200 West Street is not allow, except for public streets and utility parcels as approved by City Public Works Director and City Engineer. 1200 West Street is designated as a limited access corridor;

(2) Developer shall install a low maintenance safety barrier fence or wall to ensure privacy and safety. Said fence or wall design is to be coordinated and approved by the Brigham City Staff DRT prior to installation;

(3) Landscaping along 1200 West shall meet Brigham City's 1200 West Street corridor standards. Developer is responsible for installing the landscape and City is responsible to maintain the landscape.

(I) Trail Master Plan. The alignment of trails within the development shall be generally consistent with the layout depicted in the trail master plan attached as Exhibit "B". It is understood that the plan may be modified as approved by the Brigham City DRT (Development Review Team) as development progresses to accommodate subdivision layouts, topographical conditions, and accessibility requirements. The overall objective is to establish a public trail system with potential connections to external trail networks beyond the boundaries of the Planned District.

(1) Developer to install trails.

(2) City to maintain trails once dedicated to the City.

(3) The surface for City dedicated trails shall be concrete for trails less than 8 feet in width, asphalt for trails 8 feet and greater, unless otherwise approved by the Brigham City DRT, and except for the Historic Orchard Path which is a 10-foot-wide concrete surface pathway.

(4) Trails labeled as "Potential Trail Alignment" identifies a potential trail location connecting to the trails in the Planned District at a future time and requires property owner approval and coordination.

(J) Recreation and playground areas. Usable recreation area standards for the residential parcels will be as required under the R-M-7 District, subject to the following superseding clarifications: (i) any on-site, above grade storm drainage detention basin may count toward a portion of the recreation area if the applicant provides sufficient evidence that the basin is safe for recreational purposes and usable pertaining to the area, slope and landscape, (ii) indoor recreational facilities shall count toward the required square footage and may include club rooms, fitness areas, spas, game rooms and other similar facilities, (iii) pathways or trails within the designated areas may be included in the calculation (although portions of sidewalks which border a parking lot and/or building are not considered recreation area).

(K) Street Connectivity.

(1) Streets shall connect throughout the neighborhoods within the Planned District.

(2) Cul-de-sacs are not allowed unless there is no feasible way to connect to another street due to a physical natural barrier.

(3) Stub streets shall be extended to the end of the Planned District boundary.

(L) Signage.

(1) A master sign plan, which may include neighborhood identification signs, trail signs, and wayfinding signs shall be submitted for Brigham City Staff DRT review and approval.

All other signage, including neighborhood commercial and live-work units, shall comply with City Code sign regulations.

(2) Traffic control signs and street signs shall comply with adopted City standards and policy.

(M) Outdoor Lighting. Subject to Brigham City Code Section 156.361 Outdoor Lighting Standards.

§ 156.287 EXPANSION OR ANNEXATION OF ADDITIONAL PROPERTY.

(A) The Developer may apply to expand the boundaries of the Avian Shore Planned District or annex additional property into the District as the Developer acquires more property adjacent to the current district boundaries. Any expansion or annexation shall be subject to approval by the Brigham City Council. The inclusion of additional property shall be incorporated into the terms of this District and shall adhere to the provisions outlined herein, as applicable.

(B) City-Owned Property Inclusion. If the City identifies adjacent or internal City-owned property as advantageous for development or community benefit, such property may be incorporated into the Avian Shores Planned District, subject to Brigham City Council approval, and may include park or recreational amenities, fee-in-lieu contributions, or open-space improvements.

§ 156.288 CONFLICTS AND SEVERABILITY.

(A) Conflict. Whenever regulations in these sections conflict with other provisions of the Brigham City Code, the provisions of these sections shall govern. If there is a conflict in these sections with the development plan, the development plan shall prevail. Whenever regulations of these sections conflict with address matters that are not addressed by the development plan, then said provisions of these sections shall govern. Whenever regulations of other provisions of the Brigham City Code address matters that are not addressed by these sections, then said other provisions of Brigham City Code shall govern.

(B) Severability. These sections and the various parts, subsections, and clauses are hereby declared to be severable. If any part, section, paragraph, sentence, clause or phrase is adjudged to be unconstitutional or invalid, it is hereby declared that the remainder of the sections shall not be affected thereby. The City Council of Brigham City, Utah, hereby declares that it would have passed these sections on each part, subsection, paragraphs, sentence, clause or phrase hereof, irrespective of the fact that any one or more portions thereof be declared invalid.

§ 156.289 EXHIBITS.

Exhibit "A" Legal Description and General Development Plan

Exhibit "B" Open Space and Trail Master Plan

Exhibit "C" Elevations

Exhibit "A"

Avian Shores Planned District Legal Descriptions and Site Map / General Development Plan

03-112-0006

SE/4 OF NE/4 SEC 22, T 09N, R 02W, SLM.

ALSO: THE EAST 2.0 RDS OF W/2 OF SE/4 SEC 22, T 09N, R 02W, SLBM.

~~ALSO: THE SOUTH 10 RDS OF W/2 OF NE/4 SEC 22, T 09N, R 02W, SLBM.~~

LESS: THE SOUTH 4.0 RDS AND NORTH 3.0 RDS OF THE SE/4 OF NE/4 SEC 22, T 09N, R 02W, SLM.

Note: The strike through is the portion of property west of 1400 West to remain as A-5

03-112-0030

NE/4 OF SE/4 OF SEC 22, T 09N, R 02W, SLBM.

LESS: [03-112-0029] TRACT DEEDED TO BRIGHAM CITY CORPORATION. PART OF THE NE/4 OF THE SE/4 OF SECTION 22, T 09N, R 02W, SLBM. BEGINNING AT THE NE CORNER OF SAID SE/4 BEING A POINT LOCATED 2649.30 FT S 00°14'42" E ALONG THE EAST LINE OF SAID SEC FROM THE NE CORNER OF SAID SEC 22; THENCE S 00°14'42" E 1328.23 FT TO THE SE CORNER OF THE NE/4 OF SAID SE/4; THENCE S 90°00'00" W 32.09 FT ALONG THE SOUTH LINE OF THE NE/4 OF SAID SE/4 TO THE NEW WEST R/W LINE OF 1200 WEST ST; N 00°15'29" W 1328.23 FT ALONG SAID NEW WEST R/W LINE TO THE NORTH LINE OF THE NE/4 OF SAID SE/4; N 90°00'00" E 32.39 FT TO THE POINT OF BEGINNING.

03-112-0034

A PART OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 9 NORTH, RANGE 2 WEST OF THE SALT LAKE BASE AND MERIDIAN. BEGINNING AT A POINT ON THE BOUNDARY OF BOX ELDER COUNTY REDEVELOPMENT AGENCY PROPERTY, TAX ID NO. 03-112-0024 LOCATED 33.00 FEET NORTH 89°54'27" WEST ALONG THE NORTH LINE OF SAID NORTHEAST QUARTER AND 1324.86 FEET SOUTH 00°15'29" EAST AND 369.41 FEET NORTH 89°54'02" WEST FROM THE NORTHEAST CORNER OF SAID SECTION 22; RUNNING THENCE ALONG SAID BOUNDARY THE FOLLOWING TWO (2) COURSES: (1) NORTH 89 54'02" WEST 937.77 FEET; AND (2) NORTH 00°12'20" WEST 336.24 FEET; THENCE SOUTH 89°54'02" EAST 462.04 FEET; THENCE SOUTH 00°05'58" WEST 75.90 FEET; THENCE SOUTH 44°40'54" EAST 255.51 FEET; THENCE SOUTH 89°54'02" EAST 297.54 FEET; THENCE SOUTH 00°05'58" WEST 78.98 FEET TO THE POINT OF BEGINNING

03-112-0036

A PART OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 9 NORTH, RANGE 2 WEST OF THE SALT LAKE BASE AND MERIDIAN. BEING THE NORTH 3 RODS OF THE SOUTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 9 NORTH, RANGE 2 WEST SALT LAKE BASE AND MERIDIAN.

LESS: BEGINNING AT THE NORTHEAST CORNER OF THE SOUTHEAST QUARTER OF SAID NORTHEAST QUARTER BEING A POINT LOCATED 1324.65 FEET SOUTH 00°14'42" EAST ALONG THE EAST LINE OF SAID NORTHEAST QUARTER FROM THE NORTHEAST CORNER OF SAID SECTION 22; RUNNING THENCE SOUTH 00°14'42" EAST 49.50 FEET ALONG SAID WEST LINE TO GRANTORS SOUTH BOUNDARY LINE; THENCE SOUTH 89°59'37" WEST 32.69 FEET ALONG SAID SOUTH BOUNDARY LINE TO THE NEW WEST RIGHT-OF-WAY LINE OF 1200 WEST STREET; THENCE NORTH 00°15'29" WEST 49.50 FEET ALONG SAID NEW WEST RIGHT-OF-WAY LINE TO THE NORTH LINE OF THE SOUTHEAST QUARTER OF SAID NORTHEAST QUARTER BEING GRANTORS NORTH BOUNDARY LINE; THENCE NORTH 89°59'27" EAST 32.70 FEET ALONG SAID NORTH BOUNDARY LINE TO THE POINT OF BEGINNING.

03-112-0038

EAST HALF OF THE FOLLOWING DESCRIBED PARCEL:

SE/4 OF THE SE/4 OF SEC 22, T 09N, R 02W, SLBM.

LESS: [03-112-0025] PART OF THE SE/4 OF THE SE/4 OF SEC 22, T 09N, R 02W, SLBM. BEGINNING AT THE SE CORNER OF SAID SEC 22 AND RUNNING THENCE N 90°00'00" W 31.78 FT ALONG THE SOUTH LINE OF SAID SE/4 TO NEW WEST R/W LINE OF 1200 WEST ST; N 00°15'29" W 1325.84 FT ALONG SAID NEW WEST R/W LINE TO THE NORTH LINE OF THE SE/4 OF SAID SE/4; N 90°00'00" E 32.09 FT ALONG SAID NORTH LINE TO THE EAST LINE OF SAID SEC 22; S 00°14'42" E 1325.84 FT ALONG SAID EAST LINE TO THE POINT OF BEGINNING.

03-112-0039

WEST HALF OF THE FOLLOWING DESCRIBED PARCEL:

SE/4 OF THE SE/4 OF SEC 22, T 09N, R 02W, SLBM.

LESS: [03-112-0025] PART OF THE SE/4 OF THE SE/4 OF SEC 22, T 09N, R 02W, SLBM. BEGINNING AT THE SE CORNER OF SAID SEC 22 AND RUNNING THENCE N 90°00'00" W 31.78 FT ALONG THE SOUTH LINE OF SAID SE/4 TO NEW WEST R/W LINE OF 1200 WEST ST; N 00°15'29" W 1325.84 FT ALONG SAID NEW WEST R/W LINE TO THE NORTH LINE OF THE SE/4 OF SAID SE/4; N 90°00'00" E 32.09 FT ALONG SAID NORTH LINE TO THE EAST LINE OF SAID SEC 22; S 00°14'42" E 1325.84 FT ALONG SAID EAST LINE TO THE POINT OF BEGINNING.

03-114-0021

BEGINNING AT POINT LOCATED SOUTH 3391.2 FT FROM NW CORNER SEC 23, T 09N, R 02W, SLM. THENCE SOUTH 1466.5 FT ALONG WEST LINE SAID SEC; SOUTH 89°42'46" EAST 1425.9 FT; NORTH 01°03'14" WEST 763.8 FT; NORTH 85°58'19" EAST 897.6 FT TO WESTERLY LINE OF OSLRR R/W; NORTH 27°03'38" EAST 772.6 FT ALONG SAID R/W; NORTH 89°32'16" WEST 2664.1 FT TO POB. ALL COURSES BEING ALONG EXIST FENCE LINES.

LESS [03-108-0116]: A PART OF THE NORTHEAST QUARTER AND SOUTHEAST QUARTER OF SECTION 10 AND A PART OF THE NORTHWEST QUARTER AND SOUTHWEST QUARTER OF SECTION 14 AND A PART OF THE NORTHEAST QUARTER AND SOUTHEAST QUARTER OF SECTION 15 AND A PART OF THE NORTHEAST QUARTER AND SOUTHEAST QUARTER OF SECTION 22 AND A PART OF THE NORTHWEST QUARTER AND SOUTHWEST QUARTER OF SECTION 23, TOWNSHIP 9 NORTH, RANGE 2 WEST OF THE SALT LAKE BASE AND MERIDIAN. BEGINNING AT THE INTERSECTION OF THE NEW WEST RIGHT-OF-WAY LINE OF 1200 WEST STREET AND THE SOUTH RIGHT-OF-WAY OF THE OREGON SHORT LINE RAIL ROAD LOCATED 2435.02 FEET SOUTH 00°23'59" WEST ALONG THE WEST LINE OF SAID SECTION 10 AND 3093.22 FEET NORTH 90°00'00" EAST FROM THE NORTHWEST CORNER OF SAID SECTION 10, SAID POINT BEING LOCATED NORTH 74°31'13" WEST FEET FROM THE NORTHEAST CORNER OF LOT 1, AMEND AND EXTENDED JACK FRANCIS TRUCKING SUBDIVISION - ADDITION NO 1; RUNNING THENCE ALONG SAID NEW WEST RIGHT-OF-WAY LINE SOUTH 15°29'43" WEST 163.10 FEET; THENCE SOUTHERLY TO THE LEFT ALONG THE ARC OF A 876.21 FOOT RADIUS CURVE, A DISTANCE OF 748.00 FEET, CHORD BEARS SOUTH 08°57'37" EAST 725.49 FEET, HAVING A CENTRAL ANGLE OF 48°54'47"; THENCE SOUTH 33°25'00" EAST 467.00 FEET; THENCE SOUTH 33°31'33" EAST 1235.15 FEET; THENCE SOUTH 33°21'38" EAST 553.43 FEET; THENCE SOUTHEASTERLY TO THE RIGHT ALONG THE ARC OF A 19619.00 FOOT RADIUS CURVE, A DISTANCE OF 284.02 FEET, CHORD BEARS SOUTH 32°56'45" EAST 284.01 FEET, HAVING A CENTRAL ANGLE OF 00°49'46"; THENCE SOUTH 32°31'52" EAST 25.04 FEET, THENCE SOUTHEASTERLY TO THE LEFT ALONG THE ARC OF A 13189.33 RADIUS CURVE, A DISTANCE OF 273.10 FEET, CHORD BEARS SOUTH 33°07'28" EAST 273.10 FEET, HAVING A CENTRAL ANGLE OF 01°11'11"; THENCE SOUTH 33°43'03" EAST 163.36 FEET,

THENCE IN A SOUTHEASTERLY DIRECTION WITH A REVERSE TANGENT CURVE TO THE RIGHT OF A 864.44 FOOT RADIUS CURVE, A DISTANCE OF 310.81 FEET, CHORD BEARS SOUTH 24°22'16" EAST 309.14 FEET, HAVING A CENTRAL ANGLE OF 20°36'03"; THENCE SOUTH 14°04'14" EAST 519.26 FEET; THENCE SOUTHERLY TO THE RIGHT ALONG THE ARC OF A 2000.00 FOOT RADIUS CURVE, A DISTANCE OF 501.02 FEET, CHORD BEARS SOUTH 06°53'38" EAST 499.71 FEET, HAVING A CENTRAL ANGLE OF 14°21'12"; THENCE SOUTH 00°16'58" WEST 2966.35 FEET TO THE NORTH RIGHT-OF-WAY LINE OF THE FOREST STREET; THENCE SOUTH 00°34'57" WEST 103.97 FEET TO THE SOUTH RIGHT-OF-WAY LINE TO FOREST STREET; THENCE SOUTH 00°15'29" EAST 2853.03 FEET TO THE NORTH BOUNDARY LINE OF THE PETER MORRIS CLAWSON PROPERTY, TAX ID. NO. 03-112-0009; THENCE NORTH 90°00'00" EAST 14.50 FEET ALONG SAID NORTH BOUNDARY LINE TO AN EXISTING FENCE LINE LONG ESTABLISHED AS BEING THE ACCEPTED WEST RIGHT-OF-WAY LINE OF 1200 WEST STREET; THENCE ALONG SAID FENCE LINE SOUTH 00°07'18" WEST 66.00 FEET TO THE SOUTH BOUNDARY LINE OF SAID CLAWSON PROPERTY; THENCE NORTH 90°00'00" WEST 14.07 FEET ALONG SOUTH BOUNDARY LINE TO THE NEW EAST RIGHT-OF-WAY LINE OF 1200 WEST STREET; THENCE ALONG SAID NEW WEST RIGHT-OF-WAY LINE SOUTH 00°15'29" EAST 2654.07 FEET TO THE SOUTH LINE OF SAID SECTION 22; THENCE SOUTH 90°00'00" EAST 31.77 FEET ALONG SAID SOUTH LINE TO THE SOUTHEAST CORNER OF SAID SECTION 22; THENCE SOUTH 89°54'43" EAST 19.86 FEET ALONG THE SOUTH LINE OF SAID SECTION 23 TO AND EXISTING FENCE LINE; THENCE NORTH 75°22'58" EAST 56.11 FEET ALONG SAID FENCE LINE TO THE NEW EAST RIGHT-OF-WAY LINE OF 1200 WEST STREET; THENCE ALONG SAID NEW EAST RIGHT-OF-WAY LINE NORTH 00°15'29" WEST 5517.35 FEET TO THE SOUTH RIGHT-OF-WAY LINE OF FOREST STREET; THENCE NORTH 00°20'43" EAST 136.21 FEET TO THE NORTH RIGHT-OF-WAY LINE OF FOREST STREET; THENCE NORTH 00°16'58" EAST 2975.21 FEET; THENCE NORTHERLY TO THE LEFT ALONG THE ARC OF A 2106.00 FOOT RADIUS CURVE, A DISTANCE OF 527.58 FEET; CHORD BEARS NORTH 06°53'38" WEST 526.20 FEET, HAVING A CENTRAL ANGLE OF 14°21'12"; THENCE NORTHWESTERLY TO THE LEFT ALONG THE ARC OF A 19725.00 FOOT RADIUS CURVE, A DISTANCE OF 285.55 FEET, CHORD BEARS NORTH 32°56'45" WEST 285.55 FEET, HAVING A CENTRAL ANGLE OF 0°49'46"; THENCE NORTH 33°21'38" WEST 448.06 FEET (448.63' BY RECORD) TO THE NORTH BOUNDARY LINE OF THE BRIGHAM CITY CORPORATION PROPERTY TAX ID. NO. 03-075-0041, SAID POINT BEING THE SOUTHWEST CORNER OF THE NUCOR SUBDIVISION; THENCE NORTH 90°00'00" EAST 3.59 FEET ALONG THE SOUTH BOUNDARY LINE OF SAID NUCOR SUBDIVISION TO THE SOUTHWEST CORNER OF LOT 1 OF SAID NUCOR SUBDIVISION; THENCE ALONG THE WEST BOUNDARY LINE OF SAID LOT 1 AND THEN LOT 2 OF SAID NUCOR SUBDIVISION THE FOLLOWING THREE (3) COURSES; (1) NORTH 33°21'38" WEST 106.93 FEET; (2) NORTH 33°31'33" WEST 1235.20 FEET; AND (3) NORTH 33°25'00" 357.49 FEET TO THE NORTH BOUNDARY LINE OF SAID NUCOR SUBDIVISION, SAID POINT BEING ON THE SOUTH BOUNDARY LINE OF THE JACK FRANCIS TRUCKING SUBDIVISION; THENCE NORTH 87°54'17" WEST 3.69 FEET ALONG THE SAID SOUTH BOUNDARY LINE TO THE SOUTHWEST CORNER OF SAID JACK FRANCIS TRUCKING SUBDIVISION; THENCE ALONG THE WEST BOUNDARY LINE OF SAID JACK FRANCIS TRUCKING SUBDIVISION THE FOLLOWING THREE (3) COURSES; (1) NORTH 33°25'00" WEST 107.84 FEET; (2) NORTHERLY TO THE RIGHT ALONG THE ARC OF A 770.21 FOOT RADIUS CURVE, A DISTANCE OF 657.51 FEET, CHORD BEARS NORTH 08°57'37" WEST 637.73 FEET, HAVING A CENTRAL ANGLE OF 48°54'43"; AND (3) NORTH 15°29'43" EAST 163.14 FEET TO THE SOUTH RIGHT-OF-WAY LINE OF THE OREGON SHORT LINE RAIL ROAD; THENCE NORTH 74°31'13" WEST 106.00 FEET ALONG SAID SOUTH RIGHT-OF-WAY LINE TO THE POINT OF BEGINNING.

03-118-0008

BEG AT A POINT 388 FT M/L NORTH FROM SE CORNER OF NW/4 OF SEC 23, T 09N, R 02W, SLM; TO NORTH BANK OF DITCH. THENCE WESTERLY ALONG NORTH BANK OF DITCH 270 FT TO A LANE, N 15°00'00" W 150 FT, S 67°45'00" E 150 FT, SOUTHEASTERLY 170 FT M/L TO 1/4 SEC LINE, SOUTH 63 FT M/L TO BEG.

03-118-0021

BEG AT A POINT 25.39 CHS SOUTH & 53 LKS EAST OF NW CORNER OF SEC 23, T 09N, R 02W, SLM. THENCE S 89°47'00" E 35.26 CHS, N 03°30'00" W 91 LKS, N 88°15'00" E 9.61 CHS, S 44°45'00" E 1.0 CH, S 04°00'00" W 2.68 CHS, S 87°00'00" E 7.24 CHS, S 25°00'00" W 3.14 CHS, EAST 14.92 CHS, SOUTH 8.80 CHS, WEST 19.60 CHS TO OSLRR, N 24°30'00" E 6.42 CHS, WEST 8.81 CHS, N 47°15'00" W 1.36 CHS, N 67°45'00" W 2.25 CHS, N 13°45'00" E 70 LKS, N 89°47'00" W 34.90 CHS, NORTH 5.93 CHS TO BEG.

LESS: THAT PORTION LYING EASTERLY OF THE RR R/W.

LESS: [03-118-0020] THAT DEEDED TO BRIGHAM CITY CORPORATION (ENT# 380806). PART OF THE NW/4 OF SEC 23, T 09N, R 02W, SLBM. BEGINNING AT GRANTORS NW PROPERTY CORNER, BEING A POINT ON THE EXISTING EAST R/W LINE OF 1200 WEST ST, LOCATED 1680.43 FT S 00°14'42" E ALONG THE WEST LINE OF SAID SEC AND 34.98 FT N 89°43'55" E FROM THE NW CORNER OF SAID SEC 23, SAID POINT DESCRIBED OF RECORD AS BEING LOCATED 25.39 CHAINS SOUTH AND 53 LINKS EAST OF THE NW CORNER OF SAID NW/4; THENCE S 89°31'35" E (S 89°47'00" E BY RECORD) 38.41 FT ALONG GRANTORS NORTH BOUNDARY LINE TO THE NEW EAST R/W LINE OF 1200 WEST ST; S 00°15'29" E 391.23 FT ALONG SAID NEW EAST R/W LINE TO GRANTORS SOUTH BOUNDARY; N 89°44'27" W (N 89°47'00" W BY RECORD) 38.50 FT ALONG SAID NORTH R/W LINE TO GRANTORS SW PROPERTY CORNER, BEING ON SAID EXISTING EAST R/W LINE; N 00°14'42" W (NORTH BY RECORD) 391.38 FT ALONG SAID EXISTING EAST R/W LINE TO THE POINT OF BEGINNING.

03-118-0023

BEG 3.79 CHS NORTH & 50 LKS EAST OF SW CORNER OF NW/4 OF SEC 23, T 09N, R 02W, SLM. THENCE NORTH 1.43 CHS, S 89°47'00" E 35.24 CHS, S 15°30'00" E 1.43 CHS, N 89°47'00" W 35.93 CHS TO BEG.

ALSO: BEG 5.22 CHS NORTH & 50 LKS EAST OF SW CORNER OF NW/4 OF SD SEC. THENCE NORTH 1.43 CHS, S 89°47'00" E 35.24 CHS, S 15°30'00" E 1.43 CHS, N 89°47'00" W 35.93 CHS TO BEG.

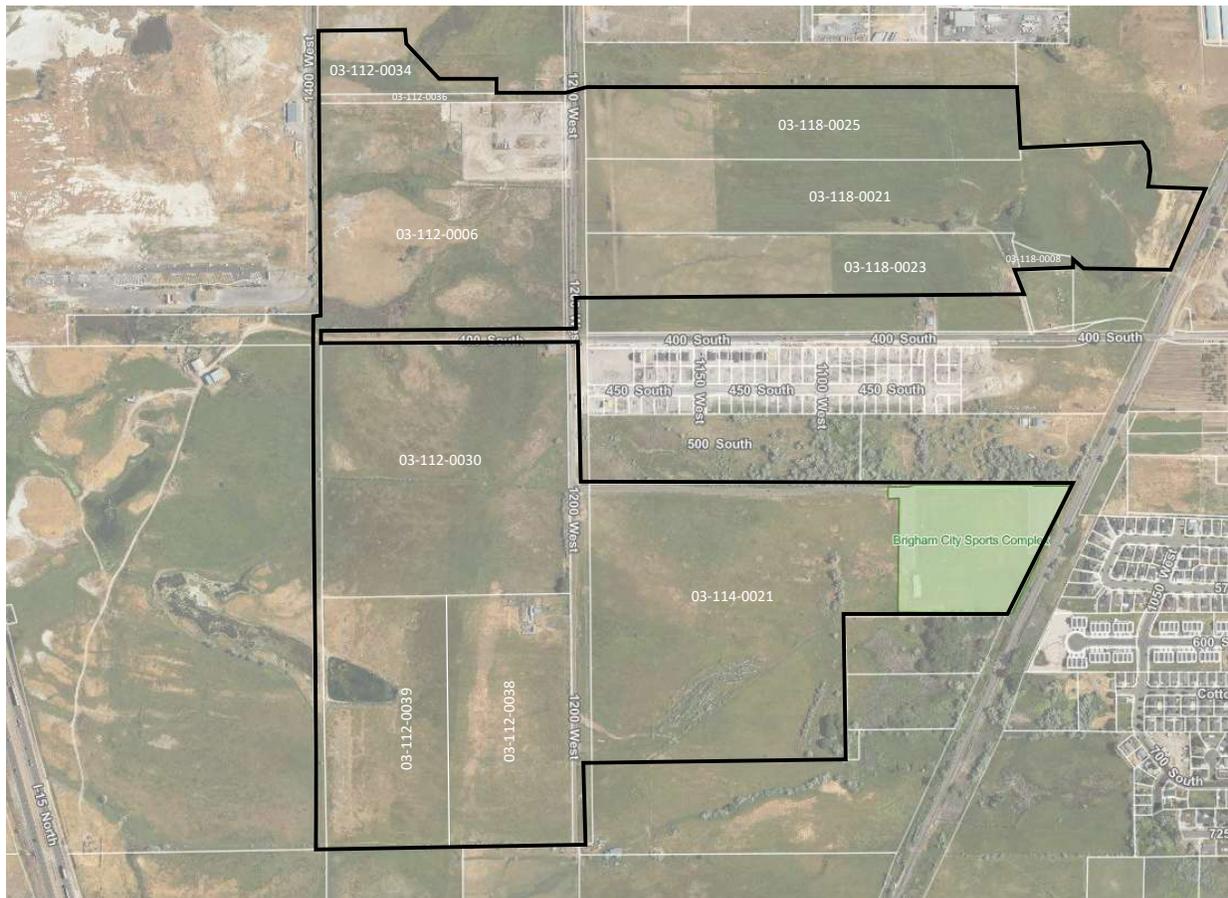
ALSO: BEG 6.45 CHS NORTH & 50 LKS EAST OF SW CORNER OF NW/4 OF SEC 23, NORTH 2.23 CHS, S 89°47'00" E 34.90 CHS, S 13°45'00" W 70 LKS, S 15°30'00" E 1.53 CHS, N 89°47'00" W 35.24 CHS TO BEG.

LESS: [03-118-0022] TRACT DEEDED TO BRIGHAM CITY CORPORATION (ENT# 380802). PART OF THE NW/4 OF SEC 23, T 09N, R 02W, SLBM. BEGINNING AT GRANTORS SW PROPERTY CORNER BEING A POINT ON THE EXISTING EAST R/W LINE OF 1200 WEST ST, LOCATED 2401.86 FT S 00°14'42" E ALONG THE WEST LINE OF SAID SEC AND 33.00 FT N 89°45'18" E FROM THE NW CORNER OF SAID SEC 23, SAID POINT DESCRIBED OF RECORD AS BEING LOCATED 3.79 CHAINS NORTH AND 50 LINKS EAST OF THE SW CORNER OF SAID NW/4; THENCE N 00°14'42" W 330.06 FT (NORTH 322.74 FT BY RECORD) ALONG SAID EXISTING EAST R/W LINE TO GRANTORS NW PROPERTY CORNER; S 89°47'07" E (EAST BY RECORD) 40.48 FT ALONG GRANTORS NORTH BOUNDARY LINE TO THE NEW EAST R/W LINE OF 1200 WEST ST; S 00°15'29" E 330.19 FT ALONG SAID NEW EAST R/W LINE TO GRANTORS SOUTH BOUNDARY; N 89°36'00" W (WEST BY RECORD) 40.55 FT ALONG SAID NORTH R/W LINE TO THE POINT OF BEGINNING.

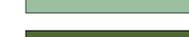
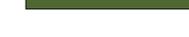
03-118-0025

BEG AT A POINT 19.62 CHS SOUTH OF NW CORNER OF SEC 23, T 09N, R 02W, SLM. THENCE S 89°47'00" E 35.26 CHS, S 03°30'00" E 5.78 CHS, N 89°47'00" W 35.57 CHS, NORTH 5.77 CHS TO BEG.

LESS: [03-118-0024] TRACT DEEDED TO BRIGHAM CITY CORPORATION (ENT# 380798). PART OF THE NW/4 OF SEC 23, T 09N, R 02W, SLBM. BEGINNING AT A POINT ON WEST LINE OF SAID SEC 23, LOCATED 1295.00 FT S 00°14'42" E (19.62 CHAINS SOUTH BY RECORD) ALONG SAID WEST LINE OF SAID SEC FROM THE NW CORNER OF SAID SEC 23; THENCE S 89°32'54" E (S 89°47'00" E BY RECORD) 73.30 FT ALONG GRANTORS NORTH BOUNDARY LINE TO THE NEW EAST R/W LINE OF 1200 WEST ST; S 00°15'29" E 385.02 FT ALONG SAID NEW EAST R/W LINE TO GRANTORS SOUTH BOUNDARY LINE; N 89°52'48" WEST (N 89°47'00" W BY RECORD) 73.39 FT ALONG SAID NORTH BOUNDARY LINE TO THE WEST LINE OF SAID SEC; N 00°14'42" W 385.45 FT (NORTH 380.82 FT BY RECORD) ALONG SAID EAST LINE TO THE POINT OF BEGINNING.



LEGEND

-  COMMERCIAL
-  CONDOMINIUMS
-  TOWNHOME LOTS
-  VILLAS SINGLE FAMILY
-  COTTAGES SINGLE FAMILY
-  ESTATE SINGLE FAMILY
-  PRIVATE OPEN SPACE
-  PUBLIC OPEN SPACE



MASTER PLAN COMMUNITY

EXHIBIT A

CITY OF BRIGHAM, BOX ELDER COUNTY

Exhibit "B"

Open Space and Trail Master Plan

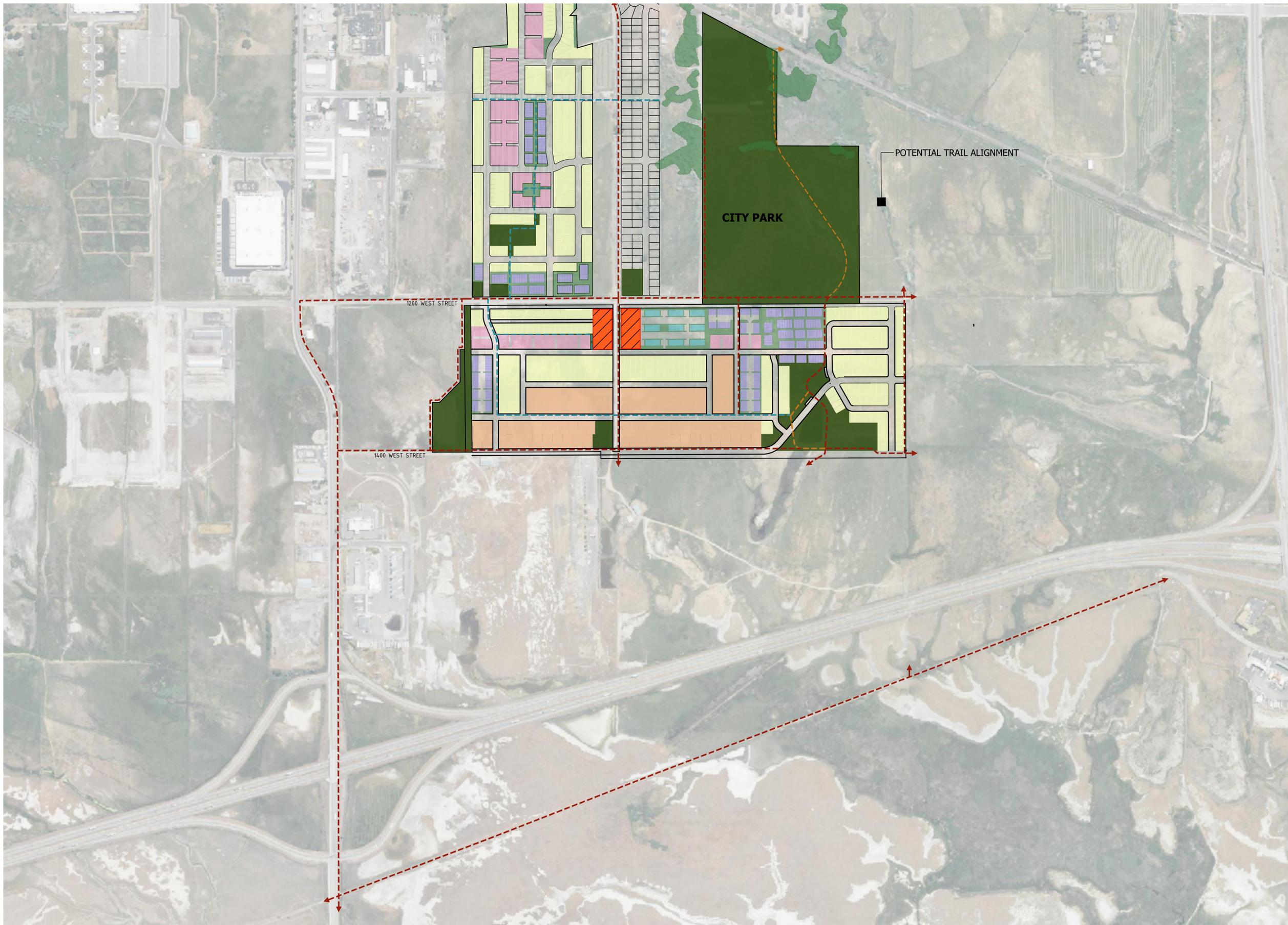


AVIAN SHORES

MASTER PLANNED COMMUNITY

LEGEND

-  PRIVATE OPEN SPACE
-  PUBLIC OPEN SPACE
-  REGIONAL TRAIL
-  LOCAL TRAIL
-  RECREATIONAL TRAIL
-  URBAN TRAIL



OPEN SPACE AND TRAIL MASTER PLAN

EXHIBIT A B

CITY OF BRIGHAM, BOX ELDER COUNTY

1. Regional Trails (Backbone Trails)

Purpose:

- Connect cities, towns, and major recreation areas.
- Serve long-distance cyclists, hikers, and multi-use travelers.

Characteristics:

- Typically 10+ miles long.
- Paved or concrete surfaces for durability.
- Wayfinding signage and trailheads at major access points.
- Often part of statewide or national trail systems (e.g., Rails-to-Trails).

Examples:

- Intercity bike corridors.
 - Multi-use greenways connecting multiple municipalities.
-

2. Community or Corridor Trails

Purpose:

- Connect neighborhoods, parks, schools, and commercial areas.
- Serve daily commuters and recreational users.

Characteristics:

- 1–10 miles in length.
- Multi-use: pedestrians, cyclists, sometimes equestrians.
- Medium-width (8–12 ft for urban bike/ped trails).
- Lighting in urban contexts; natural surface in suburban/recreational contexts.
- Signage for directions, distances, and rules.

Examples:

- Urban greenways along rivers or rail lines.
 - Suburban trail loops linking residential areas with schools and parks.
-

3. Neighborhood Trails / Local Connector Trails/ Urban Trail Corridor

Purpose:

- Provide direct access within a neighborhood or between residential streets.
- Encourage walking, jogging, short bike trips, and connectivity to larger trail systems.

Characteristics:

- Typically <1–3 miles.
- Narrower paths (4–6 ft for pedestrian trails; 6–8 ft for shared use).
- Sidewalks in urban areas.
- Signage is optional; may include benches or small plazas.

Examples:

- Trails connecting cul-de-sacs to schools or parks.
 - Small urban “pocket trails” through vacant lots or utility easements.
-

4. Recreational / Park Trails

Purpose:

- Focused on leisure, fitness, or adventure rather than connectivity.
- Located in parks, nature preserves, or protected open spaces.

Characteristics:

- Single-use or multi-use depending on location (hiking, mountain biking, equestrian).
- Surface: natural soil, gravel, boardwalks over sensitive areas.
- Often loops or circuits, ranging from 0.5–10 miles.
- Includes interpretive signs, viewpoints, and amenities like picnic areas.

Examples:

- Hiking loops in city parks.
 - Mountain bike trails in forested areas.
 - Nature trails with educational signage.
-

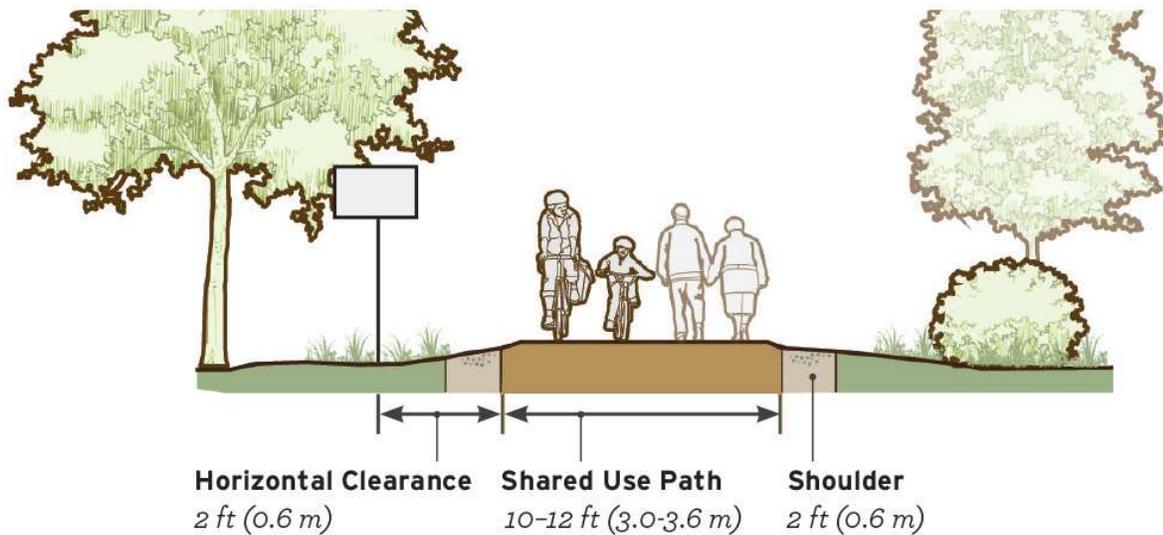
5. Specialty Trails

Purpose:

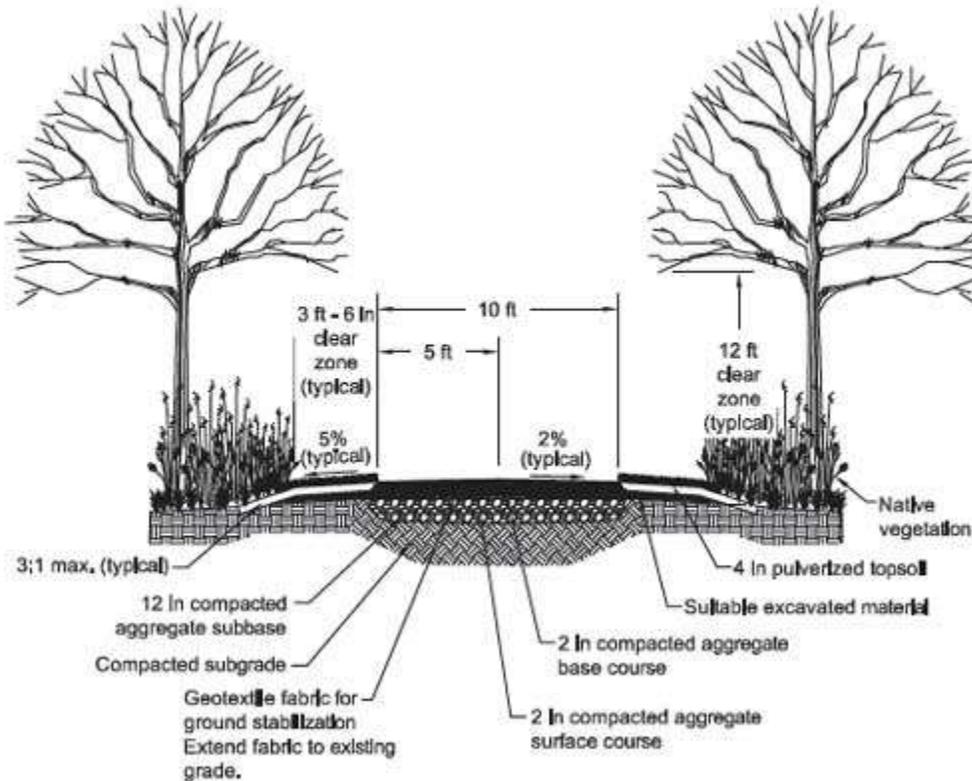
- Cater to specific user groups or activities.

Types:

1. **Fitness / Jogging Trails** – Short loops with distance markers and exercise stations.
2. **Equestrian Trails** – Dirt surfaces, wider corridors, often outside urban areas.
3. **Waterfront / Boardwalk Trails** – Along rivers, lakes, or coasts; scenic recreation.
4. **Adventure / Technical Trails** – Mountain biking singletracks or trail running courses.



The trail should include:



- Surface material: A crushed stone, 10 foot wide trail
- Permanent easement: A permanent public access easement to ensure the trail remains accessible to the community despite any future land development changes.
- Easement width and buffer: A total corridor width of 20-22 feet, including a 5-foot buffer on each side for vegetation and maintenance.

Exhibit "C"
Elevations

Villas - Front Load



Cottages - Alley load



Cottages - Front Load



Estate Homes

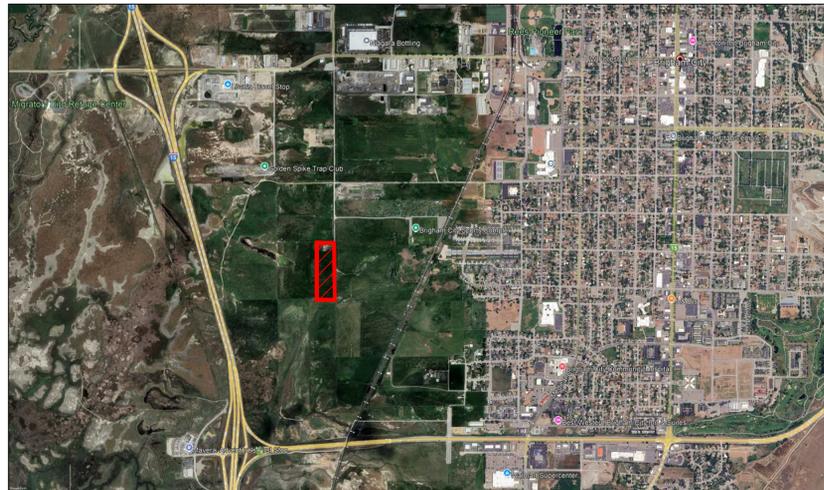


Townhomes - Front Load



Townhomes - Rear Load





VICINITY MAP

HERON RIDGE SUBDIVISION PRELIMINARY PLAT



SCALE: 1" = 60' (24x36 PLAN SET)

NOTES
1-TOTAL AREA: 7.375 ac
2-NUMBER OF UNITS: 63
23 single family lots
40 multi-family units

LEGEND

- BOUNDARY LINE
- - - PROPOSED EASEMENT (10' TYPICAL)
- PROPOSED CENTERLINE

BOUNDARY LEGAL DESCRIPTION
Part of the Southeast Quarter of Section 22, Township 9 North, Range 2 West of the Salt Lake meridian described as follows:

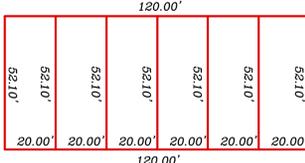
Commencing at the Northwest Corner of Section 22, Township 9 North, Range 2 West of the Salt Lake Base and Meridian thence S00°33'46"E 5303.31 feet along the east line of said Section 22 to the Southeast Corner of said Section 22; thence S 89°40'56" W 31.77 feet to an H&I rebar and the POINT OF BEGINNING and running

thence S 89°33'13" W 269.57 feet;
thence N 00°48'32" W 98.05 feet;
thence N 08°45'53" E 60.85 feet;
thence N 00°34'33" W 514.66 feet;
thence S 89°08'44" W 85.19 feet;
thence N 00°35'52" W 60.00 feet;
thence S 89°08'44" W 61.70 feet;
thence N 00°34'33" W 497.01 feet;
thence N 89°08'44" E 297.00 feet;
thence N 00°34'33" W 48.24 feet;
thence N 89°25'27" E 110.00 feet to the west right of way line of 1200 West Street;
thence S 00°34'33" E 1279.34 feet along said right-of-way line to the point of beginning, containing 9.29 acres, more or less.

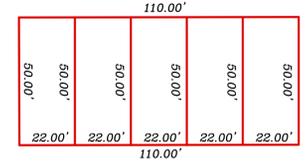


ALLIANCE CONSULTING ENGINEERS
150 EAST 1200 NORTH SUITE P
TREMONTON, UT 84321
PHONE: 435-730-8208
EMAIL: allianceng@allianceeng.com

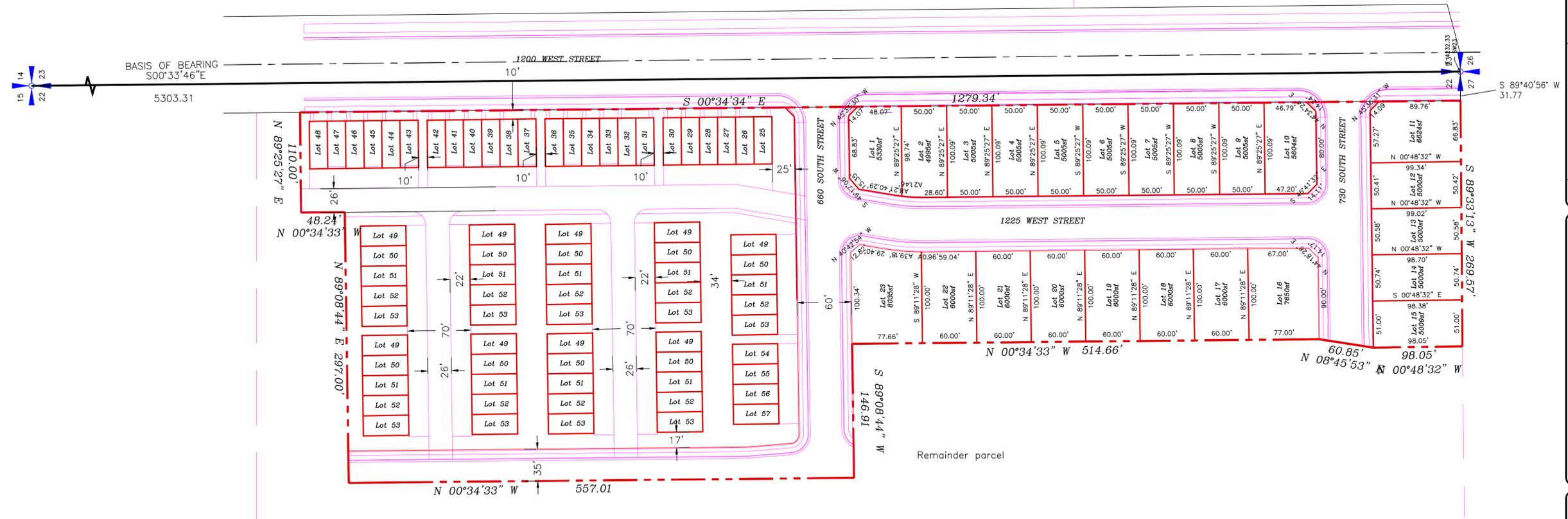
OWNER: HERITAGE LAND DEVELOPMENT
470 NORTH 2450 WEST
TREMONTON, UTAH
ATTN: GARTH DAY
435-730-8208



TYPICAL FRONT-LOAD UNIT



TYPICAL REAR-LOAD UNIT



NO.	REVISIONS/SUBMISSIONS	DATE

REMOVED: _____
DRAWN: _____
PROJECT NO.: _____
CAD FILE: _____

PROJECT TITLE: **HERON RIDGE SUBDIVISION**
PART OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 9 NORTH, RANGE 2 WEST, SALT LAKE BASELINE AND MERIDIAN BRIGHAM CITY, UTAH

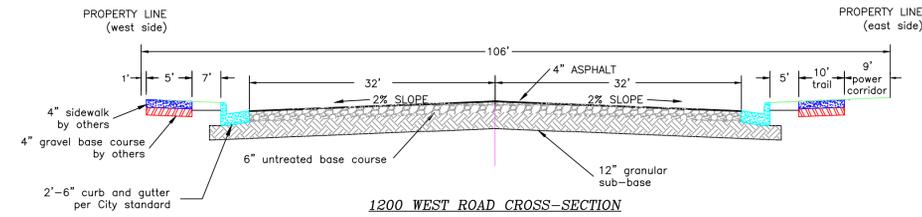
DRAWING TITLE: **PRELIMINARY PLAT**

DATE: DEC, 2025
DRAWING No. 1

HERON RIDGE SUBDIVISION PRELIMINARY PLAN

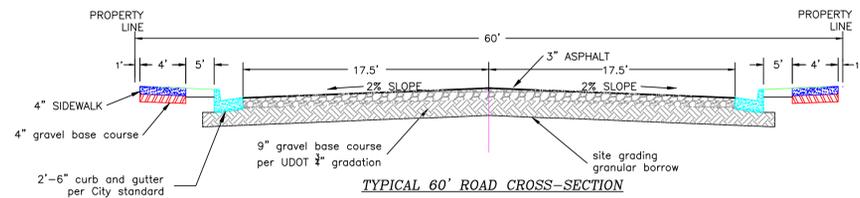


0 60 120
SCALE: 1" = 60' (24x36 PLAN SET)



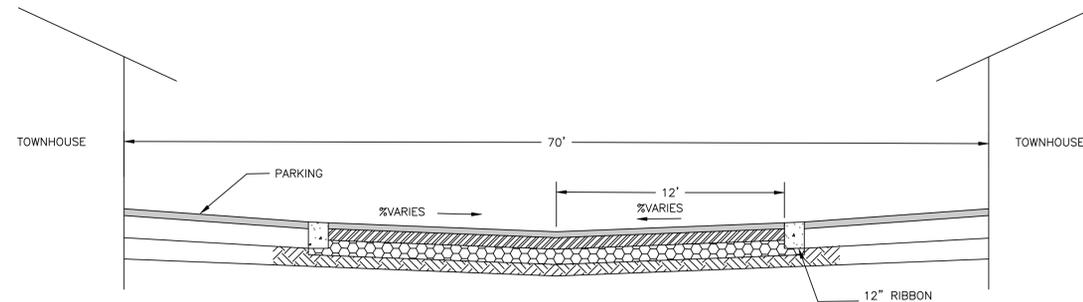
1200 WEST ROAD CROSS-SECTION

Contractor to follow site preparation per the GSH geotechnical report sections 5.2.1 and 5.8. Contractor required to obtain and keep a copy of the GSH geotechnical report on site.



TYPICAL 60' ROAD CROSS-SECTION

Contractor to follow site preparation per the GSH geotechnical report sections 5.2.1 and 5.8. Contractor required to obtain and keep a copy of the GSH geotechnical report on site.



TYPICAL DRIVE AISLE CROSS SECTION

LEGEND	
--- BOUNDARY LINE	PROPOSED STREET LIGHT
— E 8 PVC SEWER	PROPOSED CENTERLINE
— 12 C-900 WTR	PROPOSED SEWER—AS NOTED
— EXISTING SEWER—AS NOTED	PROPOSED WATER—AS NOTED
— EXISTING WATER—AS NOTED	PROPOSED STORM—AS NOTED
— EXISTING STORM	PROPOSED IRRIGATION—AS NOTED
— EXISTING FENCE	PROPOSED ASPHALT
— EXISTING 1' MNR CONTOUR	
— EXISTING 5' MJR CONTOUR	
— PROPOSED 0.5' MNR CONTOUR	
— PROPOSED 2.5' MJR CONTOUR	



ALLIANCE CONSULTING ENGINEERS
150 EAST 200 NORTH SUITE P
LOGAN, UTAH 84321
(435) 755-5121
alliancelogan@yahoo.com



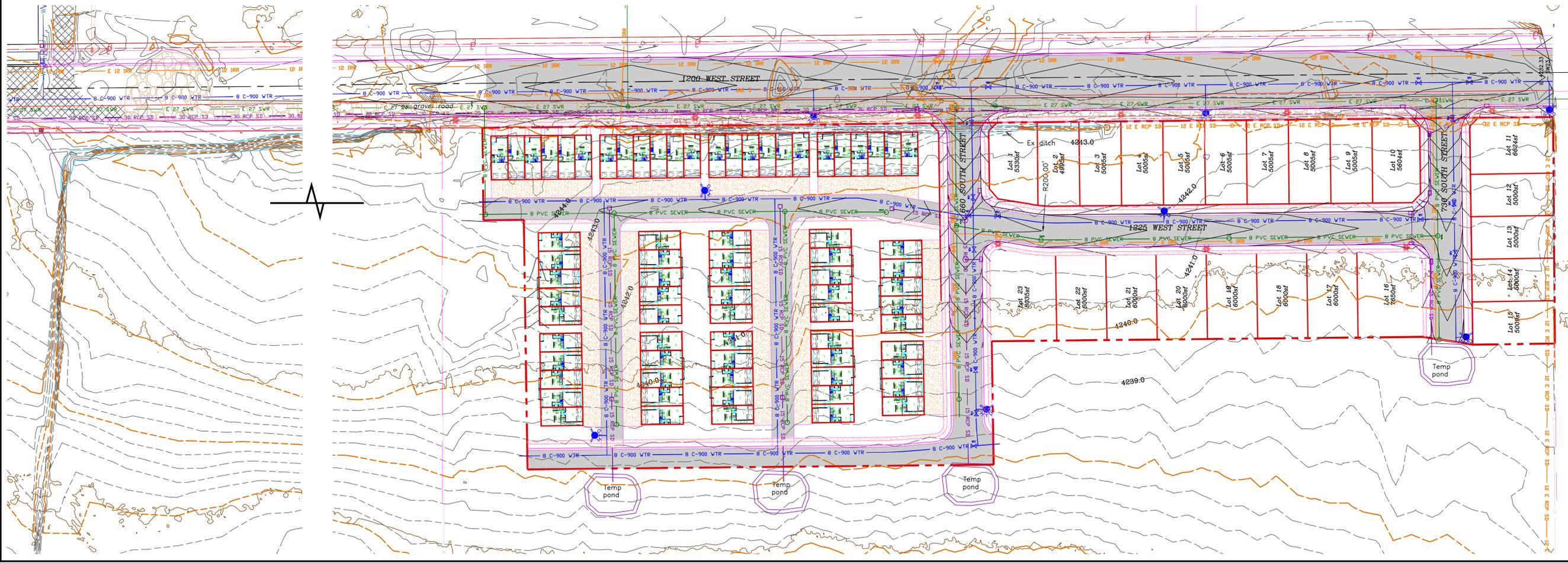
OWNER: HERITAGE LAND DEVELOPMENT
470 NORTH 2450 WEST
TREMONTON, UTAH
ATTN: GARTH DAY
435-730-8208

NO.	REVISIONS / SUBMISSIONS	DATE

PROJECT TITLE:
HERON RIDGE SUBDIVISION
PART OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 9
NORTH RANGE AND WEST MERIDIAN
SALT LAKE COUNTY, UTAH

DRAWING TITLE:
PRELIMINARY PLAN

DATE: DEC, 2025
DRAWING NO.: **2**



HERON RIDGE SUBDIVISION PRELIMINARY PROFILES

0 60 120
SCALE: 1" = 60' (24x36 PLAN SET)
SCALE: 1" = 10' VERTICAL



ALLIANCE CONSULTING ENGINEERS
150 EAST 200 NORTH SUITE P
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alliancelogan@yahoo.com

OWNER: HERITAGE LAND DEVELOPMENT
470 NORTH 2450 WEST
TREMONTON, UTAH
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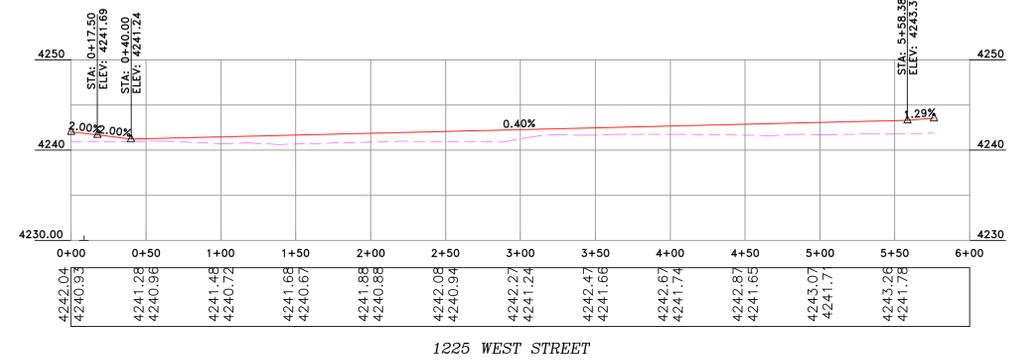
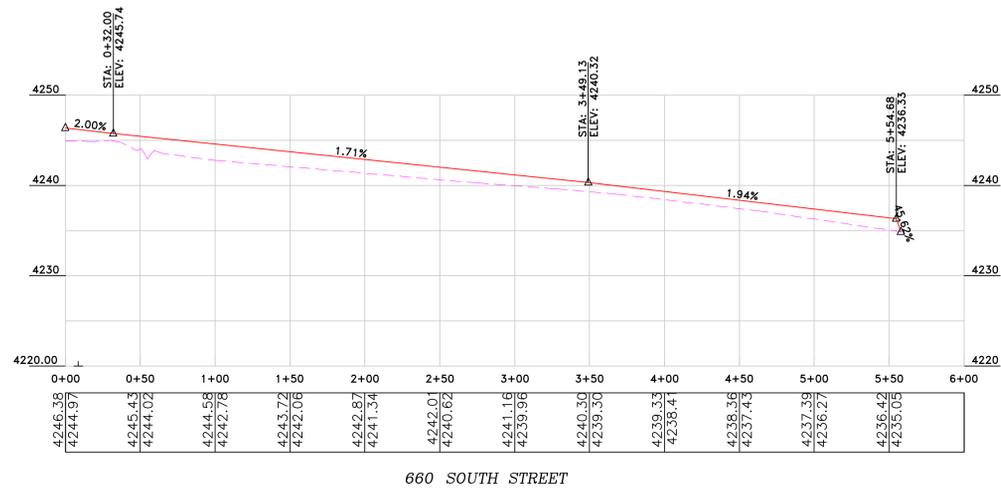
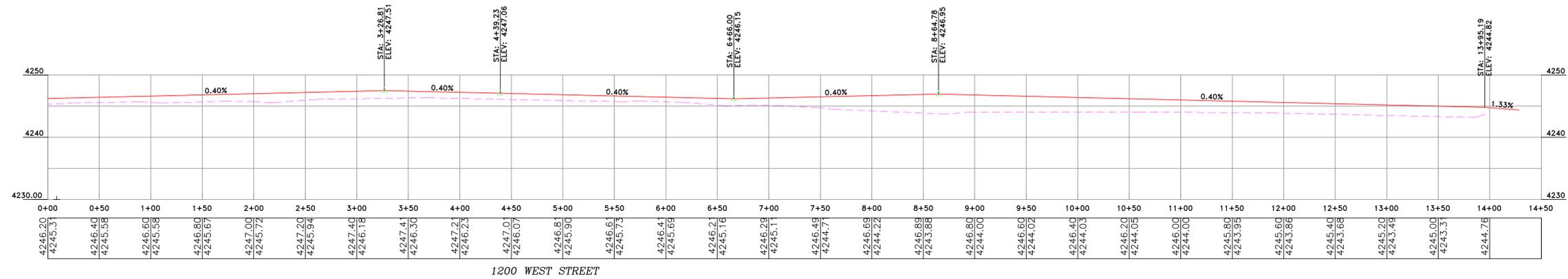
No.	REVISIONS/ SUBMISSIONS	DATE

REVIEWED: _____ DRAWN: _____
DATE FILE: _____ PROJECT NO.: _____

PROJECT TITLE
HERON RIDGE SUBDIVISION
PART OF THE NORTHWEST QUARTER OF SECTION 23,
RANGE 10N, TOWNSHIP 9
SALT BASIN AND MERIDIAN
BRIGHAM CITY, UTAH

DRAWING TITLE
PRELIMINARY PROFILES

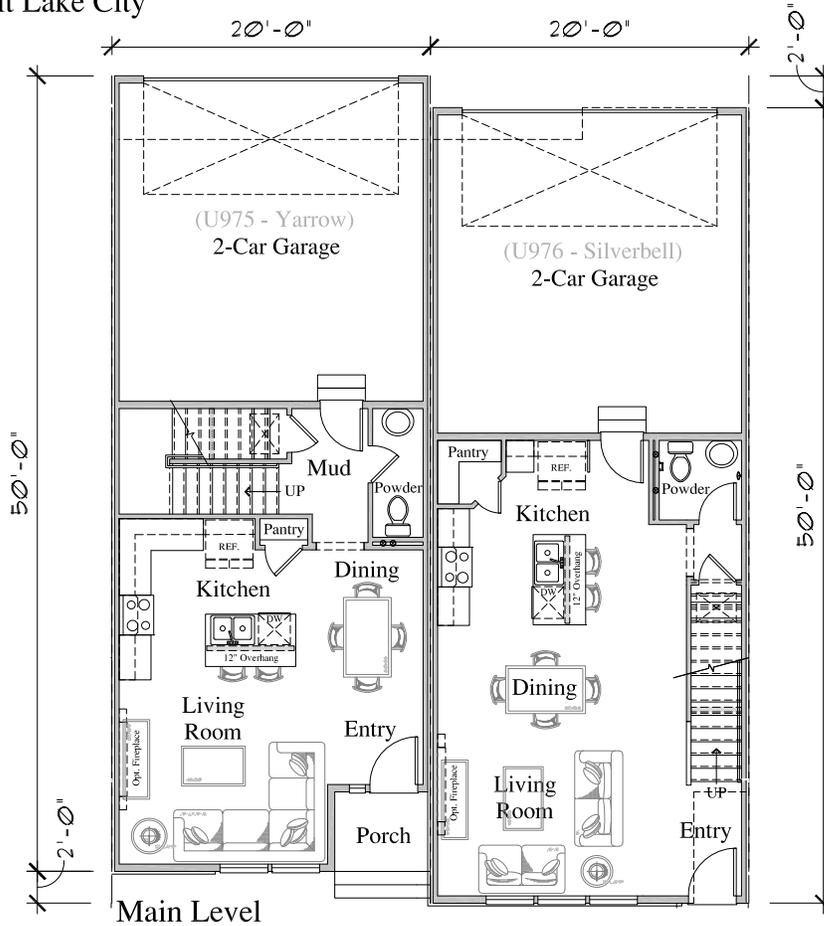
DATE: DEC, 2025
DRAWING No. **3**



Yarrow / Silverbell

U975/U976

Salt Lake City

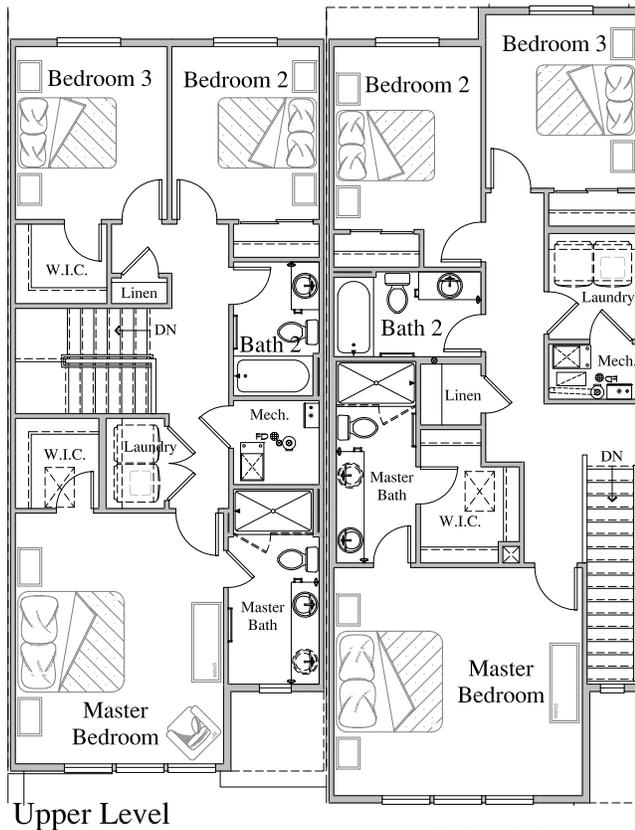


Features:

- **U975 - Yarrow**
- **1,451 Sq. Ft.**
- 2 Story Townhome
- 3 Bedrooms
- 2 1/2 Baths
- 2-Car Garage

Features:

- **U976 - Silverbell**
- **1,548 Sq. Ft.**
- 2 Story Townhome
- 3 Bedrooms
- 2 1/2 Baths
- 2-Car Garage



Yarrow / Silverbell

U975/U976

Salt Lake City (U975-U976-4-001)

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



Elevation A

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



Elevation B

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



Elevation C

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



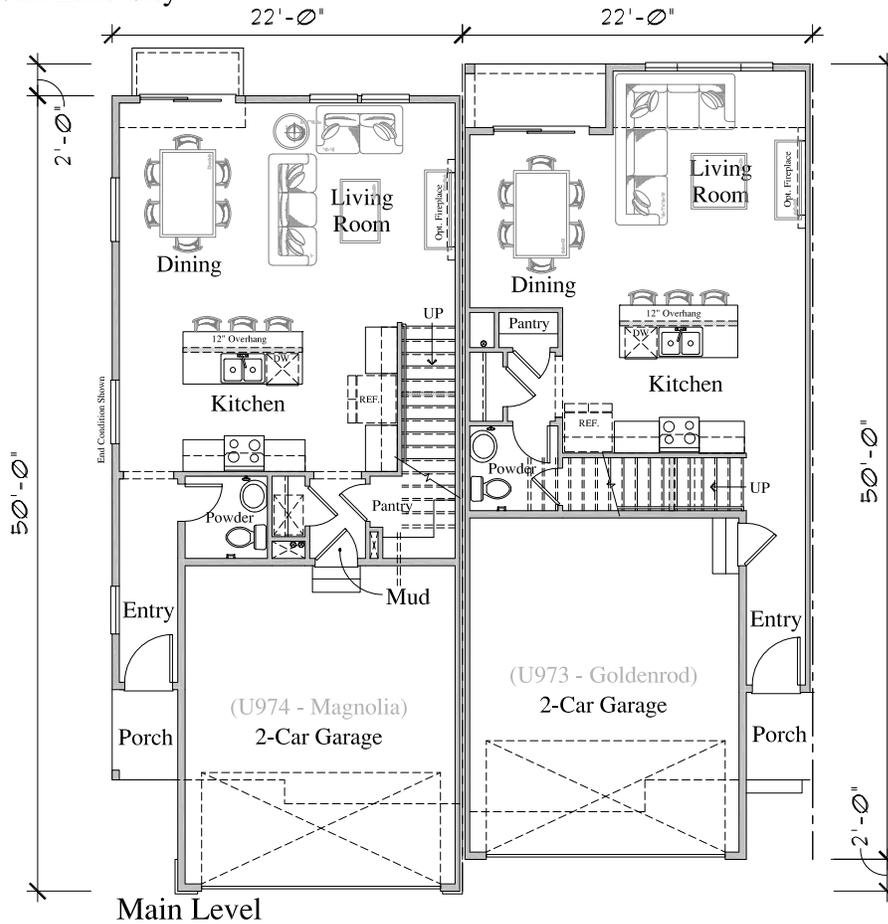
Elevation D



Goldenrod / Magnolia

U973/U974

Salt Lake City

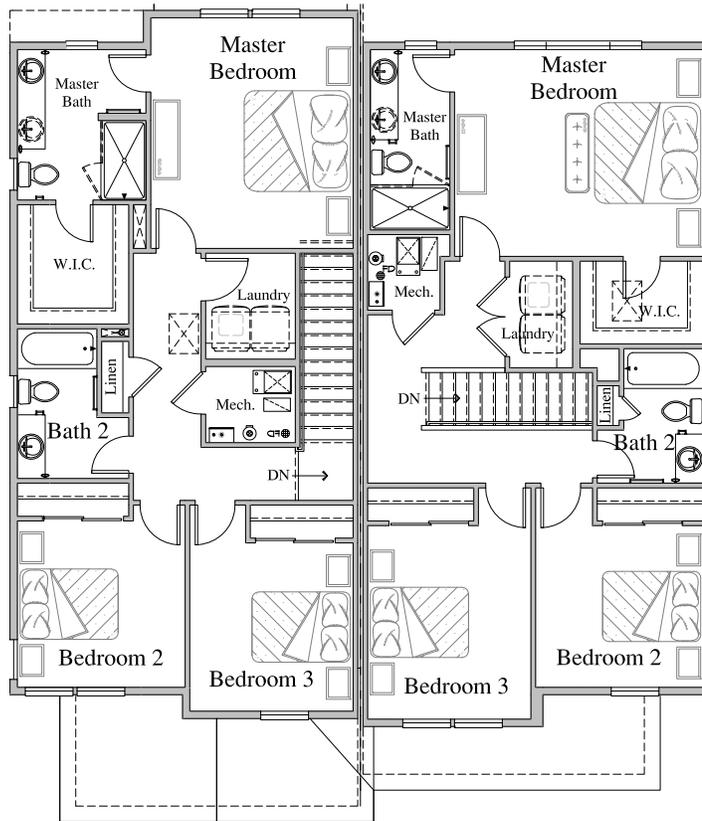


Features:

- **U973 - Goldenrod**
- **1,533 Sq. Ft.**
- 2 Story Townhome
- 3 Bedrooms
- 2 1/2 Baths
- 2-Car Garage

Features:

- **U974 - Magnolia**
- **1,572 Sq. Ft.**
- 2 Story Townhome
- 3 Bedrooms
- 2 1/2 Baths
- 2-Car Garage



Goldenrod / Magnolia

U973/U974

Salt Lake City (U973-U974-4-001)

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



Elevation A

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



Elevation B

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



Elevation C

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



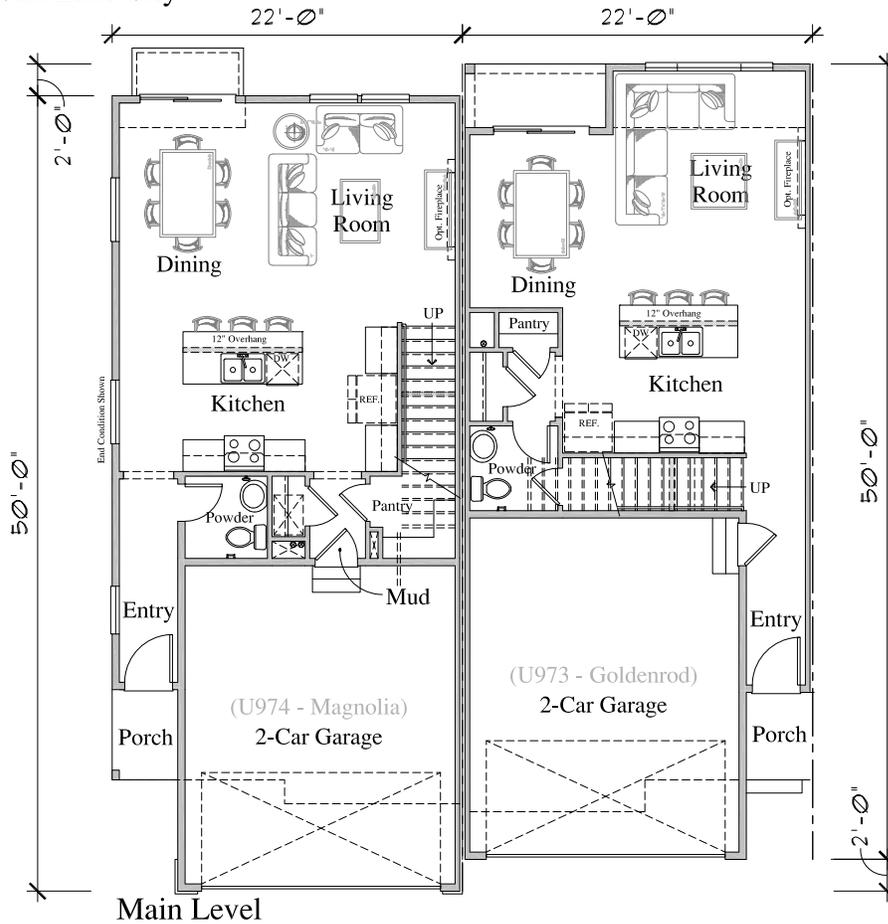
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Goldenrod / Magnolia

U973/U974

Salt Lake City

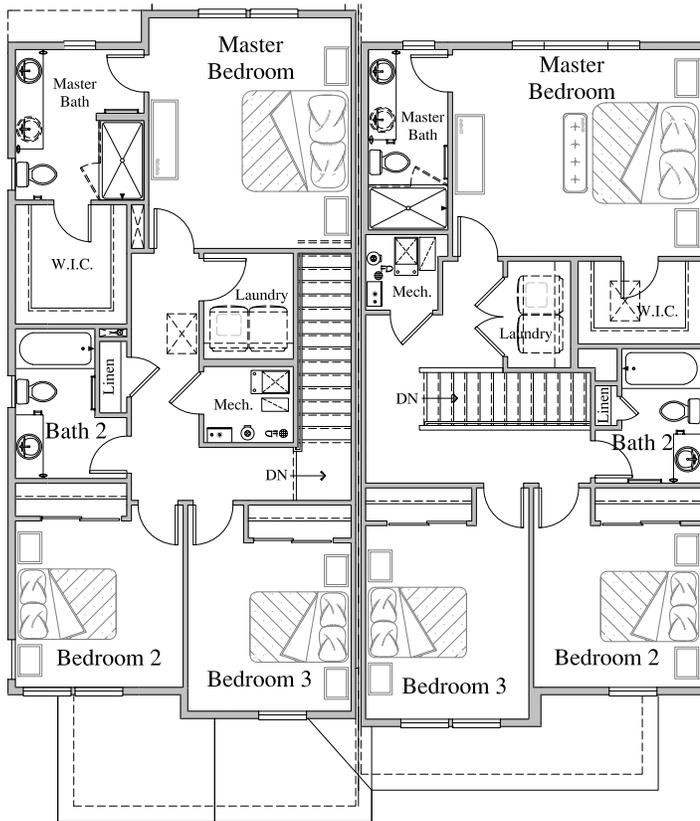


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Goldenrod / Magnolia

U973/U974

Salt Lake City (U973-U974-4-001)

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



Elevation A

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



Elevation B

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



Elevation C

(U974 - Magnolia)

(U973 - Goldenrod)

(U973 - Goldenrod)

(U974 - Magnolia)



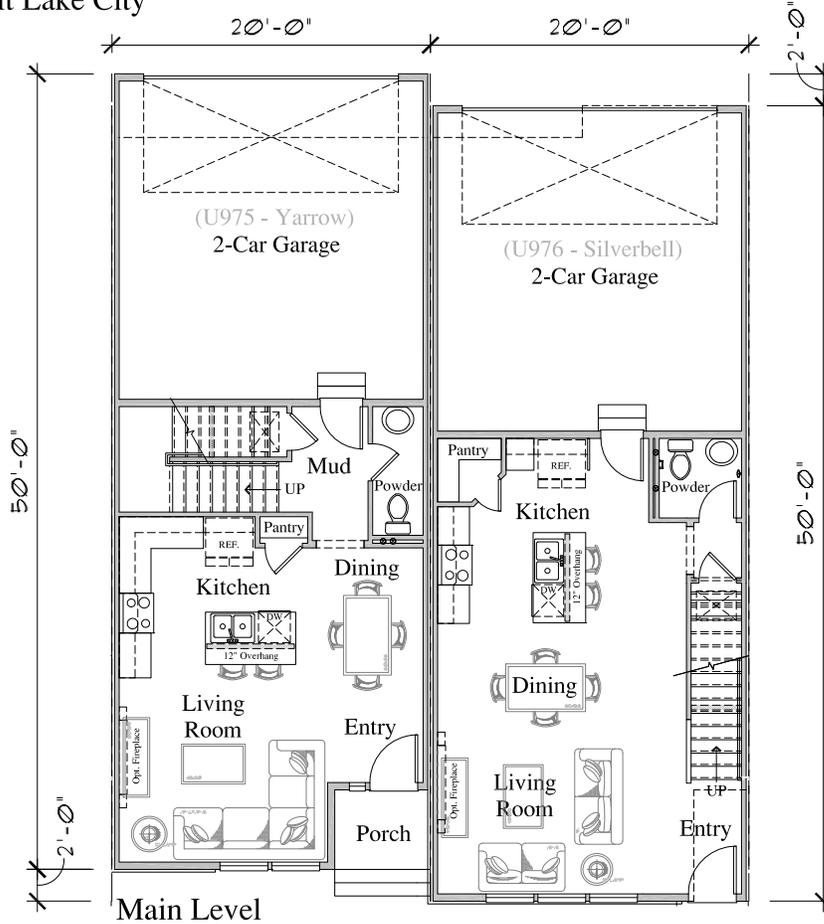
Elevation D



Yarrow / Silverbell

U975/U976

Salt Lake City

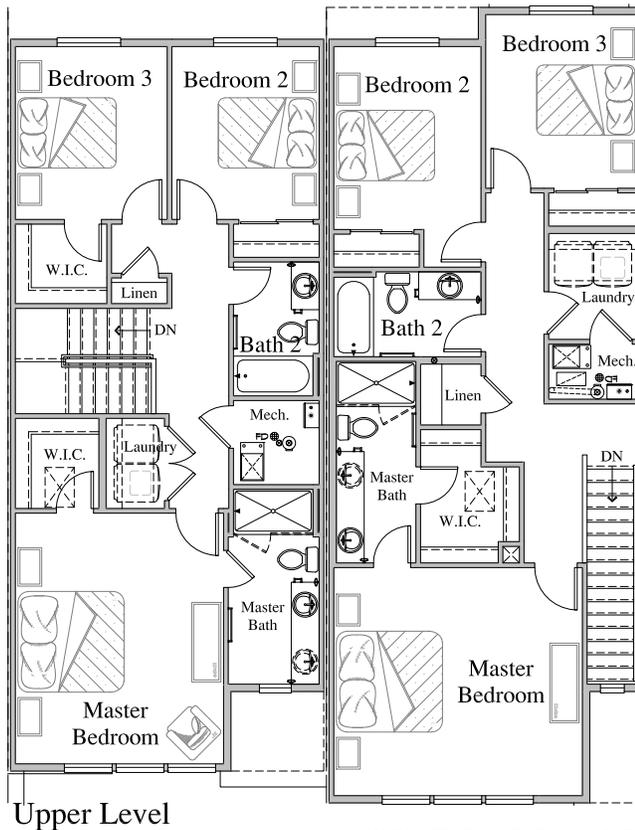


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Yarrow / Silverbell

U975/U976

Salt Lake City (U975-U976-4-001)

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



Elevation A

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



Elevation B

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



Elevation C

(U976 - Silverbell)

(U975 - Yarrow)

(U976 - Silverbell)

(U975 - Yarrow)



Elevation D



**BRIGHAM CITY, UTAH
PLANNING COMMISSION
STAFF EVALUATION**

APPLICANT: Garth Day, Heritage Land Development
OWNER'S NAME: Valentine Nicholas L. TTEE
PROPERTY ADDRESS: Approximately 250 S 1200 W
PARCEL NO.: Portion of 03-118-0021 / 0023 / 0025

APPLICATION NO.: 26-015
PARCEL AREA: ~13 acres
ZONING DISTRICT: P-R-M-7
DATE: March 12, 2026

PLANNING COMMISSION MEETING: March 17, 2026
APPLICATION TYPE: Legislative
PLANNING COMMISSION ROLE: Recommending Body to City Council

NATURE OF REQUEST:

Subdivision Preliminary Plat – Sandpiper Meadows Subdivision

OVERVIEW:

Updated plans have been submitted for review. This is one of two preliminary plat applications submitted together in the new Avian Shores Planned District. Garth Day mentioned that the name on the submitted plat is incorrect and will be Sandpiper Meadows. This subdivision will consist of two phases with a total of 54 townhome units and 55 single family lots:

- Phase 1 – 54 townhome units, 37 single family lots
- Phase 2 – 18 single family lots

Sandpiper Meadows is located north of 400 South Street, and north of the Dickamore property, straddling the three Valentine properties along 1200 West Street. An updated Avian Shores Master Plan and Site Location is provided.

Per the adopted Avian Shores Planned District, “the standards for density, height regulations, and parking, shall be governed by the standards of the R-M-7 District. *The Planned District provides flexibility for lot sizes and yard requirements (setbacks) to allow a variety of lot sizes and housing type, and for compatible design with mixed-uses, and street scale. Design flexibility must provide a variation of lot sizes.* The overall density of the Planned District area, excluding the City properties, is limited to a maximum of 7 dwelling units per acre.” City Code Section 156.286(A)

The following are items we need to continue to work through:

- Process to incorporate the updated Avian Shores Master Plan
- What is the desirable and acceptable setback along 1200 West Street, which is a 106' ROW?
- What are the allowable setbacks for the single-family dwelling lots? It appears they are proposing 20 ft front, 5 ft side, and 15 ft rear.
- What are acceptable minimum single-family dwelling lot sizes? The lots range from 3,564 sq. ft. to 5,950 sq. ft. with a majority being 3,960 sq. ft. The Planned District encourages a variety of lot sizes and housing types. These lot sizes are quite small but basically provide a detailed style townhome.

- This phase is a higher density than 7 du/ac, which is fine if the overall project doesn't exceed the maximum 7 du/ac. Each phase hereafter should provide a density tracking provision.
- Fencing detail – 1200 West Corridor. A wrought iron fence like Cardamine Village and North Point PUDs is being proposed where townhomes face 1200 West Street. Recommend concrete curbing under the fence, an aesthetic structure column regularly spaced throughout to create a structural integrity component and aesthetic component. A solid type of wall needs to be determined for areas where lots back up to 1200 West Street.
- Consideration of the 1200 West Street cross-section.
- Trail and Open Space details. Some illustrations have been provided to paint the picture. The first step in the process.
- Private drive details and distances.
- Signage/Wayfinding Plan. Locations and who is responsible for maintenance.
- Landscape Plans. 1200 West Corridor and Inside Development

STAFF COMMENTS:

Community and Economic Development Department:

1. *Standards for density, height regulations, and parking, shall be governed by the standards of the R-M-7 District. The Planned District provides flexibility for lot sizes and yard requirements (setbacks) to allow a variety of lot sizes and housing type, and for compatible design with mixed-uses, and street scale. Design flexibility must provide a variation of lot sizes. The overall density of the Planned District area, excluding the City properties, is limited to a maximum of 7 dwelling units per acre (156.286(A)).*
 - A. Recommend setbacks be established for the single-family dwelling lots. Developer is proposing a 20 ft front, 5 ft side, and 15 ft rear.
 - B. Recommend a minimum setback along 1200 West Street corridor be 25 feet for both townhomes and single-family lots.
 - C. Concerned with the proposed lot sizes backing up against 1200 West Street. Would like to see a footprint plan with the proposed lots to show how setbacks can be met.
 - D. Not supportive of lot sizes ranging from 3,500 – 6,000 sq. ft. throughout the Sandpiper area of the project. A variety of lot sizes and housing types should be planned for in this area.
2. Street numbers. Street numbers must be a whole, half, three-quarters, or one-quarter numbers.
 - A. 310 South, 1170 West, and 1185 West must be revised.
3. Addresses. Addresses to be assigned with final plat. Unable to work on address assignments until street numbers are solidified.
4. *Landscape and screening plan shall be submitted with the final design and construction plans for Planning Commission review and approval (156.286(C)(3)).*
 - A. Received landscape illustrations for open space, wetlands, and 1200 West Street Corridor.
 - B. Need a detailed plan for the landscape and trail for the open space shown between Lots 42 and 43.
 - C. Need landscape plan for this phase.
5. Trail and Open Space.
 - A. The sidewalk width along 310 South (to be revised) must be updated to the Urban Trail standard, a wider sidewalk.
 - B. Need a detailed plan for the landscape and trail for the open space shown between Lots 42 and 43

6. Fencing Plan. *The fencing plan shall be submitted with the final design and construction plans for Planning Commission review and approval (156.286(E)).*
 - A. Recommend concrete curbing with wrought iron fence along with aesthetic structural columns spaced uniformly along 1200 West where townhome units face 1200 West.
 - B. Recommend other fencing through out the development be discussed. Focus is primarily on the 1200 West Street Corridor, but there are other areas of consideration (i.e. along trails, open space, etc.)
7. 1200 West Street Corridor. *Developer shall install a low maintenance safety barrier fence or wall to ensure privacy and safety. Said fence or wall design is to be coordinated and approved by the Brigham City Staff DRT prior to installation (156.286(H)(2)).*
 - A. A privacy wall along 1200 West Street is required for single-family lots. Developer would like to use concrete panel like installed at Beeton Path Subdivision by the railroad. The wall design is to be coordinated and approved by the Brigham City Staff DRT prior to installation.
 - B. Recommend a different concrete panel for this corridor to be used. Requires additional discussion with the Staff DRT.
 - C. Provide an updated cross-section to show Public Works Standards and existing overhead powerline.
8. General Development Plan. The applicant has provided an amended General Development Plan (Master Plan) for the project. However, some of the information is missing and colors are off. Per the purpose and intent of the Avian Shores Planned District, ... *The district's development plan provides general guidance on the location and density of residential units and land uses compatible with adjacent properties, ensuring adequate infrastructure and services (156.285).*
 - A. Recommend the proposed updated General Development Plan (Master Plan) be cleaned up and run through the process to be incorporated in the Avian Shores Planned District. This would be an amendment to the zoning code.

Engineering Department:

1. Please note that the proposed townhomes are only wide enough for single vehicle parking within the garage. Please ensure that your parking calculations include one vehicle in the garage per unit.
2. We will review the geotechnical report and provide any necessary feedback.
3. Please update the 1200 W. cross section on SHEET L-101 to match the City Standard 1200 W. cross section on SHT R3 of the City Standard Drawings.
4. We will provide a thorough review of the utilities when construction drawings are provided.
5. The corner of 1170 W and 350 South should have a curved radius and not a 90 degree turn. City Standard drawings specify this radius. Unless directed differently by Community Development or Public Works, a curved radius will be required. This improves traffic flow and plowing operations.

Fire Department:

1. No comments

Police Department:

1. No comments

Public Power Department:

1. Show location of existing overhead power pole and lines.

2. Reminder: From the pitch of the roof to the outer most phase on the power line needs to be a minimum of 15 feet.

**Public Works Department:
(Culinary Water, Street, and Wastewater Treatment/ Storm Drain Divisions)**

1. Public Works supports the application with no additional comments.
 2. See Engineering Department comments.
-

PLANNING COMMISSION RESPONSIBILITY:

1. The Planning Commission considers the application and determines whether the proposed subdivision plat is in harmony with the Avian Shores Planned District. *The purpose and intent of establishing the Avian Shores Planned District is to create a dynamic and vibrant area that fosters the orderly development of residential uses along with potential neighborhood commercial uses that would service the residential area. ... The district's development plan provides general guidance on the location and density of residential units and land uses compatible with adjacent properties, ensuring adequate infrastructure and services. City Code 156.285 Purpose and Intent*
- D. The Planning Commission is the recommending body to the City Council for this application. Process established with the Planned District.
- E. The Planning Commission may recommend approval, disapproval, or request additional information to make a recommendation to the City Council.

STAFF RECOMMENDATIONS:

1. Recommend the Planning Commission review the application against the Avian Shores Planned District. A copy is attached to the Heron Ridge Subdivision application.
2. Recommend the applicant present the application, discuss the project, timing, and details.
3. Recommend the application be continued to allow the applicant time to provide a more complete application to advance the project in accordance with the Avian Shores Planned District.

STIPULATIONS:

1. Update plans to address Staff Comments
2. Provide details to address items note in application submission.
3. The landscaping and screening plan, and the fencing plan shall be submitted with the final design and construction plans for Planning Commission review and approval.

FINDINGS OF FACT:

1. The proposed subdivision is in the Avian Shores Planned District.
2. The Avian Shores Planned District was adopted by the City Council on November 6, 2026 as a P-R-M-7 zoning district.
3. The Avian Shores Planned District allows for townhomes and variation of single-family dwelling lots.
4. The Avian Shores Planned District allows for an overall density of 7 dwelling units per acre.
5. The proposed subdivision exceeds 7 dwelling units per acre by itself but will balance out with future phases according to the adopted General Development Plan (Master Plan).
6. Additional details on certain design elements must be worked out and finalized with the final design and construction drawings.

ATTACHMENTS:

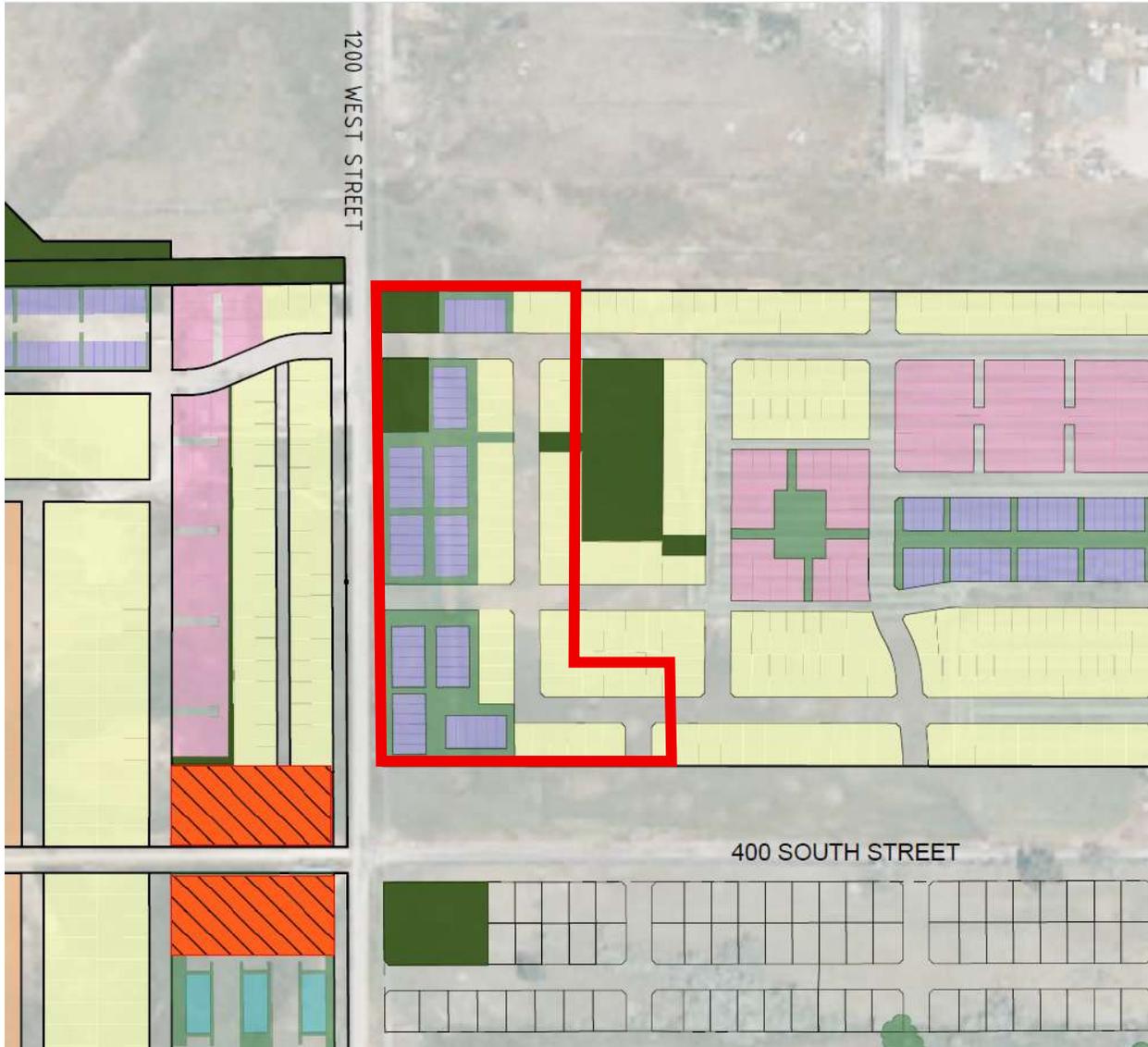
1. Locator Map
2. Avian Shores Planned District (*Note: A copy is attached to the Heron Ridge Subdivision application, where that is the first application being considered*)
3. Fence Detail
4. Sandpiper Preliminary Plat and Drawings
5. Elevations (Emailed Electronically)
6. Landscape Architect Illustrations (Emailed Electronically)

LOCATOR MAP



AVIAN SHORES MASTER PLAN – SITE LOCATION

Sandpiper Meadows Subdivision Area



MONTAGE PLUS[®]

WELDED ORNAMENTAL STEEL FENCE



FENCE PRODUCTS

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ASSA ABLOY, the global leader in door opening solutions

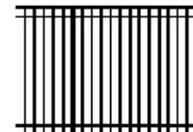
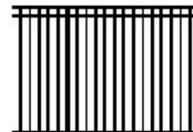
AMERISTAR[®]

ASSA ABLOY

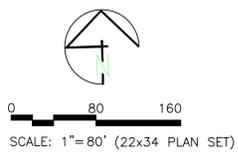


MAJESTIC[™]

- *3', 3½', 4', 5' or 6' Heights*
- *2-Rail or 3-Rail Panels*
- *Extended Picket or Flush Bottom Panels*
- *4" Standard or 3" Pet, Pool & Play Picket Air-Space*



SANDPIPER RIDGE SUBDIVISION PRELIMINARY PLAN



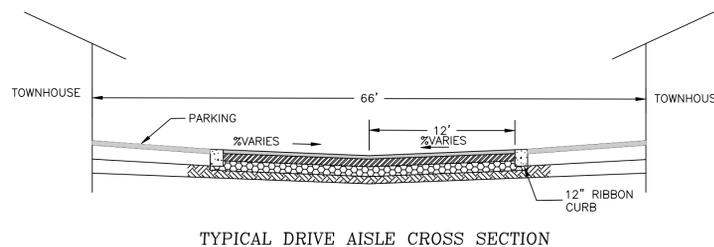
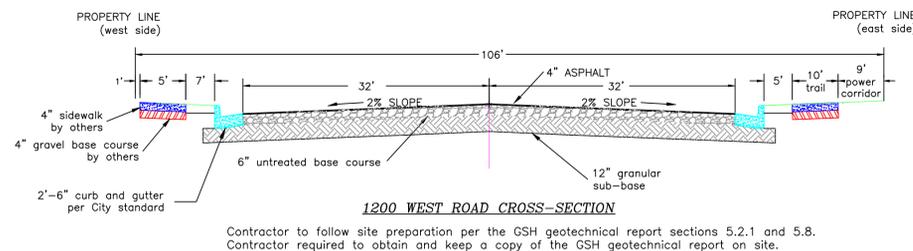
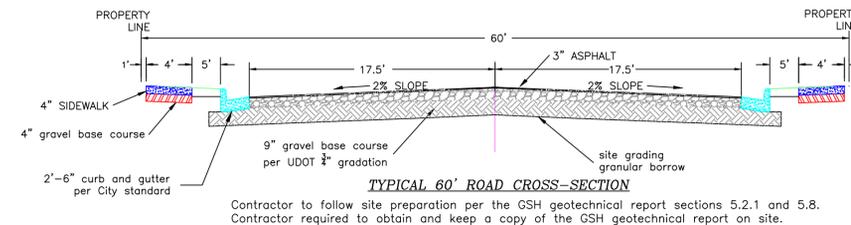
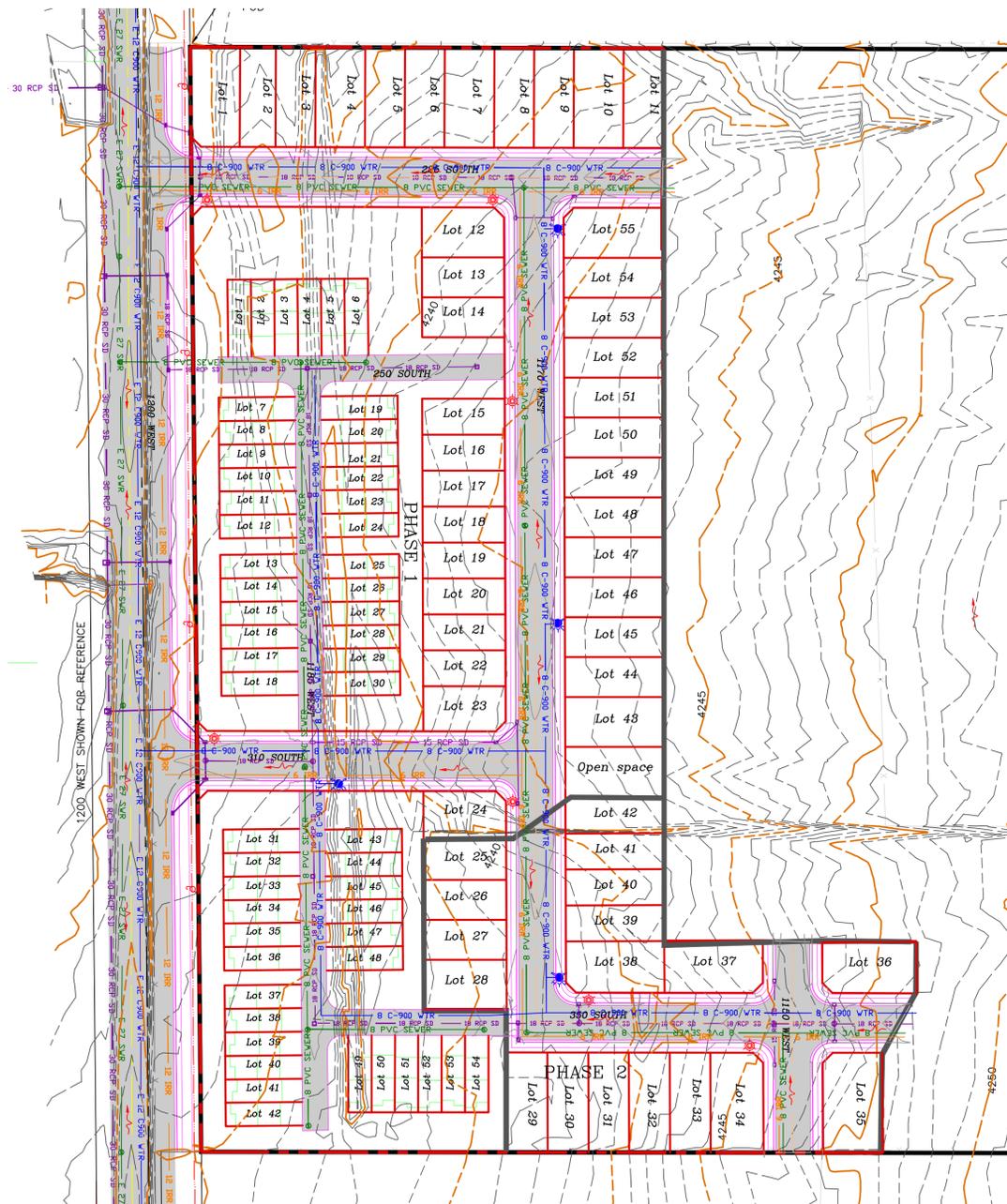
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	BOUNDARY LINE		PROPOSED STREET LIGHT
	EXISTING SEWER-AS NOTED		PROPOSED CENTERLINE
	EXISTING WATER-AS NOTED		PROPOSED SEWER-AS NOTED
	EXISTING STORM		PROPOSED WATER-AS NOTED
	EXISTING FENCE		PROPOSED STORM-AS NOTED
	EXISTING 1' MNR CONTOUR		PROPOSED IRRIGATION-AS NOTED
	EXISTING 5' MJR CONTOUR		PROPOSED ASPHALT
	PROPOSED 1' MNR CONTOUR		
	PROPOSED 5' MJR CONTOUR		



**ALLIANCE CONSULTING
ENGINEERS**
150 EAST 200 NORTH SUITE P
LOGAN, UTAH 84321
(435)755-5121
alliance@alliance.com



OWNER: HERITAGE LAND DEVELOPMENT
470 NORTH 2450 WEST
TREMONTON, UTAH
ATTN: GARTH DAY
435-730-8208



NO.	REVISIONS / SUBMISSIONS	DATE

REVIEWED: _____ DRAWN: _____
CAD FILE: _____ PROJECT NO.: _____

PROJECT TITLE: **SANDPIPER RIDGE SUBDIVISION**
PART OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 9 NORTH, RANGE 2 WEST, MERIDIAN SALT LAKE BASIN, BRIGHAM CITY, UTAH

DRAWING TITLE: **PRELIMINARY PLAN**

DATE: DEC, 2025
DRAWING No. **2**

SANDPIPER RIDGE SUBDIVISION PRELIMINARY PROFILES

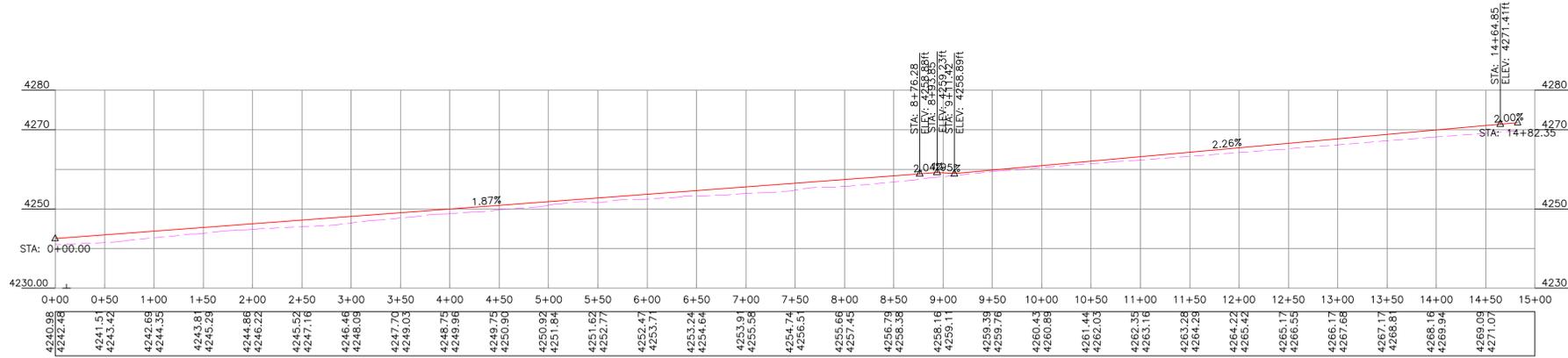
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SCALE: 1"=20' VERTICAL



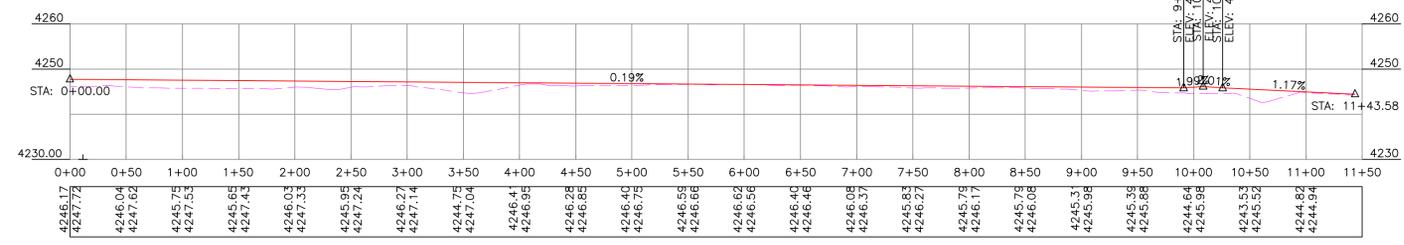
ALLIANCE CONSULTING ENGINEERS
150 EAST 200 NORTH SUITE P
LOGAN, UTAH 84321
(435)755-5121
allianceeng.com@yahoo.com



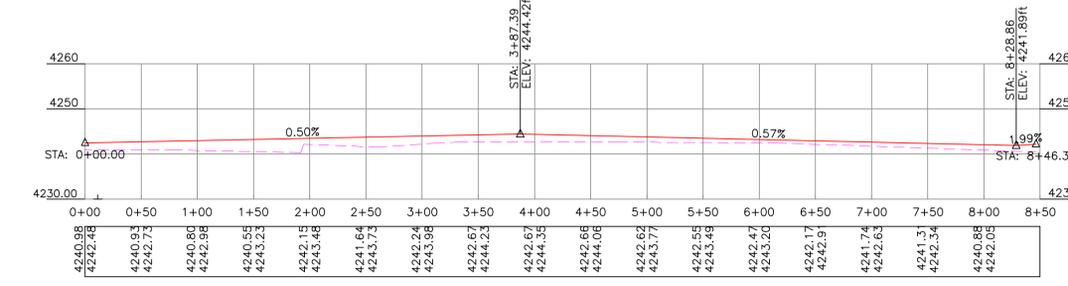
OWNER: HERITAGE LAND DEVELOPMENT
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TREMONTON, UTAH
ATTN: GARTH DAY
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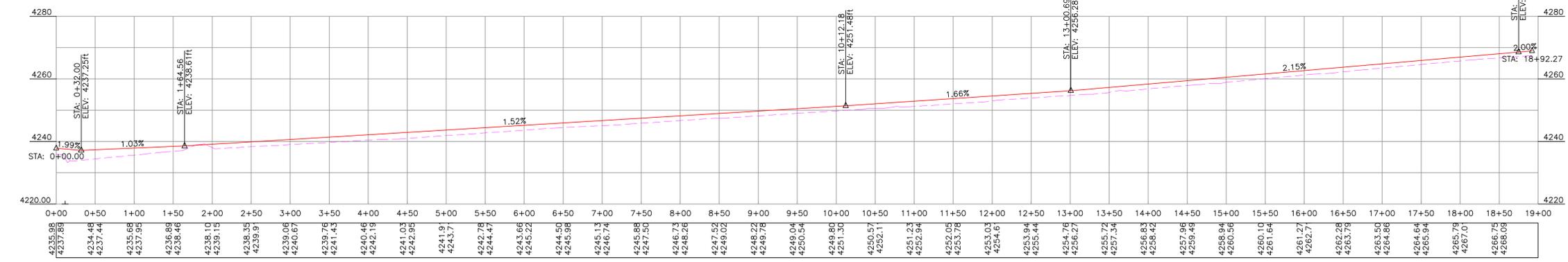
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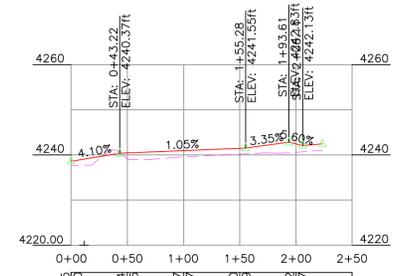
1150 SOUTH STREET



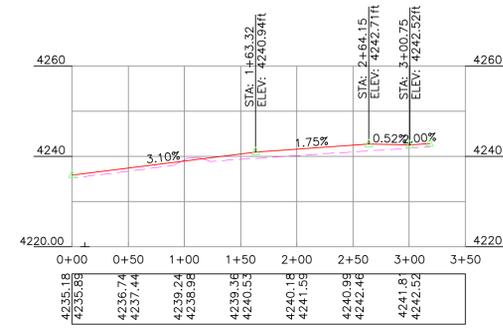
1170 SOUTH STREET



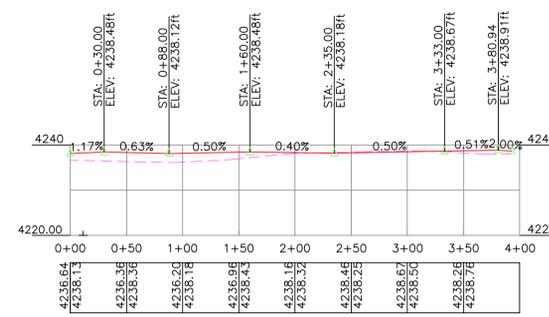
225 SOUTH STREET



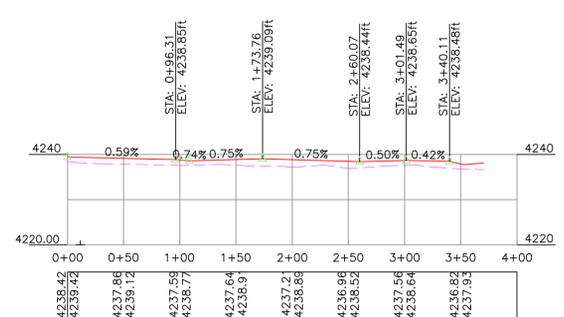
350 SOUTH



250 SOUTH



1185 WEST - NORTH SECTION



1185 WEST - SOUTH SECTION

NO.	REVISIONS / SUBMISSIONS	DATE

PROJECT TITLE: SANDPIPER RIDGE SUBDIVISION
PART OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 9 NORTH, RANGE 2 WEST, MERIDIAN SALT Lake, BRIGHAM CITY, UTAH
DRAWING TITLE: PRELIMINARY PROFILES
DATE: DEC, 2025

DRAWING NO.

Beechwood: 50' frontage



Cali: 50' frontage



Clover: 40' frontage

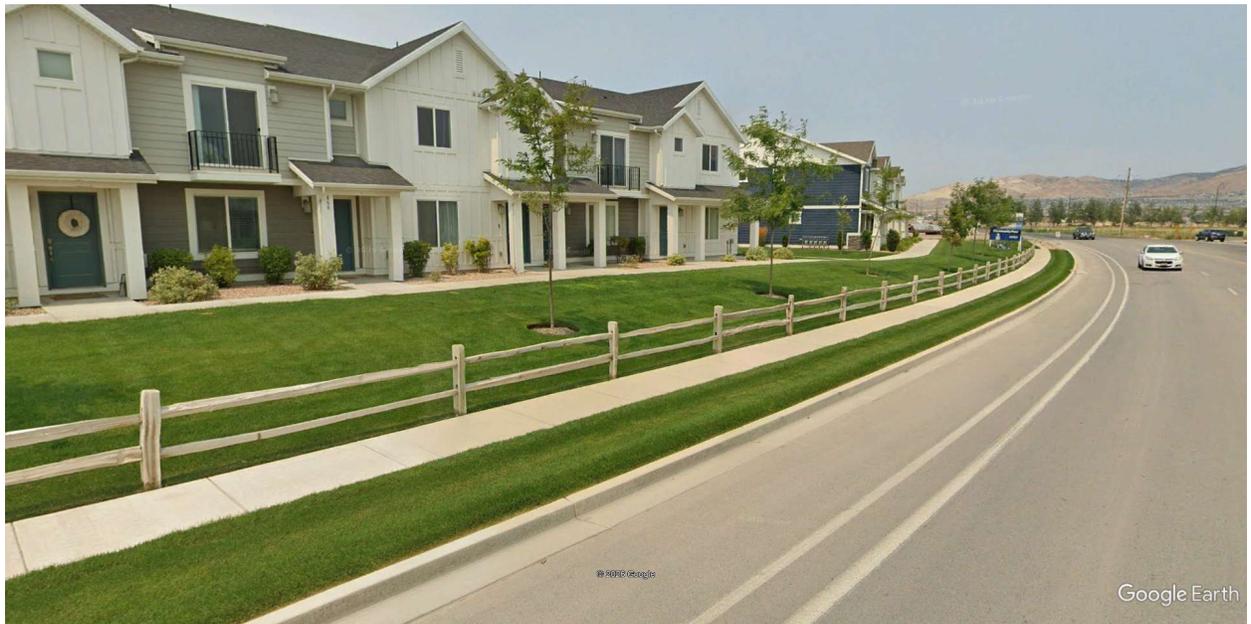


Alta: 36' frontage





Google Earth



Google Earth

CEILING HEIGHTS	
UPPER CEILING	10'-0" MIN
TRIM CEILING	8'-0" MIN
TO GARAGE CEILING	7'-0" MIN

ROOF VENT NOTE:
SEE PAGE 002, LIGHT AND VENTILATION NOTES, PARAGRAPH B9
12" SOFFIT

SECTION AND ELEVATION DRAWINGS TO BE USED FOR PERMITS AND TO CONFORM TO THE CITY OF TREMONTON, UT. THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND FOR OBTAINING ALL NECESSARY APPROVALS FROM THE CITY OF TREMONTON, UT.

ENCLOSURE:
ALL INTERIORS SHALL BE FINISHED WITH A FINISH GRADUALLY APPROXIMATE TO THE EXTERIOR FINISH GRADUALLY APPROXIMATE TO THE EXTERIOR FINISH.

ADDRESS:
MOUNTAIN VALLEY LOT 16
#16, 818 S. 800 E, BOX 6, BOX 6, BOX 6
SMITHFIELD, UTAH 84404

MV 1-5 - PLEX - SMITHFIELD

SIERRA
PROFESSIONAL ARCHITECTS
470 NORTH 2450 WEST
TREMONTON, UT, 84337
CONTACT: SIERRA HODGES
(435) 257-4863

5/2/2025

REVISION: R-33E

FRONT & REAR ELEVATIONS

A06
MATT/AARON



SCALE: 1/4" = 1'

REAR ELEVATION

CEILING HEIGHTS		
(3)2ND FLOOR TREY	8'-1 1/8"	
(2)MAIN FLOOR CEILING	9'-1 1/8"	
(1) S.O.G.	4'-0"	

ROOF VENT NOTE:
SEE PAGES 11, 12 AND VENTILATION NOTES, PARAGRAPH #8

12" SOFFIT

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SIERRA HOMES CONSTRUCTION INC.

ADDRESS:
FREELY ESTATES LOT B0-83
2216 2111 2097 2003 B 1000
W NIBLEY, UTAH 84321

PROJECT:
FFE B0-83 - 4 PLEX - SPEC

SIERRA HOMES
470 NORTH PACED WALK
TREMONTON, UT 84337
CONTACT: SIERRA HOMES
(833) 257-4963

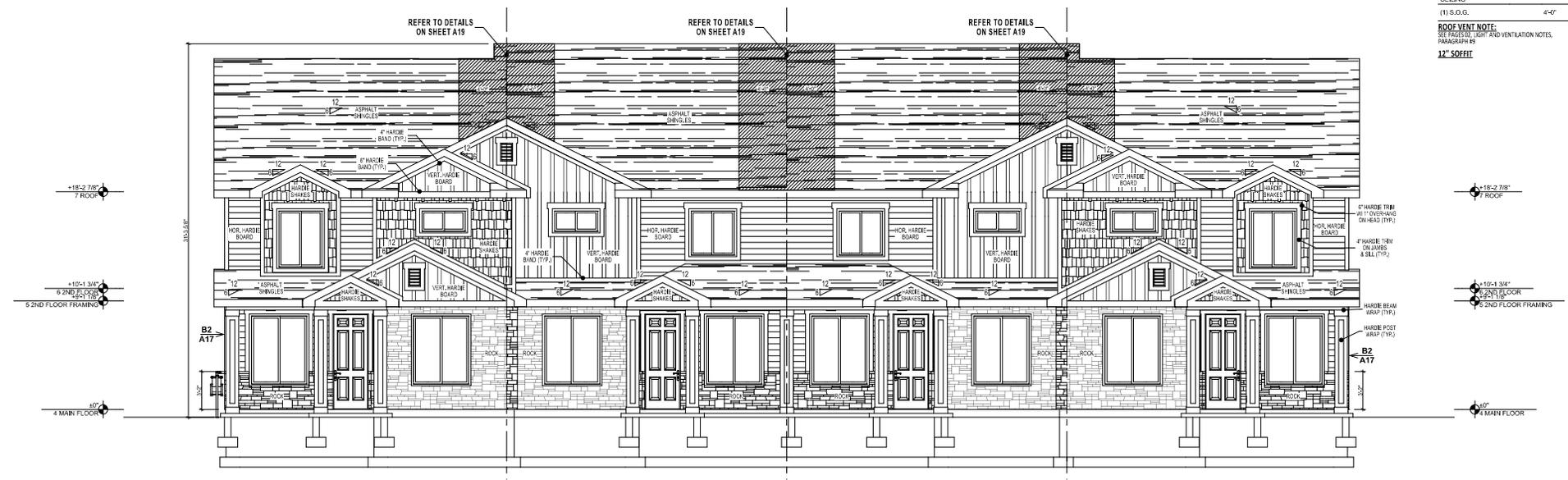
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REVISION R-5E

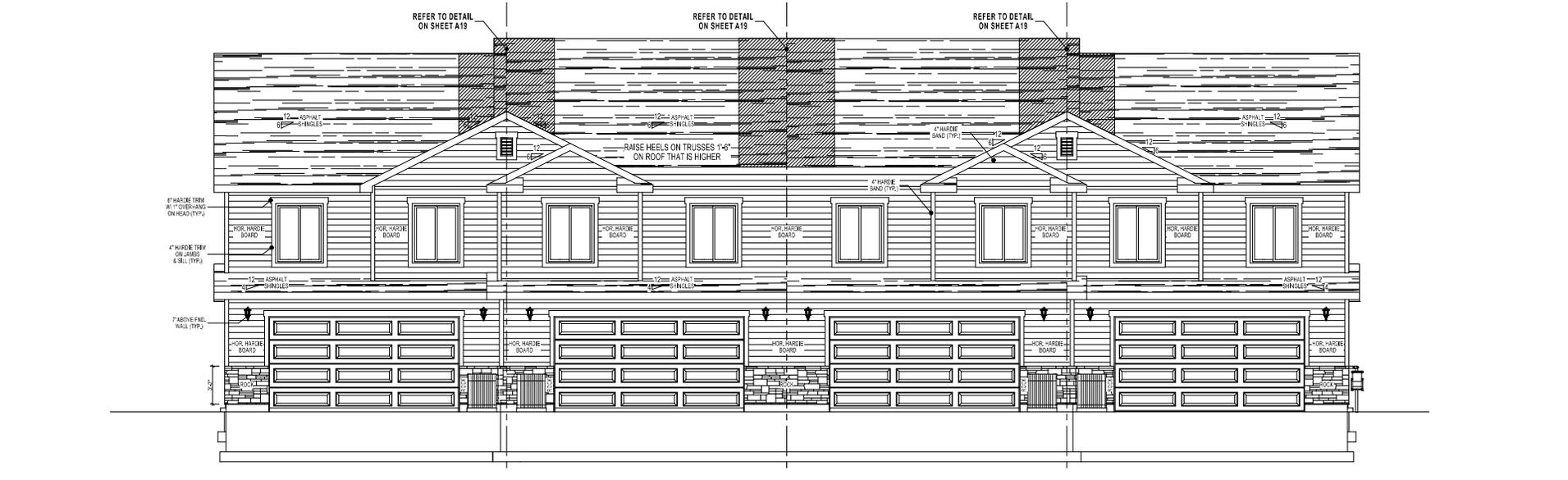
SHEET TITLE:
FRONT & REAR ELEVATIONS

BRANDON T.S.D.

A05



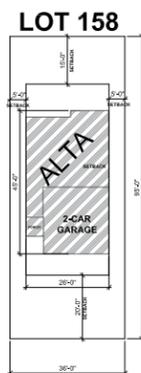
C1 FRONT ELEVATION
SCALE: 1/4" = 1'-0"



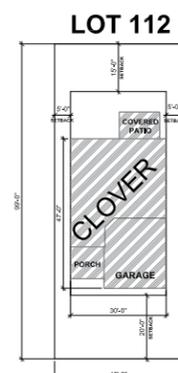
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SCALE: 1/4" = 1'-0"

AVIAN SHORES (SANDPIPER)

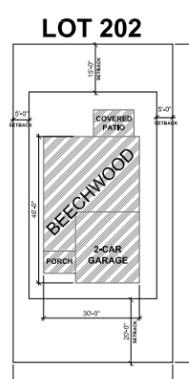
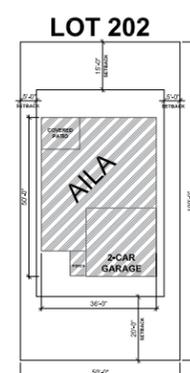
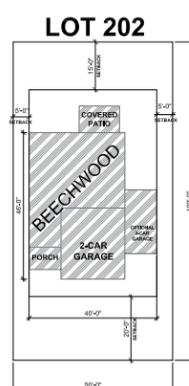
36' WIDE

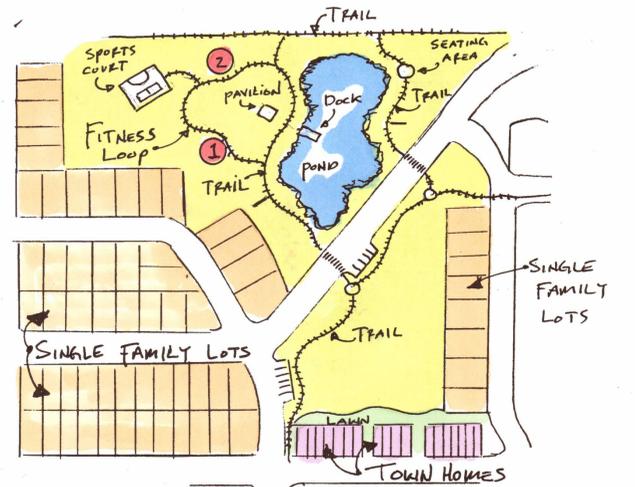
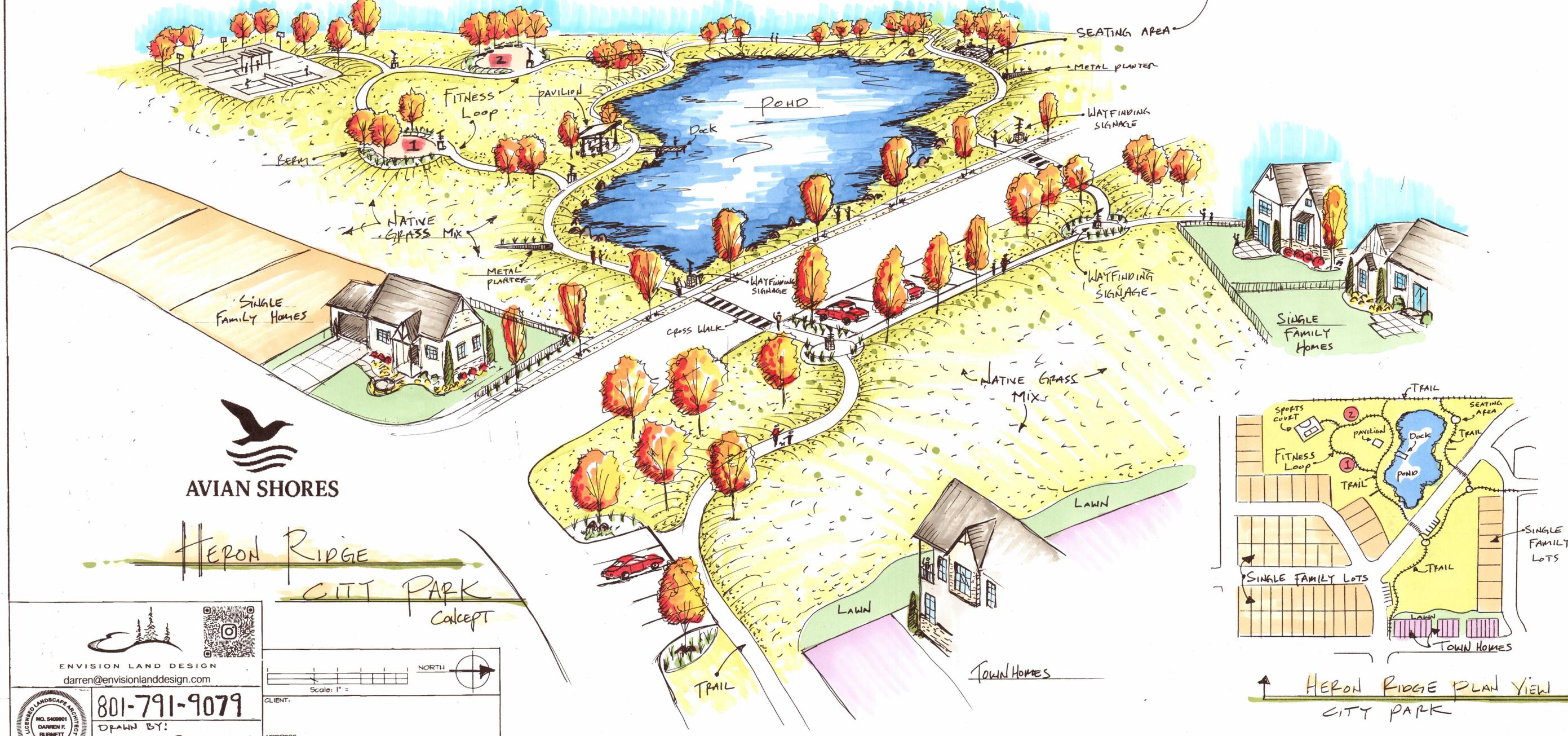


40' WIDE



50' WIDE





AVIAN SHORES

HERON RIDGE
CITY PARK
CONCEPT

ENVISION LAND DESIGN
darren@envisionlanddesign.com

801-791-9079

DRAWN BY:
DARREN BURNETT
LANDSCAPE ARCHITECT

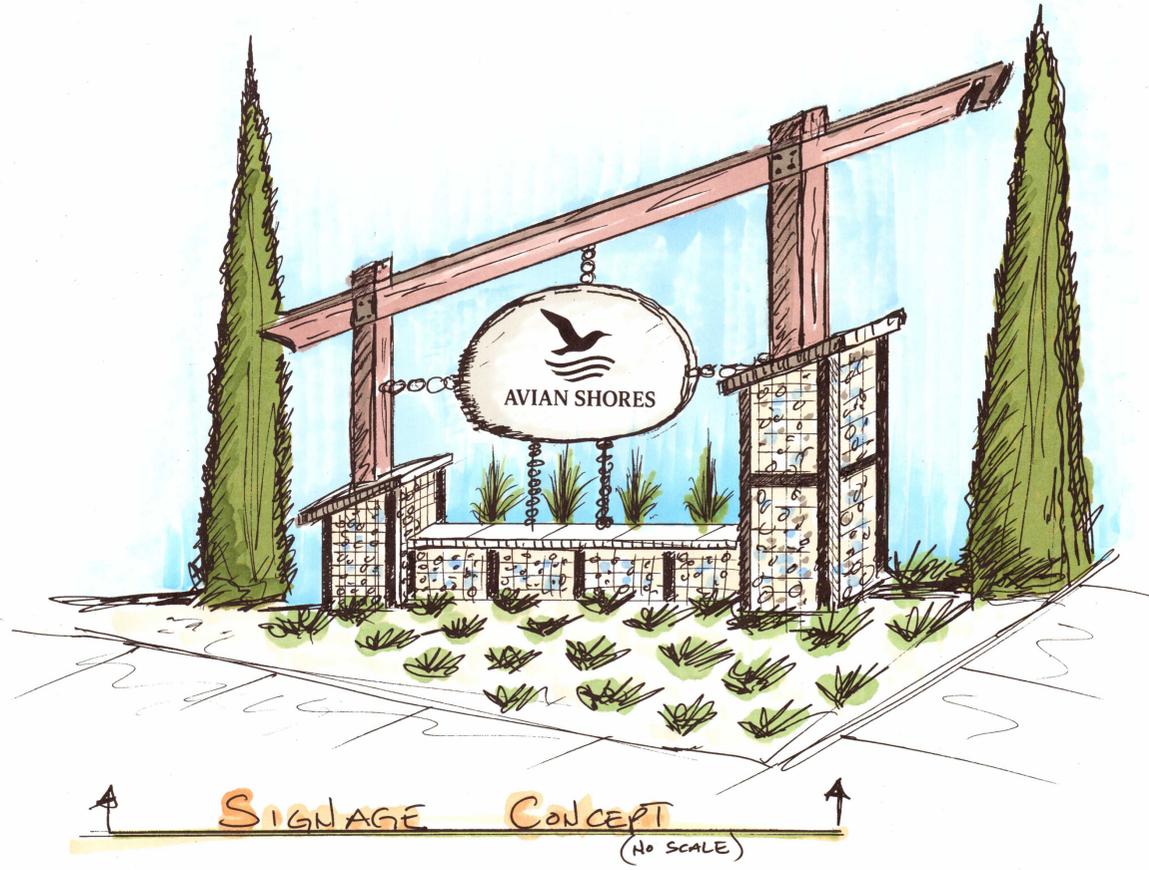
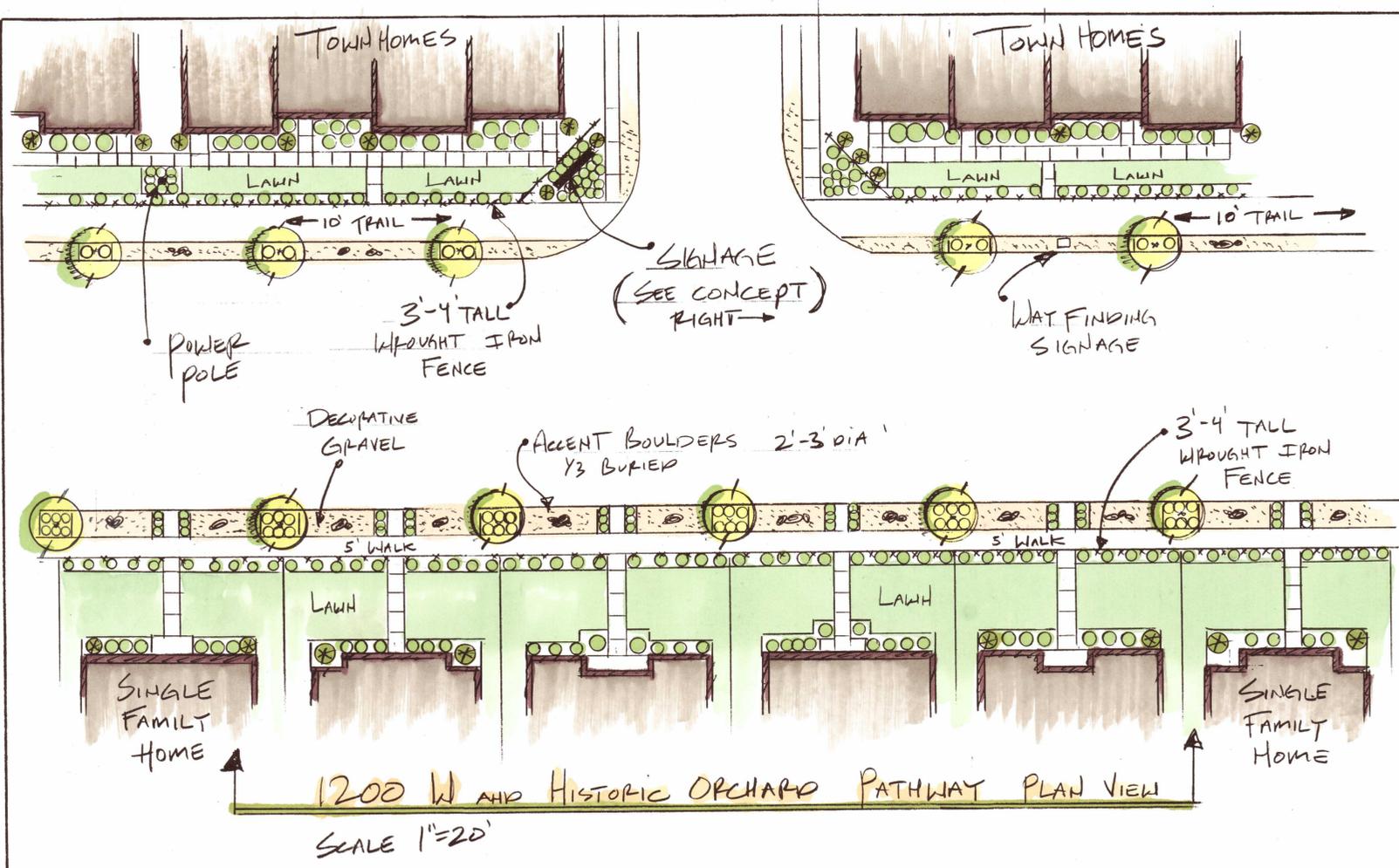
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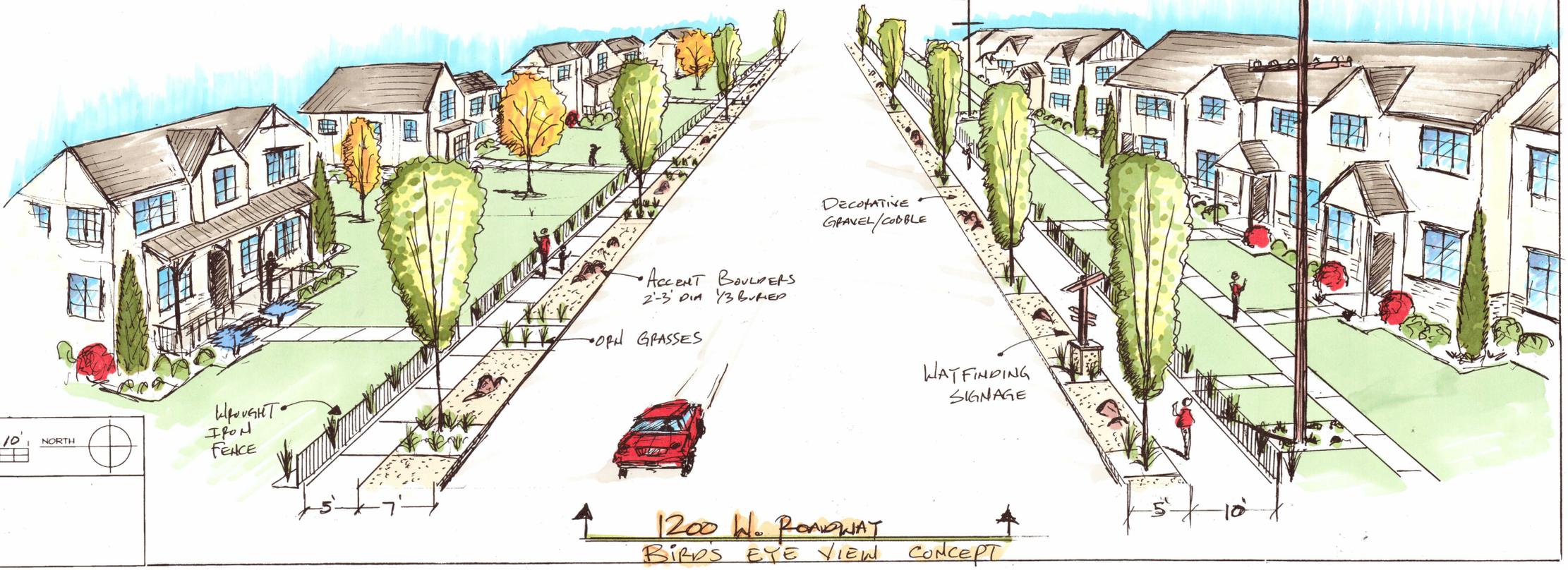
ADDRESS:

NORTH

HERON RIDGE PLAN VIEW
CITY PARK



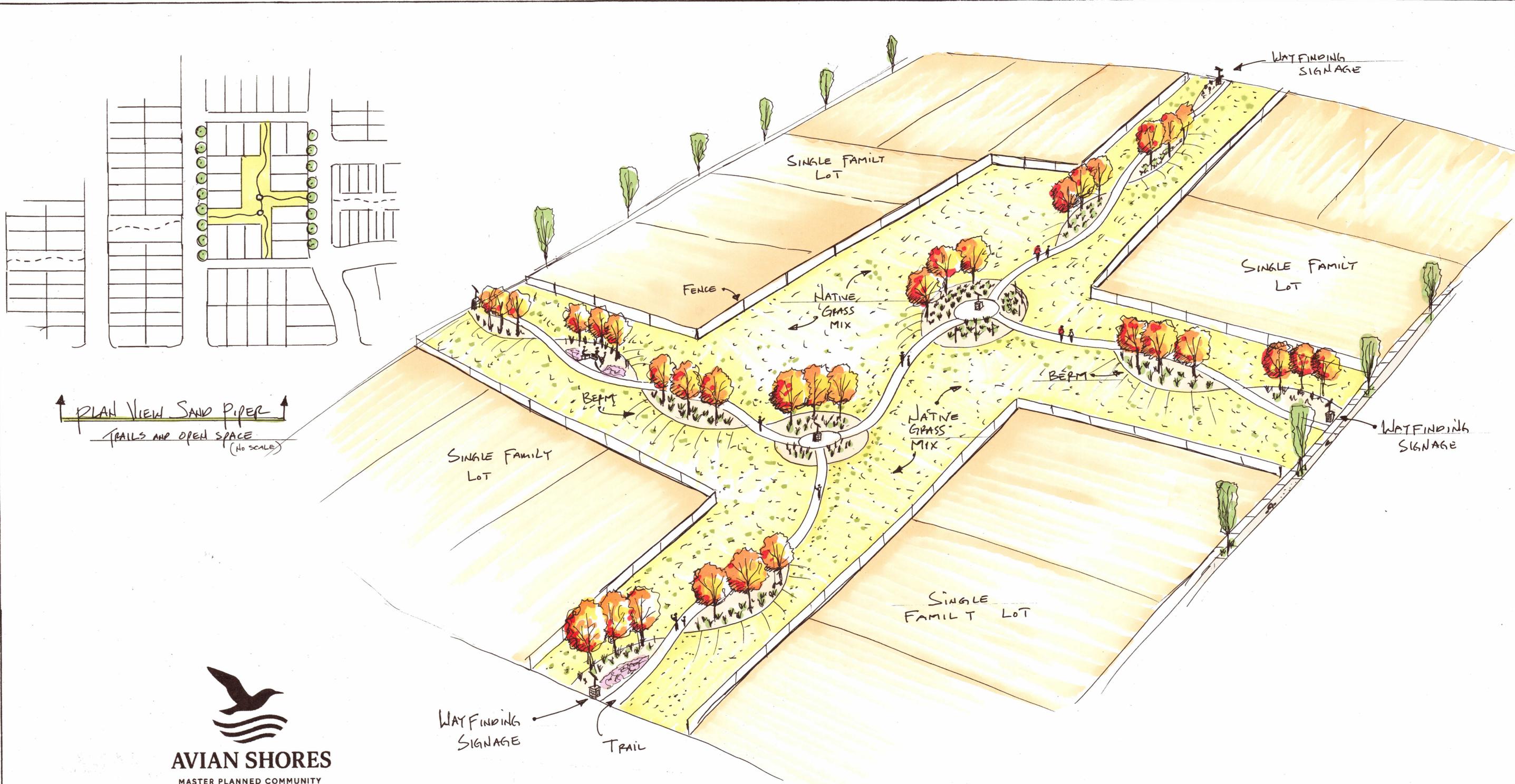

AVIAN SHORES
 MASTER PLANNED COMMUNITY



ENVISION LAND DESIGN
 darren@envisionlanddesign.com
 801-791-9079
 DRAWN BY:
 DARRIN BURNETT
 LANDSCAPE ARCHITECT

Scale: 1" = 20'
 NORTH
 CLIENT:
 ADDRESS:





PLAN VIEW SAND PIPER
TRAILS AND OPEN SPACE
(NO SCALE)

BIRD'S EYE VIEW SAND PIPER
TRAIL AND OPEN SPACE
(NO SCALE)



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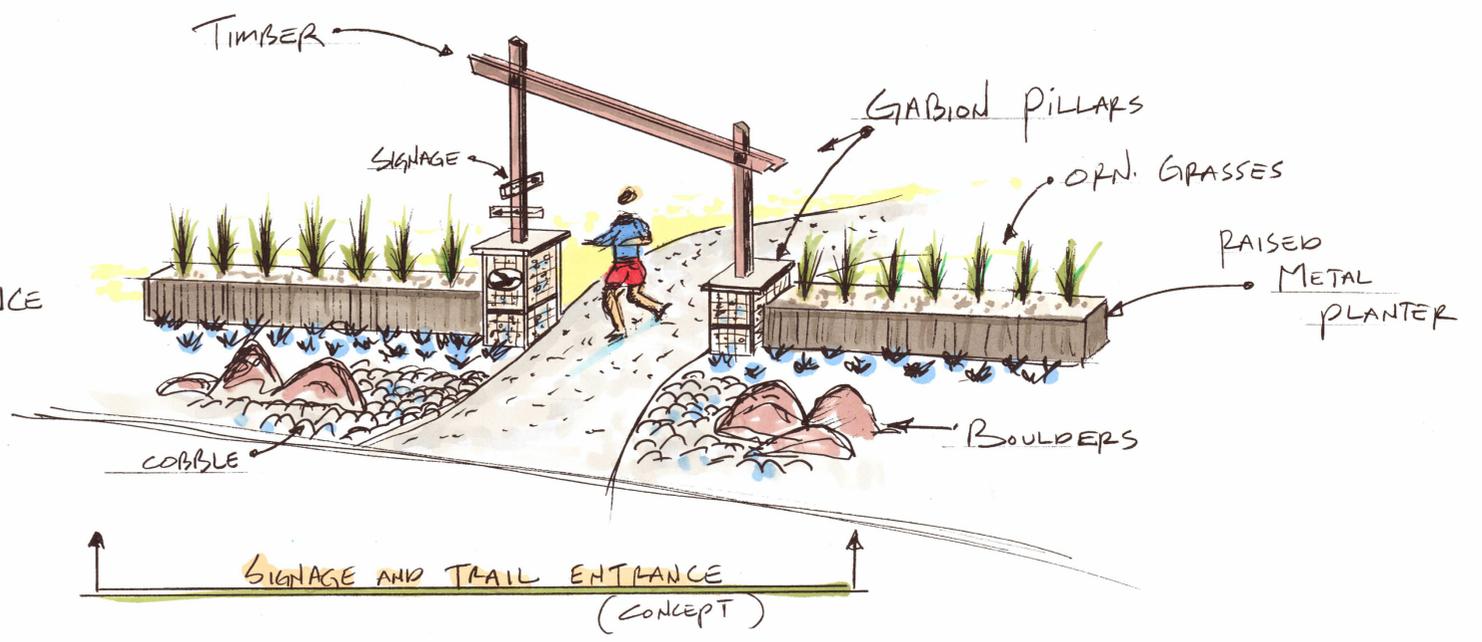
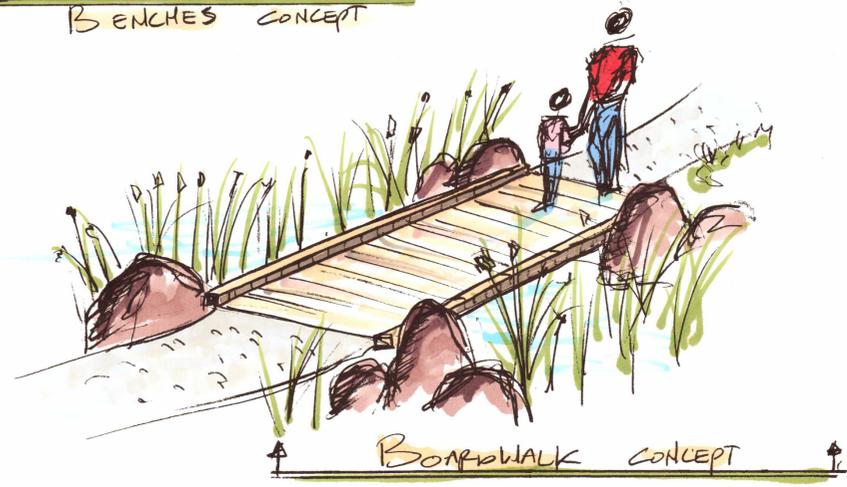
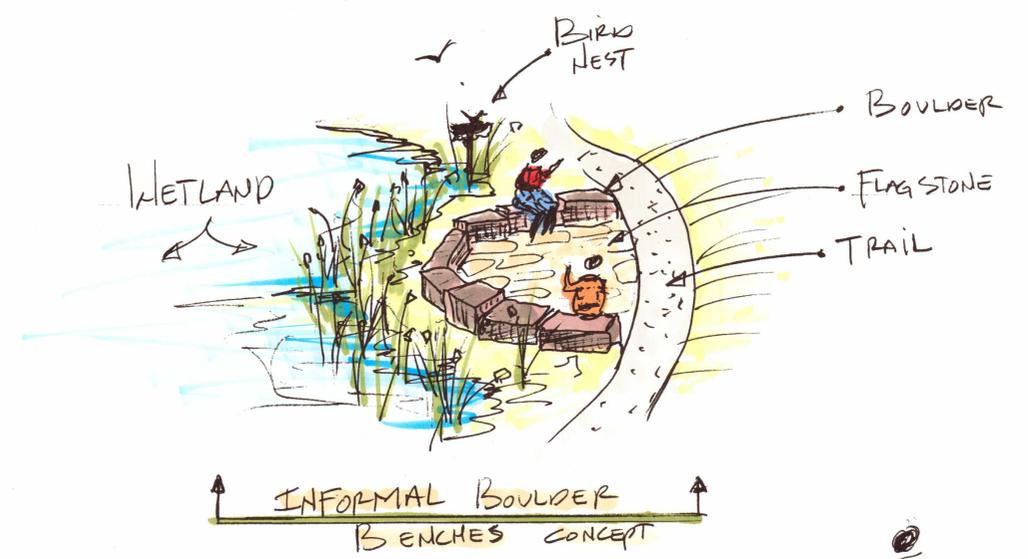
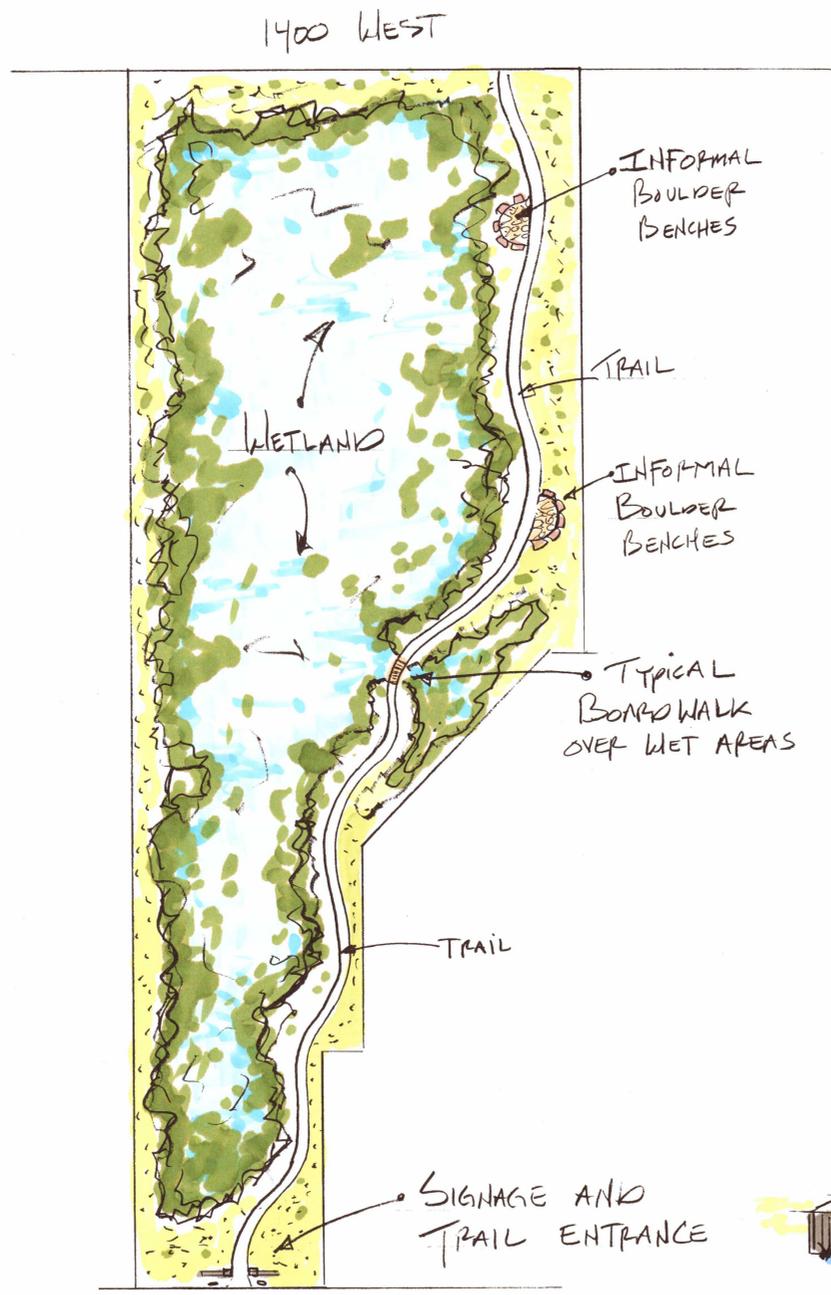
DRAWN BY:
DARREN BURNETT
LANDSCAPE ARCHITECT

Scale: 1" =

NORTH

CLIENT:

ADDRESS:



1200 WEST

ENVISION LAND DESIGN
darren@envisionlanddesign.com

801-791-9079

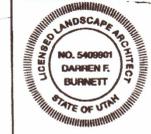
DRAWN BY:
DARREN BURNETT
LANDSCAPE ARCHITECT

Scale: 1" = _____

NORTH

CLIENT: _____

ADDRESS: _____





MEMORANDUM

TO: Planning Commission
FROM: Mark Bradley, City Planner
SUBJECT: Discussion Item: Development of Affordable Single-Family Housing
DATE: March 11, 2026
ACTION: No Action Taken. Discussion Item Only.

OVERVIEW:

Randy David Navarro, property owner of the three properties located south of 100 West stub street, south of 700 South Street has requested a discussion item with the Planning Commission and City Staff. He would like to discuss his property and the development of affordable single-family housing.

Parcel Number	Address	Use
03-147-0013	755 South 100 West	Vacant Boarded Up Home
03-147-0015	761 South 100 West	Occupied Home
03-147-0080	Unassigned	Undeveloped Property

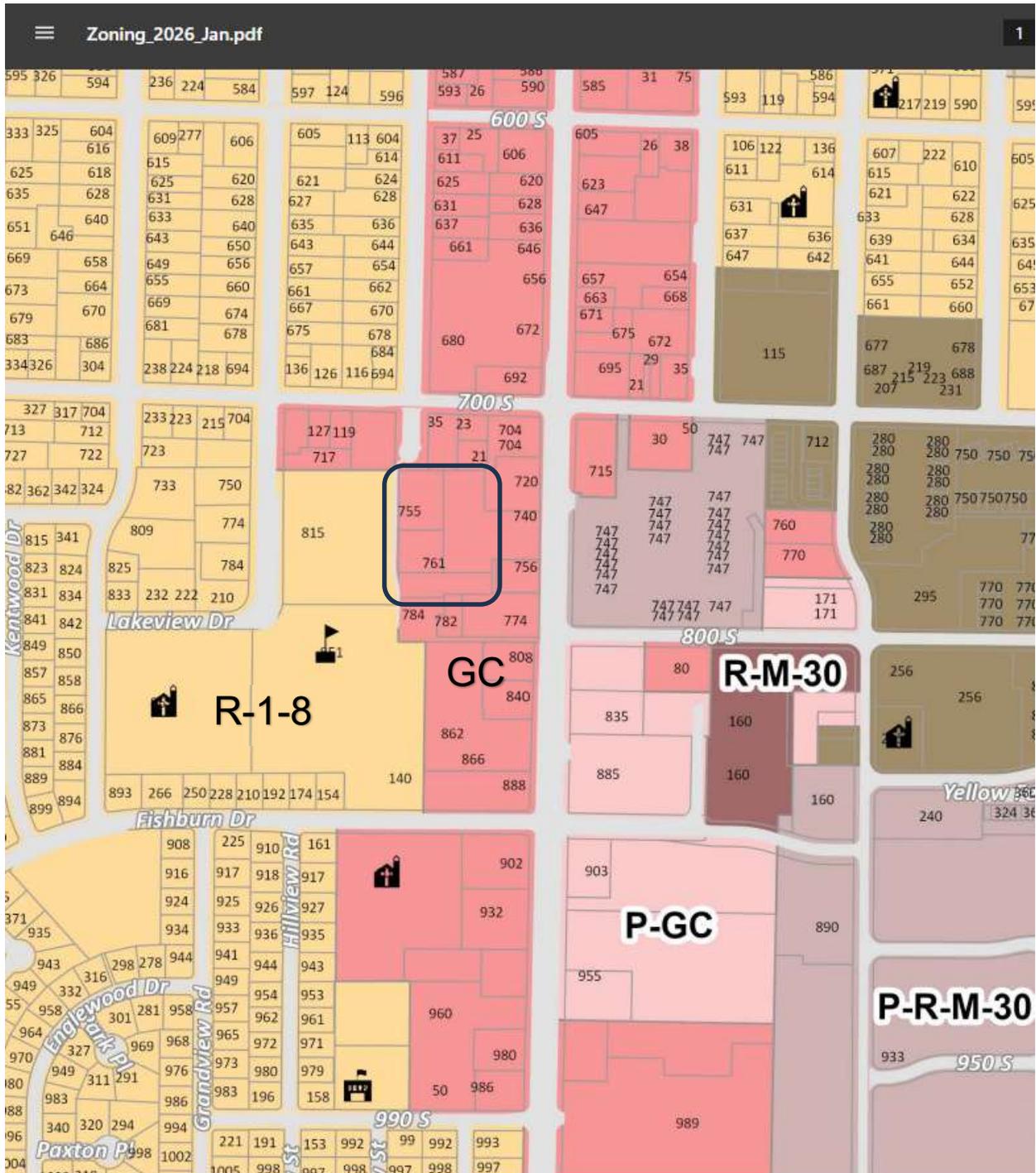
The property is zoned GC General Commercial and does not allow new residential uses except *“single or multiple-family dwellings located in the same building and located above, behind or beneath a principal commercial use”* (City Code 156.136) through a conditional use permit. A rezone application would need to be filed and considered by the Planning Commission and City Council.

The General Plan Future Land Use Designation for these properties is USU District. The table in the General Plan Land Use chapter that includes future land use designation types, purpose, and the “Accompanying/recommended zoning districts” identifies GC, R-M-30, R-M-15 as the accompanying/recommended zoning districts. Single-family dwellings are a permitted use in both the R-M-30 and R-M-15 districts.

ATTACHMENTS:

1. Locator / Parcel Map
2. Zoning Map
3. General Plan Future Land Use Map

ZONING MAP



GENERAL PLAN FUTURE LAND USE MAP

