



HARRISVILLE CITY

363 W. Independence Blvd · Harrisville, Utah 84404 · 801-782-4100
www.harrisvillecity.gov

PLANNING
COMMISSION:

Angie Francom
Chad Holbrook
Isaac Thomas

Harrisville City Planning Commission
Harrisville City Offices
363 W Independence Blvd – Harrisville
Wednesday, July 9, 2025

AGENDA

[Zoom Meeting Link](#)

Meeting ID: 874 4153 6329

Passcode: 478451

- 1. CALL TO ORDER**
- 2. CONSENT APPROVAL** – of Planning Commission meeting minutes from June 11, 2025.
- 3. DISCUSSION/ACTION/RECOMMEND** – to grant Preliminary Site Plan approval for Harrisville Commercial development located at approximately 2550 N 750 W.
- 4. DISCUSSION/ACTION/RECOMMEND** – to grant proposed Conditional Use Permit #147 for Public Safety/City Hall building located at approximately 686 W 1750 N.
- 5. PUBLIC COMMENTS** – (3 minute maximum)
- 6. COMMISSION/STAFF FOLLOW-UP**
- 7. ADJOURN**

Certificate of Posting and Notice

I, Cynthia Benson, certify that I am the Deputy Recorder of Harrisville City, Utah, and that the foregoing Planning Commission agenda was posted and can be found at City Hall, on the City's website www.harrisvillecity.gov, and at the Utah Public Meeting Notice Website at <http://pmn.utah.gov>. Notice of this meeting has also been duly provided as required by law. In accordance with the Americans with Disabilities Act (ADA), the Harrisville City will make reasonable accommodations for participation in the meeting. Please make a request for accommodation with the City Offices at 801-782-4100 at least three (3) business days prior to any meeting.



Harrisville City Planning Commission

Harrisville City Offices
363 W Independence Blvd – Harrisville
Wednesday, June 11, 2025 – 7:00 PM

Commissioners: Angie Francom
Chad Holbrook
Isaac Thomas

Staff: Jennie Knight (City Administrator)
Cynthia Benson (Deputy Recorder)
Sarah Wichern (City Planner)

Visitors: Cory Campell, Taylor Zampedri.

1. CALL TO ORDER

Chair Francom called the meeting to order and welcomed all in attendance.

2. CONSENT APPROVAL – of Planning Commission meeting minutes from May 14, 2025.

MOTION: Commissioner Holbrook motioned to approve Planning Commission meeting minutes from May 14, 2025, as written. Commissioner Thomas seconded the motion.

Angie Francom	Yes
Chad Holbrook	Yes
Isaac Thomas	Yes

The motion passed with all voting in the affirmative.

3. DISCUSSION/ACTION/RECOMMEND – to grant Preliminary Site Plan approval for Harrisville Commercial development located at approximately 2550 N 750 W.

Jennie Knight, City Administrator, informed the commissioners the developer asked for this item to be tabled and placed on the next agenda. The item will be placed on the Planning Commission agenda for July 9, 2025.

MOTION: Commissioner Holbrook motioned to table Preliminary Site Plan approval for Harrisville Commercial development located at approximately 2550 N 750 W. Chair Francom seconded the motion.

Angie Francom	Yes
Chad Holbrook	Yes
Isaac Thomas	Yes

The motion passed with all voting in the affirmative.

4. DISCUSSION/ACTION/RECOMMEND – to grant manufacturing site plan amendment for Elite 3 for property located at approximately 1601 N 750 W.

Sarah Wichern, City Planner, pointed out the changes to the site plan since it was last approved. She added there is not much on the memo outside of adding a handicap stall and receiving approval from the fire department. She asked the commissioners to reference the memo in their motion.

Commissioner Holbrook asked for an explanation of the construction and appearance of the building. Ms.

Wichern explained the building is meant to be individual rented units with a garage door. There will be a retention pond for storm water run-off. Cory Campbell, the developer, further explained the design will be a 60 x 250 steel building structure with 6 flex space units. Each unit is designed to be 2,500 square feet. Each unit will have one (1) garage bay door with a storefront glass entry. The office space can be finished anyway the tenant would like. The current design is flexible to have multiple units combined according to the tenants' needs. They have worked with the fire department and have been signed off. No sprinkler system will be required as long as fire rated partitions are installed to separate the units. They will be adding one additional handicap stall per recommendation. They are discussing the potential of more islands for landscape. The building will have individual store front entrances with alternating finishing to avoid one long line of building and give the building a moderate look. Currently, they are offering a standard office with restroom facilities with the potential of a second-floor office space. The units will be ADA compliant with emergency exits to the rear of the building. Each unit is intended to have individual HVAC systems.

Commissioner Holbrook asked about the hours of operation. Mr. Campbell explained each tenant will determine their own office hours and be required to obtain their own business license.

MOTION: Commissioner Thomas motioned to grant manufacturing site plan amendment for Elite 3 for property located at approximately 1601 N 750 W subject to Harrisville City Municipal code, Engineer's Memo dated June 10, 2025, and any other staff or agency requirements. Commissioner Holbrook seconded the motion.

Angie Francom	Yes
Chad Holbrook	Yes
Isaac Thomas	Yes

The motion passed with all voting in the affirmative.

Mr. Knight asked Mr. Campbell for an update on the building permit status since he had submitted his plans to the building permit portal prior to this meeting. Mr. Campbell replied he received the finalized plans earlier in the day and will be uploading them shortly.

5. DISCUSSION/ACTION/RECOMMEND – to grant proposed Conditional Use Permit #146 for Daycare services as a home occupation located at approximately 2249 N 650 W.

Commissioner discussion recommended to table the item due to applicant not present to answer any questions by the commission.

MOTION: Commissioner Holbrook motioned to table Conditional Use Permit #146. Commissioner Thomas seconded the motion.

Angie Francom	Yes
Chad Holbrook	Yes
Isaac Thomas	Yes

The motion passed with all voting in the affirmative.

6. PUBLIC COMMENTS – (3 minute maximum)

No public comments.

7. COMMISSION/STAFF FOLLOW-UP

Ms. Knight updated the commission on Ordinance #562 – Housing Affordability. The staff are suggesting the next presentation be a combined City Council and Planning Commission work session. In leu of the busy upcoming City Council agendas, the work session will most likely not occur until September. If there are any questions or comments, reach out to herself or Ms. Wichern. In the meantime, the city attorney is looking over the ordinance.

8. ADJOURN

MOTION: Chair Francom motioned to adjourn the meeting. Commissioner Thomas seconded.

Angie Francom	Yes
Chad Holbrook	Yes
Isaac Thomas	Yes

The motion passed with all voting in the affirmative.

Meeting adjourned at 7:15 PM.

Angie Francom
Chair

Cynthia Benson
Deputy Recorder

MEMORANDUM

TO: Harrisville City Planning Commission

FROM: Matt Robertson, City Engineer
Sarah Wichern, City Planner

RE: **2550 NORTH 750 WEST COMMERCIAL SITE**
Preliminary Site Plan Review

Date: June 5, 2025

Our office has completed a review of the preliminary site plan for the commercial site located at approximately 2550 North 750 West. The parcel has previously been rezoned to CP-2. The proposed plan shows a new 28,000 SF building and associated parking lot per the attached plans. The development will improve the roadway on 750 West and on 2550 North. We recommend preliminary approval at this time with the following comments that will need to be addressed prior to final approval:

1. Rear parking lot setback should be 15 feet.
2. All improvements along 2550 North will need to be approved by Pleasant View City and will need to follow their standards.
3. Update the pavement section on 750 West to follow the City Standards (4" HMA, 6" UTBC, 12" GB).
4. Provide a landscape plan for the site per city code 11.13.040.
5. Call out the type of fencing shown to ensure that screening requirements are met.
6. Update the storm drain design and calculations to follow the City Standard of an allowable release rate of 0.1 cfs/acre.
7. The trash enclosure floor should be sloped; see city code 11.20.230.
8. The west drive approach on 2550 North and south drive approach on 750 West are too close to the existing drive approaches on adjacent property; see city code 11.13.030. Check Pleasant View code as well for spacing requirements.
9. Provide details for any signage that will be used, see city code 11.23.
10. Check with Weber Basin Conservancy District and ensure that the improvements are allowed within the source protection zone of the well on the adjacent property.
11. Revise site plan drawings per red-line comments on the plans.
12. Provide approval letters from Pleasant View City, Bona Vista Water, Pineview Water, North View Fire District, and Weber Basin (well).

Please let us know if you have any questions.

HARRISVILLE COMMERCIAL

CONSTRUCTION DOCUMENTS HARRISVILLE CITY, WEBER COUNTY , UTAH

TRAFFIC CONTROL & SAFETY NOTES

1. BARRICADING AND DETOURING SHALL BE IN CONFORMANCE WITH THE REQUIREMENTS OF THE CURRENT STATE OF UTAH DEPARTMENT OF TRANSPORTATION MANUAL OF TRAFFIC CONTROLS FOR CONSTRUCTION AND MAINTENANCE WORK ZONES, AND THE HARRISVILLE CITY STANDARD DRAWING, AND SHALL BE APPROVED BY THE HARRISVILLE CITY ENGINEER PRIOR TO ANY WORK.
2. NO STREET SHALL BE CLOSED TO TRAFFIC WITHOUT WRITTEN PERMISSION FROM THE HARRISVILLE CITY TRAFFIC ENGINEER, EXCEPT WHEN DIRECTED BY LAW ENFORCEMENT OR FIRE OFFICIALS.
3. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO PROVIDE FOR SMOOTH TRAFFIC FLOW AND SAFETY. ACCESS SHALL BE MAINTAINED FOR ALL PROPERTIES ADJACENT TO THE WORK.
4. DETOURING OPERATIONS FOR A PERIOD OF SIX CONSECUTIVE CALENDAR DAYS, OR MORE, REQUIRE THE INSTALLATION OF TEMPORARY STREET STRIPING AND REMOVAL OF INTERFERING STRIPING BY SANDBLASTING. THE DETOURING STRIPING PLAN OR CONSTRUCTION TRAFFIC CONTROL PLAN MUST BE SUBMITTED TO THE HARRISVILLE CITY TRAFFIC ENGINEER FOR REVIEW AND APPROVAL.
5. ALL TRAFFIC CONTROL DEVICES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE END OF THE WORK TO THE SATISFACTION OF THE HARRISVILLE CITY TRAFFIC ENGINEER
6. TRAFFIC CONTROL DEVICES (TCDs) SHALL REMAIN VISIBLE AND OPERATIONAL AT ALL TIMES.

UTILITY DISCLAIMER

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT EXISTING UNDERGROUND UTILITIES AND IMPROVEMENTS ARE SHOWN IN THEIR APPROXIMATE LOCATIONS BASED UPON RECORD INFORMATION AVAILABLE AT THE TIME OF PREPARATION OF PLANS. LOCATIONS MAY NOT HAVE BEEN VERIFIED IN THE FIELD AND NO GUARANTEE IS MADE AS TO ACCURACY OR COMPLETENESS OF THE INFORMATION SHOWN. IT SHALL BE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE EXISTENCE AND LOCATION OF THOSE UTILITIES SHOWN ON THESE PLANS OR INDICATED IN THE FIELD BY LOCATING SERVICES. ANY ADDITIONAL COSTS INCURRED AS A RESULT OF CONTRACTOR'S FAILURE TO VERIFY LOCATIONS OF EXISTING UTILITIES PRIOR TO BEGINNING OF CONSTRUCTION IN THEIR VICINITY SHALL BE BORNE BY THE CONTRACTOR AND ASSUMED INCLUDED IN THE CONTRACT.

NOTICE TO CONTRACTOR

ALL CONTRACTORS AND SUBCONTRACTORS PERFORMING WORK SHOWN ON OR RELATED TO THESE PLANS SHALL CONDUCT THEIR OPERATIONS SO THAT ALL EMPLOYEES ARE PROVIDED A SAFE PLACE TO WORK AND THE PUBLIC IS PROTECTED. ALL CONTRACTORS AND SUBCONTRACTORS SHALL COMPLY WITH THE "OCCUPATIONAL SAFETY AND HEALTH REGULATIONS" OF THE U.S. DEPARTMENT OF LABOR AND THE STATE OF UTAH DEPARTMENT OF INDUSTRIAL RELATIONS CONSTRUCTION SAFETY ORDERS". THE CIVIL ENGINEER SHALL NOT BE RESPONSIBLE IN ANY WAY FOR CONTRACTORS AND SUBCONTRACTORS COMPLIANCE WITH SAID REGULATIONS AND ORDERS.

CONTRACTOR FURTHER AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB-SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE CIVIL ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.

SANITARY SEWER GENERAL NOTES

1. ALL SANITARY SEWER CONSTRUCTION SHALL BE IN CONFORMANCE WITH HARRISVILLE CITY STANDARDS AND SPECIFICATIONS.
2. ALL GRAVITY SANITARY SEWER LINES SHALL BE SDR-35 PVC MATERIAL. SEWER LINE CONSTRUCTION AND MATERIALS SHALL CONFORM TO ASTM STANDARDS AND SPECIFICATIONS.
3. DISTANCES SHOWN ON PLANS ARE APPROXIMATE AND COULD VARY DUE TO VERTICAL ALIGNMENT.
4. RIM ELEVATIONS SHOWN ARE APPROXIMATE ONLY AND ARE NOT TO BE TAKEN AS FINAL ELEVATION. PIPELINE CONTRACTOR SHALL USE PRECAST CONCRETE ADJUSTMENT RINGS, GROUT AND STEEL SHIMS TO ADJUST THE MANHOLE FRAME TO THE REQUIRED FINAL GRADE IN CONFORMANCE WITH THE STANDARD SPECIFICATIONS. ALL FRAMES SHALL BE ADJUSTED TO FINAL GRADE.
5. ALL SANITARY SEWER MAIN TESTING SHALL BE IN ACCORDANCE WITH HARRISVILLE CITY STANDARDS AND SPECIFICATIONS. COPIES OF ALL TEST RESULTS SHALL BE PROVIDED TO THE PUBLIC WORKS SANITARY SEWER DEPARTMENT HEAD PRIOR TO FINAL ACCEPTANCE.
6. COMPACTION TESTING OF ALL TRENCHES WITH THE PROJECT SITE MUST BE ATTAINED AND RESULTS SUBMITTED TO THE HARRISVILLE CITYENGINEER PRIOR TO FINAL ACCEPTANCE.
7. CONTRACTOR IS RESPONSIBLE TO PROTECT ALL EXISTING STRUCTURES AND IMPROVEMENTS DURING INSTALLATION OF SANITARY SEWER LINE.
8. WHERE CONNECTION TO EXISTING UTILITY IS PROPOSED, CONTRACTOR SHALL VERIFY LOCATION AND ELEVATION AND NOTIFY OWNER/ENGINEER IF LOCATION AND ELEVATION OF EXISTING UTILITY VARIES FROM THE DESIGN.
9. CAMERA TESTING AND PRESSURE TESTING PER HARRISVILLE CITY STANDARD.

GENERAL NOTES

1. ALL MATERIALS, WORKMANSHIP AND CONSTRUCTION OF SITE IMPROVEMENTS SHALL MEET OR EXCEED THE STANDARDS AND SPECIFICATIONS SET FORTH BY THE HARRISVILLE CITY ENGINEER, PLANNING, CODES AND SPECIFICATIONS AND APPLICABLE STATE AND FEDERAL REGULATIONS. WHERE THERE IS CONFLICT BETWEEN THESE PLANS AND SPECIFICATIONS, OR ANY APPLICABLE STANDARDS, THE HIGHER QUALITY STANDARD SHALL APPLY.
2. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND OR ELEVATION OF EXISTING UTILITIES, AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY PERTINENT LOCATIONS AND ELEVATIONS, ESPECIALLY AT THE CONNECTION POINTS AND AT POTENTIAL UTILITY CONFLICTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS.
3. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM ALL APPLICABLE AGENCIES. THE CONTRACTOR SHALL NOTIFY THE DESIGNATED PUBLIC WORKS INSPECTOR AT LEAST 48 HOURS PRIOR TO THE START OF ANY EARTH DISTURBING ACTIVITY, OR CONSTRUCTION ON ANY AND ALL PUBLIC IMPROVEMENTS.
4. THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH HARRISVILLE CITY AND ALL UTILITY COMPANIES INVOLVED WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION AND TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION AND WITH A MINIMUM DISRUPTION OF SERVICE.
5. THE CONTRACTOR SHALL HAVE ONE (1) COPY OF APPROVED PLANS, AND ONE (1) COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS AND A COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB, ON SITE AT ALL TIMES.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.
7. IF DURING THE CONSTRUCTION PROCESS CONDITIONS ARE ENCOUNTERED BY THE CONTRACTOR, HIS SUBCONTRACTORS, OR OTHER AFFECTED PARTIES, WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY.
8. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF THE INTENDED IMPROVEMENTS SHOWN ON THESE DRAWINGS OR DESIGNATED TO BE PROVIDED, INSTALLED, CONSTRUCTED, REMOVED AND RELOCATED UNLESS SPECIFICALLY NOTED OTHERWISE.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING AS-BUILT DRAWINGS ON A SET OF RECORD DRAWINGS KEPT AT THE CONSTRUCTION SITE, AND AVAILABLE TO THE CITY INSPECTOR AT ALL TIMES.
11. THE CONTRACTOR SHALL SEQUENCE INSTALLATION OF UTILITIES IN SUCH A MANNER AS TO MINIMIZE POTENTIAL UTILITY CONFLICTS. IN GENERAL, STORM SEWER AND SANITARY SEWER SHOULD BE CONSTRUCTED PRIOR TO INSTALLATION OF WATER LINES AND DRY UTILITIES.
12. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE ALL UTILITY RELOCATIONS CONSISTENT WITH THE CONTRACTORS SCHEDULE FOR THIS PROJECT, WHETHER SHOWN OR NOT SHOWN AS IT RELATES TO THE CONSTRUCTION ACTIVITIES CONTEMPLATED IN THESE PLANS.

SWPPP GENERAL NOTES

1. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AS REQUIRED BY HARRISVILLE CITY AND STATE OF UTAH.
2. ALL STRUCTURAL EROSION MEASURES SHALL BE INSTALLED AS SHOWN ON THE SWPP PLAN, PRIOR TO ANY OTHER GROUND-DISTURBING ACTIVITY. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED IN GOOD REPAIR BY THE CONTRACTOR, UNTIL SUCH TIME AS THE ENTIRE DISTURBED AREAS ARE STABILIZED WITH HARD SURFACE OR LANDSCAPING.

STORM SEWER GENERAL NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING:
A)OBTAIN ALL REQUIRED PERMITS FROM HARRISVILLE CITY OR REGULATORY AGENCIES, INCLUDING PERMITS TO WORK IN THE RIGHT-OF-WAY.
B)RESTORATION OF EXISTING IMPROVEMENTS INCLUDING BUT NOT LIMITED TO FENCES, SOD, LANDSCAPING, PAVEMENT, SPRINKLER SYSTEM.
C)VERIFICATION AND PROTECTION OF ALL EXISTING IMPROVEMENTS WITHIN THE LIMITS OF CONSTRUCTION.
D)PROVIDING AS-BUILT DRAWINGS TO HARRISVILLE CITY AND THE ENGINEER.
E)ALL PERMITTING, DEVELOPMENT, LOCATION, CONNECTION AND INSPECTION AND SCHEDULING FOR SUCH.
2. ALL STORM SEWER CONNECTIONS SHALL BE IN CONFORMANCE WITH HARRISVILLE CITY STANDARDS AND SPECIFICATIONS.
3. RIM ELEVATIONS SHOWN ARE APPROXIMATE ONLY AND ARE NOT TO BE TAKEN AS FINAL ELEVATION. PIPELINE CONTRACTOR SHALL USE PRECAST CONCRETE ADJUSTMENT RINGS, GROUT, AND STEEL SHIMS TO ADJUST THE MANHOLE FRAME TO THE REQUIRED FINAL GRADE IN CONFORMANCE WITH HARRISVILLE CITY STANDARDS AND SPECIFICATIONS AND PLANS. ALL FRAMES SHALL BE ADJUSTED TO FINAL GRADE PRIOR TO PLACEMENT OF ASPHALT PAVING.
4. COMPACTION OF ALL TRENCHES WITHIN THE PROJECT SITE MUST BE ATTAINED AND COMPACTION RESULTS SUBMITTED TO THE ENGINEER AND HARRISVILLE CITY PRIOR TO FINAL ACCEPTANCE.
5. ALL STORM DRAIN PIPES IN THE HARRISVILLE CITY RIGHT-OF-WAY SHALL BE RCP CL III.
6. ALL STORM SEWER MANHOLES IN PAVED AREAS SHALL BE FLUSH WITH THE PAVEMENT AND SHALL HAVE TRAFFIC BEARING LIDS. ALL STORM SEWER LIDS SHALL BE LABELED "STORM DRAIN".
7. WHERE CONNECTION TO EXISTING UTILITY IS PROPOSED, CONTRACTOR SHALL VERIFY LOCATION AND ELEVATION AND NOTIFY OWNER/ENGINEER IF LOCATION AND ELEVATION OF EXISTING UTILITY VARIES FROM THE DESIGN.

GENERAL GRADING NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST APWA STANDARDS AND SPECIFICATION FOR PUBLIC WORKS AND HARRISVILLE CITY STANDARDS. CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE AWAY FROM BUILDING FOUNDATIONS AND ENTRIES. FINISHED GRADE AT FOUNDATION FOR WOOD FRAMED STRUCTURES SHALL BE 8 INCHES BELOW TOP OF FOUNDATION AND DRAINAGE SHALL BE A MINIMUM OF 5% WITHIN 10 FEET FROM THE BUILDING.
2. MAXIMUM SLOPES SHALL BE 3:1 FOR CUT AND FILL UNLESS OTHERWISE NOTED.
3. COMPACTION REQUIREMENTS AND TESTING SHALL BE PERFORMED TO MEET HARRISVILLE CITY STANDARDS.
4. NO FILL SHALL BE PLACED UNTIL VEGETATION HAS BEEN REMOVED AND SUB-GRADE PREPARED PER THE SOILS REPORT.
5. DUST SHALL BE CONTROLLED BY WATERING OR OTHER APPROVED METHODS.
6. CONTRACTOR SHALL COMPLY WITH STORM WATER POLLUTION PREVENTION PLAN BY INSTALLING BMP'S PRIOR TO COMMENCEMENT OF EXCAVATION ACTIVITIES. CONTACT THE HARRISVILLE CITY INSPECTOR FOR INSPECTION.
7. ALL RECOMMENDATIONS OF THE GEOTECHNICAL REPORT AND ALL SUBSEQUENT REPORTS, ADDENDUM ETC. SHALL BE CONSIDERED A PART OF THIS GRADING PLAN AND SHALL BE COMPLIED WITH.
8. THE CONTRACTOR SHALL CONTACT BLUE STAKES FOR LOCATION MARKING PRIOR TO COMMENCING EXCAVATION ACTIVITIES.
9. HARRISVILLE CITY MAY REQUIRE A PRE-CONSTRUCTION MEETING BEFORE A PERMIT IS ISSUED.
10. STREETS ADJACENT TO THE PROJECT SHALL BE CLEAN AT ALL TIMES.
11. CONTRACTOR IS RESPONSIBLE FOR ARRANGING FOR ALL REQUIRED INSPECTIONS.
12. PRIOR TO TAKING WATER FROM A HARRISVILLE CITY FIRE HYDRANT, THE CONTRACTOR SHALL MAKE ARRANGEMENTS WITH BONA VISTA WATERTO OBTAIN A WATER METER.

CULINARY WATER GENERAL NOTES

1. ALL INSTALLATION AND MATERIALS SHALL CONFORM TO BONA VISTA WATER STANDARDS, SPECIFICATIONS AND PLANS.
2. THRUST BLOCKING IS REQUIRED AT ALL BENDS AND FITTINGS. THE RODS SHALL BE USED AT ALL BENDS AND FITTINGS WHERE THRUST BLOCKS DO NOT BEAR AGAINST UNDISTURBED SOIL.
3. ALL WATERLINES AT SEWER CROSSINGS SHALL BE LOCATED ABOVE AND HAVE AN 18-INCH VERTICAL SEPARATION FROM THE SEWER PIPE. IF THIS IS NOT PROVIDED, THE WATERLINE SHALL BE INSTALLED WITH 20 L.F. OF CONCRETE CASING CENTERED OVER THE SEWER PIPE.
4. DISINFECTION TESTS SHALL BE PERFORMED BY THE WATER UTILITY WITH COOPERATION FROM THE CONTRACTOR IN PERFORMING ANY NECESSARY EXCAVATION AND SUBSEQUENT BACKFILLING AT NO COST TO BONA VISTA WATER.
5. CHLORINATION OF COMPLETED WATER LINE. THE NEW WATER LINES SHALL BE DISINFECTED BY CHLORINATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL RELATED COSTS AND FEES RELATED TO THE CHLORINATION OF THE COMPLETED WATER LINE. THIS TEST SHALL BE PERFORMED PRIOR TO CONNECTION OF THE NEW WATER LINES TO THE EXISTING WATER SYSTEM. THE CONTRACTOR SHALL NOTIFY BONA VISTA WATER AT LEAST 24 HOURS BEFORE THE CHLORINATION IS DESIRED.
6. A MINIMUM HORIZONTAL CLEARANCE OF 10 FEET SHALL BE MAINTAINED FROM SANITARY SEWER MAINS.
7. UNLESS OTHERWISE SPECIFIED, ALL WATERLINES SHALL BE AWWA C900 PVC CLASS 150, PER ASTM D2241.
8. CONTRACTOR SHALL LOCATE VALVES PRIOR TO CONNECTION WITH EXISTING SYSTEM, BUT SHALL NOT OPERATE ANY VALVE WITHOUT PERMISSION FROM BONA VISTA WATER.
9. ALL WATER MAINS, VALVES, FIRE HYDRANTS, SERVICES AND APPURTENANCES SHALL BE INSTALLED, TESTED, AND APPROVED PRIOR TO PAVING.
10. THERE SHALL BE A WATER SUPPLY TO THE DEVELOPMENT BEFORE ANY WOOD CONSTRUCTION STARTS.
11. BONA VISTA WATER REQUIRES THE USE OF CORROSION RESISTANT MATERIALS FOR ALL CULINARY WATER IMPROVEMENTS. SPECIFICALLY, ROMAC BLUE BOLTS OR STAINLESS STEEL BOLTS MUST BE USED ON ALL FITTINGS. FURTHER, ALL METAL FITTINGS SHALL BE POLY WRAPPED.

SECONDARY WATER GENERAL NOTES

1. ALL INSTALLATION AND MATERIALS SHALL ADHERE TO PINEVIEW WATER STANDARDS AND SPECIFICATIONS.
2. PIPES SHALL BE C-900 DR18 PVC (PURPLE), WITH LOCATOR TAPE AND 14 GA. COPPER LOCATOR WIRE.

SHEET INDEX

C1	-	COVER SHEET
C2	-	DEMOLITION PLAN
C3	-	SITE PLAN
C4	-	SITE GRADING PLAN
C5	-	SITE UTILITY PLAN
DT1	-	UNDERGROUND DETENTION AND DETAILS
DT2	-	STANDARD SITE DETAILS
DT3	-	STANDARD SITE DETAILS
EC1	-	EROSION CONTROL PLAN



ALL IMPROVEMENTS TO CONFORM TO HARRISVILLE CITY STANDARDS AND SPECIFICATIONS

CULINARY WATER IMPROVEMENTS TO CONFORM TO THE BONA VISTA WATER STANDARDS AND SPECIFICATIONS

SECONDARY WATER IMPROVEMENTS TO CONFORM TO THE PINEVIEW WATER STANDARDS AND SPECIFICATIONS



HARRISVILLE COMMERCIAL
CONSTRUCTION DOCUMENTS

C1

SKEEN, DAVID W &
WF LINDA G SKEEN
170660102
ZONED CP-2

WAYMENT, CINDA D.
170660076
ZONED A-1

LOT 1
2.19 ACRES
95,405 SQ. FT.

2550 NORTH ST.

750 WEST ST.

PRELIMINARY
NOT FOR CONSTRUCTION

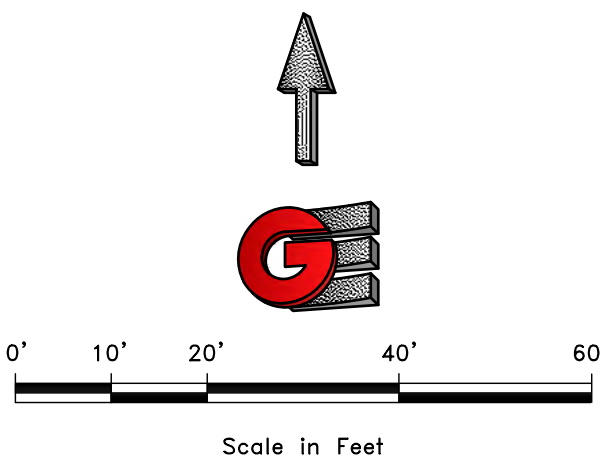
GENERAL DEMOLITION NOTES:

- The location of existing utilities shown are approximate. The contractor shall field verify the location and elevation of all utilities shown or not shown on the plans. Notify the civil engineer of any conflicts prior to any connections being made.
- The existing asphalt paving to be removed adjacent to existing asphalt paving to remain is to be sawcut prior to removal.
- All utilities to remain shall be protected in place to prevent damage or debris from entering boxes or manholes.
- Adjust rim to grad on existing storm drain or sanitary sewer structures that will remain in place.
- Service laterals in 750 West St. to be removed at the time of home demolition. The contractor shall coordinate with Harrisville City, Pleasant View City and appropriate utility companies to ensure proper termination of all existing laterals.

DEMOLITION PLAN KEY NOTES

- EXISTING BUILDING TO BE DEMOLISHED
- EXISTING CONCRETE TO BE REMOVED
- EXISTING FIRE HYDRANT TO BE RELOCATED (REFER TO SHEET C5)
- EXISTING FENCE TO BE PROTECTED IN PLACE
- EXISTING FENCE TO BE REMOVED
- EXISTING GAS METER TO BE REMOVED
- EXISTING DRAINAGE DITCH TO BE FILLED IN AND ABANDONED
- EXISTING ASPHALT TO BE SAWCUT
- EXISTING ASPHALT PAVING TO BE REMOVED
- EXISTING POWER POLES AND POWER BOXES TO BE RELOCATED
- EXISTING COMMUNICATION BOXES TO BE RELOCATED
- EXISTING PIPE CULVERT TO BE PROTECTED IN PLACE
- EXISTING PIPE CULVERT TO BE REMOVED

LEGEND	
	PROPERTY BOUNDARY
	LOT LINE
	PUBLIC UTILITY EASEMENT (P.U.E.)
	BUILDING SETBACK LINE
	EXISTING GROUND 1' CONTOUR
	EXISTING GROUND 5' CONTOUR
	FINISH GRADE 1' CONTOUR
	FINISH GRADE 5' CONTOUR
	EXISTING ASPHALT SURFACE
	NEW ASPHALT SURFACE
	EXISTING CULINARY WATER
	NEW WATER LATERAL AND METER
	EXISTING STORM DRAIN
	NEW STORM DRAIN LINE
	EXISTING SANITARY SEWER
	NEW SANITARY SEWER LATERAL
	EXISTING OVERHEAD POWER LINE
	EXISTING SECONDARY WATERLINE
	NEW SECONDARY WATER SERVICE LATERAL



GARDNER
ENGINEERING

CIVIL & LAND PLANNING
MUNICIPAL & LAND SURVEYING
1580 W 2100S, WEST HAVEN, UT 84401
P 801.476.0202 F 801.476.0066



C2

DEMOLITION PLAN

HARRISVILLE COMMERCIAL

2550 NORTH 750 WEST

HARRISVILLE CITY, WEBER COUNTY, UTAH

REVISIONS

DATE DESCRIPTION

SCALE: 1" = 20'

DATE: 4-15-2025

DESIGN: BSD

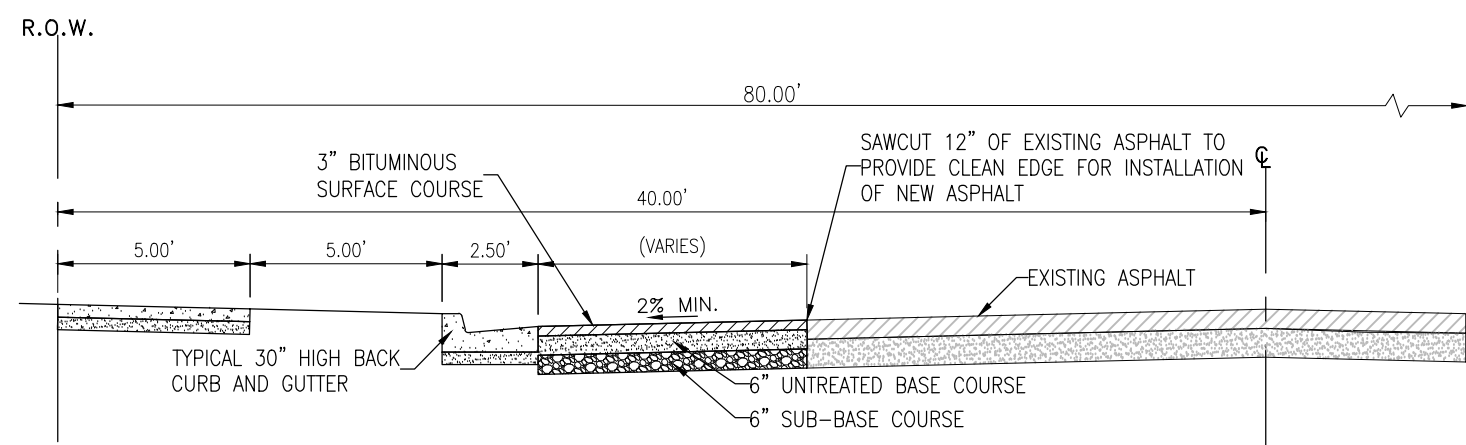
DRAWN: BSD

CHECKED: TN

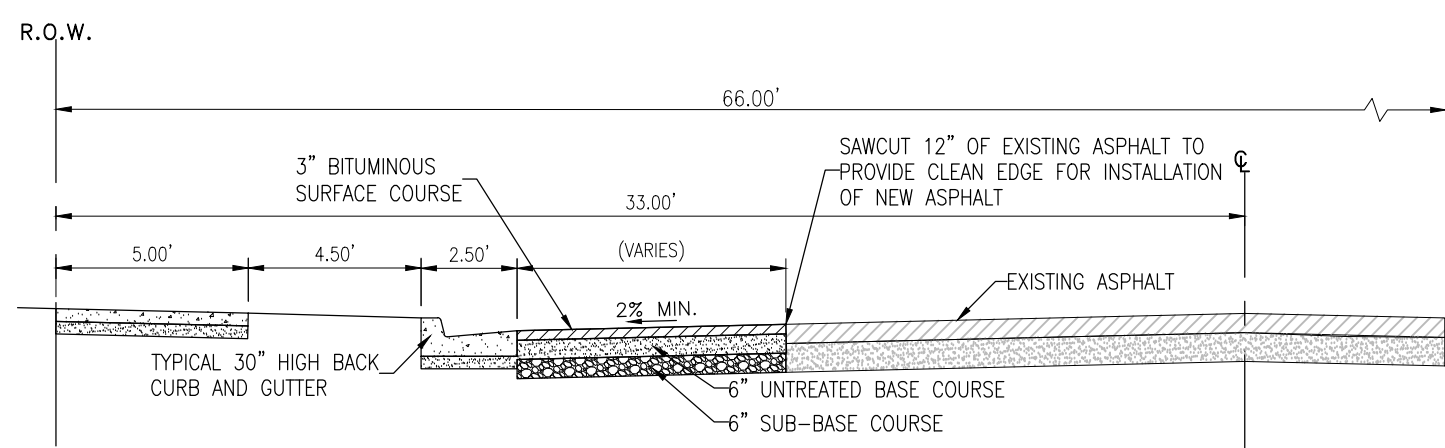
DWG: 255025 - 1580 W 2100S - HARRISVILLE COMMERCIAL - C2

SKEEN, DAVID W &
WF LINDA G SKEEN
170660102
ZONED CP-2

WAYMENT, CINDA D.
170660076
ZONED A-1



TYPICAL 2550 NORTH ST. SECTION
NOT TO SCALE



TYPICAL 750 WEST ST. SECTION
NOT TO SCALE

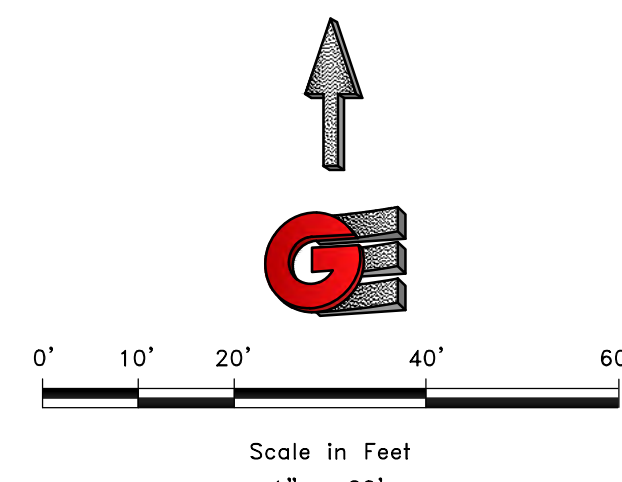
SITE PLAN KEY NOTES

- 30" STANDARD CURB AND GUTTER. (SEE DETAIL 1 ON SHEET DT2)
- 24" STANDARD CURB AND GUTTER. (SEE DETAIL 2 ON SHEET DT2)
- 5' WIDE CONCRETE SIDEWALK AS PER HARRISVILLE CITY STANDARDS
- NEW ASPHALT PAVING. (SEE DETAIL 3 ON SHEET DT2)
- THICKENED EDGE SIDEWALK. (SEE DETAIL 8 ON SHEET DT2)
- NEW DRIVE APPROACH AS PER CITY STANDARDS. (SEE DETAIL 1 ON SHEET DT3)
- TYPICAL HARRISVILLE CITY ADA RAMP WITH TRUNCATED DOME PATTERN AS PER ADA STANDARDS AND GUIDELINES (SEE DETAIL 12 ON SHEET DT2)
- NEW ADA RAMP FROM PARKING LOT. (SEE DETAIL 4 ON SHEET DT2)
- TYPICAL "NO PARKING" STRIPING (SEE DETAIL 6 ON SHEET DT2)
- TYPICAL PARKING STRIPING TO BE 4" WHITE
- TYPICAL ADA PARKING SYMBOL (SEE DETAIL 7 ON SHEET DT2)
- TYPICAL DUMPSTER ENCLOSURE (SEE DETAIL 11 ON SHEET DT2)
- ADA PARKING SIGN (SEE DETAIL 10 ON SHEET DT2)
- SAWCUT, REMOVE AND REPLACE EXISTING ASPHALT PATCH AS PER HARRISVILLE CITY STANDARDS

STANDARD SITE TABLE

BUILDING	28,000.00 SQ. FT.	29.35%
ASPHALT PAVING	56,860.93 SQ. FT.	59.62%
LANDSCAPE	10,523.94 SQ. FT.	11.03%
TOTAL SITE AREA	95,404.87 SQ. FT.	100%
PARKING STALLS PROVIDED = 86 (INCLUDES 4 HANDICAP STALLS)		

LEGEND	
	PROPERTY BOUNDARY
	LOT LINE
	PUBLIC UTILITY EASEMENT (P.U.E.)
	BUILDING SETBACK LINE
	EXISTING GROUND 1' CONTOUR
	EXISTING GROUND 5' CONTOUR
	FINISH GRADE 1' CONTOUR
	FINISH GRADE 5' CONTOUR
	EXISTING ASPHALT SURFACE
	NEW ASPHALT SURFACE
	EXISTING CULINARY WATER
	NEW WATER LATERAL AND METER
	EXISTING STORM DRAIN
	NEW STORM DRAIN LINE
	EXISTING SANITARY SEWER
	NEW SANITARY SEWER LATERAL
	EXISTING OVERHEAD POWER LINE
	EXISTING SECONDARY WATERLINE
	NEW SECONDARY WATER SERVICE LATERAL



PRELIMINARY
NOT FOR CONSTRUCTION

GARDNER
ENGINEERING

CIVIL & LAND PLANNING
MUNICIPAL & LAND SURVEYING

1580 W 2100S, WEST HAVEN, UT 84401
P 801.476.0202 F 801.476.0066



C3

SITE PLAN

HARRISVILLE COMMERCIAL

2550 NORTH 750 WEST

HARRISVILLE CITY, WEBER COUNTY, UTAH

REVISIONS

DATE	DESCRIPTION
------	-------------

SCALE 1" = 20'

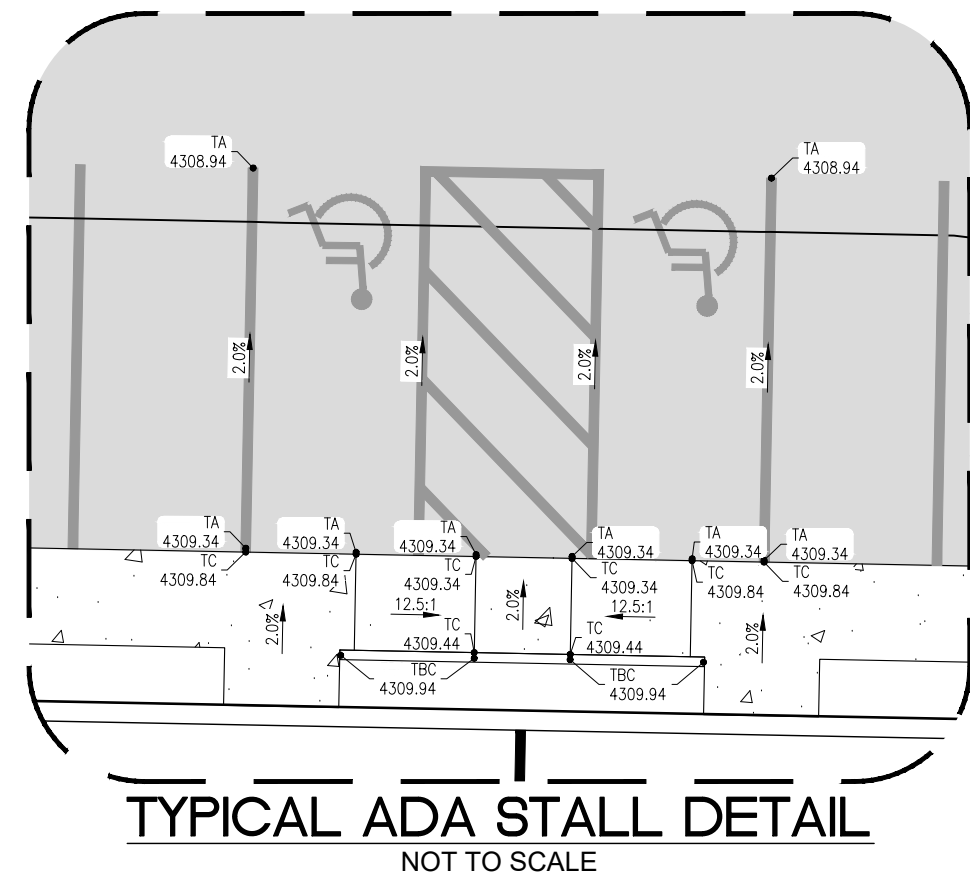
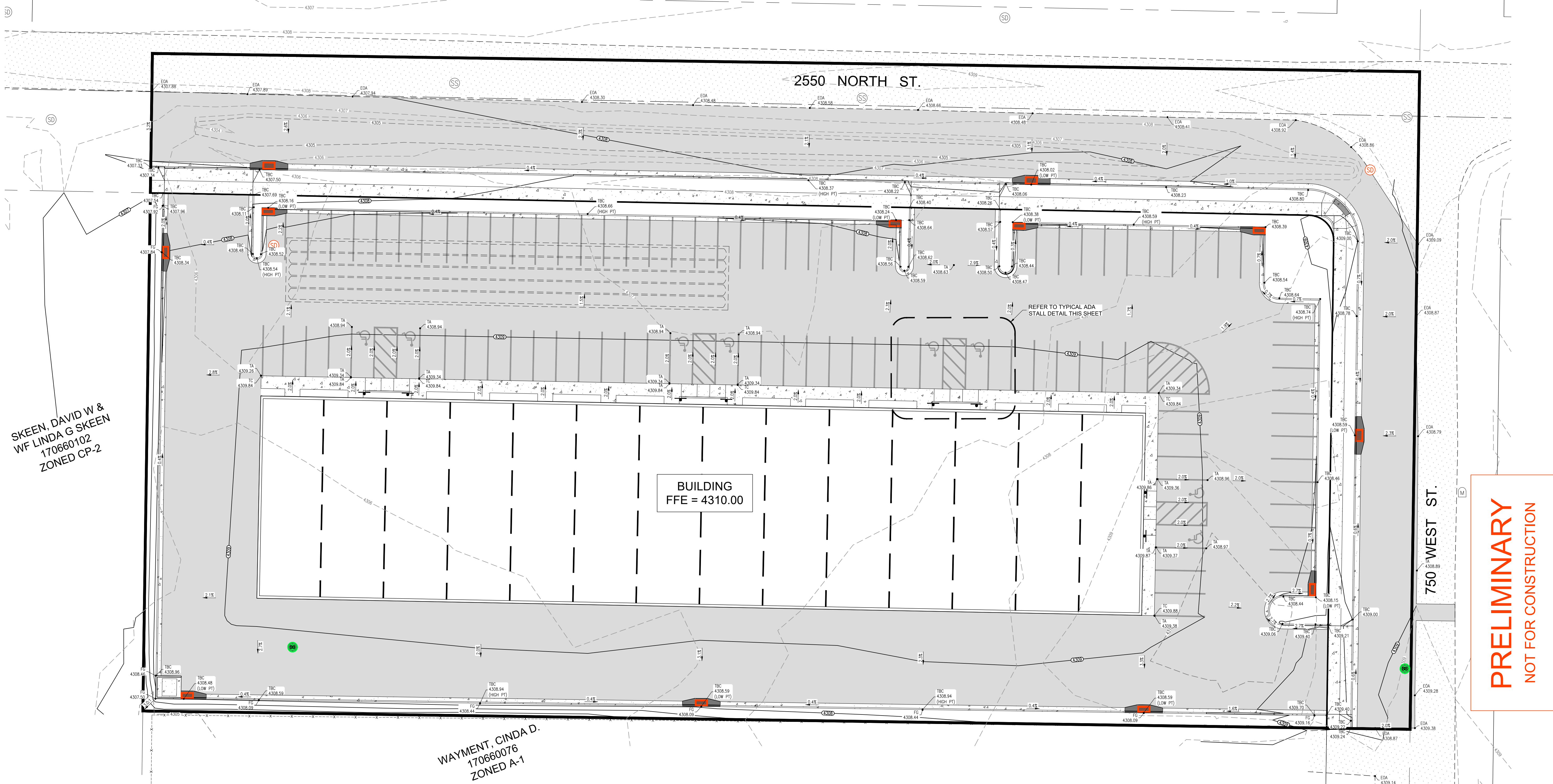
DATE 4-15-2025

DESIGN BSD

DRAWN BSD

CHECKED TN

DWG: 170660102 - 170660102 - HARRISVILLE COMMERCIAL - 05/20/25



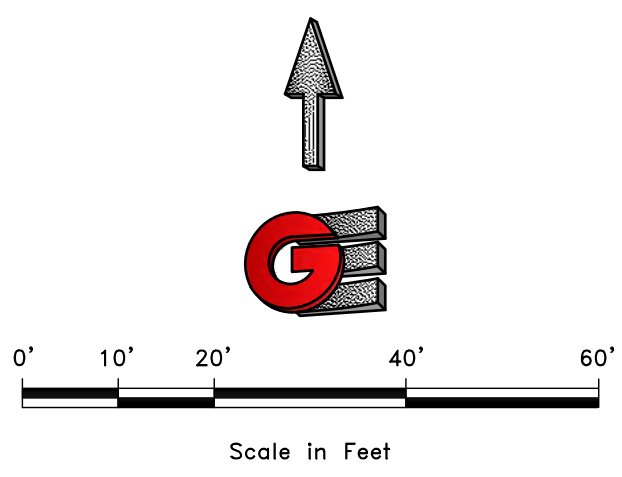
ABBREVIATIONS:

- EG - EXISTING GRAUND
- EOA - EDGE OF ASPHALT
- FG - FINISH GRADE
- FL - FLOWLINE
- TA - TOP OF ASPHALT
- TBC - TOP BACK OF CURB
- TC - TOP OF CONCRETE

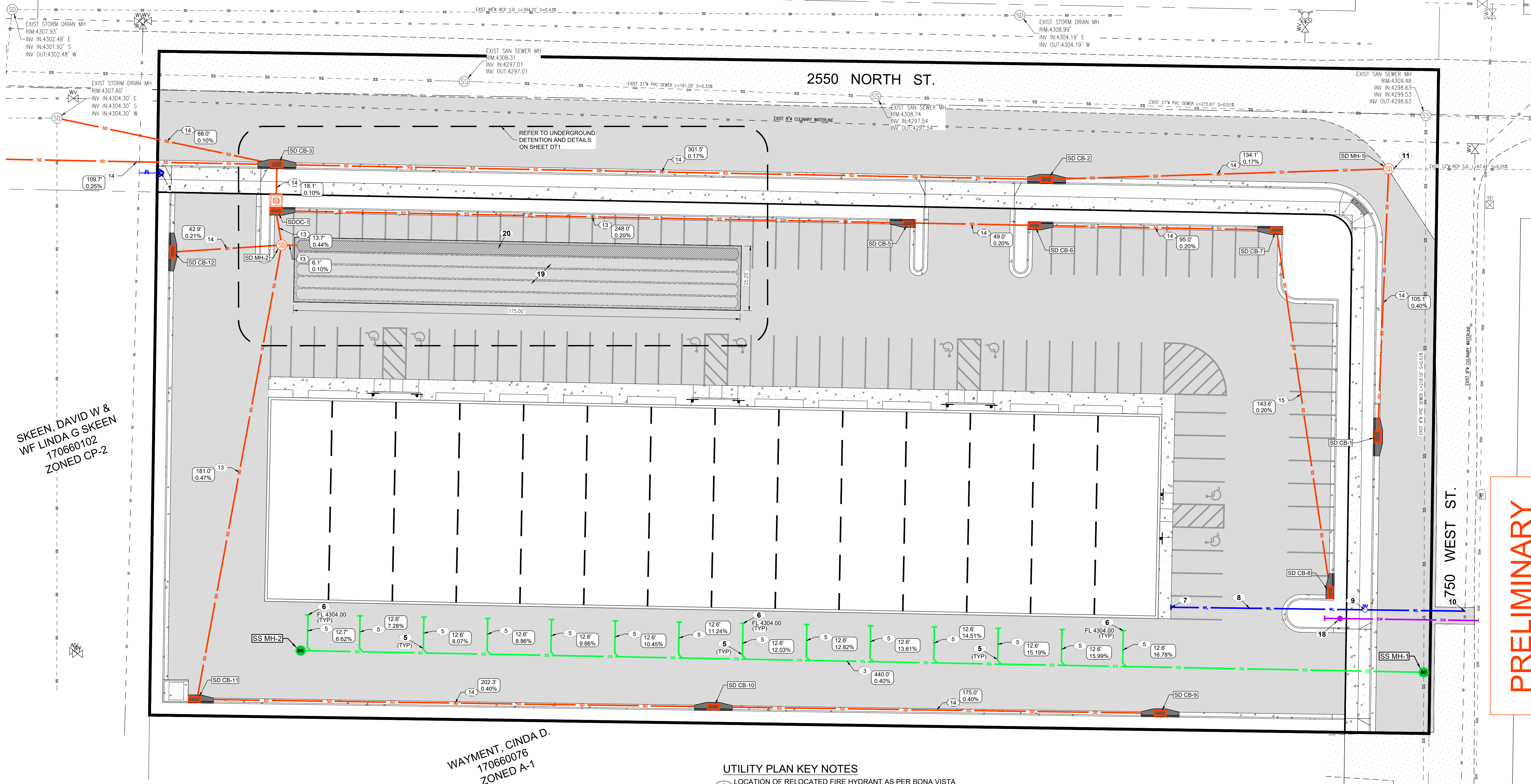
GENERAL GRADING NOTES:

- All work shall be in accordance with Harrisville City Public Works Standard.
- Cut slopes shall be no steeper than 2 horizontal to 1 vertical.
- Fill slopes shall be no steeper than 2 horizontal to 1 vertical.
- Fills shall be compacted per the recommendations of the geotechnical report prepared by CMT Technical Services, for the project and shall be certified by the geotechnical engineer.
- Areas to receive fill shall be properly prepared and approved by the Harrisville City inspector and geotechnical engineer prior to placing fill.
- Fills shall be benched into competent material as per specifications and geotechnical report.
- All trench backfill shall be tested and certified by the site geotechnical engineer per the grading code.
- A geotechnical engineer shall perform periodic inspections and submit a complete report and map upon completion of the rough grading.
- The final compaction report and certification from the geotechnical engineer shall contain the type of field testing performed. Each test shall be identified with the method of obtaining the in-place density, whether sand cone or drive ring and shall be so noted for each test. Sufficient maximum density determinations shall be performed to verify the accuracy of the maximum density curves used by the field technician.
- Dust shall be controlled by watering.
- The location and protection of all utilities is the responsibility of the permittee.
- Approved protective measures and temporary drainage provisions must be used to protect adjoining properties during the grading project.
- All public roadways must be cleared daily of all dirt, mud and debris deposited on them as a result of the grading operation. Cleaning is to be done to the satisfaction of Harrisville City Inspector.
- The site shall be cleared and grubbed of all vegetation and deleterious matter prior to grading.
- The contractor shall provide shoring in accordance with OSHA requirements for trench walls.
- Aggregate base shall be compacted per the geotechnical report prepared for the project.
- Elevations shown on this plan are finish grades. Rough grades are the subgrades of the improvements shown hereon.
- The recommendations in the Geotechnical Engineering Reported by CMT Technical Services.
- As part of the construction documents, owner has provided contractor with a topographic survey performed by manual or aerial means. Such survey was prepared for project design purposes and is provided to the contractor as a courtesy. It is expressly understood that such survey may not accurately reflect existing topographic conditions.
- Accessible route to be a max. slope of 5% and a max. cross slope of 2% 2009 ANSI 117.403.3.
- Maximum elevation difference between the asphalt surface of the parking surface and the bottom of the concrete curb ramps or sidewalk height shall not exceed 1/4" vertical or 1/2" when beveled. 2009 ANSI 117.303.502.5.

- LEGEND
- PROPERTY BOUNDARY
 - LOT LINE
 - PUBLIC UTILITY EASEMENT (P.U.E.)
 - BUILDING SETBACK LINE
 - EXISTING GROUND 1' CONTOUR
 - EXISTING GROUND 5' CONTOUR
 - FINISH GRADE 1' CONTOUR
 - FINISH GRADE 5' CONTOUR
 - EXISTING ASPHALT SURFACE
 - NEW ASPHALT SURFACE
 - EXISTING CULINARY WATER
 - NEW WATER LATERAL AND METER
 - EXISTING STORM DRAIN
 - NEW STORM DRAIN LINE
 - EXISTING SANITARY SEWER
 - NEW SANITARY SEWER LATERAL
 - EXISTING OVERHEAD POWER LINE
 - EXISTING SECONDARY WATERLINE
 - NEW SECONDARY WATER SERVICE LATERAL



SCALE: 1" = 20'	DATE: 4-15-2023	DESIGN: BSD	DRAWN: BSD	CHECKED: TN	DWG: 170660102 - WILD MOUNTAIN/TAH-HARRISVILLE COMMERCIAL - C4
REVISIONS					
DATE	DESCRIPTION				
SITE GRADING PLAN					
HARRISVILLE COMMERCIAL					
2550 NORTH 750 WEST					
HARRISVILLE CITY, WEBER COUNTY, UTAH					
GARDNER ENGINEERING					
CIVIL & LAND PLANNING					
MUNICIPAL & LAND SURVEYING					
1580 W 2100S, WEST HAVEN, UT 84401					
P 801.476.0202 F 801.476.0066					
C4					



SKEEN, DAVID W &
WF LINDA G SKEEN
170660102
ZONED CP-2

WAYMENT, CINDA D.
170660076
ZONED A-1

UTILITY PLAN KEY NOTES

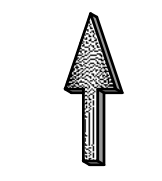
- 1 LOCATION OF RELOCATED FIRE HYDRANT AS PER BONA VISTA WATER STANDARDS
- 2 OIL WATER SEPARATOR
- 3 8" SDR 35 SANITARY SEWER LINE.
- 4 4" SDR 35 SANITARY SEWER LINE.
- 5 STANDARD WYE CONNECTION TO SANITARY SEWER.
- 6 STUB SEWER 5' OUTSIDE BUILDING (REFER TO PLUMBING PLANS).
- 7 STUB WATER LATERAL 5' OUTSIDE OF BUILDING (REFER TO PLUMBING PLANS)
- 8 INSTALL 2"Ø POLY PIPE CULINARY WATER SERVICE LATERAL AS PER BONA VISTA WATER STANDARDS
- 9 2"Ø WATER METER PER BONA VISTA WATER STANDARDS. (SEE DETAIL 2 ON SHEET DT3)
- 10 CONNECT TO EXISTING 8"Ø WATER MAIN AS PER BONA VISTA WATER STANDARDS
- 11 CONNECT EXISTING 12"Ø RCP TO NEW 5"Ø STORM DRAIN MANHOLE
- 12 INSTALL 15"Ø RCP STORM DRAIN LINE
- 13 INSTALL 12"Ø STORM DRAIN LINE (SEE GENERAL UTILITY NOTES FOR PIPE MATERIAL)
- 14 INSTALL 10"Ø STORM DRAIN LINE (SEE GENERAL UTILITY NOTES FOR PIPE MATERIAL)
- 15 INSTALL 8"Ø STORM DRAIN LINE (SEE GENERAL UTILITY NOTES FOR PIPE MATERIAL)
- 16 4"Ø ROOF DRAIN. (SEE GENERAL UTILITY NOTES FOR PIPE MATERIAL)
- 17 ROOF DRAIN STUB (5' OUTSIDE BUILDING) (SEE MECH. PLANS FOR CONTINUATION)
- 18 INSTALL 1"Ø POLYETHYLENE SDR-9 SECONDARY WATER LATERAL AS PER HARRISVILLE CITY STANDARDS (SEE DETAIL 7 ON SHEET DT3)
- 19 INSTALL STORM TECH DC-780 CHAMBERS OR APPROVED EQUAL (SEE DETAIL 1 ON SHEET DT1)
- 20 STORM TECH DC-780 ISOLATOR ROW PLUS OR APPROVED EQUAL (SEE DETAIL 1 ON SHEET DT1)

General Utility Notes:

1. Coordinate all utility connections to building with plumbing plans and building contractor.
2. Verify depth and location of all existing utilities prior to constructing any new utility lines. Notify Civil Engineer of any discrepancies or conflicts prior to any connections being made.
3. All catch basin and inlet box grates are to have bicycle safe grates.
4. Field verify all existing and/or proposed Roof Drain/Roof Drain down spout connections to storm drain system with Civil, Mechanical, and Architectural plans Notify Engineer of any discrepancies.
5. Refer to the site electrical plan for details and locations of electrical lines, transformers and light poles.
6. Gas lines, telephone lines, and cable TV lines are not a part of these plans.
7. All culinary water facilities shall be installed per Bona Vista Water Standards and specifications. It will be the contractor's responsibility to install all items required.
8. Water lines, valves, fire hydrants, fittings etc. are to be constructed as shown. Contractor is responsible to construct any vertical adjustments necessary to clear sewer, storm drain or other utilities as necessary including valve boxes and hydrant spools to proper grade.
9. Where waterline must cross sanitary sewer, waterline shall be installed at least 18" above sewer. Where waterline must cross storm drain, adjust water line to maintain 18" separation.
10. Utility Piping Materials: All piping to be installed per manufacturers recommendations.
Culinary Service Laterals
a. 2" diameter pipe - copper tube ASTM B, Type K, Soft Temper or poly pipe.
Sanitary Sewer Line
a. All Sewer piping to be Polyvinyl Chloride (PVC) sewer pipe, ASTM D 3034, Type FSN, SDR 35.
Storm Drain and Roof Drain Lines
a. 10" pipes or smaller - Polyvinyl Chloride (PVC) SDR 35
b. 12" pipes - Non-reinforced Concrete Pipe, ASTM C14, Class III
c. 15" pipes or larger - Reinforced Concrete Pipe, ASTM C76, Class III

LEGEND

	PROPERTY BOUNDARY
	LOT LINE
	PUBLIC UTILITY EASEMENT (P.U.E.)
	BUILDING SETBACK LINE
	EXISTING GROUND 1' CONTOUR
	FINISH GRADE 1' CONTOUR
	EXISTING ASPHALT SURFACE
	NEW ASPHALT SURFACE
	EXISTING CULINARY WATER
	NEW WATER LATERAL AND METER
	EXISTING STORM DRAIN
	NEW STORM DRAIN LINE
	EXISTING SANITARY SEWER
	NEW SANITARY SEWER LATERAL
	EXISTING OVERHEAD POWER LINE
	EXISTING SECONDARY WATERLINE
	NEW SECONDARY WATER SERVICE LATERAL



0' 10' 20' 40' 60'
Scale in Feet
1" = 20'

**GARDNER
ENGINEERING**

**CIVIL & LAND PLANNING
MUNICIPAL & LAND SURVEYING**
1580 W 2100S, WEST HAVEN, UT 84401
P 801.476.0202 F 801.476.0066



C5

SITE UTILITY PLAN

HARRISVILLE COMMERCIAL

2550 NORTH 750 WEST

HARRISVILLE CITY, WEBER COUNTY, UTAH

REVISIONS

DATE DESCRIPTION

SCALE: 1" = 20'

DATE: 4-15-2025

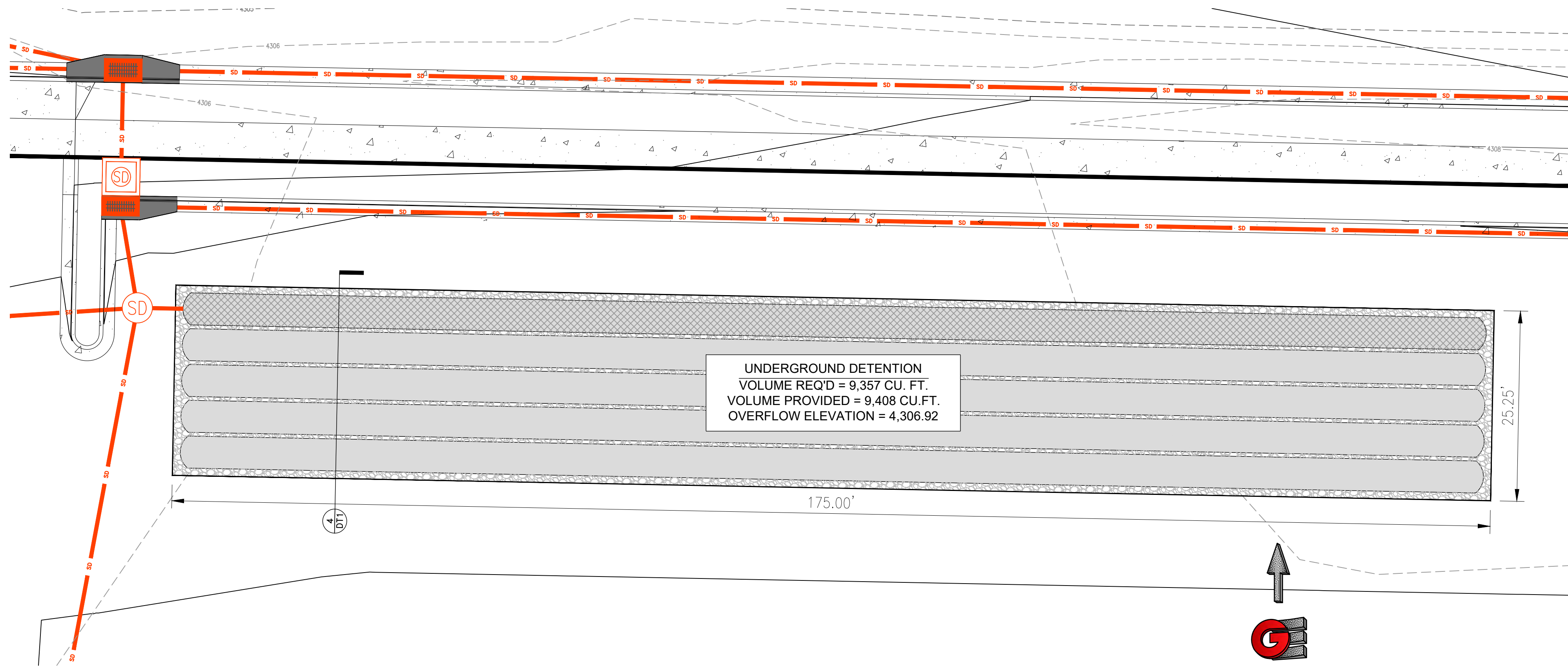
DESIGN: BSD

DRAWN: BSD

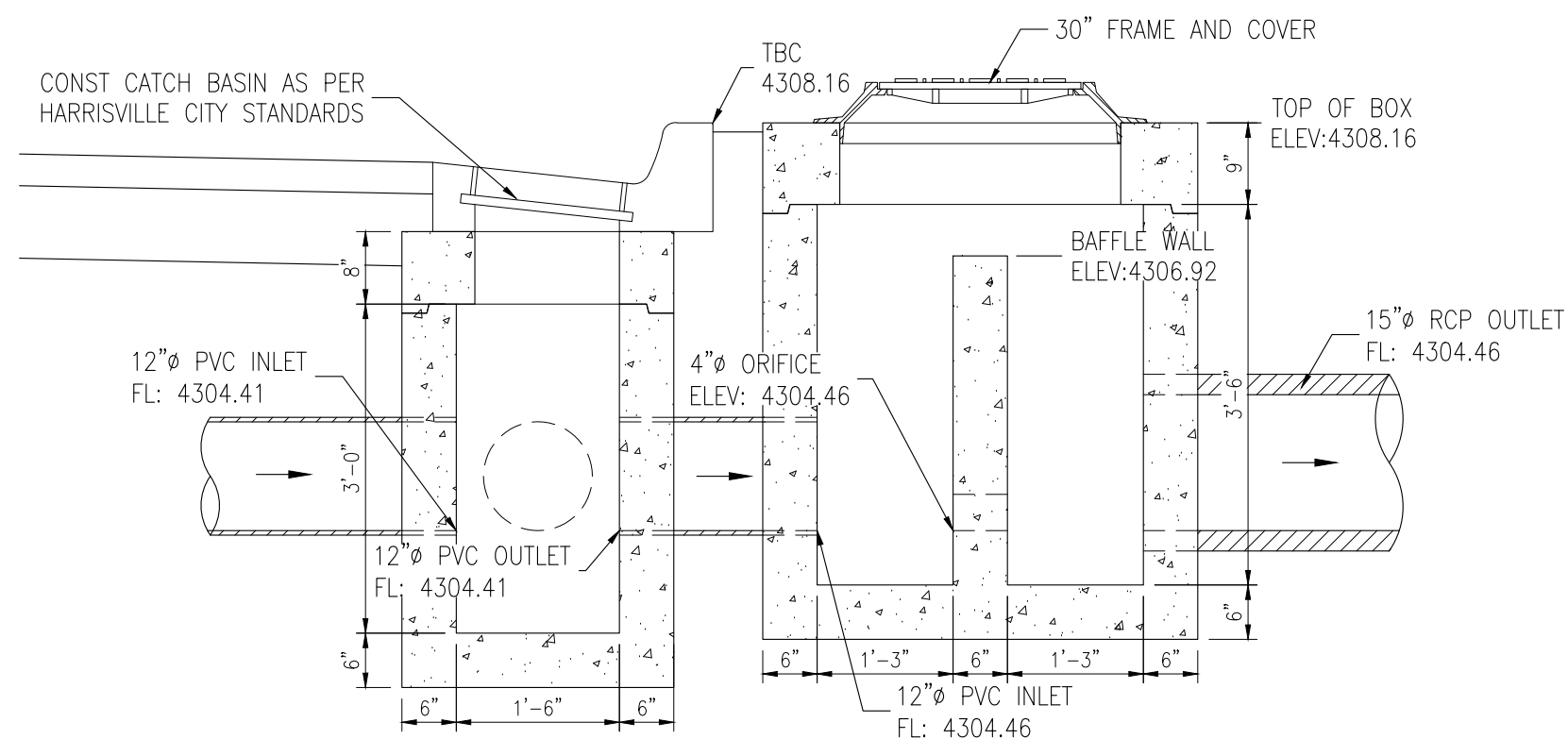
CHECKED: TN

DWG: 05/2025 - 1580 W 2100S - HARRISVILLE COMMERCIAL - HARRISVILLE COMMERCIAL - C50606

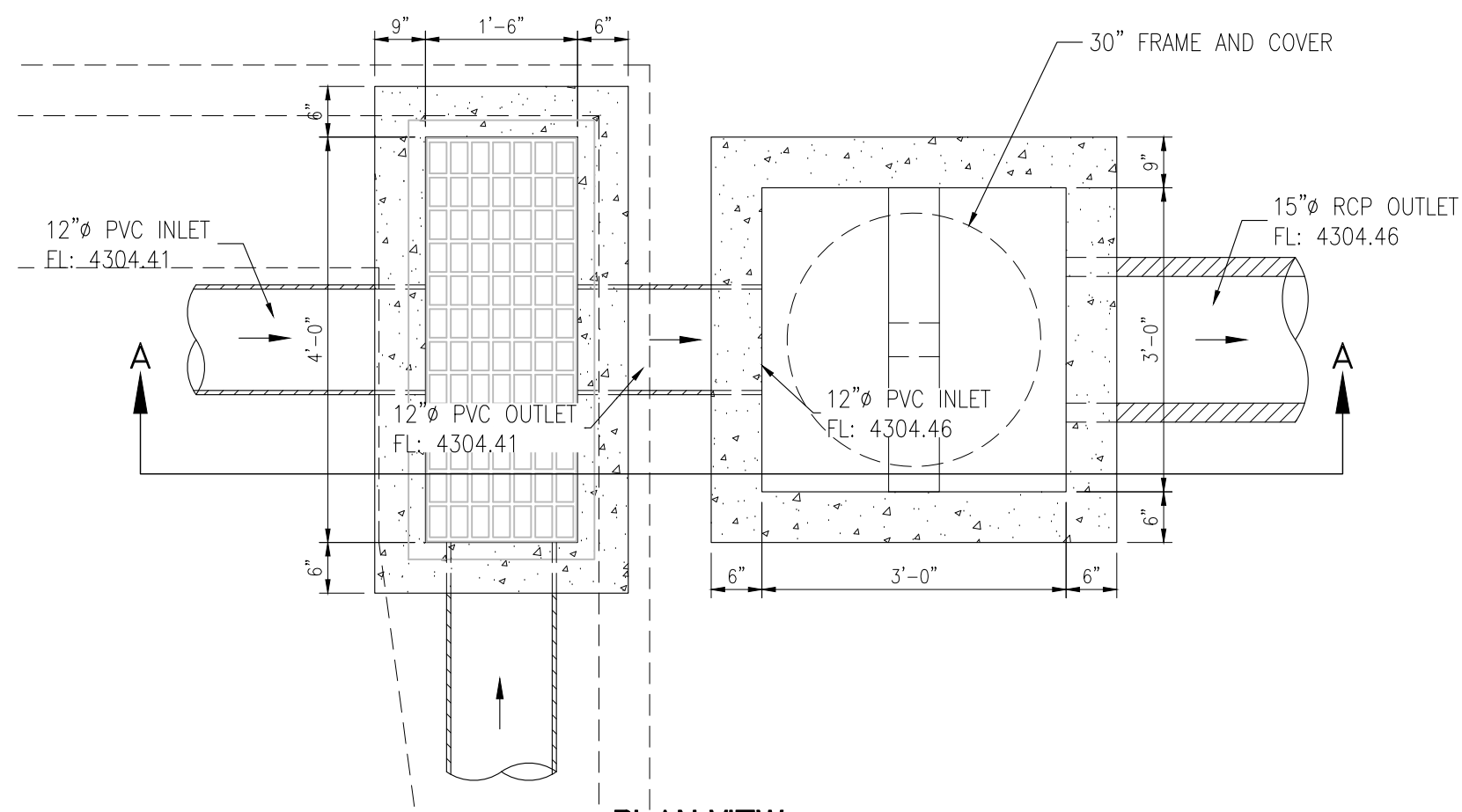
9/3/2025: Video Manager\\04301-Harrisville Commercial\\04301-Harrisville Commercial\\CD\\Plan_4/16/2025 3:37:33 PM



1 UNDERGROUND DETENTION ENLARGED PLAN
Scale: (NOT TO SCALE)

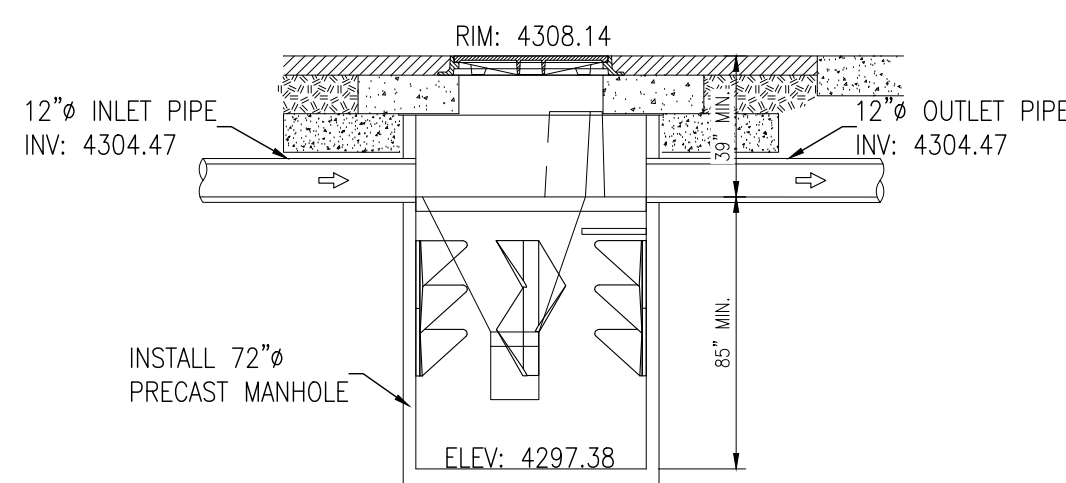


SECTION - A



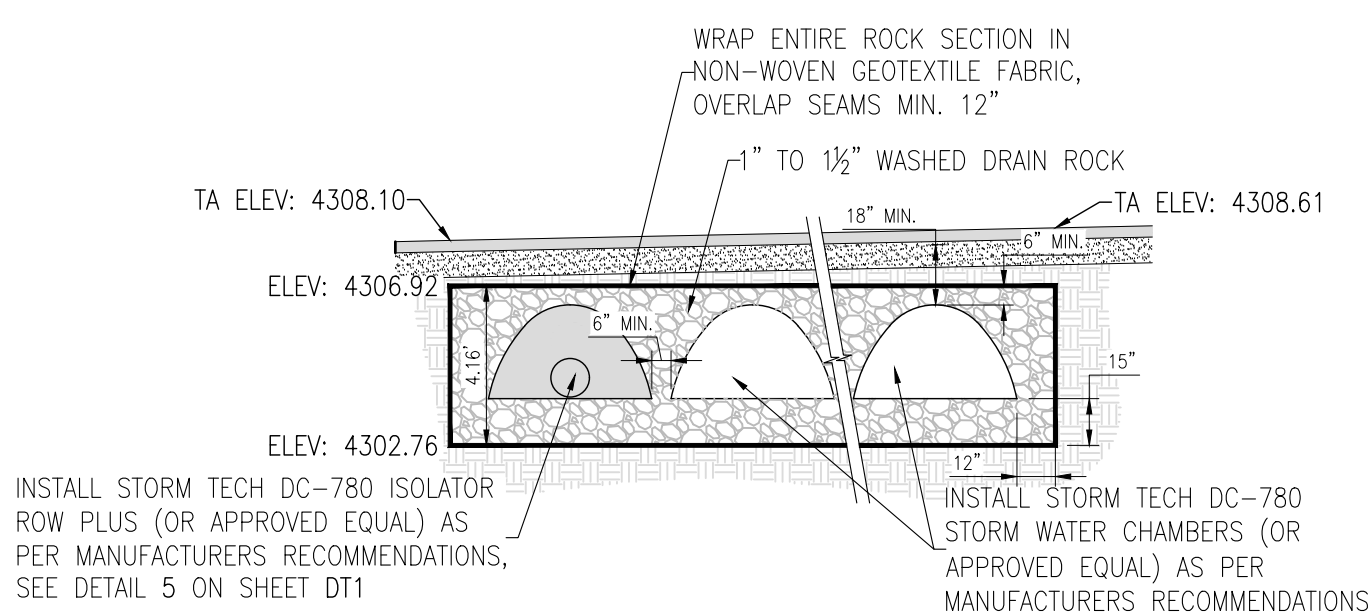
PLAN VIEW

2 OUTLET CONTROL STRUCTURE DETAIL
Scale: (NOT TO SCALE)

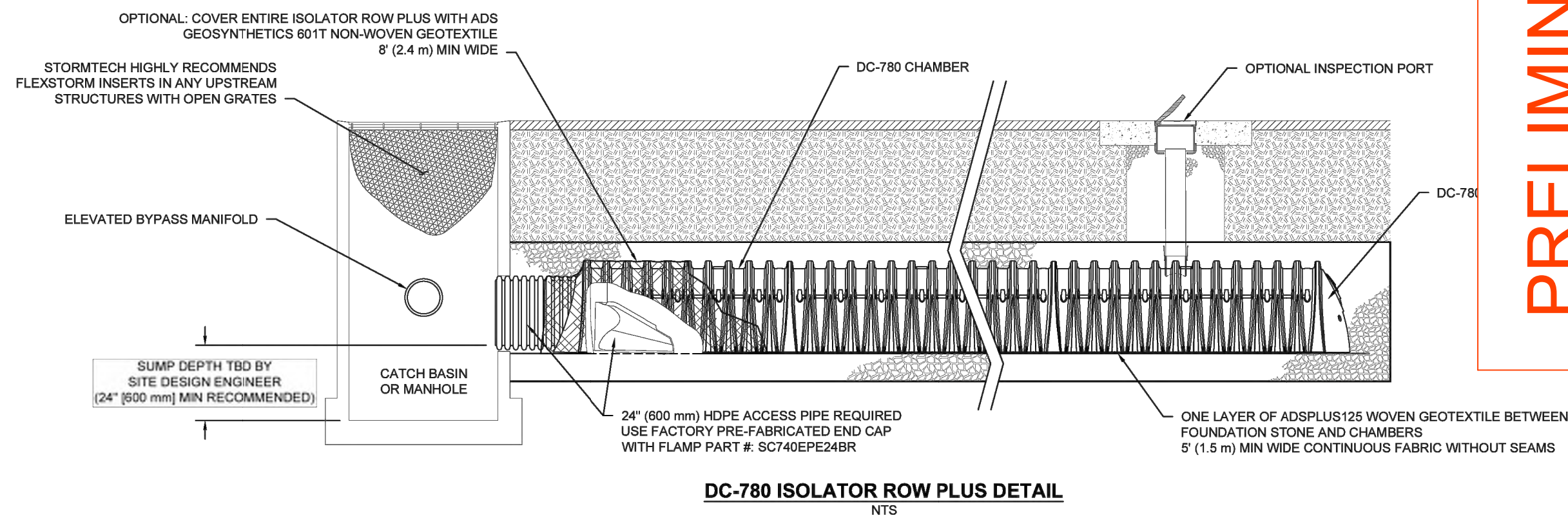


- NOTES:
1. INSTALL BARRACUDA MAX S6 (OR APPROVED EQUAL) AS PER MANUFACTURER'S RECOMMENDATION

3 STORM WATER TREATMENT MANHOLE DETAIL
Scale: (NOT TO SCALE)



4 STORM TECH DC-780 CHAMBERS DETAIL
Scale: (NOT TO SCALE)



INSPECTION & MAINTENANCE

- STEP 1) INSPECT ISOLATOR ROW PLUS FOR SEDIMENT
- A.1. INSPECTION PORTS (IF PRESENT)
 - A.2. REMOVE/OPEN LID ON NYLOPLAST INLINE DRAIN
 - A.3. REMOVE AND CLEAN FLEXSTORM FILTER IF INSTALLED
 - A.4. USING A FLASHLIGHT AND STADIA ROD, MEASURE DEPTH OF SEDIMENT AND RECORD ON MAINTENANCE LOG
 - A.5. LOWER A CAMERA INTO ISOLATOR ROW PLUS FOR VISUAL INSPECTION OF SEDIMENT LEVELS (OPTIONAL)
 - A.6. IF SEDIMENT IS AT OR ABOVE, 3" (80 mm) PROCEED TO STEP 2. IF NOT, PROCEED TO STEP 3.
- B. ALL ISOLATOR PLUS ROWS
- B.1. REMOVE COVER FROM STRUCTURE AT UPSTREAM END OF ISOLATOR ROW PLUS
 - B.2. USING A FLASHLIGHT, INSPECT DOWN THE ISOLATOR ROW PLUS THROUGH OUTLET PIPE
 - i) MIRRORS ON POLES OR CAMERAS MAY BE USED TO AVOID A CONFINED SPACE ENTRY
 - ii) FOLLOW OSHA REGULATIONS FOR CONFINED SPACE ENTRY IF ENTERING MANHOLE
 - B.3. IF SEDIMENT IS AT OR ABOVE, 3" (80 mm) PROCEED TO STEP 2. IF NOT, PROCEED TO STEP 3.
- STEP 2) CLEAN OUT ISOLATOR ROW PLUS USING THE JETVAC PROCESS
- A. A FIXED CULVERT CLEANING NOZZLE WITH REAR FACING SPREAD OF 45° (1.1 m) OR MORE IS PREFERRED
 - B. APPLY MULTIPLE PASSES OF JETVAC UNTIL BACKFLUSH WATER IS CLEAN
 - C. VACUUM STRUCTURE SUMP AS REQUIRED
- STEP 3) REPLACE ALL COVERS, GRATES, FILTERS, AND LIDS, RECORD OBSERVATIONS AND ACTIONS.
- STEP 4) INSPECT AND CLEAN BASINS AND MANHOLES UPSTREAM OF THE STORMTECH SYSTEM.

NOTES

1. INSPECT EVERY 6 MONTHS DURING THE FIRST YEAR OF OPERATION. ADJUST THE INSPECTION INTERVAL BASED ON PREVIOUS OBSERVATIONS OF SEDIMENT ACCUMULATION AND HIGH WATER ELEVATIONS.
2. CONDUCT JETTING AND VACTORING ANNUALLY OR WHEN INSPECTION SHOWS THAT MAINTENANCE IS NECESSARY.

5 DC-780 ISOLATOR ROW PLUS DETAIL
Scale: (NOT TO SCALE)

Harrisville Commercial Site
Harrisville, Utah

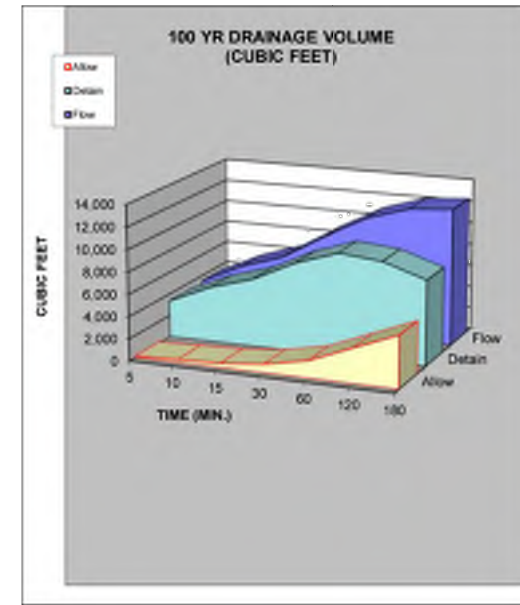
4/16/2025

Areas	Sq. Ft.	Acres	C
Building	28,000	0.6428	0.90
Hard Surface	59,390	1.3634	0.80
Landscape	8,015	0.1840	0.15
Total/Weighted	95,405	2.1902	0.77

Allow Release Rate (cfs/acre): 0.200
Q Allowable (cfs): 0.44

Runoff Vol (cf)	Inch / Hr	Total Vol (cf)	Detain Vol (cf)
MIN	100	100	100
5	0.05	3,407	3,395
10	0.10	5,304	5,041
15	0.15	6,920	6,180
30	0.30	8,857	8,089
60	0.60	10,934	9,387
120	1.20	12,156	9,002
180	1.80	12,462	7,731
360	3.60	13,891	4,400
720	7.20	17,006	0
1440	14.40	19,059	0

NOAA Data



Orifice Calculation

H = 3 Maximum water height (ft)
Q = 0.44 Flowrate out of orifice (cfs)
Cc = 0.62 Coefficient of Contraction
Cv = 0.98 Coefficient of Velocity
Area = 0.052 Orifice Area (ft²)
Tt = 3.14 Orifice Area (ft²)
g = 32.17 Gravitational Constant
d = 3.08 Orifice Diameter (in)

100 YEAR STORM RECOMMENDED MIN. VOLUME DETAINED	CUBIC FEET	CUBIC YARDS
	9,357	347

PRELIMINARY
NOT FOR CONSTRUCTION

UNDERGROUND DETENTION AND DETAILS

HARRISVILLE COMMERCIAL

2550 NORTH 750 WEST

HARRISVILLE CITY, WEBER COUNTY, UTAH

GARDNER
ENGINEERING

CIVIL & LAND PLANNING
MUNICIPAL & LAND SURVEYING

1580 W 2100S, WEST HAVEN, UT 84401
P 801.476.0202 F 801.476.0066



DT1



1 TYPICAL 30" CURB AND GUTTER
Scale: (NOT TO SCALE)

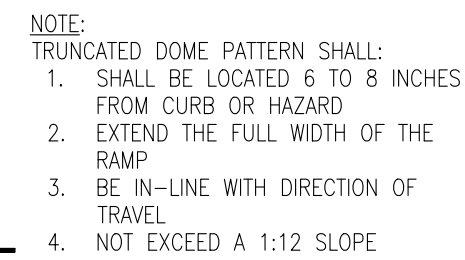


2 TYPICAL 24" CURB AND GUTTER
Scale: (NOT TO SCALE)

1. OPEN FACE GUTTER SHALL BE CONSTRUCTED WHERE DRAINAGE IS DIRECTED AWAY FROM CURB.
2. OPEN FACE CURB & GUTTER LOCATIONS ARE INDICATED BY HATCHING AND NOTES ON THE GRADING PLAN.
3. IT IS THE RESPONSIBILITY OF THE SURVEYOR TO ADJUST TOP OF CURB GRADES AT THE TIME OF CONSTRUCTION STAKING.
4. REFER TO THE TYPICAL DETAILS FOR STANDARD (CLOSED) AND OPEN FACE CURB AND GUTTER FOR DIMENSIONS.
5. TRANSITIONS BETWEEN OPEN FACE AND STANDARD (CLOSED) CURB AND GUTTER ARE TO BE SMOOTH, HAND FORM THESE AREAS IF NECESSARY.



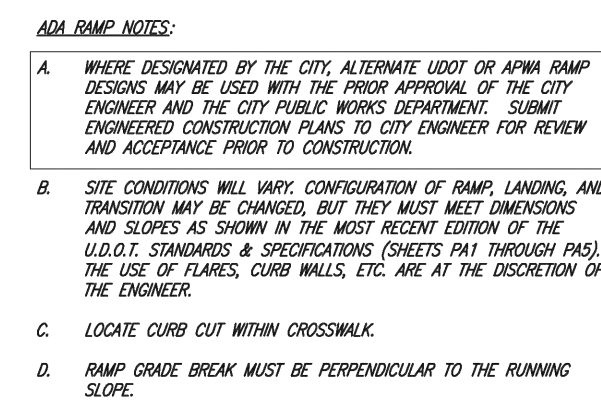
3 TYPICAL ASPHALT PAVING SECTION



5 TYPICAL TRUNCATED DOME DETAIL
Scale: (NOT TO SCALE)



11 CEMENT CONCRETE PAVEMENT DETAIL



* RUNNING SLOPE IS IN THE DIRECTION OF PEDESTRIAN TRAVEL. CROSS SLOPE IS PERPENDICULAR TO PEDESTRIAN TRAVEL.

¹ 5% MAX OR NATURAL SLOPE OF LAND

² NOT TO EXCEED 2% IN ANY DIRECTION

**GARDNER
ENGINEERING**

CIVIL ■ LAND PLANNING
MUNICIPAL ■ LAND SURVEYING

1580 W. 2100S., WEST HAVEN, UT 84401
P 801.476.0202 F 801.476.0066



DT2

REVISIONS		SCALE (AS NOTED)
DATE	DESCRIPTION	
DATE	4-15-2025	
DESIGN	SSD	
DRAWN	SSD	
CHECKED	TN	



-
- HOT MIX ASPHALT (HMA) PER CITY STANDARD SPECIFICATIONS. MATCH EXISTING PAVEMENT THICKNESS OR 4" MIN., WHICHEVER IS GREATER.**
- 12" MIN. FROM EDGE OF EXCAVATION**
- SAW CUT, CLEAN AND TACK COAT ALL EXISTING EDGES (SEE SHEET CS-04 PATCH DETAIL)**
- 12" MIN. UNB.C COMPACTED TO 95% MODIFIED PROCTOR (SEE APWA 32 11 23 AND CITY MODIFICATIONS)**
- SAND, GRAVEL OR EARTH CONTAINING NO LIMBS, ORGANIC MAT. OR ROCKS LARGER THAN 3/4" IN ANY DIRECTION. HAND OR MECHANICAL COMPACTION.**
- TRENCH NOTES:**
- BACKFILL PER APWA 33 05 20 AND CITY MODIFICATIONS.
 - COMPACTION TEST REQUIRED AT SPRING-LINE FOR ALL P.I.C. OR M.D.G.E.
 - PAVEMENT RESTORATION PER APWA 33 05 25 AND CITY MODIFICATIONS.
 - GRAVEL SHOULDER AREAS, SUCH AS ROADS AND SHOULDERS, PARKING AREAS, AND UNPAVED DRIVEWAYS, SHALL BE REPAIRED WITH HOT MIX (HMA) "I" UNTRREATED SAND OR UNB.C. COMPACTED TO 95% MODIFIED PROCTOR.
 - WATER & SEWER LINES, INCLUDING SERVICE LINES, SHALL NOT BE INSTALLED IN THE SAME TRENCH.
- BED P.I.C PIPE IN SAND. ALL OTHER PIPE TO BE BEDDED IN SAND OR GRAVEL IN UNSTABLE GROUND AREAS OR THROUGH ROCK COVERED 6" MIN. DEPTH UNDER PIPE TO BE EXCAVED / BEDDING MATERIAL. BEDDING DEPTH PIPE LARGER THAN 12" DIA. SHALL BE SPECIFIED BY THE CITY ENGINEER**
- PIPE ØL.D. = 18" MIN.**
- WIDTH @ TOP OF PIPE**
- 1'-6" MIN.**
- SHAPE TRENCH BY HAND TO FIT BOTTOM QUADRANT OF PIPE FOR ALL SEWER AND DRAIN LINES**

6 TYPICAL TRENCH SECTION
Scale: (NOT TO SCALE)



PRELIMINARY
NOT FOR CONSTRUCTION

REVISIONS	
DATE	DESCRIPTION

STANDARD SILE DE JAILS
HARRISVILLE COMMERCIAL
2550 NORTH 750 WEST
HARRISVILLE CITY, WEBER COUNTY

**GARDNER
ENGINEERING**

**CIVIL ■ LAND PLANNING
MUNICIPAL ■ LAND SURVEYING**

1580 W 2100S, WEST HAVEN, UT 84401
P 801.476.0202 F 801.476.0066



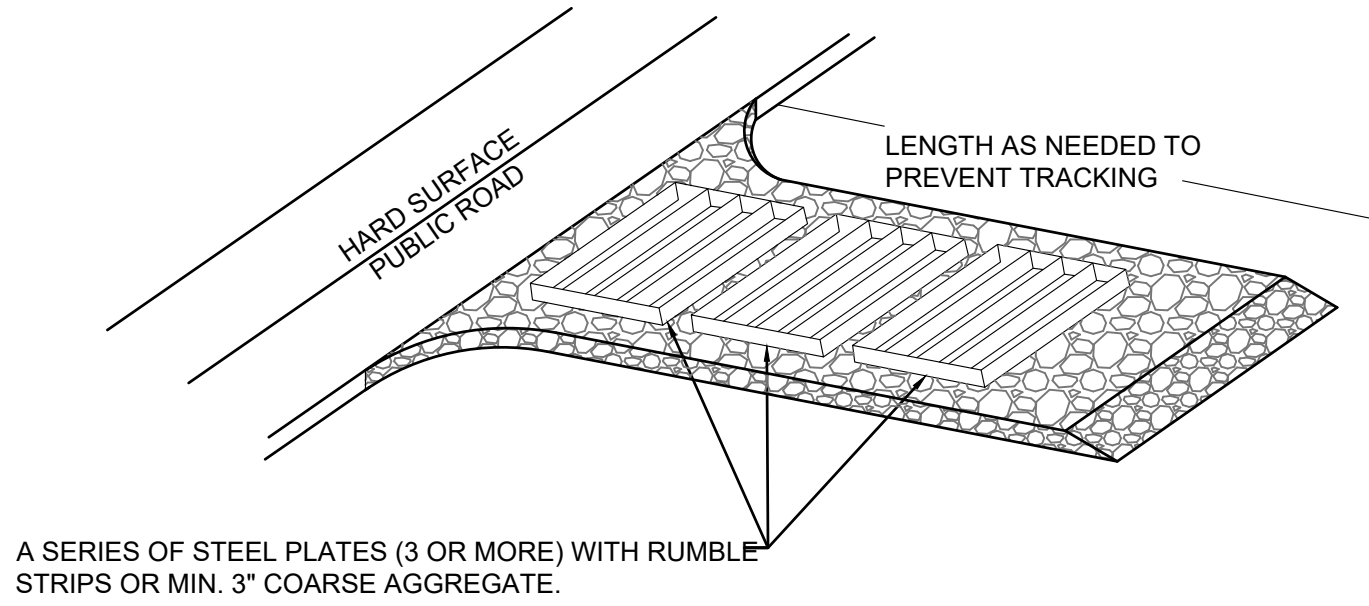
DT3

HARRISVILLE CITY, WEBER COUNTY, UTAH

DWG: R:\2205 - VALEO MANAGEMENT\2401-HARRISVILLE COMMERCIAL\DESIGN\DWG\HARRISVILLE COMMERCIAL - CDS.DWG

EROSION CONTROL NOTES:

1. SANDBAGS WILL BE PLACED AT DISCHARGE LOCATIONS TO CONTAIN AND DIVERT STORM WATER THROUGH THE INLET PROTECTION.
2. AN EARTHEN BERM 6" HIGH WILL BE CONSTRUCTED TO CONTAIN THE STORM WATER AND DIVERT IT TO DISCHARGE AREAS.
3. STORM WATER WILL BE DISCHARGED INTO AN EXISTING DRAINAGE SYSTEM. EXISTING LINES SHALL BE INSPECTED PRIOR TO CERTIFICATE OF OCCUPANCY AND CLEANED IF NECESSARY.
4. THE STORM WATER POLLUTION PREVENTION PLAN SHALL CONFORM TO ALL STATE DIVISION OF ENVIRONMENTAL PROTECTION REGULATIONS.



ENTRANCE STABILIZATION NOTES:

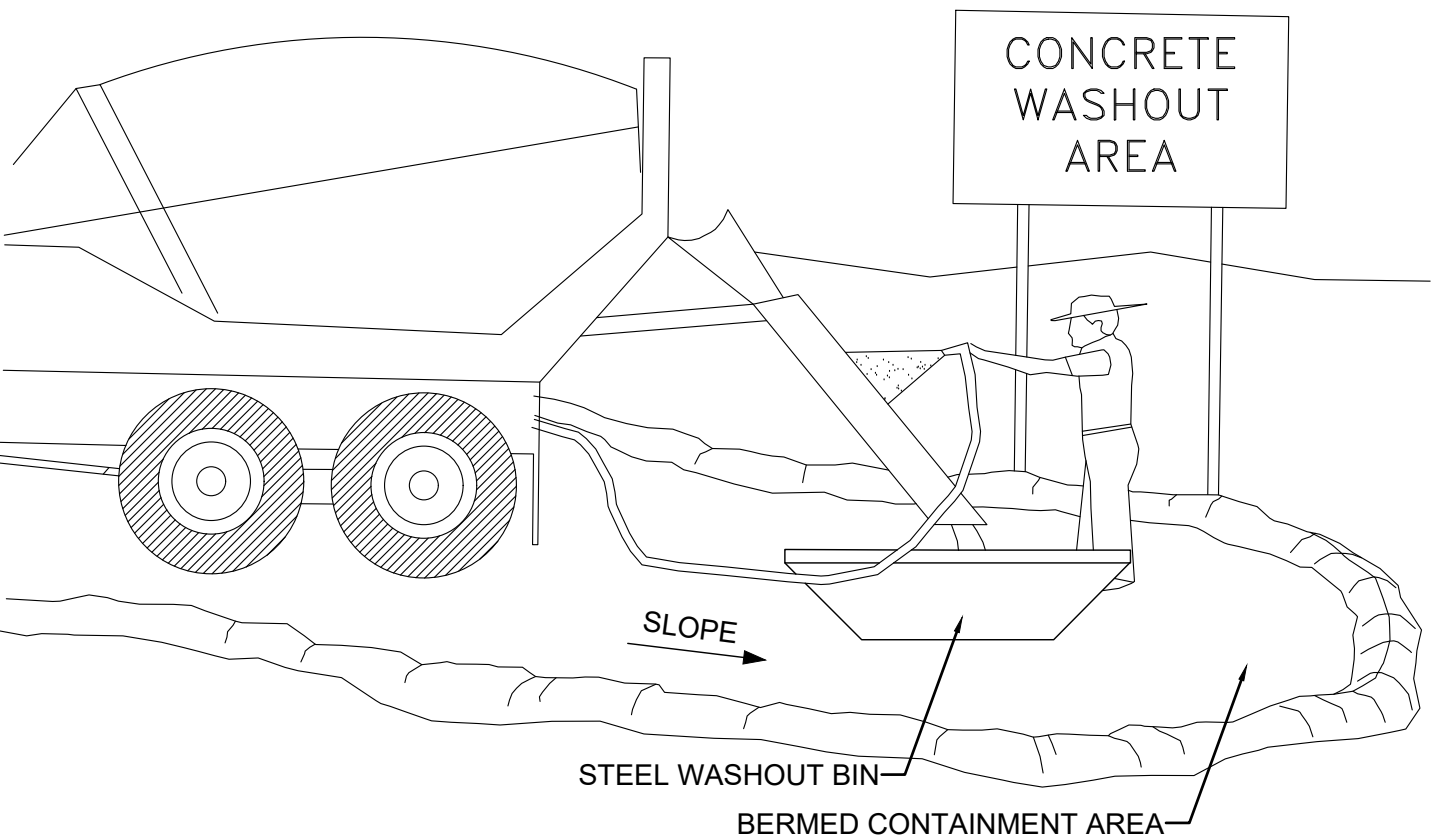
1. SEDIMENTS AND OTHER MATERIALS SHALL NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONSTRUCTION ENTRANCE ROADWAYS SHALL BE STABILIZED SO AS TO PREVENT SEDIMENTS FROM BEING DEPOSITED INTO THE STORM DRAIN SYSTEMS. DEPOSITIONS MUST BE SWEEPED UP IMMEDIATELY AND MAY NOT BE WASHED DOWN BY RAIN OR OTHER MEANS INTO THE STORM DRAIN SYSTEM.
2. STABILIZED CONSTRUCTION ENTRANCE SHALL BE:
 - a. LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY AND SIDEWALK OR PARKING AREA.
 - b. A SERIES OF STEEL PLATES WITH "RUMBLE STRIPS", AND/OR MIN. 3" COARSE AGGREGATE WITH LENGTH, WIDTH AND THICKNESS AS NEEDED TO ADEQUATELY PREVENT ANY TRACKING ONTO PAVED SURFACES.
3. ADDING A WASH RACK WITH A SEDIMENT TRAP LARGE ENOUGH TO COLLECT ALL WASH WATER CAN GREATLY IMPROVE EFFICIENCY.
4. ALL VEHICLES ACCESSING THE CONSTRUCTION SITE SHALL UTILIZE THE STABILIZED CONSTRUCTION ENTRANCE SITES.

STREET MAINTENANCE NOTES:

1. REMOVE ALL SEDIMENT DEPOSITED ON PAVED ROADWAYS IMMEDIATELY.
2. SWEEP PAVED AREAS THAT RECEIVE CONSTRUCTION TRAFFIC WHENEVER SEDIMENT BECOMES VISIBLE.
3. PAVEMENT WASHING WITH WATER IS PROHIBITED IF IT RESULTS IN A DISCHARGE TO THE STORM DRAIN SYSTEM.

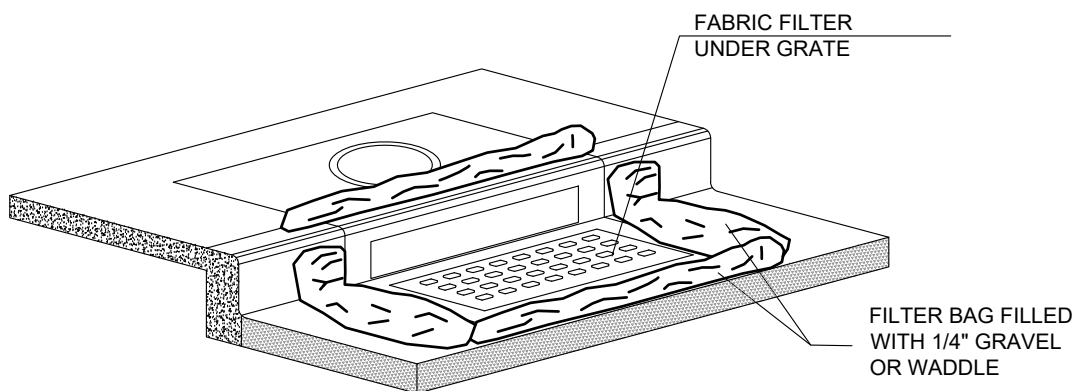
NOTE:

CONTRACTOR SHALL COMPLETE AND SUBMIT A STATE NOTICE OF INTENT (NOI) AND A STORM WATER POLLUTION PREVENTION PLAN BOOKLET



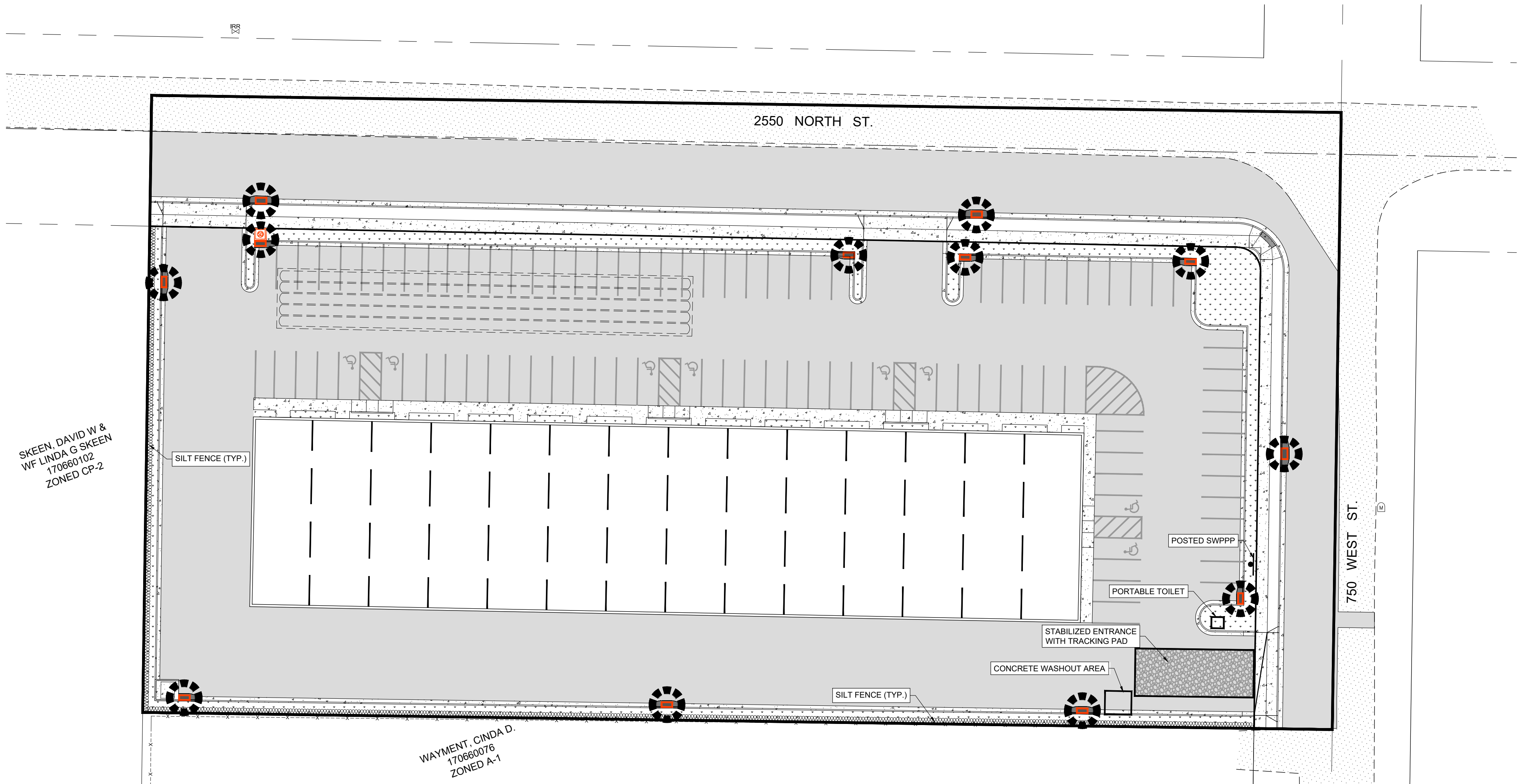
NOTES:

1. EXCESS AND WASTE CONCRETE SHALL BE DISPOSED OF OFF SITE OR AT DESIGNATED AREAS ONLY.
2. EXCESS AND WASTE CONCRETE SHALL NOT BE WASHED INTO THE STREET OR INTO A DRAINAGE SYSTEM.
3. FOR WASHOUT OF CONCRETE AND MORTAR PRODUCTS ONSITE, A DESIGNATED CONTAINMENT FACILITY OF SUFFICIENT CAPACITY TO RETAIN LIQUID AND SOLID WASTE SHALL BE PROVIDED.
4. ONSITE CONCRETE WASHOUT CONTAINMENT FACILITY SHALL BE A STEEL BIN OR APPROVED ALTERNATE.
5. SLURRY FROM CONCRETE AND ASPHALT SAW CUTTING HAL BE VACUUMED OR CONTAINED, DRIED, PICKED UP AND DISPOSED OF PROPERLY.

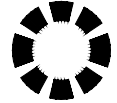


1B INLET PROTECTION - OPTION 2

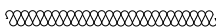
Scale: NTS



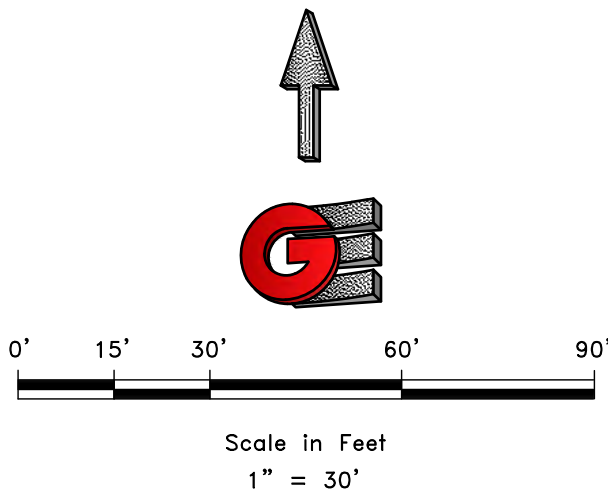
LEGEND



INLET PROTECTION
(EITHER OPTION)

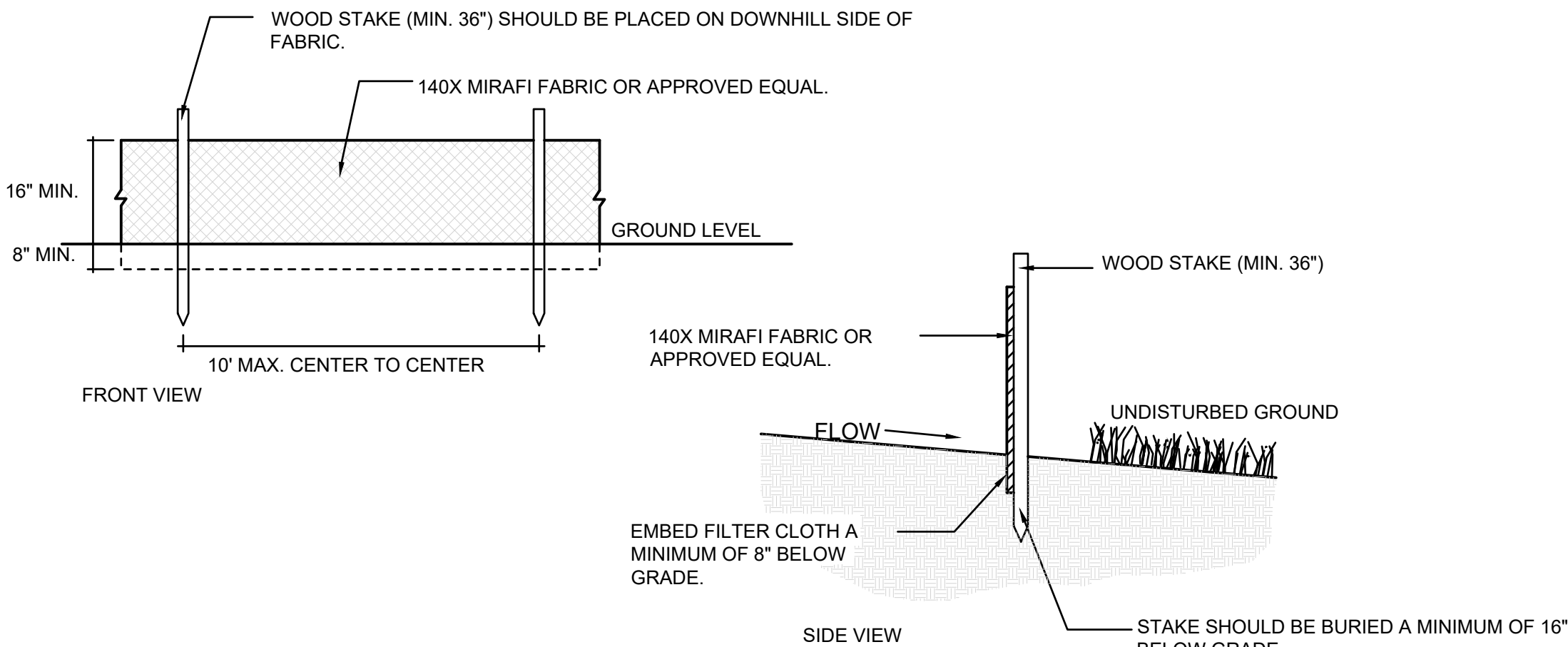
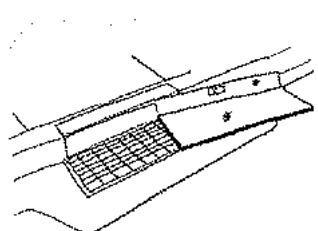
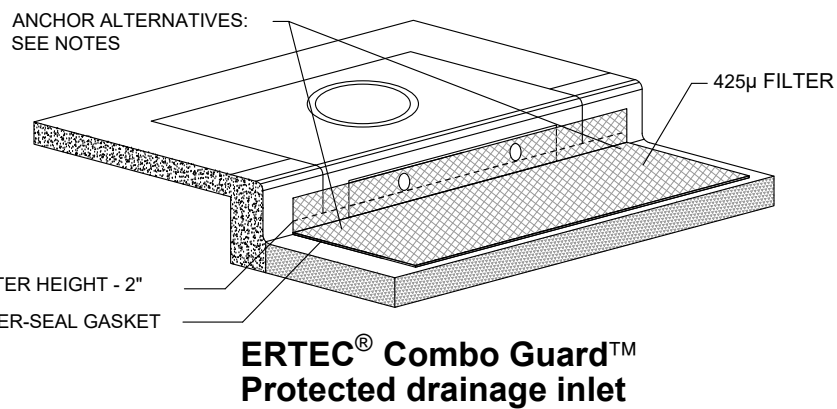


SILT FENCE



INSTALLATION NOTES

1. PLACEMENT: PLACE CG TIGHTLY AGAINST CURB. OPENING AND COVER ENTIRE GRATE. CG SHOULD EXTEND AT LEAST 2 INCHES PAST GRATE TOWARDS STREET.
2. OVERLAP FOR LONG OPENINGS: OVERLAP CG UNITS AT LONGER OPENINGS.
3. ANCHOR: ANCHOR CG SO THAT WATER CANNOT FLOW BEHIND IT.
4. ALTERNATE ANCHOR METHODS: A) INSTALL GRAVEL BAGS AT EACH SIDE OF CG - HALF-ON AND HALF-OFF THE EDGES. USE HALF-FILLED GRAVEL BAGS (15 OR 20 LBS). ROUND ROCK IS RECOMMENDED. OR B) ATTACH WITH 16 GAUGE TIE-WIRE. CUT WIRE TO 18" LENGTH. AT EACH CORNER OF CG, FEED ONE END OF WIRE DOWN THROUGH CG, AROUND GRATE BAR, AND BACK UP THRU CG. ABOVE GROUND, TWIST WIRES SEVERAL TIMES, CUT-OFF EXCESS. OR C) FASTEN WITH CONCRETE ANCHORS/NAILS AT THE OUTSIDE EDGES OF CG.



2 SILT FENCE

Scale: NTS

PRELIMINARY
NOT FOR CONSTRUCTION

EROSION CONTROL PLAN

HARRISVILLE COMMERCIAL

2550 NORTH 750 WEST

HARRISVILLE CITY, WEBER COUNTY, UTAH

GARDNER
ENGINEERING

CIVIL & LAND PLANNING
MUNICIPAL & LAND SURVEYING

1580 W 2100S, WEST HAVEN, UT 84401
P 801.476.0202 F 801.476.0066



EC1

SCALE: 1" = 30'

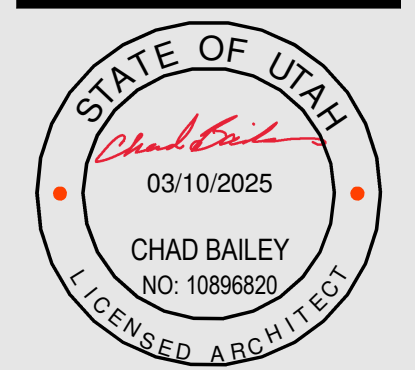
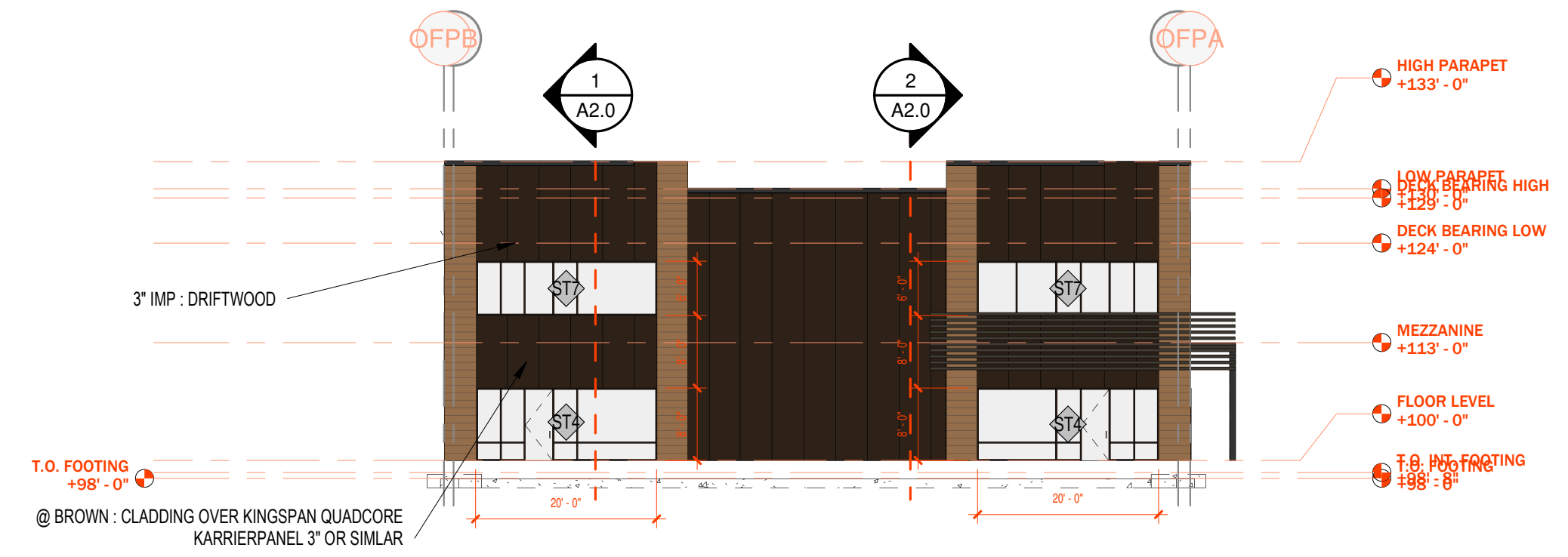
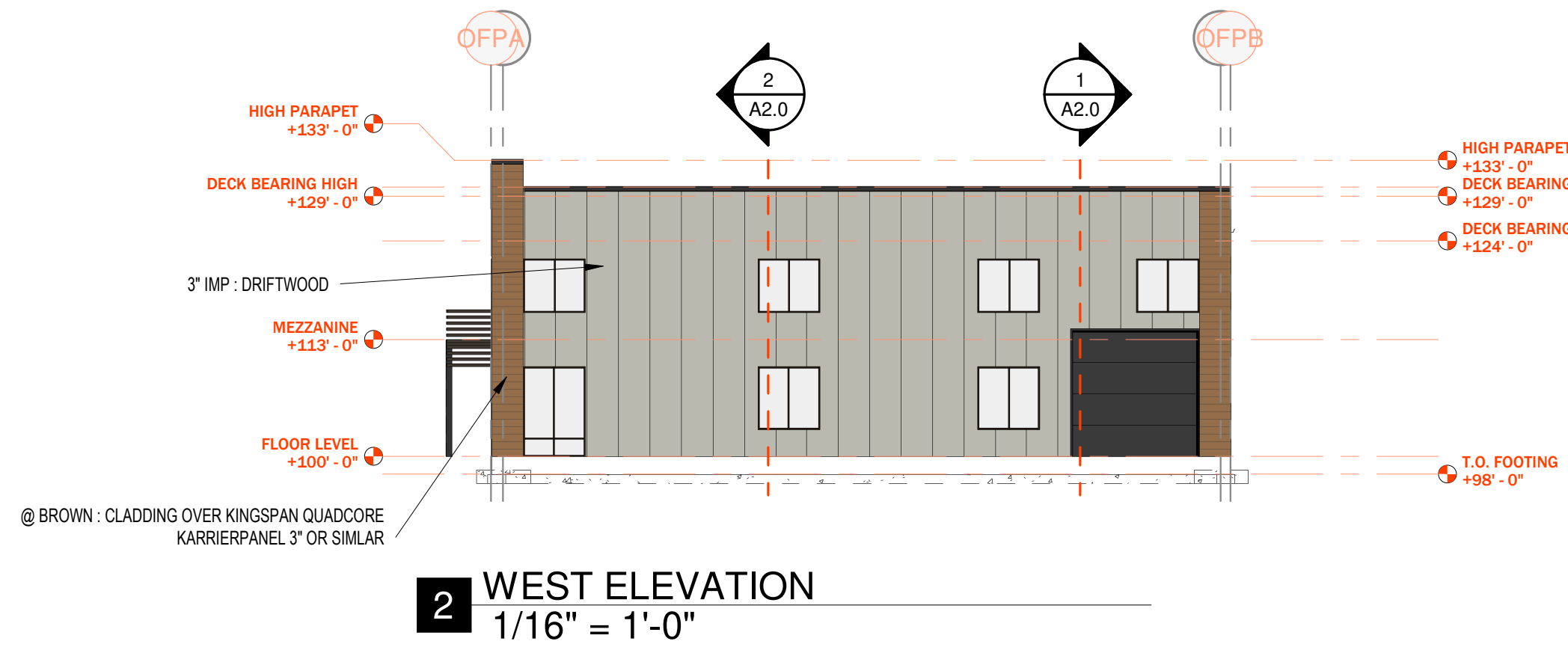
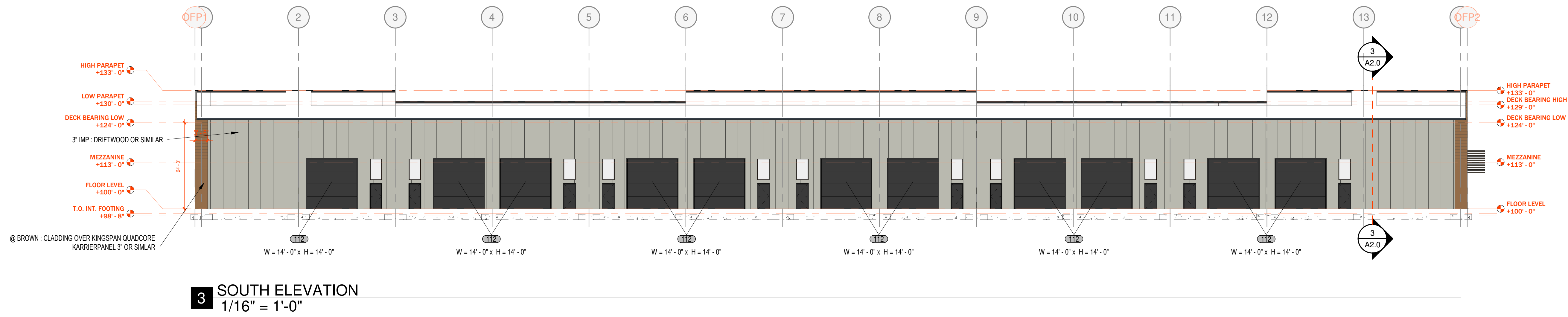
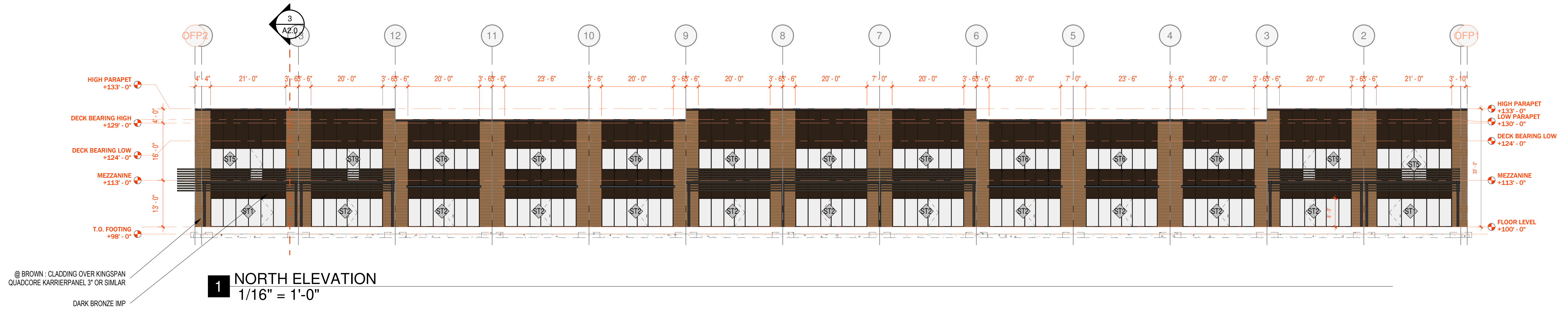
DATE: 4-15-2025

DESIGN: BSD

DRAWN: BSD

CHECKED: TN

DWG: 250205 - WILD MOUNTAIN/2401-HARRISVILLE COMMERCIAL - CDS/MS



DEVELOP
ARCHITECTS

DEVELOP ARCHITECTS, LLC
a: 1144 E 2800 N OGDEN, UT 84414
p: 801.823.9506
e: info@developarchitects.com
web: www.developarchitects.com

HARRISVILLE COMMERCIAL

Approx. 3155 North Highway 89
Pleasant View City, Utah 84404

OWNER:

ISSUE DATE:
03/10/2025

PROJECT PHASE
SCHEMATIC N.F.C.

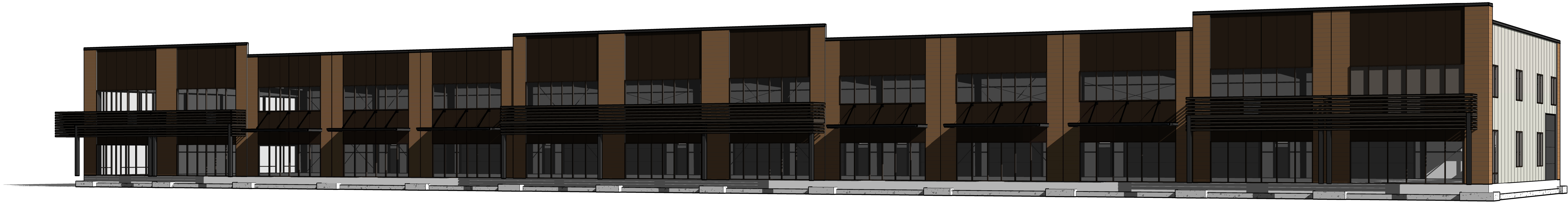
REVISIONS

NOT FOR CONSTRUCTION

ELEVATIONS

A3.0

SHEET NUMBER

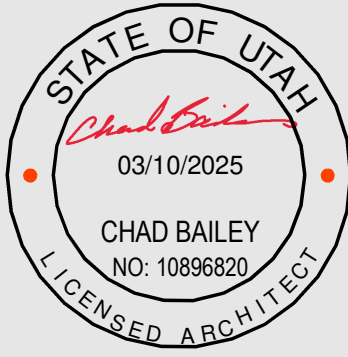
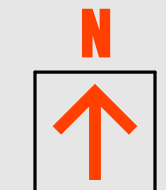


1 SIDE



2 FRONT

COMPASS:



DEVELOP
ARCHITECTS

DEVELOP ARCHITECTS, LLC
a: 1144 E 2800 N OGDEN, UT 84414
p: 801.823.9506
e: info@developarchitects.com
web: www.developarchitects.com

HARRISVILLE COMMERCIAL

Appx. 3155 North Highway 89
Pleasant View City, Utah 84404

OWNER:

ISSUE DATE:

03/10/2025

PROJECT PHASE
SCHEMATIC N.F.C.

REVISIONS

NOT FOR CONSTRUCTION

IMGS

IMGS

SHEET NUMBER



HARRISVILLE CITY

363 W. Independence Blvd Harrisville, Utah 84404 801.782.4100
www.cityofharrisville.com

PLANNING
COMMISSION

Angie Francom
Chad Holbrook
Isaac Thomas

Staff Report

Conditional Use Permit Application
Public Service Building
July 9, 2025

Synopsis

Application Information

Agenda Date: July 9, 2025
Applicant: Harrisville City
Application Date: July 1, 2025
Permit Number: Conditional Use Permit #147 – Public Service Building
Application Request: Applicant is requesting a Conditional Use Permit to consider a Public Service Building located at approximately 686 West 1750 North in the Open Space Zone (O-1).

Property Information

Approximate Address: 686 West 1750 North, Harrisville, Utah
Current Zoning: O-1

Overview:

Harrisville City has received a conditional use permit application for a new joint City Hall and Police station at approximately 686 West 1750 North in the Open Space Zone (O-1). In accordance with Harrisville Municipal Code §11.07.030(2) a conditional use permit is required for a Public Service Building to be located in the Open Space Zone.

Analysis:

Analysis of the application with regard to the Municipal Code is as follows (Comments in Bold):

1. HCMC §11.18.050 Basis For Issuance of Conditional Use Permit

- 1) That such use will not, at the particular location be detrimental to the health, safety, and general welfare of persons nor injurious to property or improvements of the surrounding land uses or community, but will be compatible with the existing surrounding uses, buildings, and structures. In determining compatibility and mitigation of detrimental effects, the planning commission shall consider:
- a) The location of parking lots, access ways, delivery areas and on site vehicle circulation patterns created by the site design and their relationship to adjoining uses and whether or not such site design adversely impacts the surrounding uses by exposing them to loss of privacy, objectionable views of large paved or graveled areas or loading and unloading areas and whether or not there are design considerations or property improvements that can mitigate these impacts;

Parking lots and longer access driveways have various sized landscape islands to avoid the appearance of a large parking lot. In addition, much of the parking is hidden behind the building and a large landscape buffer separates the building from the adjacent rural residential zone.

- b) The location of the use does not create unusual pedestrian or vehicle traffic patterns or volumes that would not be expected with the development of a permitted use. In determining unusual patterns, the planning commission shall consider:
- i) the orientation of driveways and if they direct traffic to the major streets or the local streets and if directed to the local streets the impact it creates to the safety, purpose and character of the local street; **The city is planning to construct 1750 North and extend it to connect with Highway 89. This connection is scheduled for completion before the new city offices become operational. The link to Highway 89 is expected to mitigate traffic impacts on surrounding local streets.**
- ii) parking locations and size and if they encourage street side parking and walking to the proposed use which impacts adjacent land uses; and **Adequate parking is provided off-street for the intending building use.**
- iii) hours of peak land use creating traffic volumes at times of the day or night that would impact the surrounding uses. **Peak use is anticipated for regular office hours Monday through Friday from 7 am to 5 pm.**
- c) The design of the building or buildings and the exterior building materials proposed to be used on the building and if the design and materials are similar in visual qualities such as, but not limited to, roof line shapes, building material color, reflectivity, and other visual qualities in order to ensure that the building design is not out of character with the surrounding area or creates a visual nuisance that impacts adjacent properties; **Permitted land uses in the area range significantly, from rural residential to industrial. The proposed two-story building as designed serves as a transition in both scale and appearance between the rural residential and industrial zones.**

- d) The hours of operation of the proposed use when compared with the hours of activity of the surrounding uses and the potential of such hours of operation to create noise, light or other nuisances not acceptable to the enjoyment of the existing surrounding uses or common to the surrounding uses; **The standard hours of operation are consistent with surrounding businesses and is not anticipated to cause a nuisance for the residences in the area.**
 - e) The location and size of outdoor storage areas and their relationship to adjacent land uses and if such storage creates adverse impacts to the surrounding uses in terms of visual appearance, noise, dust, odor, fire potential or hazardous material storage and the safe distances or other measures taken to screen or absorb the impacts on the proposed site; and **N.A.**
 - f) The location of exterior lighting and signage will not be directed to or impact adjacent residential uses. **Lighting is designed to prevent any trespass into the surrounding residential areas.**
- 2) That the proposed use will comply with the land use regulations specified in this Ordinance for such use in the specific zone the use is proposed in; **Public Service Buildings are allowed in the Open Space Zone with a conditional use permit**
- 3) That the proposed location does not have any unresolved actual or alleged violations of the municipal code. **N.A.**
- 4) That the proposed use conforms to the goals, policies and governing principles and land use of the Master Plan for Harrisville City. **A large area is planned for the South Side of the building to allow for outdoor public engagement opportunities.**
- 5) That the proposed use will not lead to the deterioration of the environment by emitting pollutants to the ground or air of such a type or of such a quantity so as to detrimentally effect, public or private property including the operation of existing uses thereon, in the immediate vicinity or the community or area as a whole. **No detrimental environmental effects are anticipated from the proposed use.**

Conditions:

None

Recommendation:

Staff recommends approving the conditional use permit based on compliance with Harrisville Municipal Code.



New submission from Conditional Use Permit Application (Commercial & Manufacturing Zones)

From Harrisville City Websites <no-reply@harrisvillecity.gov>

Date Tue 7/1/2025 3:38 PM

To Jack Fogal <jfogal@harrisvillecity.gov>

Date

07/01/2025

Name

Jennie Knight

Phone

801-782-4100

Email

jknight@harrisvillecity.gov

Applicant's Address

363 West Independence Blvd

Harrisville, UT 84404

[Map It](#)

Property Owner's Name

Harrisville City

Harrisville Property Address

686 West 1750 North

Present Zoning of Property

CP-2

Please describe the proposed conditional use or uses for the property:

Current zoning of the property is O-1 (Open-Space Zone). Harrisville City is applying for a conditional use permit under Harrisville Municipal Code 11.07.030(2) Public Service building to build a Public Safety/City Hall building.

Applicant's Signature

Property Owner Signature and Authorization (If you are not the property owner, please upload a signed letter from the property owner giving authorization to process this conditional use permit with Harrisville City)



Please upload a digital copy of your site plan. The site plan needs to show an aerial view of the business, marking where the business is located on the property and where parking or additional storage will be located on site.

- [Floor-Plans.pdf](#)

Payment Information - \$307.50 Fee

Confirmation Email Address

jknight@harrisvillecity.gov