

HALLIDAY ESTATES DEVELOPMENT

Request for Three Exemptions: Private Street Designation, an exemption to allow a 42' right-of-way, and a Type H curb and gutter

SUB-000519-2025— Request for Private Street Designation and 42' ROW

Location: 1280 W Marinwood Ave, SUB-000519-2025

Applicant: George and Cynthia Halliday

Date: 04/22/2025

We respectfully request approval for three exemptions related to our proposed residential subdivision:

1. **An exemption to designate our street as a private street**, under Taylorsville City Code 13.21.110.
2. **An exemption to allow a 42' right-of-way** instead of the standard 50' required for public roads.
3. **Type H curb and gutter** instead of a type A curb and gutter

We believe these requests are fully justified by the unique constraints of the property, the limited public benefit of a public road, and the precedent of similar developments in our immediate neighborhood. Below are the key reasons we urge the city to support this request:

1. The Property Cannot Connect to the Public Street Network

City Code 13.21.110 clearly allows for private streets when **connectivity to the existing street pattern is not feasible**. This applies directly to our situation:

- There is a **public road only 300 ft to the east** and another **800 ft to the west**.
- Our proposed street is a **dead-end** and provides **no through-street benefit** to the city or surrounding neighborhood.
- Connecting our road to the north would require **purchasing and demolishing at least two private homes**, which is speculative at best and highly unlikely in the foreseeable future.
- If the proposed road was public and the road was completed to 4800 S. It would only service homes along a one block street. There are two public roads that are through streets to the entire neighborhood 300ft to the east and 800 ft to the west.

*See Exhibit B - Aerial view of the property if a full-sized public road were to be built. (colors are to scale)

Orange = Road to the end of the proposed property line

Purple = future road

Red 'Xs'= structures needing to be demolished.

Compared to private street proposal, there will be no need for purchasing of north properties, no need for demolition and access to the neighborhood is not compromised.

This private road proposal fits precisely within the spirit and letter of the code: "A private street/lane system may be approved where it can be shown, based upon property width, connectivity (or the inability to connect to the existing street patterns), topographical concerns, overall subdivision design, the ability to provide service (which includes, but is not limited to, snowplowing, street sweeping, trash collection, and overall street maintenance), that a full width public street may not be the best solution for a new subdivision." This code does not give a reason to deny a private street on the basis of the ability to connect to a **future or speculative** street pattern.

2. An Underground Drainage Ditch at the North End Creates Major Barriers

There is a **subsurface waterway or ditch** at the north boundary of our property. If a public road were to be extended across this area, it would:

- Require **major engineering upgrades**, including culvert reinforcement, roadbed stabilization, and possibly re-routing of drainage.
- Greatly **increase construction costs** for the city or future developers.
- **Restrict access** to the ditch for inspections, emergency clearing, or repairs.
- Create potential **liability** for flooding, erosion, or damage to downstream water users if the road fails or interferes with water flow.

*See Exhibit C – Email from our engineer

These challenges would be **completely avoided** with a private street, as we would retain full control and access for any needed maintenance along the ditch.

3. Service & Maintenance Simplified with a Private Street

A public road at this location would result in an **operational burden for city services**, with **no real public benefit**:

- **Trash collection trucks and snowplows would have to reverse out of the dead-end street**, creating inefficiency and **increased risk for accidents**, especially in winter conditions or with limited visibility.
- **General maintenance**—pavement, sidewalks, signage, and utilities—would fall to the city indefinitely.

By contrast, a **private street shifts all responsibility to the HOA**:

- **Trash bins will be moved to Marinwood Avenue** on collection days, keeping service routes efficient and safe.
- **Snow removal will be managed by the homeowners**, ensuring prompt and consistent maintenance without relying on city services.
- **Ongoing street upkeep**, signage, sweeping, and repairs will be privately funded and executed—**saving city time and taxpayer dollars**.

The private street option is a clear benefit to city services and budget.

4. Emergency Access Is Fully Addressed

We are proposing a **private street with a dedicated emergency vehicle turnaround** that is **permanent, meets all applicable fire code standards**, and ensures **unobstructed access at all times**.

To maintain compliance and functionality:

- The turnaround is designed with the **required turning radius for emergency vehicles**.
- **“No Parking” signage will be posted and enforced**, ensuring the turnaround remains clear and accessible for fire, ambulance, and police services.

*See Exhibit A

This solution addresses all **public safety requirements** while preserving more usable land for homes and landscaping, something which a public road would take away from.

By using a well-planned private street and turnaround instead of a public dead-end street, we are enhancing both **neighborhood livability and emergency readiness**—all at **no cost to the city**.

5. 42' Right-of-Way is Logical, Safe, and Consistent with the Area

We are asking for an exemption to allow a **42' right-of-way (ROW)** with **26' of pavement**, sidewalks, and a landscaped park strip. This request is fully supported by:

- **Precedent in the neighborhood:** At least **three private streets in the area** have Merely a 25' ROW and 20' pavement significantly less than our 26' of pavement and 42' ROW.
- **Superior design:** Our plan includes features those private streets do not—**sidewalks, park strips, a wider pavement and ROW.**
- **Prior approval:** A 42' ROW had previously been approved for this property on prior plans and we would like to maintain that approval.

*See Exhibit D – Aerial view of Saxton Place in the same neighborhood,

Width of private street = 20'

*See Exhibit E – City records for Saxton Place 20' paved street with 25' ROW with no sidewalk or park strip

*See Exhibit F – Aerial view Treasure Cove less than 2 blocks away shows 20' paved road, less than 25' ROW with no sidewalk or park strip

*See Exhibit G – Unnamed private lane – less than 20' paved road, and 25' ROW with no sidewalk or park strip

*See Exhibit N – street view of Treasure cove (no curb and gutter), Saxton place (curb and gutter only on north side), unnamed private drive (no curb and gutter)

We are not asking for less—we are offering more than what has been accepted nearby. The 42' ROW supports safety, aesthetics, and functional use of the land.

6. We Are a Planned Development—and Deserve Equal Treatment

In correspondence with the City Planner, we were told:

“Private streets have only been approved as part of a planned development that is providing an added benefit to the city.”

We ask: Why should we be treated differently than large developers?

This is, in fact, a **planned development**—a small-scale, high-quality project of **four single-family homes** that brings long-term stability to the neighborhood. This is a benefit to the city and to local residents, especially considering:

- Nearby multi-family proposals have faced **significant public opposition.**
- Our design is consistent with the city’s vision and character.
- We are not investors or speculators—we are long-time residents seeking to build our family homes.
- If our development is not considered a planned Development than the statement, “private streets have not only been approved as part of a planned development that is providing an

added benefit to the city" is false as private streets have been approved for small scale developments within our same neighborhood.

*See Exhibit D – Aerial view of Saxton Place in the same neighborhood could have been a public road if you demolish only one structure. (They were allowed a private road)

*See Exhibit F – Aerial view Treasure Cove less than 2 blocks away butts up against Fore Lakes Golf course which has also been speculated to become a large development, yet they were allowed a narrow private street. The city wouldn't have to demolish any structures, unlike the speculative road from Halliday estates to 4800.

*See Exhibit G – Unnamed private lane – Same situation as Halliday Estates. Speculation shows they could connect to 4800 s with demolition of 2 structure, yet they were allowed a narrow private lane.

7. A History of Collaboration and Compromise

We have been working with the City of Taylorsville **since late 2022**, adjusting and compromising repeatedly in hopes of reaching a mutually beneficial solution.

Our original change from a cul-de-sac to a private lane was denied and so we changed our plans again to a private street. This was also denied by the city in lieu of a longer road which then required a 150' road and a turn-around. This is now being denied in lieu of a public road and increased width. We have acted in good faith at every turn and now feel that we have compromised enough.

Our family has lived in Taylorsville for over **40 years**. This is our home, and it always has been. We are not outside developers. We are simply trying to create a space where our children can stay rooted in the city we love.

Yet the process has begun to feel less like collaboration and more like resistance. It feels as though the city is prioritizing internal politics or future possibilities over the clear language of the law and the needs of the people who already live here.

We believe our request is fair, responsible, and entirely within the spirit of the city's long-term planning goals.

*See Exhibit H – Original plan that was denied

*See Exhibit I – Second plan that was denied

*See Exhibit J – Third plan that was denied

*See Exhibit K – all the lengthy emails showing our compliance with every ask.

8. Public Road Requirements Would Waste Valuable Private Investment

If forced to construct a public road to city standards—including a full 50' right-of-way, potential structural reinforcement over the drainage ditch, and future street extension preparation—it would require a significant financial outlay from the private property owners.

This cost would be borne entirely by us, not the city.

Instead of allocating that money toward unnecessary, oversized infrastructure that serves **no broader community benefit**, we could invest in:

- **Enhanced landscaping and beautification** of the neighborhood.
- **Higher quality home construction** that adds long-term value to the city.
- **Long-term private road maintenance**, ensuring durability without burdening city services.
- **Family-focused property improvements** that promote stable, long-term residency.

As long-time residents of Taylorsville, we want to **build with intention, beauty, and lasting quality**. Requiring a public road would compromise our ability to do that—for no gain to the city.

*See Exhibit C – Email from engineer

*See Exhibit A – Property proposal and use your own logic that a road twice as large takes up a large area of the property.

9. Major Sewer Design Constraint Prevents Future Extension

Another key constraint is the existing sewer depth:

- According to our project engineer, the **existing sewer main along Marinwood Avenue is extremely shallow**.
- As designed, the new sewer main under the private street would have only 3 feet 4 inches of cover over the 8-inch sewer pipe at the north manhole sewer.
- This shallow depth makes it impossible to extend the sewer northward past the north boundary line in the future without extensive, costly redesign and reconstruction.
- Page 7 (CPP.01) of the civil plans shows a profile of the surface of the road (black solid line) compared to the existing ground surface (dashed gray line) as well as the sewer main design.

In other words:

- **No viable future connection of the sewer to the north** can happen based on current infrastructure and natural grade of the property.
- **Public road standards anticipate future connectivity**, but in this case, utility constraints alone make future expansion impractical and cost-prohibitive.

This fact further supports the appropriateness of a private street exemption and **confirms that requiring a public street here would serve no future connectivity purpose**.

*See Exhibit C – Email from engineer

*See Exhibit A – Civil Plans

10. Update to Type H Curb and Gutter for Long-Term Flexibility

As part of our request, we are proposing to update the curb and gutter specification from Type A to Type H for the private street.

This change is better suited for the long-term success of this property because:

- **Type H (mountable) curb and gutter** allows future homeowners to install driveways wherever needed, **without the need for costly and damaging curb modifications**.
- Since **home designs for each lot have not yet been finalized**, the Type H curb provides **critical flexibility**—driveways can be easily installed by simply paving directly behind the mountable curb at the appropriate locations.
- With a **Type A curb**, extensive labor would later be required to **cut down the curb wall** and shape new driveway flares, causing:
 - Higher construction costs
 - Potential damage to the curb and gutter
 - Greater disruption to finished street surfaces

Additionally:

- By using **mountable curb throughout the entire private street**, there is **no need for transitions**, special curb cuts, or future street modifications.
- This choice ensures **cleaner, more uniform street aesthetics** and **lowers future maintenance needs**.

The **Type H curb and gutter** design is a **practical, cost-effective, and homeowner-friendly solution** that supports both the initial subdivision development and the future build-out of individual homes.

*See Exhibit L – Curb and gutter examples

*See Exhibit M – Street view Type H curb and gutter in Taylorsville Morning crest dr and neighborhood and showing that type A and Type H have and can be used in the same neighborhood

11. Increased Safety of a 26' Private Dead-End Road vs. a 50' Public Dead-End Road

While both road types terminate without through access, a narrower 26' private dead-end road (with 42' ROW) offers **greater safety and practicality** for residents and the city alike when compared to a 50' public dead-end road. Here's why:

Controlled traffic:

- Private roads are not open to the public by default, meaning **traffic is limited to residents and invited guests**.
- With fewer vehicles and only local users, **driver behavior is more predictable**, reducing risk to pedestrians and children.

Better Emergency management:

- Our **code-compliant emergency turnaround** ensures that fire and emergency vehicles can enter, turn around, and exit safely — something often overlooked on oversized dead-end public roads.
- "**No Parking**" signage will be posted at the turnaround and along one side of the private street to ensure constant access.
- In contrast, wider public roads without enforceable local management often experience **illegally parked vehicles or blocked access**, complicating emergency response.

Safer for pedestrians and children

- Narrower roadways naturally **shorten the distance pedestrians must cross**, reducing exposure to traffic.
- With **fewer vehicles and no pass-through traffic**, the environment becomes safer for **kids playing, people walking, and/or pets**.
- The inclusion of **sidewalks and park strips** offers a further safety buffer, which other private lanes and other public streets within the neighborhood lack.

For a family-oriented project like ours — intended for long-term, owner-occupied homes — the **narrower, private configuration provides the highest level of daily safety** for everyone involved, while maintaining adequate ROW for emergency vehicles and utility access.

Conclusion & Request

We are respectfully requesting approval of:

1. **An exemption to allow a private street**, as supported by code 13.21.110.
2. **An exemption to allow a 42' right-of-way and 26' pavement**

3. **Type H curb and gutter**, which includes ample pavement, sidewalks, and park strips—exceeding other nearby private street standards.

This plan reduces city liability, fits existing conditions, and reflects thoughtful development by long-standing members of this community. We ask that the city support us in building a future for our family here.

Thank you for your time and consideration.

Attachments:

- ❖ Exhibit A: Proposed plan for private street
- ❖ Exhibit B: Aerial view of the property if a full-sized public road were to be built. (Colors are to scale)
 - Red Highlight = Road to the end of the proposed property line
 - Purple = future road
 - Red 'Xs' = structures needing to be demolished.
 - Orange Line = distance from proposed street to nearest through road
 - Blue Line = distance from proposed street to second nearest through road
- ❖ Exhibit C: Email from our engineer
- ❖ Exhibit D: Aerial view of Saxton Place
- ❖ Exhibit E: City records for Saxton Place
- ❖ Exhibit F: Aerial view Treasure Cove
- ❖ Exhibit G: Unnamed private lane
- ❖ Exhibit H – Original plan that was denied
- ❖ Exhibit I – Second plan that was denied
- ❖ Exhibit J – Third plan that was denied
- ❖ Exhibit K – Email correspondence over the 3 years of trying to work with city planners/engineer
- ❖ Exhibit L – Curb and Gutter Designs and definitions
- ❖ Exhibit M – Type H curb and gutter in Taylorsville

Thank you,

George Halliday

Cynthia Halliday

Exhibit A – See other submitted files for full details and descriptions of the plans including the civil engineering plans

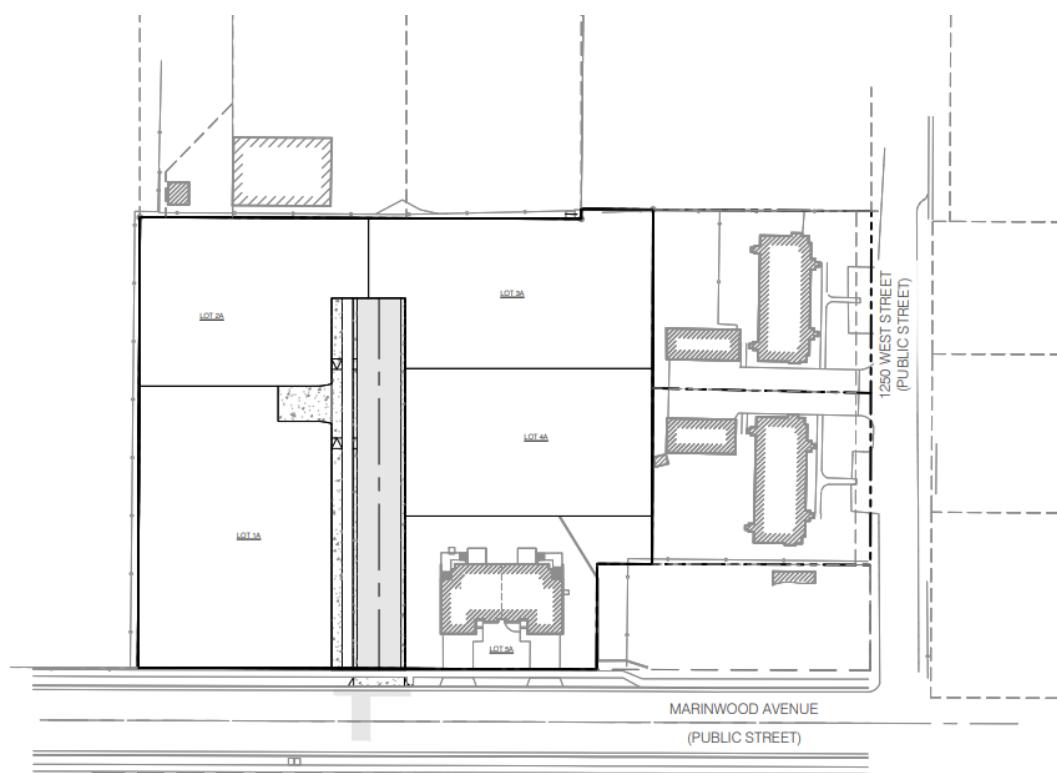


Exhibit B -

- Blue line shows a through street 800 feet to the west
- Orange line shows a through street 300 feet to the east
- Red box shows what the city proposes for a public street
- Purple Shadow shows the cities proposed through street
- Red X's show the structures that would need to be torn down just for the cities speculated through street.

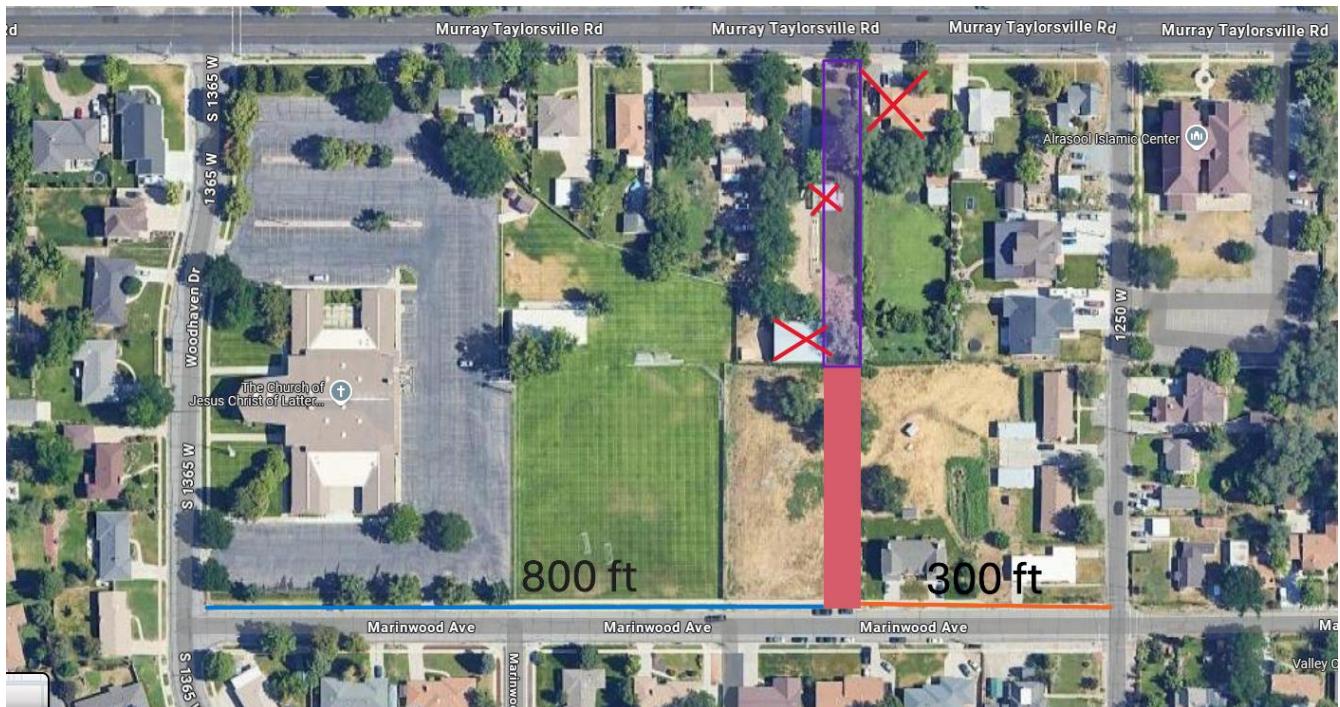


Exhibit C – Screenshot of email from Engineer showing work that would need to be done for a



Isaac Halliday <isaacwhalliday@gmail.com>

Your question about the Existing Irrigation Ditch for Halliday Estates Subdivision (2401012)

Zadok Budd <zadok@benchmarkcivil.com>
To: Isaac Halliday <isaacwhalliday@gmail.com>
Cc: Allison Albert <Allison@benchmarkcivil.com>

Fri, Apr 25, 2025 at 1:56 PM

Isaac,

I looked closely at the location of the ditch, and it appears that you would not be required to pipe the irrigation ditch as it is further to the North than your actual property line. (The existing fence is over two feet to the north of your property line and the ditch appears (from the existing contour lines) to be on the other side of that fence where the public road would intersect the north property line. The flowline of the ditch appears to be about 5 feet from the existing fence. It may be wise therefore to approach the topographic issue by explaining to the city that in a future redevelopment if they intend to extend the road to the north to connect with Murray Taylorsville Road that they will need to cover the cost to installing a concrete pipe sized to match the existing pipe as well as irrigation boxes, etc.

Another arguments for granting an exception to the 50 ft wide right of way requirement may likely be found in the width of your property. Between the existing duplex and west property line at the frontage of Marinwood, there is only 172.33 feet of width. Then the property narrows as it heads north, so there is only 152.22 feet of width by the time you reach the North (back) property line. Using a 50' ROW cuts into that, meaning lots 2A and 1A will have 8 foot less width to build in.

Lastly, the Sewer seems to be the largest design constraint. The existing sewer main within Marinwood is very shallow, as a result the new sewer main with the private road, there is only 3'-4" of cover over the 8" sewer main pipe. Meaning that sewer main could never be extended north.

Thank you so much!

Zadok Budd
Engineering Drafter



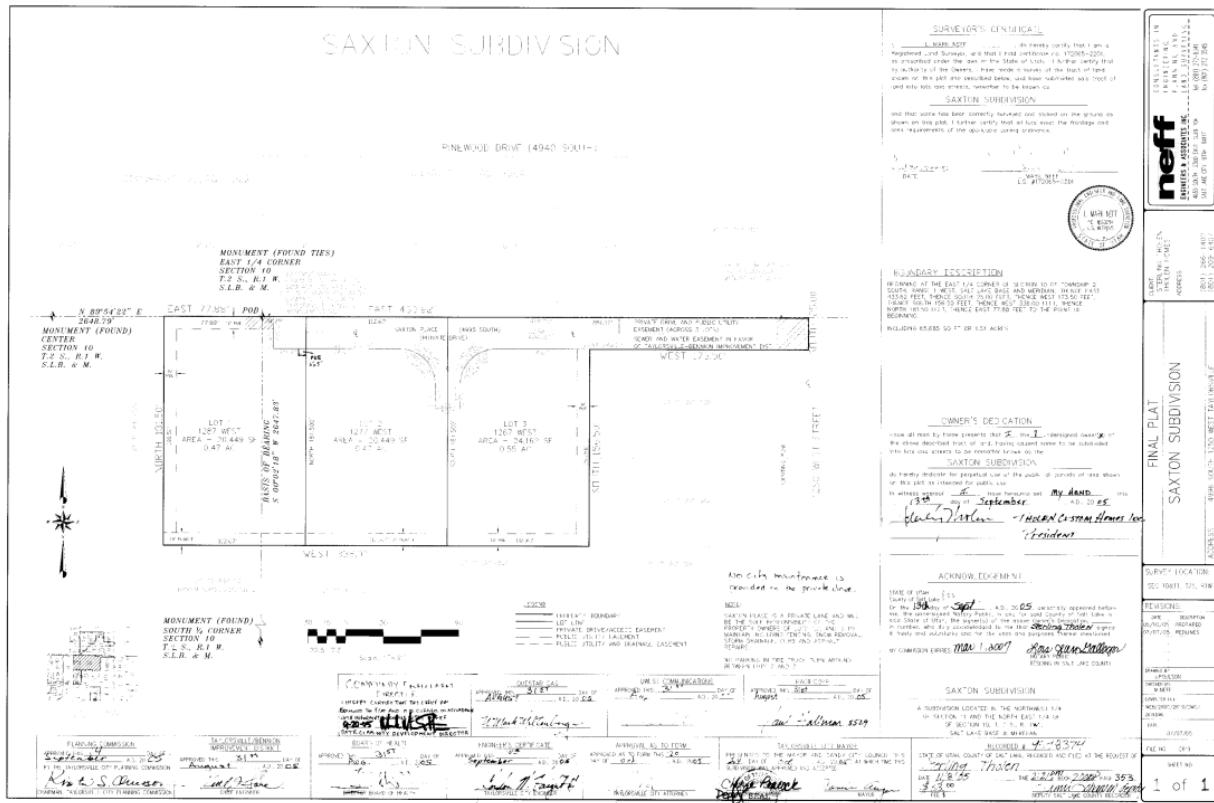
BENCHMARK
Engineering & Land Surveying, LLC
9138 South State Street, Suite 100
Sandy, Utah 84070
zadok@benchmarkcivil.com
[801.542.7192](tel:801.542.7192) – Office

Exhibit D

- Purple shaded area shows saxton place and it was measured at 20' wide
- Same situation of a possible through road if you demolish structures and 2 other through road close by



Exhibit E – See submitted Documents of the Public record of Saxton place showing 20' paved road was approved and 25' ROW approved. Here is a screen shot of the plans which were approved.



Planning Commission Staff Report

**6S05
3-lot subdivision
amendment
4996 South 1250 West**



**City of Taylorsville
Department of Community Development**

Date:	September 7, 2005
Meeting Date:	September 13, 2005
Agenda Item:	3-lot Subdivision (preliminary)
Subject Property Address:	4996 South 1250 West
Applicant:	Tholen Custom Homes Inc.
Applicant Agent:	Sterling Tholen
Author:	Dan Udall
Zoning designation:	A-1
Parcel #:	21-11-301-008
Applicable Ordinances:	Title 12 - Subdivisions
Public Notice:	Yes
Agenda item #:	14

Attachments:

- Exhibit A – Vicinity Map**
- Exhibit B – Subdivision Plat**
- Exhibit C – Private Road and Parkstrip**

Exhibit D – Application

Summary:

The applicant is requesting an amendment to a subdivision on property located at 4996 South 1250 West. On April 12, 2005 the Planning Commission approved a 3-lot subdivision on the subject property. The 3 lots are flag or deep lots accessed by a private road.

The applicant desires to provide a 5' wide parkstrip along the north side of the property. This improvement constitutes a subdivision amendment request.

Analysis:

The 3 lots that were approved for the subdivision constitute 20,580 square feet for lots 2 and 3 and 19,375 square feet for lot 1. Each lot will need deep lot review from the Planning Commission.

According to the deep lot policies “the private street or right-of-way serving the property must be at least 20 feet wide for one or two dwelling units served by the private right-of-way; or 25 feet wide, for three dwelling units served by the private right-of-way. Three deep-lot parcels is the maximum which may be served by a private street or right-of-way.”

The policy states that the “private street or right-of-way” needs to be 25 feet wide. The Planning Commission originally approved a 25-foot wide private road to access the lots. The policy does not state that the 25-foot right-of-way needs to be paved. The applicant is now proposing a 20' wide paved private road. The applicant desires a parkstrip for aesthetic reasons. The city engineer desires curb and gutter along both sides of the private road for storm drain purposes. The applicant does not desire weeds growing between the fence along the north side and the curb and gutter. The city engineer and the Fire Department are ok with the proposal. Planning staff is also ok with the proposal.

If the parkstrip is approved by the Planning Commission, the subdivision will be amended to allow the installation of the 5' wide grass strip. Staff desires a 2" caliper tree every 25 feet on center along the parkstrip.

Staff believes that the purpose of the 25' wide right-of-way required for 3 lots compared to the 20' wide right-of-way required for 2 lots is the increased traffic accessing 3 lots.

Adjacent Land Uses:

North: residential homes

South: residential homes

East: residential homes

West: residential homes

Staff Recommendation:

Staff recommends that the final regular subdivision be **approved** subject to the following conditions (these conditions were the same conditions approved at the preliminary stage, except condition numbers 10 and 11):

1. Receive approval from and remain compliant with all applicable reviewing agencies.
2. That the subdivision receives final plat approval from city staff.
3. That the subdivision is recorded by plat and that the plat complies with City Ordinance 12.16.010.
4. That any subdivision amendments proposed after the initial recordation are reviewed and approved by the Planning Commission. The amendment must then be recorded.
5. That the project receives storm drain approval from the city engineering department and pays the appropriate drain fees.
6. That any street lights should be installed if determined by the city engineer. That the city engineer should decide if curb and gutter is provided along both sides of the private road.
7. That each lot in the subdivision is reviewed by the Planning Commission as a deep lot.
8. That a vinyl 6' high fence is provided along the perimeter of the subdivision.
9. That the gross square footage of all the lots be provided on the final subdivision plat.
10. That 2"caliper trees are planted along the parkstrip every 25' on center.
11. That the private road is maintained and no parking is allowed along the road.

Discussion Points:

- Does the Planning Commission accept the subdivision as proposed?

Exhibit E

- Are the parkstrip and the private road acceptable?

Exhibit F

- Treasure Cove – shows a 20 ft wide private street to 3 lots and a fire turnaround.
- No space for a 50' ROW



Exhibit G

- Private lane to 2 houses less than 2 blocks away from proposed development
- Private lane is only 20 ft wide
- Through road across the street and east and west of it (same situation as proposed plat)

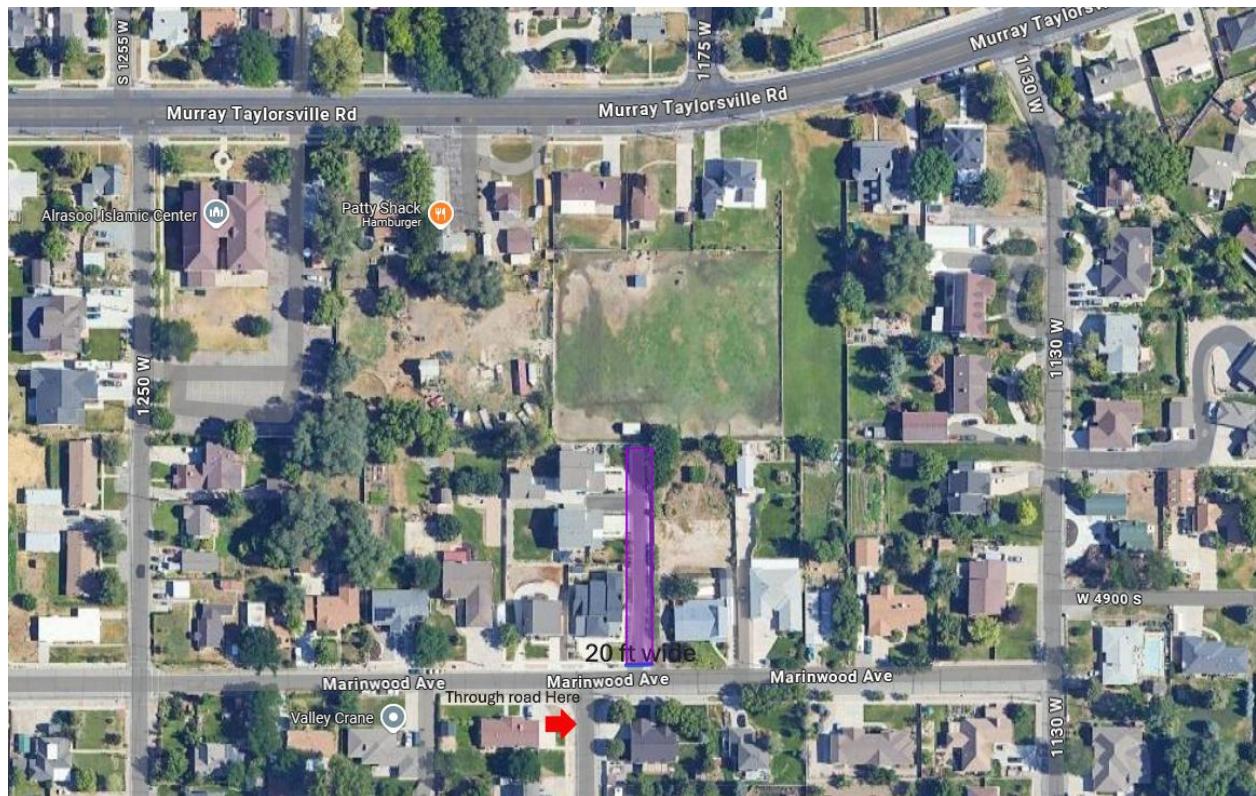


Exhibit H

- See submitted document for Original Plans not marked
- Arrow points to the original property line due to plans of a cul-de-sac

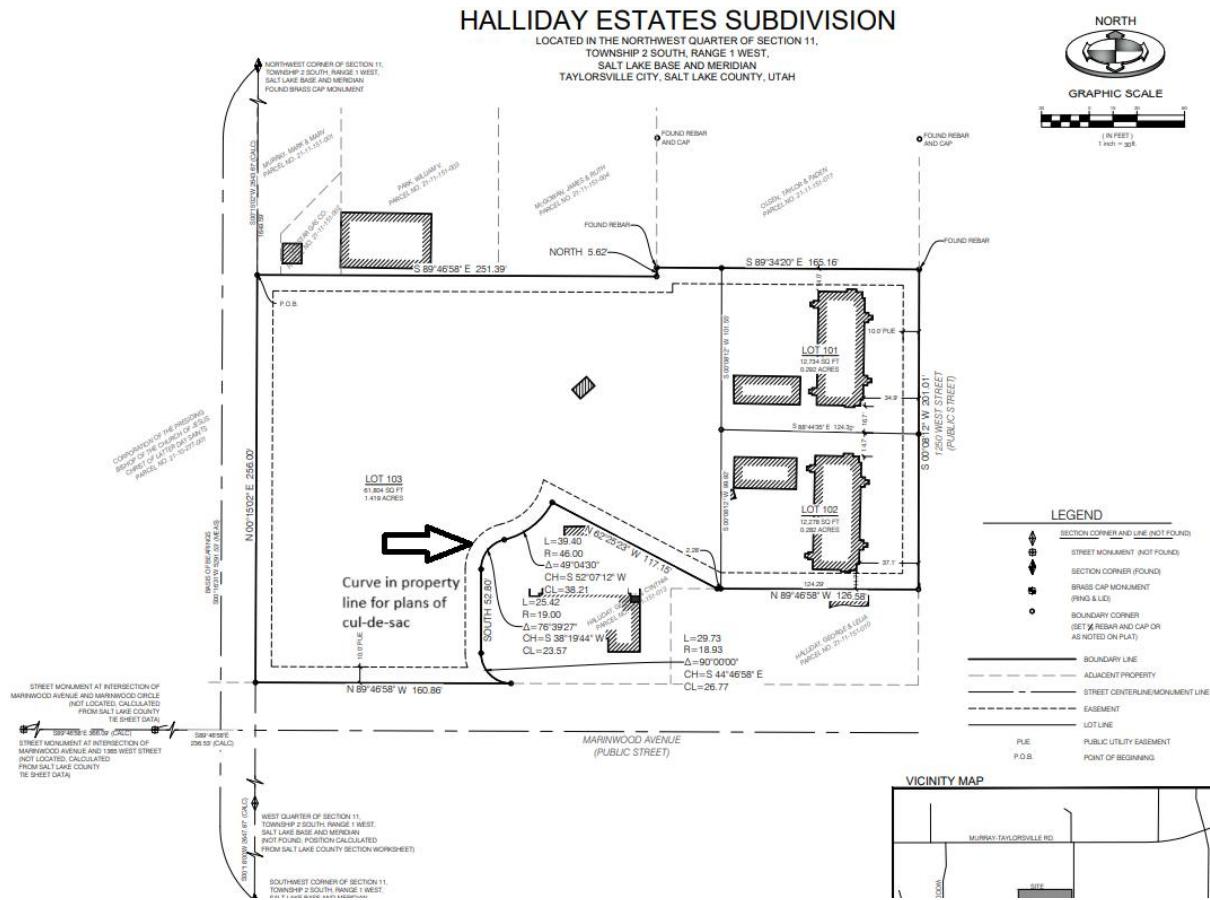


Exhibit I

- Second Concept of development showing two shared driveways equaling 40' in pavement and 50' ROW which was denied by city

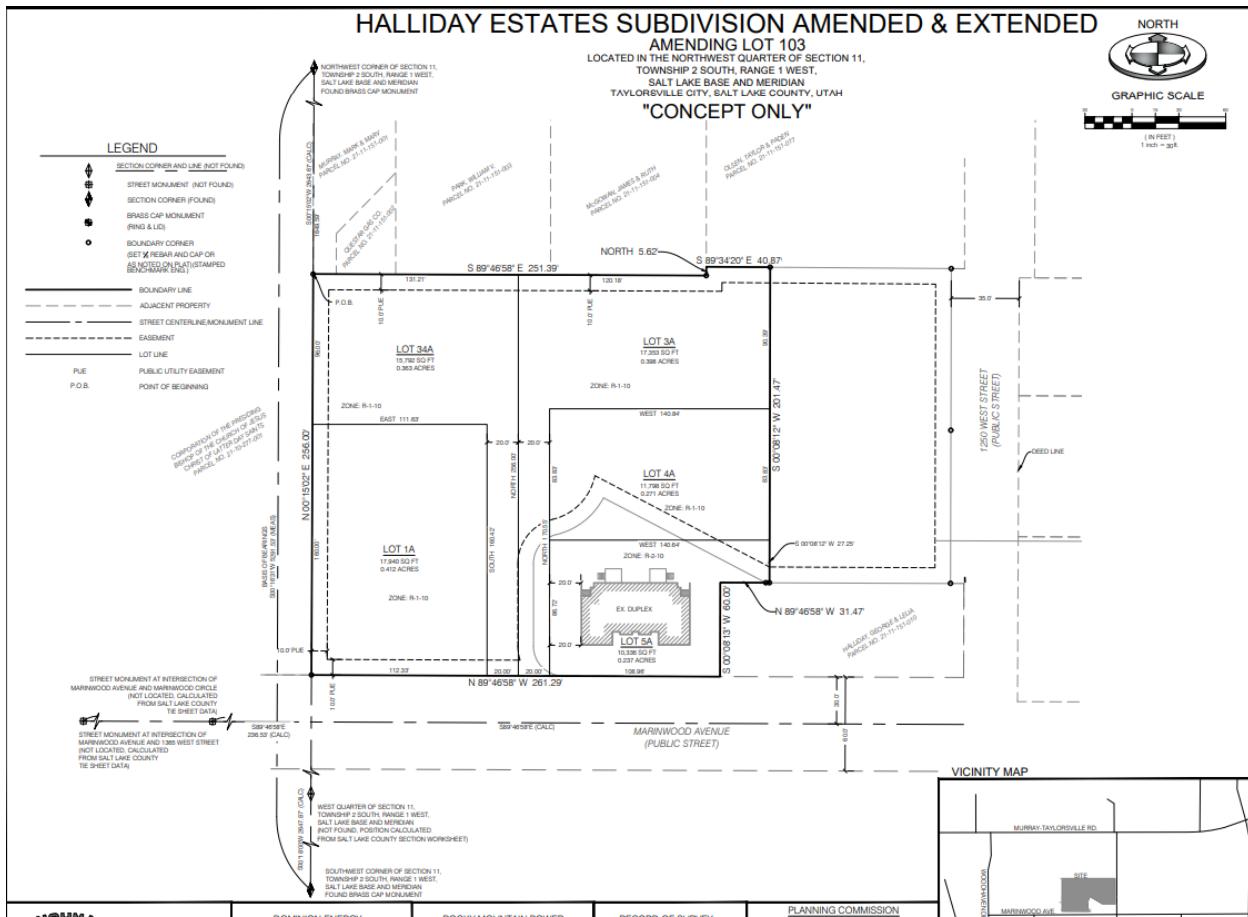


Exhibit J

- Third Concept that was denied by city with statements that it needs to be a through road to the north end of property (statements in Exhibit K)

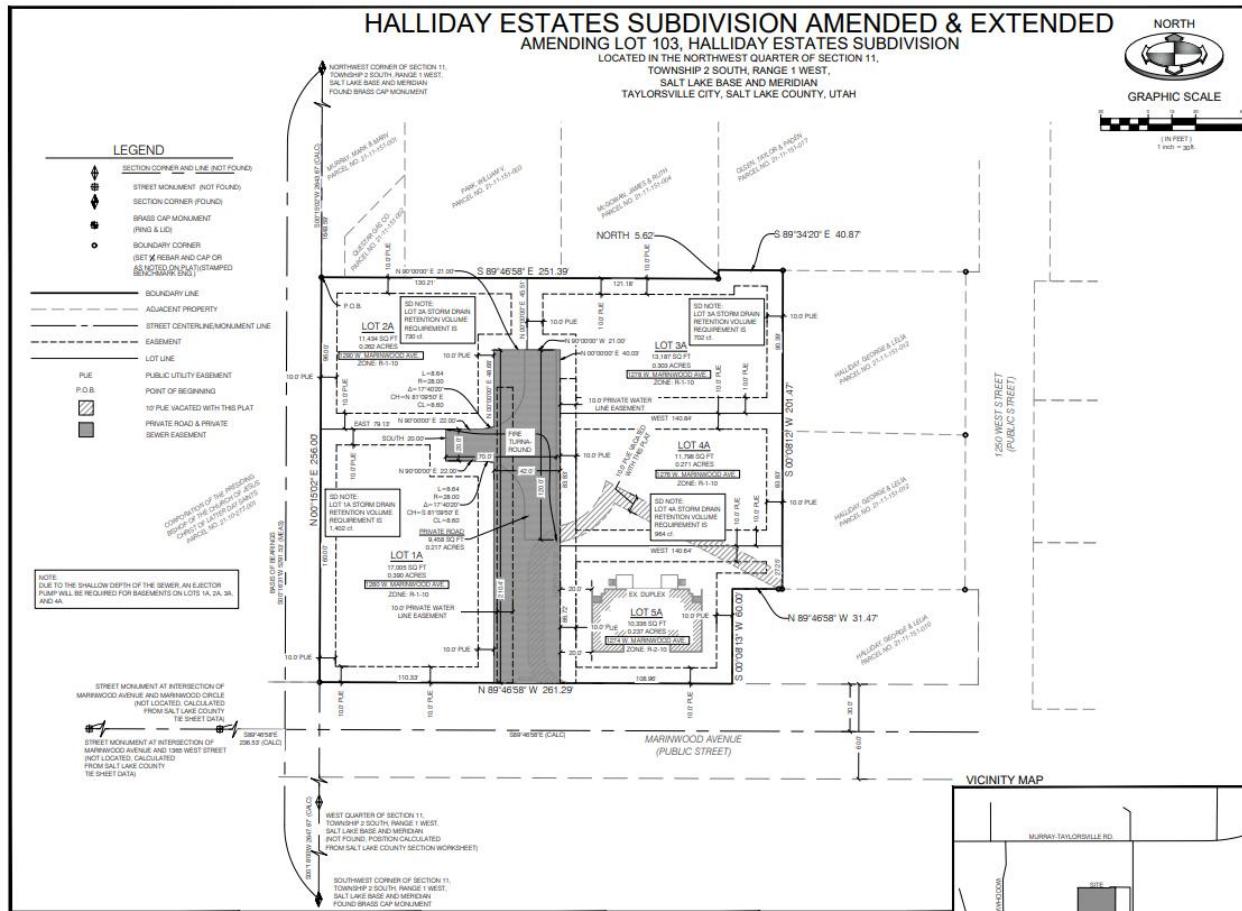


Exhibit K

- See submitted documents of many email chains of approved and denied plans and changes that have been made already
- These do not include the many meetings we had with the city engineer to get this far just to have plans denied (these were off record)



Ex K- Original Halliday Estates Plat Correspondence

Halliday Estates

Isaac Halliday <isaacwhalliday@gmail.com>
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Cc: George Halliday <gvhalliday@gmail.com>

Tue, Dec 27, 2022 at 2:17 PM

Terryne,

I uploaded the Title Search. Let me know if that is what you were looking for to answer issue #3. We have sent it to our engineer as well to do an ALTA survey. Hopefully he will get that done soon.

1. North property has a dedicated right-of-way. Show on plat, include dedicated right-of-way along 1250 West for properties to the south.
2. The rezone application related to the creation of Halliday Estates Subdivision affects lots that are not included in the proposed subdivision. This application is the opportunity to clean up all lot lines; parcel 21111510130000 and 21111510100000 could be included in the subdivision and property lines adjusted to create more uniform property lines and prevent the need to submit a separate application and fee in the future.
3. Submit a title report for all property that is to be included in the subdivision.
4. Ensure owner names on plat match exact names on file with county records.
5. Submit utility plans to show utilities for structures will not be affected by the changing and new property lines. Show utility easements on updated plat, if necessary.
6. Show front, rear, and side setbacks from structures to proposed property lines on survey.
7. Include boundary lines for neighboring parcels.
8. Upload drainage plans to show stormwater does not drain onto adjacent lots.
9. See comments related to signature boxes in attachment.

I am assuming the engineer will answer problems 1, 5,6,7,8 with the ALTA survey?

We are confused about issue #2 as the current proposed plan includes all affected lots that will be subdivided. Parcels 21111510100000 and 21111510130000 are not being affected by the subdivision. Only parcels 211115101800000 and 211115101200000 will be affected by the lot subdivision.

Thank you,

Isaac Halliday

City of Taylorsville | Proposed Halliday Estates Subdivision

Terryne Bergeson <tbergeson@taylorsvilleut.gov>
To: Cameron Lister <CLister@slco.org>
Cc: George Halliday <gvhalliday@gmail.com>, Isaac Halliday <isaacwhalliday@gmail.com>

Tue, Jan 17, 2023 at 11:00 AM

Good morning-

Attached is the draft plat for Halliday Estates Subdivision in Taylorsville. We have been working with the applicant, and I expect with their next submittal the draft will be ready for preliminary approval. Please review the attachment and advise of county comments. Thank you!

Terryne Bergeson | Planner I

(385) 308-0534

 **Halliday Estates Draft 1-4-23.pdf**
384K

From: Terryne Bergeson
Sent: Thursday, January 19, 2023 11:45 AM
To: Isaac Halliday <isaacwhalliday@gmail.com>; George Halliday <gvhalliday@gmail.com>
Subject: Halliday Estates Subdivision Review - File #14S22

George and Isaac,

This transmittal contains comments for the Halliday Estates Subdivision draft plat. Please contact the individual reviewer directly with questions relating to their comments.

Taylorsville Engineering Department:

Reviewer: Ben White, City Engineer, bwhite@taylorsvilleut.gov, 801-293-8344

1. Show width for public streets.
2. Include addresses for subdivision lots on plat.
3. Remove structures from plat.
4. Demonstrate/ provide explanation how exception in title report for parcel two (p. 9) is identified in proposed plat.

Taylorsville Building Department:

Reviewer: Steve Porten, Building Official, sporten@taylorsvilleut.gov, 385-379-5495

5. No comments until building permit.

Taylorsville-Bennion Improvement District:

Reviewer: Tammy North, District Engineer, tnorth@tbid.org, 801-968-9081

6. No additional comments.

Unified Fire Authority:

Reviewer: Tom Smolka, Fire Marshal, tsmolka@unifiedfire.org, 801-743-7236

7. No additional comments.

Taylorsville Community Development Department:

Reviewer: Terryne Bergeson, tbergeson@taylorsvilleut.gov, 385-308-0534

8. Provide utility plan.
9. Owner's dedication: signature lines for trustees should be identified as (Name), trustee for (Name of Trust)
10. Grading and drainage plan will be required when Lot 103 is developed.
11. Shared parking between lot 101 and 102, consider adding plat note or easement.

Please upload revised plans to the online permit portal and send me an email letting me know revised plans have been uploaded. As discussed, February 28th is the date we are expecting to bring the rezone and subdivision application to the Planning Commission. I will keep you updated of any changes. Please reach out with any questions you may have.

Terryne Bergeson | Planner I

(385) 308-0534



Isaac Halliday <isaacwhalliday@gmail.com>

Updates for preliminary plat

2 messages

Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Wed, Feb 22, 2023 at 11:06 AM

To: Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>

Good morning-

I got some feedback on two comments that have been discussed.

1. Utility plan: We discussed that we do want to make sure that the city doesn't sign off on the plat and then down the road properties get sold separately and learn that utilities for one structure are actually on another lot. We just need confirmation that utilities for each structure are on their own lot, or that easements are created where necessary. We understand the additional cost though- so if you can have blue stakes come out and mark where utilities are and then provide a sketch just to verify that utilities for each structure are on their own lot, that will suffice and save the cost of having them officially drawn up.
2. For the new proposed Lot 103, an address has been assigned and will need to be on the plat as well: 1276 W. Marinwood Ave.

Let me know if you have additional questions.

As mentioned, you are on the planning commission meeting agenda for 2/28 where they will review and consider the rezone and preliminary subdivision. I will send a copy of the agenda and staff report for your review when those are ready.

Planner I

George Halliday <gvhalliday@gmail.com>

Wed, Feb 22, 2023 at 6:44 PM

To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>, Isaac Halliday <isaacwhalliday@gmail.com>, Dale Bennett <dale@benchmarkcivil.com>, Brian Linam <brian@benchmarkcivil.com>, Esther Halliday <hallidaye@msn.com>

Thanks Terryne, I will call blue stakes and have them mark everything out.

On the address however 1276 W. Marinwood is an address that is assigned to the Duplex on Marinwood. The duplex has two addresses 1272 W. Marinwood & 1276 W. Marinwood. Brian has put 1270 W. Marinwood on the Plat will that work?

[Quoted text hidden]



Isaac Halliday <isaacwhalliday@gmail.com>

FW: Halliday Estates Subdivision Review - File #14S22 Draft Plat Comments

3 messages

Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Tue, Apr 4, 2023 at 5:17 PM

To: Dale Bennett <dale@benchmarkcivil.com>, Isaac Halliday <isaacwhalliday@gmail.com>, Brian Linam <brian@benchmarkcivil.com>, George Halliday <gvhalliday@gmail.com>, Esther Halliday <hallidaye@msn.com>

Hi George, thanks for following up today. Below are the last comments the city had on the draft plat. As I said, we just need a new draft with the below comments addressed and I will send that off to the county and circulate for comments... We're getting close to wrapping this up!

Terryne Bergeson | Planner I

(385) 308-0534

Resent comments from 1/19/23, page 2.

Brian Linam <brian@benchmarkcivil.com>

Wed, Apr 5, 2023 at 7:25 AM

To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>, Dale Bennett <dale@benchmarkcivil.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>, Esther Halliday <hallidaye@msn.com>

Here is a copy of the plat. I did the comments last month. Sorry it didn't make it to you. Let me know if you need anything else. Thanks

Regards,

Brian A. Linam, PLS

Survey Department Manager

2209256sp-2-13-23.pdf
378K

Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Wed, Apr 5, 2023 at 3:44 PM

To: Brian Linam <brian@benchmarkcivil.com>, Dale Bennett <dale@benchmarkcivil.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>, Esther Halliday <hallidaye@msn.com>

Thank you- I will circulate and send comments within 2 weeks.

Terryne Bergeson | Planner I

(385) 308-0534

Halliday Estates Subdivision Review - File #14S22 Draft Plat Comments

9 messages

Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Fri, Apr 7, 2023 at 11:56 AM

To: Esther Halliday <hallidaye@msn.com>, Dale Bennett <dale@benchmarkcivil.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>, Brian Linam <brian@benchmarkcivil.com>

This transmittal contains comments on the proposed draft plat for the creation of the Halliday Estates Subdivision. Please contact the individual reviewer with questions related to their comment.

Taylorsville Engineering Department

Reviewer: Ben White, City Engineer, bwhite@taylorsvilleut.gov, 801-293-8344

1. Need verification of how the Quit Claim Deed , Entry No. 2672133 is accounted for. From the deeds I have, I believe it is within the boundary of this plat. See attachment.

Taylorsville Building Department:

Reviewer: Steve Porten, Building Official, sporten@taylorsvilleut.gov, 385-379-5495

No comments until building permits.

Taylorsville-Bennion Improvement District:

Reviewer: Tammy North, District Engineer, tnorth@tbid.org, 801-968-9081

No comments.

Unified Fire Authority:

Reviewer: Tom Smolka, Fire Marshal, tsmolka@unifiedfire.org, 801-743-7236

No comments.

Taylorsville Community Development Department:

Reviewer: Terryne Bergeson, tbergeson@taylorsvilleut.gov, 385-308-0534

2. [1276 West Marinwood Avenue](#) is the address assigned to Lot 103, please update address.

Please upload a copy of plans with comments addressed to the online portal and send me an email letting me know that they are ready for review. Thank you.

Terryne Bergeson

Planner I

Brian Linam <brian@benchmarkcivil.com>

Thu, Apr 13, 2023 at 12:30 PM

To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>, Esther Halliday <hallidaye@msn.com>, Dale Bennett <dale@benchmarkcivil.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>

Here is the updated plat. Let me know if you need anything else. Thanks!

Regards,

Brian A. Linam, PLS

Survey Department Manager

Benchmark Engineering & Land Surveying

Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Mon, Apr 24, 2023 at 9:12 AM

To: Brian Linam <brian@benchmarkcivil.com>, Esther Halliday <hallidaye@msn.com>, Dale Bennett <dale@benchmarkcivil.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>

Hello- sorry for the delay in responding since we last spoke, George.

The remaining items we needed were:

1. An explanation about the strip of property along 1250 W that may or may not be part of this plat (the "less and excepting" identified in the title report).
2. The address of Lot 103. I spoke with the city engineer and 1280 W Marinwood Ave can be used for that lot.

Additionally, if you are interested in changing the address of the 1274 Marinwood property, that would be done through an address change affidavit and recorded with the county. That is just for your information and only if you want to update the address and is not related to this plat process.

Terryne Bergeson | Planner I

Brian Linam <brian@benchmarkcivil.com>

Fri, Apr 28, 2023 at 9:49 AM

To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>, Esther Halliday <hallidaye@msn.com>, Dale Bennett <dale@benchmarkcivil.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>

The 1. Comment. This portion has been removed from the subdivision. It was done after the original survey, and I was unaware. It is fixed now. Thanks for helping with that.

The 2. Comment. I changed it to 1280 W. Let me know if you need it to be something else. Thanks

Let me know if you need anything else. Thanks

Great, thanks. I have sent it to the recorder's office for review by the county and will let you know of next steps once I hear back!

[Quoted text hidden]

From: Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Sent: Tuesday, May 9, 2023 4:59 PM

To: Brian Linam <brian@benchmarkcivil.com>; Esther Halliday <hallidaye@msn.com>; Dale Bennett <dale@benchmarkcivil.com>; Isaac Halliday <isaacwhalliday@gmail.com>; George Halliday <gvhalliday@gmail.com>
Subject: RE: Halliday Estates Subdivision Review - File #14S22 Draft Plat Comments

George- I received comments from the county today. I am going out of town for the remainder of the week so if you have questions regarding the comments below, please reach out to the County Recorder's office. The comments in the attachment are city comments. Please submit a draft with all comments addressed from county and city reviews. If you have questions, I will be back on Monday. Thanks!

COUNTY COMMENTS:

A pre-recordation review of the attached "Halliday Estates Subdivision" plat was completed.

Our understanding is Taylorsville City approved the plat and therefore our review is for compliance to the recording requirements of Utah State Code Section 10-9a-603 or 17-27a-603.

Prior to final approval, we recommend the following review comments be considered before the mylar is printed or approval signatures gathered.

These comments include— but are not limited to the following:

1. Record of Survey
 - a. None
2. Plat title -
 - a. None
3. Exhibit -
 - a. The northeasterly corner of the subdivision should reflect a boundary line agreement recorded as Entry No. 11972247 – **I believe this has already been adjusted.**
 - b. The easterly subdivision boundary should include that area of 1250 West Street that needs to be dedicated. The courses of S. 89°34'20" E. 156.66 feet and N. 89°46'58" W. 118.08 feet will be lengthened to accommodate the dedication area **The area was dedicated by document already. That is why I removed it from the plat.**
 - c. Is the division line between Lots 101 and 102 intended to be along an existing fence line or another physical feature? If so, add a note to the plat indicating the intent. **It is in a good location. No feature needed.**
 - d. Add a distance along the easterly boundary of Lot 103 **added**
 - e. Several distances and angles are inconsistent with the record data for parcel 21-11-151-013 – **the survey was used for the plat**
4. Legend -
 - a. Describe the cap that will be placed at boundary corners **added**
 - b. Verify line types, scales, and symbols are consistent with the exhibit – **looks good**

5. Notes & Tables -
 - a. None
6. Narrative -
 - a. None
7. Vicinity Map -
 - a. None
8. Signature Blocks -
 - a. Note
9. Surveyor's Certificate -
 - a. Review the certificate language to accommodate the requirements of Utah State Code Section 10-9a-603(6)(b) **looks good**
10. Description -
 - a. In the caption, include recording re the vesting deed
 - b. Incorporate the boundary line agreement into the description **added**
 - c. Extend the easterly boundary to include that area of 1250 West that will be dedicated **The area was dedicated by document already. That is why I removed it from the plat.**
 - d. To clarify the intent, include bound/narrative calls:
 - i. To and along the boundary line agreement **added**
 - ii. To and along the westerly right of way line of 1250 West Street - **added**
 - iii. To an along the boundary of adjoining parcels **I try not to put bounds calls to parcels. Only subdivisions and right of ways.**
 - iv. To an along the west line of section 11 - **added**
 - e. Adding the area to be dedicated will increase the total acreage - **The area was dedicated by document already. That is why I removed it from the plat.**
10. Owner's Dedication -
 - a. None
11. Acknowledgement -
 - a. None
12. See attached review plat - **updated**

Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Fri, Jun 23, 2023 at 2:05 PM

To: Brian Linam <brian@benchmarkcivil.com>, Esther Halliday <hallidaye@msn.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>

Good afternoon,

The recorder informed your engineer that the plat is marked as "Ready to Record" with the county today, and all city comments are complete. Attached is the invoice for the final review (which is actually complete). Below are the steps:

1. Pay the invoice online or by calling city offices (801) 963-5400.
2. Print the mylar.
3. Gather signatures from outside agencies.
4. Once outside signatures are gathered, contact me to find the amount due for the recorder office (usually about \$52).
5. Bring the mylar to city offices and we will circulate for signatures and record.

You are pretty much done! Contact me with questions. Thanks!

Terryne Bergeson | Planner I
385-308-0534

Brian Linam <brian@benchmarkcivil.com>

Fri, Jun 23, 2023 at 2:06 PM

To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>, Esther Halliday <hallidaye@msn.com>, Isaac Halliday <isaacwhalliday@gmail.com>, George Halliday <gvhalliday@gmail.com>

George,

The Mylar will be at the front desk.



Exhibit K - pre-app questions Nov 2023

Halliday Estates shared driveway

6 messages

Isaac Halliday <isaacwhalliday@gmail.com>
 To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Sat, Nov 4, 2023 at 5:21 PM

Hello Terryne,

I have been working with my Dad, George Halliday, about how to engineer the property that we were able to subdivide earlier this year.

Could we have some clarification about shared driveways? How wide do they have to be? How many single family homes can be attached to a shared driveway?

And where are these codes available for us to look at?

Thank you,
 Isaac Halliday

Terryne Bergeson <tbergeson@taylorsvilleut.gov>
 To: Isaac Halliday <isaacwhalliday@gmail.com>

Mon, Nov 6, 2023 at 2:32 PM

Hi Isaac- below are some of the key requirements you'll want to review related to the individual lots and the subdivision, which will be required as lot 103 is subdivided and developed. A shared driveway that provides access to two homes may be able to be installed (similar to the shared driveway for the duplexes on 1250 W) . The design will need to show required improvements such as streetlights, utility boxes and fire hydrants. Since those elements are usually installed on property corners and putting a shared driveway would eliminate some of that space, you will want to evaluate whether shared driveways will leave enough space for the installation of required improvements (those can be found in the Subdivision Design chapter).

Both of the chapters below will apply to the development of lot 103.

[CHAPTER 13.20 Residential Development Standards](#) 13.20.050 has some standards for driveways/ pavement
[CHAPTER 13.21 Subdivision Design Standards](#)

13.21.030: RESIDENTIAL DRIVEWAYS:

A. *Driveways shall be provided for all residential building lots. The drive approach for the driveway shall be a minimum width of twelve feet (12') and shall not exceed the maximum width of thirty five feet (35'). A secondary drive approach may be permitted upon review and approval by the city engineer.*

B. *No down sloping driveways shall be permitted unless otherwise approved by the city engineer due to unusual topographic constraints. The driveway must maintain a positive slope away from the home as required by applicable building codes.*

C. *The minimum grade at which a driveway shall be allowed to be built is two percent (2%) slope, and the maximum grade at which a driveway shall be allowed to be built is twelve percent (12%) slope.*

D. *Residential driveways shall be constructed with a minimum concrete thickness of five inches (5") installed on a minimum of six inches (6") of compacted untreated base course or six inches (6") clean, two inch (2") minus sewer rock.*

E. *Residential driveway approaches shall be constructed with a minimum concrete thickness of six inches (6") installed on a minimum of six inches (6") of compacted untreated base course or six inches (6") clean, two inch (2") minus sewer rock. (Ord. 12-15, 7-11-2012)*

Let me know if there are any other questions.

Terryne Bergeson | Planner I

385-308-0534

From: Isaac Halliday <isaacwhalliday@gmail.com>
Sent: Saturday, November 4, 2023 5:21 PM
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Subject: [EXTERNAL] Halliday Estates shared driveway

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

[Quoted text hidden]

Isaac Halliday <isaacwhalliday@gmail.com>
 To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Mon, Nov 6, 2023 at 5:08 PM

Thank you so much this is super helpful.

How wide does the shared driveway have to be? We've been told 50' which seems extremely wide considering the public road is only 30'.

I found in section 14.12.130 it says 20' and I think it was 14.12.100 it says that a 50' right of way consists of 25' of pavement. So can it be 20' or has to be 25' wide?

Thank you

Isaac

[Quoted text hidden]

Terryne Bergeson <tbergeson@taylorsvilleut.gov>
 To: Isaac Halliday <isaacwhalliday@gmail.com>

Tue, Nov 7, 2023 at 9:44 AM

To make sure we're on the same page, when I say driveway I mean the curb cut and pavement leading from the road to a home's garage (and 50' is too wide so not sure where that comes from). We don't have a specific standard for a shared driveways so we would probably do more of a combined standard for a single driveway (previous email). The curb cut for a single-family driveway is usually 20' wide, but can range from 12-35' wide. You could plan on a single curb cut that is about 35' wide and that would probably provide enough space for each home to access the paved portion leading back to their garage. If it needed to be a little wider, it would require engineer review, but this could be discussed in the early stages.

For the road 50' wide right-of-way for the paved road, sidewalk and park strips. The width of paved surface will need to be 26 feet since the street will be used to access more than 2 homes. This is taken from the streets subsection in 13.21.100:

Right Of Way Width

Street type:

Major arterial	106+ feet
Major collector	80 feet
Minor collector	66 feet
Local	50 feet
Local rebuild	42 feet

Private street/lane type:

Private street	50 feet (26 foot pavement width minimum)
Private lane (2 lot maximum)	25 feet (20 foot pavement width minimum)

Let me know if you have more questions, or if you and your team have a concept design that you're interested in sharing. We can get you on the pre-application meeting when you are ready for preliminary feedback as well.

Terryne Bergeson | Planner I
385-308-0534

From: Isaac Halliday <isaacwhalliday@gmail.com>
Sent: Monday, November 6, 2023 5:08 PM
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Subject: [EXTERNAL] Re: [EXTERNAL] Halliday Estates shared driveway

[Quoted text hidden]

Isaac Halliday <isaacwhalliday@gmail.com>
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Tue, Nov 7, 2023 at 10:55 AM

This makes sense thank you!!!

So helpful

You're the best!

Isaac

[Quoted text hidden]

Isaac Halliday <isaacwhalliday@gmail.com>
To: George Halliday <gvhalliday@gmail.com>

Tue, Nov 7, 2023 at 10:56 AM

According to Terryne a private street needs to be 26' minimum.

A private lane is a 2 lot maximum and is 20' minimum width.

[Quoted text hidden]

On Tue, Apr 23, 2024 at 9:51 AM Dale Bennett <dale@benchmarkcivil.com> wrote:

Good morning George,

Sorry I missed your call yesterday. In speaking with Brian, he mentioned that he still hasn't received the updated title report to finalize the subdivision plat. Can you send that to him at your earliest convenience?

We will finish getting everything ready to submit to the city shortly after receiving the current title reports. If you would like us to reach out to the title company, please let me know and I will.

Thanks George!

Dale

From: George Halliday <gvhalliday@gmail.com>
Sent: Friday, April 26, 2024 3:43 PM
To: Dale Bennett <dale@benchmarkcivil.com>
Cc: Isaac Halliday <isaacWHalliday@gmail.com>; Esther Halliday <hallidaye@msn.com>; Chris Poulsen <chris@benchmarkcivil.com>; Brian Linam <brian@benchmarkcivil.com>
Subject: Re: File #14S22 - Halliday Estates Subdivision - Preliminary Subdivision Review

Dale and Brian I have attached title information that you need. If that doesn't work let me know and I will track down what is needed.

Thanks,



Isaac Halliday <isaacwhalliday@gmail.com>

RE: File #14S22 - Halliday Estates Subdivision - Revised layout - sketch -

3 messages

Dale Bennett <dale@benchmarkcivil.com> Thu, Jun 20, 2024 at 3:54 PM
To: George Halliday <gvhalliday@gmail.com>
Cc: Isaac Halliday <isaacWHalliday@gmail.com>, Esther Halliday <hallidaye@msn.com>, Chris Poulsen <chris@benchmarkcivil.com>

Hi George,

Thanks for taking my call today.

Attached is the updated concept of the plat. Please take a look at this and let me know if you want to change any lines.

The areas of each lot are approximate but should be close.

Lot 1A is around 18,400 s.f. +/

Lot 2A is 13,015 s.f. +/-

Lot 3A is 13,406 s.f. +/-

Lot 4A is 10,601 s.f. +/-

Lot 5A is the duplex and is a little larger than what the plat shows because we don't need to put a curve in the 20' flag pole.

It may be a little different than what you sketched so please let me know if you want me to make any changes to the lot lines. If this is good, we can move ahead with the plat and get Chris and the civil team on the civil plans needed for the city.

Thanks,

Dale

Dale Bennett <dale@benchmarkcivil.com>
To: George Halliday <gvhalliday@gmail.com>
Cc: Isaac Halliday <IsaacWHalliday@gmail.com>, Esther Halliday <hallidaye@msn.com>, Chris Poulsen <chris@benchmarkcivil.com>

Thu, Jun 20, 2024 at 3:59 PM

George,

Also, as discussed in the meeting we had at the city, you can submit for a building permit anytime on the entire property for your home anytime. This way you can get your home going while the subdivision process is happening.

[Quoted text hidden]

 **Lot 1A exhibit showing the approximate building footprint for your home designer.pdf**
381K

George Halliday <gvhalliday@gmail.com>
To: Isaac Halliday <isaacwhalliday@gmail.com>, gchallidays@msn.co

Sat, Jun 29, 2024 at 2:43 PM

----- Forwarded message -----

From: Dale Bennett <dale@benchmarkcivil.com>
Date: Thu, Jun 27, 2024, 12:31 PM
Subject: File #14S22 - Halliday Estates Subdivision - Revised layout - sketch -
To: George Halliday <gvhalliday@gmail.com>
Cc: Chris Poulsen <chris@benchmarkcivil.com>

George,

Are you good with this layout? Our team is ready to move forward with the 5 lot subdivision (two flag poles and the duplex included do to the lot line adjustment).

If you just want to submit for your building permit first and hold off on doing anything with the subdivision, please let us know.

We are ready to move ahead but want to make sure you are good with the layout and want to proceed.

Thanks George!

Dale

**FW: 2401012 - [EXTERNAL] RE: [EXTERNAL] Halliday Estates Amended Sub
Proposed changes**

2 messages

Allison Albert <Allison@benchmarkcivil.com>
To: "isaacwhalliday@gmail.com" <isaacwhalliday@gmail.com>
Cc: Dale Bennett <dale@benchmarkcivil.com>

Tue, Apr 15, 2025 at 3:51 PM

Isaac,

Attached and below is the conversation with Terryne regarding the redesign/ use of the 42' private road. This is something that has been coordinated with the city before the civil plans were revised, and as you are coordinating with them this time around, I want to make sure you have all the appropriate information and communications.

Let us know if you have any questions.

Thanks,

Allison Albert, PE
Project Manager

From: Dale Bennett <dale@benchmarkcivil.com>
Sent: Wednesday, October 18, 2023 4:46 PM
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Cc: George Halliday <gvhalliday@gmail.com>
Subject: [EXTERNAL] Halliday Estates Amended & Extended - very rough concept

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Terryn,

Attached is the very rough sketch we discussed. We are showing the 50' private lane and the lots going to the centerline of the private lane.

Please call George when you can to discuss.

Thanks Terryn!

From: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Sent: Wednesday, October 18, 2023 5:03 PM
To: Dale Bennett <dale@benchmarkcivil.com>
Cc: George Halliday <gvhalliday@gmail.com>
Subject: Re: [EXTERNAL] Halliday Estates Amended & Extended - very rough concept

Thank you for sending this. I checked a few places in our code and the area for a private lane DOES NOT count toward the total area for the lots. The area is determined from back of sidewalk to rear property line. I know this will change some things for your subdivision but wanted to let you know.

The turnaround would need to meet fire codes but could be approved in place of the cul-de-sac and hopefully conserve some valuable space.

I will be out of the office until Monday but will watch email and do my best to answer any other questions. If you have immediate questions, you can call city offices (801)963-5400.

Terryne Bergeson | Planner I
385-308-0534

From: Dale Bennett <dale@benchmarkcivil.com>
Sent: Monday, November 13, 2023 4:50 PM
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Cc: George Halliday <gvhalliday@gmail.com>; Chris Poulsen <chris@benchmarkcivil.com>
Subject: [EXTERNAL] 2209256 - Halliday Estates Amended & Extended - very rough concept

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Terryne,

Thank you very much for speaking with us this afternoon. Attached is what Mr. & Ms. Halliday would like to do with their lot 103. They plan to build on lot 101 and their kids on lots 102-104. Lot 105 is the duplex, which will maintain a minimum of 10,000 s.f. but the lot lines will be cleaned up. Each lot will have 10,000 plus square feet not including the private street. Lots 102 and 103 will access from the fire turnaround in this proposed concept.

The private road is 50 feet wide with a fire turnaround at the end. Please let us know if this is something that can be submitted for approval with the planning commission. If you have any suggested changes, please don't hesitate to mark this up and send it back so I can have my team begin working on the preliminary subdivision submittal.

Thanks!

From: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Sent: Thursday, November 16, 2023 2:00 PM
To: Dale Bennett <dale@benchmarkcivil.com>
Cc: George Halliday <gvhalliday@gmail.com>; Chris Poulsen <chris@benchmarkcivil.com>
Subject: Re: [EXTERNAL] Re: [EXTERNAL] 2209256 - Halliday Estates Amended & Extended - very rough concept

Hi, I sent your proposal to the City Engineer and he returned a concept with the cul-de-sac in the same area where there is currently a curve already in the property lines. A hammerhead would need to be approved by the Planning Commission (their approval is needed anyway so they would just look at the hammerhead during the review of the subdivision amendment). The hammerhead will require the same 50 right-of-way as the road. Additionally as you know the concept requires a rezone for the areas of the duplex lot as you change lines there- which is subject to the Planning Commission recommendation and City Council discretion.

I am going to attach the pre-application meeting information here:

Before submitting a formal application, it is best to bring your proposal to a pre-application meeting so city staff and reviewing departments and agencies can discuss process, timelines, standards, etc. with you and your team. Attached is a pre-application meeting handout summarizing the intent and procedure for this meeting. This is a courtesy meeting allowing applicants and staff to have a dialog regarding application and review processes, design, or other questions to help ensure a smooth application and review process. The following folks generally attend to provide feedback and to answer questions:

From: Dale Bennett <dale@benchmarkcivil.com>
Sent: Monday, November 13, 2023 4:50 PM
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Cc: George Halliday <gvhalliday@gmail.com>; Chris Poulsen <chris@benchmarkcivil.com>
Subject: [EXTERNAL] 2209256 - Halliday Estates Amended & Extended - very rough concept

Terryne,

Thank you very much for speaking with us this afternoon. Attached is what Mr. & Ms. Halliday would like to do with their lot 103. They plan to build on lot 101 and their kids on lots 102-104. Lot 105 is the duplex, which will maintain a minimum of 10,000 s.f. but the lot lines will be cleaned up. Each lot will have 10,000 plus square feet not including the private street. Lots 102 and 103 will access from the fire turnaround in this proposed concept.

The private road is 50 feet wide with a fire turnaround at the end. Please let us know if this is something that can be submitted for approval with the planning commission. If you have any suggested changes, please don't hesitate to mark this up and send it back so I can have my team begin working on the preliminary subdivision submittal.

Thanks!

Dale K. Bennett, PLS, PE

From: Allison Albert <Allison@benchmarkcivil.com>
Sent: Monday, October 28, 2024 3:22 PM
To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Cc: George Halliday <gvhalliday@gmail.com>; Chris Poulsen <chris@benchmarkcivil.com>; Dale Bennett <dale@benchmarkcivil.com>; gchallidays@msn.com
Subject: [EXTERNAL] Halliday Estates Amended Sub Proposed changes

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Terryne,

Attached is the proposed subdivision changes we discussed on the phone. If you (planning) and engineering could review and let us know if everything looks acceptable/good- that would be great!

Thanks and let us know if you have any questions.

From: Terryne Bergeson <tbergeson@taylorsvilleut.gov>
Sent: Monday, October 28, 2024 4:12 PM
To: Allison Albert <Allison@benchmarkcivil.com>
Cc: George Halliday <gvhalliday@gmail.com>; Chris Poulsen <chris@benchmarkcivil.com>; Dale Bennett <dale@benchmarkcivil.com>; gchallidays@msn.com
Subject: RE: [EXTERNAL] Halliday Estates Amended Sub Proposed changes

Received, thank you Allison. We will take a look and let you know if there are any major concerns with the layout.

Terryne Bergeson | Planner I

(385) 308-0534

From: Allison Albert <Allison@benchmarkcivil.com>

Sent: Friday, November 15, 2024 12:56 PM

To: Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Subject: [EXTERNAL] RE: [EXTERNAL] Halliday Estates Amended Sub Proposed changes

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hey Terryne,

I just wanted to circle back on this since your latest email listing the requirements for the private street. Did you or your team see anything that would prevent us from revising the plans as redlined in the attached document? We want to just make sure before revising the entire set of plans.

From: Terryne Bergeson <tbergeson@taylorsvilleut.gov>

Sent: Friday, November 15, 2024 1:37 PM

To: Allison Albert <Allison@benchmarkcivil.com>

Subject: RE: [EXTERNAL] RE: [EXTERNAL] Halliday Estates Amended Sub Proposed changes

Hi Allison,

The document in your email looks like the one we gave comments on a couple weeks ago. Below are those comments from the last email for what would need to be shown with the private street configuration:

- The private street will need to include the area for hammerhead.
- Recalculate the lot areas after that reconfiguration- 10,000 SF minimum for all lots based on zone.
- Street designed to AASHTO standards with high back curb and gutter.
- Sidewalk will be required to be installed on one side.
- Private street will be owned and maintained by an HOA.
- Notes on plat releasing city of responsibility for private street will be required.
- Protection for fire hydrant may be needed with road next to property line or moved (contact fire marshal for more information).
- Departure from base roadway standards requires approval from the Planning Commission, City Engineer, and Fire Authority.

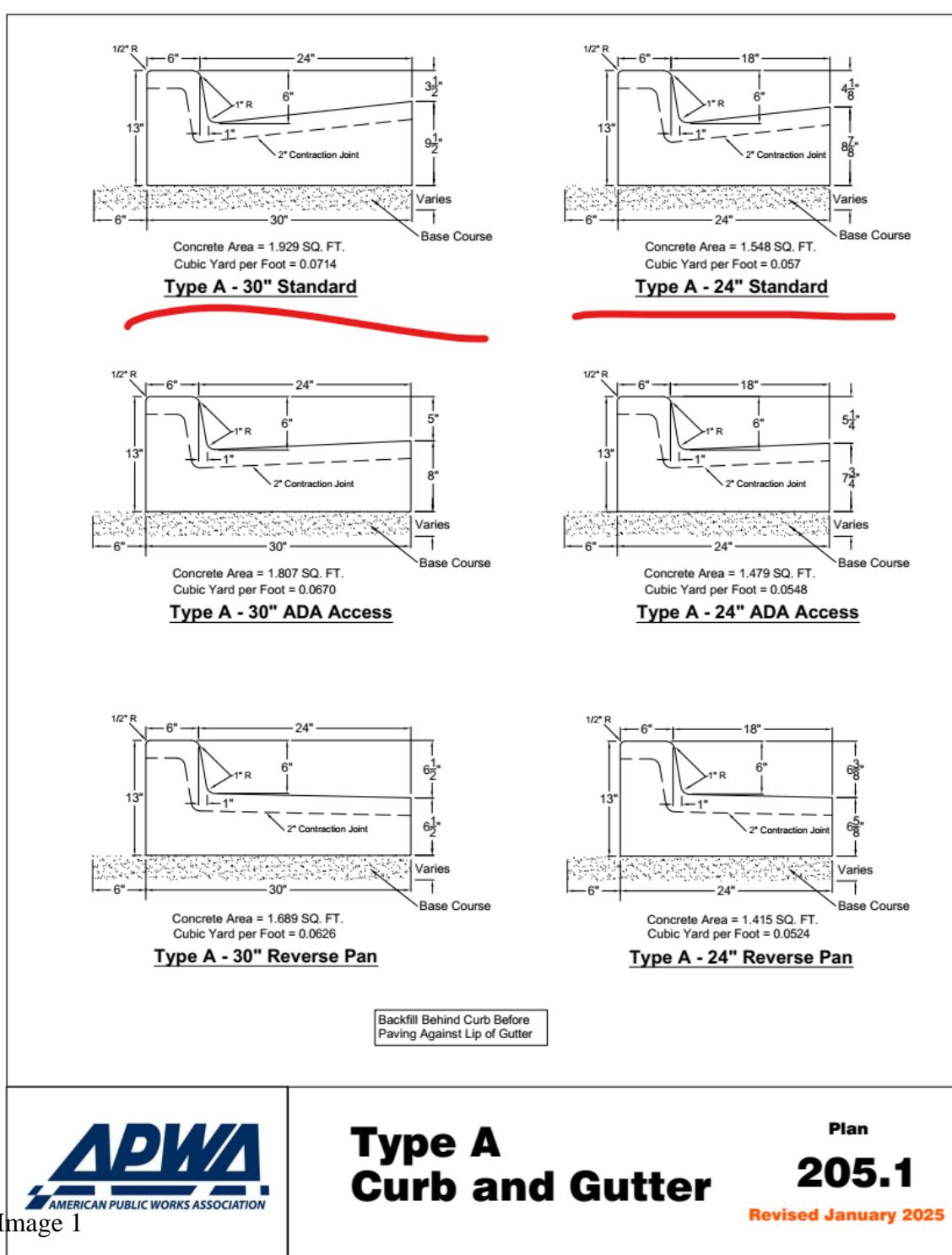
I'm headed out for the rest of the day but wanted to get these to you... I'll be back in on Monday and can answer any additional questions then.

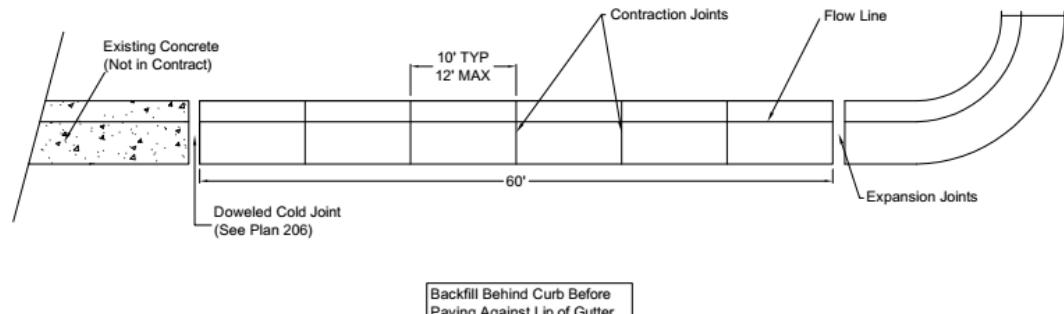
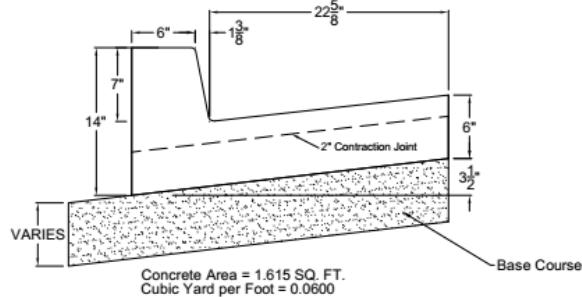
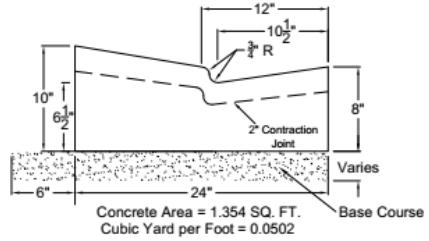
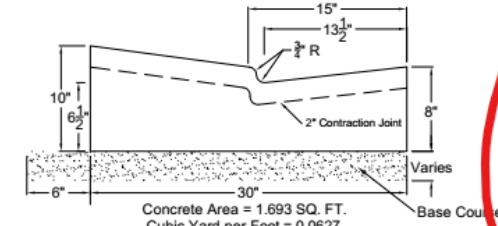
Have a nice weekend!

Terryne Bergeson | Planner I

Exhibit L

- Image 1 - Type A curb and gutter engineering standards
- Image 2 – Type H curb and gutter engineering standards
- Image 3 – Work required to make a drive in a Type A curb and gutter, shows where the cuts need to be made as well as sidewalk (not needed in Type H Curb and Gutter)



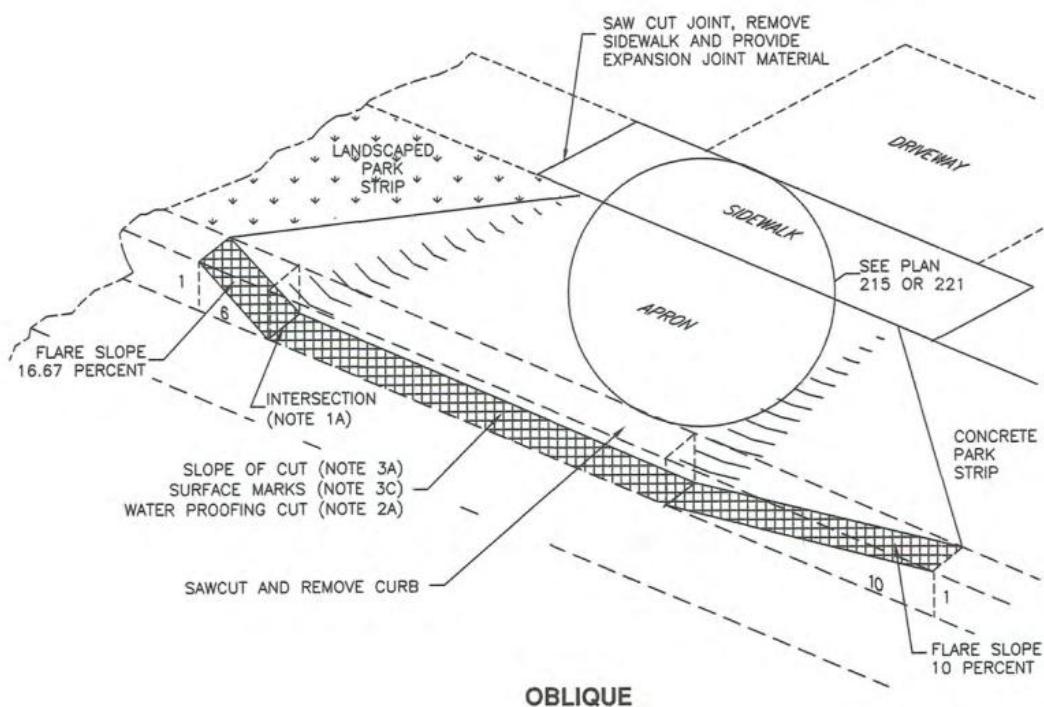


Type H Curb and Gutter

Plan
205.3
Revised January 2025

Image 2

NARRATIVE: THIS PLAN IS USED IF AN EXISTING CURB MUST BE CUT TO INSTALL A DRIVEWAY APPROACH. THE SLOPE OF THE CURB FLARE DEPENDS UPON WHETHER THE PARK STRIP IS LANDSCAPED OR IF THE PARK STRIP IS CONCRETE.



Saw-cut driveway approach

Plan
222
February 2011

Image 3

Exhibit M

- Image 1 shows the next neighborhood over with Both type A and Type H curb and Gutter
- Image 2 shows the type H curb and gutter used throughout the neighborhood (same location 1270 Morning Crest Dr, Taylorsville UT 84123)



Image 1

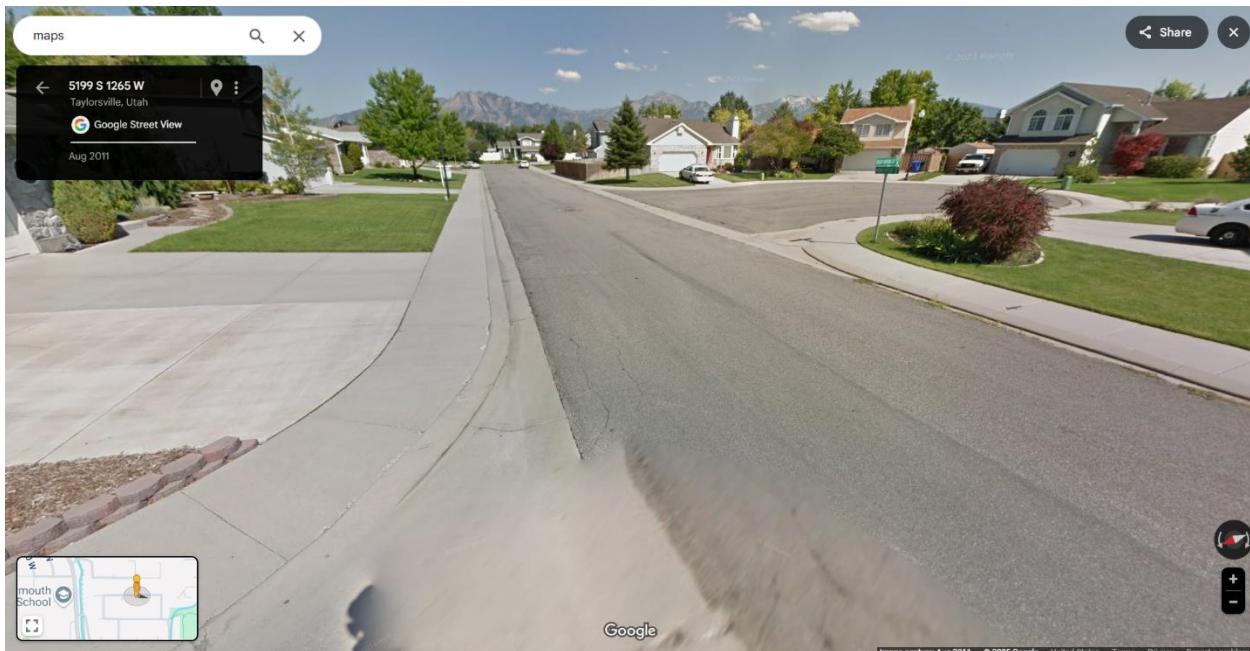


Image 2

Exhibit N

- Image 1 shows Saxton place (private street) with only a curb and gutter on the north side of 20 ft pavement.
 - No sidewalk, no curb and gutter on south side
- Image 2 shows Treasure cove (private street) with no curb and gutter or sidewalk
- Image 3 shows a private lane with no curb and gutter or sidewalk



Image 1

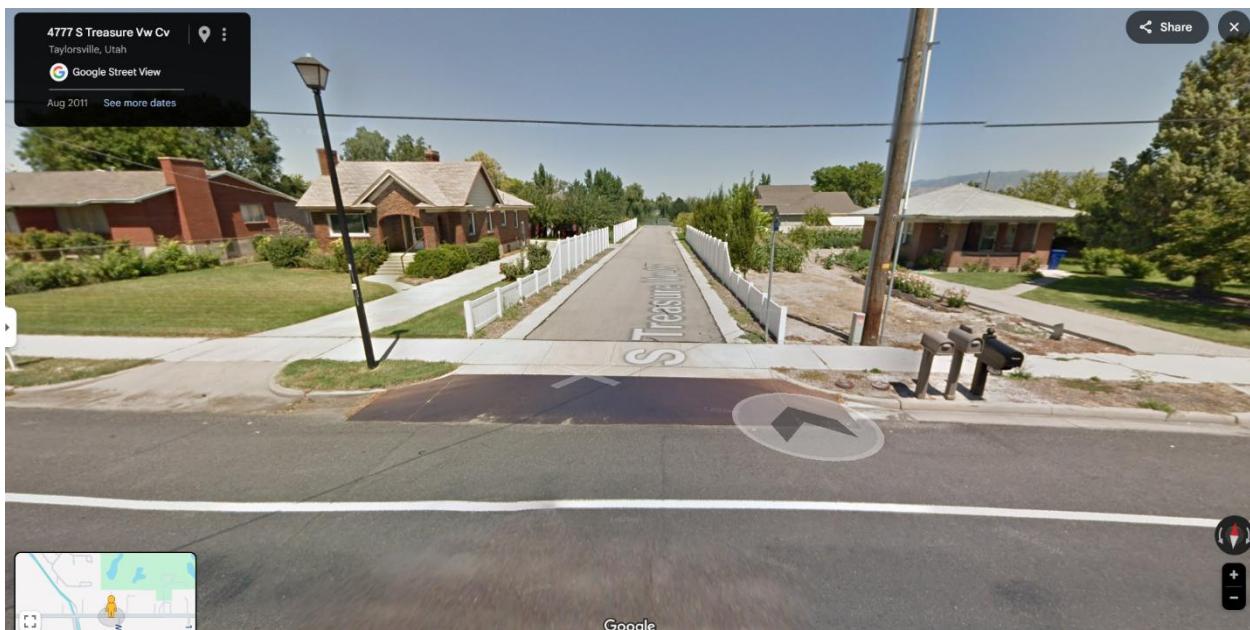


Image 2

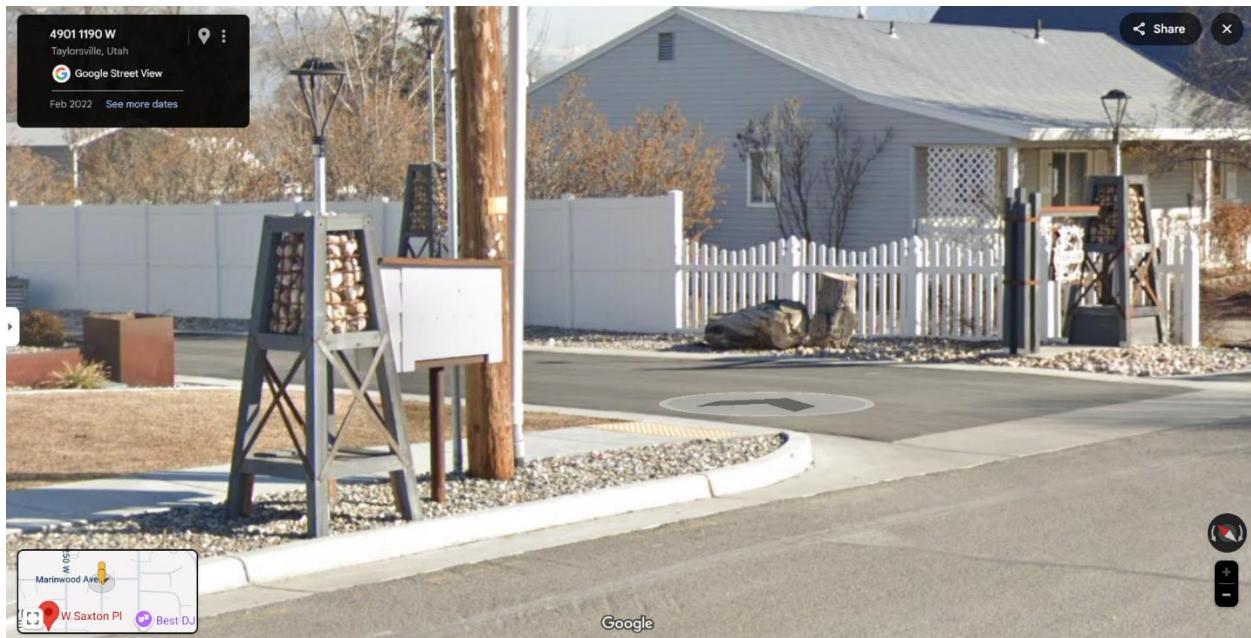


Image 3