



**CITY COUNCIL**

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IRVIN JONES  
KEVIN D. RAPP  
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**South Salt Lake City Council  
REGULAR MEETING AGENDA**

Public notice is hereby given that the **South Salt Lake City Council** will hold a Regular Meeting on **Wednesday, September 3, 2014** in the City Council Chambers, 220 East Morris Avenue, commencing at **7:00 p.m.**, or as soon thereafter as possible.

Conducting: Sharla Beverly, District 3  
Council Chair: Irvin H. Jones, Jr.  
Sergeant at Arms: Jennifer Smartt

**Opening Ceremonies**

- 1. Welcome/Introductions Sharla Beverly
- 2. Serious Moment of Reflection/Pledge of Allegiance Ryan Gold

**Approval of Minutes**

- August 27, 2014 Work Meeting
- August 27, 2014 Regular Meeting

**No Action Comments**

- 1. Scheduling City Recorder
- 2. Citizen Comments/Questions
  - a. Response to Comments/Questions  
(at discretion of conducting council member)
- 3. Mayor Comments
- 4. City Attorney Comments
- 5. City Council Comments
- 6. Information
  - a. Environmental Crimes TASK Group Alex Huggard &  
Ron Lund

**Action Items**

**Unfinished Council Business**

- 1. An Ordinance of the City of South Salt Lake City Council Paul Roberts  
Amending Chapter 9.16 of the City Code Related to Firearms

**New Council Business**

- 1. PUBLIC MEETING – Amendment to the South Salt Lake City Frank Lilly  
Land Use Code Title 17 for Adoption of the East Streetcar  
Corridor Form Based
  
- 2. PUBLIC MEETING – Amendment to the South Salt Lake Frank Lilly  
Zoning Map for Properties Adjacent to the Streetcar  
Corridor at Approximately 2200 South from the East Side of  
State Street to the West Side of 500 East from Single Family  
Residential to East Streetcar Corridor

**Motion for Closed Meeting**

**See Page Two for Continuation of Agenda**

In accordance with State Statute and Council Policy, one or more Council Members may be connected via speakerphone.

Those needing auxiliary communicative aids or other services for this meeting should contact Craig Burton at 801 483-6027, giving at least 24 hours' notice.

Each of the Deseret News and Salt Lake Tribune was advised of the Agenda of the Regular Meeting of the City Council to be held Wednesday, September 3, 2014, by fax transmittal of the foregoing agenda on Friday, August 29, 2014.

Craig D. Burton, City Recorder  
Dated this 29<sup>th</sup> day of August, 2014

  
\_\_\_\_\_  
Craig D. Burton, City Recorder

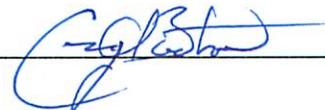
**Citizen Comments/Question Policy**

Time is made available for anyone in the audience to address the Council and/or Mayor concerning matters pertaining to City business. When a member of the audience addresses the Council and/or Mayor, he or she will come to the podium and state his or her name and address. Citizens will be asked to limit their remarks/questions to five (5) minutes each. The conducting Councilmember shall have discretion as to who will respond to a comment/question. In all cases the criteria for response will be that comments/questions must be pertinent to City business, that there are no argumentative questions and no personal attacks. Some comments/questions may have to wait for a response until the next regular council meeting. The conducting Councilmember will inform a citizen when he or she has used the allotted time. Grievances by City employees must be processed in accordance with adopted personnel rules.

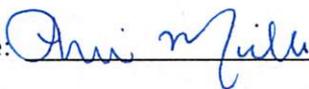
**CERTIFICATE OF COMPLIANCE WITH OPEN MEETING LAW**

The undersigned, duly qualified and acting City Recorder of the City of South Salt Lake, does hereby certify that on the 29<sup>th</sup> day of August, 2014, pursuant to Utah Code Annotated Section 52-4-202 (1953), as amended, there was posted (at least 24 hours prior to the meeting time) at the regular meeting place of the City Council of the City of South Salt, written notice of the Agenda or the Regular Meeting of the Council, a copy of which is attached and incorporated herein as Exhibit "A". The undersigned does further certify that there was mailed or delivered to all persons shown on Exhibit "B", Notice of Agenda of the above mentioned regular meeting, a copy of which is attached hereto and incorporated herein.

Name: CRAIG D. BURTON  
Title: CITY RECORDER

Signature: 

Witnessed the 29<sup>th</sup> day of August, 2014 by  
Name: SHERI MILLER

Signature: 

CITY OF SOUTH SALT LAKE  
CITY COUNCIL MEETING

COUNCIL MEETING Wednesday, September 3, 2014  
7:00 p.m.

CITY OFFICES 220 East Morris Avenue – Suite 200  
South Salt Lake, Utah 84115

PRESIDING Council Chair Irvin H. Jones, Jr.

CONDUCTING Council Member Sharla Beverly

SERIOUS MOMENT OF REFLECTION/  
PLEDGE OF ALLEGIANCE Council Member Ryan Gold

SERGEANT AT ARMS Jennifer Smartt

COUNCIL MEMBERS PRESENT:

Sharla Beverly, Ryan Gold, Irvin H. Jones, Jr., Kevin Rapp,  
Michael Rutter, Debbie Snow, and LeRoy Turner

STAFF PRESENT:

Mayor Cherie Wood  
Charee Peck, Chief of Staff  
Lyn Creswell, City Attorney  
Paul Roberts, Deputy City Attorney  
Randy Sant, Economic Development Consultant  
Mike Florence, Community Development Director  
Frank Lilly, Deputy Community Development Director  
Hayley Pratt, City Planner  
Glenn Smith, Urban Livability Director  
Sharen Hauri, Urban Design Director  
Craig Burton, City Recorder  
Paula Melgar, Deputy City Recorder

OTHERS PRESENT:

See attached list

**APPROVAL OF MINUTES**

**August 27, 2014 Work Meeting.** Council Member Rapp moved to approve these minutes.

MOTION: Kevin Rapp  
SECOND: Michael Rutter

Voice Vote:

Beverly	Aye
Gold	Aye
Jones	Aye
Rapp	Aye
Rutter	Aye
Snow	Aye
Turner	Aye

**August 27, 2014 Regular Meeting.** Council Member Turner moved to approve these minutes.

MOTION: LeRoy Turner

SECOND: Debbie Snow

Voice Vote:

Beverly	Aye
Gold	Aye
Jones	Aye
Rapp	Aye
Rutter	Aye
Snow	Aye
Turner	Aye

**NO ACTION COMMENTS**

1. **SCHEDULING.** The City Recorder informed those at the meeting of upcoming events, meetings, activities, etc.
2. **CITIZEN COMMENTS/QUESTIONS. Cortland Redding, 1802 S. 1510 W. Woods Cross.** He expressed his concerns with the transportation ordinance being proposed in Salt Lake City relating to transportation to and from the airport and the elimination of the minimum requirement of taxis. There could be a significant drop in taxi services.
3. **MAYOR COMMENTS.** Mayor Wood advised that restaurant week just concluded. Fifteen restaurants in the City participated and the event was highlighted on Channel 2 news.
4. **CITY ATTORNEY COMMENTS.** City Attorney, Lyn Creswell, asked if the Council would be willing to approve the police evidence releases on a quarterly basis on the consent agenda. They would like to try that in the future. There may be other business that could be done on the consent agenda as well. He also encouraged the Council to be involved in the legislative process.

Mr. Creswell advised the Council that there has been a lot of action taking place since the Diamond Tree fire. The City suspended their license which they appealed. The City is currently in the process of dealing with the Diamond Tree

attorney, State regulators, and others to try and come up with a business plan that all three are comfortable with. If it doesn't work out they are set to go before the Administrative Law Judge. In the meantime, Chief Morris and Deputy City Attorney, Paul Roberts, will be going before the State Fire Prevention Board to report out the new ordinance the Council passed.

- 5. **CITY COUNCIL COMMENTS.** Council Member Turner complimented everyone regarding the "Night Out Against Crime" event. It is valuable for the residents to become acquainted with the Police and Fire people.

6. **INFORMATION**

a. **Environmental Crimes TASK Group.** Ron Lund, and Sgt. Alex Huggard reviewed how the Health Department, D.A.'s office and South Salt Lake work together on environmental crimes. South Salt Lake has been a leader in environmental issues. People have a better place to work and live because of the efforts the City's staff has put forth and South Salt Lake is great to work with.

**UNFINISHED COUNCIL BUSINESS**

- 1. **An ordinance of the City of South Salt Lake City Council amending Chapter 9.16 of City code related to firearms.** Mr. Roberts advised the Council that this ordinance removes provisions in the code that are inconsistent with the State code regulating firearms. In his view there is no need to keep these inconsistent laws in place.

Mr. Creswell said he met with David Church, legal counsel for the League of Cities and Towns, and reviewed South Salt Lake's code and the State code and Mr. Church felt the City needed to come into compliance with the State code.

Council Member Snow moved to adopt this ordinance.

MOTION: Debbie Snow  
SECOND: LeRoy Turner

Roll Call Vote:

Beverly	Aye
Gold	Aye
Jones	Aye
Rapp	Aye
Rutter	Aye
Snow	Aye
Turner	Aye

**NEW COUNCIL BUSINESS**

- 1. **PUBLIC MEETING – Amendment to the South Salt Lake City Land Use Code Title 17 for adoption of the East Streetcar Corridor Form Based.** Deputy Community Development Director, Frank Lilly reviewed a presentation of the form based code with the Council. A copy is attached and incorporated by

this reference. Mr. Lilly explained that the form based code is very descriptive. Much more so than traditional zoning code. This was done because they want a code that responds to specific neighborhood conditions and will also allow for a quick and certain approval process. That's what developers want. They want to know what the rules are and how to get through the process. It is staff's hope that the standards are clear enough, concise enough, and communicate the vision well enough, that they can proceed with a development process that gives the Council the confidence that the outcome will be a positive benefit to the community.

The Council raised some concerns regarding parking requirements for multi-family, mixed use projects. The requirement in the proposal is 1.4 stalls per unit. Most of the Council wanted something higher than that.

Mr. Florence advised that the standard parking requirement for a non-TOD project in the City is 2.0 with a half a space per unit for visitor parking. He feels this is a little too high being so close to transit. The Council members suggested parking requirements ranging from at least 1.5 to 1.8 per unit.

Council Member Beverly opened the meeting for public comment at 8:20 p.m.

**Rinaldo Hunt, representing Zellerbach property.** He is in favor of the form based code. It allows them to create vision for the community on the transit line. He feels an appropriate parking requirement would be between 1.25 and 1.4. He has three developments in Salt Lake City with the parking requirement at 1.0 and they are all ninety-five percent filled. He is very sensitive to the parking issues and he wants to address the concerns.

**Desire' Skeen, 2255 S. 400 E.** She is in favor of the form based code as well. They could have developed a long time ago if they didn't care what was developed and didn't care if it sold. She understands the Council's concern on parking but hopes that they recognize her concerns as well. They have to be able to develop and if they have to put more land into parking and have no room left for a building it's going to be very difficult for them. It's not going to be successful if they're giving everyone a parking space. They need to push people into this change. They don't want to try to market and sell something no one is going to buy. She would also like to work with the Planning Commission as they go through the process. They have probably spent tens of thousands of dollars bring ideas to the Council that have not made it through because of confusion. Maybe the Council or staff didn't know what needed to happen. The form based code is specific and would eliminate the questions of what they can do.

**Mark Issac, didn't give an address.** He will be working with Mr. Hunt on the new project in the City. He thinks staff has done a beautiful job in trying to create a zoning document that is flexible enough and the elected officials, appointed staff and senior leaders can still have an effect on a per project basis. It's not a blanket document. They have a unique opportunity with transit oriented development. Parking never does create pedestrian friendly, pedestrian connectively, or a "cool" factor. The cool places aren't seas of asphalt and seas of

parking. If they increase parking on the streetcar line and they build a bigger parking structure, he's not sure that is what the City envisioned for the corridor where they are trying so hard to connect the Pratt Trail, the Parley's Trail, the Jordan River Trail, and the Bonneville Shoreline Trail. The more parking they provide the more traffic they will incur on the adjacent corridors.

**Cortland Redding, 1802 S. 1510 W., Woods Cross.** The close friends he has bike and walk to work. He thinks this would be something they would be interested in.

There were no other comments and the public meeting was closed.

Council Member Rutter moved to put this on Unfinished Business for the September 17, 2014 City Council Meeting.

MOTION: Michael Rutter

SECOND: LeRoy Turner

Voice Vote:

- Beverly Aye
- Gold Aye
- Jones Aye
- Rapp Aye
- Rutter Aye
- Snow Aye
- Turner Aye

- 2. PUBLIC MEETING – Amendment to the South Salt Lake Zoning Map for properties adjacent to the Streetcar Corridor at approximately 2200 South from the east side of State Street to the west side of 500 East from Single Family Residential to East Streetcar Corridor.** This item was covered at the same time as item number one.

Council Member Beverly opened the meeting for public comment at 8:40 p.m. There were no comments.

Council Member Gold moved to put this on Unfinished Business for the September 17, 2014 City Council Meeting.

MOTION: Ryan Gold

SECOND: Debbie Snow

Voice Vote:

- Beverly Aye
- Gold Aye
- Jones Aye
- Rapp Aye
- Rutter Aye
- Snow Aye
- Turner Aye

Council Member Snow moved to adjourn.

MOTION: Debbie Snow

SECOND: Kevin Rapp

Voice Vote:

Beverly Aye

Gold Aye

Jones Aye

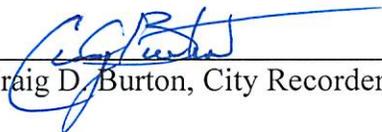
Rapp Aye

Rutter Aye

Snow Aye

Turner Aye

The meeting adjourned at 8:42 p.m.

  
\_\_\_\_\_  
Irvin H. Jones, Jr., Council Chair  
\_\_\_\_\_  
Craig D. Burton, City Recorder



ORDINANCE NO. 2014- 20

**AN ORDINANCE OF THE CITY OF SOUTH SALT LAKE CITY  
COUNCIL AMENDING CHAPTER 9.16 OF CITY CODE RELATED TO  
FIREARMS.**

WHEREAS: the City of South Salt Lake City Council has authority to enact ordinances necessary to advance the public health and welfare; and

WHEREAS: many provisions of Chapter 9.16 are inconsistent with state codes regarding firearms; and

WHEREAS: the City wishes to bring its ordinances into conformity with state law on the issue; and

WHEREAS: the discharge of firearms, except when necessary for the defense of life and property, in the exercise of a law enforcement officer's duties, or at an appropriately located shooting range, is inconsistent with the health and welfare of our community,

BE IT ORDAINED by the City Council of the City of South Salt Lake that Chapter 9.16 of the City of South Salt Lake code is amended as follows:

SECTION 12.52.0890 of the City of South Salt Lake code is amended, as follows:

**Chapter 16 Discharge of Firearms**

**Article I. Firearm Purchase Review Period**

~~9.16.010 Purpose of article.~~

~~9.16.020 Definitions.~~

~~9.16.030 Five-day review period.~~

~~9.16.040 Possession of firearms by and sale of firearms to minors.~~

~~9.16.050 Criminal penalties.~~

~~9.16.060 Civil penalties.~~

**9.16.010 Purpose of article.**

The purpose of this article is to provide for a waiting period before the purchase of a handgun, and for the establishment of a background check requirement for firearms dealers before the transfer of any firearm, and to address the use of firearms by minors.

**9.16.020 Definitions.**

For the purposes of this chapter, the words set out in this section shall have the following meanings:

"Ammunition" means unexpended bullets or shells for any firearm.

"Dangerous weapon" means any item that in the manner of its use or intended use is capable of causing death or serious bodily injury. The following factors shall be used in determining whether an item, object or thing not commonly known as a dangerous weapon is a dangerous weapon:

1. The character of the instrument, object or thing;
2. The character of the wound produced, if any; and
3. The manner in which the instrument, object or thing was used.

"Firearms" means pistols, revolvers, rifles, shotguns, sawed off shotguns or sawed off rifles or any other device that could be used as a weapon by a projectile which is expelled by the force of burning or exploding substances, including, but not limited to, gunpowder, smokeless powder, black powder and rocket fuel (solid or liquid).

"Handgun" means a pistol, revolver or other firearm of any description, loaded or unloaded, from which any shot, bullet or other missile can be discharged, the length of the barrel of which, not including any revolving, detachable or magazine breech, does not exceed twelve (12) inches.

"Hunting" means:

~~1. For species with a specific season, the pursuit or shooting of such a species, during a designated hunting season for that species, in an area designated for hunting the species and with a valid hunting license and firearm permitted for hunting that species.~~

~~2. For species for which there is no specified season, the pursuit or shooting of any nonseasoned species for which hunting is allowed by law in an area where such hunting is allowed, with a valid license to hunt and in possession of a legal weapon for hunting that species.~~

~~3. Hunting includes the transportation of firearms by animal or motor vehicles in areas where hunting is allowed by law.~~

~~"Licensed dealer" means any person or business licensed by any city, state, county or federal agency for the sale of firearms.~~

~~"Licensed shooting range" means:~~

~~1. Lawfully operated target concessions at amusement parks, piers and similar locations, provided that the firearms to be used are firmly chained or affixed to the counters;~~

~~2. Commercial trap or skeet fields or shooting ranges, during regular business hours; or~~

~~3. Other shooting ranges allowed by law.~~

~~"Loaded weapon" means:~~

~~1. For the purpose of this article, any pistol, revolver, shotgun, rifle or other weapon described in this section shall be deemed to be loaded when there is an unexpended cartridge, shell or projectile in the firing position.~~

~~2. Pistols and revolvers shall also be deemed to be loaded when an unexpended cartridge, shell or projectile is in a position whereby the manual operation of any mechanism once would cause the unexpended cartridge, shell or projectile to be fired.~~

~~3. A muzzle loading firearm shall be deemed to be loaded when it is capped or primed and has a powder charge and ball or shot in the barrel or cylinders.~~

~~"Multiburst trigger activator" means a device designed or redesigned to be attached to a semi-automatic firearm which allows the firearm to discharge two or more shots in a burst by activating the device.~~

~~"Police department" means the city of South Salt Lake police department.~~

~~"Private firearm sale" means the sale or other transfer of a firearm for value, other than a sale to a licensed dealer.~~

~~"Sawed-off shotgun" or "sawed-off rifle" means a shotgun having a barrel or barrels of fewer than eighteen (18) inches in length, or in the case of a rifle, having a barrel or~~

barrels of fewer than sixteen (16) inches in length, or any dangerous weapon made from a rifle or shotgun by alteration, modification or otherwise, if the weapon as modified has an overall length of fewer than twenty six (26) inches.

"Sport shooting" means the firing of firearms at targets in an area where the discharge of firearms is legal.

**9.16.030 Five-day review period.**

**A. Review Period for Firearm Purchase.**

1. After January 1, 1994, it is unlawful for a firearm to be delivered to a purchaser in a private firearm sale less than five working days after the purchaser has signed, dated and timed a "Notice of Intent to Take Delivery," in substantially the following form:

"I, purchaser's name, address, date of birth, and driver's license or identification card number intend to take delivery of a firearm. I have given (Seller) notice of this intent to take delivery at \_\_\_\_\_ o'clock \_\_\_\_\_ .m. on \_\_\_\_\_ the \_\_\_\_\_ day of \_\_\_\_\_, 19\_\_\_\_\_, a firearm described as (make, model, and serial #) \_\_\_\_\_, and I acknowledge that I cannot lawfully take delivery or possession of the firearm until after \_\_\_\_\_ o'clock \_\_\_\_\_ .m. on \_\_\_\_\_ the \_\_\_\_\_ day of \_\_\_\_\_, 19\_\_\_\_\_."

2. The seller and buyer involved in a private firearm sale of a firearm may agree between themselves when the purchase price of the firearm shall be paid.
3. Within twenty four (24) hours of the execution of a notice of intent, the proposed firearm seller shall deliver a true and accurate copy of the notice of intent to the police department for a background check on the proposed purchaser.
4. If the seller fails to deliver the notice of intent to the police department within twenty four (24) hours, the running of the review period shall be suspended until the notice of intent is delivered to the police department. The review period shall resume upon delivery and shall expire four working days after delivery of the notice of intent to the police department.
5. As soon as is practical but not later than five working days after receipt of the notice of intent, the police department shall inform the proposed seller whether the proposed buyer is prohibited by law from the purchase of the firearm.
6. It is unlawful to sell the firearm to the buyer if the police department has informed the seller that the buyer is prohibited by law from buying the firearm.
7. If upon expiration of the review period the police department has not informed the proposed seller that the sale is prohibited, the sale may be completed.

~~8. It is unlawful for the seller to intentionally become unavailable or to otherwise avoid contact with the police department for the purpose of avoiding notification during the review period.~~

~~9. The police department is authorized to conduct investigations of criminal history and identification for the purpose of ensuring compliance with this section.~~

~~B. Privacy Requirements.~~

~~1. All information, records, reports or similar documentation which is obtained by the police department pursuant to this section shall be destroyed within forty-eight (48) hours after the proposed buyer is informed the purchase may take place.~~

~~2. Said information, records, reports or documentation shall not be destroyed if the proposed buyer is prohibited by law from purchasing the firearm.~~

~~C. Scope. This section applies to any private firearm sale or other transfer of a firearm for value other than a sale to a licensed dealer.~~

~~D. Penalty. A person who violates this section is guilty of a Class B misdemeanor. The civil penalty provisions of Section 9.32.160 also apply to violations of this section.~~

~~**9.16.040 Possession of firearms by and sale of firearms to minors.**~~

~~A. Scope of Sale Restrictions. The firearm sale provisions of this section apply to any private firearm sale within the city of South Salt Lake whether or not from a licensed dealer.~~

~~B. Identification Required Before Sale. It is unlawful for any firearm to be sold until the seller has seen a presently valid form of identification and the seller is reasonably satisfied that the identification card is genuine, is that of the purchaser and shows the purchaser's age to be that allowed for sale of the firearm to be purchased. The identification card shall include a photograph and the name, address and date of birth of the purchaser, such as a drivers' license or similar state issued identification.~~

~~C. Selling Firearms and Ammunition to Persons Under Eighteen (18) Prohibited. It is unlawful for a firearm or ammunition to be sold to any person under the age of eighteen (18) years unless the minor is accompanied by a parent or guardian.~~

~~D. Sale of Multiburst Trigger Activators Prohibited. It is unlawful to manufacture, distribute, give, use or sell a multiburst trigger activator.~~

~~E. Possession.~~

~~1. Exceptions. The provisions of this section shall not apply to any of the following:~~

~~a. United States Marshals while engaged in the performance of their official duties;~~

~~b. Federal official required to carry firearms while engaged in the performance of their official duties;~~

~~c. Law enforcement officials of the state of Utah or other jurisdictions authorized by law to carry firearms while engaged in the performance of their official duties or as otherwise permitted by law;~~

~~d. Common carriers while engaged in the regular and ordinary transport of firearms as merchandise;~~

~~e. Persons authorized by the United States or by the state of Utah to carry concealed weapons.~~

~~F. Sawed-off Rifles and Sawed-off Shotguns Prohibited. Unless otherwise provided by law, it is unlawful for any person eighteen (18) years of age or older to possess, own, use or transport a sawed-off rifle or sawed-off shotgun within the city of South Salt Lake.~~

~~G. Prohibition of Possession of Certain Weapons by Minors.~~

~~Except as specified in subsections E and H of this section:~~

~~1. A minor under eighteen (18) years of age may not possess a handgun.~~

~~2. Except as provided by federal law, a minor under eighteen (18) years of age may not possess the following:~~

~~a. Rifles;~~

~~b. Shotguns;~~

~~c. Ammunition;~~

~~d. Any other firearm not specifically listed in subsection G(2).~~

~~3. Dangerous weapons other than firearms or ammunition already prohibited in subsection (G)(1) and (2) above, may not be possessed by a minor under eighteen (18) years of age unless the minor:~~

~~a. Has the written permission of the minor's parent or guardian to have the weapon and is accompanied by a person over the age of twenty-one (21) while he has the weapon in his possession; or~~

~~b. Is accompanied by a parent or guardian while the minor is in possession of the weapon.~~

~~H. Exceptions to Prohibitions. The provisions of subsection G of this section shall not apply to the following:~~

~~1. Patrons firing at lawfully operated target concessions at amusement parks, piers and similar locations, provided that the firearms to be used are firmly chained or affixed to the counters;~~

~~2. Any person in attendance at a hunter safety course or a firearms safety course;~~

~~3. Any persons engaging in practice or any other lawful use of a firearm at an established, licensed range;~~

~~4. Any person engaging in a lawfully organized competition involving the use of a firearm;~~

~~5. Any minor under eighteen (18) years of age is not considered to be in possession of a handgun, shotgun, rifle or ammunition if the minor is an employee and on the premises where the minor is employed and the employer lawfully has a handgun, shotgun, rifle or ammunition on the premises;~~

~~6. Any resident or nonresident hunters with a valid hunting license traveling to or from a lawful engagement in hunting;~~

~~7. Any person traveling to or from any activity described in subsection (H)(2), (3), (4) or (5) of this section with an unloaded firearm in his possession.~~

~~I. Parent or Guardian Knowing of Minor's Possession of Dangerous Weapon. A parent or guardian of a minor who knows that the minor is in possession of a dangerous weapon in violation of this section and fails to make reasonable efforts to remove the dangerous weapon from the minor's possession or, if the weapon cannot be reasonably removed, fails to promptly notify the police department, is guilty of a Class B misdemeanor.~~

~~J. Possession Prohibited Under the Influence of Drugs or Alcohol.~~

~~1. It is unlawful for any person under the influence of any drug or other substance prohibited by statute or having a blood alcohol content in excess of .04 percent or greater by weight to be in physical possession of a firearm or to transport a firearm in any vehicle in such a manner that the person under the influence has access to the firearm.~~

~~2. The standards for chemical breath analysis as provided by Section 41-6-44.3, U.C.A., and the provisions for the admissibility of chemical test results as provided by Section 41-6-44.5, U.C.A., apply to determination and proof of blood alcohol content under this section.~~

~~3. Percent by weight of alcohol in the blood is based upon grams of alcohol per one hundred cubic centimeters of blood.~~

~~4. The fact that a person charged with violating this subsection is on or has been legally entitled to use alcohol or a drug is not a defense to any charge of violating this subsection.~~

~~5. Evidence of a defendant's blood or breath alcohol content or drug content is admissible except when prohibited by rules of evidence or the constitution.~~

~~6. For purposes of this subsection, "motor vehicle" means any self-propelled vehicle and includes any automobile, truck, van, motorcycle, train, engine, watercraft or aircraft.~~

~~K. Knowingly Allowing or Assisting a Violation is Prohibited.~~

~~1. It is unlawful for any person to knowingly provide a firearm to any person where the possession of said firearm would be in violation of this section.~~

~~2. It is unlawful for any person to knowingly allow any person to possess or transport a firearm in violation of this section.~~

### **~~9.16.050 Criminal penalties.~~**

~~A violation of this article is a Class B misdemeanor. Pursuant to state law, second or subsequent violation may be prosecuted as higher offenses.~~

### **~~9.16.060 Civil penalties.~~**

~~A. Applicability. The civil penalty provisions for this article apply to any person selling, possession, providing or knowingly allowing any person to possess or transport a firearm in~~

~~violation of the provisions of this article whether or not that person has been charged or convicted with a criminal offense under this article.~~

~~B. Business License Suspension or Revocation.~~

~~1. The city business license of any business selling firearms shall be revoked for not less than thirty (30) days or revoked for violation of any provision of this article.~~

~~2. The city business license of any business selling firearms shall be revoked for not less than one year for any second violation of this article which has occurred within twelve (12) months of another violation.~~

~~3. All other city authorizations required for a business shall be terminated for any business selling any firearm during a period of suspension or revocation. No firearms may be sold by a business after the terminations provided in this article except to a licensed firearm dealer.~~

~~4. If a business is licensed and engages in sales or activities other than the selling of firearms, the suspension and/or revocation referred to in this section shall apply only to the firearms and ammunition portion of the business.~~

~~C. Forfeiture.~~

~~1. Any firearm which a police or other law enforcement officer has probable cause to believe is being sold, transferred, possessed or used in violation of the provisions of this article may be seized. Upon a finding by a court that the firearm was possessed or used in violation of the provisions of this article may be seized. Upon a finding by a court that the firearm was possessed in violation of this article, the firearm shall be:~~

~~a. Confiscated by the court;~~

~~b. Conveyed to the governmental entity seizing the firearm; and~~

~~c. Upon the expiration of any time for appeal, sold at a public auction or otherwise disposed of by the seizing governmental entity; provided that sawed-off shotguns, sawed-off rifles and any handgun not usable by the seizing agency shall be destroyed.~~

~~2. The forfeiture provisions set forth in this subsection apply to any motor vehicle used in violation of this article where such use is with the knowledge of the registered owner of such vehicle.~~

~~3. The office of the city attorney shall promulgate rules and procedures pertaining to forfeiture of vehicles as provided for in this article.~~

**Article II. Discharge of Firearms**

**9.16.010 Definitions**

**9.16.070-020 Prohibited within city limits—Exceptions.**

**9.16.010 Definitions**

**“Firearm” means a pistol, revolver, shotgun, short-barreled shotgun, rifle or short-barreled rifle, or a device that could be used as a dangerous weapon from which is expelled a projectile by action of an explosion, whether produced by gunpowder, black powder or other fuels.**

**“Sworn law enforcement officer” means a United States marshal, a federal official authorized to carry a firearm, or a peace officer of this or any other jurisdiction.**

**9.16.070-020 Discharge of Firearms Prohibited within city limits—Exceptions.**

A. In cases not amounting to a violation of state law, it is unlawful to discharge any firearm within the city limits of the city of South Salt Lake, except:

(1) in self-defense, or defense of personal property as defined in Utah Code; Annotated Sections 76-2-401 through 76-2-407, or

(2) in the case of any ~~civil~~ sworn law enforcement officer, in the performance of that officer's his duty; or

(3) at a duly licensed shooting gallery or range.

B. A violation of this section is a Class B misdemeanor.

This ordinance will take effect upon execution by the Mayor or after fifteen days from transmission to the office of Mayor if neither approved nor disapproved by the Mayor.

DATED this 3<sup>RD</sup> day of SEPTEMBER, 2014.

BY THE CITY COUNCIL:

[Signature]  
Irvin Jones, Council Chair

ATTEST:

[Signature]  
Craig D. Burton, City Recorder

City Council Vote as Recorded:

Beverly	<u>Aye</u>
Gold	<u>Aye</u>
Jones	<u>Aye</u>
Rapp	<u>Aye</u>
Rutter	<u>Aye</u>
Snow	<u>Aye</u>
Turner	<u>Aye</u>

Transmitted to the Mayor's office on this 4<sup>th</sup> day of SEPTEMBER, 2014.

[Signature]  
Craig D. Burton, City Recorder

MAYOR'S ACTION: Approve

Dated this 4 day of September, 2014.

[Signature]  
Cherie Wood, Mayor

ATTEST:

[Signature]  
Craig D. Burton, City Recorder



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 P.O. BOX 704005  
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CITY OF SOUTH SALT LAKE, ATTN: AMY DALLEY 220 E MORRIS AVE  SOUTH SALT LAKE CITY UT 84115	9001382697	9/8/2014

ACCOUNT NAME	
CITY OF SOUTH SALT LAKE,	
TELEPHONE	ADORDER# / INVOICE NUMBER
8014836000	0000981363 /
SCHEDULE	
Start 09/07/2014	End 09/07/2014
CUST. REF. NO.	
Ord. 2014-20	
CAPTION	
CITY OF SOUTH SALT LAKE ORDINANCE NO. 2014-20 An Ordinance of the City of South S:	
SIZE	
18 Lines	1.00 COLUMN
TIMES	RATE
2	
MISC. CHARGES	AD CHARGES
TOTAL COST	
27.50	

**CITY OF SOUTH SALT LAKE  
 ORDINANCE NO. 2014-20**  
 An Ordinance of the City of South Salt Lake City Council amending Chapter 9.16 of City Code Related to Firearms.  
 The complete Ordinance 2014-19 is available in the office of the City Recorder, 220 East Morris Avenue, South Salt Lake, Utah.  
 Published: Sunday, September 7, 2014  
 981363 UPAXLP

AFFIDAVIT OF PUBLICATION

AS NEWSPAPER AGENCY COMPANY, LLC dba MEDIAONE OF UTAH LEGALBOOKER, I CERTIFY THAT THE ATTACHED ADVERTISEMENT OF CITY OF SOUTH SALT LAKE ORDINANCE NO. 2014-20 An Ordinance of the City of South Salt Lake City Council amending Chapter 9.16 of City Code Related to Firearms. FOR CITY OF SOUTH SALT LAKE, WAS PUBLISHED BY THE NEWSPAPER AGENCY COMPANY, LLC dba MEDIAONE OF UTAH, AGENT FOR THE SALT LAKE TRIBUNE AND DESERET NEWS, DAILY NEWSPAPERS PRINTED IN THE ENGLISH LANGUAGE WITH GENERAL CIRCULATION IN UTAH, AND PUBLISHED IN SALT LAKE CITY, SALT LAKE COUNTY IN THE STATE OF UTAH. NOTICE IS ALSO POSTED ON UTAHLEGALS.COM ON THE SAME DAY AS THE FIRST NEWSPAPER PUBLICATION DATE AND REMAINS ON UTAHLEGALS.COM INDEFINATELY. COMPLIES WITH UTAH DIGITAL SIGNATURE ACT UTAH CODE 46-2-101; 46-3-104.

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DATE 9/8/2014



  
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## CITY COUNCIL STAFF REPORT

**MEETING DATE:** 3 September 2014  
**APPLICANT:** South Salt Lake City  
**PROJECT NUMBERS:** T-14-001 and Z-14-004

**REQUESTS:** **T-14-001: A recommendation to the City Council to establish the East Streetcar Corridor Land Use District and to adopt the East Streetcar Corridor Form-Based Code as an amendment to the South Salt Lake City Land Use Code.**

**Z-14-004: A recommendation to the City Council to amend the zoning map for properties adjacent to the streetcar corridor at approximately 2200 South from the east side of State Street to the west side of 500 East from Commercial Corridor and Single Family Residential to East Streetcar Corridor.**

**PREPARED BY:** Francis Xavier Lilly, AICP

**SYNOPSIS:** South Salt Lake City is petitioning the City Council to amend Title 17 of the South Salt Lake Municipal Code to establish a new land use district and to adopt the East Streetcar Corridor Form-Based Code to facilitate new development along the streetcar corridor, while preserving the values and integrity of surrounding single-family neighborhoods. A form-based code has been developed for properties along the corridor that addresses building forms, height transitions, street and open space standards, parking requirements, and landscape standards.

South Salt Lake City is also petitioning the City Council to amend the zoning map for properties along the Streetcar Corridor from State Street to 500 East from Commercial Corridor and Single Family Residential to East Streetcar Corridor.

### **SUMMARY:**

- The City has undertaken a comprehensive outreach program, spanning two years and over a dozen meetings involving residents, property owners, developers, financing partners, and other stakeholders.
- The City Council adopted the East Streetcar Master Plan on May 28, 2014. The Master Plan included a vision and design principles that responded to neighborhood concerns, and that informed the development of the form-based code.
- The form-based code is intended to be a zoning ordinance that responds to the specific characteristics of the East Streetcar Corridor. The code includes specific requirements for height transitions, open space and street requirements, and parking standards.
- The form-based code establishes a design review committee to review plans and an expedited subdivision approval process to streamline development approvals.

- Adoption of the form-based code is a crucial step in facilitating appropriate redevelopment along the corridor to maximize the S-Line Corridor's potential as a valuable community amenity.
- The Planning Commission gave unanimous recommendations of approval for both the zone change and the adoption of the form-based code.
- The attached ordinance includes proposed amendments to Chapters 17.11, 17.13, and 17.47 of the South Salt Lake Municipal Code, adopting language establishing the East Streetcar Corridor land use district, and adding definitions pertaining to the form-based code.



## PLANNING COMMISSION STAFF REPORT

### **Synopsis:**

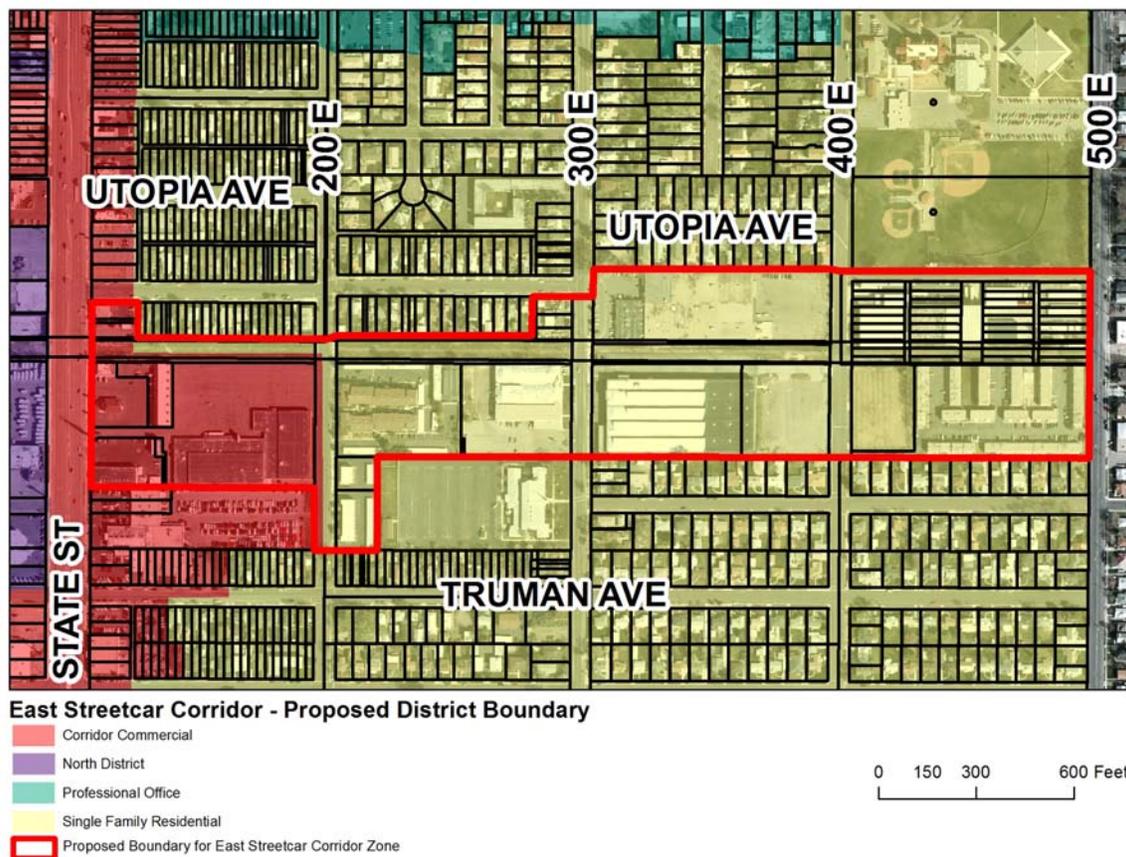
South Salt Lake City is petitioning the City Council to amend Title 17 of the South Salt Lake Municipal Code to establish a new land use district and to adopt the East Streetcar Corridor Form-Based Code to facilitate new development along the streetcar corridor, while preserving the values and integrity of surrounding single-family neighborhoods. A form-based code has been developed for properties along the corridor that addresses building forms, height transitions, street and open space standards, parking requirements, and landscape standards.

The form-based code is longer and more descriptive than traditional zoning ordinances, placing greater emphasis on form rather than uses, although land use regulations remain an important part of the code. The code identifies street types, building types, open space types, and incentives that developers can apply toward their projects. The form-based code includes a vision and guiding principles on page 2 of the form-based code, included as Attachment 1 to this staff report.

South Salt Lake City is also petitioning the City Council to amend the zoning map for properties along the Streetcar Corridor from State Street to 500 East from Commercial Corridor and Single Family Residential to East Streetcar Corridor.

The South Salt Lake City Planning Commission made a unanimous recommendation for both the rezone and the adoption of the form-based code in their meeting on August 21, 2014. The minutes of that meeting are included as attachment 4 to this document.

Figure 1: Proposed Zoning Boundary



### **General Plan Considerations:**

By creating a provision for approving a master plan as part of a land-use district designation, this ordinance could further several goals of the General Plan, depending on where the district is located and what uses are proposed. Relevant General Plan goals include:

**Goal LU-1.** Regulate land uses based on compatibility with surrounding uses, residential areas and economic feasibility. Maintain residential, business and industrial areas that are vibrant and where the health and safety of all are protected.

**Goal LU-4.** Capitalize on South Salt Lake's vast transit options by creating specific transit-oriented development land use plans, using current best practices.

**Goal LU-5.** Preserve existing historically significant buildings and landmarks in South Salt Lake City.

**Goal LU-7.** Protect low-density residential areas adjacent to businesses districts.

**Goal LU-8.** Accommodate higher density housing in appropriate areas.

**Goal LU-9.** Create an individual identity for each residential neighborhood.

**Goal LU-11.** Make business and commercial areas more pedestrian and bicycle friendly.

**Goal LU-13.** Redevelopment of property should be actively pursued and incentives, when deemed appropriate, should be offered to further this goal.

**Goal TE-3.** Develop walking and bicycling as viable transportation choices for residents of all abilities, age, and income levels.

**Goal HE-2.** Improve the overall quality of existing housing stock.

**Goal ES-3.** Enhance the quality of life in South Salt Lake by improving the community's appearance, safety, education, positive outlook, gathering places and positive momentum.

### **East Streetcar Master Plan Considerations**

The East Streetcar Master Plan includes the following goals:

- Protect neighboring homes and property values
- New development will be context sensitive through height transitions, density blending, and compatible design.
- Improve neighborhood safety and appeal.
- Revitalize or replace neglected properties.
- Increase home-ownership of existing homes.
- Build high-quality new housing for a wide spectrum of residents (age, income, culture).
- Create a quality urban neighborhood that appeals to new residents.
- Maximize the opportunity to locate housing and jobs adjacent to transit, bike, and pedestrian ways.
- Support building a true downtown South Salt Lake to the west.

The East Streetcar Master Plan includes a list of design principles of the form-based code. These can be found in Attachment 3 of this staff report.

### **Ordinance Requirements and Considerations:**

#### **Land Use Amendment Procedure:**

##### **17.01.150 Amendments.**

- A. Amendments to the Zoning Map. Amendments to the zoning map shall be made in compliance with the provisions of this chapter and Utah State Code Annotated.
- B. Initiating Amendments and Corrections. Any citizen, property owner, the planning commission, the city council, or the community development director may initiate proposals for change or amendment of the South Salt Lake General Plan or any chapter or regulation of this code or the official city zoning map.
- C. Application. Any person seeking an amendment to the land use code or zoning map shall submit an application with the community development department indicating the change desired and how the change will further promote the goals and objectives of the general plan. Application will be processed and noticed to

the public, when applicable, according to this code and Utah Code Annotated. Applications will be processed in an efficient manner in order to not cause the applicant unwarranted delays.

D. Planning Commission. The planning commission shall:

1. Fulfill all duties outlined in state statute that are to be performed by the planning commission.
2. Prepare and recommend to the city council the general plan or amendments to the general plan, any proposed land use ordinance or ordinances and a zoning map, and amendments thereto that represent the planning commission's recommendation for regulating the use and development of land within all or any part of the area of the municipality.
3. Hold a public hearing on a proposed land use ordinance or zoning map amendment.

E. City Council. The city council:

1. May adopt or amend:
  - a. The number, shape, boundaries, or area of any zoning district on the official city zoning map;
  - b. Any regulation of or within the zoning district; or
  - c. Any provision of the development code.
2. The municipal legislative body shall consider each proposed land use ordinance and zoning map recommended to it by the planning commission, and, after providing notice as indicated in this code and Utah State Code Annotated and holding a public meeting, the legislative body may adopt or reject the ordinance or map either as proposed by the planning commission or after making any revision the municipal legislative body considers appropriate. The city council is not bound by any recommendation from the planning commission.

**17.05.110 Purpose of the land use code.**

- A. The purposes of this title are to provide for the health, safety, and welfare, and promote the prosperity, improve the morals, peace and good order, comfort, convenience, and aesthetics of the city of South Salt Lake and its present and future inhabitants and businesses, to protect the tax base, to secure economy in governmental expenditures, to foster the city's agricultural and other industries, to protect both urban and nonurban development, to protect and ensure access to sunlight for solar energy devices, to provide fundamental fairness in land use regulation, and to protect property values.
- B. To accomplish the purposes of this chapter, the city may enact all ordinances, resolutions, and rules and may enter into other forms of land use controls and

development agreements that it considers necessary or appropriate for the use and development of land within the city, including ordinances, resolutions, rules, restrictive covenants, easements, and development agreements governing uses, density, open spaces, structures, buildings, energy efficiency, light and air, air quality, transportation and public or alternative transportation, infrastructure, street and building orientation and width requirements, public facilities, fundamental fairness in land use regulation, considerations of surrounding land uses and the balance of the foregoing purposes with a landowner's private property interests, height and location of vegetation, trees, and landscaping, unless expressly prohibited by law.

- C. This code is adopted to implement the city of South Salt Lake's general plan and to promote public health, safety, convenience, aesthetics, welfare, stable land values, efficient land use, sustainable land use and building practices, crime prevention, transportation and accessibility, and efficiency in land use review and administration.
- D. The land use code is also established to facilitate orderly growth and development in the City of South Salt Lake and to enhance the lives of the citizens of the city.
- E. The land use code intends to establish a fair and efficient process for development and land use applications, as accomplished through the delegation of powers among city officials and a transparent review process.

#### **17.15.010 Commercial and Residential Land Uses - Purpose and applicability.**

- A. Purpose. The land use matrix and standards are intended to:
  1. Allow a mixture of complimentary land uses that may include retail, offices, commercial services, civic uses and industrial uses to create economic and social vitality;
  2. Encourage a diversity of businesses to expand and locate to the city;
  3. Continue to support the business areas as regional employment centers;
  4. Develop districts that encourage walking as an alternative to driving.

#### **Components of the East Streetcar Form-Based Code**

**Street Types:** The ordinance establishes standards, both for existing streets and for new roadways in the East Streetcar Corridor zone. The standards are intended to improve access via complete streets, and to create accents and visual interest as one approaches the S-Line Corridor. The ordinance proposes a new street type to facilitate vehicular, pedestrian, and bike access to new development along the corridor. The proposed lane is a 32-foot right-of-way that is a shared space for all users.

**Subdistricts:** Recognizing that conditions change along the corridor, the code identifies three subdistricts:

- State Street Gateway – State Street to 200 East

- North Haven – 200 East to 400 East
- 500 East Gateway – 400 East to 500 East.

Specific use and building type standards vary by subdistrict.

**Uses:** The code identifies uses in a series of broad categories, listing them as permitted or conditional by subdistrict. Uses not listed as permitted or conditional are prohibited. Staff calibrated the list of available uses to ensure compatibility with the surrounding neighborhood. Recognizing that existing buildings along the corridor could be adaptively reused, allowances were made to allow retail and craftsman industrial uses in existing buildings, subject to review by the land use authority. Generally, neighborhood retail uses cannot exceed 12,00 square feet, but such uses can be aggregated into larger retail developments along the corridor.

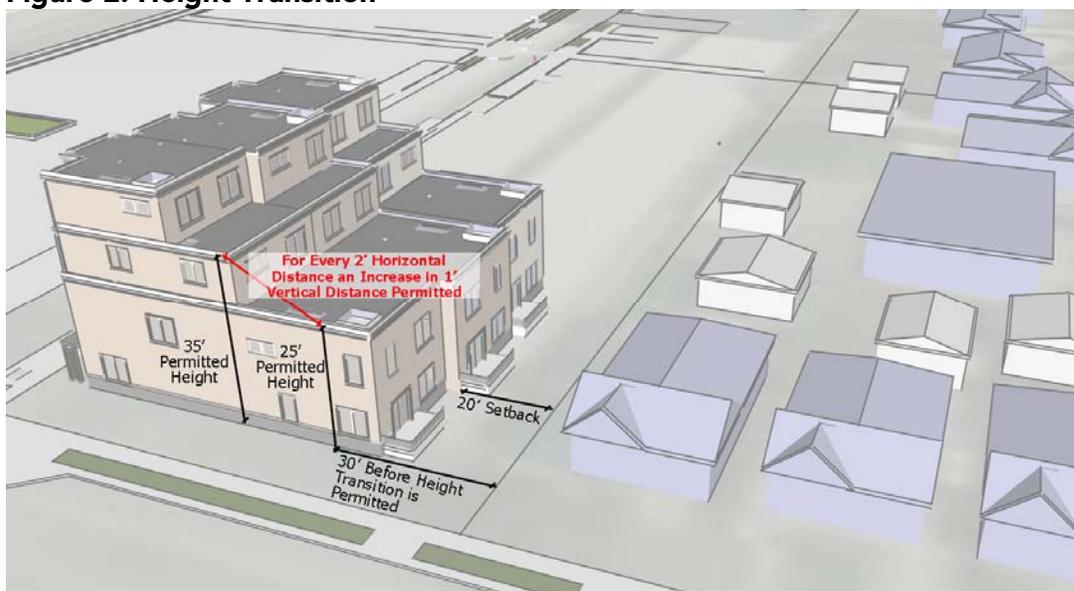
**Building Types:** As with the City's residential design standards, this code contemplates allowable building types by subdistrict. Recognizing the mixed-use nature of the corridor and the goals of the Master Plan, the following building types are included in the code:

- Storefront building
- Urban Style apartment
- Townhome
- Mansion Style multifamily
- Civic Building
- Parking Structure
- Adaptive Reuse

Each building type has setback, height, façade, and roof standards, as well as use restrictions.

**Heights and Height Transitions Adjacent to Existing Single-Family Neighborhoods:**

The potential height of new development along the S-Line Corridor emerged as one of the critical issues among nearby residents. Heights of buildings do not exceed 62 feet (5 stories) at State Street and 50 feet (4 stories) anywhere else along the corridor. Staff recognized that an appropriate height transition and buffering requirement will help maintain the livability, desirability, and value in surrounding single-family neighborhoods. The height transition is described on page 20 of the code. Where a property borders an existing single-family residential zone, a 20-foot setback is required along a property boundary with single-family homes. At 20 feet to 30 feet from the boundary, a building cannot exceed 25 feet in height. Beyond 30 feet, a building's height may increase by 1 foot for every two feet of additional horizontal distance. The height transition is illustrated in figure 2.

**Figure 2. Height Transition**

**Height Transitions along the S-Line Corridor:** To ensure that sunlight can access the S-Line corridor, staff is recommending the addition of text that requires developers to consider solar access to the corridor when submitting site plans. This could include a setback for upper stories or another configuration proposed by the developer. This language is not included in the review draft attached to this staff report, but will be included in the formal ordinance.

**Materials:** The ordinance is more restrictive on allowable materials than the residential design standards are. A list of primary materials is described on page 34 of the code.

**Residential Densities:** The form-based code restricts residential density through the parking and open space requirement. There is no maximum density included in the code. Staff believes that appropriate building types, height transitions, parking requirements and street standards will ensure compatible and complementary building development along the corridor.

**Open Space Types:** The open space types described in the code are designed to emphasize, promote, and expand the S-Line corridor. Each developer will be required to provide 250 square feet of livable open space per unit, up to 20 percent of the total project area required. Non-residential development has a 10 percent open space requirement. Open spaces may take the form of S-Line corridor enhancements, plazas, pocket parks, commons or courtyards, private open spaces, and passageways.

Open space reductions are provided for if developers provide quality open space amenities or enhancements to their frontage along the S-Line Corridor.

The open space and building type are intended to fulfill the goal of creating development that fronts the S-Line Corridor and that provides ample mid-block access to the S-Line corridor.

**Landscape:** Landscape requirements are included in this code; standards vary by the type of landscaping proposed. The landscape requirements include a specification for side and

rear buffers along existing single-family residential district boundaries. These requirements can be found on page 46 of the attached code.

**Parking:** Parking for residential uses is established at 1.4 stalls per unit, based on a parking study completed by the City, included as Attachment 5. Parking requirements for all other uses will be subject to the City's existing Parking and Access Requirements ordinance.

Staff proposes adding language to the ordinance that will restrict residential parking as follows:

1. Available on-street parking will be calculated only for the streets and lanes that are in the East Streetcar Corridor land use district. A developer will not be able to calculate on-street parking in single-family neighborhoods toward the parking total.
2. Rental residential will be approved as a conditional use permit. As a standard condition of approval, developers will be required to note in tenant leases that tenant and guest parking is restricted on surrounding neighborhood streets.
3. Guest parking is typically marked separately in residential developments. The code could include language that directs developers to mark sufficient guest parking stalls within the development, at .25 stalls per unit. This would increase the effective parking requirement to 1.65 stalls per unit.

**Sign Types:** The ordinance includes specific requirements for signage, that are more restrictive than the City's general sign standards. The sign types chapter allows developers to identify historic or iconic signs for preservation and adaptive reuse.

**Administration:** The final chapter of the code addresses approvals and administration. This chapter establishes a design review committee that will review each application for compatibility with the ordinance's intent and will review the building architecture and open spaces to ensure an enhancement of the S-Line Corridor.

**Amending or Modifying a Site Plan:** Recognizing that circumstances can change during the development a master-planned project, staff incorporated a process to amend or modify a site plan. Minor modifications of a master plan, addressing parcel reconfigurations, modifications to landscaping or building designs, or alternative parking plans, can be approved at staff level using mechanisms already adopted in ordinance.

**Subdivision Approvals and Development Standards:** The code allows for owner-occupied townhouses to be developed using PUD standards, but through a standard subdivision process, where the Planning Commission acts as the Land Use Authority. Staff believes this delegation is appropriate, given the strength of the vision for the East Streetcar Corridor and the specificity and scope of the form-based code.

Since this code will be applied in a redevelopment area, the City Council, acting as the City's Redevelopment Agency, will have opportunities to review and take comment on new developments that occur in the corridor, and that seek participation in the redevelopment area.

Staff believes that the subdivision process included in the ordinance would function as an incentive to developers, by reducing the time and cost of approving subdivisions in the East Streetcar Corridor land use district.

**Establishment of Districts and Definitions:** Chapters 17.11 and 17.13 of the South Salt Lake Municipal Code regulate the establishment of land use districts, and Chapter 17.47 establishes definitions. Attachment 2 includes the proposed language establishing the East Streetcar Corridor Land Use district, as well as additional definitions of terms found in the form-based code.

**Staff Recommendation:**

Staff finds that the form-based code responds to the South Salt Lake City General Plan and the East Streetcar Corridor Master Plan. With the addition of height transitions, specific open space requirements, and appropriate building types, future development will be compatible and complementary with existing development, and will not detrimentally impact the livability, desirability, and value of existing residential development near the corridor. Staff recommends approval of the rezone and the form-based code.

**Attachments:**

**Attachment 1:** Proposed Form-Based Code

**Attachment 2:** Proposed Amendments to Chapters 17.11, 17.13, and 17.47 of the South Salt Lake Municipal Code.

**Attachment 3:** East Streetcar Master Plan Excerpt: Design Principles. Note: the full master plan can be found at the following URL:

<http://www.southsaltlakecity.com/department-listings/community-development/streetcar>

**Attachment 4:** Planning Commission Meeting Minutes – 21 August 2014

**Attachment 5:** Multifamily Parking Study – Hales Engineering

**ATTACHMENT 1: DRAFT FORM-BASED CODE**

# East Streetcar Neighborhood Form-Based Code Contents

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## **1.0 The East Streetcar Neighborhood**

1.1 The East Streetcar Neighborhood

## **2.0 Street Types**

- 2.1 General Requirements
- 2.2 General Street Type Standards
- 2.3 General Street Layout Requirements
- 2.4 Lane
- 2.5 200 East and 400 East
- 2.6 300 East and 500 East
- 2.7 State Street
- 2.8 Streetcar Corridor
- 2.9 Parley's Trail

## **3.0 Subdistricts**

- 3.1 Introduction
- 3.2 Zoning Map

## **4.0 Uses**

- 4.1 General Requirements
- 4.2 Definition of Uses
- 4.3 List of Uses

## **5.0 Building Types**

- 5.1 Introduction to Building Type Standards
- 5.2 Introduction of Building Type Table Standards
- 5.3 Storefront Building
- 5.4 Urban Style
- 5.5 Townhome
- 5.6 Mansion Style
- 5.7 Civic Building
- 5.8 Parking Structure
- 5.9 Adaptive Reuse
- 5.10 Entrance Types
- 5.11 Roof Types
- 5.12 Additional Design Requirements

## **6.0 Open Space Types**

- 6.1 General Requirements
- 6.2 S-Line Corridor
- 6.3 Plazas
- 6.4 Pocket Park
- 6.5 Commons or Courtyard
- 6.6 Individual Open Space
- 6.7 Passageway

## **7.0 Landscape**

- 7.1 General Requirements
- 7.2 Frontage Buffer
- 7.3 Side & Rear Buffer
- 7.4 Interior Parking Lot Landscape
- 7.5 Screening of Open Storage, Refuse Areas, & Utility Appurtenances.
- 7.6 S-Line Buffer
- 7.7 Streetscape Requirements

## **8.0 Parking**

- 8.1 General Requirements

## **9.0 Sign Types**

- 9.1 General Requirements
- 9.2 Sign Types
- 9.3 Wall Sign
- 9.4 Projecting Sign
- 9.5 Projecting Marquee Sign
- 9.6 Awning Sign
- 9.7 Canopy-Mounted Sign
- 9.8 Window Sign
- 9.9 Monument Sign

## **10.0 Administration**

- 10.1 General Provisions
- 10.2 Nonconformities
- 10.3 Regulations

# 1.0 The East Streetcar Neighborhood

## 1.1 The East Streetcar Neighborhood

### 1. Description and Intent.

The East Streetcar Master Plan is the culmination of over a decade of discussion, planning and design to create a neighborhood that blends the best of both the past and the future; the plan was adopted by the South Salt Lake City Council on May 28, 2014. It started with visioning for a new mode of transit to serve a growing neighborhood that was long recognized as a shopping and entertainment destination outside of downtown Salt Lake City. After determining that this was an ideal location for transit-oriented development, as well as a walkable, urban neighborhood, the current S-Line streetcar was designed and the surrounding neighbors planned for redevelopment. This code builds upon the community input and decisions from the 2012 Streetcar visioning process, 2013 Streetcar Greenway visioning process, the 2013 S-Line branding process and the 2014 East Streetcar Master Plan.

The East Streetcar Area is a four-block, 50-acre area in South Salt Lake, located along the S-Line streetcar and Parley's Trail. The East Streetcar Area is located directly east of the Downtown South Salt Lake redevelopment area, and directly west of Sugar House. The Central Pointe TRAX station in the Downtown South Salt Lake Area is approximately two miles from Highland Drive in Sugar House. 500 East is roughly the mid-point between the two districts and is the boundary between the two cities.

### 2. Vision

This will be a vibrant, walkable neighborhood with a wide selection of homes for current and future residents of South Salt Lake. Its character, amenities and strong connections to both Downtown South Salt Lake and to Sugar House will make it one of the most desirable neighborhoods on the Wasatch Front.

(1) This plan strives to:

- (a) Build high-quality new housing for a wide spectrum of residents (age, income, culture)
- (b) Create an appealing urban neighborhood
- (c) Encourage new development that respects the existing neighborhood context
- (d) Improve neighborhood safety and appeal
- (e) Protect neighboring homes and property values
- (f) Support transit use, biking and walking
- (g) Complement the future downtown South Salt Lake

### 3. History and Character

South Salt Lake City is a classic American, post-war suburb. It also has a significant business and industrial base. The neighborhood included in the East Streetcar Master Plan is the most established, well-respected and one of the most desirable in the city. Each block in this neighborhood is unique. The architecture varies from street to street (Craftsman bungalow, Victorian, Cottage), and the land uses do



as well. It is comprised of single family homes with a mix of duplexes, apartments, and townhomes. There is also a strong lingering presence of industrial warehouses along the new streetcar line, from when it was once the Denver & Rio Grande freight line. The block adjacent to State Street has a long commercial history, having grown up along "Utah's Main Street."

The S-Line streetcar route is a remnant of our industrial history. The rails between South Salt Lake and Sugar House were originally built by the Denver & Rio Grande Railroad. Beginning in 1908, freight and passenger trains ran in this corridor. The line was called several different names by locals over the years, including the Granite Line, Sugar House Spur, and the Silver Line. The line began at Roper Rail Yards, where it connected to major north-south routes. Heading east, the line passed through the neighborhoods of South Salt Lake, Forest Dale, and Sugar House before making its way through the deep pass in Emigration Canyon and into Park City, where it served the booming mines and mills. Silver, sheep, and other valuable commodities were brought to Salt Lake for processing. The Denver & Rio Grande (D&RG) line to Park City was abandoned in 1947, and the corridor was purchased by Union Pacific Railroad. Eventually, the Park City Branch Line was reduced to the present-day corridor of the Sugar House Spur; it was purchased by the Utah Transit Authority (UTA) in 2005. Today, it is known as the S-Line—the first streetcar in modern Salt Lake City.

This railway helped South Salt Lake become "A City of Industry" in the mid-20th century. Local businesses such as Burton Lumber and Granite Mill helped build this neighborhood, literally and figuratively. Lumber, stone, building supplies, and furniture were loaded into and out of businesses on this spur line. South Salt Lake and Sugar House became major business districts and desirable residential neighborhoods for a growing merchant class.

Designs for the modern streetcar greenway in the S-Line corridor reflect this industrial history. This industrial history theme was reflected in the S-Line logo, in the artwork that re-uses historic building and rail materials, and in the adaptive reuse of historic buildings. While this neighborhood is considered historic, it also has a history of being thoroughly modern and on the cutting edge of industry. Several words to describe the industrial history are honest, timeless, and tough. These characteristics are at play in this neighborhood and should be considered touchstones for development today.



Figure 1.1(1). Historic Rail Line.

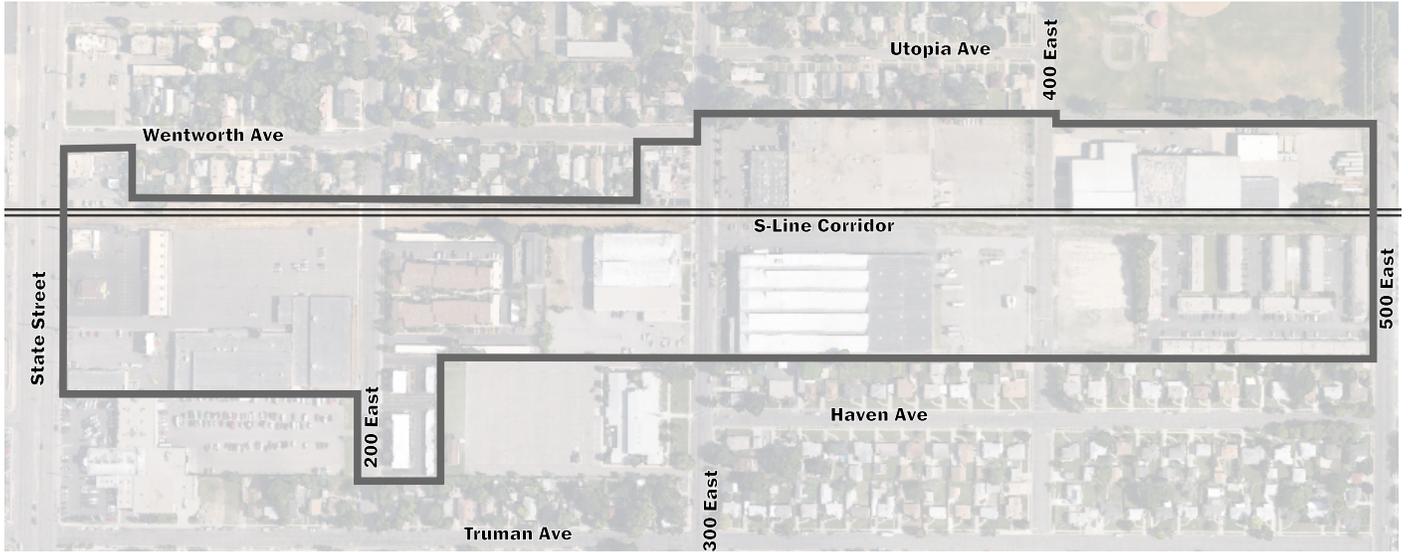


Figure 1.1(2). Map of the East Streetcar Neighborhood.

# 2.0 Street Types

## 2.1 General Requirements.

### 1. Intent.

The standards outlined in this section are intended to:

- (1) Create complete streets that address all modes of travel including pedestrian traffic, bicycle traffic, transit, and vehicular traffic at speed appropriate for shared use.
- (2) Address all features of the street right-of-way (ROW), including sidewalks, parkways, traffic lanes, bicycle lanes, and medians.
- (3) Create streets and public ROWs that reduce stormwater runoff quantity and improve quality of stormwater runoff.
- (4) Create focal points that emphasize the S line corridor intersections.

### 2. General Requirements.

All proposed streets, landscaping or furnishing zones, and sidewalks shall be located in dedicated vehicular ROWs as required by this section.

- (1) Street Types. All new vehicular ROWs shall match one of the street types, refer to 2.4 through 2.8, whether publicly dedicated or privately held.
- (2) Public Use. Streets may be privately or publicly owned but all streets shall be available for public use at all times.
- (3) Gated streets are not permitted.

### 3. Street Construction Specifications.

All construction in the ROWs shall follow specifications defined by the South Salt Lake City Engineer and Fire Marshal.

## 2.2 General Street Type Standards.

### 1. Street Types.

Street Types defined in this section outline acceptable street configurations. New streets should be designed using the principles and characteristics defined by each street type. The City Engineer may require additional right-of-way, pavement width, or additional street elements depending on unique site characteristics.

### 2. Graphics.

The graphics provided here, illustrating each street type, are samples of recommendations and illustrate a configuration of that street type. By applying the standards outlined and working with the City Engineer and Fire Marshal, other configurations are possible.

### 3. Typical Street Elements.

Typical elements of a vehicular right-of-way are divided into the vehicular and pedestrian realm. Each street type detailed in this section outlines which facilities are applicable.

- (1) Vehicular Realm. The vehicular realm is comprised of the travel

lanes, bicycle lanes, and parking lanes.

- (2) Pedestrian Realm/Street Buffer. The pedestrian realm is typically comprised of pedestrian facilities, such as sidewalk, path/trail or off-street bicycle path, and a buffer area, consisting of a landscape zone or furnishings zone that serves to buffer pedestrians or bicyclists from the movements of higher speed vehicles in the vehicular realm. Pedestrian paths should be public if they extend beyond the project boundary. Pedestrian paths should have direct access to existing public passageways as appropriate.
  - (a) Landscape Zone. A landscape area between the back of curb or edge of pavement to the sidewalk in which street trees, swales, lighting, and signage may be located. Typically used adjacent to residential buildings.
  - (b) Furnishings Zone. A hardscape area that extends from the sidewalk to the back of curb, in which street trees, street furniture, lighting, and signage may be located. Typically used adjacent to commercial or office buildings.

### 4. Vehicular Travel Lanes.

The number and width of vehicular travel lanes are determined by the Street Type.

### 5. Bicycle Facilities.

The following types of bicycle accommodations are permitted in the vehicular realm per Street Type. Refer to Figure 2.2 (3).

- (1) Dedicated Bicycle Lane. Dedicated bicycle lanes are striped lanes on the outside of the outermost travel lanes that are designated only for bicycle use. This lane occurs on both sides of the street and shall be four to five feet wide. This is required on 300 East and 500 East.
- (2) Designated Shared Lane. A designated shared lane is a lane that is shared between vehicles and bicycles. This lane is typically wider than a standard vehicular lane, minimum 13 feet, in order to accommodate both types of users, and includes a painted

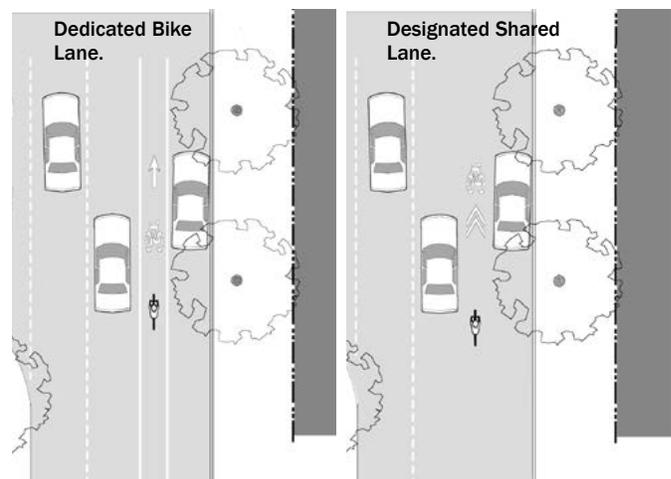


Figure 2.2 (1). On-Street Bicycle Facilities.

bicycle marker combined with a double arrow (known as a “sharrow”). This improvement occurs in both directions on 200 East and 400 East

- (3) Shared Lane. A shared lane refers to a street that does not have bicycle lanes or a designated shared lane, but the speed and configuration of the street is such that bicycles could comfortably share lanes with traffic.

### 6. Stormwater Management.

Incorporation of stormwater management best practices is encouraged, such as incorporating drainage swales and slotted curbs into the Landscape Zone adjacent to parking lanes. Where achievable, permeable pavement materials should be selected, such as unit pavers, porous concrete, and porous asphalt. For suggested methods refer to Figure 2.2 (2). Final design shall meet the requirements of the City Engineer.

### 7. Street Trees.

Street trees are required along all street frontages and, where possible, along the S-Line corridor. Spacing for large and medium trees is 30 feet. Spacing for smaller trees is 20 feet from center. For street tree requirements refer to South Salt Lake Landscape Handbook.

### 8. Fire Access.

Street configurations have been calculated to provided emergency vehicle access. Where the total width of all travel lanes is narrower than 20 feet, the following shall apply.

- (1) Room to Pass. At 120 foot increments, a 20 foot opening in the on-street parking or a 20 foot dedicated pull-off space must be provided to allow vehicles to pull over for a fire truck to pass.
- (2) Driveway or Fire Hydrant Zone. A driveway or fire hydrant zone may be utilized to fulfill the requirement.
- (3) Suitable down-rigger locations must be approved by the Fire Marshal. See Chapter 2.4.

## 2.3 General Street Layout Requirements.

### 1. Intersections.

- (1) Curb Radii. The following curb radii shall be utilized unless otherwise authorized.
  - (a) Intersections should be designed for actual turning radius of the typical design vehicle as opposed to the maximum design vehicle. Small curb radii at intersections shorten pedestrian crossing distances and reduce vehicle turning speeds, thereby balancing the ease of travel of the vehicles and pedestrians. Refer to Figure 2.3 (1).
  - (b) 200 East, 300 East, 400 East, 500 East. At the intersections of these streets the following curb radii shall be utilized.
    - (i) With on-street parking on both streets, a 5 foot radius may be utilized.

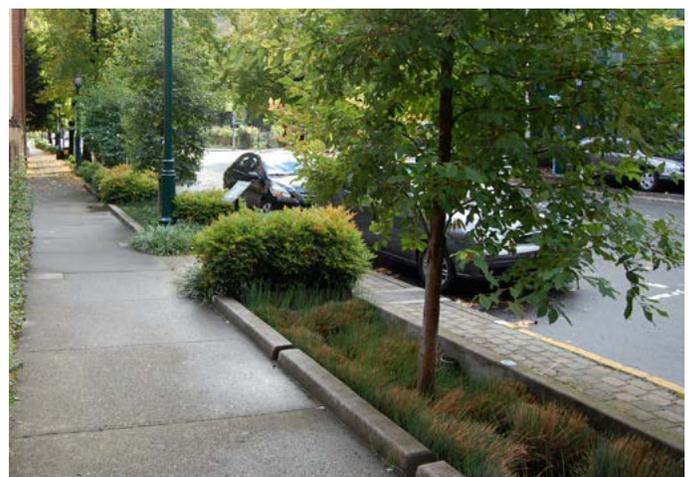


Figure 2.2 (2). Curb Cut and Landscape Storm Drainage Methods

# 2.0 Street Types

- (ii) Without on-street parking, a 15-foot radius is required.
  - (c) State Street. At the intersections of 200, 300, 400, or 500 East, the following curb radii shall be utilized.
    - (i) With on-street parking on both streets, a 10-foot radius is required.
    - (ii) Without on-street parking on either streets, a 25-foot radius is required.
  - (d) Larger Radius. When the design vehicle requires a larger curb radius and no on-street parking exists, a 30-foot radius may be utilized. Larger radii require approval of the City Engineer.
  - (e) Lane Intersections. The curb radius at intersections involving Lanes shall be no greater than 5 feet.
- (2) Crosswalks. Crosswalks shall be required at all intersections in the East Streetcar Corridor land use district, including mid-block pedestrian crossings.
- (a) Dimensions. Crosswalks shall be at least six feet wide, measured from mid-stripe to mid-stripe, per the Manual on Uniform Traffic Control Devices (MUTCD).
  - (b) Markings. Crosswalks shall be appropriately indicated on the finished street surface or where required in parking and access areas. Crosswalks shall be marked with textured or colored pavement, thermoplastic applications, or another marking approved by the Land Use Authority.
  - (c) Crossing Distances. To encourage pedestrian activity, typical crosswalks shall not extend over 38 feet without a landscape median, bulb-outs and/or other pedestrian refuge to mitigate the effects of vehicular traffic on crossing and increase pedestrian safety and comfort. Refer to Figure 2.3 (2).
  - (d) Accessible ramps and warning panels, per the American Disabilities Act or any more stringent state requirement, are required where all sidewalks or trails terminate at a crosswalk or curb.
  - (e) Ramp Orientation. Ramps shall be oriented perpendicular to traffic, requiring two ramps per corner at intersecting streets.
- (3) Bulb-outs. To shorten pedestrian crossing distances, bulb-outs should be utilized at all intersections, unless otherwise required by the City Engineer. Refer to Figure 2.3 (3).
- (a) The depth of the bulb-out shall match the width of utilized on-street parking.
  - (b) The radius of the bulb-out shall match the requirements for the intersection.
  - (c) Planted areas shall be included where appropriate to delineate pedestrian crossings and to enhance the streetscape.

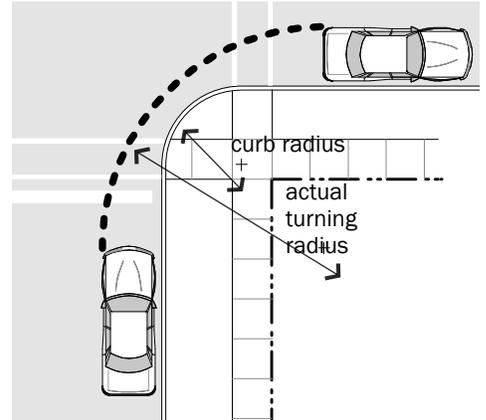


Figure 2.3 (1). Curb Radius Diagram

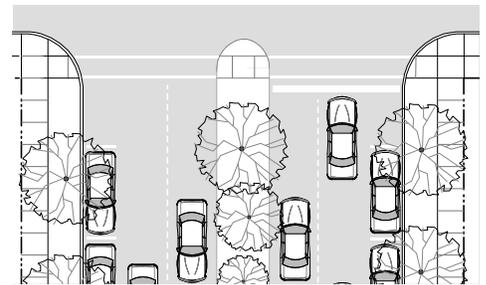


Figure 2.3 (2). Pedestrian Crossing (Median)

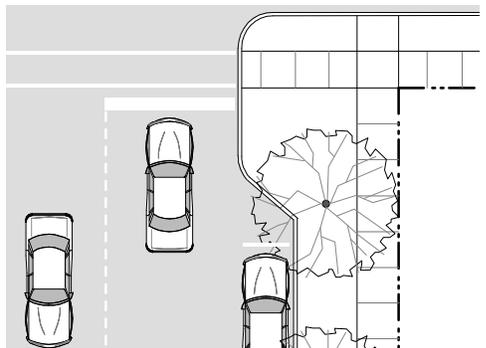


Figure 2.3 (3). Bulb-out Diagram

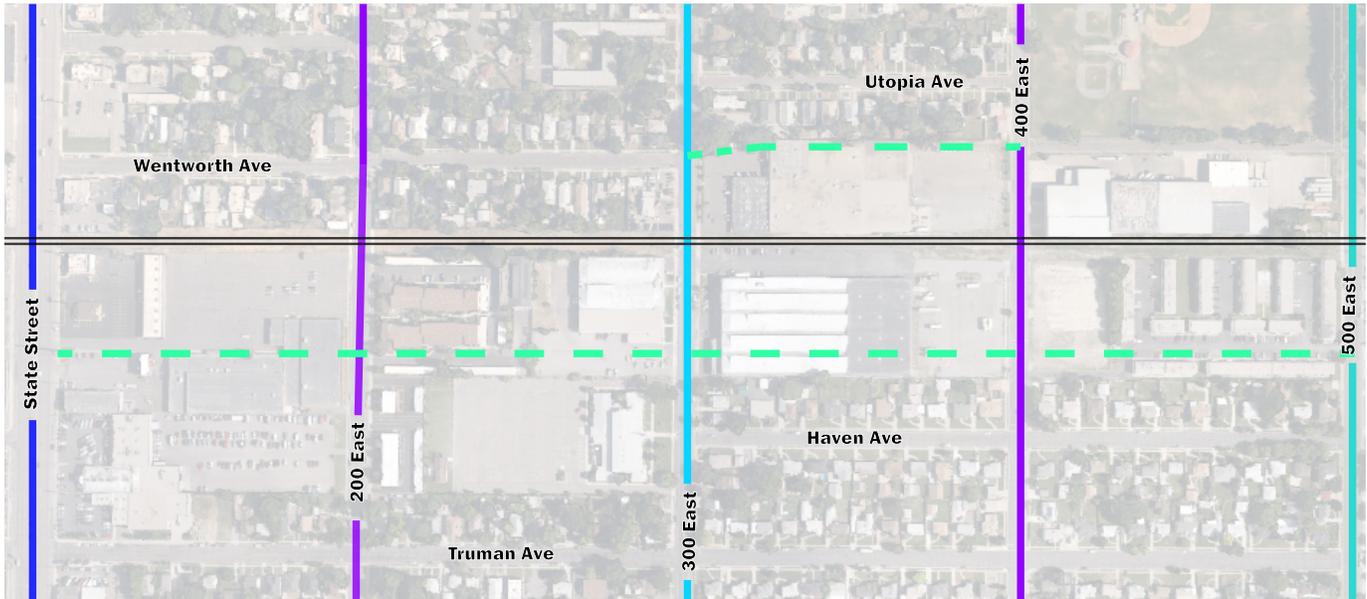


Figure 2.3 (4). Street Type Map.

### Street Type Map Key

-  Streetcar Corridor
-  State Street
-  300 East and 500 East
-  200 East and 400 East
-  Proposed Lanes

# 2.0 Street Types

## 2.4 Lane.

### 1. Intent.

A one-way lane is an option that allows shared access to the interior of a block at appropriate speeds. A one way lane can also serve as a separation between more intense residential uses and adjacent single family detached housing. This one way street may be located between uses or along the property line separating existing single family homes from new development. The lane includes shared bicycle and pedestrian access, parallel parking on one side, and vehicle traffic. Traffic-calming measures shall be incorporated into the lane.

### 2. General Requirements.

Lanes shall be developed using the standards in Table 2.4 (1) and include down-rigger points.

Lane Requirements	
Permitted Districts	All Districts
Permitted Adjacent Building Types	All Building Types
Typical Right-of-Way Width	32'
Vehicular Realm	
Travel Lanes	One way (eastbound) yield lane
Lane Width	12'
Allowable Turn Lanes	Not applicable
Parking Lanes	One side (may be staggered), 8' required including gutter
Pavement Width	Minimum 23'
Fire Access	300' max distance between down-rigger pads (3'x4' pad)
Bicycle Facilities	Shared
Pedestrian Realm	
Pedestrian Facilities	Shared; travel lane is shared among drivers, pedestrians and bicyclists, 5'
Street Buffer	8' wide, switch sides at 100' maximum distance to create parking pockets and accommodate storm water

Table 2.4 (1). Lane Requirements.

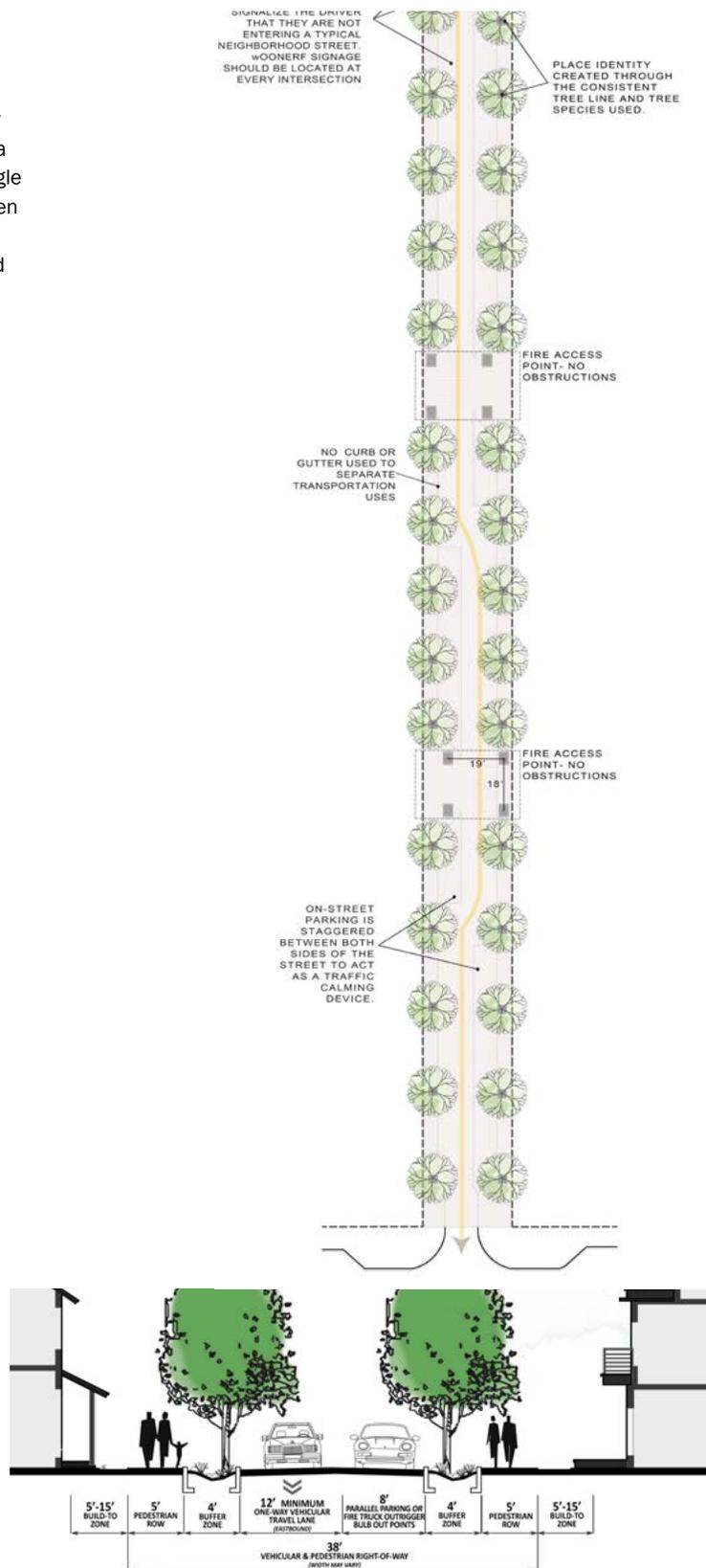


Figure 2.4 (1). Lane Cross Section and Plan-view

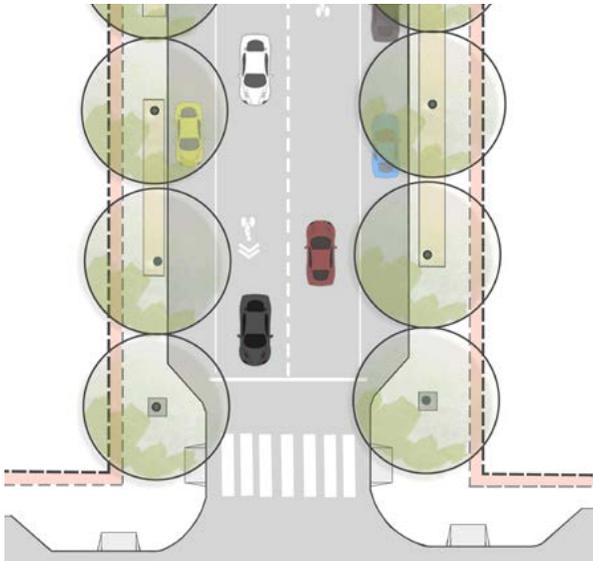
## 2.5 200 East and 400 East.

### 1. Intent.

These recommendations to existing streets are suggested to calm traffic, and to improve walkability and bicycle access. Special treatment of landscaping and street trees is encouraged to promote and signify the East Streetcar neighborhood identity. Changes in street tree types are encouraged in order to promote and identify the East Streetcar Neighborhood and to accent the S-Line corridor; street tree requirements are found in 7.7. On street parking is provided. Bike riders share the road on these streets.

### 2. General Requirements.

This street type shall be developed using the standards in Table 2.5(1).



## 200 East and 400 East Street Requirements

<b>Permitted Districts</b>	Permitted for 200 East and 400 East
<b>Permitted Adjacent Building Types</b>	All building types
<b>Typical Right-of-Way Width</b>	66'
<b>Vehicular Realm</b>	
<b>Travel Lanes</b>	1 lane in each direction
<b>Lane Width</b>	13'
<b>Allowable Turn Lanes</b>	Not applicable
<b>Parking Lanes</b>	Parallel required on both sides of street
<b>Pavement Width</b>	41'
<b>Median</b>	Permitted
<b>Bicycle Facilities</b>	Shared
<b>Pedestrian Realm</b>	
<b>Pedestrian Facilities</b>	Minimum 5' wide sidewalk combined with an additional adjacent 2' of similarly paved sidewalk within the front yard setback area
<b>Street Buffer</b>	Minimum 6 feet wide Landscape Zone

Table 2.5 (1). 200 E and 400 E Street Requirements.

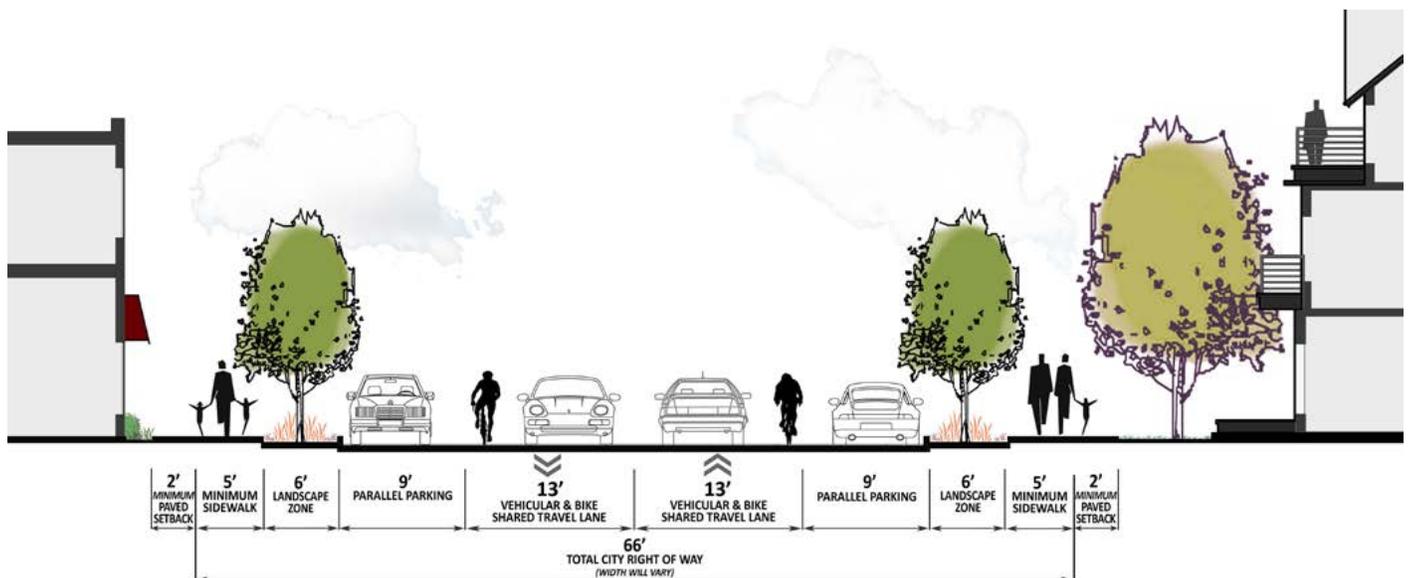


Figure 2.5 (1). 200 E and 400 E Cross Section and Plan-view

# 2.0 Street Types

## 2.6 300 East and 500 East Street.

### 1. Intent.

300 East has a slightly greater transportation role and is intended to accommodate bicycles, pedestrians, and vehicles. A striped bike lane is included in the configuration along with on-street parking.

500 East accommodates larger volumes of traffic at greater speeds than 300 East, but includes a bike lane with on-street parking.

### 2. General Requirements.

Streets shall be developed using the standards in Table 2.6 (1).

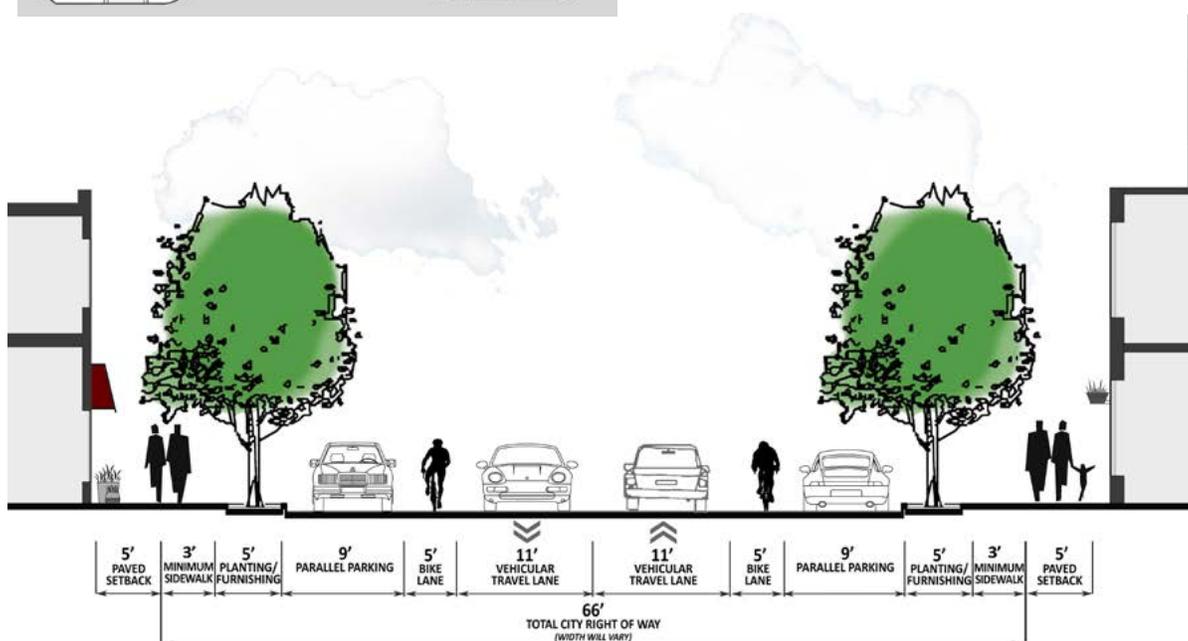
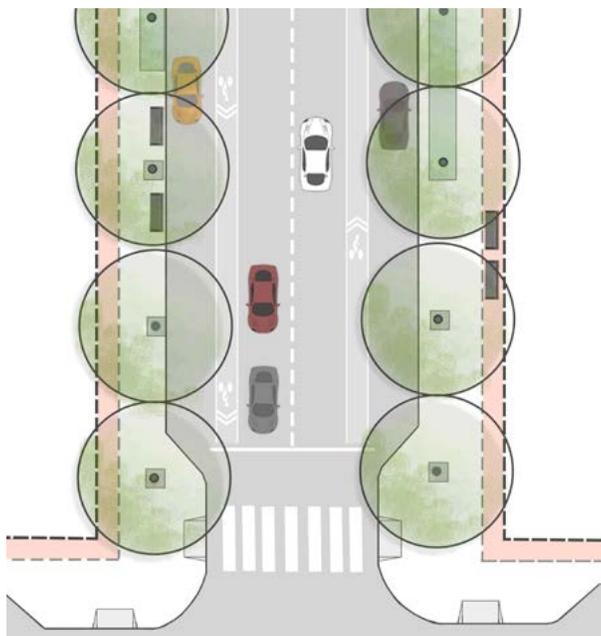


Figure 2.6 (1). 300 East and 500 East Cross Section and Plan-view

### 300 East and 500 East Requirements

**Permitted Districts** Permitted for 300 East and 500 East

**Permitted Adjacent Building Types** All Building Types

**Typical Right-of-Way Width** 66' to 72'

#### Vehicular Realm

**Travel Lanes** 1 lane in each direction

**Lane Width** 11'

**Allowable Turn Lanes** Right permitted in place of parking at intersections; left only with median.

**Parking Lanes** Parallel required on both sides of street.

**Pavement Width** 48' or 52' for alternative

**Bicycle Facilities** Dedicated bike lanes on 300 East  
Sharrows exist on 500 East

#### Pedestrian Realm

**Pedestrian Facilities** Minimum 3' wide sidewalk combined with an additional adjacent 5' of similarly paved sidewalk within the front yard setback area

**Street Buffer** Minimum 4 feet wide planting zone or furnishings zone

Table 2.6 (1). 300 E and 500 E Requirements.

## 2.7. State Street.

### 1. Intent.

Adjacent the East Streetcar Neighborhood, State Street is the primary arterial thoroughfare. Near the intersection with the S-Line, cross sectional changes are encouraged to promote greater walkability through the provision of larger park strips, wider sidewalks, and on street parking. Although controlled by UDOT, other means of identifying the S-Line corridor such as art, signs, specialty lighting, and street trees are suggested. This street accommodates the highest volumes of traffic at the highest speeds.

### 2. General Requirements.

State street shall be developed using the standards in Table 2.7 (1).

### State Street Requirements

<b>Permitted Districts</b>	Permitted for State Street Gateway
<b>Permitted Adjacent Building Types</b>	All Building Types
<b>Typical Right-of-Way Width</b>	138'
<b>Vehicular Realm</b>	
<b>Travel Lanes</b>	3 lanes in each direction
<b>Lane Width</b>	12'
<b>Allowable Turn Lanes</b>	Yes
<b>Parking Lanes</b>	Parallel required on both sides of street.
<b>Pavement Width</b>	108'
<b>Median</b>	Permitted
<b>Bicycle Facilities</b>	Not designated
<b>Pedestrian Realm</b>	
<b>Pedestrian Facilities</b>	Minimum 10' wide clear sidewalk
<b>Street Buffer</b>	A larger street buffer should be negotiated with adjacent land owners

Table 2.7 (1). State Street Requirements.

Figure 2.7 (1). State Street Cross Section and Plan-view

# 2.0 Street Types

## 2.8 Streetcar Corridor.

### 1. Intent.

The S-Line streetcar is a UTA-operated rail line connecting from Sugar House to the Central Pointe TRAX station. It is operated as a streetcar, which means frequent stops (every 2 blocks), slow speeds (15 mph max) and quiet operations. It is intended to help alleviate commuter congestion in this neighborhood as well as spur economic development.

The S-Line corridor has been built as a multiple-user corridor to accommodate transit, pedestrians and cyclists on a trail, and slower moving pedestrians on a strolling path. The corridor includes the streetcar and trail and additional elements will be built as development goes in.

### 2. General Requirements.

This corridor is owned by UTA and all improvements, maintenance and management are governed by their regulations, as well as the Federal Transit Authority requirements. Safety is paramount. Improvements to the corridor and access by the public have been made under special agreement between South Salt Lake, Salt Lake City, Salt Lake County and UTA. Improvements adjacent to the corridor and in

the setback areas should respect the need to continuously operate the train, protect public safety around trains, and avoid conflict with the overhead catenary system that powers the train. Buildings and improvements close to the corridor may require additional review by the City Engineer or UTA to ensure they are not creating a hazard.

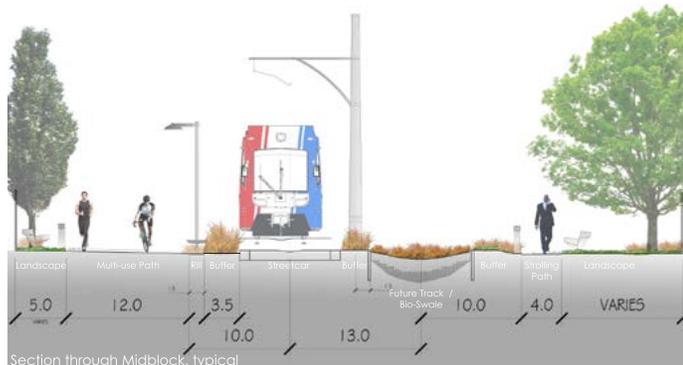


Figure 2.8 (1). Corridor Cross Section.

### Streetcar Corridor Requirements

<b>Permitted Districts</b>	All
<b>Permitted Adjacent Building Types</b>	All Building Types
<b>Typical Right-of-Way Width</b>	66' for State Street to 300 South, 57'-60' for 300 to 400 South, 50' for 400 to 500 South
<b>Vehicular Realm</b>	
<b>Track</b>	One way track 10', Double track
<b>Pavement Width</b>	10' concrete embedded track
<b>Median</b>	Not permitted
<b>Pedestrian and Bike Realm</b>	
<b>Pedestrian Facilities</b>	Minimum 10' wide clear sidewalk
<b>Streetcar Buffer</b>	Minimum 5' from edge of embedded track

Notes

Table 2.8 (1). Streetcar Corridor Requirements.



Figure 2.8 (2). S-Line Corridor Before and After.



## 2.9 Parley's Trail.

### 1. Intent.

Parley's Trail is an 8-mile regional trail connecting the Jordan River to the Bonneville Shoreline Trail at Parley's Canyon. This trail serves multiple users on bicycle, foot, skates and also allows on-leash dogs. The trail is designed in this 2-mile section adjacent to the streetcar for slower speeds than other sections of the trail. The greenway adjacent to it includes landscaping, public art, plazas and a strolling path. The greenway is fully developed in Salt Lake City and under consideration for funding in South Salt Lake. This trail is intended to serve non-vehicle commuters and also help access local businesses and residences. The trail is an amenity designed to attract residents and spur local economic development. It also is the primary source of recreation and open space for the East Streetcar neighborhood.

### 2. General Requirements.

The function and safety of the trail should be preserved by keeping it clear for trail users. Additional plaza areas, sidewalks and strolling paths should be built to serve and access residential units, commercial, outdoor dining, and other uses. The greenway is a narrow corridor with severe limitations on what can be planted in some areas due to space, safety and UTA requirements. The intent is to expand the virtual experience of the greenway by adding landscaping and pedestrian circulation within setback areas adjacent to the corridor. In addition, open spaces such as plazas, courtyards and balconies are encouraged to open onto the trail to add visual interest, take advantage of the views of the greenway, and to put more eyes on the street in this corridor.

## Parley's Trail Requirements

<b>Permitted Districts</b>	All
<b>Permitted Adjacent Building Types</b>	All Building Types
<b>Typical Right-of-Way Width</b>	12' trail corridor by easement to Salt Lake County
<b> Vehicular Realm</b>	
<b>Trail</b>	12' wide for two way trail
<b>Pavement Width</b>	12'

Notes: Buffer. 5' minimum buffer from UTA streetcaar with continuous landscaping.

Table 2.9 (1). Parley's Trail Requirements.

# 3.0 Subdistricts

## 3.1 Introduction.

The following subdistricts are hereby created to regulate the location of distinct mixes of building forms and uses permitted within the East Streetcar Neighborhood.

The subdistricts are organized into three categories. Each subdistrict consists of a series of uses and building types at different heights.

### 1. State Street Gateway.

State Street to 200 East

Located on the east side of State Street, this subdistrict allows for more commercial activity, coupled with an emphasis on housing. It becomes the western gateway to the neighborhood and is defined by the prominent road crossing for the streetcar and trail. Gateway identification and beautification strategies are implemented within the subdistrict. This subdistrict has a permitted height of five stories with appropriate transitions from single family homes. Streetcar access for pedestrians, bikes, and vehicles is required when applicable.

### 2. North Haven.

200 East to 400 East

As a primarily residential area, this subdistrict compliments the corridor by providing extensive housing opportunities, improved connections to the S-line corridor, increased walkability, and neighborhood supportive commercial uses. Housing height is limited to four stories

maximum with appropriate height transitions from single family homes. Streetcar access for pedestrians, bikes, and vehicles is required when applicable.

### 3. 500 East Gateway.

400 East to 500 East

Located to the west of 500 East, this subdistrict represents the east entry into the City and is clearly identified by the S-Line station. Additional identifiers are suggested within the requirements of this district. Land uses emphasize housing but may include larger neighborhood supportive commercial uses. Building height is limited to four stories maximum with appropriate height transitions from single family homes. Streetcar access for pedestrians, bikes, and vehicles is required when applicable.

## 3.2 Zoning Map.

### 1. Mapped Districts.

The areas and boundaries of the subdistricts listed in 3.1 above are established as shown on the map entitled "Subdistricts".

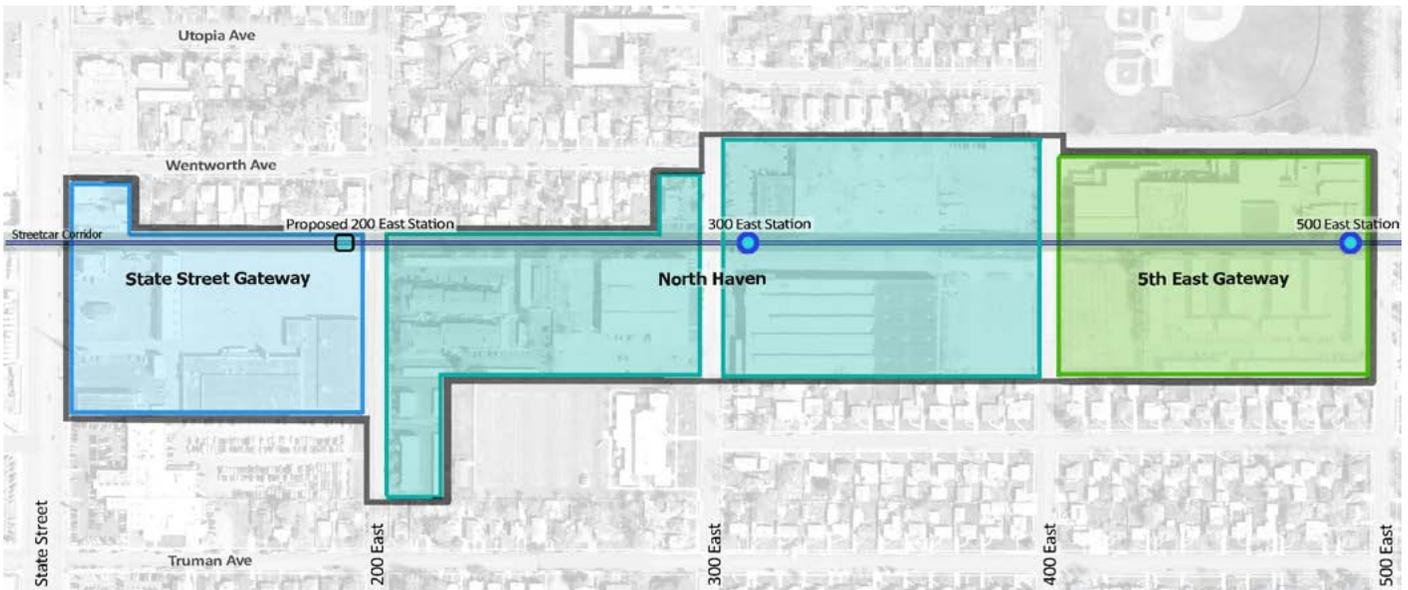


Figure 3.1 (1). Subdistricts.

# 4.0 Uses

## 4.1 General Requirements.

### 1. General Provisions.

The following general provisions apply to the uses outlined in this section.

- (1) A lot may contain more than one use.
- (2) Each of the uses may function as either a principal use or accessory use on a lot, unless otherwise specified.
- (3) Uses are either permitted by-right in a subdistrict, permitted by-right with specific development or design parameters, or require a Conditional Use Permit in order to be developed.
- (4) Each use shall be located within a permitted Building Type (Refer to 5.0 Building Types), unless otherwise specified.
- (5) Each use may have both indoor and outdoor facilities, unless otherwise specified.

### 2. Organization.

The uses are grouped into general categories, which may contain lists of additional uses or clusters of uses.

- (1) Unlisted Similar Use. If a use is not listed but is similar in nature and impact to a use permitted within a zoning district, the Land Use Authority may interpret the use as permitted.
  - (a) The unlisted use will be subject to any development standards applicable to the similar permitted use.
  - (b) If the unlisted use is similar in nature and impact to a use requiring a Conditional Use Permit, the Land Use Authority may interpret the use as also requiring a Conditional Use Permit.
- (2) Unlisted Dissimilar Use. If a use is not listed and cannot be interpreted as similar in nature and impact to a use within a land use that is either permitted or requires a Conditional Use Permit, the use is not permitted and may only be approved through an amendment of this code.

### 3. Use Table.

Table 4.1 (1). Uses by Subdistrict outlines the permitted uses in each land use subdistrict. Each use is given one of the following designations for each zoning subdistrict in which that use is permitted.

- (1) Permitted (“P”). These uses are permitted by-right in the districts in which they are listed.
- (2) Requires a Conditional Use Permit (“C”). These uses require administrative review and approval in order to occur in the districts in which they are listed and must follow any applicable development standards associated with the use as well as meet the requirements of the Conditional Use.
- (3) Listed uses that are not permitted in the subdistrict are indicated by a blank space.

Uses	Subdistricts		
	State Street Gateway	North Haven	5th East Gateway
<b>Residential &amp; Lodging</b>			
Residential	C	C	C
Hotel & Inn	P		
<b>Civic</b>			
Assembly	C	C	C
Transit Station	P	P	P
Library/Museum/Post Office (no distribution)	P	P	P
Police & Fire	P	P	P
School	P <sup>1</sup>	P <sup>1</sup>	P
<b>Retail</b>			
Neighborhood Retail	C	C <sup>1</sup>	C <sup>1</sup>
Small Outdoor Sales Display	P	P	P
<b>Service</b>			
Neighborhood Service	C	C <sup>1</sup>	C <sup>1</sup>
<b>Office &amp; Industrial</b>			
Office	P	P	P
Craftsman Industrial	C <sup>2</sup>	C <sup>2</sup>	C <sup>2</sup>
<b>Infrastructure</b>			
Utility & Infrastructure	C <sup>3</sup>	C <sup>3</sup>	C <sup>3</sup>
Open Space	P	P	P
<b>Accessory Uses</b>			
Home Occupation	*	*	*
Parking Lot	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>
Parking Structure	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>

Table 4.1 (1). Uses by Subdistrict.

**KEY**

P: Permitted

C: Requires a Conditional Use Permit

<sup>1</sup>: Within 200' of S-Line intersection (from back of curb)

<sup>2</sup>: In existing buildings at time of adoption

<sup>3</sup>: Not on street frontages or S-Line corridor

<sup>4</sup>: Parking facilities shall not occupy ground story within 75' of S-Line corridor

\*: See 17.06.130 of South Salt Lake City Municipal Code

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#### 4. Building Types.

The uses permitted within the subdistrict may be further limited by the building types permitted. Refer to 5.0 Building Types.

#### 4.2 Definition of Uses.

##### 1. Residential and Lodging Uses.

A category of uses that include several residence types.

- (1) Residential. One or more dwelling units located within the principal structure of a lot, in which the units may or may not share a common wall with the adjacent (horizontally or vertically) unit or have individual entrances from the outside. Rental residential developments must be 50 units or more in size. For complete requirements for rental multifamily development refer to South Salt Lake City's Land Development Code Title 15. The Land Use Authority may waive certain requirements for buildings that existed prior to the adoption of this code.
- (2) Hotel and Inn. An establishment which provides services such as meals, room and board accommodations, and other hospitality services to short-term stay guests.
  - (a) Bed and Breakfasts are permitted.

##### 2. Civic Uses.

A category of uses related to fulfilling the needs of day-to-day community life including assembly, public services, educational facilities, and hospitals.

- (1) Assembly. A facility that has organized services, meetings, or programs to benefit, educate, entertain, or promote discourse amongst the residents of the community in a public or private setting. Assembly includes such uses as a community center, house of worship, and private clubs and lodges.
- (2) Transit Station. A covered passenger boarding and alighting facility with a platform(s), which may include a waiting room, ticket office or machines, restrooms, or concessions.
- (3) Library/Museum. A structure open to the general public housing educational, cultural, artistic, or historic information, resources, and exhibits. May also include food service and a gift shop.
- (4) Post Office. A publicly accessed facility for the selling of supplies and mail related products and the small scale collection and distribution of mail and packages. Large-scale postal sorting and distribution is not permitted.
- (5) Police and Fire. A facility providing public safety and emergency services; training facilities, locker rooms, and limited overnight accommodations may also be included. The facilities shall be housed in a permitted building, but shall have the following additional allowances:
  - (a) Garage doors are permitted on the front facade.
  - (b) Exempt from maximum driveway widths.
- (6) School. An education facility with classrooms and offices that

may also include associated indoor facilities such as ball courts, gymnasium, theater, and food service.

##### 3. Retail Uses.

A category of uses involving the sale of goods or merchandise to the general public for personal or household consumption.

- (1) Neighborhood Retail. A use in this category occupies a space of less than 12,000 square feet, however such uses can be aggregated into larger retail developments along the corridor. Neighborhood retail includes such uses as those listed in Table 4.2 (1). Typical Retail Uses.
- (2) Small Scale Outdoor Sales Display. A use involving the sale of goods or merchandise to businesses and/or the general public, where the majority of the goods are stored or displayed outdoors. Outdoor sales lots include such uses as temporary food vendors or the sale of garden supplies.

##### 4. Service.

A category of uses that provide patrons services and limited retail products related to those services. Visibility and accessibility are important to these uses, as most patrons do not utilize scheduled appointments.

- (1) Neighborhood Service. A use in this category occupies a space of less than 12,000 square feet. Neighborhood service includes such uses as those listed in Table 4.2 (2). Multiple neighborhood service uses can be aggregated in one development.

##### 5. Office and Industrial Uses.

A category of uses for businesses that involve the transaction of affairs of a profession, service, industry, or government. Patrons of these businesses usually have set appointments or meeting times; the businesses do not typically rely on walk-in customers. Office uses include those listed in Table 4.2 (3).

- (1) Craftsman Industrial. A use involving small scale manufacturing, production, assembly, and/or repair with little to no noxious by-products that includes a showroom or small retail outlet. Craftsman industrial includes such uses as those found in Table 4.2 (4). This use may also include associated facilities such as offices and small scale warehousing, but distribution is limited. The maximum overall gross floor area is limited to 20,000 square feet, unless otherwise noted. The land use authority may waive certain requirements for buildings that existed prior to the adoption of this code.

##### 6. Infrastructure.

- (1) Utility and Infrastructure. A lot that is primarily utilized for the City's infrastructure needs. Utility and infrastructure includes such uses as electric or gas services, sewage treatment, water treatment and storage, and energy conversion systems. In all districts, utilities and infrastructure require a Conditional Use Permit ("C").

# 4.0 Uses

(2) Open Space. A use of land for active or passive, public or private, outdoor space, including such uses as parks, plazas, greens, playgrounds, or community gardens. Refer to 6.0 Open Space Types for permitted forms of open space. Open space uses may also be utilized to host temporary private or community events, such as a farmer’s market or art fair. This may involve small scale food and beverage service, no more than 200 square feet in space, located in a kiosk.

## 7. Accessory Uses.

A category of uses that are not permitted to serve as the principal use on a zoning lot.

(1) Home Occupation. See 17.06.130 of South Salt Lake City Code

(2) Parking Lot. An uncovered paved surface used solely for the parking of vehicles, intended for use by the occupants in an adjacent building on the lot. Parking lot locations are regulated by Building Type. Refer to 5.0 Building Types.

(3) Parking Structure. A structure used solely for the parking of vehicles, intended for use by the occupants in an adjacent building on the lot. Parking Structures within the buildings are regulated by Building Type. Refer to 5.0 Building Type. Separate structure locations are also regulated by Building Type, but shall also meet all of the requirements of 5.8. Parking Structure.

## 8. Time Restriction.

Business activities in the East Streetcar Corridor land use district shall not normally be conducted before 6:00 a.m. or 10:00 p.m.

## 4.3 List of Uses.

### Neighborhood Retail

Alcohol & Liquor Sales (refer to state law)  
 Antique Shop  
 Apparel & Accessory Store  
 Art & Education Supplies  
 Bakery, Retail  
 Bicycle Sales & Repair  
 Book, Magazine, & Newspaper Store  
 Building Materials, Hardware, and Garden Supply  
 Camera & Photo Supply Store  
 Drug Store/Pharmacy  
 Fabric & Craft Store  
 Florist  
 Gift, Souvenir Shop  
 Grocery Store  
 Hardware Store  
 Hobby Shop  
 Jewelry Sales & Repair  
 Live-Work  
 Luggage & Leather Goods  
 Music Store  
 Musical Instrument Repair & Sales  
 Office Supply  
 Optical Goods  
 Paint & Wallpaper  
 Party Supply Shop  
 Pet & Pet Supply  
 Specialty Food Market (Butcher, Candy, Fish Market, Produce, etc.)  
 Sporting Goods Sales & Rental  
 Stationary & Paper Store  
 Toy Shop  
 Video/Game Sales & Rental

Table 4.2 (1) Typical Retail Uses.

### Neighborhood Service

Bank or other Financial Service  
 Barber Shop, Beauty Salon, & Spa  
 Catering  
 Day Care, Adult or Child  
 Dry Cleaning & Laundry  
 Emergency Care Clinic  
 Fitness, Dance Studio, & Gym  
 Framing  
 Home Furniture & Equipment Repair  
 Live-Work  
 Locksmith  
 Mailing Services  
 Pet Grooming  
 Photocopying & Printing  
 Photography Studio & Supplies (on-site processing permitted)  
 Restaurants (refer to state law for alcoholic beverage requests)  
 Shoe Repair  
 Tailor & Seamstress  
 Tanning Salon  
 Theater  
 Training Center  
 Travel Agency & Tour Operator  
 Veterinarian

Table 4.2 (2) Typical Service Uses.

### Office

Architecture/Engineering/Design  
 Building Contractor (office only)  
 Business Consulting  
 Charitable Institutions  
 Computer Programming & Support  
 Detective Services  
 Educational Services (tutor & testing)  
 Employment Agency  
 Financial & Insurance  
 Government Offices  
 Legal Services  
 Live-Work  
 Management Services  
 Physical Therapy/Physical Rehabilitation  
 Medical & Dental with Laboratory  
 PR & Advertising  
 Property Development  
 Radio & TV Studio  
 Real Estate  
 Recording & Sound Studio  
 Research & Development  
 Research Agency  
 Surveying

Table 4.2 (3) Typical Office Uses.

### Craftsman Industrial

Apparel & Finished Fabric Products  
 Bakery & Confections  
 Botanical Products  
 Brooms & Brushes  
 Commercial Scale Copying & Printing  
 Construction Special Trade Contractors  
 Electronics Assembly  
 Engraving  
 Electrical Fixtures  
 Fabricated Metal Products  
 Film Making  
 Furniture & Fixtures  
 Glass  
 Household Textiles  
 Jewelry, Watches, Clocks, & Silverware  
 Musical Instruments & Parts  
 Pasta  
 Pottery, Ceramics, & Related Products  
 Printing, Publishing & Allied Industries  
 Shoes & Boots  
 Signs & Advertising  
 Small Goods Manufacturing  
 Textile, Fabric, Cloth  
 Toys & Athletic Goods  
 Upholstery  
 Woodworking

Table 4.2 (4) Typical Craftsman Industrial Uses.

# 5.0 Building Types

## 5.1. Introduction to Building Type Standards.

### 1. Introduction.

The Building Types detailed in 5.0 Building Types outline the required building forms for new construction and renovated structures within the East Streetcar Neighborhood.

### 2. General Requirements.

All Building Types must meet the following requirements.

- (1) Zoning Districts. Each Building Type shall be constructed only within its designated subdistricts. Refer to Table 5.1 (1) Allowed Building Types by Subdistrict.
- (2) Uses. Each Building Type can house a variety of uses depending on the subdistrict in which it is located. Refer to 4.0 Uses for uses permitted per subdistrict. Some Building Types have additional limitations on permitted uses.
- (3) No Other Building Types. All buildings constructed must meet the requirements of one of the Building Types permitted within the zoning district of the lot.
- (4) Permanent Structures. All buildings constructed shall be permanent construction without a chassis, hitch, or wheels, or other features that would make the structure mobile, unless otherwise noted.
- (5) Accessory Structures.
  - (a) Attached accessory structures are considered part of the principal structure.
  - (b) Detached accessory structures are permitted per each Building Type and shall comply with all setbacks except the following:
    - (i) Detached accessory structures are not permitted in the front yard.
    - (ii) Detached accessory structures shall be located behind the principal structure in the rear yard.
    - (iii) Detached accessory structures shall not exceed the height of the principal structure.
  - (c) Accessory structures shall be built in a manner compatible with the primary building.

Building Types by Subdistrict				
		Subdistricts		
		State Street Gateway	North Haven	5th East Gateway
Building Types	Storefront	A	A	A
	Urban Style	A	A	A
	Townhome	A	A	A
	Mansion Style	A	A	A
	Civic Building	A	A	A
	Parking Structure	A	A	A
	Adaptive Reuse	A	A	A

Table 5.1 (1). Allowed Building Types by Subdistrict.

**KEY**

A Allowed

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## 5.2 Explanation of Building Type Table Standards.

The following explains and further defines the standards outlined on the tables for each Building Type, refer to 5.3 through 5.9.

### 1. Building Siting.

The following explains the line item requirements for each Building Type Table within the first section entitled "Building Siting".

- (1) Multiple Principal Structures. The allowance of more than one principal structure on a lot.
- (2) Front Property Line Coverage. Refer to Figure 5.2 (1). Measuring Front Property Line Coverage. Measurement defining the minimum percentage of street wall or building facade required along the street. The width of the principal structure(s) (as measured within the front build-to zone) shall be divided by the maximum width of the front build-to zone.
  - (a) Certain buildings have this number set to also allow the development of a courtyard along the front property line.
  - (b) Some building types allow side yard parking to be exempted from the front lot line coverage calculation. If such an exemption is permitted, the width of up to one double loaded aisle of parking, located with the drive perpendicular to the street and including adjacent sidewalks and landscaping, may be exempted, to a maximum of 65 feet. No parking, under any circumstances, is allowed along the S-Line.
- (3) Occupation of Corner. Occupying the intersection of the front and corner build-to zones with a principal structure.
- (4) Front Build-to Zone. The build-to zone or setback parallel to the front property line. Building components, such as awnings or signage, are permitted to encroach into the build-to zone.
  - (a) All build-to zone and setback areas not covered by building must contain either landscape, patio space, or sidewalk space.
- (5) Corner Build-to Zone. The build-to zone or setback parallel to the side property line.
  - (a) All build-to zone and setback areas not covered by building must contain either landscape, patio space, or sidewalk space.
  - (b) S-Line corners shall include a 10' triangular area measured from the intersection of the property lines intended as a corner plaza.
- (6) Minimum Side Yard Setback. The minimum required setback along a side property line.
- (7) Minimum Rear Yard Setback. The minimum required setback along a rear property line.
- (8) Minimum & Maximum Lot or Building Width. Depending on the Building Type, either the minimum or maximum building or unit width will be noted or the minimum and maximum width of a lot, all measured at or parallel to the front property line.
- (9) Maximum Impervious Coverage. (Refer to Figure 5.2(2)), Maximum Impervious & Semi-Pervious Coverage). The maximum

percentage of a lot permitted to be covered by principal structures, accessory structures, pavement, and other impervious surfaces.

- (10) Additional Semi-Pervious Coverage. The additional percentage of a lot beyond the Maximum Impervious Coverage may be surfaced in a semi-pervious material, including a green roof or pavers.
- (11) Parking & Loading Location. The yard in which a surface parking lot, detached garage, attached garage door access, loading and unloading, and associated drive is permitted.
- (12) Vehicular Access. The permitted means of vehicular ingress and egress to the lot.
  - (a) Lanes shall always be the primary means of access when present.
  - (b) When lanes are not present, a driveway may be permitted per Building Type and, if an alternative is available, shall not be located off a primary thoroughfare.

### 2. Height.

The following explains the line item requirements for each Building Type Table within the second section entitled "Height". Refer to figure 5.2 (3).

- (1) Minimum Overall Height. The minimum overall height for the building shall be located within the build-to zone; stories above the required minimum height may be stepped back from the facade.
- (2) Maximum Overall Height. The sum of a building's total height.
  - (a) Half stories are located dormer style completely within the roof structure with street-facing windows or in a visible basement exposed a maximum of one half story above ground. That portion which is visible above ground level shall be included in the overall height.
- (3) Single Family Zones Setbacks. In order to assure compatibility of new construction with adjacent single family zones, additional setbacks and a graduated setback are defined in 5.2 (4).
  - (a) Transitions from Single Family Homes. A 20-foot setback is required from the property line adjacent to a single family detached home. At 20 feet, 25-foot building height is permitted in between the property line and 30 feet. After 30 feet, every 2 feet in additional horizontal distance from the property line permits 1 foot of additional vertical building height. See figure 5.2 (4).
- (4) Ground Story and Upper Story, Minimum and Maximum Height. (Refer to Figure 5.2 (3). Measuring Height). Each frontage type includes a permitted range of height in feet for each story. Additional information is as follows:
  - (a) Floor height is measured in feet between the floor of a story to the floor of the story above it.
  - (b) Floor height requirements apply only to street facing facades.
  - (c) For single story buildings and the uppermost story of a multiple story building, floor to floor height shall be measured from the floor of the story to the tallest point of the ceiling.

# 5.0 Building Types

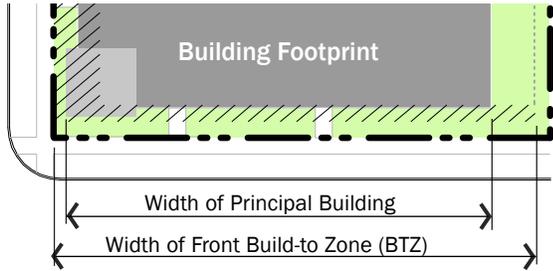


Figure 5.2 (1). Measuring Front Property Line Coverage.

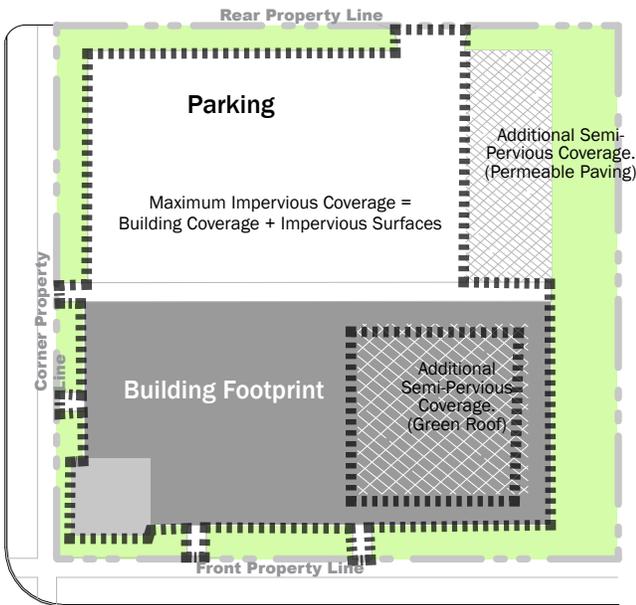


Figure 5.2 (2). Maximum Impervious & Additional Semi-Pervious Coverage.

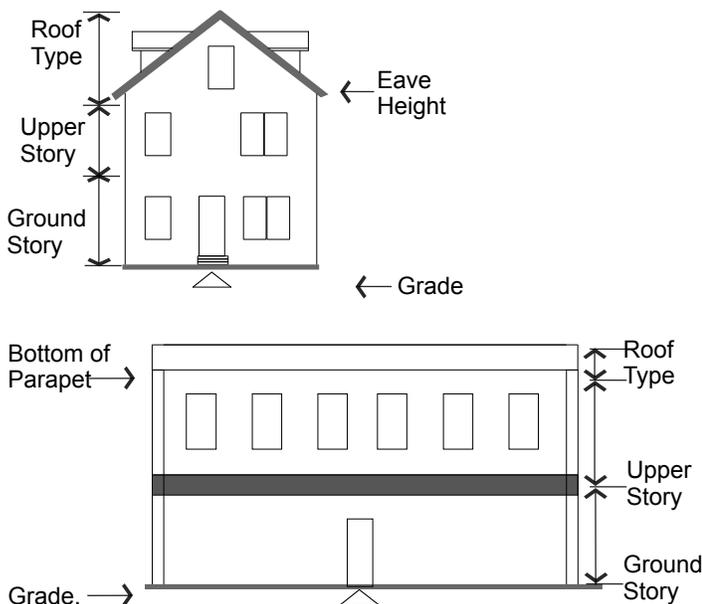


Figure 5.2 (3). Measuring Height.

### 3. Uses.

The following explains the line item requirements for each Building Type Table within the third section entitled “Uses.” Refer to Section 4.0. Uses for uses permitted within each Zoning District. The requirements in this section of the Building Type Tables may limit those uses within a specific Building Type.

- (1) Ground and Upper Story. The uses or category of uses which may occupy the ground and/or upper story of a building.
- (2) Parking Within Building. The area(s) of a building in which parking is permitted within the structure.
- (3) Required Occupied Space. The area(s) of a building that shall be designed as occupied space, defined as interior building space regularly occupied by the building users. It does not include storage areas, utility space, or parking.



Figure 5.2 (4). Transitions from Single Family Homes.



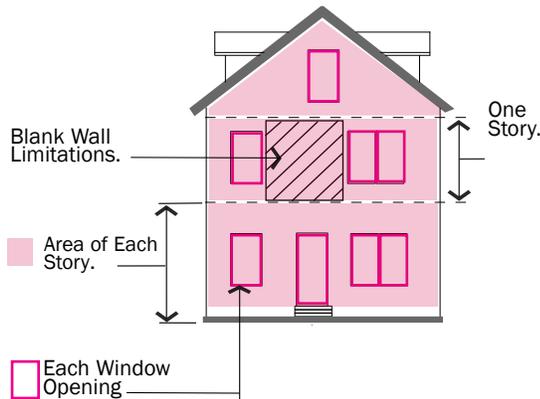
Figure 5.2 (5). Corner Building.

#### 4. Street Facade Requirements.

The following explains the line item requirements for each Building Type Table 5.3 through 5.8, within the fourth section entitled “Street Facade Requirements”. Street Facade Requirements apply only to facades facing a public or private right-of-way. The rear or interior side yard facades (except when facing a courtyard) are not required to meet these standards unless otherwise stated.



Measuring Ground Floor Transparency on a Storefront base.



Measuring Transparency on Each Story.

Figure 5.2 (6). Measuring Transparency.

- (1) Minimum Ground Story and Upper Floor Transparency. (Refer to Figure 5.2 (6), Measuring Transparency per Facade). The minimum amount of transparency required on street facades with street frontage.
  - (a) Transparency is any glass in windows and/or doors, including any mullions, that is highly transparent with low reflectance.
  - (b) Ground Story Transparency, when defined separately from the overall minimum transparency, shall be measured between two feet and eight feet from the average grade at the base of the front facade.
  - (c) A general Minimum Transparency requirement shall be measured from floor to floor of each story.
- (2) Blank Wall Limitations. A restriction of the amount of windowless area permitted on a facade with street frontage. If required, the following shall both be met for each story:
  - (a) No rectangular area greater than 30% of a story’s facade, as measured from floor to floor, may be windowless.
  - (b) No horizontal segment of a story’s facade greater than 15 feet in width may be windowless.
- (3) Entrance Type. The Entrance Type(s) permitted for the entrance(s) of a given Building Type. A mix of permitted Entrance Types may be utilized. Refer to 5.10 Entrance Types for definition of and additional requirements for each Entrance Type.
- (4) Principal Entrance Location. The facade on which the primary building entrance is to be located.
- (5) Required Number of Street Entrances. The minimum number of and maximum spacing between entrances on the ground floor building facade with street or S-Line frontage.
- (6) Vertical Facade Divisions. The use of a vertically oriented expression line or form to divide the facade into increments no greater than the dimension shown, as measured along the base of the facade. Elements may include a column, pilaster, facade setbacks, or other continuous vertical ornamentation a minimum of one and a half inch depth.
- (7) Horizontal Facade Divisions. The use of a horizontally oriented expression line or form to divide portions of the facade into horizontal divisions. Elements may include a cornice, belt course, molding, string courses, or other continuous horizontal ornamentation a minimum of one and a half inch depth.

#### 5. Roof Type.

The following explains the line item requirements for each Building Type Table in Sections 5.3 through 5.8, within the fifth section entitled “Roof Types”.

- (1) Permitted Roof Type. The roof type(s) permitted for a given Building Type. Refer to 5.11. Roof Types for more specific requirements.
- (2) Tower. A vertical building extension that may be permitted in conjunction with another roof type on certain Building Types and located on an S-Line corridor intersection. Refer to 5.11. Roof Types.

# 5.0 Building Types

## 5.3 Storefront Building.

### 1. Description & Intent.

The Storefront Building is intended for use as a mixed use building located close to the front and/or S-Line corridor property line with parking typically in the rear or side of the lot.

The key facade element of this Building Type is the storefront required on the ground floor front facade, with large amounts of glass and regularly spaced entrances. This building type is encouraged near the S-Line corridor intersections.

This building is available in a variety of heights, depending on the district within which it is located. For example, maximum heights are highest in the State Street subdistrict and lowest in the North Haven subdistrict.

### 2. Regulations.

Regulations for the Storefront Building Type are defined in the adjacent table.

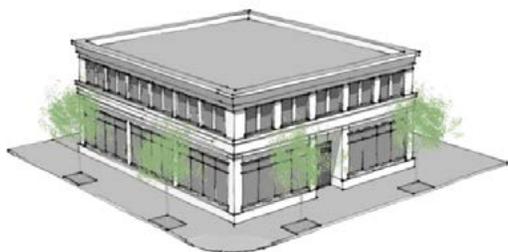


Figure 5.3 (1). Sample Illustration of the Storefront Building.

### Notes

<sup>A</sup> Lots wider than 140 feet are permitted one double-loaded aisle of parking (maximum width of 72 feet), located perpendicular to the front property line, which is exempt from front property line coverage.

<sup>B</sup> Above the third story, the upper stories of any building facade with street frontage shall have a step back from the lower stories that is a minimum of six feet.

<sup>C</sup> If 18 feet or more in height, ground story shall count as two stories towards maximum building height.

<sup>D</sup> A one-story minimum height is acceptable for properties located at 290 E Wentworth Avenue and 2220 South 300 East, but nowhere else.

	Permitted Districts		
	State Street Gateway	North Haven	5th East Gateway

	State Street Gateway	North Haven	5th East Gateway
<b>(1) Building Siting</b>			
Multiple Principal Buildings	A	A	A
Occupation of Corner	A	A	A
Front Build-to Zone on S-Line	15-20'	15-20'	15-20'
Front Build to Zone on Public ROW	5-15'	5-15'	5-15'
Non S-Line Corner Build-to Zone	5-10'	5-10'	5-10'
Minimum Side Yard Setback	0'	0'	0'
Minimum Rear Yard Setback	5'	20'	20'
S-Line Corridor Intersection	15' from property line intersection <sup>1</sup>		
Parking & Loading Location	Not on S-Line	Not on Frontages	
Vehicular Access	Not on S-Line, Not within 75'		
Permitted Location	No restriction; promoted within 200' of S-Line corridor intersection		
<b>(2) Height</b> Refer to Figure 5.3 (2).			
Minimum Overall Height	26' <sup>3</sup>	26' <sup>3</sup>	26' <sup>3</sup>
Maximum Overall Height	62'	50'	50'
Ground Story: Minimum Height	14' <sup>4</sup>	14' <sup>4</sup>	14' <sup>4</sup>
Maximum Height	16'	16'	16'
Upper Stories: Minimum Height	9'	9'	9'
Maximum Height	14'	14'	14'
Transitions from single family homes	Refer to section 5.2.2 (d)		
<b>(3) Uses</b> Refer to Figure 5.3 (2). Refer to 4.0 Uses for permitted uses.			
Ground Story	Office, Neigh. Retail, Neigh. Service	Office, Neighborhood Retail, Neighborhood Service, Residential	
Upper Story	Office, Neighborhood Service, Residential		
Parking within Building	Tuck Under Parking Allowed		
Required Occupied Space	30' Required within 200' of S-Line		
<b>(4) Street Facade Requirements</b>			
Minimum Ground Story Transparency <small>Measured between 2' and 8' above grade</small>	65%	65%	65%
Minimum Transparency <small>per each additional Story</small>	25%	25%	25%
Blank Wall Limitations	Required, see 5.2.4 (2)		
Front Facade Entrance Type	Storefront, arcade		
Principal Entrance Location	Required on all Frontages including S-Line		
Required Number of Street Entrances	One per every 75' of Frontages		
Facade Depth Variation	Minimum of every 50' <sup>2</sup>		
<b>(5) Roof Type Requirements</b>			
Permitted Roof Types	Parapet, Flat, Pitched, Shed		
Tower	Flat, Pitched		

### Key

<sup>1</sup>: Subject to City Engineer review for compliance with UTA Line of Sight requirements

<sup>2</sup>: 1 ft. minimum depth

<sup>3</sup>: 18' beyond 200' of the S-Line corridor intersections

<sup>4</sup>: May be reduced to 9' beyond 200' of an S-Line Corridor Intersection

## 5.4 Urban Style.

### 1. Description & Intent.

The Urban Style Multiple Family Building Type permits a wide range of building facades and allows for more flexibility in building height. It can accommodate mixed uses or can be used strictly for residential.

This Building Type is still intended to be built close to the front and corner property lines, but some setback is required along the S-Line corridor to enhance the corridor and to allow easy access to passing pedestrians and transit riders. Parking may be provided in the rear of the lot, internally in the building, or to the side for one double loaded aisle of parking.

The minimum and maximum heights of this Building Type depend on the subdistrict within which it is located: taller heights are permitted in the State Street subdistrict.

### 2. Regulations.

Regulations for the Urban Style Type are defined in the adjacent table.



Figure 5.4 (1). Sample Illustration of the Urban Style Building.

#### Notes

<sup>A</sup> Lots wider than 140 feet are permitted one double-loaded aisle of parking (maximum width of 72 feet), located perpendicular to the front property line, which is exempt from front property line coverage.

<sup>B</sup> Upper stories above the third story on any building facade with street frontage or S-Line shall have a step back from the lower stories that is a minimum of six feet.

<sup>C</sup> If 18 feet or more in height, ground story shall count as two stories towards maximum building height.

	Permitted Districts		
	State Street Gateway	North Haven	5th East Gateway

	State Street Gateway	North Haven	5th East Gateway
<b>(1) Building Siting</b>			
Multiple Principal Buildings	A	A	A
Occupation of Corner	A	A	A
Front Build-to Zone on S-Line	15-20'	15-20'	15-20'
Front Build-to Zone on Public ROW	5-15'	5-15'	5-15'
Non S-Line Corner Build-to Zone	5-10'	5-10'	5-10'
Minimum Side Yard Setback	0'	0'	0'
Minimum Rear Yard Setback	5'	20'	20'
S-Line Corridor Intersection	15' from property line intersection <sup>1</sup>		
Parking & Loading Location	Not on S-Line	Not on Frontages	
Vehicular Access	Not within 75' of S-Line corridor intersections		
<b>(2) Height</b> Refer to Figure 5.3 (2).			
Minimum Overall Height	23' <sup>3</sup>	23' <sup>3</sup>	23' <sup>3</sup>
Maximum Overall Height	62'	50'	50'
Ground Story: Minimum Height	14' <sup>4</sup>	14' <sup>4</sup>	14' <sup>4</sup>
Maximum Height	16'	16'	16'
Upper Stories: Minimum Height	9'	9'	9'
Maximum Height	14'	14'	14'
Transitions from single family homes	Refer to section 5.2.2 (d)		
<b>(3) Uses</b> Refer to Figure 5.3 (2). Refer to 4.0 Uses for permitted uses.			
Ground Story	Office, Neighborhood Retail, Neighborhood Service, Residential		
Upper Stories	Residential, Office, Neighborhood Service		
Parking within Building	Tuck Under Parking Allowed		
Required Occupied Space	30' Required within 200' of S-Line		
<b>(4) Street Facade Requirements</b>			
Minimum Ground Story Transparency Measured between 2' and 8' above grade	65%	65%	65%
Minimum Transparency per each additional Story	25%	25%	25%
Blank Wall Limitations	Required, see 5.2.4 (2)		
Front Facade Entrance Type	Storefront <sup>5</sup> , stoop, porch		
Principal Entrance Location	Required on all Frontages including S-Line		
Required Number of Street Entrances	One per every 75' of Frontages		
Facade Depth Variation	Minimum of every 50' <sup>2</sup>		
<b>(5) Roof Type Requirements</b>			
Permitted Roof Types	Parapet, Flat		
Tower	Flat, Pitched		

#### Key

<sup>1</sup>: Subject to City Engineer review for compliance with UTA Line of Sight requirements

<sup>2</sup>: 1 ft. minimum depth

<sup>3</sup>: 18' beyond 200' of the S-Line corridor intersections

<sup>4</sup>: May be reduced to 9' beyond 200' of an S-Line Corridor Intersection

<sup>5</sup>: Permitted only on S-Line Corridor Intersections or buildings designed for live-work

# 5.0 Building Types

## 5.5 Townhome.

### 1. Description & Intent.

The Townhome is a building typically comprised of multiple vertical units, each with its own entrance to the street. This Building Type may be organized as townhouses or row houses, and could also incorporate live/work units.

No more than three garages in a row may face the primary street or parking is required to be located in the rear yard and may be incorporated either into a detached garage or in an attached garage accessed from the rear of the building. When the garage is located within the building, a minimum level of occupied space is required on the front facade to ensure that the street facade is active.

### 2. Regulations.

Regulations for the Townhome type are defined in the adjacent table.



Figure 5.5 (1). Sample Illustration of the Townhome Building.

#### Notes:

<sup>A</sup> The townhome consists of a series of two or more units. When permitted, multiple buildings may be located on a lot with the minimum required space between them. However, each building shall meet all requirements of the Building Type unless otherwise noted.

<sup>B</sup> Each building shall meet the front property line coverage requirement, except one of every five units may front a courtyard with a minimum width of 30 feet. The courtyard shall be defined on three sides by units.

<sup>C</sup> Rear yard setback on alleys is five feet.

<sup>D</sup> When the storefront entrance type is utilized, the maximum ground story transparency for the unit is 55% as measured between two feet and eight feet above grade.

<sup>E</sup> The storefront entrance type is permitted only on corners or buildings that are designated for live/work units.

	Permitted Districts		
	State Street Gateway	North Haven	5th East Gateway
<b>(1) Building Siting</b>			
Multiple Principal Buildings	A	A	A
Occupation of Corner	A	A	A
Front Build-to Zone on S-Line	15-20'	15-20'	15-20'
Front Build-to Zone on Public ROW	5-15'	5-15'	5-15'
Non S-Line Corner Build-to Zone	5-10'	5-10'	5-10'
Minimum Side Yard Setback	5'	5'	5'
Minimum Rear Yard Setback	5'	20'	20'
S-Line Corridor Intersection	15' from property line intersection <sup>1</sup>		
Parking & Loading Location	Not on S-Line	Not on Frontages	
Vehicular Access	Not within 75' of S-Line corridor intersections		
<b>(2) Height</b> Refer to Figure 5.3 (2).			
Minimum Overall Height	23' <sup>3</sup>	23' <sup>3</sup>	23' <sup>3</sup>
Maximum Overall Height	62'	50'	50'
Ground Story: Minimum Height	9'	9'	9'
Maximum Height	14'	14'	14'
Upper Stories: Minimum Height	9'	9'	9'
Maximum Height	14'	14'	14'
Transitions from single family homes	Refer to section 5.2.2 (d)		
<b>(3) Uses</b> Refer to Figure 5.3 (2). Refer to 4.0 Uses for permitted uses.			
Ground Story	Office, Neighborhood Retail, Neighborhood Service, Residential		
Upper Story	Residential		
Parking within Building	Tuck Under Parking Allowed, garages		
Required Occupied Space	30' Required within 200' of S-Line		
<b>(4) Street Facade Requirements</b>			
Minimum Ground Story Transparency Measured between 2' and 8' above grade	25%	25%	25%
Minimum Transparency per each additional Story	25%	25%	25%
Blank Wall Limitations	Required, see 5.2.4 (2)		
Front Facade Entrance Type	Storefront <sup>4</sup> , stoop, porch		
Principal Entrance Location	Required on all Frontages including S-Line		
Required Number of Street Entrances	One per unit		
Facade Depth Variation	Every two units <sup>2</sup>		
<b>(5) Roof Type Requirements</b>			
Permitted Roof Types	Parapet, Flat, Pitched		
Tower	Flat, Pitched		

#### Key

<sup>1</sup>: Subject to City Engineer review for compliance with UTA Line of Sight requirements

<sup>2</sup>: 1 ft. minimum depth

<sup>3</sup>: 18' beyond 200' of the S-Line corridor intersections

<sup>4</sup>: Permitted only on S-Line Corridor Intersections or buildings designed for live-work

## 5.6 Mansion Style.

### 1. Description & Intent.

The Mansion Style Building is primarily residential building, incorporating a landscaped yard surrounding all sides of the building. Parking and garages are limited to the rear only with preferred access from an alley.

The Mansion Style Building can be utilized in newly developing locations to create somewhat denser traditional neighborhoods, or as a buffer to existing neighborhoods.

### 2. Regulations.

Regulations for the Mansion Style are defined in the adjacent table.

#### Notes

<sup>A</sup> Each building shall meet all requirements of the Building Type.

<sup>B</sup> When multiple buildings are located on a single lot, each building shall meet the front property line coverage requirement, except one of every three buildings may front a courtyard with a minimum width of 30 feet. The courtyard shall be defined on three sides by units.

<sup>C</sup> Rear yard setback for detached garages on alleys is five feet.

	Permitted Districts		
	State Street Gateway	North Haven	5th East Gateway
<b>(1) Building Siting</b>			
Multiple Principal Buildings	A	A	A
Occupation of Corner	A	A	A
Front Build-to Zone on S-Line	15-20'	15-20'	15-20'
Front Build-to Zone on Public ROW	5-15'	5-15'	5-15'
Non S-Line Corner Build-to Zone	5-10'	5-10'	5-10'
Minimum Side Yard Setback	5'	5'	5'
Minimum Rear Yard Setback	5'	20'	20'
S-Line Corridor Intersection	15' from property line intersection <sup>1</sup>		
Parking & Loading Location	Not on S-Line	Not on Frontages	
Vehicular Access	Not within 75' of an S-Line corridor intersection		
Permitted Location	Not allowed within 200' of S-Line corridor intersections		
<b>(2) Height</b> Refer to Figure 5.3 (2).			
Minimum Overall Height	18'	18'	18'
Maximum Overall Height	35'	35'	35'
Ground Story: Minimum Height	9'	9'	9'
Maximum Height	14'	14'	14'
Upper Stories: Minimum Height	9'	9'	9'
Maximum Height	14'	14'	14'
Transitions from single family homes	Refer to section 5.2.2 (d)		
<b>(3) Uses</b> Refer to Figure 5.3 (2). Refer to 4.0 Uses for permitted uses.			
Ground Story	Office, Neighborhood Retail, Neighborhood Service, Residential		
Upper Story	Residential		
Parking within Building	Tuck Under Parking Allowed		
Required Occupied Space	30' Required within 200' of S-Line		
<b>(4) Street Facade Requirements</b>			
Minimum Ground Story Transparency Measured between 2' and 8' above grade	25%	25%	25%
Minimum Transparency per each additional Story	25%	25%	25%
Blank Wall Limitations	Required, see 5.2.4 (2)		
Front Facade Entrance Type	Storefront, stoop, porch		
Principal Entrance Location	When adjacent to S-Line, oriented on S-Line Corridor; otherwise orient to public/private ROW		
Required Number of Street Entrances	Minimum of one		
Facade Depth Variation	Minimum of every 50' <sup>2 3</sup>		
<b>(5) Roof Type Requirements</b>			
Permitted Roof Types	Parapet, Flat, Pitched		
Tower	Flat, Pitched		

#### Key

<sup>1</sup>: Subject to City Engineer review for compliance with UTA Line of Sight requirements

<sup>2</sup>: 1 ft. minimum depth

<sup>3</sup>: May be accomplished through inclusion of a front porch

# 5.0 Building Types

## 5.7 Civic Building.

### 1. Description & Intent.

The Civic Building is intended only for civic and institutional use types. These buildings are distinctive within the urban fabric created by the other Building Types and could be designed as iconic structures. Parking is limited to the rear in most cases.

The minimum and maximum heights of this Building Type depend on the subdistrict within which it is located.

### 2. Regulations.

Regulations for the Civic Building type are defined in the adjacent table.

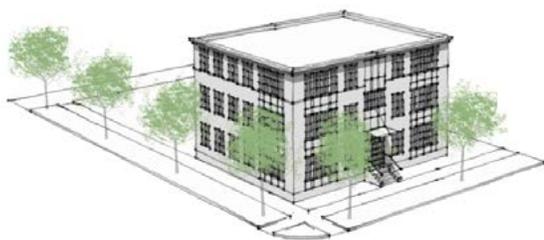


Figure 5.7 (1). Sample Illustration of the Civic Building.

#### Notes

<sup>A</sup> Lots wider than 140 feet are permitted one double-loaded aisle of parking (maximum width of 72 feet), located perpendicular to the front property line, which is exempt from front property line coverage.

<sup>B</sup> If 18 feet or more in height, ground story shall count as two stories towards maximum building height.

	Permitted Districts		
	State Street Gateway	North Haven	5th East Gateway
<b>(1) Building Siting</b>			
Multiple Principal Buildings	A	A	A
Occupation of Corner	A	A	A
Front Build-to Zone on S-Line Front Build-to Zone on Public ROW	15-20' 5-15'	15-20' 5-15'	15-20' 5-15'
Non S-Line Corner Build-to Zone	5-10'	5-10'	5-10'
Minimum Side Yard Setback	5'	5'	5'
Minimum Rear Yard Setback	5'	20'	20'
S-Line Corridor Intersection	15' from property line intersection <sup>1</sup>		
Parking & Loading Location	Not on S-Line	Not on Frontages	
Vehicular Access	Not within 75' of S-Line corridor intersections		
<b>(2) Height</b> Refer to Figure 5.3 (2).			
Minimum Overall Height	23'	23'	23'
Maximum Overall Height	62'	50'	50'
Ground Story: Minimum Height Maximum Height	14' 62'	14' 50'	14' 50'
Upper Stories: Minimum Height Maximum Height	9' 14'	9' 14'	9' 14'
Transitions from single family homes	Refer to section 5.2.2 (d)		
<b>(3) Uses</b> Refer to Figure 5.3 (2). Refer to 4.0 Uses for permitted uses.			
Ground Story	Civic uses only		
Upper Story	Civic uses only		
Parking within Building	Tuck Under Parking Allowed		
Required Occupied Space	30' Required within 200' of S-Line		
<b>(4) Street Facade Requirements</b>			
Minimum Ground Story Transparency Measured between 2' and 8' above grade	10%	10%	10%
Minimum Transparency per each additional Story	10%	10%	10%
Blank Wall Limitations	Required, see 5.2.4 (2)		
Front Facade Entrance Type	Arcade, stoop, storefront		
Principal Entrance Location	When adjacent to S-Line, oriented on S-Line Corridor; otherwise orient to public/private ROW		
Required Number of Street Entrances	Minimum of one		
Facade Depth Variation	Not required		
<b>(5) Roof Type Requirements</b>			
Permitted Roof Types	Flat, Pitched		
Tower	Flat, Pitched		

#### Key

<sup>1</sup>: Subject to City Engineer review for compliance with UTA Line of Sight requirements

## 5.8 Parking Structure.

### 1. Description & Intent.

Parking Structures are encouraged within the East Streetcar Neighborhood. When proposed adjacent to a street or the S-Line corridor, first floors shall be designed to accommodate active non-parking uses.

The minimum and maximum heights of this Building Type depend on the subdistrict within which it is located.

### 2. Regulations.

Regulations for the Parking Structure type are defined in the adjacent table.



Figure 5.8 (1). Parking Structure Examples

	Permitted Districts		
	State Street Gateway	North Haven	5th East Gateway

<b>(1) Building Siting</b>			
Multiple Principal Buildings	Not Allowed	Not Allowed	Not Allowed
Occupation of Corner	Not Allowed	Not Allowed	Not Allowed
Front Build-to Zone on S-Line	15-20'	15-20'	15-20'
Front Build-to Zone on Public ROW	5-15'	5-15'	5-15'
Non S-Line Corner Build-to Zone	5-10'	5-10'	5-10'
Minimum Side Yard Setback	0'	0'	0'
Minimum Rear Yard Setback	5'	20'	20'
Parking & Loading Location	Not on S-Line corridor		
Vehicular Access	Not within 75' of S-Line corridor		
Permitted Location	Not allowed on lots adjacent to single family homes; not allowed directly adjacent to S-Line corridor or public street within 200' of S-Line corridor intersection		
<b>(2) Height</b> Refer to Figure 5.3 (2).			
Minimum Overall Height	2 story minimum	2 story minimum	2 story minimum
Maximum Overall Height	62'	50'	50'
Ground Story: Minimum Height Maximum Height	Not specified		
Upper Stories: Minimum Height Maximum Height	Not specified		
Transitions from single family homes	Refer to section 5.2.2 (d)		
<b>(3) Uses</b> Refer to Figure 5.3 (2). Refer to 4.0 Uses for permitted uses.			
Ground Story	Office, Neighborhood Retail, Neighborhood Service required within 30' of public street		
Upper Story	Parking		
Parking within Building	Permitted		
Required Occupied Space	30' Required within 200' of S-Line		
<b>(4) Street Facade Requirements</b>			
Minimum Ground Story Transparency Measured between 2' and 8' above grade	65%	65%	65%
Minimum Transparency per each Story	30%	30%	30%
Blank Wall Limitations	Required, see 5.2.4 (2)		
Front Facade Entrance Type	Storefront, arcade when alternative uses exist on the ground floor		
Principal Entrance Location	When alternative uses exist on the ground floor, orient entrances to S-Line Corridor		
Required Number of Street Entrances	When alternative uses exist on the ground floor, one per 75 feet		
Facade Depth Variation	When alternative uses exist on the ground floor, minimum of every 50' <sup>1</sup> Not required on upper stories		
<b>(5) Roof Type Requirements</b>			
Permitted Roof Types	Flat		
Tower	Flat, pitched		

Key  
<sup>1</sup>: 1 ft. minimum depth

# 5.0 Building Types

## 5.9 Adaptive Reuse.

### 1. Description & Intent.

Many of the existing buildings within the East Streetcar Neighborhood have the potential to be reworked into a new use. The warehouses and other buildings have value and could change to residential, office or other uses. Adaptive reuse is encouraged with the following general requirements:

- (1) Reconstruction should attempt to retain much of the character of the existing building.
- (2) Lighting should reflect the historic nature of the building.
- (3) Windows shall emphasize the adjacent street and the S-Line corridor and encompass at least 50% of first story building facades.
- (4) Parking shall be located to the rear of the building and may front on the S-Line corridor if a frontage buffer (see 7.2) or a public landscaped open space is provided.
- (5) The Land Use Authority may waive certain requirements for buildings that existed prior to the adoption of this code.

The maximum height of an adaptive reuse building is five stories in the State Street subdistrict, and four stories in the North Haven and 500 East sub-districts. Heights adjacent to single family residential zones shall be regulated by the same formula used in previous building types.

#### Notes

<sup>A</sup> Lots wider than 140 feet are permitted one double-loaded aisle of parking (maximum width of 72 feet), located perpendicular to the front property line, which is exempt from front property line coverage.

<sup>B</sup> If 18 feet or more in height, ground story shall count as two stories towards maximum building height.



Figure 5.9 (1). Examples of Adaptive Reuse.

## 5.10 Entrance Types.

Entrance type standards apply to the ground story and visible basement of front facades of all Building Types as defined in this Section. Refer to the Building Type Table Requirements, Sections 5.3 through 5.8.

### 1. General.

The following provisions apply to all entrance types.

- (1) Intent. To guide the design of the ground story of all buildings to relate appropriately to pedestrians on the street. Treatment of other portions of the building facades is detailed in each Building Type standard (refer to Building Types 5.3 through 5.9).
- (2) Applicability. The entire ground story street-facing facade(s) of all buildings shall meet the requirements of at least one of the permitted entrance types, unless otherwise stated.
- (3) Measuring Transparency. Refer to 5.2 Explanation of Building Type Table Standards, for information on measuring building transparency.
- (4) Visible Basements. Visible basements, permitted by entrance type, are optional. The visible basement shall be a maximum of one-half the height of the tallest story.

### 2. Storefront Entrance Type.

(Refer to Figure 5.10 (1)). The Storefront entrance type is a highly transparent ground story treatment designed to serve primarily as the

display area and primary entrance for retail or service uses.

- (1) Transparency. Minimum transparency is required per Building Type.
- (2) Elevation. Storefront elevation shall be between zero and one foot above sidewalk.
- (3) Visible Basement. A visible basement is not permitted.
- (4) Horizontal Facade Division. Horizontally define the ground story facade from the upper stories.
- (5) Entrance. All entries shall be recessed from the front facade closest to the street.
  - (a) Recess shall be a minimum of three feet and a maximum of eight feet deep, measured from the portion of the front facade closest to the street.
  - (b) When the recess falls behind the front build-to zone, the recess shall be no wider than eight feet.

### 3. Arcade Entrance Type.

(Refer to Figure 5.10 (2)). An Arcade entrance type is a covered pedestrian walkway within the recess of a ground story; only allowed on south facing facades.

- (1) Arcade. An open-air public walkway is required from the face of the building recessed into the building a minimum of eight and a maximum of 15 feet.

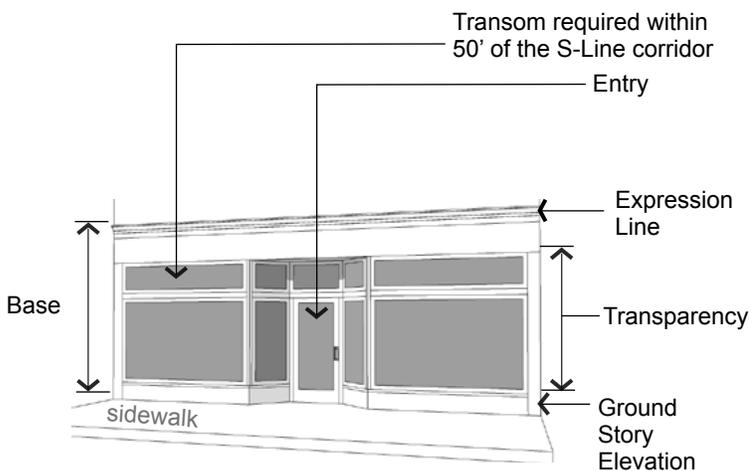


Figure 5.10 (1). Storefront Entrance Type

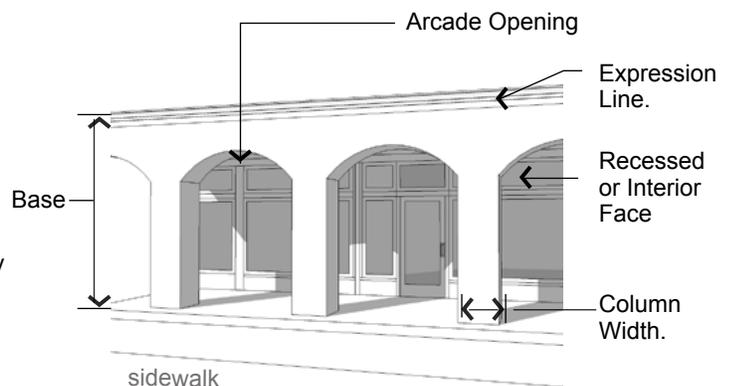


Figure 5.10 (2). Arcade Entrance Type

# 5.0 Building Types

- (2) Build-to Zone. When the Arcade is utilized, the outside face of the Arcade shall be considered the front facade, located within the required build-to zone.
- (3) Recessed or Interior Facade. Storefront entrance type is required on the recessed ground story facade.
- (4) Column Spacing. Columns shall be spaced between ten feet and 12 feet on center.
- (5) Column Width. Columns shall be a minimum of 1'-8" and a maximum 2'-4" in width.
- (6) Arcade Opening. Opening shall not be flush with interior arcade ceiling and may be arched or straight.
- (7) Horizontal Facade Division. Horizontally define the ground story facade from the upper stories.
- (8) Visible Basement. A visible basement is not permitted.

## 4. Stoop Entrance Type.

(Refer to Figure 5.10 (3)). A stoop is an unroofed, open platform.

- (1) Transparency. Minimum transparency is required per Building Type.
- (2) Stoop Size. Stoops shall be a minimum of three feet deep and six feet wide.

- (3) Elevation. Stoop elevation shall be located a maximum of 2'-6" above the sidewalk without visible basement and a maximum of 4'-6" above the sidewalk with a visible basement.
- (4) Visible Basement. A visible basement is permitted and shall be separated from the ground story by an expression line.
- (5) Entrance. All entries shall be located off a stoop.

## 5. Porch Entrance Type.

(Refer to Figure 5.10 (4)). A porch is a raised, roofed platform that may or may not be enclosed on all sides. If enclosed, the space shall not be climate controlled.

- (1) Transparency.
  - (a) Minimum transparency per Building Type is required.
  - (b) If enclosed, a minimum of 40% of the enclosed porch shall be comprised of highly transparent, low reflectance windows.
- (2) Porch Size. The porch shall be a minimum of five feet deep and eight feet wide.
- (3) Elevation. Porch elevation shall be located a maximum of 2'-6" above the sidewalk without a visible basement and a maximum of 4'-6" above the sidewalk with a visible basement.
- (4) Visible Basement. A visible basement is permitted.
- (5) Height. Porch may be two stories to provide a balcony on the second floor.
- (6) Entrance. All entries shall be located off a porch.

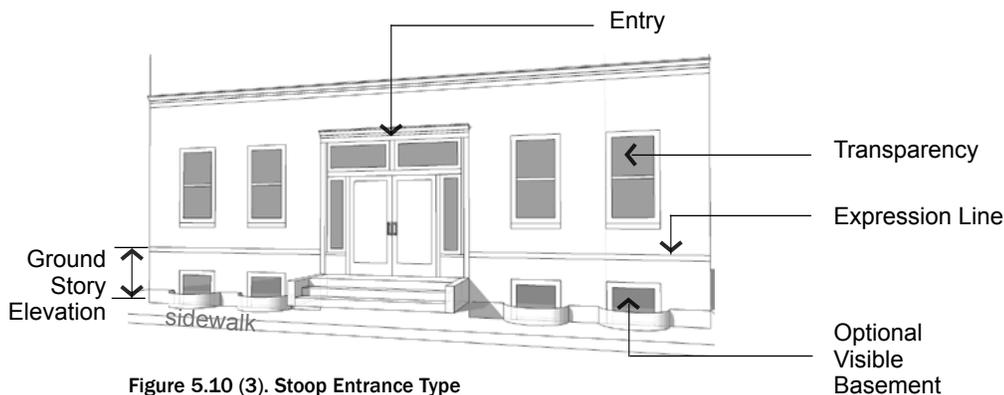


Figure 5.10 (3). Stoop Entrance Type

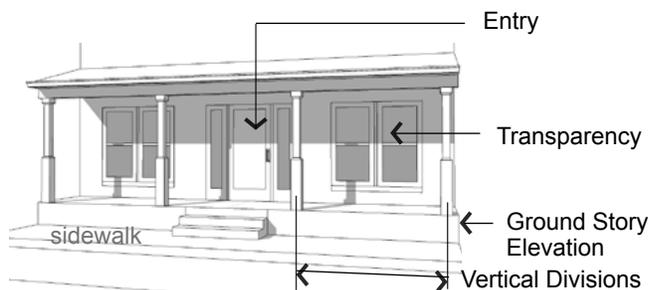


Figure 5.10 (4). Porch Entrance Type

## 5.11 Roof Types.

Roof type standards apply to the roof and cap of all Building Types as defined in this Section. Refer to the Building Type Table Requirements, Sections 5.3 through 5.8.

### 1. General Provisions.

The following provisions apply to all roof types.

- (1) Intent. To guide the design of the caps of all buildings.
- (2) Applicability. All buildings shall meet the requirements of one of the roof types permitted for the Building Type.
- (3) Measuring Height. Refer to Section 5.2.2 for information on measuring building height.
- (4) Other Roof Types. Other building caps not listed as a specific type may be made by a request to the Land Use Authority with the following requirements:
  - (a) The roof type shall not create additional occupied space beyond that permitted by the Building Type, except for private open space.
  - (b) The shape of the Roof Type shall be significantly different from those defined in this section 5.11 Roof Types, i.e. a dome, spire, vault.
- (5) Solar panels are permitted for all roof types.

### 2. Parapet Roof Type.

(Refer to Figure 5.11 (1), Parapet Roof Type). A parapet is a low wall projecting above a building's roof along the perimeter of the building. It can be utilized with a flat or low pitched roof and also serves to limit the view of roof-top mechanical systems from the street.

- (1) Parapet Height. Height is measured from the top of the upper story to the top of the parapet.

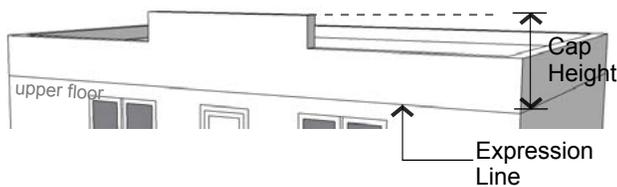


Figure 5.11 (1). Parapet Roof Type

- (a) Minimum height is two feet with a maximum height of six feet.
  - (b) The parapet shall be high enough to screen the roof and any roof appurtenances from view of the street(s).
- (2) Horizontal Expression Lines. An expression line shall define the parapet from the upper stories of the building and shall also define the top of the cap.
  - (3) Occupied Space. Occupied space shall not be incorporated behind this roof type.

### 3. Pitched Roof Type.

(Refer to Figure 5.11 (2), Pitched Roof Type). This roof type has a sloped or pitched roof. Slope is measured with the vertical rise divided by the horizontal span or run.

- (1) Pitch Measure. The roof may not be sloped less than a 4:12 (rise/run) or more than 6:12.
  - (a) Slopes less than 4:12 are permitted to occur on second story or higher roofs. (Refer to Figure 5.11 (2) - Low Pitched Roof).
- (2) Configurations.
  - (a) Hipped, gabled, and combination of hips and gables with or without dormers are permitted.
  - (b) Butterfly roofs (inverted gable roof) are permitted with a maximum height of eight feet, inclusive of overhang.
  - (c) Gambrel and mansard roofs are not permitted.
- (3) Parallel Ridge Line. A gabled end or perpendicular ridge line shall occur at least every 100 feet of roof when the ridge line runs parallel to the front lot line. (Refer to Figure 5.11 (3). Parallel Ridge Line).

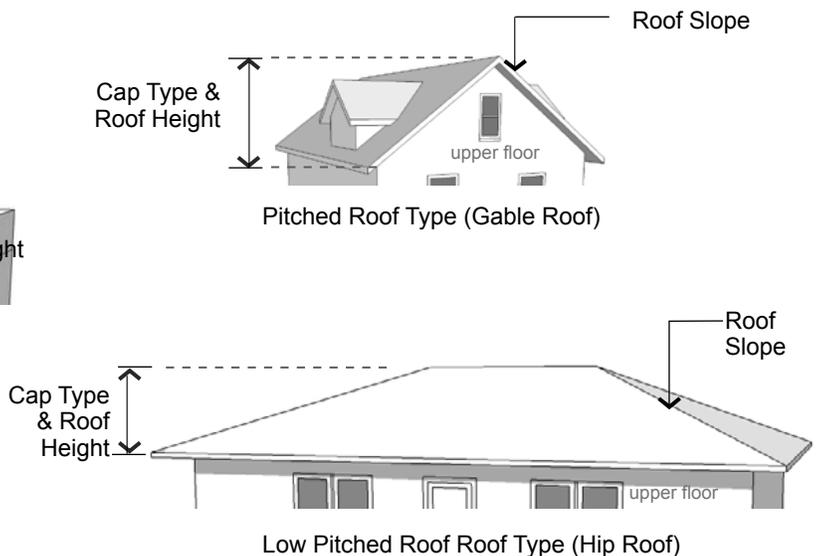


Figure 5.11 (2). Pitched Roof Type

# 5.0 Building Types

- (4) Roof Height. Roofs without occupied space and/or dormers shall have a maximum height on street-facing facades equal to the maximum floor height permitted for the Building Type.
- (5) Occupied Space. Occupied space may be incorporated behind this roof type.

## 4. Flat Roof Type.

(Refer to Figure 5.11 (5). Flat Roof Type). This roof type has a flat roof with overhanging eaves.

- (1) Configuration. Roofs with no visible slope are acceptable. Eaves are required on all street facing facades.
- (2) Eave Depth. Eave depth is measured from the building facade to the outside edge of the eave. Eaves shall have a depth of at least 14 inches.
- (3) Eave Thickness. Eave thickness is measured at the outside edge of the eave, from the bottom of the eave to the top of the eave. Eaves shall be a minimum of eight inches thick.
- (4) Interrupting Vertical Walls. Vertical walls may interrupt the eave and extend above the top of the eave with no discernible cap.
  - (a) No more than one-half of the front facade can consist of an interrupting vertical wall.
  - (b) Vertical walls shall extend no more than four feet above the top of the eave.
- (5) Occupied Space. Occupied space shall not be incorporated behind this roof type.



Figure 5.11 (3). Parallel Ridge Line

## 5. Towers.

(Refer to Figure 5.11 (4)). A tower is a rectilinear or cylindrical, vertical element, that must be used with other roof types; towers are only allowed on S-Line corridor intersections.

- (1) Quantity. All Building Types, with the exception of the Civic Building, are limited to one tower per building.
- (2) Tower Height. Maximum height, measured from the top of the parapet or eave to the top of the tower, is the equivalent of the height of one upper floor of the building to which the tower is applied.
- (3) Tower Width. Maximum width along all facades is one-third the width of the front facade or 30 feet, whichever is less.
- (4) Horizontal Expression Lines. An expression line shall define the tower from the upper stories, except on single family or attached house residential Building Types.
- (5) Occupied Space. Towers may be occupied by the same uses allowed in upper stories of the Building Type to which it is applied.
- (6) Application. May be combined with all other roof types.
- (7) Tower Cap. The tower may be capped by the parapet, pitched, low pitched, or flat roof types, or the spire may cap the tower.

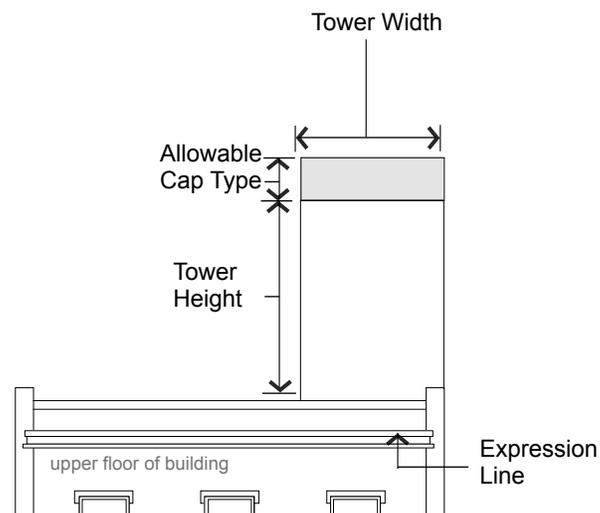


Figure 5.11 (4). Tower

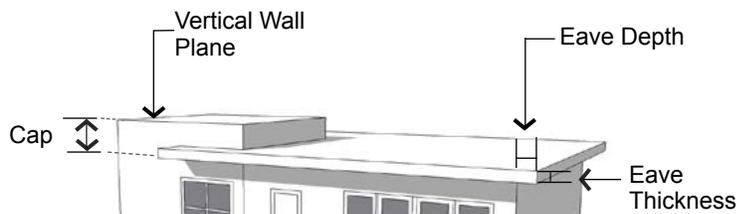


Figure 5.11 (5). Flat Roof Type

## 5.12 Additional Design Standards.

The following outlines the East Streetcar Neighborhood design standards that affect a building's appearance and place cohesiveness. They improve the physical quality of buildings, enhance the pedestrian experience, and protect the character of the neighborhood.

### 1. Materials and Color.

- (1) Primary Facade Materials. 80% of each facade shall be constructed of primary materials. For facades over 100 square feet, more than one material shall be used to meet the 80% requirement.
  - (a) Permitted primary building materials include high quality, durable, natural materials, such as stone, brick; wood lap siding; fiber cement board lapped, shingled, or panel siding; glass. On Mansion style and Townhome style buildings, up to 40% of an exterior façade may be stucco. Other high quality synthetic materials may be approved during the site plan process with an approved sample and examples of successful, high quality local installations. Refer to Figure 5.12 (1).



Primary Materials: Brick



Primary Materials: Stone



Primary Materials: Painted Wood

Figure 5.12 (1). Primary Materials.

- (2) Secondary Facade Materials. Secondary materials are limited to details and accents and include gypsum reinforced fiber concrete for trim and cornice elements; metal for beams, lintels, trim, and ornamentation, and exterior architectural metal panels and cladding.
  - (a) Exterior Insulation and Finishing Systems (EIFS) is permitted for trim only or on upper floor facades, up to 20% of total.
  - (b) Up to 40% stucco permitted on building facades. Design Review Committee may approve a higher percentage.
  - (c) The Design Review committee may make a recommendation to the Land Use Authority to accept materials not covered in this Chapter or to modify the exterior materials and colors requirements.
- (3) Roof Materials. Acceptable roof materials include 300 pound or better, dimensional asphalt composite shingles, wood shingles and shakes, metal tiles or standing seam, slate, and ceramic tile. "Engineered" wood or slate may be approved during the site plan process with an approved sample and examples of successful, high quality local installations. Refer to Figure 5.12 (2).
- (4) Color. Main building colors shall be complementary to existing building stock.
- (5) Appropriate Grade of Materials. Commercial quality doors, windows, and hardware shall be used on all Building Types with the exception of the Row Building and the Yard Building. Refer to Figure 5.12 (3).
- (6) Not allowed materials list: vinyl or aluminum siding, highly reflective metal, mirrored windows, plain cement block.



Roof Materials: Asphalt Composite Shingles



Roof Materials: Ceramic Tile

Figure 5.12 (2). Roof Materials.

# 5.0 Building Types

## 2. Windows, Awnings, and Shutters.

- (1) Windows. All upper story windows on all historic, residential, and mixed use buildings shall be recessed and double hung. Transparency requirements vary by Building Type.
- (2) Awnings. All awnings shall be canvas or metal. Plastic awnings are not permitted. Awning types and colors for each building face shall be coordinated. Refer to Figure 5.12 (4).
- (3) Shutters. If installed, shutters, whether functional or not, shall be sized for the windows. If closed, the shutters shall not be too small for complete coverage of the window. Shutters shall be wood or metal. "Engineered" wood may be approved during the site plan process with an approved sample and examples of successful, high quality local installations.



Permitted Awnings: Metal



Prohibited: Residential Grade Doors on Commercial Buildings.



Permitted Awnings: Canvas



Permitted: Commercial Grade Doors & Windows on Commercial Buildings.



Prohibited Awnings: Plastic

Figure 5.12 (3). Commercial Grade Doors & Windows.

Figure 5.12 (4). Awnings.

### 3. Livable Balconies and Porches.

The following applies in all locations where balconies are incorporated into the facade design facing any street or parking lot. Refer to Figure 5.12 (5).

- (1) Size. Balconies shall be a minimum of 6 feet deep and 10 feet wide.
- (2) Connection to Building. Balconies that are not integral to the facade shall be independently secured and unconnected to other balconies.
- (3) Percentage. 70% of units facing a frontage shall have a balcony, patio, or porch that faces surrounding common open space.
- (4) Railings. May vary and may promote privacy within the balcony; railings do not have to be open.
- (5) Projection of Balconies. Balconies may be cantilevered for up to 2 feet; projection of up to 6 feet into the build-to-zone or setback is permitted.

### 4. S-Line Corner Treatments.

When a building is located on an S-Line corridor intersection:

- (1) Corner plaza. The setback requirements of this chapter will create a triangular corner plaza defined as a clear area emanating 15' from the intersection of the corner property lines. S-Line corner plazas shall meet the requirements outlined in 6.3 Plazas and shall include:
  - Surfacing—brick pavers or similar as approved by the Land Use Authority.

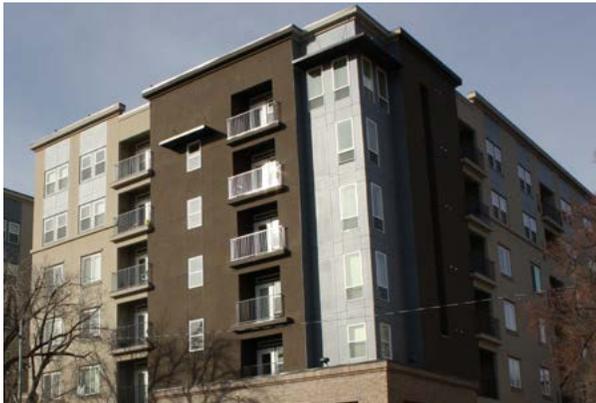


Figure 5.12 (5). Balconies Integral to Facade.

- Public benches and seating areas
- Architecture-complementary trash containers
- Specialty lighting
- Specialty landscaping
- Art sculptures, fountains, and similar installations
- Open views to corridor

### 5. Building Variety

Building design shall vary between vertical facade divisions, where required per the Building Types, and from adjacent buildings by the type of dominant material or color, scale, or orientation of that material and at least two of the following. Refer to Figure 5.12 (6) for an illustration of this requirement.

- (1) The proportion of recesses and projections.
- (2) The location of the entrance and window placement, unless storefronts are utilized.
- (3) Roof type, plane, or material, unless otherwise stated in the Building Type requirements.

### 6. Drive-through Structures

Drive-through Structures are not permitted.

### 7. Pedestrian Circulation.

Walkability principles shall be adhered to on-site and shall include openings to the S-Line Corridor at about 300' intervals. Pedestrian circulation shall be included in designs to the greatest degree that it is possible.

### 8. Meters and Equipment Placement.

Equipment shall be screened from view and not located on the S-Line Corridor nor on a public street.

### 9. Waste Containers.

Waste Containers shall be located out of public view and screened with landscaping and/or a structure that is compatible with the theme of the adjacent building. Specific requirements for waste containers can be found in 7.5 of this code. All requirements of the South Salt Lake City Municipal Code pertaining to waste containers shall apply.



Figure 5.12 (6). Building Variety.

# 5.0 Building Types

## 10. Rental Development Size and Amenities.

- (1) Residential rental developments shall include at least 50 units. All multi-family dwellings or complexes having over 50 units shall include an on-site manager’s unit and shall include the following schedule of amenities.
  - (a) An interior common social gathering area of at least four hundred (400) square feet for each fifty units, or portion thereof, within the building or development.
  - (b) Three items from the Unit Features Section, two items from the General Amenities Section, two items from the Recreation Amenities Section, and one items from the Energy Efficiency Enhancements section described in the table below for each fifty units, or portion thereof, within the building or development. Developers may propose alternative amenities in any category as part of a site plan review, subject to final approval by the Land Use Authority.
  - (c) For multifamily residential building developments in excess of 150 units, buildings shall include nine items from the Unit Features Section, six items from the General Amenities Section, six items in the Recreation Amenities section, and three items in the Energy Efficiency Enhancements section. Developers may propose an equivalent number of alternative amenities in any category as part of a site plan review, subject to final approval by the Land Use Authority.
  - (d) Refer to table 5.12 (1).

### Required Amenities for Multifamily Residential Buildings

Unit Features. Must choose 3 per 50 units or portion thereof.	General Amenities. Must choose 2 per 50 units or portion thereof.	Recreation Amenities. Must choose 2 per 50 units or portion thereof.	Energy Efficiency Enhancements. Must choose 2 per 50 units or portion thereof.
Individual Garages for at least 50 percent of units	Exterior Social Area—at least 400 square feet	Pool—at least 400 square feet	Compliance with ENERGY STAR New Homes Standard for buildings three stories or fewer
Washer/Dryer Connections	Enclosed parking	Internal Fitness Facilities	Compliance with ENERGY STAR Multifamily High Rise Program for buildings four stories or greater
Private porches, patio, or balcony—at least 70 square feet	Secured, Enclosed Storage Units	Secured, programmed children’s play areas	Installation of photovoltaic panels, wind turbines, or other electric generating renewable energy source to provide at least 20 percent of the project’s estimated electricity demand
Upgraded floor coverings, in place of or in addition to carpet	Public Transit Use Incentive	Hot Tub	Design and install required connections for the installation of PV or solar hot water system in future
Visibility features for at least 10 percent of units	Offering of Permanent On-Site Social Activities	Community Garden	Electric Vehicle Charging Station
Nine-foot ceilings for each unit	Library, Office, or Meeting Facilities	Perimeter Trail	Participation in a recycling program as part of a rental agreement or HOA
Enhanced soundproofing		Sport Court	Installation of tankless hot water systems
Solid Doors throughout unit			Demonstration compliance with any of criteria listed in the Site Improvements, Water Conservation, or Energy Efficiency sections of the 2011 Enterprise Green Communities Criteria

Table 4.2 (1) Required Amenities for Multifamily Residential Buildings.

# 6.0 Open Space Types

## 6.1 General Requirements.

### 1. Intent.

Within these subdistricts, the primary open space is the S-Line Corridor. To enhance the corridor, connections and beautification are required. Design of adjacent properties should help to expand on corridor features and create a larger open space. Courtyards that open up into the corridor are encouraged. The open space requirement of this section may be reduced depending on the types and quality of the open space provided and how it achieves the goal of corridor enhancement.

### 2. General Requirements.

For residential uses, 250 square feet of livable open space per unit, up to 20% of the total project area, is required. For non-residential uses, 10% of the project area shall be open space. All open spaces shall meet the following requirement.

- (1) All open space provided within any subdistrict development shall comply with one of the Open Space Types defined by 6.2 through 6.8.
- (2) Access. All Open Space types shall provide public access from a vehicular right-of-way and/or the S-Line Corridor.
- (3) Fencing. Open Space Types may incorporate fencing provided that the following requirements are met.
  - (a) Height. Fencing shall be a maximum height of 36 inches, unless approved by the Land Use Authority for such circumstances as proximity to railroad right-of-way and use around swimming pools, ball fields, and ball courts.
  - (b) Level of Opacity. Fence opacity shall be no greater than 30%.
  - (c) Type. Chain-link fencing is not permitted along any street or S-Line corridor frontage, with the exception of dedicated sports field or court fencing approved by the Land Use Authority.
  - (d) Spacing of Openings. Openings or gates shall be provided on every street face at a minimum of every 150 feet.
- (4) Ownership. Open Space Types may either be publicly or privately owned.
- (5) Continuity. Connections to existing or planned trails or open space types shall be made when the Open Space abuts the S-Line Corridor or other civic open space type.

### 3. Definition of Requirements.

The following further explains or defines the requirements included in Tables 6.2 (1) through 6.8 (1) for each Open Space Type. Refer to each table for the specific requirements of each Open Space Type.

- (1) Size.
  - (a) Size. The minimum size of the Open Space Type is measured within the parcel lines of the property.
  - (b) Size. The maximum size of the Open Space Type is measured within the parcel lines of the property.
  - (c) Dimension. The minimum length or width of the Open Space Type, as measured along the longest two straight lines

intersecting at a right angle defining the maximum length and width of the lot. Refer to Figure 6.1 (1).

- (d) Maximum Impervious and Semi-Pervious Surface Permitted. The amounts of impervious and semi-pervious coverage are provided separately to allow an additional amount of semi-pervious surface, such as permeable paving, above the impervious surfaces permitted, including, but not limited to, parking facilities, driveways, sidewalks, paths, and structures as permitted.

## 4. Stormwater in Open Space Types.

Stormwater management practices, such as storage and retention facilities, may be integrated into Open Space Types and utilized to meet stormwater requirements for surrounding parcels.

- (1) Stormwater Features. Stormwater features in civic open space may be designed as formal or natural amenities with additional uses other than stormwater management, such as an amphitheater, sports field, or a pond or pool as part of the landscape design. Stormwater features shall not be fenced and shall not impede public use of the land they occupy.
- (2) Qualified Professional. A qualified landscape design professional, such as a landscape architect or certified landscape designer, shall be utilized to incorporate stormwater features into the design of the civic open spaces.

## 5. Reductions.

Bonuses in the form of open space reductions may be awarded, up to 30 percent of the project open space required:

- (1) An open space bonus shall be given based on the square footage of the triangular setback area on all S-Line Corridor intersections
  - (a) This amount may be increased if additional quality features such as, but not limited to, street furniture, art, and landscaping are used.
  - (b) If a larger setback area is provided.
  - (c) The total area of the improved open space for the triangular setback area on S-Line Corridor intersections shall be doubled and credited toward the total open space requirement, up to 10% of the total required open space.
- (2) An open space bonus shall be given based on the square footage of a public pedestrian/cycle connection through the project to the S-Line corridor.
  - (a) The Land Use Authority may grant a bonus of up to 10% of the open space requirement if the connection provided includes enhanced landscaping, wayfinding features, street lighting and furnishings, or other means to encourage pedestrian activity.
  - (b) The total area of the improved open space for the connecting open space to the S-Line Corridor shall be doubled and credited toward the total open space requirement, up to 10% of the total required open space requirement.
  - (c) The Land Use Authority may grant a further reduction of 5% of the public open space requirement if the connection

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provided meets the above standards and is at least 20 feet wide.

- (d) A private open space connection shall receive no more than the 10% reduction.
- (3) Open space shall be reduced for open spaces such as courtyards that connect to the S-Line Corridor.
- (a) The Land Use Authority may grant a reduction of the open space requirement if the open spaces provided include enhanced landscaping, street lighting and furnishings, or other means to encourage pedestrian activity. The total combined area of private courtyards meeting this standard shall be credited toward the total open space requirement, up to 10% of the total required.
  - (b) The total area of the improved open space for the courtyards connecting to the S-Line Corridor shall be doubled and credited toward the total open space requirement, up to 20% of the total required open space requirement may be granted for public courtyards.
  - (c) The total area of the improved open space for the courtyards connecting to the S-Line Corridor shall be doubled and credited toward the total open space requirement, up to 15% of the total required open space requirement may be granted for private courtyards.
- (4) The Land Use Authority may grant a reduction of the open space requirement of up to 5% of the total required if the development includes:
- (a) Public art
  - (b) Transit Shelter enhancements
  - (c) Corridor furniture
  - (d) Covered bicycle storage
- (5) Open space shall be reduced for a parking structure with a roof top garden. The total area of roof top gardens associated with parking structures shall be credited toward the total open space requirement, up to 10% of the total required.

# 6.0 Open Space Types

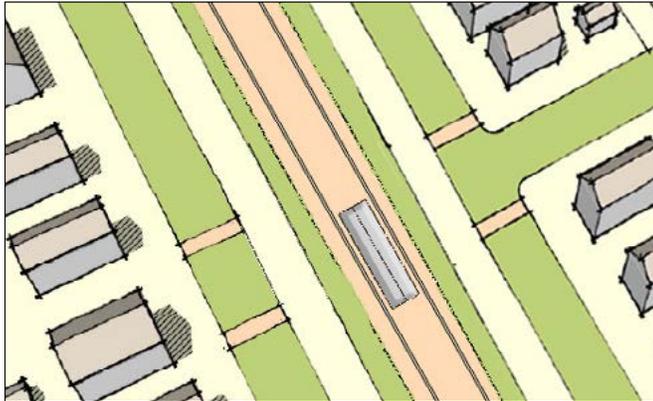


Figure 6.2 (1). S-Line Corridor.

## 6.2 S-Line Corridor.

### 1. Intent.

To complete the greenway as the premier open space for the neighborhood. The corridor includes Parley’s Trail, a strolling path, landscaping, public art, and unique experiences. Open space on private property adjacent to the S-Line that supports public access and/or enjoyment is encouraged. Activities in the entire corridor include walking, bike, recreation, commuting, and relaxing. Additional new amenities must support the UTA safety guidelines and regulations for safe streetcar operations.

Open space that is enhanced with landscaping, street furnishings and pathways adjacent to the S-Line, but within required property setbacks, shall be counted toward the a development’s open space requirements. Improvements shall follow the Urban Design standards for the S-Line and include a continuous pathways and access to the corridor. Landscaping that enhances the overall corridor, such as shade trees and screening, is encouraged. Improvements should enhance pedestrian connections to the corridor, create a safer environment by adding “eyes on the corridor” and support building designs that open onto the S-Line corridor.

- (1) Additional improvements may be installed in the S-Line corridor and become credited to the open space requirement as approved by the Land Use Authority. The open space requirement reduction shall reflect the cost and function of the improvement. S-Line improvements may include but are limited to: Bike racks at the stations, bollards, trees, pathway construction, art, fence type streetcar barriers, benches, etc.

S-Line Corridor Requirements	
<b>(1) Dimensions</b>	
Minimum Size (acres)	None
Maximum Size (acres)	None
Minimum Dimension (feet)	15' wide
Minimum Access/Exposure	100% of total length of S-Line frontage
Clear Zones	6' minimum pedestrian clear zone maintained around outdoor furnishings/merchandising
<b>(2) Adjacent Parcels</b>	
Permitted Subdistricts	All
Frontage Orientation of Adjacent Parcels	NA
<b>(3) Improvements</b>	
Designated Sports Fields Permitted	Not permitted
Playgrounds Permitted	Permitted, conditional on corridor safety review
Fully Enclosed Structures Permitted	Not Permitted
Impervious/Semi-Pervious Surface	40% minimum 80% + 10% maximum
Pathway	Required minimum 6' adjacent to corridor; must meet S-Line design standards
Lighting	Required, meet S-Line urban design standards to match existing improvements and meet minimum Illuminating Engineering Society safety standards
Seating	1 per 100 square feet; furniture on adjacent open space (such as plazas) may be counted
Trees	1 small-medium shade tree per 20' or 1 large shade tree per 30'
Landscaping	50% live plant material in planter areas
Bicycle facilities	1 bike rack per 300', meet S-Line urban design standard
Furnishing	1 bench per 300', meet S-Line urban design standard
Southside Pathway	6' Permeable material pathway to match existing
Northside Setback Area	May include landscaping, sidewalks, plazas



Figure 6.3 (1). Typical Plaza Layout.

## 6.3 Plazas.

### 1. Intent.

To provide small-scale outdoor space for civic, social and commercial purposes. Plazas on corners highlight the S-Line corridor (see 5.12 (4)). The space may also include pedestrian and building access routes. Activities may include meeting, relaxing, performance, casual workspace, outdoor dining. Plazas may also accommodate transit stations, bike facilities, food vendors, events, and performance.

The Plaza may contain a greater amount of impervious coverage than any other Open Space Type. Surfacing shall be brick or concrete pavers or stamped and colored concrete and include street furniture seating for at least 4 persons. Special features such as fountains, public art, game tables, accent lighting are encouraged.

### Plaza Requirements

#### (1) Dimensions

Minimum Size (acres)	0.01
Maximum Size (acres)	0.5
Minimum Dimension (feet)	10' in one direction
Minimum Access/Exposure	50% of total length of street or S-Line frontage.
Clear Zones	6' minimum pedestrian clear zone maintained around outdoor furnishings and merchandising

#### (2) Adjacent Parcels

Permitted Subdistricts	All
Frontage Orientation of Adjacent Parcels	Corner, corridor

#### (3) Improvements

Designated Sports Fields Permitted	Not permitted
Playgrounds Permitted	Permitted
Partially Enclosed Structures Permitted	Permitted to max of 10% of plaza space
Impervious/Semi-Pervious Surface	40% maximum
Lighting	Required, max 16' fixture height
Seating	1 per 30 square feet
Trees	1 per 30 square feet
Landscaping	50% live plant material in planter areas

# 6.0 Open Space Types

## 6.4 Pocket Park.

### 1. Intent.

To provide an informal, small to medium scale outdoor space for active recreation. Pocket Parks are encouraged to be located adjacent to and opening onto the S-Line Corridor. They may be privately owned and access controlled. Activities may include playing field, playground, swimming pool, spa pool, vegetable gardens, barbecue, pavilion, and outdoor gathering areas.

Pocket Park Requirements	
<b>(1) Dimensions</b>	
Minimum Size (acres)	0.05
Maximum Size (acres)	.5
Minimum Dimension (feet)	30' in one direction
Minimum Access/Exposure	50% of total length of street or S-Line frontage; minimum two access points a minimum of 20' width
Clear Zones	4' minimum pedestrian clear zone maintained around outdoor furnishings/merchandising
<b>(2) Adjacent Parcels</b>	
Permitted Subdistricts	All
Frontage Orientation of Adjacent Parcels	Front or Corner Side
<b>(3) Improvements</b>	
Designated Sports Fields Permitted	Not permitted
Playgrounds Permitted	Permitted
Fully Enclosed Structures Permitted	Permitted for max of 10% of space
Impervious/Semi-Pervious Surface	30%+10% maximum
Lighting	Required, max 16' fixture height; meet minimum Illuminating Engineering Society safety standards
Seating	1 per 30 square feet
Trees	1 per 500 square feet
Landscaping	50% live plant material in planter areas



Figure 6.4 (1). Typical Pocket Park Layout.

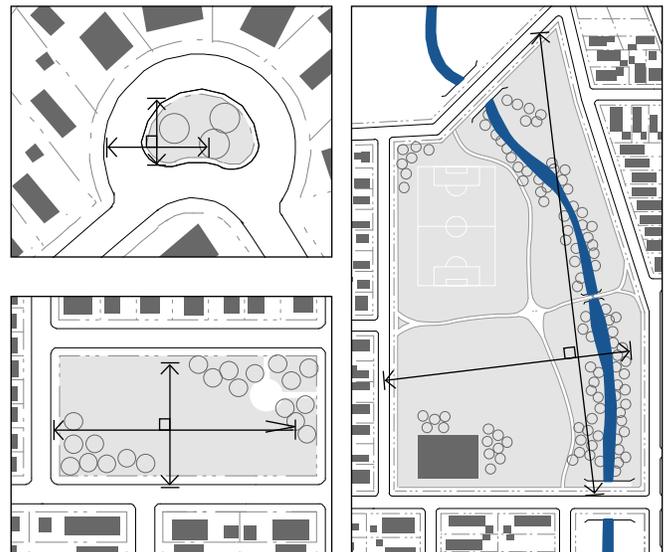


Figure 6.4 (2). Examples of Measuring the Minimum Dimension of Open Space Types.



Figure 6.5 (1). Typical Commons

## 6.5 Commons or Courtyard.

### 1. Intent.

To provide outdoor amenities and living space to property tenants. Activities may include playing field, playground, swimming pool, spa pool, rooftop garden, tot lot, vegetable gardens, barbecue, pavilion, and outdoor gathering areas. Commons are typically internal to a development and may be privately owned and access controlled, but are encouraged to be located adjacent to and opening onto the S-Line Corridor.

### Commons or Courtyard Requirements

#### (1) Dimensions

Minimum Size (acres)	0.1
Maximum Size (acres)	1.5
Minimum Dimension (feet)	45' in one direction
Minimum Access/Exposure	50% of total length of street or S-Line frontage. Two access points minimum, 20' minimum width
Clear Zones	4' minimum pedestrian clear zone maintained around outdoor furnishings and merchandising

#### (2) Adjacent Parcels

Permitted Subdistricts	All
Frontage Orientation of Adjacent Parcels	Side or Rear

#### (3) Improvements

Designated Sports Fields Permitted	Not permitted
Playgrounds Permitted	Permitted
Partially Enclosed Structures Permitted	Permitted to max of 10% of space
Impervious/Semi-Pervious Surface	30% + 10% maximum
Lighting	Required, max 16' fixture height
Seating	1 per 100 square feet, 1 table per 2000 square feet
Trees	1 per 500 square feet
Landscaping	50% live plant material in planter areas

# 6.0 Open Space Types

## 6.6 Individual Open Space.

### 1. Intent.

To provide small, individual outdoor living space to tenants of a development. This may be private or semi private space and orientation toward public open space and the S-Line corridor is encouraged. Individual open spaces include balconies, porches, stoops, front and back yards. Areas within setback spaces are not counted toward total required open space. These spaces have a minimum size requirement to encourage seating, eating and outdoor living.

Individual Open Space Requirements	
<b>(1) Dimensions</b>	
Minimum Size	60 square feet
Maximum Size	2000 square feet
Minimum Dimension (feet)	6' in one direction
Minimum Access/Exposure	None, orientation toward open space is encouraged, orientation toward single family homes is discouraged
Clear Zones	4' minimum pedestrian clear zone maintained around outdoor furnishings and merchandising
<b>(2) Adjacent Parcels</b>	
Permitted Subdistricts	All
Frontage Orientation of Adjacent Parcels	NA
<b>(3) Improvements</b>	
Designated Sports Fields Permitted	Not permitted
Playgrounds Permitted	Not Permitted
Partially Enclosed Structures Permitted	Not Permitted
Impervious/Semi-Pervious Surface	30% + 10% maximum
Lighting	None required
Seating	None required
Trees	None required
Landscaping	50% live plant material in planter areas

## 6.7 Passageway.

### 1. Intent.

To provide access to the S-Line Corridor, public amenities, and ROWs. These can connect pedestrians, bikes and non-motorized vehicles to transit, trails, pathways and the greenway. They should be safe, well-marked routes, and be open to the public wherever possible.

Passageway Requirements	
<b>(1) Dimensions</b>	
Minimum Width	15'
Maximum Length	150'
Minimum Access/Exposure	Minimum of one entry or exit into public space or ROW every 5'
Interval	Approximately every 300'; connects to downrigger points on the Lane where appropriate
Clear Zones	4' minimum pedestrian clear zone maintained around outdoor furnishings and merchandising
<b>(2) Adjacent Parcels</b>	
Permitted Subdistricts	All
Frontage Orientation of Adjacent Parcels	NA
<b>(3) Improvements</b>	
Impervious/Semi-Pervious Surface	No maximum
Lighting	Required, max 16' fixture height; meet minimum Illuminating Engineering Society safety standards
Seating	
Trees	1 per 500 square feet of landscaped area
Landscaping	50% live plant material in planter areas

# 7.0 Landscape

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## 7.1 General Requirements.

### 1. Intent.

The landscape standards outlined in this section are designed to meet the following set of goals:

- (1) To provide for a healthy, long-lived urban forest within all ROWs to improve the appearance of streets and create a buffer between pedestrian and vehicular travel lanes.
- (2) To increase the compatibility of adjacent uses and minimize the adverse impacts created by adjoining or neighboring uses.
- (3) To promote the prudent use of water and energy resources by achieving and maintaining sustainable, functional landscapes.
- (4) To shade large expanses of pavement and reduce the urban heat island effect.
- (5) To enhance the appearance and property values of the community.
- (6) To support the intent of the South Salt Lake Landscape Handbook.

### 2. Applicability.

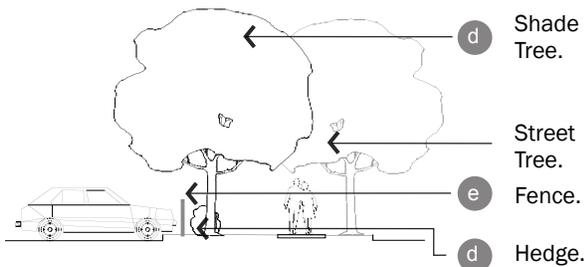
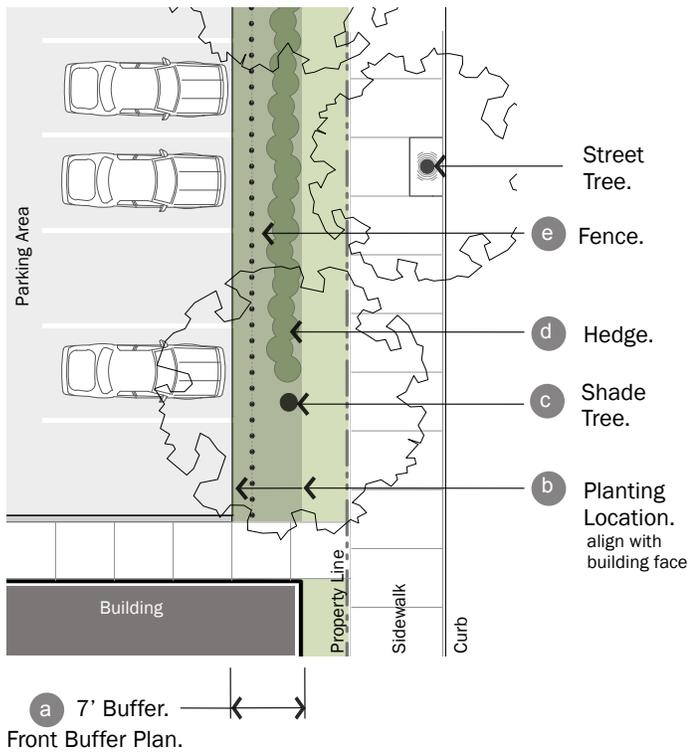
Landscaping, trees, and buffers shall be installed as detailed in the South Salt Lake Landscape Handbook.

- (1) General Compliance. Application of this section to existing uses shall occur with the following developments:
  - (a) Any development of new or significant improvements to existing parking lots, loading facilities, and driveways. Significant improvements include new driveways, new spaces, new medians, new loading facilities, or complete reorganization of the parking and aisles.
  - (b) Alteration to an existing principal or accessory structure that results in a change of 25% or more in the structure's gross floor area.
  - (c) When compliance is triggered for existing parking lots, landscape improvements shall take precedence over parking requirements.
- (2) Buffers. Landscape buffers are required according to the provisions in this section with the following exceptions:
  - (a) Shared Driveways. Buffers shall not be required along a property line where a curb cut or aisle is shared between two adjoining lots.
  - (b) Points of Access. Buffering is not required at driveways or other points of access to a lot.
- (3) Temporary Uses. These provisions do not apply to temporary uses, unless determined otherwise by the Land Use Authority.
- (4) Unless otherwise specified in this chapter, all requirements of the South Salt Lake City Municipal Code pertaining to landscape requirements shall apply.
- (5) Street Trees. Refer to the South Salt Lake Landscape handbook for appropriate street tree specifications.

## 7.2 Frontage Buffer.

### 1. Intent & Applicability.

- (1) Intent. To lessen the visual impact of vehicular areas visible from the street.
- (2) General Applicability. Applies to properties in all subdistricts where a vehicular area is located adjacent to a right-of-way.
  - (a) Exceptions. Vehicular areas along alleys, except when a residential district is located across the alley; single and two family residences.



Front Buffer Section.

Figure 7.2 (1) Frontage Buffer Plan and Section.

## 7.2 Frontage Buffer Requirements

### 1. Buffer Depth & Location <sup>1</sup>

<b>Depth</b>	7'	<b>a</b>
<b>Location on the Site</b>	Between street facing property line and parking area <sup>2</sup>	<b>b</b>

### 2. Buffer Landscape Requirements

<b>Uses &amp; Materials</b>	Uses and materials other than those indicated are prohibited in the buffer	
<b>Shade Trees</b>	Medium or large shade tree required at least every 40'; Locate on the street side of the fence; Spacing should alternate with street trees	<b>c</b>
<b>Hedge</b>	Required continuous hedge on street side of fence, between shade trees & in front of vehicular areas	<b>d</b>
<b>Hedge Composition</b>	Individual shrubs with a minimum width of 24", spaced no more than 36" on center, height maintained no more than 48"	
<b>Existing Vegetation</b>	May be credited toward buffer area	

### 3. Fence (optional) **e**

<b>Location</b>	2' from back of curb of vehicular area
<b>Materials</b>	Composites, steel, or colored PVC; Masonry columns (maximum width 2'6") and base (maximum 18" height) permitted
<b>Minimum Height</b>	3'
<b>Maximum Height</b>	4'
<b>Colors</b>	No bright or white colors
<b>Opacity</b>	Minimum 30%; Maximum 60%
<b>Gate/Opening</b>	One pedestrian gate permitted per street frontage; Opening width maximum 6'

**Notes:**

<sup>1</sup> This screening requirement does not prohibit the installation of or provision for openings necessary for allowable access drives and walkways connecting to the public sidewalk.

<sup>2</sup> In front, corner, and rear yards (on a through lot), when the parking area is located adjacent to any building on the lot, the buffer must be located so that it aligns with or is behind the face of the adjacent building back to the vehicular area. The area between the buffer and the property line must be landscaped.

# 7.0 Landscape

## 7.3 Side & Rear Buffer.

### 1. Intent & Applicability.

- (1) Intent. To minimize the impact that one zoning district may have on a neighboring district and to provide a transition between districts.
- (2) General Applicability. Applies to all properties in all subdistricts that directly abut an R-1 zone.

7.3 Rear Buffer Requirements	
<b>1. Buffer Depth &amp; Location</b>	
<b>Depth</b>	20' adjacent to R-1 zones. <span style="float: right;">a</span>
<b>Location on the Site</b>	Locate buffers on more intensively zoned lot; Buffer is measured from side and rear property lines.
<b>2. Required Landscape Screen</b>	
<b>Width</b>	5' landscape screen in addition to any other buffer landscaping <span style="float: right;">b</span>
<b>Location</b>	Directly adjacent to the rear or side property line
<b>Hedge or Fence</b>	Continuous double row of shrubs required between shade trees; fences shall be opaque and not white. <span style="float: right;">c</span>
<b>Hedge Composition</b>	Double row of individual shrubs with a minimum width of 24", spaced no more than 36" on center; Mature height in one year of 24"
<b>Hedge Frequency</b>	Minimum of 15 shrubs per 100' of property line is required
<b>Shade Trees</b>	At least 1 medium or large shade tree per every 40' within the buffer <span style="float: right;">d</span>
<b>3. Buffer Landscape Requirements</b>	
<b>Uses and Materials</b>	Uses and materials other than those indicated are prohibited within the buffer
<b>Tree Canopy Coverage</b>	1 medium or large shade tree required per 100 square feet of buffer, excluding the area within the required landscape screen
<b>Existing Vegetation</b>	May be credited toward buffer area
<b>Fence</b>	Optional; same standards for frontage buffer 8' height maximum

Notes:

<sup>1</sup> Land Use Authority may reduce width of buffer, width of landscape screen, or location of landscape screen based on existing landscaping and topography.

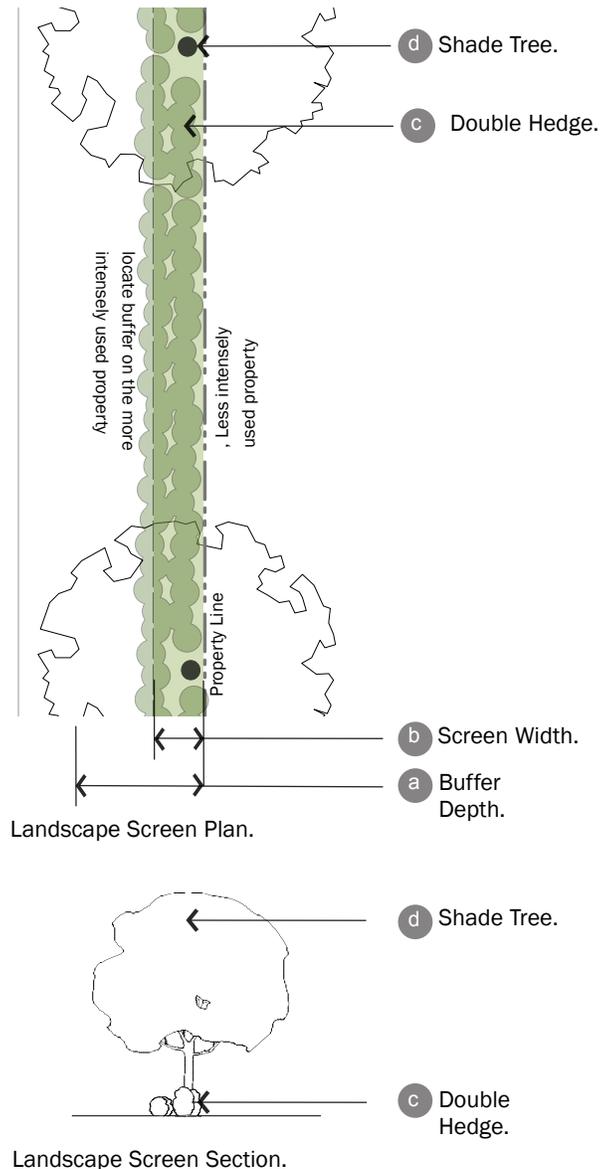


Figure 7.3 (1). Landscape Screen within Light Side & Rear Buffer.

## 7.4 Interior Parking Lot Landscape.

### 1. Intent & Applicability.

- (1) Intent. To provide shade, minimize paving & associated stormwater runoff, & improve the aesthetic look of parking lots.
- (2) General Applicability. All open-air, off-street parking lots in all subdistricts.
- (3) Other Internal Parking Lot Areas. Internal areas not dedicated to parking or drives shall be landscaped with a minimum of one medium or large shade tree for the first 150 square feet and one medium or large shade tree for every 650' thereafter.
- (4) Existing Vegetation. Existing vegetation may be credited toward these requirements.
- (5) All landscaped islands shall accommodate stormwater runoff; slotted curbs are encouraged.

7.4 Interior Parking Lot Landscape Requirements	
1. Landscape Island Requirements <span style="float: right;">a</span>	
<b>Required Island Locations</b>	Terminal ends <sup>2</sup> of free standing rows or bays of parking; After every ninth parking space for rows of parking greater than 8 spaces in length <sup>3</sup> <span style="float: right;">b</span>
<b>Minimum Width</b>	5'; Islands less than 15' must utilize structural soil under any paved surface within a tree's critical root zone; Islands under 9' shall provide for aeration
<b>Required Trees Within Islands</b>	Minimum of 1 medium or large shade tree per island <span style="float: right;">c</span>
2. Landscape Median Requirements <span style="float: right;">d</span>	
<b>Required Median Location</b>	Required in each free-standing bay of parking along the length of the bay
<b>Minimum Width</b>	5'; Medians less than 15' must utilize structural soil under any paved surface within a tree's critical root zone
3. Tree Requirements	
<b>Requirements per Parking Space <sup>4</sup></b>	Each parking space must be located within 50' of a tree planted within parking lot interior  Minimum of 1 shade tree must be planted within parking lot interior or within 4' of parking lot's edge for every 3 parking spaces
<b>Tree Shade Goal</b>	Within 20 years of tree installation, 30% of the interior of the parking lot should be shaded by tree canopy. Refer to Table 7.6 (1) for calculation.

<sup>1</sup> Parking lot interior is defined as the area dedicated to parking on a given parcel as measured from edge of pavement to edge of pavement.

<sup>2</sup> Freestanding rows or bays of parking are those not abutting the parking lot perimeter or building face, and may have a single or double row of parking.

<sup>3</sup> There shall be no more than 8 continuous parking spaces in a row without a landscape island.

<sup>4</sup> Trees within a designated buffer area may not be utilized to meet these requirements

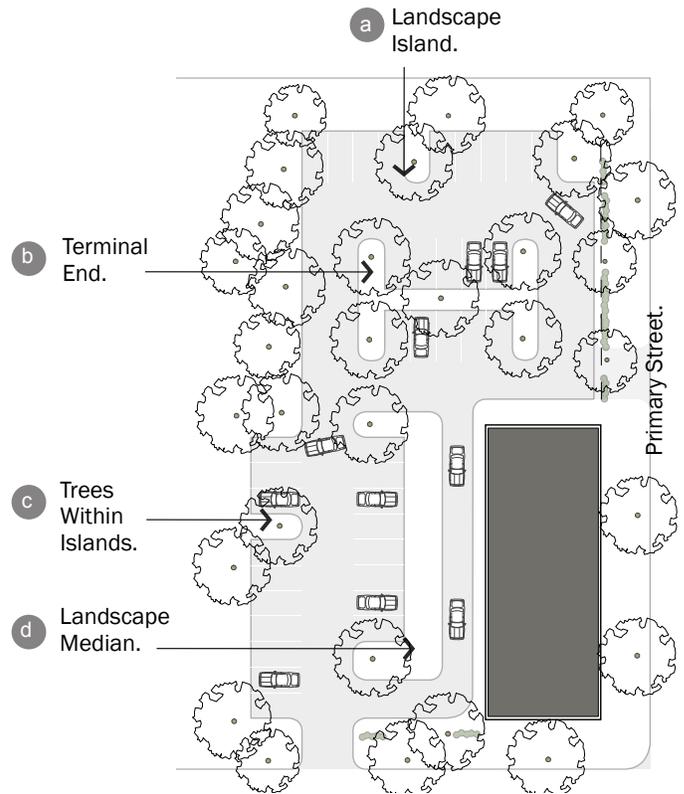


Figure 7.4 (1). Interior Parking Lot Landscaping.

Tree Size	Estimated Canopy at Maturity (sq ft)	Estimated Height at Maturity (ft)
Very Small	150	Under 15'
Small	400	15'-25'
Medium	900	25'-40'
Large	1600	40'+

Table 7.4 (1). Estimated Canopy and Height at Maturity.

# 7.0 Landscape

## 7.5 Screening of Open Storage, Refuse Areas, and Utility Appurtenances.

### 1. Intent & Applicability.

- (1) Intent. To reduce the visibility of, refuse areas, and utility appurtenances from public areas and adjacent properties.
- (2) General Applicability. All waste containers, open storage, refuse areas, and utility appurtenances in all subdistricts.

7.5 Screening of Open Storage, Refuse Areas, & Utility Appurtenances	
1. Open Storage & Refuse Area Screening Requirements	
<b>Location on the Site</b>	Not permitted in front or corner side yards
<b>Opaque Screen Wall <sup>1</sup></b>	Required around 3 sides of the dumpster and trash bin area matching building exteriors or as approved by the Land Use Authority <span style="float: right;">a</span>
<b>Screen Wall Height</b>	Height shall be the higher of the following: 1. 6' 2. Height as determined by Land Use Authority to accomplish objective of the screen
<b>Visible Openings</b>	Openings visible from the public way or adjacent properties must be furnished with opaque gates <span style="float: right;">b</span>
<b>Landscape Requirement</b>	If refuse area is located within larger paved area, such as a parking lot, landscape islands must be located on 3 sides of the area, with at least 1 medium or large shade tree in at least 1 of the landscape areas <sup>2</sup> <span style="float: right;">c</span>
2. Utility Appurtenance Screening Requirements	
<b>Large Private Mechanical Equipment <sup>3</sup></b>	Shall be fenced with opaque wood or brick-faced masonry on all sides facing a public street and/or the S-Line corridor
<b>Small Private Mechanical Equipment <sup>4</sup></b>	Shall have landscape screening and a shrub bed containing shrubs spaced no more than 36" on center

Notes:

- <sup>1</sup> Vertical structured barrier to visibility at all times such as a fence or wall
- <sup>2</sup> This tree, if located within 50' of a parking space, may be utilized to meet the minimum shade requirements
- <sup>3</sup> Large private mechanical equipment is equal to or greater than 4' in height
- <sup>4</sup> Small private mechanical equipment is smaller than 4' in height

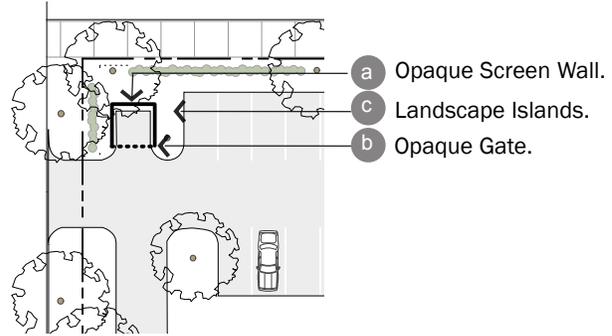


Figure 7.5 (1). Screening of Open Storage & Refuse Areas.

## 7.6 S-Line Buffer.

### 1. S-Line Buffer.

- (1) Refer to requirements in the open space section.

## 7.7 Streetscape Requirements.

### 1. Streetscape Requirements.

- (1) One large tree per 30 linear feet or one small tree per 20 linear feet.

# 8.0 Parking

## 8.1 General Requirements.

### 1. Intent.

Parking requirements are established to accomplish the following:

- (1) Ensure an appropriate level of vehicle parking, loading, and storage to support a transit oriented development neighborhood.
- (2) Provide appropriate site design standards to mitigate the impacts of parking lots on adjacent land uses and zoning districts.
- (3) Provide specifications for vehicular site access.

### 2. Applicability.

This section shall apply to all new developments and changes in use or intensity of use for existing development, in any subdistrict.

- (1) **Damage or Destruction.** When a use that has been damaged or destroyed by fire, collapse, explosion, or other cause is reestablished, any associated off-street parking spaces or loading facilities must be reestablished based on the requirements of this section.
- (2) **Site Plan Approval Required.** Parking quantities, design, and layout shall be approved through the development application process and meet the standards of the current parking chapter with the following exceptions:
  - (a) The standard requirement for residential parking is 1.4 stalls per unit. Parking requirements for all other uses can be found in chapter 17.27 of the South Salt Lake City Municipal Code. The Land Use Authority may consider increases or reductions to standard outlined in Table 8.1 (1).
  - (b) Parking lots shall accommodate stormwater via landscaped islands that allow for infiltration.
- (3) Unless otherwise stated in this chapter, all requirements of the South Salt Lake City Municipal Code pertaining to parking and access requirements shall apply.

## 3. Vehicular On-Street Parking.

On-street parking, as permitted on designated street types, shall meet the following requirements. Refer to Figure 8.1 (1)

- (1) Parallel parking is permitted on designated street types and shall not be striped.
- (2) **Vehicular Parking Space Dimensions.** The appropriate dimensions for on-street parking spaces are 23' by 8' (measurements include the gutter but not the curb).
- (3) On-street parking located directly adjacent to the site's property lines may be counted toward meeting the development's parking requirement, especially for visitor or on-site business related parking demand.

## 4. Stormwater Management in Parking Lots.

Incorporation of stormwater management best practices is encouraged, such as incorporating drainage swales and slotted curbs into the Landscape Zone in parking lots. Final design shall meet the requirements of the City Engineer. See Figure 2.2 (2) for reference.

Possible Parking Rate Reductions	
Amenity	Recommended Reduction (stalls/unit)
Car Share (limit 1 car/100 units)	0.1
Unbundled Parking (100%)	0.15
Bike Share	0.05
Bike Lockers/Storage	0.05
Development Supplied Transit Passes	0.25
Senior Housing	0.2
Student Housing (< .25 miles from campus)	0.1
Project Controlled On-Street Parking	0.1

Hales Engineering, 2014

Table 8.1 (1) Possible Parking Rate Reductions, Hales Engineering.

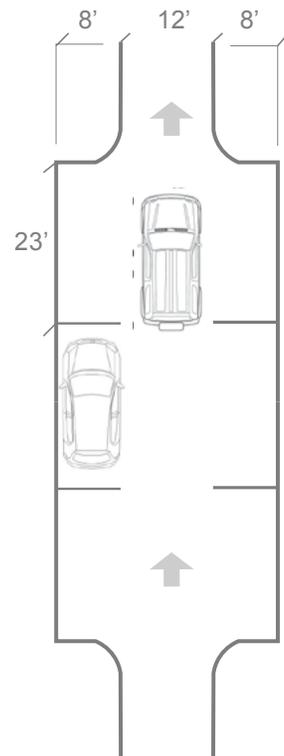


Figure 8.1 (1) On-Street Parking Dimensions.

# 9.0 Sign Types

## 9.1 General Requirements.

### 1. Intent.

This section seeks to enhance the economic and aesthetic appeal of the East Streetcar Neighborhood through the reasonable, orderly, safe, and effective display of signage.

### 2. Applicability.

These standards shall apply to signage in all subdistricts for non-residential uses only. Unless otherwise stated in this chapter, all requirements of the South Salt Lake City Municipal Code pertaining to sign requirements shall apply.

### 3. General Compliance.

Compliance with the regulations outlined shall be attained under the following situations:

- (1) Newly Constructed or Reconstructed Signage. All new signs and structural improvements to existing signs.
- (2) Change in Use for Single Business Signage. For signage serving one business, whenever the existing use is changed to a new use resulting in a change in signage, including rewording.
- (3) Multiple-Business Signage. For signage serving multiple businesses, whenever 50% or more of the existing uses are changed to new uses resulting in a change in signage, including rewording.
- (4) Damage or Destruction. When a sign has been damaged or destroyed by fire, collapse, explosion or other cause and the cost of restoration is greater than 50% of the replacement value at the time of the destruction or damage, the replacement sign shall comply with the standards in this article.

### 4. Sign Permit Process.

Refer to the current Sign Types chapter for information on the Sign Permit Process.

### 5. Sign Location.

Unless otherwise specified, signs shall only be located within the boundaries of the lot and not in the right-of-way or on public property.

- (1) Certain Sign Types may extend beyond a property line into the right-of-way or public property with permission from the Land Use Authority and in accordance with the regulations outlined in this section.
- (2) No sign shall be attached to a utility pole, tree, standpipe, gutter, or drain.
- (3) Signs shall be erected so as to permit free ingress to or egress from any door, window, the roof, or any other exit-way required by the building code or by fire department regulations.
- (4) No sign shall be erected or maintained in such a manner as to

obstruct free and clear vision of, interfere with, or be confused with any authorized traffic sign, signal, or device, or where it may interfere with vehicle or train line-of-sight.

- (5) Pedestrian Orientation. Signs oriented to the pedestrian realm are required within 14' of the ground plane for each entryway on a public street, the S-Line Corridor, or a Passageway.

### 6. Illumination.

All signs shall be illuminated according to the following provisions unless otherwise stated.

- (1) Signs shall be illuminated only by steady, stationary light sources directed solely at the sign or internal to it, except as permitted for Electronic Message Boards for Marquee signs.
- (2) Individual letters or logos may be internally illuminated as permitted per each sign type; no other portion of the sign shall be internally illuminated, except as permitted for Electronic Message Boards or unless otherwise stated.
- (3) When an external artificial light source is used to illuminate a sign, the lamp (or bulb) shall be located, shielded, and directed so as to not be visible from any public street or private residence.
  - (a) No receptacle or device housing a permitted light source which is attached to the sign itself shall extend more than 18 inches from the face of the Sign.
  - (b) If ground lighting is used to illuminate a sign, the receptacle or device should not extend more than 12 inches above ground and must be fully screened and housed.
- (4) The illumination of any sign, resulting from any internal or external artificial light source, shall not exceed 250 nits at the Sign face during the day and 125 nits at the Sign face after sunset, with no light trespass onto adjacent property.

### 7. Construction, Design, and Maintenance Standards.

All signs shall meet the construction, design standards, and maintenance requirements of the South Salt Lake City Sign Ordinance.

### 8. Computation.

The following standards generally apply to computing the area of signs by type and by building lot. Refer to the Sign Types 9.3-9.11 for more information.

- (1) Temporary signs are not included in the maximum signage area calculations, unless otherwise specified.
- (2) Height for monument signs is measured from the average grade at the front property line to the top of the sign, sign cabinet, or cap, whichever is highest.
- (3) For the purposes of determining area, lot width or frontage is measured along the front property line.
  - (a) If the lot is a corner lot, the width shall be measured along the front yard.
  - (b) Building frontage is the width of the front facade of a building.

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## 9.2 Sign Types.

### 1. Sign Type Requirements.

The following pertain to specific sign types detailed in this section.

- (1) Temporary Signs. A-Frame signs constructed of white plastic and internally weighted are allowed. Such signs shall be no greater than eight square feet per side. One such sign is allowed per business and must be located adjacent to the building but still provide a clear pedestrian path of at least 4'.
- (2) Window Signs. Window Signs shall not count towards a lot's maximum permitted amount of signage. Refer to 9.9 Window Signs.
- (3) Signs Facing Onto Parking Lots. One sign is permitted in addition to the maximum.
  - (a) Permitted Sign Types are a wall, projecting, or awning sign.
  - (b) Maximum sign area is 30 square feet.
  - (c) Permitted location is either the side or rear facade along a parking lot.
  - (d) If such signs face existing single family homes, they may not be illuminated.
- (4) Iconic Sign Elements. Iconic signs may be allowed at the discretion of the Land Use Authority on or adjacent to S-Line corners, if the lighting of the sign does not significantly impact adjacent neighbors and the sign helps to identify the East Streetcar Neighborhood. Such signs shall only be allowed in the State Street Subdistrict and shall comply with the following.
  - (a) Symbol or Logo Size. The symbol may not be larger than 8 feet in any direction, included in overall sign area and the surface area counts towards the Maximum Permitted Quantity of Signage per Lot.
  - (b) No moving parts or external illumination of the symbol may be provided.
  - (c) Text. The text component of the may not be more than 30% of the overall area of the sign.
- (5) Historic signs. Developers shall inventory historic signs as part of their site plan approval. Historic signs are encouraged to remain and be adapted for reuse, subject to approval by the Land Use Authority.

# 9.0 Sign Types

## 9.3 Wall Sign.

### 1. Description.

Wall Signs, also known as flat or band signs, are mounted directly to the building face to which the sign is parallel. Refer to Figures 9.3 (1) and 9.3 (2).

### 2. General Requirements.

Wall Signs shall be developed according to the standards in Table 9.3 (1).

- (1) Building Openings. Wall Signs shall not cover windows or other building openings.
- (2) Architectural Features. Wall Signs shall not cover architectural building features.
- (3) Murals. Murals, a type of Wall Sign painted onto the building face displaying the business name or activity, are prohibited on front facades.
- (4) Permitted location. No wall signs shall be permitted on any facade facing an existing single family residential zone, except for iconic sign elements as approved by the Land Use Authority, or parking lot signage as allowed in 9.2 (1) (3).

### 3. Computation.

The area of a Wall Sign is calculated using the following information.

- (1) Wall Signs. Area is calculated by drawing the smallest possible square or rectangle around the largest letters and/or elements, as is illustrated in Figure 9.3 (2).
  - (a) Area Credit. All areas that utilize individual alphanumeric characters or logos (including only those using wood, wood substitute, metal, or masonry) may use a total area of 90% of the calculation as outlined above.
- (2) Mural Sign. Area is calculated by measuring the area of the smallest square or rectangle that can be drawn around all of the sign elements, including any painted background.

Wall Sign Requirements	
<b>Permitted Districts</b>	All subdistricts; see 9.3.2 (4)
<b>Sign Area</b>	3 square feet per 1 linear foot of facade width with a maximum of 150 square feet per sign
<b>Height</b>	2' maximum letter or element height
<b>Location on the Building or Site</b>	Permitted on all facades facing a public street or the S-Line
<b>Placement on the Building or Site</b>	1' maximum projection from building face
<b>Quantity</b>	1 per building frontage
<b>Internal Illumination</b>	Permitted for individual letters and logos
<b>Materials</b>	Solid wood, metal, masonry & neon glass; Plastic & synthetics permitted only as separate alphanumeric characters or logos

Table 9.3 (1). Wall Sign Requirements.

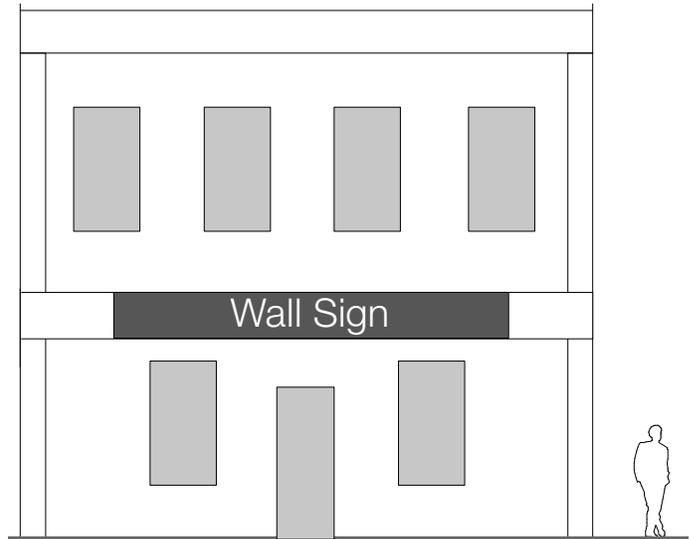


Figure 9.3 (1). Wall Sign.



Figure 9.3 (2). Measuring Wall Signs.

## 9.4 Projecting Sign.

### 1. Description.

A Projecting Sign is attached to and projects from a building face or hangs from a support structure attached to the building face. Sign faces are typically perpendicular to the building face, but may be at an angle greater than 45 degrees from the facade. The sign may be vertically or horizontally oriented. Refer to Figure 9.4 (1).

### 2. General Requirements.

Projecting Signs shall be developed according to the standards in Table 9.4 (1).

### 3. Computation.

The area of a Projecting Sign is equal to the area of one of the sign's faces.

### Projecting Sign Requirements

<b>Permitted Districts</b>	All non-residential districts
<b>Sign Area</b>	No maximum area for sign type;
<b>Height</b>	8' maximum sign length, 10' minimum clearance to pedestrian realm required
<b>Location on the Building or Site</b>	Permitted on all facades facing a public street or S-Line; Sign and structural supports shall not extend above the eave or parapet
<b>Placement on the Building or Site</b>	Shall not project further than 3' from the building
<b>Quantity</b>	1 per tenant per public ROW frontage including the S-Line corridor; 1 per tenant per side or rear facade on a parking lot
<b>Sign Separation</b>	No projecting signs shall be located closer together than 25'
<b>Internal Illumination</b>	Permitted for individual letters and logos
<b>Materials</b>	Solid wood, metal, masonry & neon glass; Plastic & synthetics permitted only as separate alphanumeric characters or logos

Table 9.4 (1). Projecting Sign Requirements.

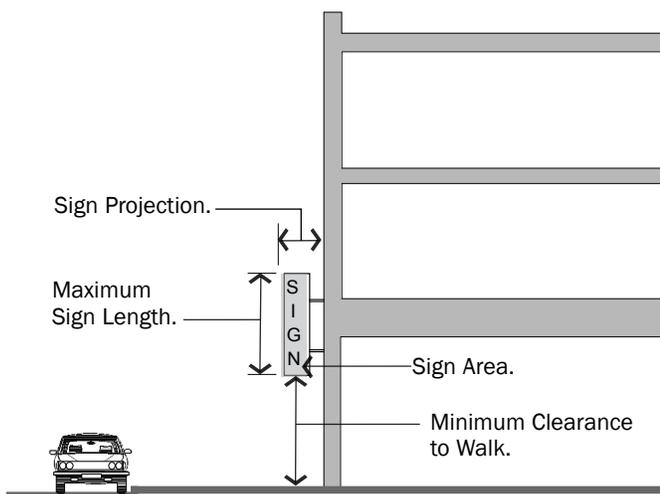


Figure 9.4 (1). Projecting Sign.

# 9.0 Sign Types

## 9.5 Projecting Marquee Sign.

### 1. Description.

A Projecting Marquee Sign is a projecting sign designed to have EMBs and two to three sign faces. Refer to Figure 9.5 (1).

### 2. General Requirements.

Projecting Marquee Signs shall be developed according to the standards in this section and Table 9.5 (1).

- (1) Manually Changeable Copy Boards. Manually Changeable Copy Boards are not permitted.
- (2) Electronic Message Boards. Permitted for Marquee signs.

### 3. Computation.

The sign area is calculated by combining the area of all exposed sign faces and the cabinet or structure surrounding them.

### Projecting Marquee Sign Requirements

<b>Permitted Districts</b>	All subdistricts, limited to Assembly Uses or Theater Uses per 4.0 Uses.
<b>Sign Area</b>	No maximum area for sign type; minimum two faces per sign.
<b>Height</b>	8' minimum clearance to walk required
<b>Location on the Building or Site</b>	Front & corner side facades only
<b>Placement on the Building or Site</b>	Maximum projection from building is 6'; Shall not project closer than 2' from back of curb
<b>Quantity</b>	1 per lot
<b>Internal Illumination</b>	Permitted for individual letters and logos. Electronic message centers are permitted
<b>Materials</b>	Solid wood, metal, masonry & neon glass; Plastic & synthetics permitted only on Sign face; Electronic Message Boards permitted for Marquee signs

Table 9.5 (1). Projecting Marquee Sign Requirements.

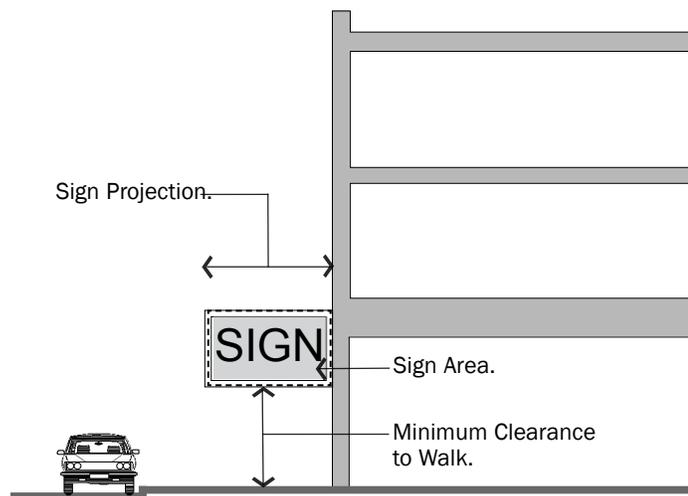


Figure 9.5 (1). Projecting Marquee Sign.

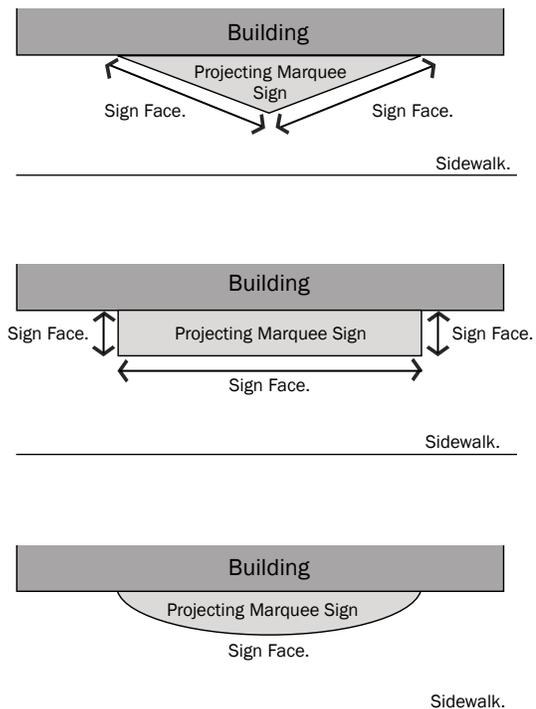


Figure 9.5 (2). Projecting Marquee Sign Plan.

## 9.6 Awning Sign.

### 1. Description.

A sign that is mounted, painted, or otherwise applied on or attached to an awning or canopy. Refer to Figures 9.6 (1) and 9.6 (2).

### 2. General Requirements.

Awning Signs shall be developed according to the standards in Table 9.6 (1).

### 3. Computation.

The area of an Awning Sign is calculated by drawing the smallest possible square or rectangle around the largest letters and/or elements of the sign portion of the awning, as is illustrated in Figure 9.6 (2).

### Awning Sign Requirements

<b>Permitted Districts</b>	All subdistricts
<b>Sign Area</b>	Up to 50% of the awning may be used for signage
<b>Height</b>	8' minimum clearance to walk required
<b>Location on the Building or Site</b>	Permitted on all facades
<b>Placement on the Building or Site</b>	Maximum projection from building is 4'; Shall not project closer than 2' from back of curb; Shall not block any window, door, or the building roof.
<b>Quantity</b>	1 per tenant per street frontage; 1 per tenant per side or rear facade on a parking lot
<b>Internal Illumination</b>	Permitted
<b>Materials</b>	Cloth, canvas, metal, or wood; All supports shall be made of metal or wood

Table 9.6 (1). Awning Sign Requirements.

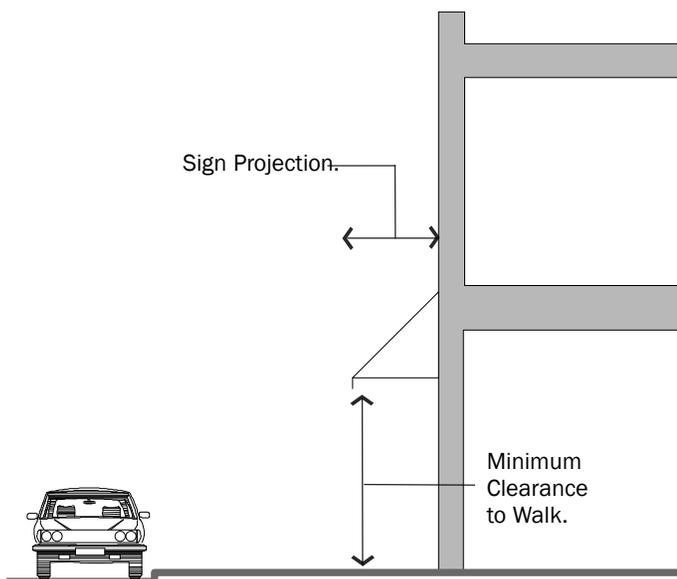


Figure 9.6 (1). Awning Sign.

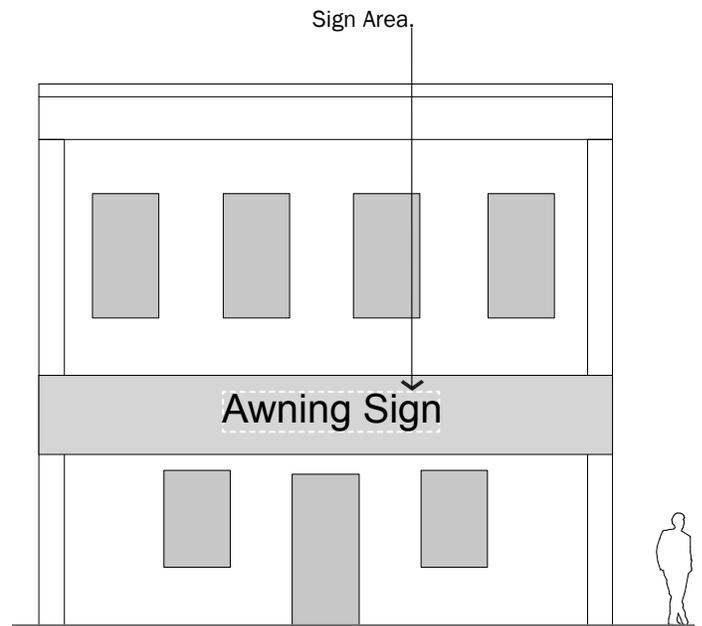


Figure 9.6 (2). Measuring Awning Signs.

# 9.0 Sign Types

## 9.7 Canopy-Mounted Sign.

### 1. Description.

A sign with individual alphanumeric characters and/or logos that is mounted on top of a permanent canopy. Refer to Figures 9.7 (1) and 9.7 (2).

### 2. General Requirements.

Canopy-Mounted Signs shall be developed according to the standards in Table 9.7 (1).

### 3. Computation.

The area of a Canopy-Mounted Sign is calculated by drawing the smallest possible square or rectangle around the largest letters and/or elements of the sign portion of the Canopy-Mounted Roof Sign, as is illustrated in Figure 9.7 (2).

### Canopy-Mounted Sign Requirements

<b>Permitted Districts</b>	All subdistricts
<b>Sign Area</b>	Up to 80% of the length of the canopy may be used for signage up to 150 square feet
<b>Height</b>	2' maximum letter or element height; Cannot project more than 2' above roof line of canopy
<b>Location on the Building or Site</b>	Permitted on all facades; not intended for the principal roof of the building
<b>Placement on the Building or Site</b>	Shall not block any window, door, or the building roof.
<b>Quantity</b>	1 per tenant per public street and S-Line corridor frontage; 1 per tenant per side or rear facade on a parking lot
<b>Internal Illumination</b>	Permitted
<b>Materials</b>	Solid wood, metal, & neon glass

Table 9.7 (1). Canopy-Mounted Sign Requirements.

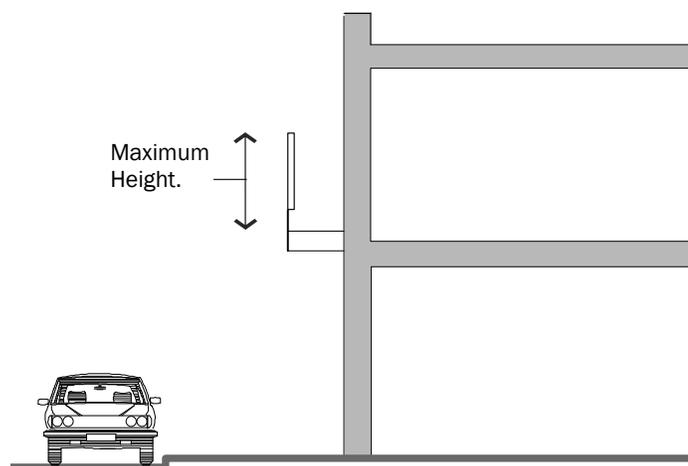


Figure 9.7 (1). Canopy-Mounted Sign.

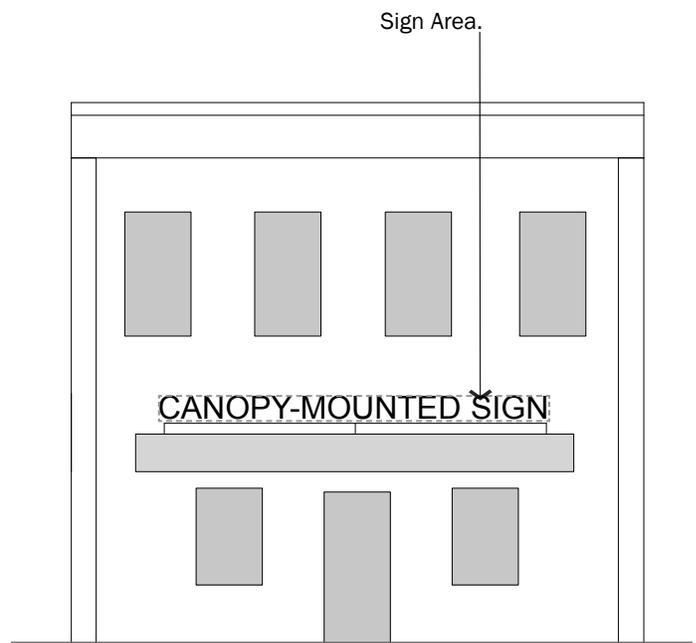


Figure 9.7 (2). Measuring Canopy-Mounted Signs.

## 9.8 Window Sign.

### 1. Description.

A Window Sign is posted, painted, placed, or affixed in or on a window exposed for public view or is a sign hung inside the building facing the window for public view. Refer to Figure 9.8 (1).

### 2. General Requirements.

Window Signs shall be developed according to the standards in Table 9.8 (1).

### 3. Computation.

A series of windows that are separated by frames or supporting material of less than six inches in width shall be considered a single window for the purposes of computation.

- (1) Measurement. To measure sign area percentage, divide the total sign area by the total window area, as illustrated in Figure 9.8 (1).
- (2) Maximum Allowance. Window Signs are not counted toward a site's maximum signage allowance.
- (3) Exempt Signs. Address and hours of operation are considered exempt Signs and are not counted in the Window Sign area calculation.
- (4) Temporary Window Signs. Temporary Window Signs must be included in the total percentage of signage per window calculation.
- (5) Window Signs may not be internally illuminated except for neon or similar illuminated window signs.

## Window Sign Requirements

<b>Permitted Districts</b>	All subdistricts
<b>Sign Area</b>	Up to 30% of a set of continuous windows may be covered with signage; No more than 50% of any one window panel may be covered with signage
<b>Height</b>	No maximum
<b>Location on the Building or Site</b>	Permitted on all facades facing a street or the S-line Corridor
<b>Placement on the Building or Site</b>	Ground or upper story windows; May be affixed to window or hung/mounted behind glass
<b>Quantity</b>	No maximum quantity, based on window Sign area for ground story; 1 per tenant per floor for upper stories
<b>Internal Illumination</b>	Not permitted, except on neon or similarly illuminated window signs
<b>Materials</b>	Drawn, painted, or affixed on the glass; Wood, metal, neon glass, plastic, or other similar materials also permitted

Table 9.8 (1). Window Sign Requirements.

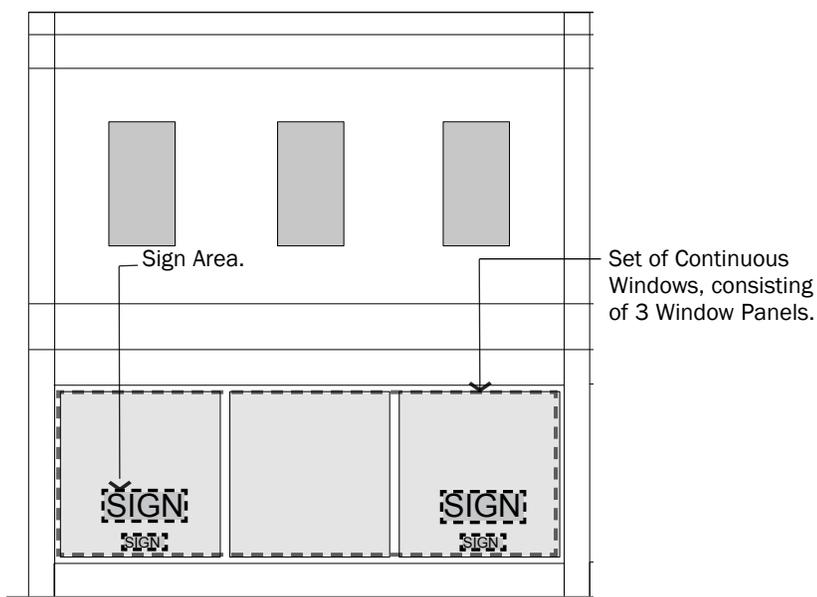


Figure 9.8 (1). Measuring Window Signs.

# 9.0 Sign Types

## 9.9 Monument Sign.

### 1. Description.

A Monument Sign is freestanding; it is located in a front or side yard of a lot. Refer to Figure 9.9 (1).

### 2. General Requirements.

Monument Signs shall be developed according to the standards in Table 9.9 (1).

- (1) Multiple Tenants. Multiple tenant buildings on a lot with a width of greater than 300 feet, measured across the front property line, may have signage with the following parameters:
  - (a) Up to two Monument Signs on one frontage.
  - (b) Signs shall be at least 150 feet apart.
- (2) Pole-Mounted Signs. Monument Signs may not be pole-mounted.
- (3) Manually Changeable Copy. The area of any Manually Changeable Copy cannot equal greater than 50% of the area of the sign face on which it is located or 20 square feet, whichever is less.
- (4) May serve multiple purposes such as seating.

### 3. Computation.

The area of a two-sided Monument Sign is equal to the area of one Sign face. The area of a three- or four-sided Monument Sign is equal to the total area of each sign face. This measurement includes the sign, any cabinet in which it is enclosed and the electronic message center, but excludes the base of the sign.

- (1) Measuring Height. Height shall include the sign face, base, cabinet, and ornamental cap.

Monument Sign Requirements	
Permitted Districts	All subdistricts
Sign Area	Maximum 70 square feet per Sign face
Height	Maximum height 4'
Location on the Building or Site	NA
Placement on the Building or Site	10' Setback from driveways & side property line; 3' Setback <sup>1</sup> from front & corner property lines
Quantity	1 per public street and 1 per S-Line corridor
Internal Illumination	Permitted for individual letters and logos
Materials	Solid wood, stone, metal & masonry; Plastic & synthetics permitted on Sign face

<sup>1</sup> If placed closer than five feet from the front and corner side property lines, sign must not be located in a sight triangle extending 10 feet from either side of an intersection of a driveway and a vehicular right-of-way or two vehicular rights-of-way.

Table 9.9 (1). Monument Sign Requirements.

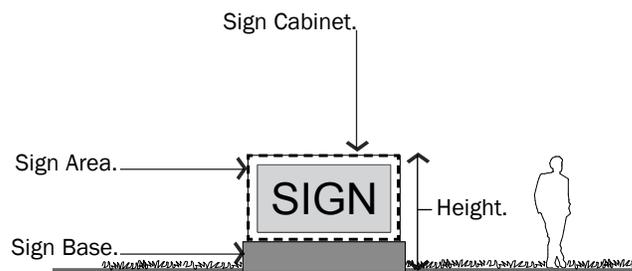


Figure 9.10 (1). Monument Sign.

# 10.0 Administration

## 10.1 General Provisions.

### 1. Purpose.

It is the intent of this code to promote public health, safety, and general welfare of the community, reflecting the goals established within the General Plan of the City of South Salt Lake.

### 2. Scope of Regulations.

- (1) **New Development.** All development, construction, and establishment of uses within the limits of this code occurring after the effective date of this code shall be subject to all applicable regulations of this code.
- (2) **Renovated Structures.** All building renovations affecting a change of use greater than 50% gross square footage of a structure within the limits of this code shall be subject to all applicable regulations of this code.
- (3) **In-Process Development.** Where a building permit for a development has been applied for in accordance with the prior law in advance of this code's effective date, said development may comply with the plans from which the permit was approved and, upon completion, receive a certificate of occupancy (provided all conditions are met) provided the following.
  - (a) Work or construction is begun within one year of the effective date of this code.
  - (b) Work or construction continues diligently toward completion.
- (4) **Nonconformance.** After the effective date of this code, existing buildings and uses that do not comply with the regulations of this code shall be considered nonconforming and are subject to the standards of Chapter 17.23 of the South Salt Lake Municipal Code.

### 3. Administration & Enforcement.

The Planning Commission shall be the land use authority for any development in the East Streetcar Corridor land use district. The provisions of this code shall be administered and enforced by the Community Development Director unless otherwise specifically stated. For the purposes of this code, the term Community Development Director shall be inclusive of his or her designees.

### 4. Development Application.

Applications (form, fees, and plan sets) shall be filed with the Community Development Department.

- (1) **Application Form.** Application forms are available from the City.
- (2) **Fees.** Fee amounts are available from the City and are due at the time the application is made; the application will be considered incomplete if fees are not paid.
- (3) **Plan Set Requirements.** Number of copies and minimum scale of drawings shall be noted on the application form. All plans shall be submitted in both a paper and an approved digital

format. All plans shall be reviewed by the Land Use Authority for completeness. Incomplete Applications shall be returned to the applicant for re-submission.

- (4) **Filing Deadline.** Filing deadlines are established by the City and available at City Hall.
- (5) **Withdrawal of Application.** Applicant may withdraw the application whole or in part at any point in the process prior to being acted or ruled upon; new application form, fees, and plan sets are required for reapplication.
- (6) **Records on File.** Applications and the resulting recommendations and rulings shall be kept on file by the Land Use Authority and shall be considered public record.
- (7) **Notice requirements for each process** are detailed in South Salt Lake City code.

### 5. Process.

- (1) Any development within a subdistrict shall be administered in accordance with the procedures defined in existing City ordinances, with exception of the Design Review Committee which is unique to the East Streetcar Neighborhood, and is summarized below:

The application shall follow the following process:

- (a) **Pre-Application Meeting**
  - (b) **Application submittal** – only complete applications shall be accepted
  - (c) **Staff review and coordination**
  - (d) **Design Review Committee** – shall review the application and make a recommendation to the Planning Commission. Such committee shall be composed of 5 persons chosen by the City with architectural, planning, landscape architecture, interior design, and/or engineering backgrounds. They shall review the application for its' compatibility with the intent of this ordinance and shall specifically review the building architecture and open spaces to ensure an enhancement of the S-Line Corridor
  - (e) **Planning Commission Review and Approval**
  - (f) **Staff processing of the Planning Commission approval**— letter of conditions (if any), site plan approval, architectural approval, engineering plans approval. Building permits are a separate process as per the Building Code.
- (2) **Exempt Activities.** The following activities are exempt from the requirements of 10.0 Administration.
    - (a) Ordinary repairs for the purpose of regular building, signage, lighting or site maintenance.
    - (b) Construction within the interior of the structure that is not visible from the exterior of the building.
    - (c) Emergency repairs ordered by any city official in order to protect health and safety.

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## 10.2. Nonconformities.

### 1. Nonconformities.

- (1) Refer to chapter 17.23 of the South Salt Lake City Code.

## 10.3 Regulations.

### 1. Amending the Code.

Amendments of the adopted code shall be approved using the procedure for an ordinance amendment.

### 2. Minor Modifications to a Site Plan.

The Land Use Authority may approve minor modifications to and approved site plan. Modifications may be evaluated through a letter of application and the provision of the reasoning behind the request. Such requests may be made for:

- (1) Minor modifications to proposed landscaping plans, pursuant to the modification standards established in Chapter 17.25 of the South Salt Lake City Municipal Code.
  - (a) Landscaping not exceeding 10% of the landscaping as required on the site plan. In no case can the minimum buffer adjacent to existing single-family zones be modified.
- (2) Minor modifications to buildings, including setbacks and materials, pursuant to the modification standards established in Chapters 17.21 and 17.23 of the South Salt Lake City Municipal Code.
  - (a) Building or sign locations that do not move more than 10'.
  - (b) Building materials that reflect the intent of the original material.
- (3) Minor modifications to parking requirements, pursuant to the development of an alternative parking plan as established in Chapter 17.27 of the South Salt Lake City Municipal Code.
  - (a) Parking arrangements and numbers that generally reflect the original approval.
- (4) Changes in lot sizes, land uses, building forms, or subdistrict designations shall be subject to a zoning map or ordinance amendment.

### 3. Requests for Modifications to Required Standards of this Ordinance.

The Land Use Authority may approve minor modifications to the standards of this ordinance using the current process found in chapters 17.21 and 17.23 of the South Salt Lake City Ordinances. Additionally, the Land Use Authority may approve modifications to building height and landscaping requirements, as provided below:

- (1) Landscaping not exceeding 10% of the landscaping as required on the site plan. Dimensions of landscaped areas may be modified within one foot of required dimensions. In no case can the minimum buffer adjacent to existing single-family zones be modified.

- (2) Building heights may be modified within 10% of the heights as required in the code

### 4. Other City Ordinances Applicable.

Unless the approved site plan specifies otherwise, all other city ordinances pertaining to site development and land use shall apply.

### 5. Subdivision Approvals and Development Standards in This District.

- (1) Commercial subdivisions shall be approved using the subdivision plat approval process established in this title and in Title 15.
- (2) Residential subdivisions for condominiums, and townhomes shall conform to the general requirements established for Planned Unit Developments in Title 15 and the regulations established in this title for Planned Unit Development Overlay Districts. Residential subdivisions shall be approved using the subdivision plat approval process established in this title and in Title 15.

**ATTACHMENT 2: PROPOSED AMENDMENTS TO  
CHAPTERS 17.11, 17.13, AND 17.47 OF THE SOUTH  
SALT LAKE MUNICIPAL CODE**

**South Salt Lake Municipal Code Chapter 17.11 – Establishment of Land Use Districts is to be amended as follows:**

**East Streetcar Corridor (ESC) District.** The East Streetcar Corridor District is established facilitate the redevelopment of the East Streetcar Corridor in a manner compatible with the South Salt Lake City General Plan and the East Streetcar Master Plan. Redevelopment in this corridor will be transit-oriented and will preserve the land values and integrity of surrounding single-family neighborhoods. Land uses and regulations for buildings and site development in the East Streetcar Corridor District are established in the East Streetcar Form Based Code.

**South Salt Lake Municipal Code Chapter 17.13 – Land Use Districts is to be amended as followed:**

**17.13.200 – East Streetcar Corridor District**

- A. Purpose.** The purpose of the East Streetcar Corridor District is to facilitate the redevelopment of the East Streetcar Corridor in a manner compatible with the South Salt Lake City General Plan and the East Streetcar Master Plan. Redevelopment in this corridor will be transit-oriented and will preserve the land values and integrity of surrounding single-family neighborhoods.

Specifically, the East Streetcar Corridor District will promote development that will:

1. Protect neighboring homes and property values;
2. Be context sensitive through height transitions and compatible design;
3. Improve neighborhood safety and appeal;
4. Revitalize or replace neglected properties;
5. Increase homeownership of existing homes;
6. Build high-quality new housing for a wide spectrum of residents;
7. Create a quality urban neighborhood that appeals to new residents;
8. Maximize the opportunity to locate housing and jobs adjacent to transit, bike, and pedestrianways; and
9. Complement the City's redevelopment strategy supporting the development of an urban core in South Salt Lake.

- B. Uses and Regulations.** Land uses and regulations for buildings and site development in the East Streetcar Corridor District are established in the East Streetcar Form Based Code. Unless the East Streetcar Form Based Code specifies otherwise, all other city ordinances pertaining to site development and land use apply.

**The following terms are to be added to Chapter 17.47 - Definitions of the South Salt Lake City Municipal Code**

“Block” - The aggregate of lots, passages, lanes, and alleys bounded on all sides by streets.

“Block Depth” - A block measurement that is the horizontal distance between the front property line on a block face and the front property line of the parallel or approximately parallel block face.

“Block Ends” - The lots located on the end of a block; these lots are often larger than the lots in the interior of the block or those at the opposite end of the block and can be located on a more intense street type. They are typically more suitable for more intensive development, such as multiple family or mixed use development.

“Block Face” - The aggregate of all the building facades on one side of a block.

“Block Length” - A block measurement that is the horizontal distance along the front property lines of the lots comprising the block.

“Building Type” - A structure defined by the combination of configuration, form, and function. Refer to 5.0 Building Types for more information and the list of permitted Building Types.

“Courtyard” - An outdoor area enclosed by a building on at least two sides and is open to the sky.

“Coverage, Building” - The percentage of a lot developed with a principal or accessory structure.

“Coverage, Impervious Site” - The percentage of a lot developed with principal or accessory structures and impervious surfaces, such as driveways, sidewalks, and patios.

“Critical Root Zone” - Also referred to as drip line. The area of soil and roots within the radius beneath the tree’s canopy, within the dripline, or within a circular area of soil and roots with a radius out from the trunk a distance of 1.5 feet for every inch of the tree’s width (measured at 4.5 feet above the mean grade of the tree’s trunk, noted as diameter breast height or DBH throughout this code).

“Dedication” - The intentional appropriation of land by the owner to the City for public use and/or ownership.

“Eave” - The edge of a pitched roof; it typically overhangs beyond the side of a building.

“Entrance Type” - The permitted treatment types of the ground floor Facade of a Building Type. Refer to 5.9 for more information and a list of permitted Entrance Types.

“Expression Line” - An architectural feature. A decorative, three dimensional, linear element, horizontal or vertical, protruding or indented at least two inches from the exterior facade or a building typically utilized to delineate floors or stories of a building.

“Impervious Surface” - Also referred to as impervious material. Any hard surface, man-made area that does not absorb water, including building roofs, sidewalks, parking, driveways, and other paved surfaces.

“Landscape Area” - Area on a lot not dedicated to a structure, parking or loading facility, frontage buffer, side and rear buffer, or interior parking lot landscaping.

“Lot, Through” - Also referred to as a double frontage lot. An interior lot having frontage on two approximately parallel vehicular rights-of-way, excluding an alley.

“Lot Area” - The computed area contained within the property lines; it is typically denoted in square feet or acres.

“Lot Depth” - The smallest horizontal distance between the Front and Rear Property Lines measured approximately parallel to the Corner and/or Side Property Line.

“Lot Frontage” - The horizontal distance between the Side Property Lines, measured at the Front Property Lines.

“Occupied Space” - Interior building space regularly occupied by the building users. It does not include storage areas, utility space, or parking.

“Open Space Type” - The permitted and regulated types of open spaces in the East Streetcar Form Based Code.

“Open Water” - A pond, lake, reservoir, or other water feature with the water surface fully exposed.

“Owner” - The legal or beneficial title-holder of land or holder of a written option or contract to purchase the land.

“Parking, Unbundled” – parking for multifamily residential areas that is leased or sold separately from the primary dwelling, rather than automatically included with building space.

“Pedestrianway” - A pathway designed for use by pedestrians; it can be located mid-block allowing pedestrian movement from one street to another without traveling along the block’s perimeter. A pedestrianway can also be referred to as a pedestrian pathway, a trail, or a greenway.

“Primary Street” - A street designated on the Zoning Map that receives priority over other streets in terms of setting front property lines and locating building entrances.

“Property Line” - Also referred to as lot line. A boundary line of a parcel of land or lot.

“Property Line, Corner” - A boundary of a lot that is approximately perpendicular to the front property line and is directly adjacent to a public Right-of-Way, other than an alley or railroad. Refer to Figure 10.5 (1) Lots.

“Property Line, Front” - The boundary abutting a right-of-way, other than an Alley, from which the required setback or build-to zone is measured, with the following exceptions.

1. Corner and Through Lots that abut a Primary Street shall have the front property line on that Primary Street.
2. Corner and Through Lots that abut two Primary Streets or do not abut a Primary Street shall utilize the orientation of the two directly adjacent lots, or shall have the front property line determined by the Zoning Administrator.

“Property Line, Rear” - The boundary of a lot that is approximately parallel to the front property line; this line separates lots from one another or separates a lot from an alley.

“Property Line, Side” -The boundary of a lot that is approximately perpendicular to the front and rear property lines; it is not adjacent to the public right-of-way.

“Roof Type” - The detail at the top of a building that finishes a Facade, including a pitch roof with various permitted slopes and a parapet. Refer to 5.10 for more information and a list of the permitted Roof Types.

“Scale” - The relative size of a building, street, sign, or other element of the built environment.

“Semi-Pervious Surface” - Also referred to as semi-pervious material. A material that allows for at least 40% absorption of water into the ground or plant material, such as pervious pavers, permeable asphalt and concrete, or gravel.

“Solar Reflectance Index (SRI)” - A measure of a constructed surface’s ability to reflect solar heat, as shown by a small temperature rise. The measure utilizes a scale from 0 to 100 and is defined so that a standard black surface is 0 and a standard white surface is 100. To calculate for a given material, obtain the reflectance value and emittance value for the material; calculate the SRI according to ASTM E 1980-01 or the latest version.

“Story” - A habitable level within a building measured from finished floor to finished floor.

“Story, Ground” - Also referred to as ground floor. The first floor of a building that is level to or elevated above the finished grade on the front and corner facades, excluding basements or cellars.

“Story, Half” - A story either in the base of the building, partially below grade and partially above grade, or a story fully within the roof structure with transparency facing the street.

“Story, Upper” - Also referred to as upper floor. The floors located above the ground story of a building.

“Street Face” - The facade of a building that faces a public right-of-way.

“Street Frontage” - Also refer to lot frontage. The portion of a building or lot directly adjacent to a vehicular right-of-way.

“Street Type” - The permitted and regulated types of streets in this code. Refer to 2.0 Street Types for more information and a list of the permitted Street Types.

“Streetwall” - The vertical plane created by building facades along a street. A continuous streetwall occurs when buildings are located in a row next to the sidewalk without vacant lots or significant setbacks.

“Structure, Principal” - Also referred to as the principal building. A building that contains the dominant Use of the Lot. It is typically located toward the front of the Lot in the front Build-to Zone or behind the Front Yard Setback.

“Subdistrict” – A designation given to any parcel within the East Streetcar District or a Master Planned Mixed Use District that dictates the standards for the development on that parcel.

“Swale” - A low lying, naturally planted area with gradual slopes that facilitate the transport, absorption, and/or filtration of storm water.

“Tree Canopy” - The uppermost area of spreading branches and leaves of a tree.

“Tree Canopy Coverage” - The area of ground covered or shaded by a tree’s canopy, measured in square feet.

“Visible Basement” - A half story partially below grade and partially exposed above with required transparency on the street facade.

“Water Body” - A body of water, such as a river, pond, or lake that may be man-made or naturally occurring.

**ATTACHMENT 3: EAST STREETCAR MASTER PLAN  
EXCERPT: DESIGN PRINCIPLES**

## Design Principles

New projects will be designed to make the most of the unique location and neighborhood amenities while fitting compatibly into their surroundings. Several principles guide this:

1. Development should emphasize the streetcar, Parley's Trail and greenway corridor.

- Maximize the amount of sunlight that reaches the corridor to promote year-round activity and safety.
- Orient activity toward the corridor, with entry doors, windows, store displays, signs, balconies, street furniture and other amenities.

2. Open space and public amenities should be added to the corridor and to the neighborhood.

- Incorporate both private and public open spaces adjacent to the corridor and increase apparent setback.
- No vehicle access adjacent to the corridor.
- Create mid-block access points to the corridor.
- Provide gathering spaces, street furniture, public art, and well-maintained landscaping.
- Create opportunities to walk and ride bikes.



*Size, scale and types of potential development, including new street.*

3. Development should create a medium-density urban form and a mix of uses.

- Include mixed-use space at the streetcar stops, with small neighborhood stores, restaurants, lifestyle services, and office space.
- Emphasize housing and office space, and avoid large-format retail.
- Include active spaces on the ground floor.
- Minimize parking requirements and eliminate surface parking wherever practical.
- Allow interesting and iconic signs or murals that lend a unique character of the neighborhood.

4. Buildings and landscapes should be environmentally sensitive and architecturally interesting.

- Maximize transparency and the amount of light entering buildings.
- Reflect the industrial history of the streetcar, the “vintage American” neighborhood, and the historic significance of this neighborhood over the years.
- Be energy-efficient and resource-conserving.
- Maximize local climate and environmental conditions.
- Include roof terraces, habitable balconies, green roofs and gardens.
- Utilize Crime Prevention Through Environmental Design (CPTED).

**ATTACHMENT 4: PLANNING COMMISSION  
MEETING MINUTES – 21 AUGUST 2014**

***Planning Commission Regular Meeting Minutes***

*Thursday, August 21, 2014*

*City Council Chambers*

*220 East Morris Avenue*

*Time 7:00 p.m.*

**Commission Members Present:**

Rachael Lauritzen - Presiding  
Jeremy Carter  
Portia Mila  
Jonathan Meakin

**Staff Members Present:**

Michael Florence  
Francis Lilly  
Hayley Pratt

**Moment of Reflection:**

**Chair Lauritzen**

**Pledge of Allegiance:**

**Commissioner Carter**

**Motion to Approve the Agenda:**

**Commissioner Mila**

**Second the motion:**

**Commissioner Meakin**

**Vote:**

**Unanimous**

Chair Lauritzen called the meeting to order.

**New Business**

- 1. Public Hearing  
T-14-001**

**Recommendation to the City Council to Adopt the East Streetcar Corridor Form Based Code as an Amendment to the South Salt Lake City Land Use Code.**

**Action Item**

**Applicant: South Salt Lake City**

Deputy Director-Housing Administrator, Francis Lilly, presented one staff report for both public hearings but requested that the public hearings be held separately. The first should be on adoption of the Form

Based Code with a motion to follow. The second public hearing would be for the Zoning Map Amendment with a motion to follow.

Mr. Lilly stated that the request before the Commission was to (1) recommend adoption to the City Council of a Form Based Code for the East Streetcar Corridor, and (2) to amend the Zoning Map for properties adjacent to the corridor at approximately 2200 South from the east side of State Street to the west side of 500 East from Commercial Corridor to Single-Family Residential to East Streetcar Corridor. A map was displayed showing the boundaries. The City's policy was reflected in the General Plan and the East Streetcar Corridor Master Plan adopted by the Council in March 2014. The policy had always been to exclude single-family residences on either side of the Streetcar Corridor from the rezone. The rezone itself includes the commercial area fronting State Street on either side of the Streetcar Corridor.

Mr. Lilly explained that under the Land Use District staff is requesting that the Commission adopt the Code for the properties to facilitate new development along the Streetcar Corridor while preserving the values and integrity of surrounding single-family neighborhoods. The Code was developed to address building forms, height transitions, street and open space standards, parking requirements, landscape standards, and uses. Development of the Code was predicated on the East Streetcar Corridor vision. The final wording was the end product of 10 years' worth of outreach and consensus building in the community. The vision was for the East Streetcar Corridor to be a vibrant walkable neighborhood with a wide selection of homes for current and future residents. Its character, amenities, and strong connections to both downtown South Salt Lake and Sugarhouse will make it one of the most desirable neighborhoods on the Wasatch Front. Downtown South Salt Lake is designated as the area between State Street and the north-south Trax line from 2100 South to I-80.

Mr. Lilly identified the following Master Plan goals as:

1. To protect neighboring homes and property values.
2. New development will be contact sensitive through height transitions, density blending, and compatible design.
3. Improve neighborhood safety and appeal.
4. Revitalize or replace neglected properties.
5. Increase home ownership of existing homes.
6. Build high quality new housing for a wide spectrum of residents.
7. Create a quality urban neighborhood that appeals to new residents.
8. Maximize the opportunity to locate housing and jobs adjacent to transit by pedestrian ways.
9. Support building in true downtown South Salt Lake to the west.

The General Plan also includes the following items that support adoption:

1. Regulate land uses based on compatibility with surrounding uses, residential areas, and economic feasibility.

2. Maintain residential business and industrial areas that are vibrant and where the health and safety of all are protected.
3. Capitalize on South Salt Lake's vast transit options by creating specific transit-oriented development land use plans using current best practices.
4. The Form Based Code was based on a template that was written through a collaborative regional process under the auspices of Envision Utah, Wasatch Front Regional Council, several municipal partners, and UTA. The City helped develop the template and support it as conforming to what is a regional best practice that was developed collaboratively and is sensitive to the unique characteristics of the Wasatch Front.
5. Accommodate higher density housing in appropriate areas.
6. To create an individual identity for each residential neighborhood.

Mr. Lilly summarized the City's outreach efforts over the past few years. At least two neighborhood meetings were held where the density study and vision were presented. Two developer and financial workshops were conducted where the design study and Form Based Code were introduced. Six meetings were held on a block-by-block basis, recognizing that the issues with each block are different. This allowed for better information and the feeling that each neighbor is heard in the process. There was very good attendance at the block meetings with 60 to 70 people attending all six meetings.

Mr. Lilly explained that the Form Based Code concept allowed staff to craft a design standard and height transition that was sensitive to the worst-case scenario, which happens to be a residence on the south side of the Streetcar Corridor on 300 East. The process allowed staff to obtain that information from the neighborhood and come up with a way to mitigate the impact. An ombudsman was hired to make door-to-door contact with every resident whose property was affected by the Form Based Code. He acted as a neutral party and the information received was very informative. Last fall there was relatively little resistance from the neighborhood. Some issues can't be resolved and the City has to implement a Code that is realistic and achieves the goals and policies of the General Plan. Staff also provided regular updates to elected and appointed officials.

The first section of the Form Based Code dealt with street types. Graphics were shown for the lane, which was intended to serve as a vehicular access since it will be greatly restricted along the Streetcar frontage. The lane will serve multiple purposes and can also act as a buffer between the single-family homes to the north and south.

The second section dealt with sub districts, which were identified. The State Street Gateway was where the highest intensity is anticipated and where heights could go up to five stories. The North Haven and Fifth East Gateway districts anticipate a lower building height and other changes were calibrated for each building type specific to each district.

With respect to uses, residential and neighborhood commercial uses, including service and retail, are allowed. It was anticipated that the commercial uses will be neighborhood commercial in scale. A significant list of allowable uses was provided for in Section 4 of the Form Based Code, found on page 17. The list is comprehensive and includes hours of operation restrictions that are similar to what exist currently in the Neighborhood Commercial Zone. Commercial uses typically cannot exceed 12,000

square feet, although smaller uses can be aggregated as part of a larger site. Mr. Lilly explained that allowances were made for adaptive reuse.

The design standards include a number of building types that are allowed specific in this corridor and include storefront building, urban style apartment, townhome, mansion style multi-family, civic building, parking structures, and adaptive reuse. Each building type has its own requirements. A special setback was included referring to the S-Line Corridor intersection that is 15 feet from the property line. This differs from the version provided to the Commissioners which shows 10 feet. Staff asked that the Commission approve the version that shows corner plazas that are 15 feet from the property intersection. The standards are fairly intricate and involved and vary significantly from building to building.

Height transition issues were discussed. Mr. Lilly stated that heights have emerged as a critical issue in the neighborhood and building heights have been discussed at every neighborhood meeting. Height maximums are five stories at the State Street Gateway and three to four along the corridor. Also discussed was how to deal with the height transition. Where the S-Line Corridor abuts or adjoins an existing residential zone, an automatic 20-foot setback is required. At that setback the base height can be no greater than 25 feet. At 30 feet a height transition is allowed. For every two feet of horizontal distance, an increase of one foot of vertical distance is permitted, up to 50 feet. Staff felt that the proposed numbers were roughly in line with the size in the backyards of the single-family homes along the corridor. In the setback a developer could choose to place a rear yard or a lane.

Mr. Lilly reported that the Form Based Code implements a more restrictive pallet of materials that requires 80% of the facades to be stone, brick, wood lap siding, fiberboard, glass, or other high quality materials that will be approved during the design review process. Residential densities are not expressed in the Code, however, they are effectively restricted through height, parking, and open space requirements. Staff felt that removing the density question from the issue helped facilitate a better discussion about what they want to achieve in the corridor. The primary concerns raised were height, privacy, and uses.

Open space types were next discussed. Mr. Lilly stated that the Code contemplates 250 square feet of open space per unit, up to 20% of the project area. A developer must choose one of the following open space types:

1. S-Line Corridor enhancement;
2. Plazas;
3. Pocket parks;
4. Commons;
5. Courtyards;
6. Private Open Spaces; or
7. Passageways.

The ordinance includes incentives and open space reductions for spaces and amenities that enhance the corridor and achieve the vision of a mixed use transit-oriented neighborhood. The reductions can be a maximum of 30% of the total, which could potentially allow for additional units.

Landscaping issues were next discussed. Mr. Lilly explained that the Landscaping section of the ordinance applies to parking areas, buffers, and exterior service areas. The importance of rear buffer requirements were discussed. With regard to parking, 1.4 stalls per unit for residential uses was the standard regardless of unit size. That figure was the result of a parking analysis commissioned by the City. It was determined to be a reasonable number given new multi-family projects occurring in transit-oriented areas and in general residential areas in the region. Reductions are given for unbundling parking or assessing a different lease for the parking spot and the home. Ride share programs and transit passes that are tied to the lease, shared parking, or a study that is used to support a reduction would also be considered.

Mr. Lilly explained that sign types are also generally more restrictive. Pole signs and roof signs are not allowed. The ordinance allows for the creation of an iconic sign and for maintaining and adaptively reusing existing iconic signs. Roof signs were removed from the ordinance. Administration issues were discussed. Mr. Lilly stated that the Planning Commission is the land use authority for land uses and subdivision approvals. A Design Review Committee was also being established to advise the Planning Commission on design issues. The committee will consist of individuals nominated by the Mayor including at least one design professional. They will meet early on in the process and talk with developers about materials and specific amenities. A consensus will be presented to the Planning Commission in the form of a recommendation. Staff felt this was an important tool to help facilitate site plan and conditional use approvals and ensure that the outcomes are consistent with the intent of the zone.

New definitions were included pertaining to the Form Based Code, which were included in the recommendation in the staff report. Language was also drafted establishing the East Streetcar Corridor Land Use District and adopting a Form Based Code as part of the Municipal Code. Staff found that the Form Based Code response to the South Salt Lake City General Plan and the East Streetcar Corridor Master Plan with the addition of height transitions, specific open space requirements, and appropriate building types, future development will be compatible and complement existing development and not detrimentally impact the livability, desirability, and value of existing residential development in the corridor. Staff recommended the Commission approve both the adoption of the Form Based Code and the rezone of the East Streetcar Corridor.

Chair Lauritzen commended staff for their thorough work.

**The Public Hearing on the Form Based Code was opened at 7:30 p.m.**

There were no public comments.

**The Public Hearing was closed at 7:31 p.m.**

**Motion to approve the Form Based Code Amendments, as listed.**

**Commissioner Meakin**

**Second the motion:**

**Commissioner Mila**

**Vote:**

**Unanimous**

**2. Public Hearing  
Z-14-003**

**Recommendation to the City Council to Amend the Zoning Map for Properties Adjacent to the Streetcar Corridor at Approximately 2200 South from the East Side of State Street to the West Side of 500 East from Single-Family Residential to East Streetcar Corridor.**

**Action Item**

**Applicant: South Salt Lake City**

**The Public Hearing on the Zoning Map was opened at 7:32 p.m.**

There were no public comments.

**Public Hearing was closed at 7:33 p.m.**

**Motion to approve the changes to the Zoning Map.**

**Commissioner Meakin**

**Second the motion:**

**Commissioner Mila**

**Vote:**

**Unanimous**

Mr. Florence reported that the Council will hold a Work Meeting on Wednesday, August 27, 2014. Their September meetings will go through the same process and it will be presented to the public.

**ATTACHMENT 5: MULTIFAMILY PARKING STUDY  
BY HALES ENGINEERING**

## MEMORANDUM

Date: August 5, 2014  
To: South Salt Lake  
From: Ryan Hales, PE, PTOE, AICP  
Subject: **Street Car Adjacent Apartments Parking Study**

UT14-624

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### Multi-family Parking Demand Rates (Salt Lake Valley)

In an effort to identify an existing / acceptable parking demand rate for multi-family projects, Hales Engineering studied sixteen projects within the Salt Lake Valley to better understand the parking supply and demand at these locations and to draw conclusions about parking rates near the Streetcar line in South Salt Lake. Twelve of these sites are typical apartment complexes, while four were identified as "Transit Friendly" sites. Data was collected at study locations in South Salt Lake on Wednesday August 6, 2014 and Thursday, February 20, 2014, Herriman on June 19, 2014, and around the Salt Lake Valley on early Thursday, November 1, 2012. The data collection times were all between 12:00 am and 4:00 am, as this is the time when the majority of tenets are home for the night and parking demand is at its greatest according to the Institute of Transportation Engineers (ITE), *Parking Generation*, 4<sup>th</sup> Edition, 2010.

## Data Collection / Study Locations

### **Timbergate Apartments**

The Timbergate apartments are located at 5605 West 11830 South in Herriman, Utah and consist of 176 two-bedroom units, and 112 three-bedroom units for a total of 288 units (see Figure 1). During our data collection, it was observed that 402 parking stalls were occupied, 177 were empty, 6 cars were parked on the street, and there were no garages within this project, for a parking supply of 579 spaces.

The following conclusions can be made:

1. Supply (striped parking stalls or carports on-site) = 2.01 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.46 stalls / occupied unit
3. 289 stalls were covered of the 579 stalls on site = 50% covered stalls



Figure 1: Timbergate Apartments – Herriman, Utah

### **Farmgate Apartments**

The Farmgate apartments are located at 5675 West 11840 South in Herriman, Utah and consist of 96 one-bedroom units, 272 two-bedroom units, and 128 three-bedroom units for a total of 496 units (see Figure 2). During our data collection, it was observed that 665 parking stalls were occupied, 206 were empty, there were 25 cars parked on the street, and there were 121 garages within this project, for a parking supply of 992 spaces.

The following conclusions can be made:

1. Supply (striped parking stalls or carports on-site) = 2.00 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.72 stalls / occupied unit
3. 476 stalls were covered of the 992 stalls on site = 48% covered stalls



Figure 2: Farmgate Apartments – Herriman, Utah

### Mission Meadowbrook Apartments

The Mission Meadowbrook apartments are located at 820 West Timbercreek Way in South Salt Lake, Utah and consist of 365 one-bedroom units and 47 two-bedroom units for a total of 412 units (see Figure 3). During our data collection, it was observed that 427 parking stalls were occupied, 361 were empty, there were no cars parked on the street, and there were eight (8) functioning garages that were closed.

The following conclusions can be made:

1. Supply (striped parking stalls or carports on-site) = 1.93 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.10 stalls / occupied unit
3. 421 stalls were covered of the 788 stalls on site = 53% covered stalls



Figure 3: Mission Meadowbrook Apartments – South Salt Lake, Utah

### Mountain Shadows Apartments

The Mountain Shadows apartments are located at 3825 South 700 West in South Salt Lake, Utah and consist of 80 one-bedroom units, 132 two-bedroom units, and 50 three-bedroom units for a total of 262 units (see Figure 4). During our data collection, it was observed that 313 parking stalls were occupied, 229 were empty, and there were six (6) cars parked on the street.

The following conclusions can be made:

1. Supply (striped parking stalls or carports on-site) = 2.09 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.28 stalls / occupied unit
3. 217 stalls were covered of the 542 stalls on site = 40% covered stalls



Figure 4: Mountain Shadows Apartments – South Salt Lake, Utah

## Egate Apartments

The Egate apartments are located at 2292 W. Ruddy Way in West Valley City, Utah within the Salt Lake Valley, and consist of 128 two-bedroom units and 176 three-bedroom units for a total of 304 units (see Figure 5). During our data collection, it was observed that 468 parking stalls were occupied, 83 were empty and there were 13 vehicles parking adjacent to the curb within the complex. An additional off-site overflow parking area was located close to the project and 22 vehicles were parked within this lot when counted.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 1.81 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.73 stalls / occupied unit
3. 127 stalls were covered of the 551 stalls on site = 23% covered stalls



Figure 5: Egate Apartments – West Valley City, Utah

### San Marino Apartments

The San Marino apartments are located at 776 West Grande Rose Way, South Jordan, Utah within the Salt Lake Valley, and consist of 112 one-bedroom units, 187 two-bedroom units and 21 three-bedroom units for a total of 320 units (see Figure 6). During our data collection, it was observed that 335 parking stalls were occupied, 145 were empty and there were 87 garages with 23 vehicles parked in the driveways and 9 vehicles parking adjacent to the curb within the complex.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.04 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.48 stalls / occupied unit
3. 308 stalls were covered of the 567 stalls on site = 54% covered stalls



Figure 6: San Marino Apartments – South Jordan, Utah

### San Moritz Apartments

The San Moritz apartments are located at 7625 S. Ropekey Drive, Midvale, Utah within the Salt Lake Valley, and consist of 168 one-bedroom units, 195 two-bedroom units and 21 three-bedroom units for a total of 390 units (see Figure 7). During our data collection, it was observed that 334 parking stalls were occupied, 140 were empty and there were 154 garages with 51 vehicles parked in the driveways and 80 vehicles parking adjacent to the curb within the complex.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.01 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.74 stalls / occupied unit
3. 364 stalls were covered of the 628 stalls on site = 58% covered stalls



Figure 7: San Moritz Apartments – Midvale, Utah

## San Tropez Apartments

The San Tropez apartments are located at 11747 South Siracus Drive, South Jordan, Utah within the Salt Lake Valley, and consist of 88 one-bedroom units, 141 two-bedroom units and 21 three-bedroom units for a total of 250 units (see Figure 8). During our data collection, it was observed that 234 parking stalls were occupied, 93 were empty and there were 93 garages with 39 vehicles parked in the driveways and 21 vehicles parking adjacent to the curb within the complex.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.05 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.68 stalls / occupied unit
3. 275 stalls were covered of the 420 stalls on site = 65% covered stalls



Figure 8: San Tropez Apartments – South Jordan, Utah

## Coppergate Apartments

The Coppergate apartments are located at 8870 South State Street, Sandy, Utah, and consist of 148 two-bedroom units and 44 three-bedroom units for a total of 192 units (see Figure 9). During our data collection, it was observed that 208 parking stalls were occupied, 200 were empty and there were 19 garages with 32 vehicles parking adjacent to the curb within the complex.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.22 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.42 stalls / occupied unit



Figure 9: Coppergate Apartments – Sandy, Utah

## Liberty Bend

The Liberty Bend apartments are located at 1048 East Liberty Bend Lane, Sandy, Utah within the Salt Lake Valley, and consist of 93 units, 36 apartments and 57 townhome units (see Figure 10). During our data collection, it was observed that 66 parking stalls were occupied, 27 were empty, and there were 82 garages with 2 vehicles parked in the driveways, and 25 vehicles parking adjacent to the curb near the complex.

The following conclusions can be made:

1. Supply (parking stalls on-site) = 2.23 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.94 stalls / occupied unit



Figure 10: Liberty Bend Apartments / Townhomes – Sandy, Utah

### Liberty Commons Apartments

The Liberty Commons apartments are located at 2785 S. Winstead Way, West Valley City, Utah within the Salt Lake Valley, and consist of 209 units (see Figure 8). During our data collection, it was observed that 131 parking stalls were occupied, 81 were empty and there were 185 garages with 0 vehicles parked in the driveways and 70 vehicles parking adjacent to the curb within the complex.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.20 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.40 stalls / occupied unit



Figure 11: Liberty Commons Apartments – West Valley City, Utah

## Lionsgate Apartments

The Lionsgate apartments are located at 136 W Fireclay Avenue, Murray, Utah within the Salt Lake Valley, and consist of 400 units (see Figure 12). During our data collection, it was observed that 522 parking stalls were occupied, 114 were empty and there were no garages and 242 vehicles parking adjacent to the curb within or near the complex.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 1.59 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 2.03 stalls / occupied unit

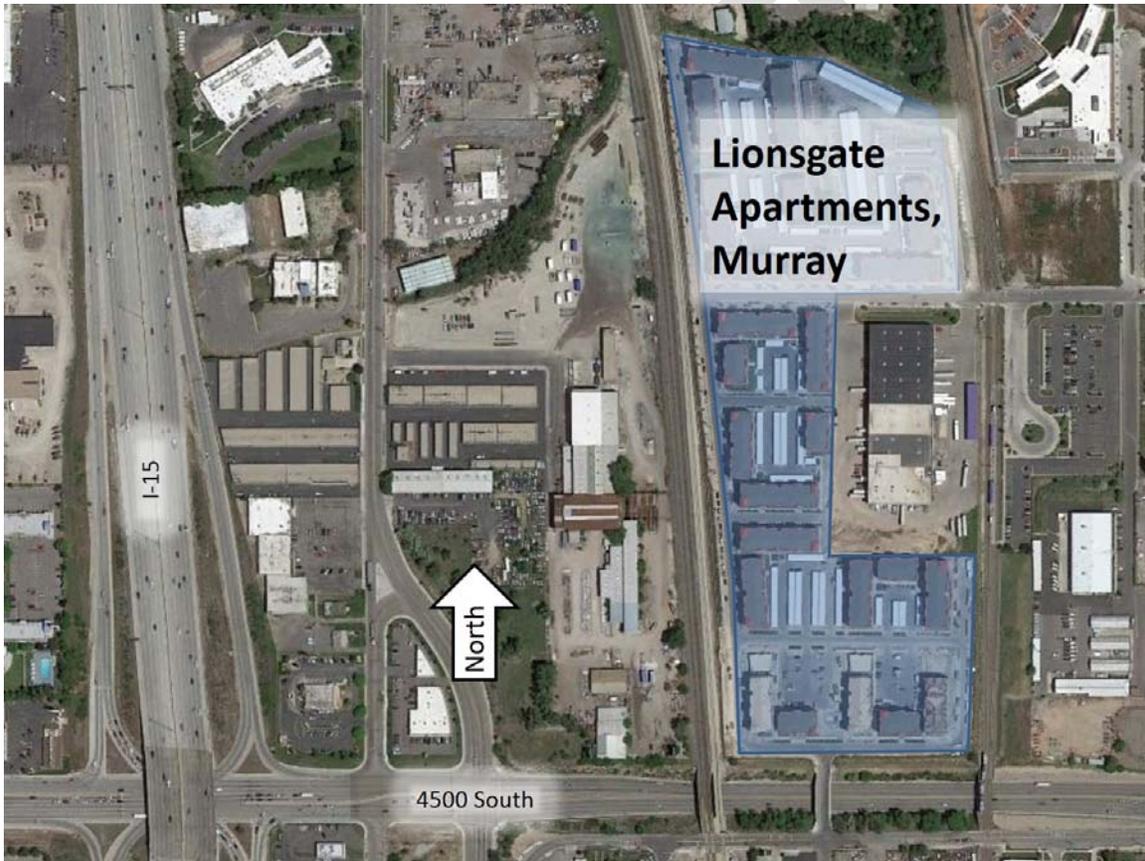


Figure 12: Lionsgate Apartments – Murray, Utah

**Data Collection Summary**

Within the Farmgate, Mission Meadowbrook, Coppergate, San Marino, San Moritz, San Tropez, Liberty Bend and Liberty Commons projects, each had closed garages that could not be counted. In order to provide a conservative estimate it was assumed that every garage space was being used for a vehicle. The following demand at the various apartment complexes was calculated:

Timbergate parking demand =	1.46
Farmgate Parking demand =	1.72
Mission Meadowbrook parking demand =	1.10
Mountain Shadows parking demand =	1.28
Egate parking demand =	1.73
San Marino parking demand =	1.48
San Moritz parking demand =	1.74
San Tropez parking demand =	1.68
Coppergate parking demand =	1.42
Liberty Bend parking demand =	1.94
Liberty Commons parking demand =	1.94
Lionsgate parking demand=	<u>2.03</u>
	1.63 Average parking demand / occupied unit (12)

**Transit Friendly Parking Rates**

The sites detailed above are typical apartment complexes across the Salt Lake Valley. The following sites were identified as being “Transit Friendly” by being located near transit stations and encouraging alternative modes of transportation. These sites include the Birkhill Apartments, 21 and View, Brickstone Apartments, and 2550 South Main.

### **Birkhill Apartments**

The Birkhill apartments are located at 16 Gilbride Avenue, Murray, Utah within the Salt Lake Valley, and consist of 202 units (see Figure 13). During our data collection, it was observed that 250 parking stalls were occupied, 117 were empty and there were 26 garages with 2 vehicles in the driveways and 64 vehicles parking adjacent to the curb within or near the complex. The project appears to be over-parked as 65 additional units are under construction and will be rented by Aug. 2014. The Murray North (4400 South) TRAX Station is located nearby along with the 200, 205, 45, 47 and 228 bus lines.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.87 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 2.48 stalls / occupied unit



Figure 13: Birkhill Apartments – Murray, Utah

Due to the current construction of 64 additional units (and no additional parking stalls) at the Birkhill apartments, this location was not used in the average rate calculation of “Transit Friendly” sites. Parking conditions will change as the units become available, so the parking rates here would not reflect the actual conditions.

## 21 and View Apartments

The 21 and View apartments are located at 2070 South View Street, Salt Lake City, Utah within the Salt Lake Valley, and consist of 29 units (see Figure 14). During our data collection, it was observed that 38 parking stalls were occupied, 10 were empty and there were no garages and 0 vehicles parking adjacent to the curb within or near the complex. Although curb parking was occupied, it appeared to be used by the adjacent restaurant. The 220, 21, and 213 bus lines run near the site and the end of the street car line is a few blocks away.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 1.66 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.36 stalls / occupied unit



Figure 14: 21 and View Apartments – South Salt Lake, Utah

### Brickstone Apartments

The Brickstone apartments are located at 220 East 3300 South, South Salt Lake, Utah within the Salt Lake Valley, and consist of 100 units (see Figure 15). During our data collection, it was observed that 76 parking stalls were occupied, 84 were empty and there were 60 garages and 0 vehicles parking adjacent to the curb within or near the complex. The manager estimated that approximately 50% of the garages were used for parking and the other 50% were used for storage. The Millcreek (3300 South) TRAX station is about 0.65 miles to the west and the 200, 205, and 33 bus lines run near the development.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.20 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.40 stalls / occupied unit

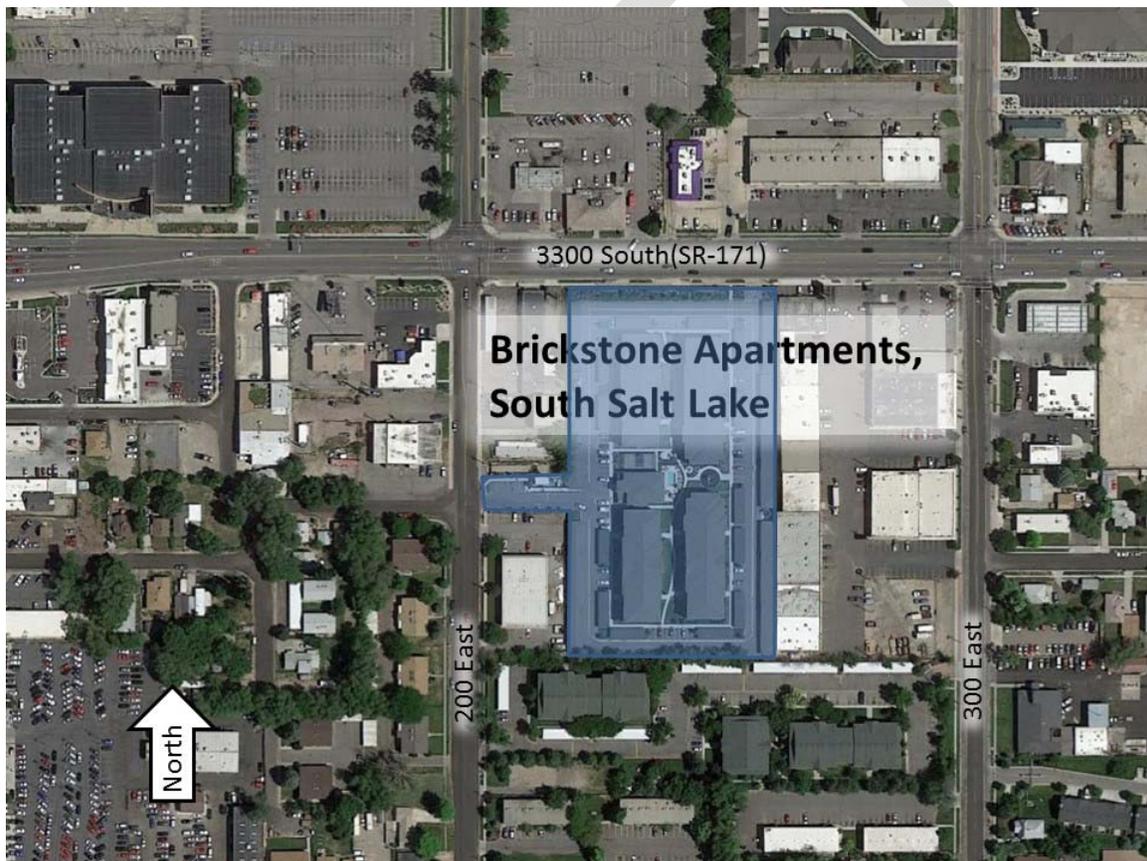


Figure 15: Brickstone Apartments – South Salt Lake, Utah

## 2550 South Main

The apartments at 2550 South Main, South Salt Lake, Utah are located within the Salt Lake Valley, and consist of 112 units (see Figure 15). During our data collection, it was observed that 78 parking stalls were occupied, 24 were empty and there were 68 garages with 34 vehicles parked in the driveway outside of the garage. There were 10 vehicles observed parking on-street adjacent to the complex. The 200 bus line runs near the development. The Central Pointe (2100 South) TRAX station is located less than a mile away.

The following conclusions can be made:

1. Supply (striped parking stalls on-site) = 2.13 stalls / unit
2. Demand (total parked vehicles on-site and off-site) = 1.71 stalls / occupied unit



Figure 15: 2550 South Main – South Salt Lake, Utah

### **“Transit Friendly” Data Collection Summary**

The following demand at the various “Transit Friendly” apartment complexes was calculated:

21 and View parking demand =	1.36
Brickstone parking demand =	1.40
2550 South Main parking demand =	<u>1.71</u>
	1.49 Average parking demand / occupied unit (3)

### **National Parking Rate Summary**

Hales Engineering conducted a review of parking studies and city codes specific to Transit-Oriented Developments (TODs) across the country. Studies included locations in California, Oregon, Wisconsin, Minnesota, and Virginia. Parking rates at these sites ranged from 0 stalls / unit to 1.59 spaces / unit. Other cities offer a percent reduction for a TOD, ranging from 10 – 22.8 percent. Provo city code allows a parking rate of 0.70 stalls / student tenant south of BYU campus. Additionally, Provo city code states that in a TOD zone, only 50 percent of the parking that is typically required within the municipal code is needed at a TOD site. Parking for residential uses on a fifth and sixth story may be reduced to 25 percent of the parking otherwise required. Provo also does not require parking for the first 2,500 square feet of retail or restaurant uses located in a building that consists of at least 50 residential units in a TOD zone.

There are several locations in Portland, Oregon where developments were allowed a 0 stalls / unit parking rate. These developments rely solely on transit or bicycle travel. Numerous sites studied in California showed a demand of only 1.07 stalls / unit. The parking studies are summarized in the appendix.

Additional amenities may also be defined as a way to reduce parking. For example, supplying a car share or a bike share on site may allow for a parking reduction. Another more aggressive parking reduction strategy would be to unbundle parking from the unit. Unbundled parking is when a tenant pays a fee or “buys” a parking stall separate from the unit. This incentivizes tenants to not use parking, or attracts tenants that do not rely on cars for transportation.

### **Conclusions and Recommendations**

Hales Engineering recommends that a parking rate of 1.4 stalls / unit be used for sites near transit (such as the streetcar in South Salt Lake). Parking counts collected throughout the Salt Lake Valley show that the parking demand at apartment complexes near transit stations are lower than a typical apartment complex. Additionally, studies throughout the Country confirm that parking demand near transit stations is lower.

Additionally, parking reductions could be considered for additional amenities. Some examples of amenities that could result in a reduction in required parking are shown in Table 1. A

reduction for proximity to transit is not included in the table because the “transit friendly” rate of 1.4 already accounts for a transit reduction.

As mentioned, unbundled parking is when a tenant pays a fee or “buys” a parking stall separate from the unit. It is recommended that this reduction only be given if 100 percent of the tenant parking is unbundled.

An additional amenity that was considered was compact car parking. However, it was determined that a compact car is still a vehicle and requires a parking stall, therefore, no reduction in parking is recommended.

Project controlled on-street parking is when a development is allowed to utilize and enforce on-street parking adjacent to the project. This may be permit parking only or something similar.

Table 1: Amenities and parking rate reductions

<b>Possible Parking Rate Reductions</b>	
<b>Amenity</b>	<b>Recommended Reduction (stalls / unit)</b>
Car Share (limit 1 car / 100 units)	0.1
Unbundled Parking (100%)	0.15
Bike Share	0.05
Bike Lockers / Storage	0.05
Development Supplied Transit Passes	0.25
Senior Housing	0.2
Student Housing (< 1/4 mile from campus)	0.1
Project Controlled On-street Parking	0.1
Hales Engineering, 2014	

Hales Engineering also recommends that adjacent on-street parking be provided at complexes where the lower parking rates are applied. This provides a factor of safety in case parking demands are higher than expected. It is also recommended that this table and the reductions applied become dynamic, so that as projects are constructed and after studies are completed, adjustments can be made to the reduction factors to reflect the actual field application. In addition, as areas in South Salt Lake continue to urbanize around the street car line, the younger generations who are less auto dependent, will likely cause a change in the future parking rates.

If you have any questions with this study please feel free to call us.

# Appendix

DRAFT

## National Parking Study Review

Location	Rate or Reduction	Source
California (26 sites)	Average supply of 1.41 spaces/unit	Willson, Richard. 2005. Parking Policy for Transit-Oriented Development: Lessons for Cities, Transit Agencies, and Developers. Journal of Public Transportation, Vol 8, No. 5.
East Bay Area, CA (16 sites)	Average supply of 1.59 spaces/unit. Average demand of 1.20 spaces/unit.	Cervero, R, Adkins, A, and Sullivan, C. 2009. Are TODs Over-Parked? UCTC Research Paper No. 882. University of California Transportation Center.
Portland, OR (15 sites)	Average supply of 1.52 spaces/unit. Average demand of 1.07 spaces/unit.	Cervero, R, Adkins, A, and Sullivan, C. 2009. Are TODs Over-Parked? UCTC Research Paper No. 882. University of California Transportation Center.
San Diego, CA	0.25 spaces/unit reduction for TOD	Tumlin, Jeffrey and Millard-Ball, Adam. 2006. Parking for Transit-Oriented Development. Institute of Transportation Engineers, Annual Meeting.
Milwaukee, WI	15% parking reduction for TOD	Tumlin, Jeffrey and Millard-Ball, Adam. 2006. Parking for Transit-Oriented Development. Institute of Transportation Engineers, Annual Meeting.
Minneapolis, MN	10% parking reduction for residential units in a TOD	Tumlin, Jeffrey and Millard-Ball, Adam. 2006. Parking for Transit-Oriented Development. Institute of Transportation Engineers, Annual Meeting.
Milpitas, CA	20% parking reduction for TOD	Tumlin, Jeffrey and Millard-Ball, Adam. 2006. Parking for Transit-Oriented Development. Institute of Transportation Engineers, Annual Meeting.
Arlington, VA	1.125 spaces/unit (at least 1.0 spaces/unit reserved and 0.125 spaces/unit shared) along BRT	Tumlin, Jeffrey and Millard-Ball, Adam. 2006. Parking for Transit-Oriented Development. Institute of Transportation Engineers, Annual Meeting.
Portland, OR	0.33 spaces/unit for TOD with more than 50 units	Portland City Code, Chapter 33.266
Various (17 sites)	Average supply of 1.21 spaces/unit (Philadelphia, Portland, San Francisco, and Washington DC). Also, 50% average reduction from ITE Trip Generation rates	Arrington, GB and Cervero, Robert. 2008. Effects of TOD on Housing, Parking, and Travel. TCRP Report 128.
Various (80 sites)	Average allowable parking reduction of 22.8% for TOD written into city zoning codes	Cervero, R, Adkins, A, and Sullivan, C. 2009. Are TODs Over-Parked? UCTC Research Paper No. 882. University of California Transportation Center.
San Francisco, CA	Average demand of 1.13 spaces/unit when parking is unbundled	FHWA. 2012. Contemporary Approaches to Parking Pricing: A Primer.
San Francisco, CA	Average demand of 0.76 spaces/unit when parking is unbundled and there is an on-site car sharing vehicle.	FHWA. 2012. Contemporary Approaches to Parking Pricing: A Primer.
Provo, UT	Parking requirements of 0.7 stalls per tenant near BYU. Only 50 percent of the parking that is typically required is needed at a TOD site. Parking for residential uses on a fifth and sixth story may be reduced to 25 percent of the parking otherwise required. Provo also does not require parking for the first 2,500 square feet of retail or restaurant uses located in a building that consists of at least 50 residential units in a TOD zone.	Provo City Code 14.23.12