



CLEARFIELD CITY COUNCIL
AGENDA AND SUMMARY REPORT
May 28, 2024 - POLICY SESSION

Meetings of the City Council of Clearfield City may be conducted via electronic means pursuant to Utah Code Ann. § 52-4-207 as amended. In such circumstances, contact will be established and maintained via electronic means and the meetings will be conducted pursuant to the Electronic Meetings Policy established by the City Council for electronic meetings.

55 South State Street
Third Floor
Clearfield, Utah

7:00 P.M. POLICY SESSION

CALL TO ORDER:

Mayor Shepherd

OPENING CEREMONY:

Pledge of Allegiance
Solemn Moment of Reflection
Council Member Thompson

APPROVAL OF MINUTES:

April 9, 2024 – work session
April 9, 2024 – policy session
April 23, 2024 – work session
April 23, 2024 – policy session

PUBLIC HEARINGS:

1. **PUBLIC HEARING TO RECEIVE PUBLIC COMMENT ON THE ADOPTION OF THE CLEARFIELD CONNECTED 2024 STATION AREA PLAN & DESIGN GUIDELINES**

BACKGROUND: Clearfield Connected 2023 is the update to the original station area plan that was completed in 2019. This plan update is to align with State Code requirements for station area plans passed during the 2022 legislative session. The most significant change in the plan is the expansion of the plan area to a ½ mile radius as required by State Code. As part of the Mixed-Use Zoning of the UTA owned property adjacent to the Frontrunner Station, a Master Development Agreement (MDA) and Plan (MDP) was executed in 2020 between Clearfield City, UTA, Hamilton Partners, and Stack Real Estate. The plan update incorporates the MDP into the overall design of the updated station area and does not alter any aspects of that agreement or plan. With the help of Landmark Design and their subconsultants, staff has

overseen and worked to create Clearfield Connected 2023. Open houses were held in June 2023 and November 2023 to solicit feedback from the community on how the station area could develop in the future. Additionally, a steering committee including Councilmember Tim Roper; Clearfield City, WRFC, and UTA planning and development staff; and members of the Stack Real Estate team have convened three times (May, June and November 2023) to review progress, analyze public comment, and provide feedback on draft elements of the plan. The Wasatch Front Regional Council was assigned by legislation to review and certify each station area plan in their metropolitan planning area. Following the adoption of the station area plan update by Clearfield City, the plan will be presented to WRFC for review and certification.

RECOMMENDATION: Receive public comment.

SCHEDULED ITEMS:

2. OPEN COMMENT PERIOD

The Open Comment Period provides an opportunity to address the Mayor and City Council regarding concerns or ideas on any topic. To be considerate of everyone at this meeting, public comment will be limited to three minutes per person. Participants are to state their names for the record. Comments, which cannot be made within these limits, should be submitted in writing to the City Recorder at nancy.dean@clearfieldcity.org.

The Mayor and City Council encourage civil discourse for everyone who participates in the meeting.

3. CONSIDER APPROVAL OF AND CONSENT TO THE APPOINTMENT OF COMMISSIONER BRIAN SWAN TO FILL THE VACANCY ON THE PLANNING COMMISSION

BACKGROUND: In April 2024, Commissioner Lauren DeSpain resigned from the Planning Commission due to a move out of state, creating a vacancy for a regular member. Brian Swan has been serving as an alternate member since February 2024. Based upon the discussion with the Mayor and City Council at the May 14, 2024 work session, Staff recommends that the City Council appoint Brian Swan to fill the regular member vacancy and complete the term of that vacancy which expires in February 2026.

RECOMMENDATION: Approve and consent to the Mayor's appointment of Brian Swan as a regular member of the Planning Commission with a term expiring in February 2026, and authorize the mayor's signature to any necessary documents.

4. CONSIDER APPROVAL OF THE AWARD OF CONTRACT FOR THE CONSTRUCTION OF PHASES 3 AND 4 AT THE MAINTENANCE AND OPERATIONS CENTER (MOC) TO STOUT BUILDING CONTRACTORS

BACKGROUND: In February of 2022 the City Council approved an agreement between Clearfield City and Think Architecture to complete a comprehensive design and plan set to construct the third and fourth phases of the MOC. The phases were identified in a Facility Needs Assessment that was produced in September 2012. The assessment identified four phases for modifying or expanding the existing facilities in the Public Works and Parks operational areas. To date, the City has completed phases one and two. As part of the 2012 Facility Needs

Assessment this project will conclude the needs addressed in the study. The proposed project includes the construction of an Operation Building, Equipment Storage Building, and the site work including a Decant Facility, Material Storage bunkers, and an employee parking lot. The Operation Building is designed to handle all of the day-to-day functions of the Public Works Department's operations including utilities, streets, and fleet. Staff have tried to be forward-thinking in the use, functionality, and long-term planning of the Public Works Department while designing an efficient facility.

RECOMMENDATION: Approve the award of contract for the construction of Phases 3 & 4 at the MOC to Stout Building Contractors and authorize the mayor's signature to any necessary documents.

5. **CONSIDER APPROVAL OF ORDINANCE 2024-09 ADOPTING THE CLEARFIELD CONNECTED 2024 STATION AREA PLAN & DESIGN GUIDELINES**

RECOMMENDATION: Approve Ordinance 2024-09 adopting the Clearfield Connected 2024 Station Area Plan & Design Guidelines, and authorize the mayor's signature to any necessary documents.

COMMUNICATION ITEMS:

- A. Mayor's Report
- B. City Council's Reports
- C. City Manager's Report
- D. Staffs' Reports

*****ADJOURN AS THE CITY COUNCIL*****

Posted May 24, 2024.

/s/Chersty Titensor, Deputy City Recorder

The City of Clearfield, in accordance with the 'Americans with Disabilities Act' provides accommodations and auxiliary communicative aids and services for all those citizens needing assistance. Persons requesting these accommodations for City sponsored public meetings, service programs or events should call Nancy Dean at 801-525-2714, giving her 48-hour notice.

The complete public notice is posted on the Utah Public Notice Website - www.utah.gov/pmn/, the Clearfield City Website - clearfield.city, and at Clearfield City Hall, 55 South State Street, Clearfield, UT 84015. To request a copy of the public notice or for additional inquiries please contact Nancy Dean at Clearfield City, Nancy.dean@clearfieldcity.org & 801-525-2700.

CLEARFIELD CITY COUNCIL MEETING MINUTES
6:00 PM WORK SESSION
April 9, 2024

City Building
55 South State Street
Clearfield City, Utah

PRESIDING: Mayor Mark Shepherd

PRESENT: Councilmember Karece Thompson, Councilmember Nike Peterson, Councilmember Tim Roper, Councilmember Megan Ratchford, Mayor Mark Shepherd, Councilmember Dakota Wurth

STAFF PRESENT: City Manager JJ Allen, Assistant City Manager Summer Palmer, Community Development Director Spencer Brimley, City Attorney Stuart Williams, Police Chief Kelly Bennett, Community Relations Director Shaundra Rushton, Senior Planner Brad McIlrath, Public Works Director Adam Favero, Community Services Deputy Director Curtis Dickson, Finance Manager Rich Knapp, City Recorder Nancy Dean, Deputy City Recorder Chersty Titensor

VISITORS: Cole Ross

DISCUSSION ON COMMUNICATION'S FOUR COUNCIL INITIATIVES

Shaundra Rushton, Communications Manager, reminded Council that she had four Council Initiative projects built into her duties with a \$15k budget. Last year's initiatives had been to support businesses, downtown branding, plane program and a bridge party. Council had already selected another bridge party which had been booked for Saturday, August 17, 2024 from 11:00 a.m. to 2:00 p.m. Ms. Rushton wanted to determine how the four initiatives would interact with the Council Projects, bridge party, mural, and bridge lighting recently decided upon. She asked for Council's input to determine what additional three initiatives it wanted her to focus on. Mayor Pro Tem Thompson said he had not thought of the bridge lighting as an initiative but as an enhancement. He thought the mural was an important priority to re-brand downtown. Councilmember Roper asked how much more work it gave her. Ms. Rushton said the bridge party and mural could be the second initiative. Councilmember Peterson asked if bridge lighting as a love note was viable. She thought having that information would impact the choices they made for the initiatives. The Council decided to move onto the second agenda item.

DISCUSSION AND CONSIDERATION FOR COUNCIL INITIATIVES – LIGHTS ON THE BRIDGE, MURAL(S) IN THE CITY & THE BRIDGE PARTY

COUNCIL PROJECTS

Shaundra Rushton, Communications Manager, asked Council to consider what type of bridge party they wanted her to plan. Mayor Pro Tem Thompson wanted whatever was planned to be sustainable and consistent. Councilmember Peterson agreed and thought the bigger events could be delayed until more development was completed. She recommended that they could do two bridge parties or some money could be re-routed to murals. Councilmember Wurth asked how the cost would be impacted if two themed bridge parties were combined. Ms. Rushton thought it

would be slightly less if combined. Councilmember Wurth agreed with Councilmember Peterson that re-routing money to murals for a bigger impact was a good option. Councilmember Peterson suggested focusing on a teen group if having a second bridge party. JJ Allen, City Manager, pointed out that the glow party needed to be at night time, but feedback from the bubble party was that it got cold in the evening so those two particular parties might not be combinable. Councilmember Roper recommended the City stay consistent.

Mayor Pro Tem Thompson questioned why the Aquatic Center was not used for teen events. He recommended utilizing that venue more often for the teens. Councilmember Wurth said it would be a great venue for a silent disco. Mayor Shepherd said the outdoor events were intended to involve more residents but was supportive of events at the Aquatic Center and murals. Mr. Allen asked Curtis Dickson, Deputy Community Services Director, about potential scheduling conflicts at the CAFC at the end of the school year. Mr. Dickson said the CAFC closed at 7:00 p.m. on Fridays and Saturdays for rentals. He said the City typically shut down the CAFC when school resumed in September for maintenance. Mr. Dickson said any events would need to be scheduled before the last week of school when the gym floor would be resurfaced and closed for a few weeks. Councilmember Peterson liked an end-of-year party so communication channels through the schools could be utilized.

LIGHTS ON THE BRIDGE

Mr. Dickson informed Council of the difficulties with installing patio lights during bridge parties due to the design and construction of the fence and streetlights. He showed alternative styles for Council to consider. Council did not think the lighting alternatives were feasible due to impact on Staff. The Council discussed the advantage of having inflatables at the entrance to the bridge to indicate an event was in progress as long as it was not difficult for Staff to setup. Council was not interested in murals on the jersey barriers because of the deterioration from traffic and road salt in the winter. Mr. Dickson posed the idea of having the jersey barriers wrapped. Councilmember Wurth suggested a community art project where residents could paint the jersey barriers. Mayor Pro Tem Thompson thought the City needed to be very conscientious about art due to constitutional rights that could be brought up. The group discussed various parameters that would need to be put in place. Councilmember Peterson said she would be open to it, but thought the priority was art on bigger surfaces, more permanent in nature.

Ms. Rushton asked if Council wanted to have the inflatable in place for the bridge party in August. Mr. Dickson said he thought the turnaround time was approximately 40 days so it was possible. Councilmember Ratchford thought two inflatables would be preferable so it could be seen from both sides of the bridge.

MURALS

Spencer Brimley, Community & Economic Development Director, showed Council some locations along State Street that might work for murals. He encouraged Council's feedback and requested input if it knew of other properties. He said the rough cost for a 10ft x 30ft mural would be approximately \$8,400- \$8,500. Mr. Brimley said additional research needed to be completed for cost. He did not feel like the restrictions in the Code would be too limiting but would need to review to determine whether Code needed to be amended. He said there were application fees and inspection fees for murals but if the City were the applicant, costs could be

covered. He mentioned that Ms. Rushton had obtained a copy of what an agreement for a mural might look like, which could be reviewed.

Councilmember Ratchford wondered whether the revitalization of the closed gas station on the corner of 300 North and Main Street could be a possible project. Mr. Brimley said there was someone interested in revitalizing the property. Brad McIlrath, Senior Planner, said the revitalization would require changes to meet the standards of the Form Based Code, so having a mural could be discussed as part of the Master Development Agreement process. Specific locations were discussed. Mayor Shepherd expressed his concern that any mural needed to be done right so it would not look ghetto. Councilmember Peterson asked how that could be ensured. He thought choice of building was key. Councilmember Peterson recommended Mayor Shepherd speak with Mayor Mendenhall about the murals in Salt Lake City and what formula was taken into consideration. Mr. Allen said Ogden would be similarly comparable as it had some murals in downtown Ogden that were commissioned and then in alleyways that were freelance graffiti artists. Mr. Brimley said Staff would speak with property owners to gauge their interest.

Councilmember Wurth moved to adjourn the work session and reconvene in a policy session at 6:57 p.m., Councilmember Ratchford seconded the motion. All aye.

RESULT: Passed [5 TO 0]

YES: Councilmember Thompson, Councilmember Peterson, Councilmember Roper, Councilmember Ratchford, Councilmember Wurth

NO: None

The work session reconvened at 8:46 p.m.

DISCUSSION ON COMMUNICATION'S FOUR COUNCIL INITIATIVES – (cont'd)

Mayor Pro Tem Thompson wanted the plane program to be intentionally completed. Councilmember Wurth thought the planes were important to focus on. Ms. Rushton said the planes had been committed in three places in downtown. Councilmember Peterson suggested the City allow a year to determine the success of the plane program and readdress it in 2026. Ms. Rushton thought the City could purchase two planes and host a bridge party with her \$15k budget. Councilmember Peterson said she was less worried about continuing the plane program due to Ms. Rushton's familiarity with the process. Ms. Rushton confirmed that Council agreed that they wanted her to focus on the plane program, a normal bridge party, and a teen or other event. Ms. Rushton would look into the possibility of consolidating the two budgets to determine whether combined they would yield a better mural.

DISCUSSION ON A ZONING TEXT AMENDMENT REQUEST TO AMEND SECTIONS 11-10A AND 11-21 OF THE CLEARFIELD CITY CODE TO UPDATE THE LANDSCAPE OPEN SPACE REQUIREMENTS FOR THE P-F ZONE (PUBLIC FACILITIES) AND MAKE MINOR AMENDMENTS TO LANDSCAPING STANDARDS AND REQUIREMENTS

Brad McIlrath, Senior Planner, gave background information as outlined in the presentation

materials. He identified properties that were included in the P-F Zone and described which properties the changes would affect. He reviewed the proposed amendments to Clearfield City Code related to public utility facilities and public works facilities in the P-F Zone. Additionally, it was determined that a definition was needed for Public Works Facility which would be added to Section 11-3-3: Terms Defined. He said that Public Works Facility would be added to the list of permitted uses in the P-F Zone and that Public Works and Public Utility Facilities would follow industrial landscaping standards.

He reviewed a summary of Chapter 21 Landscaping Standards which would clean up language. He said the Planning Commission had recommended a change to the tree quantity standards to a minimum of one tree for every 500 sq. ft. of landscape on commercial properties instead of the one tree for every 600 sq. ft. as proposed by staff. He said multi-family developments would remain as a minimum of one tree for every 400 sq. ft. He said in industrial zones the standard was one tree for every 1000 sq. ft. He reported that since that Planning Commission meeting, staff had performed research on landscape standards in surrounding communities and reported the results to Council.

Councilmember Peterson thought for commercial development one tree for every 600 sq. ft. was appropriate because it became difficult to achieve a higher ratio due to some constraints with commercial properties. She mentioned that on the Clearfield Station property it was difficult to put the number of trees in due to line of sight issues. She understood the desire to keep that standard with multi-family developments for the cooling advantages but for commercial development she was in favor of 1:600 that had been successful in the City. She thought that standard met the desire for water-wise planting. Councilmember Roper asked whether the same ratio should be applied in multi-family developments. Councilmember Peterson said she supported 1:600 for multi-family, but recognized that the Planning Commission had a say in the process and would be okay with their recommendation for 1:400. Mr. McIlrath stated that Staff's original recommendation had been 1:600 for commercial and multi-family properties.

Councilmember Ratchford asked for the reasoning behind the dissenting vote from Planning Commission. Mr. McIlrath said the dissenter was comfortable with staff's recommendation of 1:600. Councilmember Ratchford asked whether Staff thought the requirement for shrubs every 200 feet balanced out the tree ratio enough. Mr. McIlrath said when they originally proposed the reduction from 1:600 to 1:400 the idea was that the grass areas were being lost so the properties should have more trees. Ultimately, Staff realized that with the increase in shrubs the groundcover was covering the area where they would have had grass and it was providing beautification so they could back off on the tree requirements. He summarized the discussion on the benefits of trees held by the Planning Commission. Councilmember Ratchford accepted the 1:600 if there was an element included which specified that every 200 feet there was a landscaped area with at least one shrub. She thought 50% groundcover at maturity should be enough. Mr. McIlrath said Staff agreed.

DISCUSSION ON A ZONING TEXT AMENDMENT REQUEST TO ADOPT AN UPDATED SET OF DEVELOPMENT, DESIGN, AND CONSTRUCTION STANDARDS

Brad McIlrath, Senior Planner, said the Public Works Standards had been updated approximately 10 years ago. He informed the Council that since that time State Code had been amended to consider City Standards as a Land Use Regulation, therefore they must be adopted by ordinance. Staff proposed it be adopted as new section 11-1-18, which stated in summary, the design standards would be drafted by the City Engineer or designee and could be amended as determined by the City Engineer and any appeal would follow the modification process as outlined in Section 1.06 of the Development, Design, and Construction Standards.

He pointed out the proposed changes to the Detention and Retention Basin Elements. He said if the detention area was for multi-use purposes a drought resistant grass would be required. However, if it was to be used solely for detention/retention it would require that it be rock-lined and incorporated decorative, drought tolerant landscaping as required by the land use ordinance. Additionally, the proposed change would require that an oil/sediment separator shall be installed upstream of any private basin utilizing rock lining and said separators would be required to be cleaned on an annual basis, with documentation providing proof of cleaning be submitted to the City. He explained the proposed changes to the section in 3.03 Design/Layout and the standards for walkways greater than 800 feet in length. Staff wanted to ensure that the Code called out that the fencing could be chain link where permitted in the zoning.

Councilmember Peterson verified that the proposed changes were for new projects and that existing properties would be grandfathered and not have the same requirements. Mr. McIlrath confirmed that was the case. Mr. McIlrath reviewed graphics which detailed the requirements for a Standard Public Street and options for Private Streets. A private road would be at a minimum, 32-feet including curb and gutter on the side of the road and a sidewalk. In section B, the sidewalk could be optional if the sidewalk space was provided to the residents and No Parking Signs were present. Enforcement would be completed privately. Councilmember Peterson, in reference to Section B, in addition to the No Parking Signage, asked whether language could be included that required red curbed striping. Mr. McIlrath said it was a requirement. Mr. McIlrath explained requirements for temporary turnaround design and materials.

Curtis Dickson, Deputy Community Services Director pointed out that the plan included the Parks and Open Space standards as well. Mr. McIlrath confirmed it applied to Public Works as well as Parks and Open Space.

Councilmember Peterson mention she had a list of small non-substantive changes to discuss. Mr. McIlrath asked her to speak with Adam Favero. Councilmember Peterson called attention to the following sections:

1. Section 1 - Item 1.04 Definitions, Item (J) where it referenced governance in Title 12. She asked why Title 12 definitions not line up with this document. Spencer Brimley, Community Services & Economic Development Director, said it was standard language included in case something was not fully defined or where there was ambiguity.
2. Section 4 - Item 4.02 (B). She asked how the word excavating was interpreted; whether it

included general earthwork or just subterranean excavation. She hoped it would prevent a developer from working on a site before getting approval. Mr. Brimley said it differentiated between grubbing and digging and that it had more teeth than previously. Councilmember Peterson called attention to the language which gave administrative control over future edits to the standards because it became land use by being adopted into the land use ordinance. She wanted clear language delineating what the trigger was to bring back to Council. She said she had a problem with it when it said that there were sections of Title 11 that were granted blanket administrative changes without a clear mechanism of when it gets reviewed by Council. Mr. McIlrath said additional language could be added to specify that if an entire section was changed it would be presented to Council for approval. Councilmember Peterson said she was looking for clear guidelines that defined administrative authority as soon as dealing with a Land Use Authority, which was designated to a Planning Commission or City Council. Mr. McIlrath explained that Staff had seen that Layton City had adopted standards by reference and used some of the same language. He said the idea was that the standards were not codified but adopted by reference. He agreed that since it was being adopted by reference, there needed to be language that specified any major change would be presented to Council. Mr. Brimley asked for Councilmember Peterson's input as to what should trigger the need for Council's review. She thought that anything that was normally governed by the Planning Commission or Council in Title 11 would need to be reviewed.

DEPARTMENT UPDATES

JJ Allen, City Manager, in the interest of time, offered a brief overview of the Department Updates. He informed Council of the topics covered in the presentation material:

- MIDA Municipal Services Agreement.
- Enforcement on Signage – Mr. Allen advised the Council to review this information because it might get complaints from businesses. Mr. McIlrath said the enforcement was on how the code currently read. He said if needed, changes to sign ordinances could be reviewed in the future.
- Master Transportation Plan grant awarded.
- Clearfield High School Reimagined – Mr. Allen said the upcoming changes would impact parking and Clearfield City's parks.
- Council Initiatives update.

Mr. Allen said he would send the Department Updates presentation to the members of the Council via email for their review.

DISCUSSION ON FISCAL YEAR 2025 PROPOSED BUDGET

Rich Knapp, Finance Manager, gave a summary of upcoming budget meetings. He showed Council the remaining budget questions that needed to be answered through the budget review process.

Enterprise/Utility Funds

Mr. Knapp showed the potential impact of the scheduled 5.5% rate increase from Waste Management to the residents as well as the direct cost to the City. Mr. Knapp had spoken with

Waste Management where he was able to negotiate a better rate, but it would be based on a 3-year contract. He said there would be a three month lead time for 100% recycling.

Water Conservation Rates History

Mr. Knapp gave a brief history of the rate changes since 2023. He said the City had a cash problem in water because rates were not increased soon enough. He went over the Water Conservation Rates versus the Water Rate Study – January 2025 which showed the rates suggested in the Water Study compared to the current conservation rate and a draft conservation rate. He reviewed historical water revenues and the FY25 projections.

Water Sustainability

He pointed out the different factors that were taken into consideration when determining water sustainability:

- Rate Study
- Water Conservation
- Water Supply/Weber Basin cost continues to increase and was unpredictable
- Unanticipated Projects
- Inflation – impact on operations as well as project costs
- Fund some future projects with debt
- Anticipated more revenue with the Water Meter Project
- Other ways to fund Freeport

He showed the Enterprise/Utility Funds – Sustainability chart which showed that the projected FY25 Budgeted Net Revenue for the Water Fund was ahead of the rate study and the Storm Water Fund was ahead of the Rate Study. He reviewed the rate increases from Weber Basin through 2028. Mr. Knapp compared the Water Projects Cost Variance report which showed the difference between the Water Study and the Budget for water projects. It reflected that \$5.1M was not anticipated in the Rate Study. Councilmember Thompson asked how much was new infrastructure compared to repairs. Adam Favero, Public Works Director estimated that 95% of water projects were replacement.

Mr. Knapp showed the anticipated FY25 ending cash amount for water, sewer and storm. He presented some possible options based on the numbers shown: slow projects down and/or borrow more money. He was not sure how much the City could borrow with its current level of cash.

Mr. Allen asked Mr. Favero if there was an argument for slowing down projects. Mr. Favero said he drove around the City to get a feel for which projects could be delayed or cut. Mr. Favero said they could slow down the projects, but explained there were many variables that impacted the Capital Improvements Plan. Councilmember Thompson thought the City should get projects done to avoid any further inflation impacts. Mr. Allen said the City had an aging system and it would be great to have additional cash to fund them, but there were not grants to replace aging infrastructure so the City would have to borrow and hope to get best pricing on the money but to borrow the rates would have to be raised. Councilmember Ratchford asked if the City could combine purchases with surrounding cities to share in costs. Mr. Favero said it had

not been done. There was a discussion about the need to raise rates to keep infrastructure maintained.

Mr. Knapp offered some data for Council to consider when contemplating rate increases. He informed them that each 1% increase in base fee represented \$36k annually. He said the base fee was about 70% of the revenues which would be more impactful than the usage rates. He said the City should have 150 cash in the water fund and that the current cash was insufficient. He said it could not be anticipated that projects would go up 120%. Mr. Allen said that Mr. Knapp had reached out on bond financing and asked about that timeline. Mr. Knapp said the financial advisors said it would take 6-8 months. He said the City had time if the Council wanted to start the process now. Mr. Allen pointed out that it was because the projects were not under contract right now, but on deck. Mr. Knapp said if the City borrowed sooner, it could still show that it had cash. Councilmember Thompson wanted more aggressive bonding because the projects needed to get done. Mr. Allen told Council to anticipate future discussions about a water bond.

Mr. Knapp explained that each increase to the base rate for Sewer was equivalent to \$20k yearly. Mr. Favero talked about the need to coordinate sewer projects with street maintenance and water repairs. Mr. Knapp brought up that the City had six years to spend impact fees. Cash was insufficient. Mr. Knapp said the City might need to raise rates, delay projects and borrow. Mr. Knapp said the Meter Project was not in the numbers but the bond in the rate study planned to use bond money for meter project.

Councilmember Wurth moved to adjourn at 10:02 p.m., seconded by Councilmember Roper.

RESULT: Passed [5 TO 0]

YES: Councilmember Thompson, Councilmember Peterson, Councilmember Roper, Councilmember Ratchford, Councilmember Wurth

NO: None

**APPROVED AND ADOPTED
This day of 2024**

/s/ Mark R. Shepherd, Mayor

ATTEST:

/s/ Nancy R. Dean, City Recorder

I hereby certify that the forgoing represents a true, accurate, and complete record of the Clearfield City Council meeting held Tuesday, April 09, 2024.

/s/ Nancy R. Dean, City Recorder

CLEARFIELD CITY COUNCIL MEETING MINUTES
7:00 PM POLICY SESSION
April 9, 2024

City Building
55 South State Street
Clearfield City, Utah

PRESIDING: Mayor Pro Tem Karece Thompson

PRESENT: Mayor Mark Shepherd (via Zoom), Councilmember Nike Peterson, Councilmember Tim Roper, Councilmember Karece Thompson, Councilmember Megan Ratchford, Councilmember Dakota Wurth

STAFF PRESENT: City Manager JJ Allen, Assistant City Manager Summer Palmer, Public Works Director Adam Favero, City Attorney Stuart Williams, Police Chief Kelly Bennett, Community Services Deputy Director Curtis Dickson, Community Development Director Spencer Brimley, Senior Planner Brad McIlrath, Finance Manager Rich Knapp, Communications Manager Shaundra Rushton, City Recorder Nancy Dean, Deputy City Recorder Chersty Titensor

VISITORS: Pam Woods – Job Corps, Juli McIntosh – Wasatch Integrated, Vern Phipps, Lamont Hampton – Circles of Davis County

Mayor Pro Tem Thompson called the meeting to order at 7:04 p.m.

Council Member Peterson led the opening ceremonies

RECOGNITION OF CIRCLES/JOB CORPS GRADUATES

Mr. Lamont Hampton from Circles Davis County expressed his appreciation for the support from Clearfield City and looked forward to its work with Job Corps students. Certificates of Completion were presented to each Circles Program participant. Mayor Pro Tem Thompson shared his experience as a Job Corps student and encouraged the students to appreciate their opportunity at Job Corps and make the most of it.

SWEARING IN OF NEW CLEARFIELD CITY POLICE OFFICER TREVOR HAMILTON

Kelly Bennett, Police Chief, introduced Officer Trevor Hamilton as the newest addition to the police department. Nancy Dean, City Recorder, administered the Oath of Office. His wife pinned on his badge.

PUBLIC HEARING TO RECEIVE PUBLIC COMMENT ON A ZONING TEXT
AMENDMENT TO AMEND THE TRANSITION SETBACK IN THE DOWNTOWN FORM
BASED CODE

Brad McIlrath, Senior Planner, reviewed the section of the Downtown Form Based Code that Staff was proposing to change regarding the transition setback requirements for building development adjacent to single-family residential zones and explained the reasons for the proposed amendments to the zoning text.

Mayor Thompson declared the public hearing open at 7:30 p.m.

There were no public comments.

Councilmember Wurth moved to close the public hearing at 7:31p.m., Councilmember Ratchford seconded the motion.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Ratchford, Roper, Thompson, Wurth

NO: None

OPEN COMMENT PERIOD

Juli McIntosh, Wasatch Integrated Waste Management District, 1997 East 3500 North, Layton, Utah, read a statement from Nathan Rich, Executive Director, Wasatch Integrated Waste Management District regarding Clearfield City Resolution 2024R-07. He addressed inaccuracies he perceived within the resolution.

Vern Phipps, Clearfield resident, spoke regarding to Resolution 2024R-07 involving Wasatch Integrated Waste Management District (WIWMD). He discouraged the Council from passing the Resolution. He was concerned that the resolution made a statement without a solution and thought the Council needed to cooperatively work with WIWMD. He stated that the “whereas” clauses were not sufficient to warrant the conclusions. He thought the intent of the Resolution violated standard practices of the City, in that the Council approved representation of the City to the WIWMD Board. He said the proper conduit for dealing with issues with the WIWMD Board was not to address the board directly, but through the City’s representative. He was concerned that the Resolution undermined the decision of the representative and set a dangerous precedence for the Council to publicly reprimand their representative through a resolution. He recommended that the Council go to the representative expressing their dissatisfaction with the Board’s decision and ask the representative to communicate to the Board that they would like to reconsider their vote and provide specific criteria that would need to be met in order for the City to support the issue.

APPROVAL OF ORDINANCE 2024-05 APPROVING A ZONING TEXT AMENDMENT TO AMEND THE TRANSITION SETBACK IN THE DOWNTOWN FORM BASED CODE

Councilmember Peterson moved to approve Ordinance 2024-05 approving a zoning text amendment to amend the Transition Setback in the Downtown Form Based Code, seconded by Councilmember Roper.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

APPROVAL OF RESOLUTION 2024R-08 APPROVING THE COST SHARING AGREEMENT WITH DAVIS & WEBER COUNTIES CANAL COMPANY (DWCCC) FOR THE CANAL IMPROVEMENT PROJECT LOCATED AT APPROXIMATELY 300 NORTH AND BRUCE STREET

Adam Favero, Public Works Director, offered background information for the cost-sharing agreement with Davis & Weber Counties Canal Company for the project enclosing the canal at 300 North and Bruce Street.

Councilmember Ratchford moved to approve Resolution 2024R-08 approving the cost sharing agreement with Davis & Weber Counties Canal Company for the canal improvement project located at approximately 300 North and Bruce Street, seconded by Councilmember Peterson.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

CONSIDERATION OF RESOLUTION 2024R-07 REQUESTING A RECENT DECISION BY THE WASATCH INTEGRATED WASTE MANAGEMENT SPECIAL SERVICE DISTRICT TO MANDATE RECYCLING AND IMPOSE A RECYCLING FEES AND ESCALATING PENALTIES FOR NON-COMPLIANCE ON CLEARFIELD CITY RESIDENTS BE RESCINDED OR AMENDED TO ADDRESS CONCERNS PRESENTED BY THE CLEARFIELD CITY COUNCIL - CONTINUED

JJ Allen, City Manager, reminded attendees about the resolution passed by the Wasatch Integrated Waste Management District (WIWMD) Board that required mandatory recycling for all member cities and their residents. He clarified that the City discussions had not opposed recycling, but the process of that Board's action, whether it was as good as it could have been. He said the question for the Council was whether the resolution that had been prepared at the Council's direction, was one the Council wanted to move forward with. He asked if the Council wanted to strike any language or revise the resolution in any way. He informed Council that the copy of the resolution in the agenda packet was a prior version and not the most recent. The most recent version was presented to the Council.

Councilmember Peterson appreciated the comments made by Ms. McIntosh and Mr. Phipps, and apologized that the copy of the resolution in the agenda packet was inaccurate and for the confusion it caused. She desired to respond to the public comments made and assured them the intent behind the resolution was to ask WIWMD to amend portions of the roll out and no further. She thought it was important to note comments about how the decision was made by WIWMD because of the critical nature of recycling and the costs that would be borne by the district when the landfill was to close were not the issues at hand. She recognized that a Council or County Commission did not have the ability to directly challenge a decision that a regional board made because they had a governing body and a city representative that represented them on the Board. She expressed that her concerns were how the roll out could potentially impact the City's budget, so her intent was to ask WIWMD Board if they could make an amendment to eliminate any gray areas, for instance the costs that would be borne by

WIWMD and by the cities, to remove ambiguity. She thought there was unintentional language in the WIWMD resolution that asked the City to bear costs that the City should have jurisdiction over. She recommended the language be amended to address the penalties and incentives, to clarify how they would be rolled out. Secondly, she said in subcommittee meetings it had been discussed that down the road there would be hard costs to the roll out in each jurisdiction. She was concerned that if those costs were pushed onto the cities, then every city council would need to address it individually, which could potentially frustrate the ability for the WIWMD program to move forward. She was asking WIWMD to amend the resolution to ensure all of those issues were addressed by the WIWMD Board.

She outlined recommended changes to the language in Resolution 2024R-07:

1. Preamble – strike “opposition of” and replace with “Request to amend” Resolution 23-15.
2. Strike the “whereas” paragraphs that called attention to the GRAMA requests. Item had been settled.

Councilmember Peterson read the resolve items of the Resolution, drawing attention to the intentional respectful tone asking for collaboration. She recommended that language be stricken in each occurrence which referenced, “rescind” and leave the word, “amend.” She recommended that in item #1 the language be to clarify the penalties, fees, sanctions so every participating entity understood. She recommended in #3 to strike the language related to conversations with third party vendors, but maintain language that any conversations between WIWMD and third party vendors took place collaboratively where a representative of the city was included. No changes to paragraphs 4 and 5. She pointed out that those paragraphs reaffirmed that the City wanted to partner with the District.

Mr. Allen asked for clarification of the changes she proposed. Councilmember Peterson reviewed her suggested changes.

Mayor Pro Tem Thompson pointed out the third “whereas” paragraph in the resolution which mentioned the penalties, sanctions, and fines to be imposed by WIWMD with the new program. Firstly, he made clear that none of the councilmembers had opposed recycling. He explained that he felt the recycling discussion was based on the Bipartisan Infrastructure Investment and Jobs Act (BIIJA) and the changes were not happening in a vacuum. He believed that WIWMD wanted to create the recycling program in this manner because they were interested in grants that may be available through the BIIJA. Mayor Pro Tem Thompson understood that in order for entities to qualify for any grants, the EPA required the entity to achieve certain objectives such as establish, increase, expand or optimize recycling programs so all residents were required to participate. He stated that if WIWMD was interested in getting funding there were certain regulations the District needed to comply with. He did not see this initiative as a district initiative but as a federal initiative. He said we had just experienced an era of time where residents were compelled to comply with things they did not agree with and anytime he saw attacks on citizens to be compelled to buy a product it made him pause. He felt the City had to represent the citizens to protect them from mandates that were intended to get access to federal funding.

Councilmember Roper stated that he understood there were issues that he was fully aware of

while he was the City's representative to WIWMD Board during the vote for the resolution. He did not think a resolution was the way to arrive at remedies but to work with the Board. He said there were critical issues at the landfill that needed to be addressed. He said he had brought up the unique high density housing within the City compared to other cities and that it needed to be addressed. He thought those discussions could be held through the City's representative to the Board and did not support Clearfield City Resolution 2024R-07.

Councilmember Ratchford wondered if incentivizing instead of doing something for the City, was adding a cost instead of incentivizing by doing a rebate or discount. She thought the Board should look at the program to take away costs to the residents. She said there were individuals where the household income didn't match the goal of the program and thought it could suffer in Clearfield City.

Councilmember Roper said there were hardship clauses placed into the WIWMD resolution.

Councilmember Wurth also pointed out the "whereas" paragraph that addressed the penalties, sanctions, fines component of the resolution and stated that he was very sensitive to the fact that many residents were on fixed incomes. However, he was cognizant that if Davis County residents did not maintain the sustainability of the landfill and maximize its lifespan, the disproportionate impact of it coming to an end in the near future would drastically harm those individuals more. He thought it was important to extend the life of the landfill. He did not feel that putting additional roadblocks would benefit the sustainability of the landfill.

Councilmember Peterson thought the resolution would address the concerns expressed by Councilmembers Ratchford and Wurth because it would be sent back to WIWMD to address those concerns and make sure the cities understand. She pointed out that it was WIWMD Board's job to hammer out the details and concerns through the amendment process. She wanted to be careful as a City they understood it was not their call to make but the WIWMD Board. She stated that, having had a short tenure on the Board so far, after seeing some of the dynamics of the Board, she said a single city reaching out was noted but there was not a lot of weight behind it, but a formal resolution gave it a little more weight.

Councilmember Peterson moved to approve Resolution 2024R-07 with language as clarified during the meeting, seconded by Councilmember Thompson.

RESULT: Failed [2 TO 3]

YES: Councilmember Peterson, Councilmember Thompson

NO: Councilmember Roper, Councilmember Ratchford, Councilmember Wurth

Councilmember Ratchford moved to continue consideration of approval of Resolution 2024R-07 and request additional time to consider the resolution. Consideration of the resolution will be continued to May 14, 2024, seconded by Councilmember Peterson.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

APPROVAL OF A PROCLAMATION DECLARING APRIL 2024 FINANCIAL LITERACY AWARENESS MONTH IN CLEARFIELD CITY

Mayor Pro Tem Karece Thompson addressed the Job Corps students since they were required to take a financial class before graduating. He expressed the importance of financial literacy within Clearfield City. He said the household median income in Clearfield was about \$20k below the county household median income. He read the Proclamation.

Councilmember Wurth moved to approve the Proclamation declaring April 2024 Financial Literacy Awareness month in Clearfield City and authorize the mayor pro tem's signature to any necessary documents, seconded by Councilmember Roper.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

COMMUNICATION ITEMS:

MAYOR'S REPORT

Mayor Mark Shepherd

- He reported from Washington, D.C. where he had been working on a bill with Congressman Moore on military housing. He said they had gotten a financial note on it recently at \$1.7B over a 10 year period and it would be submitted in bill form to Congress soon.

CITY COUNCIL'S REPORTS

Councilmember Peterson

- She expressed her congratulations to the Circles graduates.

Councilmember Ratchford

- She reported the new fire station was 91% complete. North Davis Fire District was looking at mid-June for a possible Open House. She gave an update on various aspects of the construction project.

Councilmember Thompson

- He reported that he had visited South Clearfield Elementary with State Treasurer Oaks where they spoke about financial literacy.

Councilmember Wurth

- He expressed his congratulations to the Circles graduates.
- He was excited to see the new fire station come to completion.
- He gave his well wishes to Councilmember Roper as the Republican nominating convention would be held this Saturday.

Councilmember Roper

- He said amazing things were being done at Open Doors and the City needed to be supportive

of the Circles program. He said the food bank was at an all-time low.

CITY MANAGER'S REPORT

JJ Allen, City Manager

- Welcomed Officer Hamilton
- He reported that he and the mayor had supported Police Chief Bennett with their attendance at the banquet where Chief Bennett had received the Chief of the Year Award.
- He looked forward to conferences next week in St. George for the City Manager's conference and ULCT conference.

STAFF REPORTS

Nancy Dean, City Recorder

- No meeting April 16, 2024
- Work and Policy sessions on April 23, 2024
- Work session on April 30, 2024
- Work session on May 7, 2024
- Work and Policy sessions on May 14, 2024
- Work session on May 21, 2024
- Work and Policy sessions on May 28, 2024

Councilmember Peterson moved to adjourn at 8:31 p.m. and reconvene in work session, seconded by Councilmember Wurth.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

**APPROVED AND ADOPTED
This day of 2024**

/s/ Mark R. Shepherd, Mayor

ATTEST:

/s/ Nancy R. Dean, City Recorder

I hereby certify that the forgoing represents a true, accurate, and complete record of the Clearfield City Council meeting held Tuesday, April 09, 2024.

/s/ Nancy R. Dean, City Recorder

CLEARFIELD CITY COUNCIL MEETING MINUTES
6:00 PM WORK SESSION
April 23, 2024

City Building
55 South State Street
Clearfield City, Utah

PRESIDING: Mayor Mark Shepherd

PRESENT: Councilmember Karece Thompson, Councilmember Nike Peterson, Councilmember Tim Roper, Councilmember Megan Ratchford, Mayor Mark Shepherd, Councilmember Dakota Wurth

STAFF PRESENT: Community Development Director Spencer Brimley, Community Services Director Eric Howes, City Attorney Stuart Williams, Police Chief Kelly Bennett, City Manager JJ Allen, Community Relations Director Shaundra Rushton, Assistant City Manager Summer Palmer, Senior Planner Brad McIlrath, Public Works Director Adam Favero, Finance Manager Rich Knapp, City Recorder Nancy Dean, Deputy City Recorder Chersty Titensor, Finance Department Lee Naylor

VISITORS: Cole Ross, Roger Timmerman, Nicole Cottle, Vern Phipps, Jenna Nelson, Kathryn Murray

REPORT FROM THE UTAH TELECOMMUNICATION OPEN INFRASTRUCTURE
AGENCY (“UTOPIA”)

Roger Timmerman, Executive Director and Nicole Cottle, Director of Government Affairs of Utopia Fiber, were in attendance to present a report about the status of UTOPIA. Mr. Timmerman explained that UTOPIA was a Utah Interlocal Entity and political subdivision of the State of Utah. He reviewed a timeline which indicated that in Phase 1 its revenues covered its operating costs while cities paid the debt, and in Phase 2 its revenues covered operating costs and debt payments, which allowed them to offer no cost to participating cities. He reviewed their accomplishments as the largest open-access network with the fastest speeds in the United States. He mentioned the benefit to Utah’s economy by providing infrastructure that allowed 15 mostly local companies to compete to provide internet whose profits, revenues and employment stayed in the local economies. He explained some of the services it could provide to municipalities. He explained the fiber installed was the best version of fiber which allowed for a dedicated fiber capacity for every home/business and was not shared with a group of customers. He described future technology plans. He said UTOPIA’s success was good for the City because it allowed them to share in the cost benefit and better economies of scale. He showed that Clearfield City was trending to add 20 subscribers a month. He showed a graph which displayed the Clearfield Plan Revenue and Debt Service versus Actual numbers. It showed that the planned debt service actually came back lower and actual revenues had done very well. He said there was enough revenue to cover the debt obligations. He said the City had a fully built-out system that was paying for itself with no cost to the tax payers or the City with better quality. Mr. Timmerman said the agreement term was 27 years.

Councilmember Thompson arrived at 6:20 p.m.

Mayor Shepherd asked what UTOPIA’s plan was for undergrounding fiber in the Clearfield

Station development. Mr. Timmerman said it preferred underground if possible, and explained the process of getting notified of permits granted by the City and how it contacted the builders to know when trenches were open. Mayor Shepherd wanted to make sure the development was being monitored by UTOPIA.

PARAT TAX REVIEW

Eric Howes, Community Services Director, explained that PARAT stood for Parks Arts Recreation Aquatics and Trails. He said a sales tax initiative was first approved by the voters in 2014 and was approved for a ten-year period ending this year. The tax was \$0.01 on every \$10 spent in Clearfield (1/10 of a %). It was collected on sales from anyone shopping in Clearfield, resident or non-resident. The PARAT tax was estimated to generate an approximate \$3M; to date the City had received approximately \$2.7M.

Mr. Howes showed pictures of the various projects around Clearfield City that had been financed with PARAT Taxes:

1. Playgrounds at Central Park, Fisher Park, Hamblin Park, North Steed Park, Island View Park, and Cornerstone Park.
2. The play structure in the leisure pool at the Aquatic Center.
3. Pickleball Complex (16 lighted courts).
4. Funded Arts Supervisor as FTE 1 Year.
5. Arts Center Renovation
6. Resurfacing outdoor sport courts at Fisher, Kiwanis, Jacobsen, Barlow, Island View, Fox Hollow.
7. Dog Park at Barlow Park.
8. Bicentennial Park Playground and would be installed the first part of June.
9. Pavilion Replacements – Train Watch, Kiwanis, Bicentennial, and adding one at Thornock.
10. Mountain Bike Trails with a rolling contour trail along D&RGW Rail Trail.
11. Skate Park Upgrades – ramps had been ordered and were being fabricated.

He reviewed the PARAT Tax Budget from 2014-2024 where there was an estimated \$592,185 available. He pointed out some possible projects that could be funded with the remaining revenue. He said a second ten years of PARAT Tax would generate an estimated \$3.9M for similar projects.

He reviewed the PARAT Tax Timeline. Nancy Dean, City Recorder, said public entities were prohibited from expending public funds on promoting the proposed ballot proposition regarding PARAT Tax. She told the elected officials that they as individuals could promote, and any of the public could promote it, but the City itself could not promote it. She gave a copy of Utah State Code 20A-11-1203 to the councilmembers and let them know there was a PARAT Tax page on the City's website that could be shared. JJ Allen, City Manager, said the City could only share information which was on the website. Shaundra Rushton, Communications Manager, said a Voter Information Pamphlet would eventually be available on the City's website as well. Vern Phipps, resident, was present and offered his services to promote the PARAT Tax and he thought there were residents that would be willing to form an ad hoc committee to push it

forward. Mrs. Dean told him if any committee created were to spend money, they would need to register as a Political Action Committee with the Lieutenant Governor's office and provide campaign financial disclosures. Councilmember Ratchford asked about sharing the survey results which showed residents satisfaction with parks. Ms. Rushton said it was not factual information but rather campaigning. Stuart Williams, City Attorney, said the City could only give statistics and factual information that was neutral. Mrs. Dean encouraged the elected officials to archive anything they did to promote the tax. Mr. Phipps asked if a committee could use City facilities for meetings. Mrs. Dean said they could rent the facility.

DISCUSSION ON A DONATION TO THE DAVIS EDUCATION FOUNDATION FOR ITS ANNUAL FUNDRAISING GALA AUCTION

Nancy Dean, City Recorder, said the Davis Education Foundation had asked for a donation for its Annual Fundraising Gala Auction and Staff realized it was not a line item in the budget, which was a requirement by City Code, so it needed to be presented to Council for their approval and a public hearing needed to be held. She explained that the donations were a two-hour party pass and a Family Pass at the Aquatic Center. She said the public hearing would be held May 14, 2024.

DISCUSSION ON FISCAL YEAR 2025 PROPOSED BUDGET

Rich Knapp, Finance Manager, informed Council of the schedule of future meetings. He presented a list of questions that needed to be answered to finalize the budget. He reviewed the projection of added General Fund Revenues at various property tax rates, the monthly increase to the average household, and the average increase to a household for one year. Mr. Knapp asked for Council's feedback concerning whether the rate would be maintained or increased. Councilmember Ratchford asked to verify that the 0.001202 was the current rate. Mr. Allen confirmed that it was the current rate and that the certified tax rate would not be known until the first week of June. He said the last time the topic was discussed with Council there had been a general level of comfort to increase to the .00125 rate. He wanted to see if Council had any additional feedback. Mr. Knapp said it could impact the new staff requests. Councilmember Wurth asked to confirm whether maintaining the rate would require Truth-in-Taxation. Mayor confirmed anything other than accepting the county's rate would require Truth-in-Taxation. Summer Palmer, Assistant City Manager, pointed out that Mr. Knapp had included the projections up to .0014 because that was the highest rate Clearfield City had imposed at one point.

Mr. Knapp asked for Council's feedback on the Compensation Package discussed previously and asked Ms. Palmer to explain the medical portion. Ms. Palmer informed Council that she had recently received the information for an option from PEHP for a Higher Deductible Plan. She said they had sent some proposed plans and accompanying data. She said if the City offered a higher deductible plan, there would be a lower premium and higher employer HSA contribution. She said that plan came in at -4.7% cost difference. She said with that plan the City could offer that plan with the 90/10 split for a 6% increase instead of the 4.7% increase at the current 85/15 split. Mr. Allen clarified that there would be three plans to choose from: traditional plan, high deductible plan, and a higher high deductible plan. Ms. Palmer said the high deductible plan

cost the organization less. She said firm decisions had not been made but the City was looking at an approximate 6% premium increase.

Mayor Shepherd did not like the change, specifically going to 90/10 split of the cost for employees and the City. He said the City was picking up 85% of the insurance cost which was a lot. He said he had a long conversation with other mayors and reviewed all the cities' compensation plans. He said most cities had a merit increase and a cost of living allowance (COLA). He said most cities were around 5% and at a high of 6% . He said last year Clearfield City was at the 5% rate. He said there was a whole faction of mayors trying to put together an interlocal agreement for first responder's pay to put an end to the wage wars and stop poaching each other's employees. Mr. Allen said the City would distinguish itself with the benefits and culture.

Councilmember Ratchford asked how much demand there was for the 90/10 split on insurance premiums. Ms. Palmer said most of the time the benefits increase came in at double digits, but this year it came in at a 4.7% increase. She believed that the lower increase was due to the HDHP offerings, and thought offering an additional program might help sustain the lower increases in premiums in the future. She said by moving to the 90/10 split this year, if increases were higher next year, it would provide the City another mechanism to use to equalize rates out again. She said it was an opportunity to capitalize on the low 4.7% increase and give a lever to pull in case it was needed in the next year.

Councilmember Thompson moved to adjourn the work session to reconvene in the policy session at 7:01 p.m., seconded by Councilmember Peterson.

The work session reconvened at 8:43 p.m.

Mr. Knapp showed Council the new staff requests that had been presented to them previously and reminded the Council that they were not included in the budget numbers and if Council wanted them in the budget he needed to know. Mr. Knapp presented options for Council to consider that would allow the funding of the new staff requests. Councilmember Peterson clarified that the budget numbers reflected holding the rate and no new additional staff. Mr. Knapp confirmed.

Mr. Knapp continued by asking Council if they wanted Staff to proceed with funding the Meter Project which was estimated to cost \$4.5M. He pointed out that the 2021 rate study projected a \$3.5M cost. Adam Favero, Public Works Director, commented that the cost was more in Freeport because of lead and copper pipes and old surfaces. He said the cost included replacing the services and the boxes. He said the City had originally talked about replacing meters but after completing an audit recently there were a lot of connections that did not have meters.

Councilmember Peterson remarked on the high cost of the project but at the same time understood that Council could not keep kicking the needs for infrastructure from the 1940s down the road for another council to deal with even worse problems. Mr. Knapp pointed out that the \$3.3M might be lowered with the addition of two new staff members to help with the residential phase. Mr. Allen said there was an assumption that the City could hire two experienced staff

members to help with the project. Mr. Knapp asked if Staff could move forward with the project during FY24 and plug the project into the FY25 budget. Councilmember Peterson hesitated because she was not sure how much would be allocated for FY25. Mr. Allen said that most of the amount would be in FY25 with very little in the current year, but with Council's approval the City could start recruiting new positions. Mr. Favero said the City had enough product in stock that staff could get started on the residential portion of the project during FY24.

Mr. Knapp said the City would borrow money to get positive cash but thought some projects would need to be delayed. Councilmember Peterson asked about how much debt servicing would cost. Mr. Allen asked if Council would like Staff to get more information on bonding and what debt service would cost for better understanding of the cash implications. Council agreed that Staff needed to get more information.

There was a discussion on the need to delay capital projects and borrow because rates could not be raised high enough to generate the needed cash for projects. Council requested more information to be able to make a more informed decision. Council asked Staff to perform more research on bonding rates, priorities for delaying capital projects and recommended rates for water and then presented options for recommendations for Council to consider.

Councilmember Peterson expressed that Council needed to better understand the process behind raising rates so it could explain it more fully to residents. Councilmember Thompson brought up the increases incurred from Weber Basin Water. Mr. Allen estimated that a 15% increase to water base rate would be needed. Council wanted to see further analysis on strategy options. Mr. Allen said Staff could return with recommendations of prioritized projects, the amount Staff believed the City needed to bond for, an estimate for debt service on bonding, and based on that information what Staff thought the water rate needed to be. Councilmember Thompson thought some type of education campaign about this topic would be beneficial. He also thought Weber Basin Water needed to be included in the conversation. Councilmember Thompson said there had been a history of the baton being passed to avoid tax increases. He said he would rather tell the truth and make sure the water was right for all of us. Mr. Knapp reviewed some upcoming capital utility projects that could be delayed.

Mr. Knapp showed the rates for garbage and recycling that were being charged to the City compared to what the City was charging the residents. He asked whether Council wanted to increase the rates to residents. Councilmember Peterson said there had been a conversation at Wasatch Integrated Waste Management District (WIWMD) on rates. She said there had been a shift in the realization of how long the landfill could be extended with mandatory recycling. She said the program was anticipated to extend it by 6 months maybe a year, which was not the length they had hoped. She said the draft budget had significant fees for commercial uses. She said that was being challenged to consider increasing the cost for individual cans instead. She said rates could change. Mayor Shepherd said if recycling remained mandated then the cost for the recycling can needed to be significantly less to incentivize it. He suggested increasing the fee for the second garbage to \$15/can with information that encouraged participation in recycling. Mr. Knapp was concerned that more people would stop the second can to use a recycling can and the City would actually lose revenue. Mayor Shepherd recommended changing the recycling rate to be in line with what the City was being charged for it.

Councilmember Peterson commented that infrastructure costs needed to be borne by the residents who used the services. She did not think rates should be subsidized by the City. Mayor Shepherd said that change should be made regardless of what WIWMD decided on the mandatory recycling. Council was in agreement to increase rates to match the costs. Mr. Knapp explained the reason for the decrease in rate for the second can was because Wasatch Management charged less to pick up the second can because it was included in the one pickup. Mayor Shepherd said the City recognized the problem and changed its own rates. The Council agreed to the proposed increased rates of \$16.75 for the additional trash container and \$6.25 for the recycling container.

Councilmember Peterson wanted to have a discussion on the renovation of the dispatch space. She said she would rather buy people than materials and asked if the police department could operate without the facelift to the area. Mr. Allen said they were currently running the department without any renovation of the space, however, the police records staff were located on first floor encroaching on the area that was intended to use for victims of crime or others to come talk with a detective in an environment that was inviting and not intimidating. He said if the City were able to build out dispatch space into two suites – one for police records, and the other to move IT to be adjacent to the server room and utilize the IT space for Emergency Management. He admitted the proposed uses were optional and Staff could continue to operate as they were currently. Chief Bennett explained that if someone came into the area the records office doors could be closed, but two of the records clerks were located in the old storage room for records which was originally the closet. Mr. Howes explained components that would be involved in the renovation of the space. Mayor Shepherd pointed out that before the Council met again next week, he wanted to know what was and was not in the budget. He asked Council review the list of items and return next week with what was on their hit lists and keep lists. He asked for Council to return with guidance for Staff.

Mr. Knapp reviewed the measures of security and sustainability numbers which reflected that the General Fund currently had a net deficit of almost \$30k. He reported that the unrestricted percentage of revenues was at 17.3% which was still above the recommended 2-month Operating Expenses.

Mr. Favero wanted to mention that in regard to water projects, that if the Council considered delaying projects, it was not just delaying water, but delaying street projects as well, which the public might be unhappy about. He said things could be shuffled around, and staff could prioritize roads and forget about waterlines, but that was going against everything he thought the City should do. Mayor Shepherd said the number one responsibility the Council had was the budget. Mr. Allen said there might be areas in Operations (General Fund) where Staff could squeeze a bit but so much of operations came down to headcount and providing Staff the materials and supplies needed to do their jobs. He said there was not a lot of fluff in the current proposals. Mr. Knapp acknowledged water would be the hardest decision. He confirmed that Council would meet again to discuss April 30, 2024.

Councilmember Peterson moved to adjourn at 9:28 p.m., seconded by Councilmember Wurth.

RESULT: Passed [5 TO 0]

YES: Councilmember Thompson, Councilmember Peterson, Councilmember Roper,
Councilmember Ratchford, Councilmember Wurth

NO: None

APPROVED AND ADOPTED
This day of 2024

/s/ Mark R. Shepherd, Mayor

ATTEST:

/s/ Nancy R. Dean, City Recorder

I hereby certify that the forgoing represents a true, accurate, and complete record of the
Clearfield City Council meeting held Tuesday, April 23, 2024.

/s/ Nancy R. Dean, City Recorder

CLEARFIELD CITY COUNCIL MEETING MINUTES
7:00 PM POLICY SESSION
April 23, 2024

City Building
55 South State Street
Clearfield City, Utah

PRESIDING: Mayor Mark Shepherd

PRESENT: Mayor Mark Shepherd, Councilmember Nike Peterson, Councilmember Tim Roper, Councilmember Karece Thompson, Councilmember Megan Ratchford, Councilmember Dakota Wurth

STAFF PRESENT: City Clerk Nancy Dean

VISITORS: Nicole Cottle – Utopia, Vern Phipps, Jesse Gibbs, Kathryn Murray, and members of the Clearfield City Youth Commission – Jenna Nelson, Cole Ross, Ashlyn Peterson, Alyssa Haltli, Ian Barlow, Jason Adams, Addison Adams, Ashley Adams, Tyler Barlow, Annie Barlow, Keith Barlow, McKell Christensen – URPA,

Mayor Shepherd called the meeting to order at 7:04 p.m.

Councilmember Ratchford asked the Youth Commission to lead the group in reciting the Pledge of Allegiance and then offered a thought on diversity, inclusion and the Dignity Index implemented by the City earlier this year.

APPROVAL OF MINUTES

February 27, 2024 – work session minutes
February 27, 2024 – policy session minutes
March 19, 2024 – work session
March 26, 2024 – work session
March 26, 2024 – policy session

Councilmember Wurth moved to approve the February 27, 2024 work session minutes, February 27, 2024 policy session minutes, March 19, 2024 work session minutes, March 26, 2024 work session minutes, March 26, 2024 policy session minutes, seconded by Councilmember Thompson.

RESULT: Passed [4 TO 1]

YES: Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: Councilmember Peterson

PRESENTATION OF THE OUTSTANDING EXECUTIVE DIRECTOR OF THE
YEAR AWARD BY THE UTAH RECREATION & PARKS ASSOCIATION

McKell Christensen, Assistant Executive Director of URPA, offered her appreciation to the Council for its support of the parks and recreation programs in the City. She explained the nomination and selection process for the Outstanding Executive Professional Award which she presented to Eric Howes, Community Services Director. She explained the actions taken by

Mr. Howes to support URPA and his Staff.

PRESENTATION BY THE CLEARFIELD CITY YOUTH COMMISSION REGARDING
ATTENDANCE AT THE NATIONAL LEAGUE OF CITIES CONGRESSIONAL CITIES
CONFERENCE

Jenna Nelson, Youth Commission Advisor, introduced members of the Youth Commission who summarized their experiences in Washington, D.C. at the National League of Cities Conference:

- Ian Barlow, Chair, expressed appreciation to the Council for its support of the Youth Commission and allowing them to go to Washington, D.C. and the opportunities he had to learn how government worked at the local and national levels, and the historical value of the area. He expressed appreciation to those that helped them in their fundraising efforts.
- Jaylee Bouwhuis, Vice Chair, had the opportunity to prepare a 3-4 minute presentation on Youth Voting Rights. She said her side of the debate, Pro-Voting Rights, won the debate and discovered she cared about the topic and found it was something she would like to pursue further. She was grateful for that opportunity.
- Ashlynn Peterson, Secretary, expressed appreciation for the Council's support of the Youth Commission. She said the trip opened her eyes to the world and the bigger picture, which allowed her to see improvements that could be made to her community. She mentioned the mock-trial workshop and how big of a problem polarization was in the community. She said it helped her realize the importance of being careful with words, everyone makes mistakes, and respect for all.
- Addison Adams told Council that her favorite activity at the conference was the youth delegate workshop where they discussed lowering the local voting age. She enjoyed it because they were able to come up with their own ideas and find ways to influence society.
- Melissa Haulty, said she had gained experience and expressed her appreciation to the Council. Her favorite workshop was the discussion on polarization. She learned that people do things out of ignorance and that people needed to be taught what is wrong. She wanted to implement ways to teach others to be kinder to others, and help them recognize they could hurt others.
- Sam Bradshaw (video). He reported on his participation in the debate about lowering the voting age.

Cole Ross, Youth Commission Advisor, reported that the opportunity for the Youth Commission to participate in the conference in Washington, D.C. provided the youth a profound educational experience, which empowered them to engage in the democratic process, and learn to advocate for change. He was struck by the youth's participation in the discussions and debates where he saw the youth's understanding of opposing perspectives, allowing them to engage in thoughtful dialogue while considering all angles of the issue. He said it was very valuable for the youth to see how they could promote change at a local level and shape the future they wanted to see. He said the exposure and involvement with various elected officials offered important insights as they were able to ask questions about current events with elected officials such as Senator Mitt Romney, Congress members Blake Moore, John Curtis, Burgess

Owens and Celeste Maloy. He spoke of the importance of investing in our youth.

Jenna Nelson, Youth Commission Advisor, gave an overview of their activities on the trip and showed a slideshow presentation.

Mayor Shepherd expressed appreciation for the Youth Commission Advisors and the families that supported their youth, allowing them to attend the NLC conference.

PUBLIC HEARING TO RECEIVE PUBLIC COMMENT ON A ZONING TEXT AMENDMENT TO AMEND SECTIONS 11-3, 11-10A, AND 11-21 OF THE CLEARFIELD CITY CODE TO UPDATE THE LANDSCAPE OPEN SPACE REQUIREMENTS FOR THE P-F (PUBLIC FACILITIES) ZONE AND MAKE MINOR AMENDMENTS TO LANDSCAPING STANDARDS AND REQUIREMENTS

Spencer Brimley, Community & Economic Development Director, explained that the proposed zoning text amendment was to address the Public Works Facilities and Public Utility Facilities within the PF zones to adjust the landscaping requirements from 15% to 5%. Additionally, included a definition of Public Works Facilities. He said the amendment would add Public Works Facilities to the list of permitted uses. Public Works and Public Utility Facilities would follow Industrial landscaping standards.

He summarized additional changes to Title 11 Chapter 21 to revise some definitions, add a subsection specifically for open space and landscape standards and return tree quantity standards to 1 tree per 600 square feet of landscaped area.

Mayor Shepherd opened the public hearing at 7:45 pm

There was no public comment.

Councilmember Roper moved to close the public hearing at 7:46 p.m., seconded by Councilmember Peterson.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

PUBLIC HEARING TO RECEIVE PUBLIC COMMENT ON A ZONING TEXT AMENDMENT TO ADOPT AN UPDATED SET OF DEVELOPMENT, DESIGN, AND CONSTRUCTION STANDARDS

Mr. Brimley, informed the Council that the Planning Commission had reviewed Staff's recommendation for a zoning text amendment to adopt an updated set of Development, Design, and Construction Standards on April 9, 2024. He explained that for years there had been a desire to codify Public Works Standards within the City's Land Use Ordinances because Public Works Standards had differed throughout the State. In an attempt to do so, Staff consulted with Jones & Associates to update what had been done with a previous

consulting engineer 10 years prior. He reviewed the proposed language of the new section 11-1-18 Clearfield Development, Design, and Construction Standards in the event future changes were needed and when it would be brought to Council for review. He pointed out specifics of chain link fence changes and Traffic Impact Study Minimum Requirements. Councilmember Ratchford applauded Staff's attention to detail.

Mayor Shepherd opened the public hearing at 7:51 p.m.

There was no public comment.

Councilmember Peterson moved to close the public hearing at 7:52 p.m., seconded by Councilmember Thompson.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

OPEN COMMENT PERIOD

There was no public comment.

RECOGNITION OF THE PROCLAMATION DECLARING THE MONTH OF APRIL EACH YEAR AS "SEXUAL ASSAULT AWARENESS MONTH"

Mayor Shepherd reminded Council that April had been proclaimed "Sexual Assault Awareness Month" each year.

Jenna Nelson, Youth Commission Advisor, provided statistics on the occurrence of sexual assault. Ms. Nelson related her personal experience with sexual assault. She shared the work she had accomplished with former representative Steve Handy on two pieces of legislation. The first was to extend the statute of limitations for reporting sex crimes against minors to the age of 28. The other piece of legislation was to open the Utah Courts Exchange System to the public to allow the search of a person's criminal history. Ms. Nelson expressed the fact that education about this topic needed to be ongoing and how difficult and complex the subject was. She identified some resources available to the public: National Sexual Assault Hotline 800-656-HOPE, Rape Crisis Center, Clearfield City's Victim Advocate and Victim Housing Advocate.

APPROVAL OF A PROCLAMATION DECLARING APRIL 26, 2024 AS ARBOR DAY IN CLEARFIELD CITY

Mayor Shepherd asked Council to consider the approval of the Proclamation declaring April 26, 2024 as Arbor Day in Clearfield City. He read the Arbor Day Proclamation.

Councilmember Wurth moved to approve the Proclamation officially declaring April 26, 2024 as "Arbor Day" in the City of Clearfield and authorize the mayor's signature to any necessary

documents, seconded by Councilmember Roper.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

APPROVAL OF ORDINANCE 2024-06 A ZONING TEXT AMENDMENT TO AMEND SECTIONS 11-3, 11-10A, AND 11-21 OF THE CLEARFIELD CITY CODE TO UPDATE THE LANDSCAPE OPEN SPACE REQUIREMENTS FOR THE P-F (PUBLIC FACILITIES) ZONE AND MAKE MINOR AMENDMENTS TO LANDSCAPING STANDARDS AND REQUIREMENTS

Councilmember Peterson gave an explanation for the Council's decision to move forward with Staff's recommendation of tree spacing of 1:600 feet instead of the recommendation made by the Planning Commission of 1:500 feet in commercial developments and 1:400 feet for multi-family developments, which was based on the actual application in the community. She pointed out that line-of-sight visibility had been difficult in the Form Based Code areas. She supported the spacing of trees for both commercial and multi-family developments to remain at 1:600 feet.

Councilmember Peterson moved to approve Ordinance 2024-06 approving a zoning text amendment to amend sections 11-3, 11-10A, and 11-21 of the Clearfield City Code to update the landscape open space requirements for the P-F (Public Facilities) Zone and make minor amendments to landscaping standards and requirements, and authorize the mayor's signature to any necessary documents, seconded by Councilmember Ratchford.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

APPROVAL OF ORDINANCE 2024-07 A ZONING TEXT AMENDMENT TO ADOPT AN UPDATED SET OF DEVELOPMENT, DESIGN, AND CONSTRUCTION STANDARDS

Councilmember Peterson said she appreciated the additional wording concerning the review of future changes by Council.

Councilmember Roper moved to approve Ordinance 2024-07 approving a zoning text amendment to adopt an updated set of development, design, and construction standards, and authorize the mayor's signature to any necessary documents, seconded by Councilmember Wurth.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

COMMUNICATION ITEMS

MAYOR'S REPORT

Mayor Mark Shepherd

- He reported that he had met with the business called Draper, which was a national non-profit company focused on independent engineering and innovation who had plans for additional growth in Utah. They were currently located in the Legend Hills development in Clearfield. He said they held roundtables to help train the future generation with the main emphasis on the future workforce and STEM with the youth. Draper had a number of internships at each location for college students as well as at the high school level.
- Argentine Corner's Grand Opening would be held April 24, 2024. The ribbon cutting would be held at 11:11 a.m.
- He said Kings Market at Clearfield Junction would be opening May 1, 2024.
- Lease signed today for Hive Virtual Motor Sports, SIM racing, next to Argentine Corner at Clearfield Junction.
- He said there was one 800 sq. ft. office space remaining in Clearfield Junction left to lease.
- He talked about the work on the balconies at Clearfield Junction which had revealed more issues. He said the owner was dealing well with them and had taken responsibility.
- He announced the last "Lunch with the Mayor" for the year. He said they would discuss "Why I love America".
- He said Representative Blake Moore would be hosting a Military Focus Town Hall at the Air Force Museum on Wednesday, April 24, 2024 at 2:30 p.m.
- He and Spencer Brimley, Community & Economic Development Director, met with 47G where the company had agreed to purchase the last of the planes. He said all nine planes had been sold. He said they had put together a good marketing plan with them.
- He announced that Betty Parker, Property Manager at the Freeport Center, was retiring after over 40 years. He said her replacement was Colby Cooley. He was previously with the Economic Development Corporation of Utah (EDCU), specifically focused on business development.
- He expressed his appreciation for all who attended ULCT meetings in St. George.
- He would be on vacation, Thursday, April 25 through Sunday, April 28, 2024.
- He reported that he and Councilmember Wurth had attended a Finance, Administration, and Intergovernmental Relations (FAIR) meeting at the NLC conference where they discussed Grants and Direct Pay. He explained that direct pay was a major success for the FAIR Committee and that it was a tax credit for cities. He said it was specifically for energy-related projects coming out of the Infrastructure Investment and Jobs Act. He said it could be beneficial for the City to look into if the City were to pursue solar opportunities.

CITY COUNCIL'S REPORTS

Councilmember Peterson

- She expressed her appreciation to Staff for coordinating the St. George trip.
- She reported that the Wasatch Integrated Board would meet next week where one of the main items to be discussed was the intricacies and in-depth information on the roll out of the mandatory recycling program. She said while that was in discussion, the Clearfield Resolution was paused. She was looking forward for Wasatch Integrated to visit the Council.
- She announced she would be out of Town May 7-14, 2024.

Councilmember Thompson

- He reported on some experiences at the NLC Conference in Washington D.C. He said it was a wonderful trip, and he learned a lot. He said seeing Washington, D.C. gave him a glimpse of what the founding fathers were trying to build as they sought a more perfect union.
- He said he would be out of Town May 19-23, 2024.

Councilmember Ratchford

- She reported that she had participated in the Republican Caucus where she thought there was room for growth and ability for us to exercise the dignity index.
- She gave an update on the construction of the fire station and thought it would be open by the end of May beginning of June. NDFD would be reviewing the budget on April 25, 2024.
- She participated in Great Utah Shake Out on April 18, 2024.
- She learned about house bills being enacted at the Utah League of Cities and Towns conference. She said she learned that the State could not cool the housing rate down, but needed to raise the education to attain ability to afford the housing.
- She loved all the training and was excited to implement what she learned.

Councilmember Wurth

- Expressed appreciation to the Staff for arranging their attendance at the ULCT Conference. Met people and other elected officials and sat in on informal meetings.
- He shared his feelings about the members of the Youth Commission who attended the NLC Conference in Washington, D.C. and remarked on the fact that they did not talk about the monuments they saw but the issues they discussed there. He was struck by the Youth Commission's attitudes and engagement with the Congressional Delegation. They seemed to understand at their young age that elected officials were people and when a person talks with elected officials, they will engage about issues that matter to the person. He said the kids spoke truth with power. He said they were dedicated and impressive kids and expected to see all of them on a dais someday.
- He reported that the Mosquito Abatement gave displays of their new drones. He said those drones could cover a lot of areas that had been previously inaccessible.
- Parks Commission would be holding an event called, "Happy Trails" on Saturday, May 18, 2024 at Steed Park. He encouraged all to bring a friend that morning.

Councilmember Roper

- He said he had nothing to report. Councilmember Peterson then reported that Councilmember Roper could not join them at the ULCT conference in St. George because he attended the Chamber Business Expo which was a huge event for local cities.

CITY MANAGER'S REPORT

JJ Allen, City Manager

- He announced a new employee, Stockton Trujillo, who was the new Emergency Management Services Coordinator.
- He reported that he had been in St. George last week for the Utah City Management Association meetings. He informed Council that Summer Palmer, Assistant City Manager was the Education Chair and coordinated the conference.
- He congratulated Eric Howes for his award.
- He expressed appreciation to Admin help and would celebrate Admin Professionals Day.

STAFF REPORTS

Nancy Dean, City Recorder

- She told Council to anticipate Council meetings each week through June.

Eric Howes

- He invited Council to attend the Arbor Day Celebration on Friday, April 26, 2024 10:00 a.m. at the Aquatic Center. He said this would be the 29th year as Tree City.

Councilmember Thompson moved to adjourn the policy session at 8:31 p.m. and reconvene in a work session, seconded by Councilmember Wurth.

RESULT: Passed [5 TO 0]

YES: Councilmember Peterson, Councilmember Roper, Councilmember Thompson, Councilmember Ratchford, Councilmember Wurth

NO: None

**APPROVED AND ADOPTED
This day of 2024**

/s/ Mark R. Shepherd, Mayor

ATTEST:

/s/ Nancy R. Dean, City Recorder

I hereby certify that the forgoing represents a true, accurate, and complete record of the Clearfield City Council meeting held Tuesday, April 23, 2024.

/s/ Nancy R. Dean, City Recorder



STAFF REPORT

TO: Mayor Mark Shepherd and the Clearfield City Council
FROM: Brad McIlrath, Senior Planner
MEETING DATE: Tuesday, May 28th, 2024
SUBJECT: Public Hearing, Discussion, and Possible Action on the Clearfield Connected 2024 Station Area Plan & Design Guidelines

STAFF & PLANNING COMMISSION RECOMMENDATION

Staff recommends that the Clearfield City Council review and adopt the Clearfield Connected 2024 Station Area Plan & Design Guidelines subject to the changes recommended by the Mayor and City Council in the work sessions outlined below.

On January 3rd, 2024, the Planning Commission forwarded a recommendation of approval to the City Council for the station area plan as drafted. The recommendation was made on an unanimous vote.

CITY COUNCIL WORK SESSION – JANUARY 9, 2024

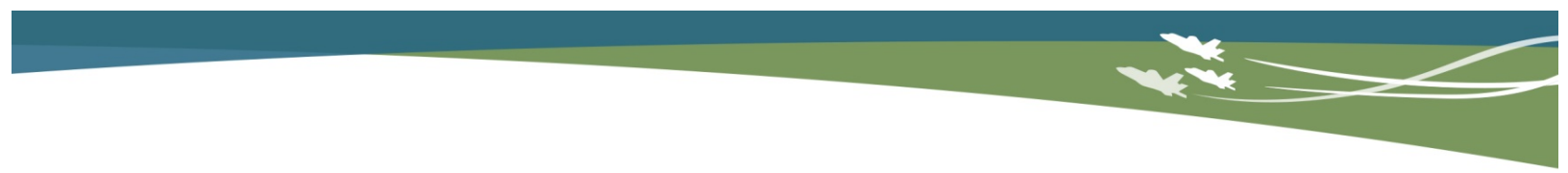
Following a recommendation of approval by the Planning Commission on January 3rd, 2024, Clearfield City Planning Staff and Landmark Design presented the draft plan to the Mayor and City Council. As a result of the comments during the work session and a follow-up discussion with councilmembers Peterson and Roper, Landmark Design and Planning Staff marked the SAP draft for the basis of the work session discussion. Staff and Landmark Design sought direction regarding the requested changes prior to making changes to the SAP and presenting the plan for final adoption with the City Council.

CITY COUNCIL WORK SESSION – FEBRUARY 27, 2024

After reviewing and coming to an understanding of the comments and needed edits Staff and Landmark Design met with the Mayor and City Council to have a thorough review (as a quorum) of the draft plan. Direction was provided by the Mayor and City Council regarding changes needed to the future land-use and illustrative master plan. The focus of the discussion was on properties east of State Street (SR -126) and scaling back those proposed future land-uses to be highway commercial. Other changes included primary facades and wording in sections that struck a policy direction. Landmark Design has made the changes noted in that meeting which have been confirmed by Staff. This discussion is to review the plan edits and confirm with the Mayor and City Council that the SAP is ready to continue with the adoption process.

CITY COUNCIL WORK SESSION – May 14, 2024

During a work session on this date, Planning Staff and Landmark Design Staff reviewed the updates to the station area plan to confirm consistency with the direction previously provided by the Mayor and



City Council. Upon reviewing the changes of the plan, the Mayor and City Council indicated that the plan had been revised in accordance with their direction and recommended it be placed on the May 28th, 2024 for public hearing and possible adoption.

The following sections are provided in this report for information and contextual purposes and have been carried over from the previous staff reports.

DESCRIPTION / BACKGROUND

Clearfield Connected 2023 is the update to the original station area plan that was completed in 2019. This plan update is to align with State Code requirements for station area plans passed during the 2022 legislative session. The most significant change in the plan is the expansion of the plan area to a ½ mile radius as required by State Code. As part of the Mixed-Use Zoning of the UTA owned property adjacent to the Frontrunner Station, a Master Development Agreement (MDA) and Plan (MDP) was executed in 2020 between Clearfield City, UTA, Hamilton Partners, and Stack Real Estate. The plan update incorporates the MDP into the overall design of the updated station area and does not alter any aspects of that agreement or plan.

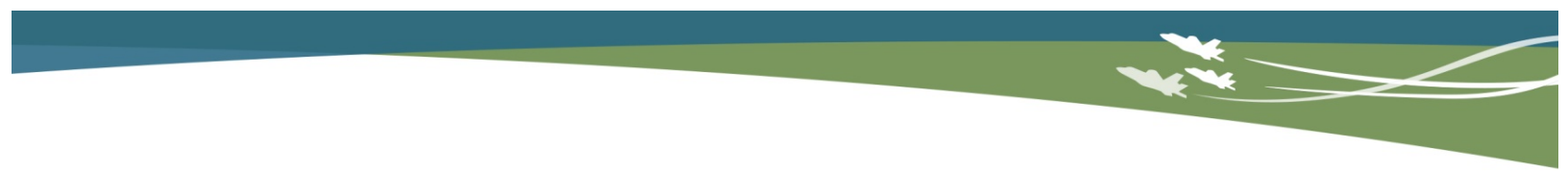
With the help of Landmark Design and their subconsultants, staff has overseen and worked to create *Clearfield Connected 2023*. Open houses were held in June 2023 and November 2023 to solicit feedback from the community on how the station area could develop in the future. Additionally, a steering committee including Councilmember Tim Roper; Clearfield City, WFRC, and UTA planning and development staff; and members of the Stack Real Estate team have convened three times (May, June and November 2023) to review progress, analyze public comment, and provide feedback on draft elements of the plan.

The Wasatch Front Regional Council was assigned by legislation to review and certify each station area plan in their metropolitan planning area. Following the adoption of the station area plan update by Clearfield City, the plan will be presented to WFRC for review and certification.

STATE CODE REQUIREMENTS

On page 8 of *Clearfield Connected 2023*, Utah State Code changes are reviewed and compliance with those standards is stated. As stated on that page of the plan, the updated Clearfield Connected Station Area Plan specifically encompasses the following additions and modifications to comply with State Code:

1. Assessment of prior studies and the existing conditions of the study area, focusing on the expanded Station Area “zone of influence,” (1/2 mile radius) changing development patterns, and recent demographic and socio-economic changes.
2. Incorporation of statewide objectives for moderate-income housing, environmental conditions, transportation choices, and access to opportunities.

- 
3. Updated design guidelines that better align with the MDP.
 4. Assessment of the Station Area's market potential and the synergies of commercial and multi-family residential uses, as part of a mixed-use transit district.
 5. Assessment of access to and from the Station Area for vehicles, transit, and active transportation modes, including pedestrians and bicyclists.

PROJECT GOALS

As shown on page 25 of the plan the twelve goals for the project are:

1. Increase the availability & affordability of housing.
2. Promote sustainable conditions & practices.
3. Enhance access to opportunities.
4. Increase transportation choices & connections.
5. Create an exciting destination.
6. Create a complete community.
7. Provide community assets.
8. Promote quality urban design.
9. Maintain convenient transit access.
10. Generate transit ridership.
11. Connect the station area to the City & region.
12. Promote the City's industrial heritage.

FUTURE LAND-USE AND ILLUSTRATIVE MASTER PLAN

Chapter 3 of the plan includes an analysis of the districts, framework for future land-use and an illustrative master plan of how the station area could develop. These plans have been updated to reflect the direction provided by the Mayor and City Council on February 27, 2024.

LIST OF ATTACHMENTS

- *Clearfield Connected 2023: Station Area Plan & Design Guidelines – DRAFT w/Change Areas*

CLEARFIELD CONNECTED 2024

STATION AREA PLAN + DESIGN GUIDELINES **DRAFT**

April 16, 2024



ACKNOWLEDGMENTS

MAYOR

MARK SHEPHERD

CITY COUNCIL

KENT BUSH
NIKE PETERSON
VERN PHIPPS
MEGAN RATCHFORD
TIM ROPER
KARECE THOMPSON
DAKOTA WURTH

PLANNING COMMISSION

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WFRC STAFF

BYRON HEAD

2024 CONSULTANT TEAM

LANDMARK DESIGN
ZIONS PUBLIC FINANCING, INC
PARAMETRIX

2019 CONSULTANT TEAM

IBI GROUP
ZIONS PUBLIC FINANCING, INC
FEHR & PEERS



Parametrix



Funding provided by the Transportation and Land Use Connection

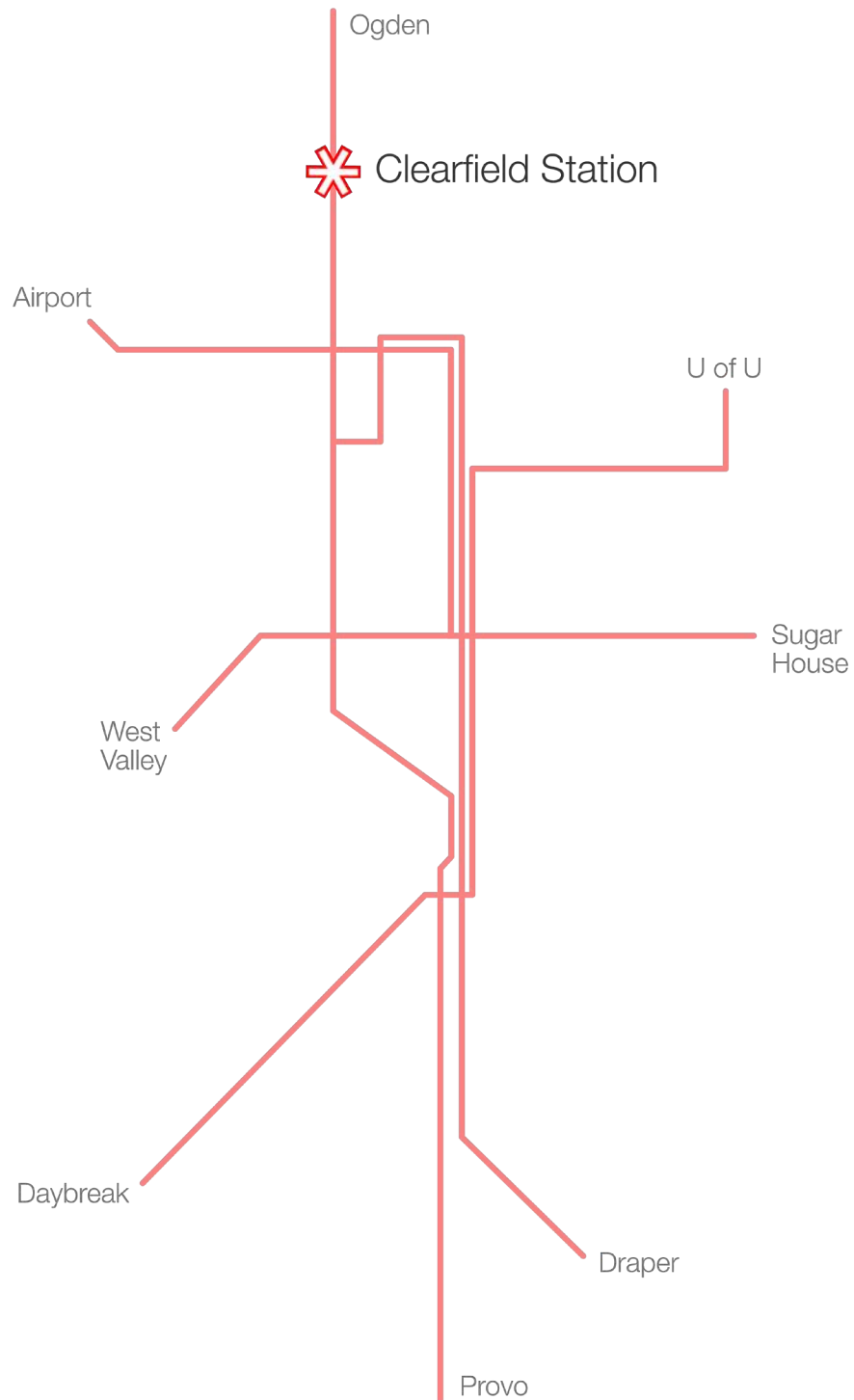
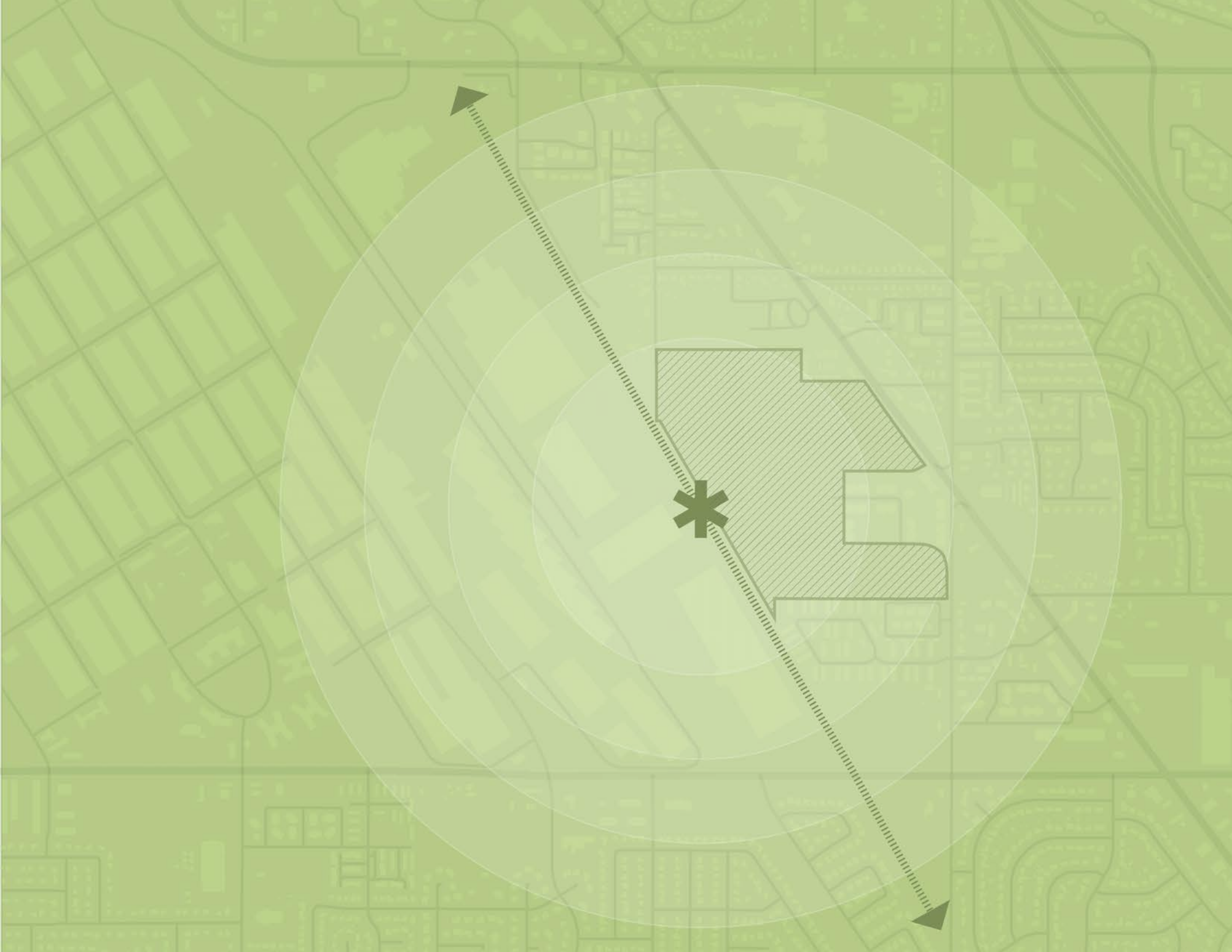


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01

INTRODUCTION

Introduction

Clearfield Connected 2024 is an update of Clearfield Connected, which was adopted in 2019. The new plan updates the vision, details and design guidelines for the Clearfield Station Area, while addressing subsequent development changes and new Station Area planning requirements recently established by the State of Utah.

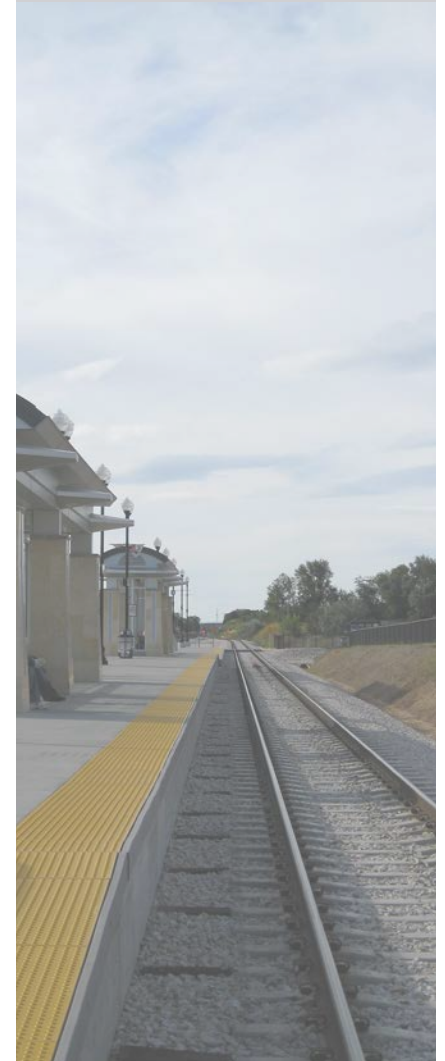
Clearfield Connected 2024 establishes the needs and vision for the FrontRunner rail system and the Clearfield Station Area, which encompasses approximately 56 acres of vacant land. In addition to meeting recent state code requirements, the Station Area plan also incorporates visioning and design elements from the Station Area Master Development Plan (MDP), which were completed in 2020 and executed between Clearfield City, UTA and the Hamilton Partners and Stack Real Estate master development team.

Clearfield Connected 2024 is a significant opportunity to meet the transit and place-making needs of Clearfield City and its residents, as well as those of UTA, the State of Utah and transit riders throughout the region. It builds upon the planning process established in the 2019 plan, expanding the vision and scope. It also establishes clear implementation principles and design guidelines to help regulate the form and quality of the area.

Clearfield Connected 2024 presents a more comprehensive vision for the area than the 2019 plan. It is fully-aligned with the comprehensive planning needs of Clearfield City, UTA and the State of Utah. Once implemented, the station and its surrounding area can leverage the benefits of current and future growth, and in the process be better connected with regional needs and changes.

DOCUMENT OVERVIEW

The purpose of **Clearfield Connected 2024** is to establish the vision, goals, urban design principles, and design guidelines that will govern future development of the Clearfield Station Area. This document lays out the structural and regulatory structure that will guide the development of the Clearfield Station Area. Graphic depictions and photos are included to help illustrate general ideas, principles, and visions for the building elements and spatial character of the station and surroundings.



COMMUNITY & STAKEHOLDER ENGAGEMENT

A comprehensive outreach strategy was utilized throughout the planning process to collect multiple levels of focused input from the public and specific individuals, groups, and stakeholders.

A Steering Committee, composed of representatives from City leadership, UTA, development partners, and other key stakeholders, met with the planning team three times at key points during the planning process.

A Plan Alternatives Public Open House was held at the Clearfield Aquatics and Fitness Center on June 28, 2023. City residents and stakeholders connected with city leaders, staff, and the planning team to learn more about the project and provide feedback on three alternative concepts. Posters were left on display for an additional week following the meeting so residents could continue to provide feedback. City staff also took the boards to Clearfield's Freedom Festival on the Fourth of July. Though the total number of participants is unknown, it is estimated that at least fifty people gave feedback during this period.

A Draft Plan Public Open House was held on November 13, 2023 at the Clearfield Aquatics and Fitness Center, providing an opportunity for residents and stakeholders to learn more about the Draft Plan and provide feedback prior to the adoption process.

A dedicated project website served as a clearinghouse for information and project updates and included comment forms for the community and stakeholders to provide feedback virtually.



Images from the Alternatives Open House on June 28, 2023

Meeting State Requirements

Recent changes in Utah State planning codes require the Clearfield Connected Station Area Plan (2019) be amended to address a wider service area and to incorporate options for affordable housing. The updated plan embraces previous efforts, translating the energy underpinning those plans into an updated and comprehensive plan that also addresses the new elements required by state code.

The updated Clearfield Connected Station Area Plan specifically encompasses the following additions and modifications:

- Assessment of prior studies and the existing conditions of the study area, focusing on the expanded Station Area “zone of influence,” changing development patterns, and recent demographic and socio-economic changes.
- Incorporation of statewide objectives for moderate-income housing, environmental conditions, transportation choices, and access to opportunities.
- Updated design guidelines that better align with the MDP.
- Assessment of the Station Area’s market potential and the synergies of commercial and multi-family residential uses, as part of a mixed-use transit district.
- Assessment of access to and from the Station Area for vehicles, transit, and active transportation modes, including pedestrians and bicyclists.



Context

HISTORIC CONTEXT

Clearfield was settled in 1877 as an agricultural community. The structure of the city began to change in the 1940's, when major defense facilities such as Hill Field and the Clearfield Naval Supply Depot were constructed within and adjacent to the city. Construction on Hill Air Force Base began in 1940, and the base soon became one of the most significant employers in the region. The air base remains one of the largest employers in the state, and continues to employ many local residents.

The Clearfield Naval Supply Depot was constructed in 1942 adjacent to the railways that line the west edge of Clearfield Station today. The depot also became a major employer, but was decommissioned in 1962. The remnant facilities of the depot eventually became the Freeport Center, which is now a major manufacturing, warehousing, and distribution center.

The city is a major employment center and home to many large companies, many of which are located in or around the Freeport Center.

The Clearfield Station site is east of the railroad tracks and has historically been used for light industrial uses.



Naval Supply Depot, 1942 (Source: Weber State University)



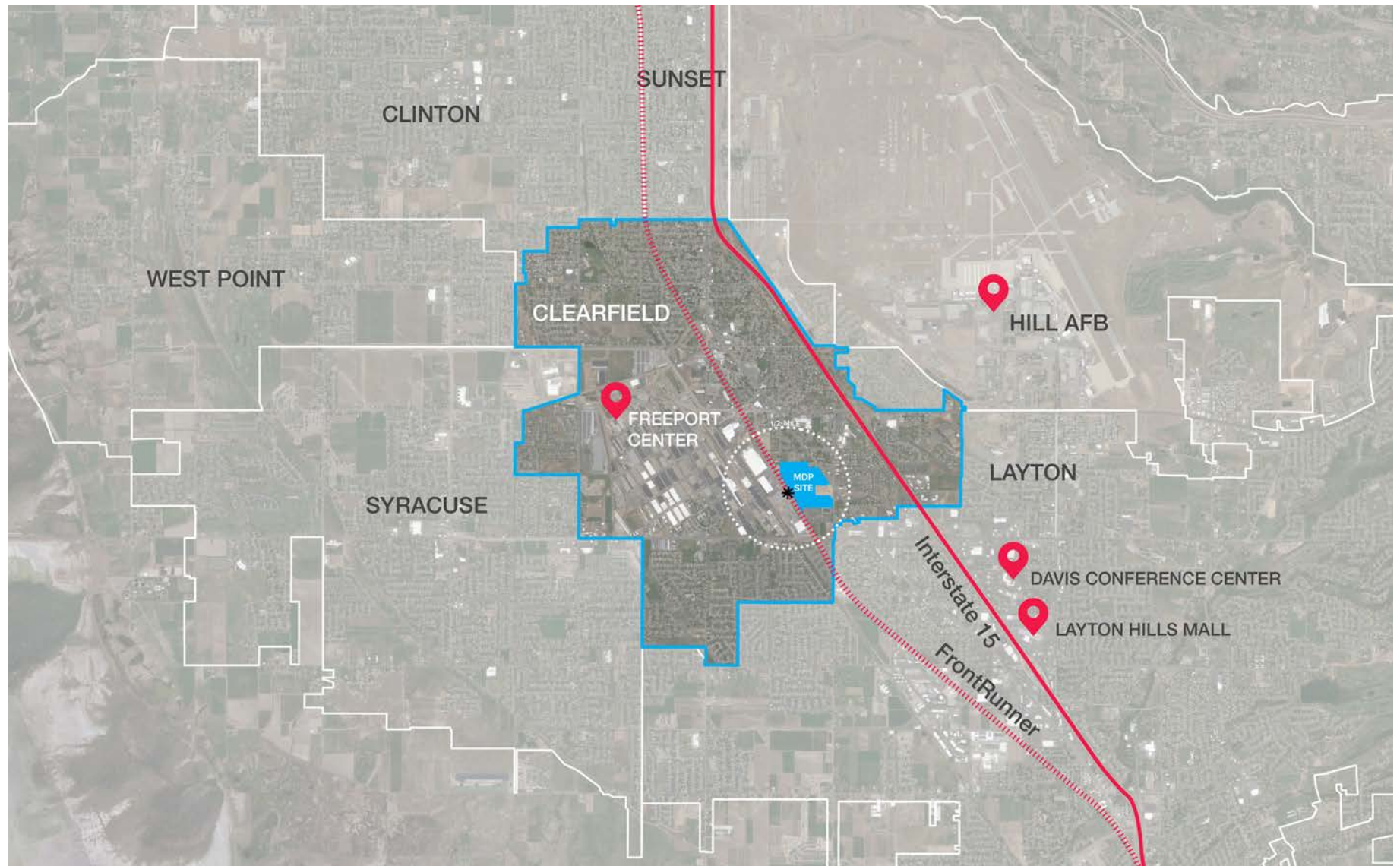
Hill Airforce Base, 1958 (Source: The Salt Lake Tribune)

REGIONAL CONTEXT

The City of Clearfield is located 28 miles north of Salt Lake City in northern Davis County. It is situated between the Great Salt Lake to the west and the Wasatch Mountains to the east, encompassing an area of about 7.7 square miles. The city is located in a key location southwest of Hill Air Force Base—the State's largest economic engine.

Interstate-15 runs along the eastern reaches of the city, providing interchanges at 650 North and 700 South / SR 193. 700 South and Antelope Drive are the largest corridors for east-west traffic movement in northern Davis County. Clearfield lies 30 miles north of the Salt Lake International Airport.

REGIONAL CONTEXT MAP



The Clearfield Station Area

The Clearfield FrontRunner Station is one of sixteen stops along the Frontrunner commuter rail line that runs approximately 90 miles along the Wasatch Front, connecting users between Ogden in the north and Provo to the south. The rail line has established Clearfield Station as a key regional connection.

The Clearfield Station Area (also known as the Station Zone of Influence) includes all parcels within a half mile radius of the Clearfield Station. As illustrated in the Local Context Map on the following page, it encompasses the UTA-owned MDP site and extends into the surrounding neighborhoods. It also includes a portion of the Freeport Center and commercial properties along State Street and Antelope Drive.

The MDP site encompasses approximately 56 acres of undeveloped land between the rail line/FrontRunner tracks and State Street. It contains the largest amount of vacant UTA-owned land adjacent to a FrontRunner or TRAX transit station in the entire UTA system. The site is currently used as a park-and-ride lot for transit riders, with new roads and a few structures currently under construction. As mentioned previously, this site has already been planned in the

Clearfield Station Master Development Plan (MDP).

This plan incorporates the existing neighborhoods within the Zone of Influence into the overall design of the Station Area, while capitalizing on opportunities for positive transformation. At buildout, the Clearfield Station Area will be a cohesive neighborhood that seamlessly incorporates existing apartments and other established uses into the overall structure of the area.

VEHICULAR ACCESS

Access to Interstate-15 is available approximately one-mile northeast of the MDP site along 700 South, and to the southeast along Antelope Drive. State Street (SR 126) is a major north/south arterial that fronts the site to the east and provides access to Clearfield City Center in the north and the greater Wasatch Front region north and south. The Salt Lake International Airport is located approximately 30 miles south of the site and is easily accessible via I-15/Legacy Highway and by FrontRunner with a direct connection along the TRAX light rail system. Local traffic in proximity to the Station Area is controlled by a signal located at the

intersection of 1000 East and State Street and will be controlled with proposed intersections at Station Boulevard and 1450 South later on as the MDP site develops.

PEDESTRIAN & BICYCLE ACCESS

The Denver and Rio Grande Western Rail Trail is a dedicated active transportation facility within the Station Area. This paved facility is part of the Golden Spoke Route and US Bike Route 77, providing trail connections north to Ogden and south to Provo. There are several planned active transportation line and point projects in the area, according to the North Davis Active Transportation Plan and the 2023 WFRC RTP.

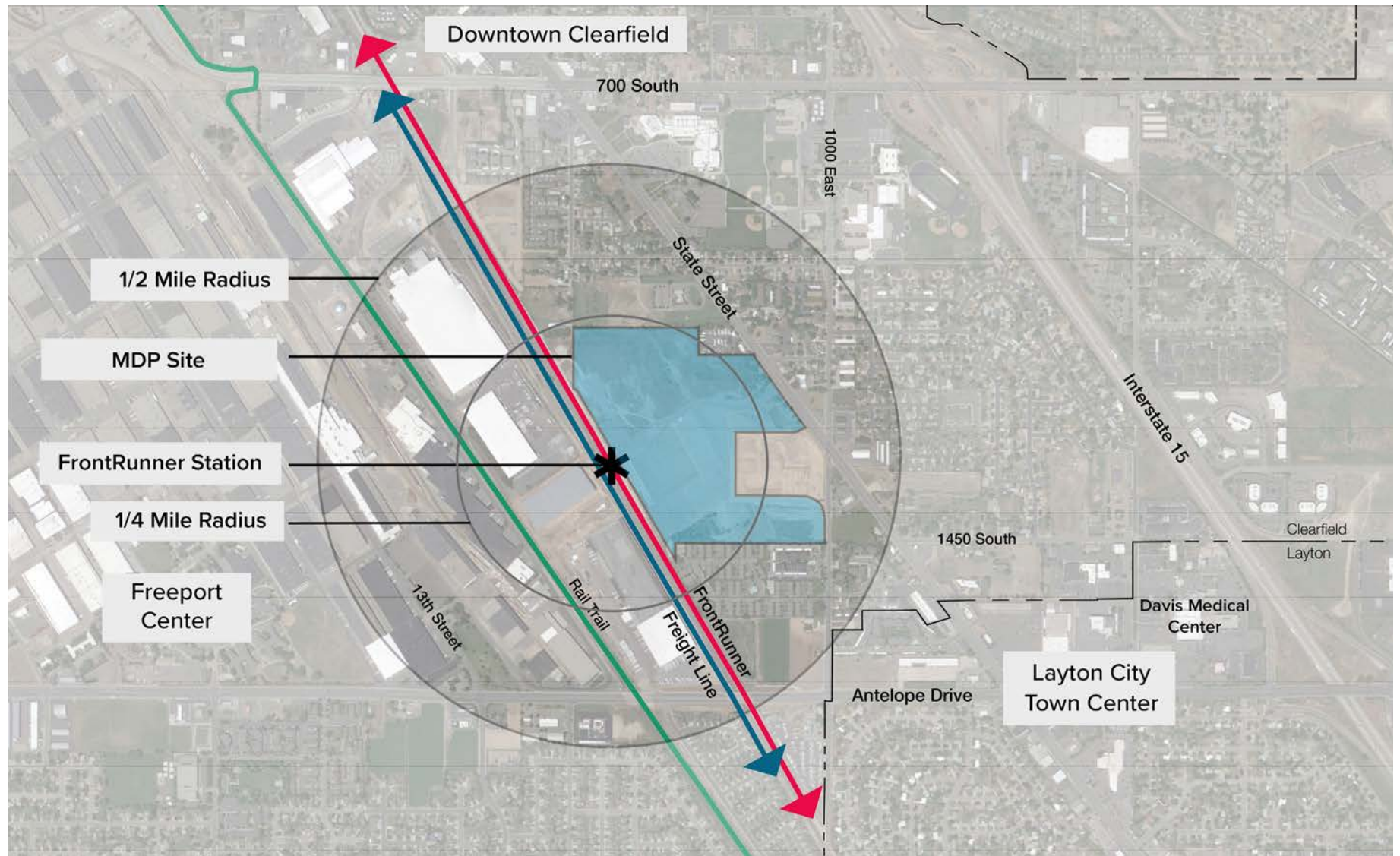
Bike lanes are planned for Depot Street, 1000 East, 1450 South, 700 South, on Antelope Drive west of 1000 East, and Station Boulevard. Additional planned projects include a protected bike lane on State Street, a trail connection from the FrontRunner Station south to Antelope Drive, a shared-use path on Antelope Drive west of 1000 East, and neighborhood byways on 1150/1100 South.

Other planned pedestrian and bicycle enhancements include at-grade pedestrian/bike crossings at 1150 South State Street and at 1000 East and Antelope Drive, and a planned at-grade trail connection between the Denver and Rio Grande Western Rail Trail and the planned shared-use path on Antelope Drive.

The site is connected to the rest of the City through streets and sidewalks on the east side of the property, although the connections are currently limited. The multi-family development on the south of the site is currently separated by a fence with no connections provided into the site. The north boundary of the site currently lacks any connections, although Depot Street is proposed to connect to the site, allowing vehicular, pedestrian, and bicycle connections to the north.

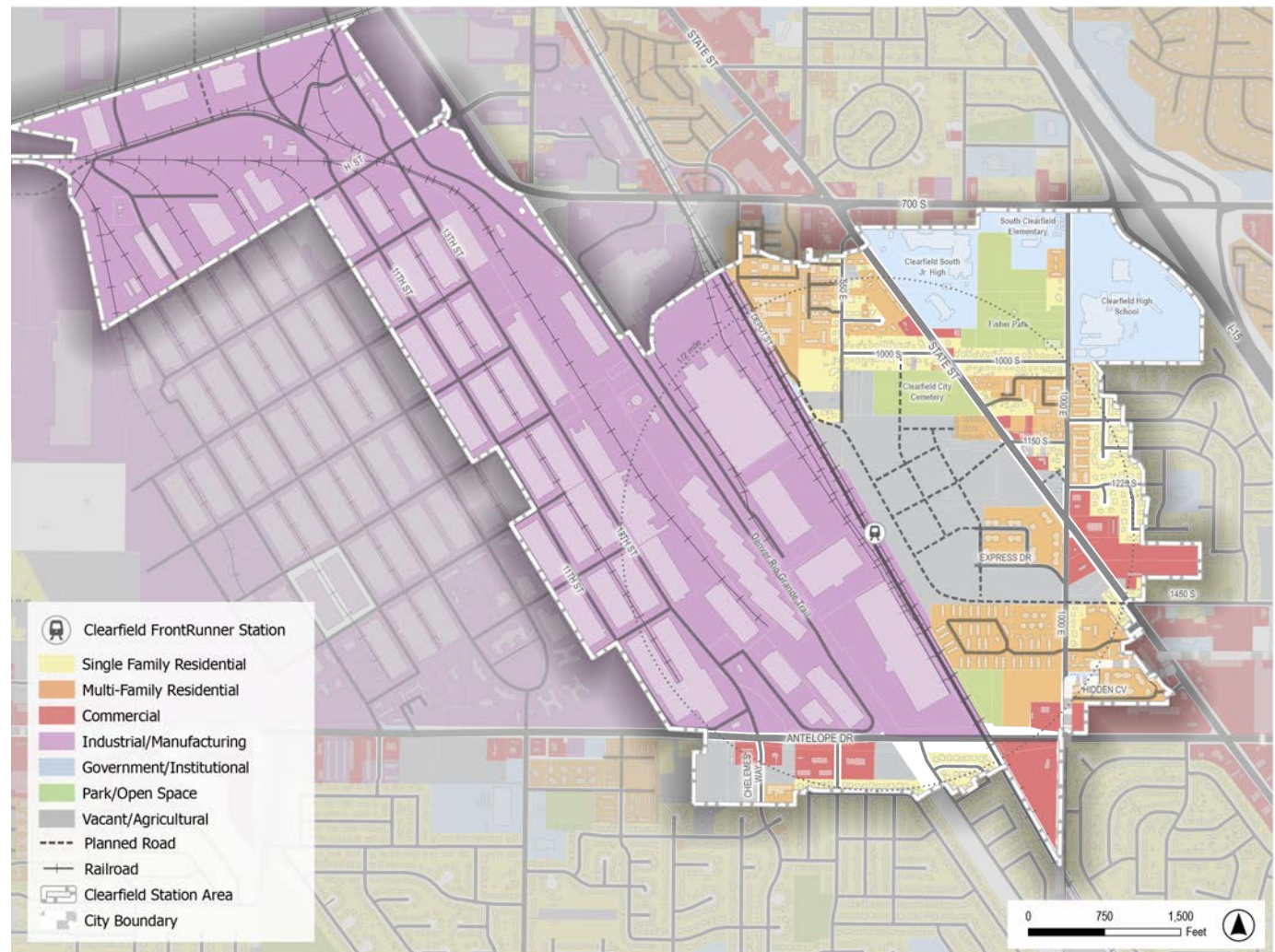
There is very limited access to the property from the Freeport Center to the west of the property. The rail lines adjacent to the site are significant barriers, preventing direct pedestrian and cycle linkages to the Station Area. Similar access and crossing challenges exist along State Street, Antelope Drive and 700 South, due to the heavy traffic and lack of bike / pedestrian infrastructure.

LOCAL CONTEXT MAP



Existing Land Use + Ownership

The accompanying map shows the general land-uses that encompass the Station Area. To summarize, the MDP site is currently owned by the Utah Transit Authority (UTA). Existing parking lots are legally non-conforming uses with maintenance rights. Current land uses surrounding the site are primarily single-family and medium-density residential housing. East of the site is the State Street commercial corridor. The Freeport Center is to the west, which hosts a variety of industrial uses including processing, assembling, manufacturing and warehouse storage. A handful of commercial uses are located on the south side of Antelope Drive.



Existing Conditions Analysis

LAND USE

With a limited amount of vacant land remaining in the Station Zone of Influence, most development is expected to occur within the MDP site. However, opportunity exists for transitional land uses along the edges of the site, which would support implementation of the MDP and help create a more complete station district. The map to the right highlights these sites as Potential Transformation Areas.

TRANSPORTATION

The Clearfield Station Area is currently auto oriented, with little to no access with adjacent land uses. Despite this, a large percentage of station users are pedestrians, even though there has been little infrastructure to support it. Recent infrastructure improvements to the MDP site will help better support pedestrians and cyclists.

Planned trails to the north and south of the station will help accommodate active transportation users, particularly the direct connection to the Denver and Rio Grande Rail Trail.

Overcoming active transportation barriers across State Street through

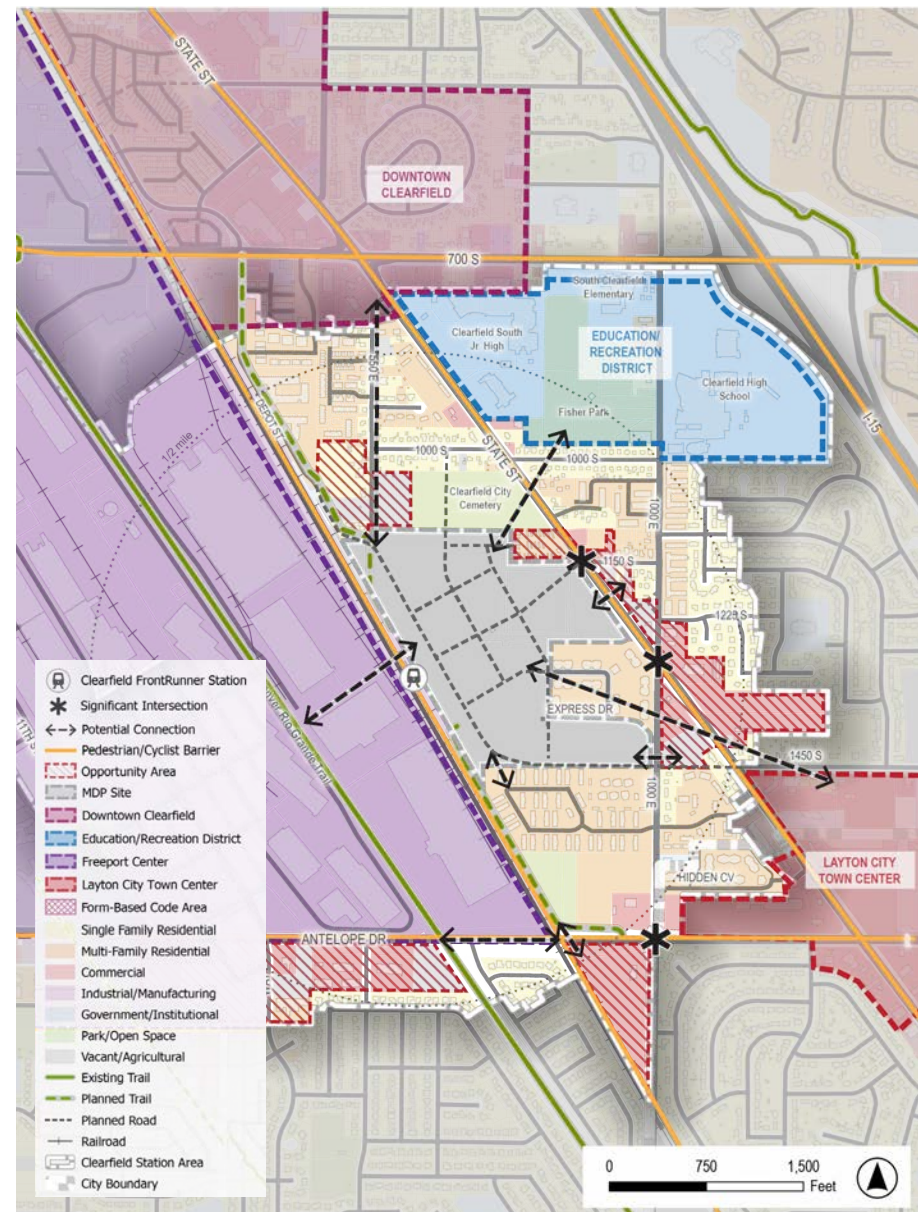
well-planned crossings will be key to providing meaningful connections to areas east of the station.

MARKET CONDITIONS

Clearfield City is a regional employment center with employment expected to continue to grow over the coming decades. Northern Davis County is projected to add 20,000 more jobs by 2040.

The city is only capturing 41% of its expected taxable sales for its population. The office space market is experiencing a slow down, with vacancy rates on the rise and negative absorption rates in 2022. The greatest market demand is for residential, flex office, and flex industrial. Strong population and employment growth are also fueling demand for retail. Retail will be the highest revenue generator for the city.

For a more detailed assessment on existing conditions see Appendix A: Existing Conditions Report.



The Need for an Updated Plan. The Potential for this Area.

Why Here? Why Now?

The current development market is thriving and this area possesses a unique mix of factors that could come together to make it a highly sought after development opportunity. The following features and factors clearly illustrate the extraordinary opportunities offered in the Clearfield Station Area, and the favorable external factors that make conditions prime for quality development.

THE FRONTRUNNER STATION

The FrontRunner Station is an incredible asset for Clearfield, as it connects the City to much of the Wasatch Front. Together with the bus system and other transit choices, it provides residents with the option of commuting and getting around the region without a car.



POPULATION GROWTH

As one of the fastest growing states in the country, Utah is expected to grow another 50% by 2040. Unfortunately, rapid growth has led to a lack of housing, which has resulted in significantly increased housing costs in recent years. This has led to a strong demand for more housing, most particularly compact and efficient multi-family residences. There is also a specific need for multi-family housing, which is most effective in high-quality, mixed-use neighborhoods.



STRONG ECONOMIC CONDITIONS

Utah currently has one of the strongest economies in the nation and is one of the fastest growing states in the nation. There is strong pressure for growth in both housing and employment opportunities.



ECONOMIC INCENTIVES

The Station Area is eligible for significant economic incentives that will help make the high-quality development that this document envisions financially feasible. Some of the key programs include funding incentives such as the local RDA/CRA that is currently in place, as well as the federally designated Opportunity Zone incentives that this area is eligible for.



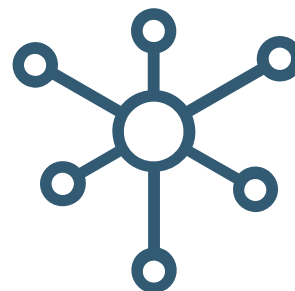
COMMUNITY ASSETS

The development of offices and housing in this area will generate demand for amenities that will provide benefits not only for residents and employees of the Station Area, but for the City as a whole. Anticipated amenities include high-quality public open space, enhanced street amenities, retail shops and restaurants, and similar uses and features.



REGIONAL HUB

The station is located across the railroad tracks from the Freeport Center near the Clearfield-Layton border. It is also close to Hill Air Force Base (northeast), Holy Cross Hospital - Davis (southeast), Downtown Clearfield (north), the planned Layton City Town Center (south), and an education/recreation district composed of three public schools and a park to the northeast.



OPPORTUNITY TO CREATE SOMETHING GREAT

The Station Area provides an opportunity to create something great in Clearfield and Northern Davis County. A thoughtful, collaborative Station Area plan that is based on market realities will encourage interest from the development community to create a great place that will help put Clearfield on the map.



Transit Oriented Development (TOD)

WHAT IS TOD?

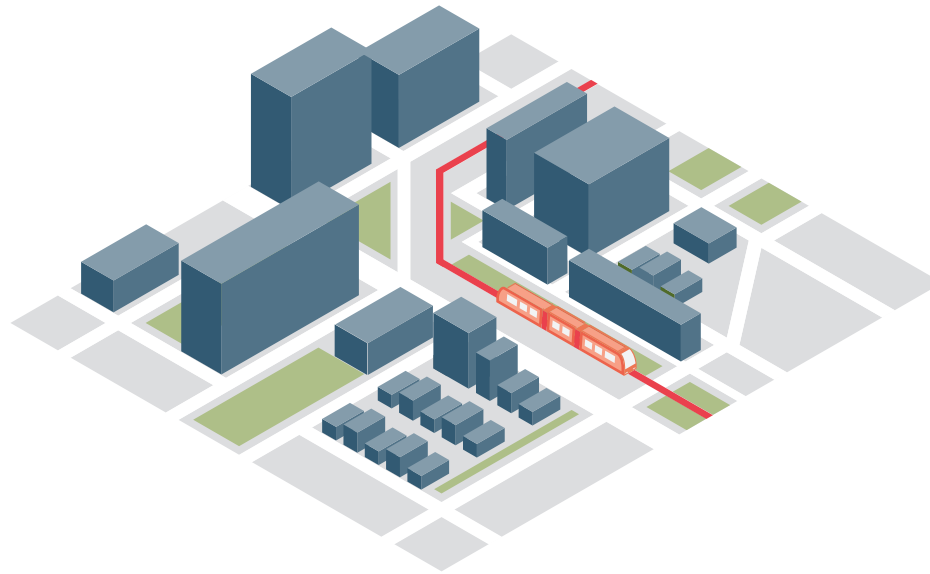
With its direct connection to a major transit station, the Clearfield Station Area is ideally suited for Transit-Oriented Development, which is essentially a development strategy that aims to make the most of the development possibilities near a major transit station. It is defined by Reconnecting America, one of the leading TOD organizations, as “a type of community development that includes a mixture of housing, office, retail and/ or other amenities integrated into a walkable neighborhood located within a half-mile of high quality public transit.”

WHAT’S DIFFERENT ABOUT TOD?

For decades, cities have often segregated uses, with single family homes, multifamily homes, offices, retail, civic uses, and more all zoned into their own areas within the larger city. TOD takes a different approach by mixing compatible uses in each neighborhood or city district, which is more akin to the way cities formed before cars became prevalent, and allowing residents to travel long distances between home, work, and other destinations. TOD leverages access to public transportation to create districts where transit, walking, biking, and other modes of transportation come together to create neighborhoods that hearken back to traditional cities and villages. The results are not only great places to live and work, but great destinations that are walkable, unique and provide a close-knit community feel.



Elements of Transit Oriented Development (TOD)



ELEMENTS OF TOD

The major elements of a TOD can be broken down into three categories (which conveniently correspond with the TOD acronym).

- Transportation
- Open Space
- Development

TRANSPORTATION **T**

TOD brings a range of transportation modes together. Transit, walking, bicycling, driving, and similar modes are served by specially-designed infrastructure and amenities (lanes, parking, transit stops, stations, sidewalks, etc.) that allow residents and visitors to travel safely, conveniently, and comfortably, regardless of the selected mode they choose.

OPEN SPACE **O**

Public spaces (i.e. plazas, patios, parks, and sidewalks) form the places between transportation facilities and buildings of the Station Area. These are where the life of the station and city play out and where people come together. Open space can be public or private, but should always be designed to be accessible, user-friendly, attractive, and fun for all.

DEVELOPMENT **D**

These are the buildings and structures where a range of human activities take place. A well-designed mix of housing, employment, shopping, and other uses are the core of station development. This mix results in appropriately-scaled and well-designed buildings that relate to and activate the surrounding open spaces and streets and support transit ridership with essential density.

UTA Goals for TOD

UTA GOALS

UTA-owned land near transit stations must be developed in accordance with Transit-Oriented Development Design Guidelines adopted by the agency. These provide direction for joint-development partners on the design elements that UTA expects to be addressed in development plans, such as connectivity and development form.

Unlike other typical land owners, UTA has development expectations and goals that extend beyond making a profit. As a public transit provider with a clear objective to generate the best return from their investments possible, UTA is also charged with maintaining a strong relation between its property development and public service activities. All development on UTA-owned land near UTA stations is carefully reviewed by UTA staff to ensure compatibility with these goals. Local jurisdictional codes must also be followed when developing plans to ensure they are not in conflict with UTA guidelines.

Clearfield Connected 2024 and the design guidelines it contains have been created to be in accordance with the following goals and UTA's Transit-Oriented Development Design Guidelines. While meeting these goals

can be challenging, staying the course will ensure that UTA continues to fulfill its responsibility to the public as a world-class transit operator, which in turn will make TOD not only feasible but a preferred model for future development.

GOAL 1: INCREASE RIDERSHIP

UTA understands that the real estate market drives development feasibility. In fact, appropriately designed residential and employment centers can generate significant increases in ridership. As a result, both vertical and horizontal mixed uses are strongly encouraged at in Station Areas.

Unfortunately, some land uses simply do not generate the level of ridership UTA expects for TOD. For example, an employment center that has low worker densities or hours of operation do not allow workers to utilize the transit system for commuting and are not considered transit supportive. The primary objective of UTA is to maximize the public transit investment at their Station Areas.

GOAL 2: OPTIMIZE DEVELOPABLE LAND AND SUPPORT THE REGIONAL GROWTH VISION

Helping to meet the challenges of rapid population growth along the Wasatch Front is a critical goal for UTA. Land uses that reduce the negative impact of this growth are at the heart of the UTA TOD program. This includes support for the 3% Strategy developed by Envision Utah, which calls for 33% of future development to occur on 3% of available land. It also supports the Wasatch Choice Vision, that calls for the development of higher density “centers” and “corridors” across the Wasatch Front that are served by high capacity transit.

Both strategies were developed with tremendous public input and regional coordination, and address issues like poor air quality, traffic congestion, auto dependency, and housing equity. They also support regional economic development and improved access to transit through first and last mile strategies.

GOAL 3: GENERATE REVENUE

Like any property owner and development partner, UTA expects to realize a suitable return when developing its property. While UTA receives most of its operating revenue from a local option sales tax, joint-development is seen as a new and innovative revenue approach to help fund future improvements and operations.

Design Guidelines Overview

INTENT

This document contains design guidelines that regulate development in Clearfield Station Area. The design guidelines correspond with the TOD elements outlined on page 18, and are found in the Transportation + Mobility (T), Open Space + Public Realm (O), and Buildings + Architecture (D), sections of this document.

The intent of the Design Guidelines is to establish strong urban design principles and quality development, while also establishing a clear and coherent design theme and a consistent look and feel throughout the Clearfield Station Area.

The guidelines provide a design vocabulary that is unique to Clearfield Station. They promote a sense of aesthetic continuity, ensure high quality development, and help establish a clear and distinct community identity.

DESIGN REVIEW COMMITTEE (DRC)

A Design Review Committee (DRC) should be established to review all development in the Clearfield Station Area to verify each project meets the vision for the greater Station Area. It is also the responsibility of the DRC to ensure all applicable design guidelines are followed.

INTENT STATEMENT

The intent statement establishes the over-arching design intent for each category or topic. This has been structured to help designers understand the rationale and aspirations that lie behind the design guidelines. In the event the guidelines and standards are not clear or appropriate, the intent statement shall be referenced as the primary source of direction for project designers and the Design Review Committee (DRC).

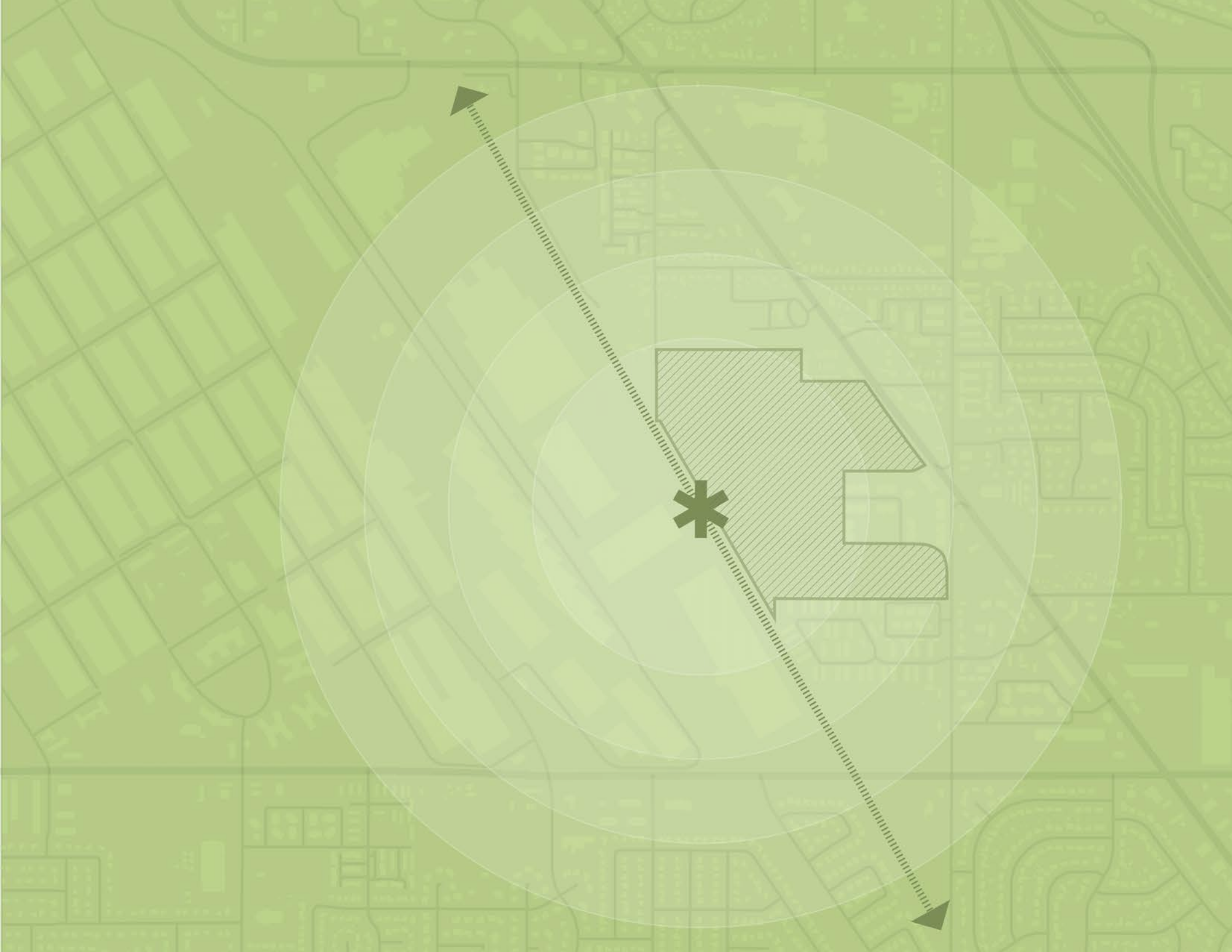
DESIGN GUIDELINES

The design guidelines provide specific direction that designers should reflect in their projects. The guidelines ensure that a level of consistency is achieved across the various projects that will occur at the station and surrounding areas, thereby helping all participants in the design and development process achieve a sustained and even level of quality.

The design guidelines typically use the term “should” or “may” to indicate ideas and directions that should be implemented when possible or practical. Conversely, when the word “shall” or “must” is applied, the designers and developers are required to meet the stated requirements to obtain approval from the DRC.

In the event that a guideline is not applicable or appropriate, a process is established to provide flexibility, whereby the DRC may grant exceptions if the applicant can clearly demonstrate that a more appropriate solution is consistent with the intent, vision and project goals as presented in this document.

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02

***PROJECT
VISION +
GOALS***



CLEARFIELD STATION

The Clearfield Station Area is a thriving, mixed-use, walkable neighborhood that leverages multiple transportation options to create a complete community connected to the Wasatch Front. It will become a regional destination that provides abundant opportunities for employment, living, shopping, recreation, and more, which will all merge together to create a great place.

The **12** Goals for this Project Are...

01 INCREASE THE AVAILABILITY & AFFORDABILITY OF HOUSING

02 PROMOTE SUSTAINABLE CONDITIONS & PRACTICES

03 ENHANCE ACCESS TO OPPORTUNITIES

04 INCREASE TRANSPORTATION CHOICES & CONNECTIONS

05 CREATE AN EXCITING DESTINATION

06 CREATE A COMPLETE COMMUNITY

07 PROVIDE COMMUNITY ASSETS

08 PROMOTE QUALITY URBAN DESIGN

09 MAINTAIN CONVENIENT TRANSIT ACCESS

10 GENERATE TRANSIT RIDERSHIP

11 CONNECT THE STATION AREA TO THE CITY + REGION

12 PROMOTE THE CITY'S INDUSTRIAL HERITAGE

Project Goals for Clearfield Station

INCREASE THE AVAILABILITY AND AFFORDABILITY OF HOUSING

As a primary TOD area in the region, Clearfield Station Area is critical for merging the affordable housing goals described in the Clearfield General Plan. It is therefore essential that the Station Area includes residential densities necessary to facilitate affordable housing options within ½ mile of the station, and in the process provide affordable living opportunities that are aligned with citywide housing and transportation goals.



PROMOTE SUSTAINABLE CONDITIONS AND PRACTICES

The Station Area and the areas that lead to it should exemplify sustainable design and development practices necessary for maintaining the environmental integrity of the city and region. Chief among these practices is the conservation of water resources through efficient land use and application of state-of-the-art practices, the improvement of air quality by reducing fuel consumption and motor vehicle trips, and establishing parks, open space, and recreational opportunities within the plan area.



ENHANCE ACCESS TO OPPORTUNITIES

The Station Area should leverage a mixed-use, TOD design approach to maintain and improve the physical and logical connections between housing, employment, education, recreation, and commerce. Enabling opportunities in proximity to the transit station should be supported through ancillary actions that provide enhanced broadband connectivity throughout the area.



INCREASE TRANSPORTATION CHOICES AND CONNECTIONS

As a regional mixed-use TOD destination, the Station Area should include the necessary infrastructure to support all modes of transportation. This will not only make better public transit investments, but also help ensure the station is a safe environment for pedestrians, cyclists, and other non-motorized modes of transportation. Such actions should be further supported through the creation of manageable and reliable traffic conditions and be aligned with regional transportation plans.



CREATE AN EXCITING DESTINATION

Clearfield Station Area provides an unique amenities that help create an exciting user experience. It will be a significant employment center and destination for people from surrounding communities and the larger Wasatch Front.

The public realm (streets and open spaces) is designed in a way that makes the neighborhood walkable and friendly, providing unique and exciting experiences for users.



CREATE A COMPLETE COMMUNITY

The Clearfield Station Area provides a mix of land-uses that work together to create a complete community. The primary land uses are office, commercial, and residential supported by retail, restaurants, food markets, public gathering spaces and other neighborhood services, all within walking distance of each other and the station.



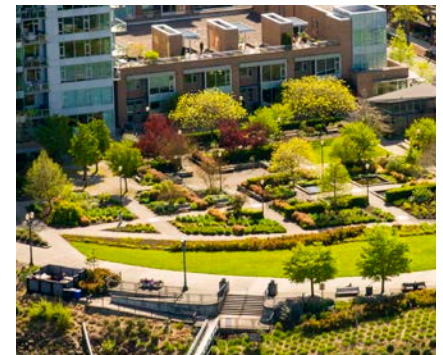
PROVIDE COMMUNITY ASSETS

Clearfield Station Area is an asset to the larger community, providing a number of community assets such as parks, plazas, recreation facilities, and vibrant, walkable streetscapes. All development in the neighborhood should promote livability for residents and visitors.



PROMOTE QUALITY URBAN DESIGN

Clearfield Station Area is designed and planned according to sound urban design principles that promote walkable, safe, and livable streets. All development exhibits quality architecture, landscape architecture, and urban design, which is unified to create a great "place."



MAINTAIN CONVENIENT TRANSIT ACCESS

The Clearfield FrontRunner Station continues to be a convenient and functional park-and-ride destination for nearby residents. Parking is provided in close proximity to the station platform to accommodate commuters, and the existing bus access loading/unloading zone will remain to encourage further transit ridership. Convenient automobile and bus access will be provided without jeopardizing safe pedestrian circulation. Improvements to the Station Area will enhance the user experience for park-and-ride users by providing a transit plaza with convenient retail options.



GENERATE TRANSIT RIDERSHIP

The land uses and location of new development are arranged to maximize transit ridership by locating the densest uses closest to the platform, with the least dense uses on the periphery. This also includes developing uses that act as origins and destinations for transit riders.



CONNECT THE STATION AREA TO THE CITY + REGION

Clearfield Station Area incorporates multiple transit modes that provide residents, commuters, and visitors with a variety of transportation choices that connect the Station Area to the city and region. These include commuter rail, bus, and personal vehicles, as well as safe and friendly pedestrian and cycling facilities. Additional streets are created that connect Clearfield Station to the rest of the city.

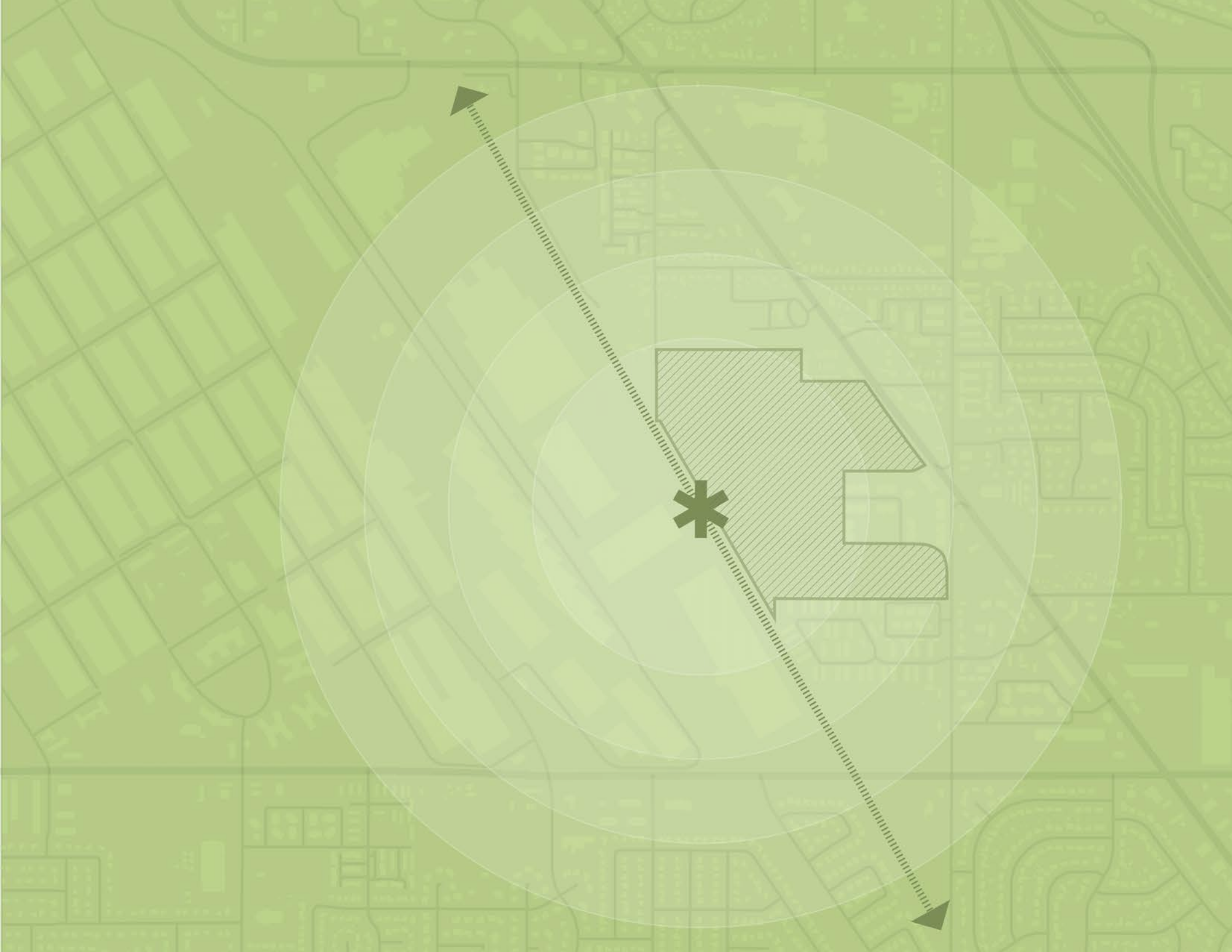


PROMOTE THE CITY'S INDUSTRIAL HERITAGE

Clearfield Station Area promotes the city's long history as an industrial job center by integrating a contemporary industrial look and feel to the architecture and design of the neighborhood. This industrial character is displayed through the spirit of the place, providing the amenities and experiences needed to support a modern-day workforce and help it perform as one of the leading employment centers in the region and state.



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03

***FRAMEWORK
+CONCEPT
PLAN***

Framework + Concept Plan

OVERVIEW

This framework and concept plan builds upon the established vision and goals (*Chapter 2: Project Vision + Goals*) and the *Existing Conditions Analysis* (see *Appendix A*). It provides a foundation for future development within the Station Area, with a focus on currently vacant and underutilized land. This concept plan includes four layers of varying detail: Districts, Framework, Future Land Use, and Illustrative Master Plan. Together these layers provide a basis for the development of a thriving walkable station district.

INTENT

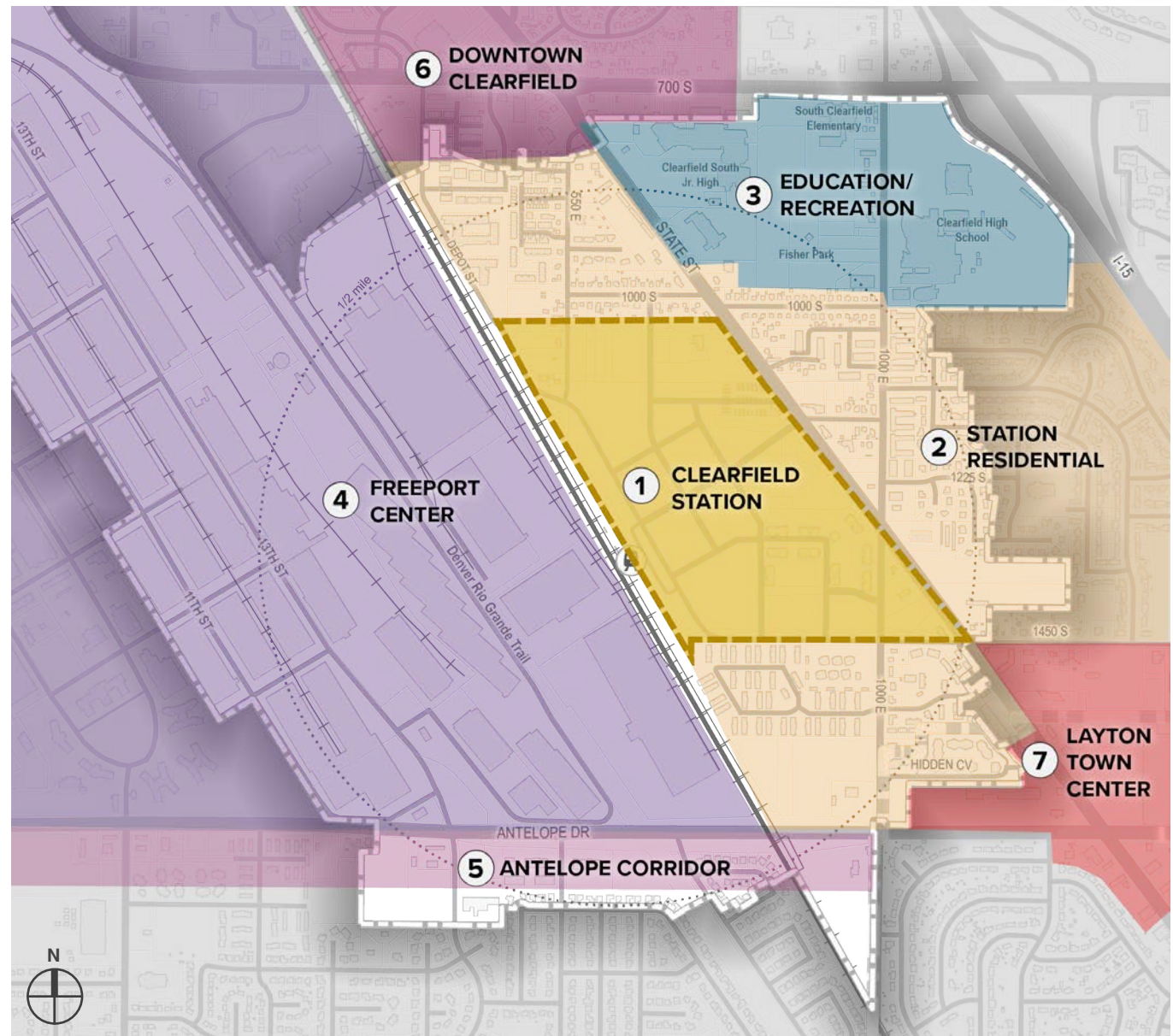
Provide a clear plan for future development of Clearfield Station Area that reflects existing conditions and the vision and goals established for the area.

ILLUSTRATIVE RENDERING: PERSPECTIVE VIEW: VILLAGE SQUARE



Districts

The station Zone of Influence is divided into five districts within the 1/2 mile Zone of Influence (Clearfield Station, Station Residential, Education/Recreation, Freeport Center, and Antelope Corridor), each with a unique character based on their land uses. The zone of influence also has relationships with two additional districts, namely Downtown Clearfield and Layton Town Center. Each district contains a unique personality established by the specific setting, character and uses.



DISTRICT CHARACTER

① CLEARFIELD STATION

Clearfield Station is the heart of the neighborhood and the focus of this plan. It is the most dense and active district, where people come to work, live and connect. As a mixed-use district, it provides connections between the station and office, residential, retail, and public open spaces.



② STATION RESIDENTIAL

This district encircles the Clearfield Station District on the north, east, and south. The area provides a range of residential and ancillary uses that help to create a transition between the densely developed MDP Site and lower-density areas beyond.



③ EDUCATION/RECREATION

This district offers access to a full range of K-12 public schools, including Clearfield High School, North Davis Junior High, and South Clearfield Elementary. The district also includes high-level park and recreation opportunities at the Clearfield Aquatic and Fitness Center and Fisher Park.



④ FREEPORT CENTER

This is an important and well-established industry and job generating district. Separated from the station by a north-south running regional rail line, the district is physically close but difficult to connect due to the barriers created by the rails. As a result, Freeport Center has limited effect and influence on the Clearfield Station Area.



⑤ ANTELOPE CORRIDOR

This district straddles the south edge of Antelope Drive, bringing a mix of roadway-oriented commercial and medium-density residential uses to the area. The district helps buffer the lower density residential uses directly to the south, while offering additional commercial and housing options within the greater Station Area.



⑥ DOWNTOWN CLEARFIELD

Downtown Clearfield lies just outside the Clearfield Station Area. Together, the two centers help to establish Clearfield as one of the most diverse, dynamic, and mixed-use communities in the region.



⑦ LAYTON TOWN CENTER

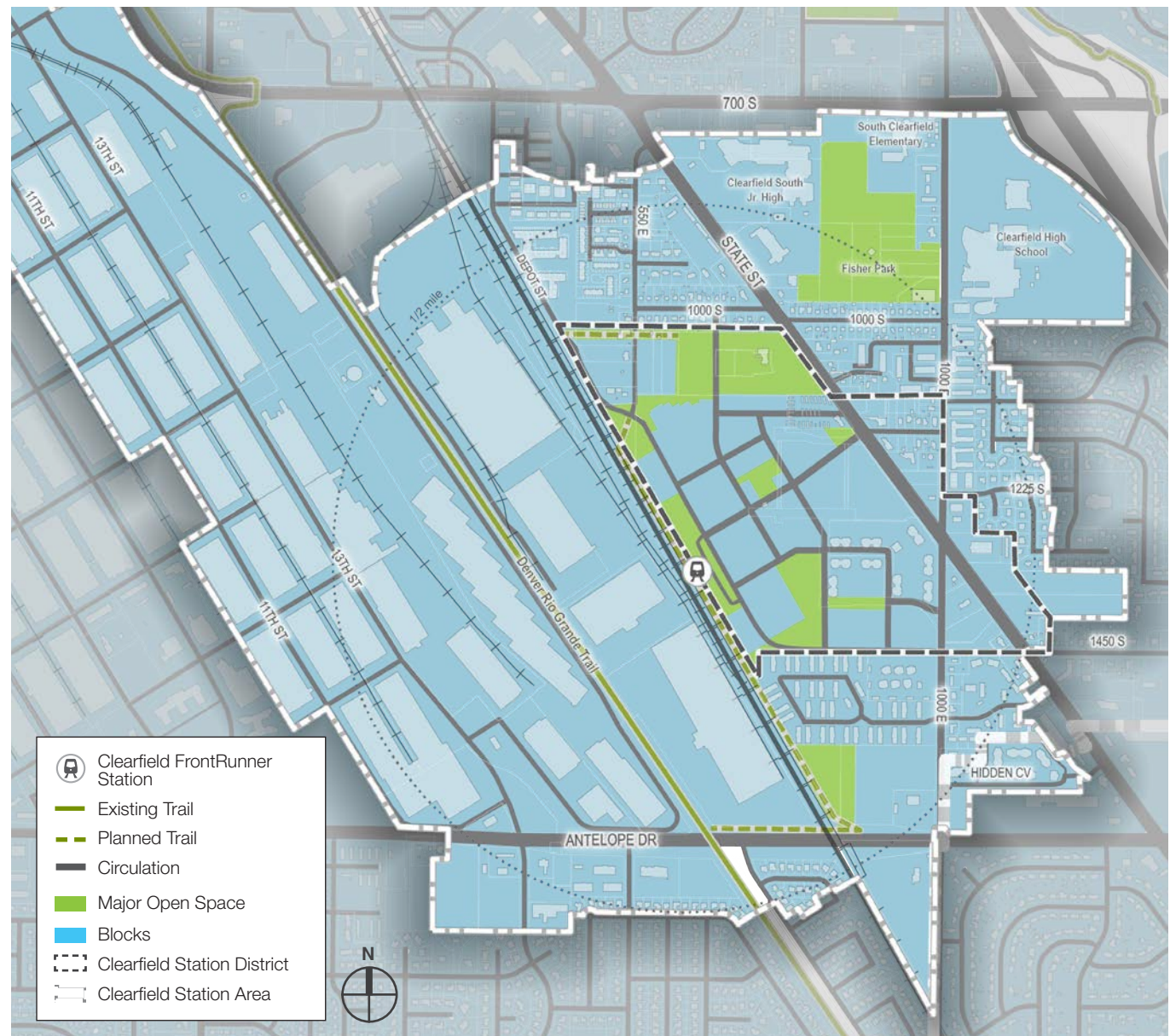
The Layton Town Center lies just beyond the half-mile zone of influence of the station, with Holy Cross Hospital -Davis and well-established residential neighborhoods just beyond. A strong connection between the Station Area and the town center will increase transportation, office, retail, commercial and residential opportunities.



Framework Plan: Streets, Blocks & Open Spaces

The Framework Plan for the Clearfield Station Area shows the defining features of the planning area, including the circulation system, block patterns, and open spaces. The physical arrangement of the streets and blocks establishes the form of the area, the framework for the Station Area, and its surroundings.

The Framework Plan highlights the key elements of the Station Area and how they are aligned and coordinated. Merging a connected street network with appropriately sized blocks and an integrated open space system is critical for ensuring the vision for the Station Area is realized.



Future Land-Use

The Clearfield Station Area is a diverse neighborhood that contains a variety of land-uses within the Station Area and its zone of influence. When complete, the area will merge existing neighborhoods and uses with new ones, resulting in a complex mix of complementary uses. These can be developed as horizontal mixed use projects (a variety of single use buildings) or vertical mixed use projects (multiple uses within individual buildings).













The accompanying land-use diagram details and refines the envisioned land uses for the area. The Clearfield Station District is where the bulk of new development and change is envisioned and is the focus of many of the subsequent sections of this document.

Future land-uses are arranged with the highest intensity uses concentrated near the center of the MDP site adjacent to the platform and are assumed to generate high transit ridership.

Table 1 indicates the anticipated areas and percentages of land allocated to each use.



TABLE 1: FUTURE LAND USE ACREAGES

	Station District		Station Area Outside Station District		Total Station Area	
Name	Acres	Percent	Acres	Percent	Acres	Percent
 Single-Family Residential	0.0	0%	42.7	6%	42.7	5%
 Medium-Density Residential	21.9	24%	90.2	12%	112.1	13%
 Low-Density Mixed-Use	4.3	5%	0.0	0%	4.3	1%
 Medium-Density Mixed-Use	11.6	15%	0.0	0%	11.6	1%
 Office	5.5	7%	0.0	0%	5.5	1%
 Retail	5.6	7%	0.0	0%	5.6	1%
 Highway Commercial	0.0	0%	29.0	4%	29.0	3%
 Industrial/Manufacturing	0.0	0%	533.7	70%	533.7	63%
 Government/Institutional	0.0	0%	48.7	6%	48.7	6%
 Cemetery	6.5	8%	0.0	0%	6.5	1%
 Park/Open Space	17.1	22%	21.1	3%	38.2	5%
 Transit Infrastructure	9.6	12%	0.0	0%	9.6	1%
Total	95.3	100%	752.8	100%	848.1	100%

SINGLE-FAMILY RESIDENTIAL

Existing single-family residential neighborhoods should be maintained and incorporated into the structure of the Clearfield Station Area. A limited amount of new single-family residences may be warranted to help improve transitions with other uses in the area.



Example of Single-Family Residential

MEDIUM-DENSITY RESIDENTIAL

These areas include a mix of townhome, duplex, and/or multi-plex units that provide “Missing Middle” housing opportunities within convenient walking distance to the commuter rail station. Heights should generally be limited to three stories.



Example of Medium-Density Residential

LOW-DENSITY MIXED-USE

These areas provide a mix of lower-density housing options including multi-plexes and small apartment buildings, from two to four stories in height. Ground floor uses are envisioned to include a mix of residential, office, and retail uses.



Example of Low-Density Mixed Use

MEDIUM-DENSITY MIXED USE

Primarily concentrated around the intersection of Station Boulevard and State Street, these areas provide medium-density mixed-use buildings between two and eight stories in height. Ground floor uses are envisioned to include a mix of retail, office, entertainment, restaurant, general commercial and residential amenity spaces. The highly visible location will provide retail services for both the Clearfield Station Area and traffic on State Street. Housing and/or office uses are encouraged over the retail ground floor.



Example of Medium-Density Mixed Use

OFFICE

The office zone accommodates office buildings in the heart of the neighborhood, directly adjacent to the commuter rail platform. The central location of this use will help establish the identity of the neighborhood as not just a residential community, but a complete community centered around an employment hub. The central location of this zone requires some active ground floor commercial uses in prominent areas.



Example of Office

RETAIL

The retail zone provides a retail element near the station. This highly visible location will provide retail services for both the Clearfield Station Area as well as vehicular traffic from State Street. Housing and/or office uses are also possible, with retail limited to the ground floor.



Example of Retail

HIGHWAY COMMERCIAL

These areas provide highway-oriented retail opportunities along Antelope Drive and the intersection with 1000 East. These are high visibility locations that will provide retail services for motorists operating in the vicinity of the area, including vehicular traffic from State Street. Carefully-incorporated residential and office uses are encouraged on the upper floors.



Example of Highway Commercial

INDUSTRIAL/ MANUFACTURING

Freeport Center uses are anticipated to grow and evolve over time, bringing greater numbers of employees to the area. To help ensure the center takes advantage of the transit, retail, office, and entertainment opportunities with the Station Area, vehicular, microtransit, pedestrian, and cycling linkages should be considered as part of any future redevelopment in the Freeport Center.



Example of Industrial/Manufacturing

CIVIC/EDUCATIONAL

The area is well served by three K-12 public schools and a public park north of the Station Area. These facilities should be preserved and enhanced to meet the needs of the Station Area and the Clearfield community as a whole.



Example of Civic/Educational

PARK / OPEN SPACE

A range of new parks, plazas, greenways, and streetscapes are proposed to establish the Station Area as a robust and engaging city center. These uses should be mixed with retail shops and other public amenities to help facilitate the creation of a gateway experience into this new and dynamic district. These efforts should be combined with upgrading efforts for Fisher Park and other existing parks on the periphery of the planning area, to help ensure a high level of park and open space opportunities are available to serve the expanded population in the area. The public space zone contains the neighborhood's significant public open spaces, including recreational and functional open spaces. The plan shows the existing drainage basin, as well as a central location for a village square.



Example of Park/Open Space

CEMETERY

The existing cemetery will be retained, with pedestrian access integrated into the park and open space network.



Example of Cemetery

TRANSIT INFRASTRUCTURE

The transit infrastructure within the Station Area provides transit users with central, comfortable, safe, and convenient infrastructure that accommodates all modes of transit. A transit plaza will provide civic space, as well as amenities that enhance the overall transit user experience. This includes small buildings and kiosks for food and beverage, bike rentals and micromobility, ticket stations, and other amenities geared toward transit riders. Transit uses are served by parking locations within 1,000 feet of the commuter rail platform to ensure an appropriate amount of parking is available for park-and-ride transit users. Parking in this area can also act as shared parking for employees and visitors in the neighborhood.



Example of a Transit Plaza

Station District Illustrative Master Plan

This section focuses on the **Station District**, as it contains the majority of proposed redevelopment (see Future Land Use on page 37).

The Illustrative Master Plan presents an example layout of how the Clearfield Station District could develop to meet the vision and principles established for the project. As previously described, this district area represents the most development-ready zone within the Station Area. The other districts are well-established and expected to generally remain within their current form.

The building sizes, shapes, and uses shown here are flexible and are intended to demonstrate the vision for the development. The layout and arrangement of the buildings is also flexible.



ILLUSTRATIVE RENDERINGS

Concept renderings demonstrate the general character and feeling of the Clearfield Station Area. They are meant to illustrate the general vision, not specific design solutions.

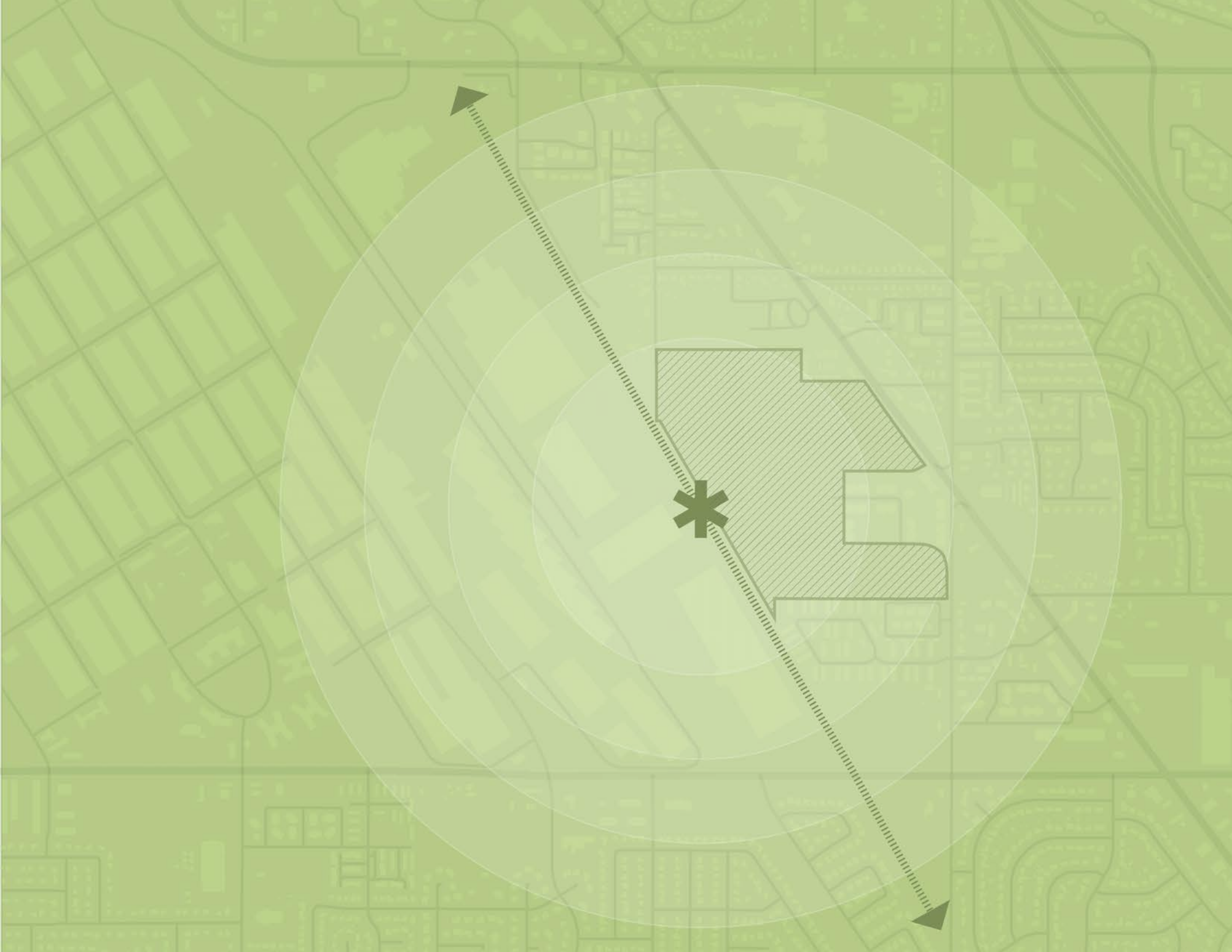
The images on this page provide two views along Station Boulevard looking toward the FrontRunner Station – one viewed from above the street, and the other from a street level perspective.

ILLUSTRATIVE RENDERING: STATION BOULEVARD LOOKING WEST



ILLUSTRATIVE RENDERING: STATION BOULEVARD LOOKING WEST







04

***MARKET
STUDY +
ECONOMICS***

Market Study + Economics

INTRODUCTION

With the proposed development within the Clearfield Station Area Plan, there is tremendous potential for Clearfield City (City) to generate increased revenues. This analysis calculates the possible revenue generation for the land use types, factoring in the City’s main General Fund revenue sources: Property Tax, Sales Tax, Municipal Energy Tax, and Class B/C Road Funds.

ASSESSED VALUES IN DAVIS COUNTY

For the most accurate revenue projections, average assessed values were calculated for different development types that are found within the Station Area plan. These are based on similar properties throughout Davis County, according to 2023 values provided by the County.

TABLE 2: AVERAGE DAVIS COUNTY ASSESSED VALUES

Land Use Category	Average Assessed Value
Office	\$186.84/SF
Mixed-Use	\$198.67/SF
Retail	\$135.82/SF
Multi-Family	\$237.72/SF
Single Family	\$186.87/SF

Source: Davis County Assessor's Office



POTENTIAL REVENUE GENERATION

Table 3 demonstrates the approximate acreage and total revenue generation of each major land use type within the Station District. This analysis does not include the entire Station Area, focusing in on just the Station District where the majority of land use change is proposed (see Districts Map on page 33 and Future Land Use Map on page 37).

The mixed-use development is planned to contain both residential and commercial uses, in a primarily vertically stacked configuration. There are additional uses proposed within the Station District, but they are primarily non-revenue generating properties such as open spaces, parking garages/ areas, and transit zones.

These calculations represent additional revenues the City may collect as the project is developed. The City will continue to receive revenue from other areas within the Station Area boundaries, however they are not reflected in these calculations.

Total revenues shown demonstrate an aggregated total of major General Fund revenue sources for the City: Property Tax, Sales Tax, Municipal Energy Tax, and Class B/C Road Funds. The total revenue generated by the development within the area depends on the final mix of development subtypes.

TABLE 3: STATION DISTRICT DEVELOPMENT REVENUE GENERATION

Land Use Category	Acres	Development Subtype	Total Revenue (Mixed-Use with Retail)
Medium-Density Residential	21.9	Townhomes	\$184,554
		Apartments	\$516,739
Low-Density Mixed-Use	4.3	Retail	\$217,691
		Office	\$147,450
Medium-Density Mixed-Use	11.6	Retail	\$826,616
		Office	\$447,644
Office	5.5	n/a	\$190,135
Retail	5.6	n/a	\$191,653

Source: ZPFI

For example, if the Medium-Density Residential develops as townhomes, the Low-Density Mixed-Use develops with a retail focus, and the Medium-Density Mixed-Use develops with an office focus, the total revenues would be estimated at \$1,231,676. As Table 3 demonstrates, the total revenue collected varies depending on what use is found within the mixed-use area.

The development type with the greatest revenue generating potential

is mixed-use with a focus on retail as the commercial development. This is in large part thanks to the impact of sales tax. The increased taxable sales projected for these businesses results in more potential revenue generation. It is interesting to note that multi-family residential development has the second greatest revenue. With the rise in online shopping, homes have become miniature retail stores, with cities able to collect point of sale revenue from these sales. With more dense residential

developments, this increases the revenue collection.

Funding Sources

The City has a number of avenues at its disposal to help incentivize development in this area, or to help offset development costs. The following table summarizes a number of these different funding opportunities.

TABLE 4: AVAILABLE FUNDING MECHANISMS

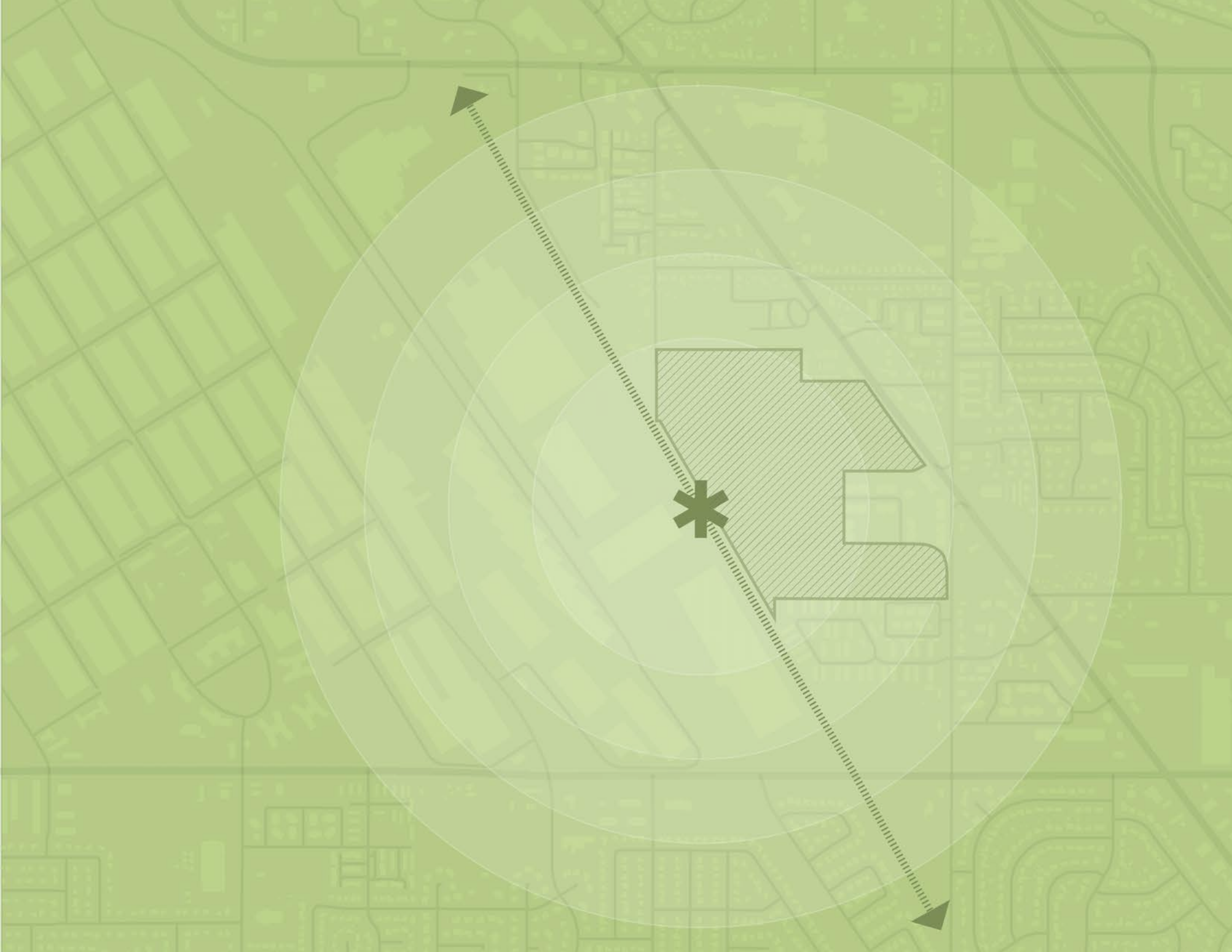
Funding Source	Advantages	Disadvantages
Tax Increment Financing (Community Reinvestment Area – CRA)	Taxes generated in an area are spent in same project area; Potential participation by other taxing entities; Can include specialized TIF areas such as HTRZs and TRZs	Must get approval of other taxing entities – subject to political will
Bonding (General Obligation GO, Sales Tax)	GO bonds have the lowest rates; Sales tax bonds do not require public approval/ vote; Funds are available immediately	GO bonds require public vote
Utility Bond	Immediate funding; No public vote required	Rates may need to be raised to cover utility costs; Used only for utilities
Impact Fees	New development pays its own way – proportionate share of capital costs; Could create separate service area for separate impact fees if extraordinary costs apply; Could be a long-term repayment source for other funding mechanisms	Receipt of impact fees takes place over many years and is not guaranteed; Not every project is impact fee eligible
Public Infrastructure District	Off the City's books; Those who benefit pay; Cost is much lower than other development financing; Used instead of impact fees and is a steady stream of revenue	Willingness of all property owners to establish a PID; Ongoing PID governance; Competitiveness of site with additional taxes
Special Assessment Area	Those who benefit pay; Could be used in conjunction with tax increment, thereby encouraging development and use of increment to pay assessments	Willingness of property owners to establish a SAA – requires 60 percent or more to agree (based on assessment method); Need to come up with equitable assessment method
Public-Private Partnerships	New revenue stream that pays for infrastructure	Relatively untried; Would lose control of rates to private investor
Grants	Additional money that does not come from the City; Ability to enhance funds already committed to projects	Funds are subject to availability from the granting institution; often times requires matches or other restrictions

Economic Incentives

HOW COULD POTENTIAL USES BECOME MORE FEASIBLE AT CLEARFIELD STATION?

- **Opportunity Zone** – This area falls in a designated Opportunity Zone. This is a major investment incentive that creates a superior advantage to most other Frontrunner Stations.
 - Significantly increases investment appeal and makes office and retail more financially feasible (investors will accept lower capitalization rates (creating higher values) due to the tax advantages).
- **Funding Incentives** – The area is part of an existing CDA. Available funding incentives should be readily marketed to attract uses the city desires.
 - Additionally, the city and UTA should consider the formation of a Transportation Reinvestment Zone (TRZ), a newly adopted economic development tool that focuses on tax increment financing for transportation specific improvements. This funding option, while very similar to an RDA/ CRA, does not require a ten percent allotment to affordable housing. It also allows for the land owner and city to have greater control regarding what can be built.
- **Increase Daytime Population** – an increase in daytime population will benefit retailers. This can be accomplished by the following:
 - Entertainment draw/attraction
 - Strong office population
 - Strong residential population (to capture remote workers)







05

BUILDINGS + ARCHITECTURE

DESIGN GUIDELINES

Buildings + Architecture

OVERVIEW

The layout and arrangement of buildings and parking typically have the most significant impact in creating a walkable destination. The arrangement of buildings and parking reinforces the quality and functionality of the building facades, streets, and open spaces and how all of these elements work together to create a more livable environment.

The following guidelines are meant to apply to the Station District (see District Map on page 33) and other mixed-use development areas within the Station Zone of Influence.

INTENT

To establish strong urban design guidelines for the Station District and other mixed-use development areas within the station Zone of Influence that will serve as the foundation to thoughtfully choreographing buildings, open space and streets.

PRIMARY FACADES

Primary facades establish a consistent streetwall with active ground floor uses. As illustrated on the map on the following page, they often line primary streets – the most important and walkable streets in the neighborhood. Primary facades should address the street with windows/transparency, high quality building materials, and a main building entrance.

Retail, residential, and/or other active uses are encouraged where a building faces a primary street.

SECONDARY FACADES

Secondary facades should be used when a building fronts multiple streets. The secondary facades should include windows/transparency and high quality building materials. However, such treatments are not as essential as they are on primary streets. Retail, residential and/or other active uses are encouraged. Blank walls should be limited.

PARKING

Parking areas should be located in the rear and to the sides of buildings, and should not face the Primary streets.

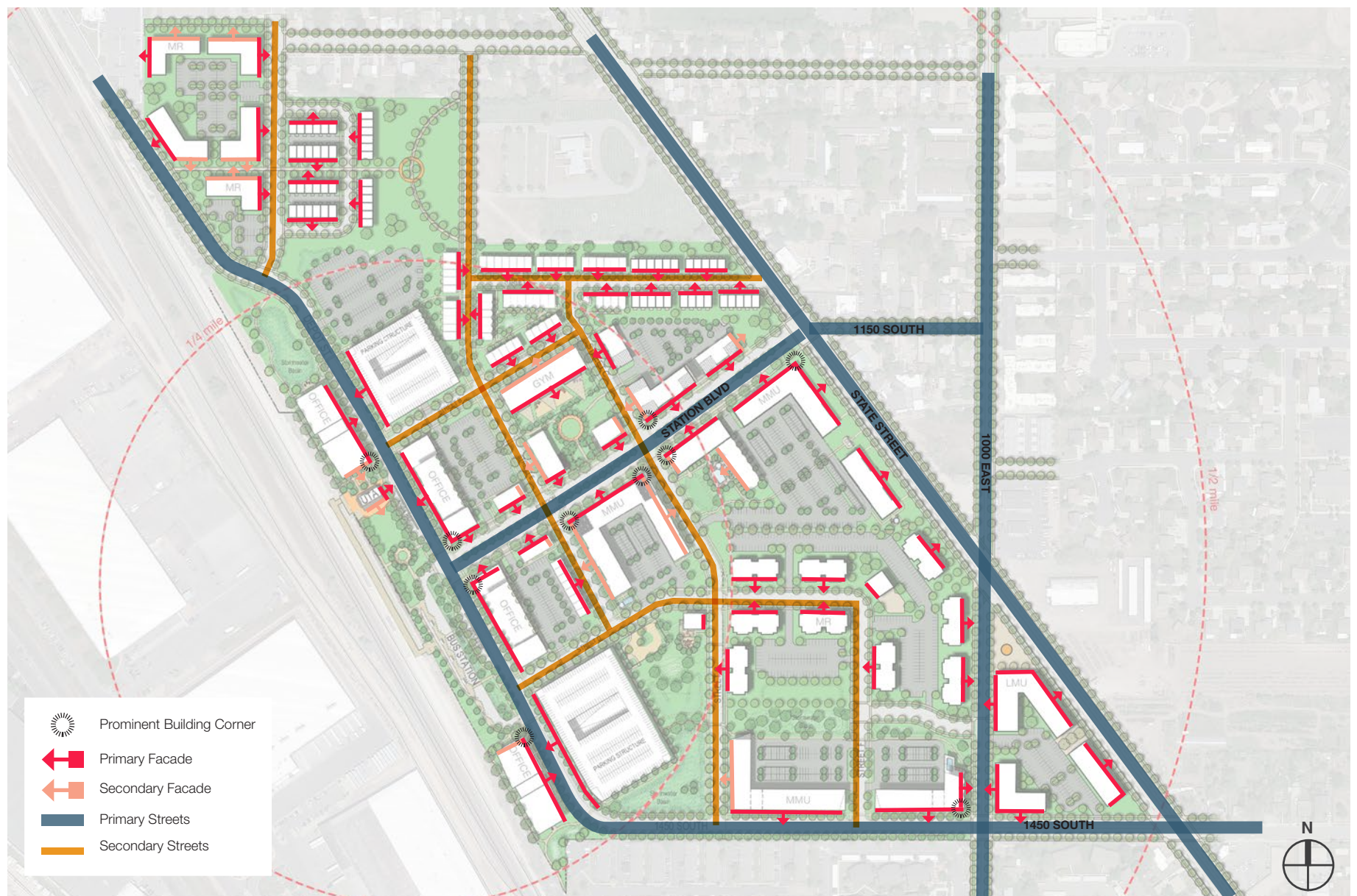
Buildings should wrap and screen parking areas from the street where possible and/or applicable.

OPEN SPACE

Open spaces should be located throughout the Station Area in prominent locations and include various sizes and user experiences.

Open space design and programming should respond to the surrounding uses and buildings.

See *Chapter 6: Open Space + Public Realm* for details.



Architectural Style

INTENT

To establish a specific “look and feel” throughout the study area to unify the area and create a design theme that is appropriate for the Clearfield Station Area.

DESIGN THEME - “CONTEMPORARY INDUSTRIAL”

The design theme for the Station Area is contemporary industrial style that is modern, yet is rooted in the industrial character of its surroundings. This industrial character helps to create a brand for the area and provides a common theme that ties the neighborhood together.

There are no historic buildings on or directly adjacent to the MDP site. Therefore, this presents an opportunity to create a new and unique, industrial inspired architectural style.

The design guidelines section will provide detailed design guidelines that should be followed to achieve a consistent and coherent architectural style as outlined above.

HISTORICAL PRECEDENTS

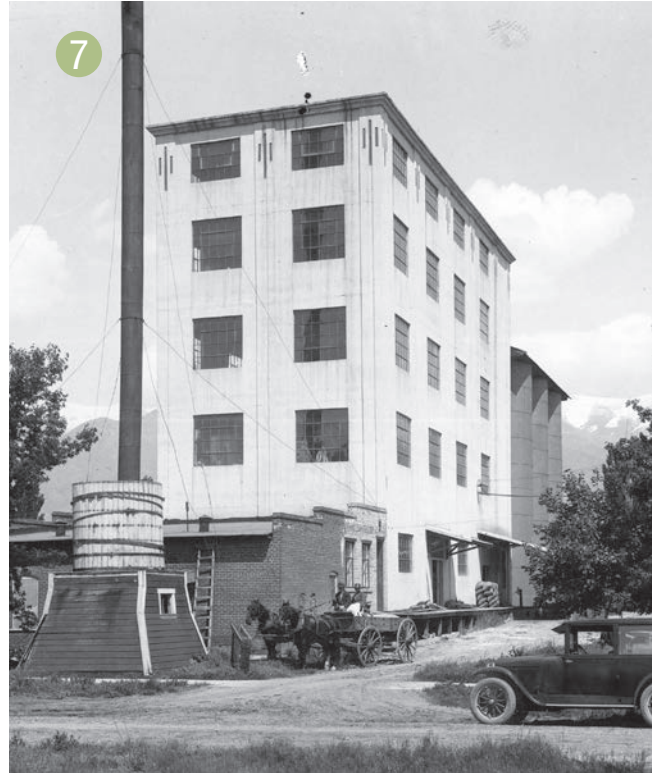
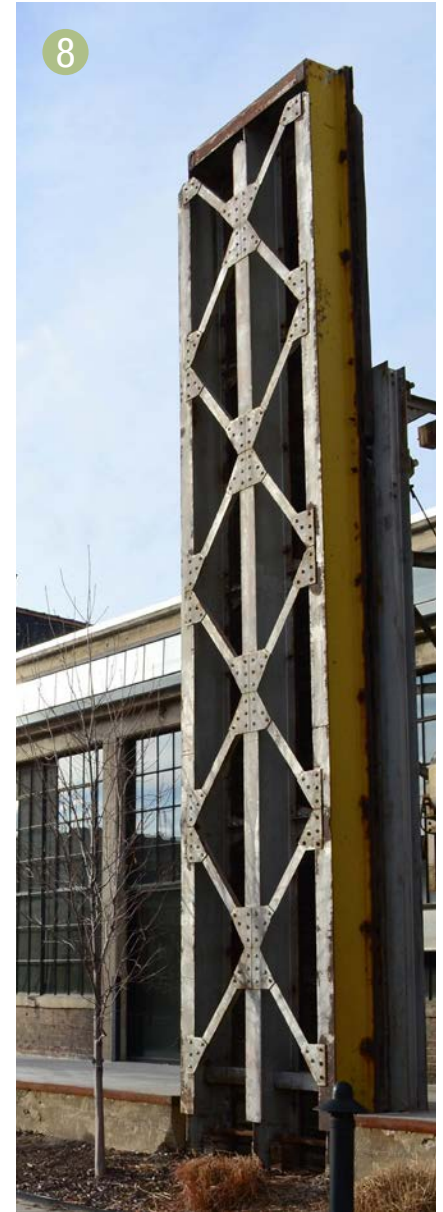
There are no historic buildings currently existing in the area, and therefore, historic precedents should be considered from around Northern Utah. Precedents should be based on traditional industrial architecture from the early to mid 20th Century that are/ were found in Northern Utah.

The images to the right display buildings found in Clearfield, as well as nearby cities such as Ogden, Layton, and Kaysville. These are just a few examples of existing and former buildings from the area that should provide inspiration for architects and designers.

PRECEDENT IMAGES

- 1 Administration building at the Clearfield Naval Supply Depot (now Freeport Center)
- 2 Layton Sugar Company
- 3 American Can Company (Ogden)
- 4 DaVinci Academy (Ogden)
- 5 Pillsbury Company (Ogden)
- 6 Warehouse (Ogden)
- 7 Kaysville Flour Mill
- 8 American Can Company (Ogden)





Architectural Style

CONTEMPORARY PRECEDENTS

The buildings on the following pages demonstrate images found throughout the country that achieve the goal of creating a contemporary, modern building that is also rooted in historic industrial architecture. They reflect the character and level of detailing envisioned for the Clearfield Station Area.

The images illustrate a range of precedents, from more abstract interpretations, to more traditional recreations. These images should be used for reference and inspiration for new development on the Clearfield Station Area.

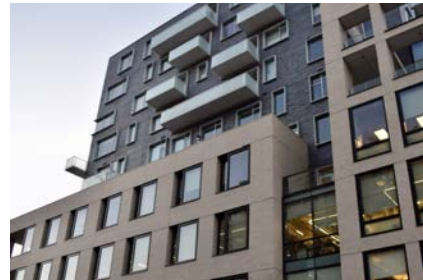
Elements often associated with industrial architecture include, but are not limited to:

- Large volumes that house large-scale industrial activities such as a mill, factory, foundry, refinery or power plant.
- Predominantly brick and steel buildings.
- Specialized building elements and apparatus such as tall chimney

stacks, exposed materials circulation apparatus, hoists and chutes.

- Exposed structural elements.
- High interior spaces with exposed brick, steel and timber.
- Divided light windows.





Materials + Colors

INTENT

To ensure a consistent application of complementary and high quality materials throughout the neighborhood that will reinforce the unique identity and a sense of place.

DESIGN GUIDELINES

- Building materials should reinforce the industrial theme by using brick, steel, timber, and concrete.
- Building materials should be durable, high quality, and authentic materials that have a long life, age well, and reflect a high level of craftsmanship.
- Building materials should add texture, depth, and visual interest to the building's facade.
- Materials should turn corners and incorporate thoughtful transitions between facades, spaces, uses, and structures.
- Materials should generally be limited to one or two predominant materials and one or two accent materials in order to keep buildings visually coherent and uncluttered.
- EIFS stucco and corrugated steel should be limited to no greater than 30% of the building's facade.

COLOR

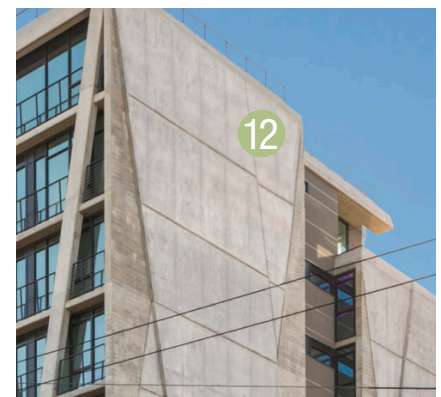
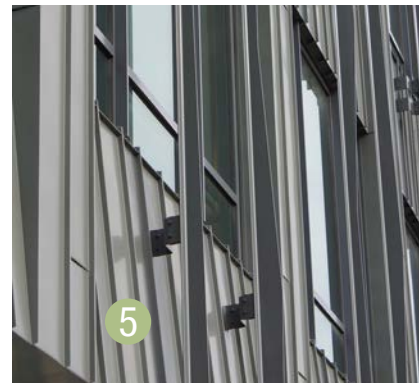
Industrial buildings typically are defined by dark, heavy colors, such as red brick, black steel and dark concrete.

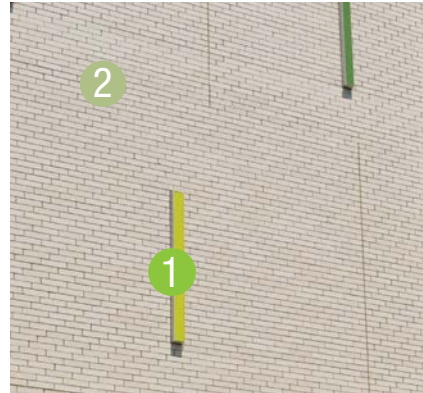
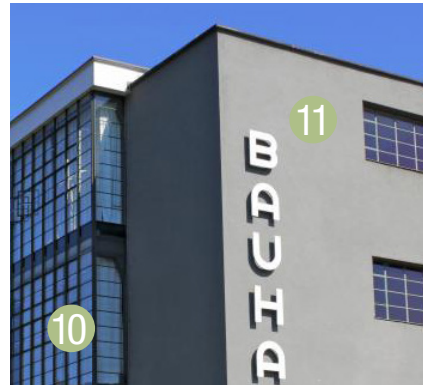
While those colors and materials are appropriate, lighter colors are highly encouraged in order to give the district a more fresh, contemporary look. Pops of color are also encouraged to accent and bring a feeling of excitement and uniqueness to the neighborhood.

1 Pop of Color as an Accent

ACCEPTABLE MATERIALS

- 2 Brick
- 3 Tumbled Brick
- 4 Black Steel
- 5 Colored Pre-Finished Metal Panels
- 6 Corrugated or Corten Steel
- 7 Stone
- 8 Wood / Timber
- 9 Curtain Walls Glazing System
- 10 Industrial Sash / Divided Light Windows
- 11 EIFS Stucco
- 12 Concrete





Architectural Massing

INTENT

To facilitate building shapes that fit comfortably within their surroundings, are friendly and unobtrusive to pedestrians, achieve an attractive urban form, and are visually interesting.

DESIGN GUIDELINES

- The most dense uses and tallest building heights should be located in Medium-Density Mixed-Use areas (see Future Land Use Map on page 35).
- Buildings should be designed to a human scale, with particular attention on the ground floor
- Floorplates should generally be less than 30,000 sf per building, with no minimum floor plate size.
- Buildings should create a consistent streetwall on both sides of the street to create “enclosure.”
- Gaps in the streetwall should be limited as much as possible.

PRECEDENTS

- 1 Building has clearly defined top, middle, and base.
- 2 Multiple buildings combine to create a good, pedestrian-scaled streetwall. The buildings also demonstrate a clearly defined top, middle, and base.





ARCHITECTURAL MASSING

Architectural massing is key in creating an inviting pedestrian environment. Care should be taken to understand the form of buildings and their impact on the public realm.

This graphic demonstrates how careful architectural massing creates an interesting and pedestrian friendly urban environment.

- 1 A consistent streetwall on both sides of street, as well as vertical elements such as trees, create a sense of enclosure.
- 2 A variety in building height, scale and bulk creates a dynamic and visually interesting experience.
- 3 Buildings include setbacks on upper stories in the building facade to ensure pedestrian scale and increase sunlight and air on the street.
- 4 The ground floor of buildings addresses the street and has a high level of transparency.
- 5 Windows, podium decks and balconies overlook the street.

Facade Articulation

INTENT

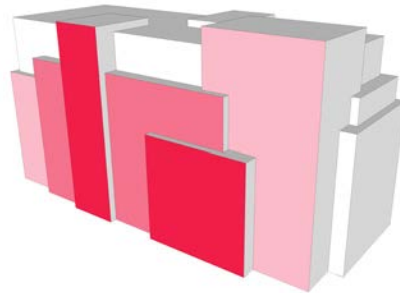
To purposefully articulate building facades in order to make the various building functions legible through the massing of the buildings, as well as to reduce the building's apparent mass.

HORIZONTAL ARTICULATION

The first 20 feet of height of building faces should have a rhythm of modules that serve to break down the scale of the building face. A module is defined as a portion of the facade that is differentiated from the adjacent facade by a change in the line of the face of the building, and/or a substantial change in material color or fenestration. Characteristics between modules should relate to one another to achieve a unified composition.

DESIGN GUIDELINES

- Modules should generally be no longer than 40 feet.
- Building facades should avoid being long, monotonous, and repetitive.
- Articulation should be used to create interest and help establish a strong sense of design and identity.
- Massing, building details, and entries should be proportionately scaled.



Vertical planes are articulated through massing and add interest to the building



VERTICAL ARTICULATION

The three segments of the building - the base, middle and top - should be articulated by such elements as cornices, string courses, stepbacks, recesses and projections, changes in floor height, and changes in color and material.

DESIGN GUIDELINES

Top Section

- Should define the roof line.
- Stepbacks are encouraged for penthouse units or to otherwise break up the mass and define the building top.
- Incorporate green roofs and other usable roof space where possible.

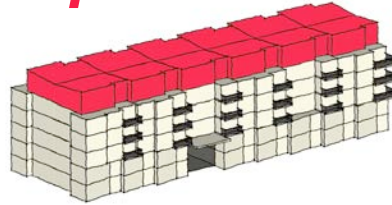
Middle Section

- Should define the principle building facade.
- Should differentiate from the base and top sections through the use of massing, materials, and/or color.

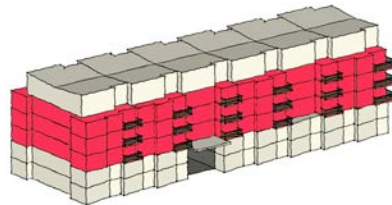
Base Section

- Should relate directly with the street.
- Should “ground” the building.

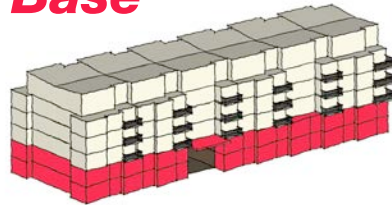
Top



Middle



Base



Setbacks

INTENT

To ensure all buildings consider their relationship with the public right-of-way with the appropriate setback distance for each unique use, and to create a human-scaled, defined streetwall.

DEFINITION

The setback refers to the space between the building facade and the public right-of-way line.

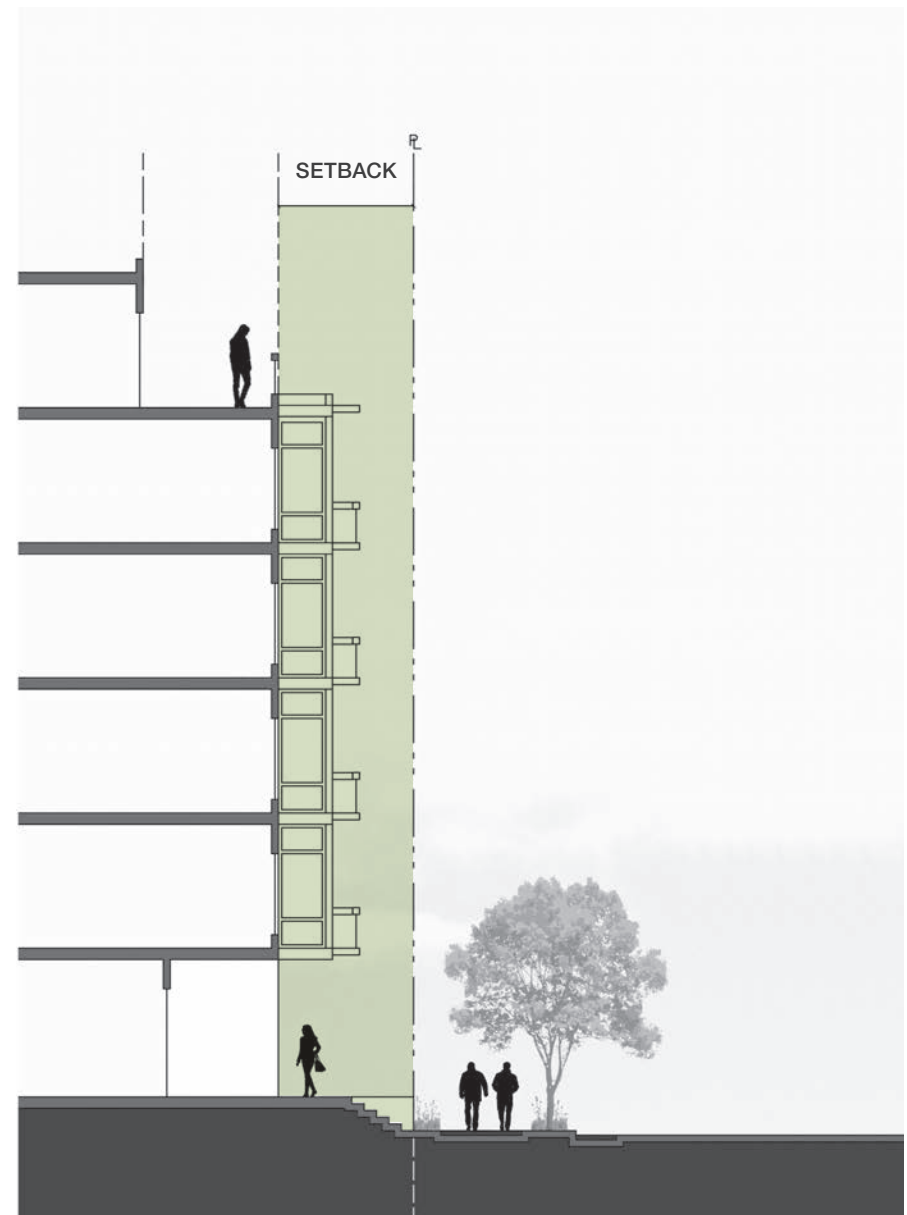
DESIGN GUIDELINES

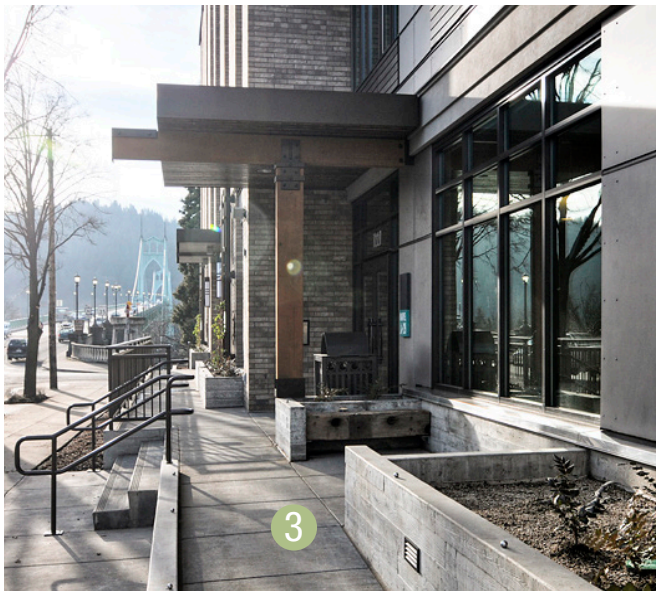
- Maximum setback distance is 15 feet unless a building fronts a plaza or open space.
- There is no minimum setback distance.
- Generally, setbacks should be no more than 5 feet.
- Setbacks, when used, should enhance the ground level environment and pedestrian experience. Examples include:
 - To create a space for outdoor dining in front of retail/restaurant spaces.
 - To provide landscape and/or a patio/stoop in front of ground level residential entrances.

- To enhance the architectural character of the building facade at street level.
- Entrance courts for office or residential building lobbies.
- To add interest and bring nature into the streetscape through planters and landscape. In-ground planters are only allowed in front of ground-floor residential units.
- Setback may be raised above sidewalk level to create feeling of semi-private space.
- See pages 68 - 61 for ground floor - base activation design guidelines.

PRECEDENTS

- 1 Setback is used for outdoor dining.
- 2 Setback along ground floor residential units contains stoops and landscape.
- 3 Setback is raised to create sense of semi-private space.
- 4 A strongly defined streetwall is created, despite having some setbacks in the building face and at the ground floor.





Projections

INTENT

To encourage facade articulation through habitable and non-habitable projections.

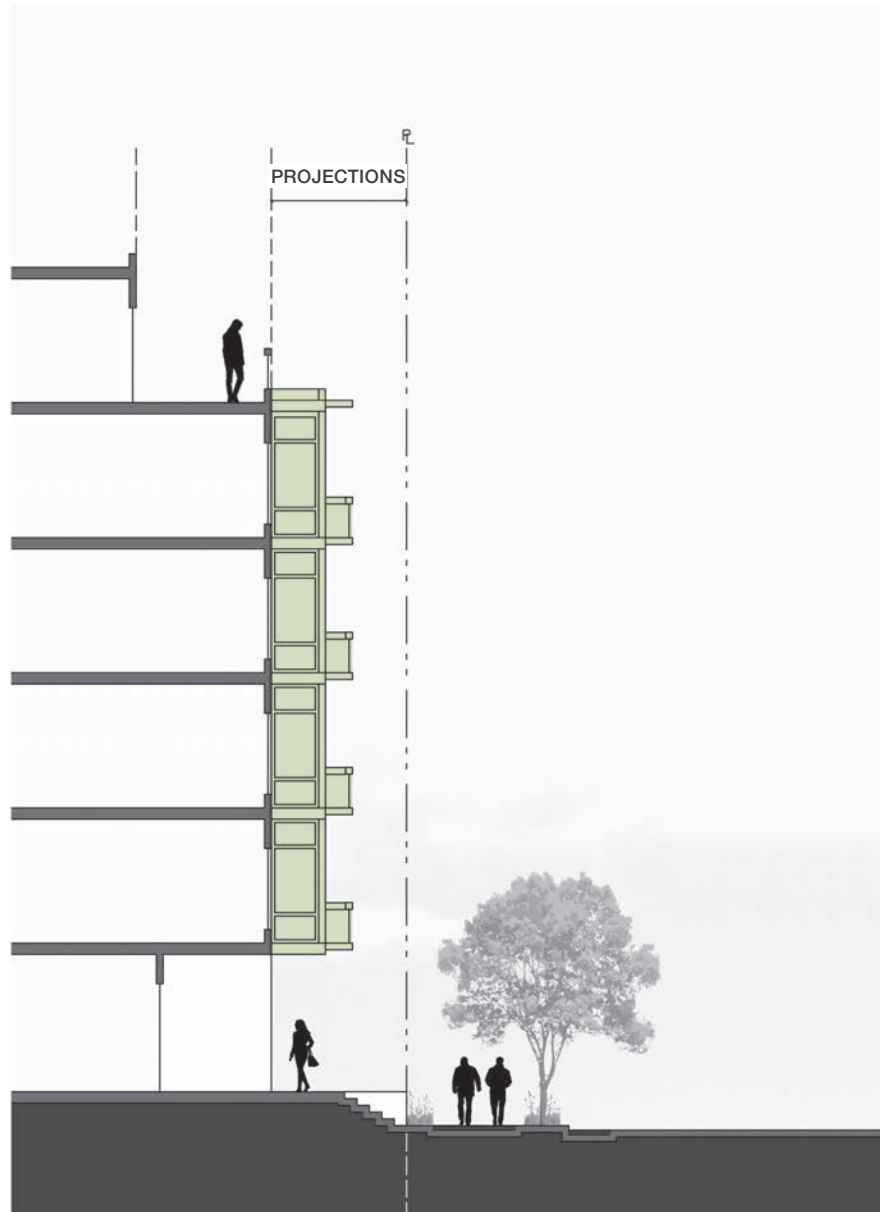
DEFINITION

Habitable projection - a portion of the building enclosed by walls and a roof, such as a bay window, corner element, or other extended bay.

Non-Habitable projection - spaces utilized by residents but not enclosed by walls and a roof, such as balconies.

DESIGN GUIDELINES

- Projections are encouraged to add visual interest to the facade, as well as to add usable balconies as residential amenities.
- Balconies should be at least 3 feet deep.
- Projections should not extend more than 6 feet into setback or common space.
- Projections should not extend more than 3 feet into public right-of-way.
- Decorative elements such as belt courses, cornices, sills and eaves are also encouraged.



Stepback

INTENT

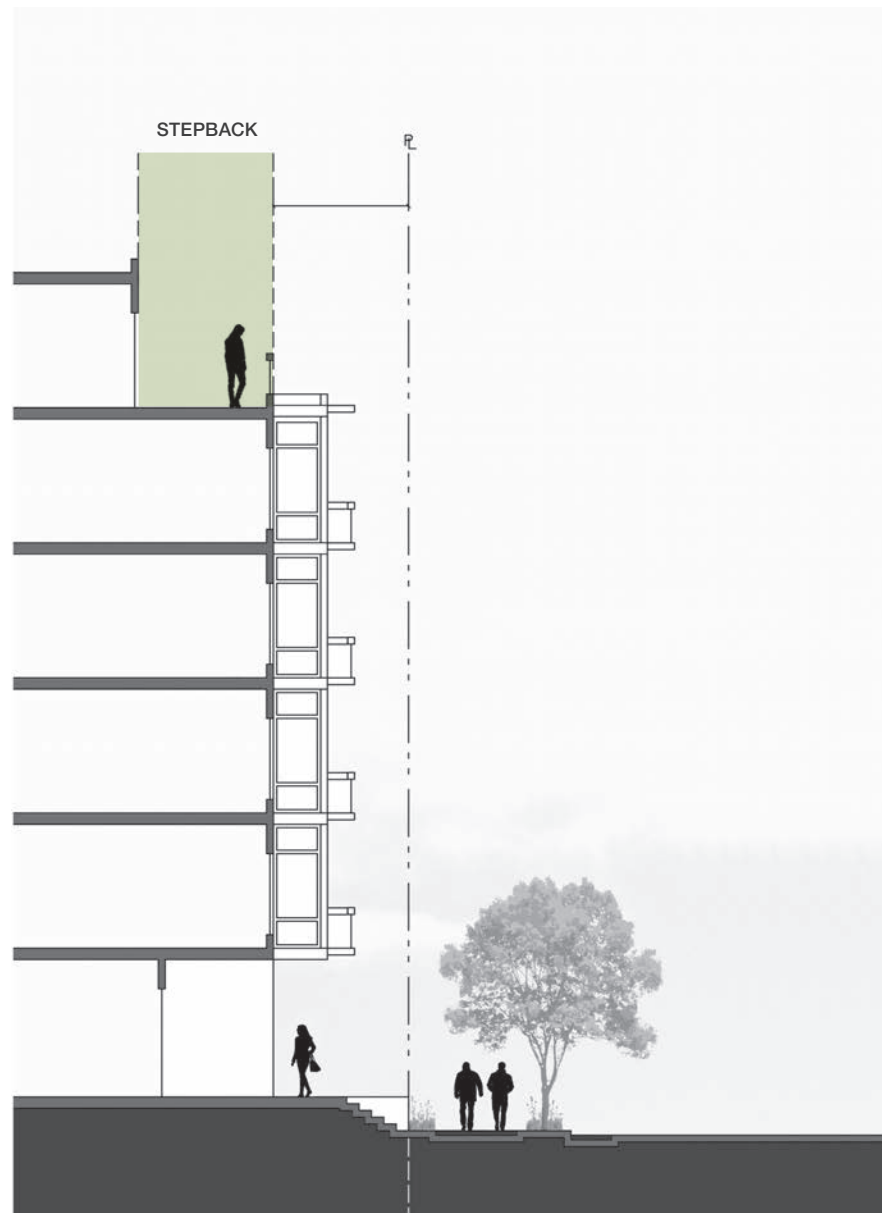
To encourage facade articulation and the creation of usable outdoor space by offsetting the upper floor(s) from the lower floor(s) of a building.

DEFINITION

Stepback is the portion of the building on upper levels that is stepped back from the building facade.

DESIGN GUIDELINES

- Stepbacks are encouraged to help break down the mass of the building by creating a defined “top,” as well as to add usable space for residential amenities.
- Roof space created by stepbacks should be designed as usable outdoor space.



Ground Floor - Base Activation

INTENT

To ensure the important interaction between the ground floor of a building and the sidewalk is carefully designed to enhance the pedestrian experience and the overall vitality of the neighborhood.

OVERVIEW

One of the most important aspects of a walkable urban neighborhood is the street level interaction between the building and the street. For a streetscape to facilitate active public life, it is essential that buildings address the street on the ground floor.

This page contains general ground floor design guidelines, while the following pages contain specific guidelines for residential and commercial uses.

DESIGN GUIDELINES

- The base of the building should be designed to foster positive activity by orienting and integrating courts, lobbies, entries, and large windows to face streets, public parks, and open spaces to provide more opportunity for interaction and safety.

- Avoid or minimize expansive blank walls at the ground floor.
- Include operable windows, roll up doors, and other features to activate and animate a building.
- Maximize transparency of ground floor commercial facades with windows and doors with visibility into active uses, such as retail spaces, lobbies, etc.
- Highlight entrances to commercial buildings through integrated signage, changes in materials and colors, and/or through changes to the buildings massing.
- Ground Floor heights should be at least 14 feet tall.
- Active uses should have a depth of at least 25 feet from the street frontage.

PRIMARY STREETS

The primary streets, as defined in the Street Hierarchy Section on page 97, are the most important streets where active ground floor uses should address the street. “Primary Street A” (the boulevard) is designed to be the primary retail and walking street in the neighborhood.

“Primary Street B” should also have active uses fronting the street. Retail is encouraged, if it is supported by the market. However, it is anticipated that this street will more likely be lined with active uses such as residential units, lobby spaces, meeting spaces, etc.

Active uses are encouraged on all other streets in the neighborhood to the extent feasible.

ACTIVE USES

Active uses are defined as any use that provides some level of interaction with the public realm. This could include uses such as residential, retail goods establishments, retail service establishments, public service portions of businesses, restaurants, taverns/ brewpubs, bar establishments, art galleries, theaters, performing art facilities and more. Uses must also be allowed by City Ordinance.

PARKING STRUCTURES

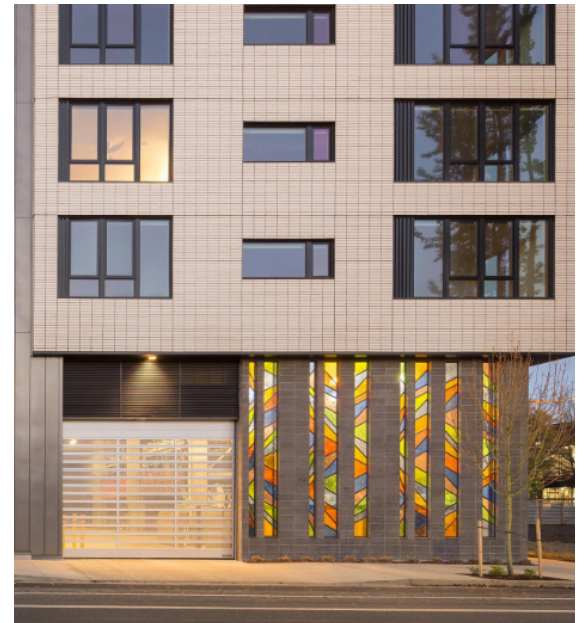
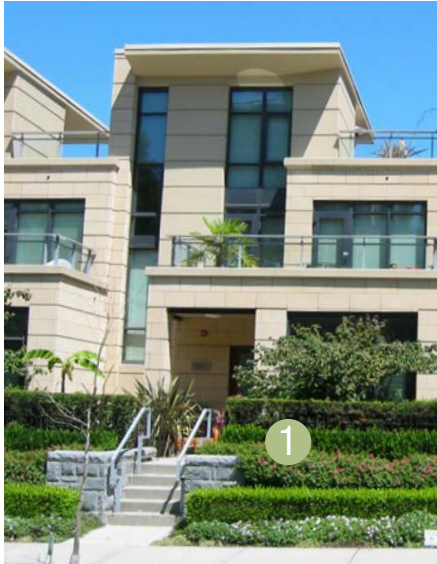
No parking structures are allowed to face “Primary Street A” and any parking structure facing “Primary Street B” should have an active ground floor use.

SCREENING METHODS FOR BLANK WALLS

Where blank walls occur, creative methods should be used to create interest on the streetscape. This could include solutions such as murals, green walls (plants growing on walls), faux windows, and more.

PRECEDENTS

- 1 Entrances at street level combined with high quality landscape buffer activates the street.
- 2 Storefront with high transparency on ground floor, along with outdoor dining, activates the street.
- 3 Roll up doors on ground level blend the indoor/outdoor space and activate the street.
- 4 Faux windows and landscape add visual interest to create feeling of activity on a facade without an active use.
- 5 Planters along blank street wall add interest to an otherwise blank wall.
- 6 Colorful glass adds interest and life to an otherwise blank wall.



Ground Floor Residential

INTENT

Residential buildings without retail or other active uses on the ground floor should activate the ground floor by putting residential units with individual entries that address the street on the ground floor.

GROUND FLOOR DESIGN ELEMENTS

1 LANDSCAPED SETBACK

Buildings with residential units on the ground floor should provide a setback, typically 10' or less, to provide space for entry steps/stoops and landscape in order to provide adequate space for the public/private transition. The landscape/plants should also be used to screen views from the street into residences (also see diagram on bottom right of this page).

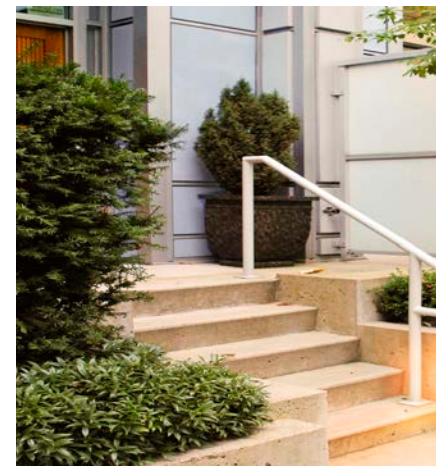
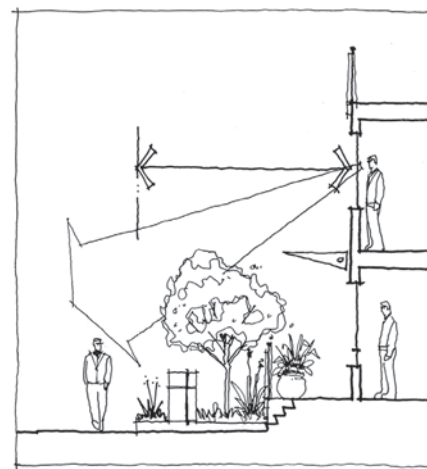
2 RESIDENTIAL ENTRY

Residential units on the ground level should generally be located at least three feet above grade, so that the unit's habitable space is above the eye level of pedestrians for increased privacy.



3 FACADE MODULATION

Buildings are vertically modulated at regular intervals of no greater than 30 feet to express individual ground floor residential units.



Ground Floor Commercial

INTENT

Commercial buildings should activate the ground floor through using retail or other active uses on the ground floor.

GROUND FLOOR DESIGN ELEMENTS

1 SETBACKS + LANDSCAPE

Commercial buildings should not have a consistent setback, but should have articulation zones as specified. Where setbacks do occur, landscaping is encouraged to soften the streetscape, add visual interest, and increase the opportunities for experiences with nature in an urban environment. Outdoor Dining or other functional uses that enhance the ground floor use are also encouraged

2 TRANSPARENCY

The ground floor of commercial buildings should be primarily composed of transparent materials in order to reveal activity of the building, as well as to add interest and security to the pedestrians.



3 FACADE MODULATION

Buildings are vertically modulated at intervals that align with the specific ground floor use, generally no greater than 80 feet. For retail uses, intervals should generally be no greater than 50 feet.



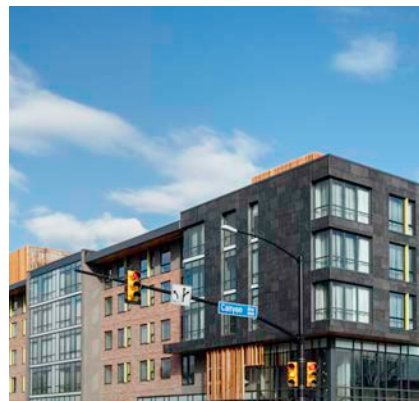
Roofs

INTENT

To emphasize the architectural style and to minimize visual impacts.

DESIGN GUIDELINES

- Roofs should be flat or appear flat from street level.
- Building heights and roof lines should modulate to create a visually appealing skyline and add interest to the skyline.
- Mechanical equipment on roofs should be screened from the street view.
- Green roofs are encouraged
- Usable roof terraces are encouraged
- Roofs should use high albedo, non-reflective materials to minimize heat island effect



Corners

INTENT

To emphasize important intersections and corners by including special architectural features on buildings in these key locations.

DESIGN GUIDELINES

- Incorporate special design details and architectural treatments that reinforce the corner's importance as a public realm element
- Corners in key locations should be emphasized by utilizing a combination of these measures:
 - A change in the building's massing and/or height
 - A contrasting facade finish
 - Transparency
- Designers/Architects are encouraged to find creative and artful solutions.



Entrances

INTENT

To emphasize the relationship between buildings and their adjacent streets by prominently featuring major entrances.

DESIGN GUIDELINES

- The main entrance to the building should provide the most important interaction between the pedestrian and building and should be emphasized through design.
- Buildings that front primary streets (as defined on page 97) should have a main entrance facing that street. A building may have an additional main entrance that faces the main parking area or drop-off zone, if applicable.
- Use lighting to highlight entrances.
- Provide canopies, awnings, or other overhead elements to protect users from weather conditions.
- The use of continuous “docks” within the build-to line is permitted to provide a semi-private space for outdoor dining or other uses that activate the streetscape. This mimics the re-purposing of loading docks that is often done on historic industrial buildings.



Fenestration

INTENT

To create a pedestrian friendly and engaging relationship between buildings and streets.

DESIGN GUIDELINES

- The ground floor of commercial buildings should have a high percentage of transparent materials where buildings front streets.
- Buildings maximize windows on upper floors that overlook streets or open spaces to increase “eyes on the street,” which discourages undesirable public behavior.
- Windows should be strategically used next to entrances and open spaces to create prominent indoor/outdoor relationships.
- Industrial windows are strongly encouraged to promote the industrial character.
- Mullions and frames are encouraged to project beyond the plane of the glass in windows to create strong shadow lines.



Building Signage

INTENT

To identify the commercial or non-commercial uses within the building with signage that promotes wayfinding, adds interest that fits with the architectural character of the building, and enhances the pedestrian experience.

DESIGN GUIDELINES

- All signs should be scaled appropriately to the size of the building.
- Signs shall be constructed of high quality and durable materials that are consistent with and complement the building materials.
- Building identification signage should be placed on facades that face the primary street(s).
- Signs should be artful and creative and work with a building's architecture to add interest.

RESTRICTIONS

Internally illuminated box signs with more than 30% of the internal area illuminated are not permitted.

Animated, blinking, or flashing signs are not permitted.

ACCEPTABLE SIGN TYPES

The following sign types are acceptable for attached building signs:

- 1 Wall signs** - Wall signs include signs that are attached to the face of a building wall. They should be mounted on the wall facing the public realm.
- 2 Window Signs** - Window signs are painted, placed, or affixed in or on the interior of a window, and intended to be viewed from the outside. Window signs should not obscure views into store or business.
- 3 Projecting Signs + Hanging Signs** - Projecting signs are attached to the building face and project out perpendicular to the building. Hanging signs are similar to projecting signs, except that they are suspended from a marquee or other overhead canopy.
- 4 Awning Signs** - Awning signs are signs that are mounted, printed on, painted on, or otherwise attached to an awning or canopy above a business door or window.
- 5 Mural** - Sign that is painted onto a wall that is visible to the public realm.



Building Lighting

INTENT

To integrate lighting on buildings into the architectural design to creatively illuminate pedestrian areas and highlight building elements.

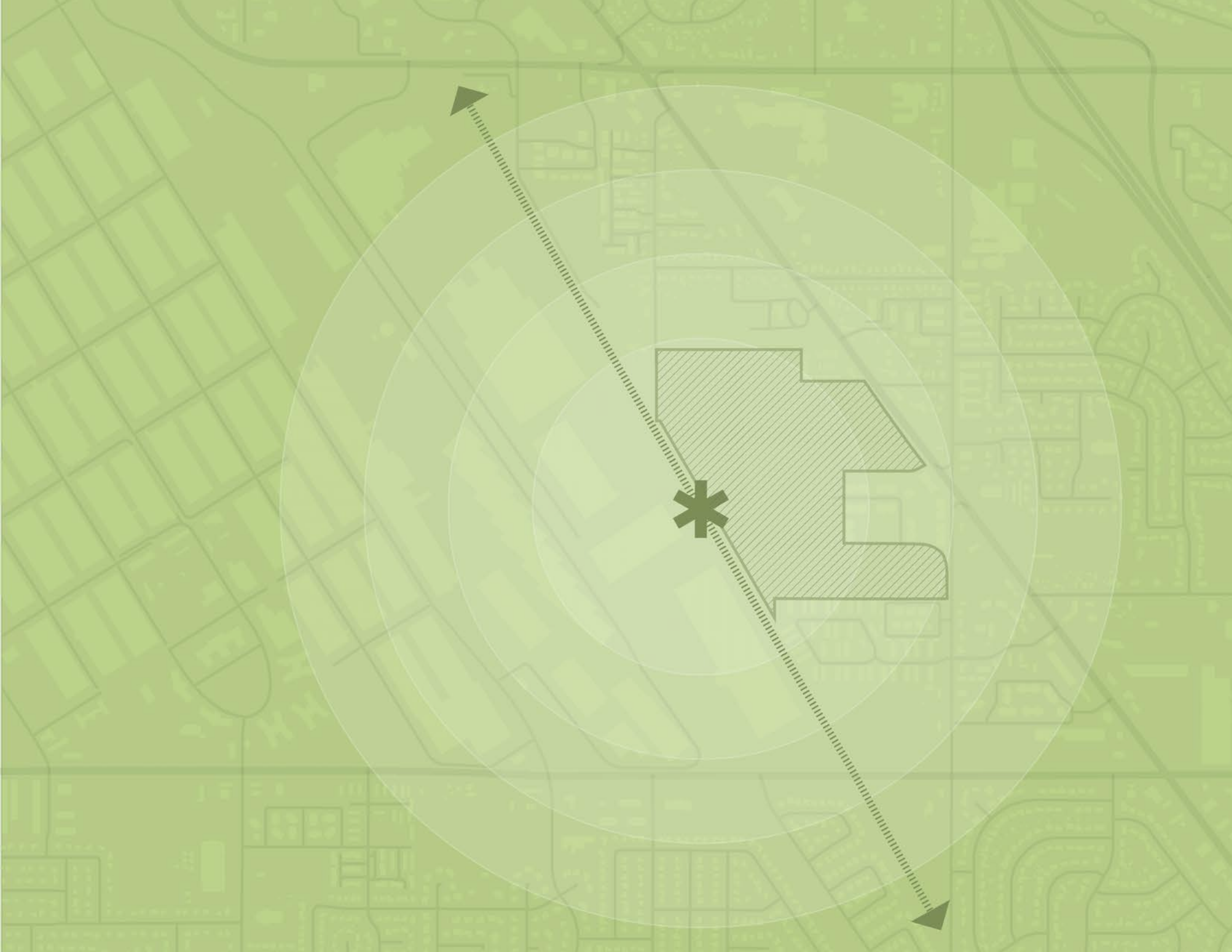
DESIGN GUIDELINES

- Pedestrian areas should have adequate illumination for safety.
- Lighting should be sensitive to residential development limiting glare, minimizing spill light, and minimizing light on upper stories of residential buildings.
- Retail buildings should integrate lighting with retail signage, storefront windows, and other building elements to enhance visibility and visual interest.
- Use creative lighting solutions to illuminate outdoor areas and add interest and life to outdoor spaces.
- All lighting should be dark-sky compliant.

PRECEDENTS

- 1 Ground floor transparency allows internal lighting to illuminate the street and creates a “glow.”
- 2 Lights on building exterior highlight the ground floor retail space and illuminate the street.
- 3 Light illuminates steps to promote pedestrian safety.
- 4 Lights used on canopy and sign add visual interest, as well as highlight the building entrance.
- 5 Overhead lights used to help create an interesting and exciting “place.”







06

OPEN SPACE + PUBLIC REALM

DESIGN GUIDELINES

Open Space Network

OVERVIEW

As part of establishing the Clearfield Station Area as a livable urban neighborhood, a high-quality, comprehensive open space network is essential. The Station Area will provide a variety of open space types to meet the needs of the various residents and visitors of the neighborhood. Open spaces will be provided in a variety of sizes and scales and will serve a range of specific functions. Most of this network will be part of the public realm and the remaining will be private, although all will contribute to the establishment of a unique and specific experience that complements one another.

INTENT

To create a comprehensive open space network that provides a unique yet unified system of parks and open spaces throughout the neighborhood.

OPEN SPACE TYPES

The open spaces shown in the Illustrative Master Plan on the following page are conceptual. The specific intent for each is defined and illustrated in the pages that follow.

The district currently contains a large drainage basin in the southwest corner which will remain. The following open space types are outlined in this document:

- Park
- Pocket Park/Plaza
- Village Square
- Transit Plaza
- Greenway
- Private Plaza + Open Space
- Cemetery
- Stormwater Basin
- Yards + Landscape Buffers

DESIGN GUIDELINES

- The open space network should provide a variety of open space types that complement one another.
- The open spaces should be integrated into the urban form of the neighborhood.
- Buildings should frame open spaces in a deliberate manner, rather than open spaces just being developed in the “leftover” spaces.
- The design and programming of each open space should reflect the latest trends in open space design to provide an experience and aesthetic that fits the wants and needs of the current day.
- Streets should be considered part of the open space network and should be designed in a pedestrian-friendly manner that promotes comfort, safety, and provides places to stop and linger.
- Green infrastructure systems and ideas should be incorporated into the open space system.
- Buildings and respective land uses should work together with adjacent open space to provide uses that complement each other.

Park

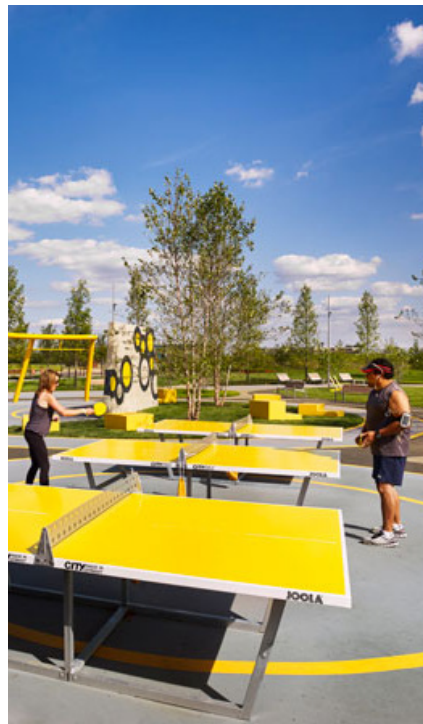
INTENT

To provide a public park space that is geared specifically toward residents in the neighborhood and functions like the backyard of the neighborhood where residents can relax and play in an informal environment.

FEATURES + ELEMENTS

The **Park** open space type should include:

- Children's playground and other play elements
- All ages play elements such as ping pong, pickleball, bocce, etc.
- Flexible lawn areas for informal active and passive recreation
- Pathway loops for exercise



Pocket Park / Plaza

INTENT

To provide a series of smaller parks and plazas that are typically located on small, irregular parcels, and are dispersed throughout the neighborhood. These spaces can serve as extensions of both the streetscape and the building.

FEATURES + ELEMENTS

The *Pocket Park/Plaza* open space type should include:

- Seating
- Interesting landscape design elements such as paving, planting, or other features
- Landscape features that reinforce the industrial theme for the neighborhood
- Outdoor dining seating (if applicable)
- Green space/planting to soften the urban environment



Village Square

INTENT

To provide a central open space of approximately 1 acre that is located in a highly visible area in the heart of the neighborhood. It should also become the primary gathering place for civic and social purposes, and should function as the living room for the neighborhood. This should become an iconic regional destination.

DESIGN GUIDELINES

The *Village Square* open space type should include:

- A strong image and identity that helps define the image of Clearfield Station.
- Framed by buildings with active ground floor uses that promote activity on the square.
- Iconic landscape features
- Flexible open gathering space for events
- Public art



Transit Plaza

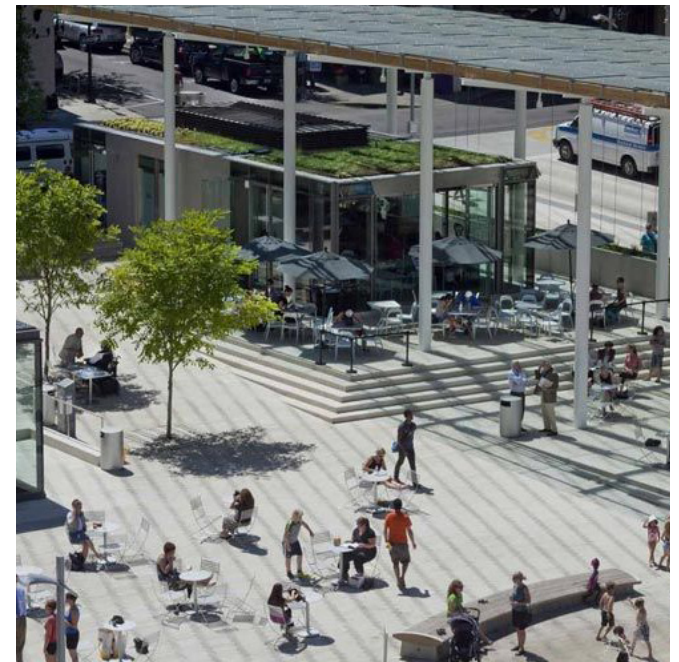
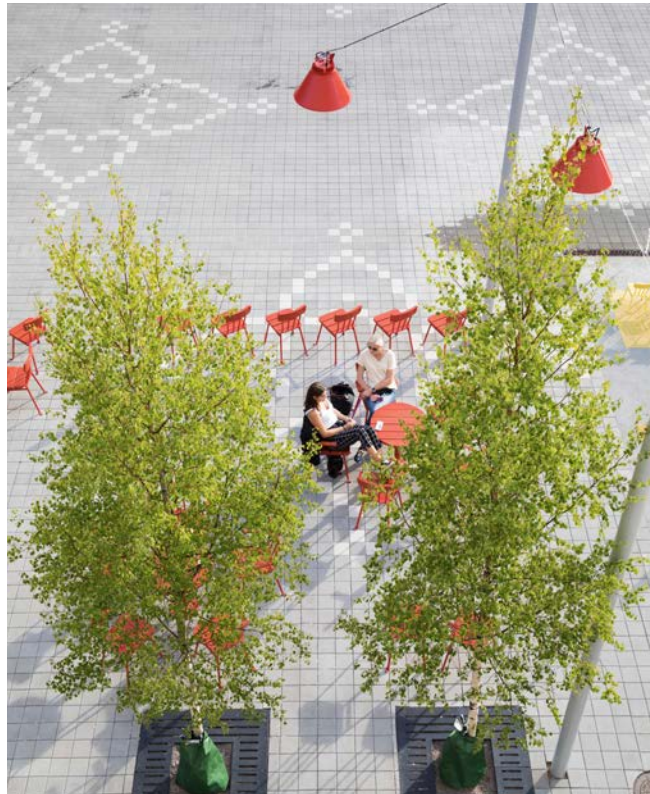
INTENT

To provide an open space adjacent to the commuter rail platform and bus loading zone that is specifically designed to enhance the experience of using public transportation by providing amenities that are geared toward transit users.

DESIGN GUIDELINES

The *Transit Plaza* open space type should include:

- Cafe, restaurant, or other convenient food options
- Public Restrooms
- Public art
- Seating
- Shade
- Landscape features that reinforce the industrial theme for the neighborhood.



Greenway

INTENT

To increase pedestrian connectivity between neighborhoods and to public open space, while also providing open space amenities for both visitors and adjacent residents.

FEATURES + ELEMENTS

The **Greenway** open space type should include:

- Pathways and trails
- Green space and trees
- Seating
- Small recreation activities
- Dedicated space for dogs and/or other pets



Private Plazas + Open Space

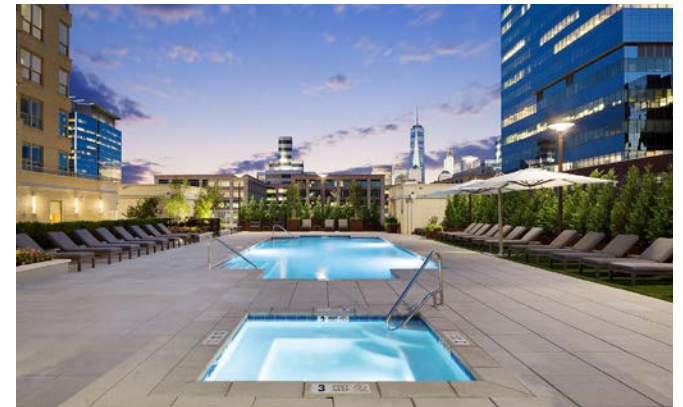
INTENT

To provide private open spaces for residents and/or employees of a building.

FEATURES + ELEMENTS

The **Private Courtyard / Rooftop Deck** open space type should include:

- Lounge and relaxation spaces
- Pools and hot tubs
- Outdoor cooking facilities
- Fire places
- Green space and trees
- Seating
- Small recreational activities
- Small private event gathering spaces



Yards + Landscape Buffers

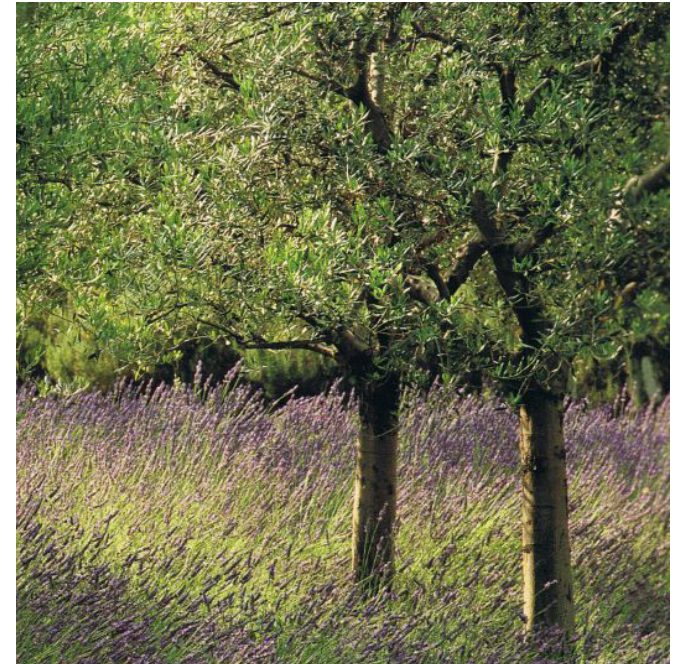
INTENT

To provide private yards and landscape buffers between buildings that are visually restorative while also being waterwise and environmentally appropriate.

FEATURES + ELEMENTS

The **Yards + Landscape Buffers** open space type should include:

- Waterwise Landscaping



Enhanced Streetscape

INTENT

To provide streets that are first and foremost designed to create a friendly pedestrian experience, in part by providing the appropriate pedestrian amenities.

FEATURES + ELEMENTS

The **Enhanced Streetscape** open space type should include:

- Seating
- Outdoor dining seating (where applicable)
- Landscape plantings
- Unique/Interesting paving
- Pedestrian lighting
- Public art integrated into functional streetscapes
- Street furniture such as trash/recycling receptacles, bollards, and more

See streetscape guidelines on pages 108-117 for more detail.



Materials + Colors

INTENT

To ensure a consistent application of complementary and high quality materials throughout the neighborhood that will reinforce the unique identity and a sense of place.

DESIGN GUIDELINES

- Landscape materials should reinforce the industrial theme by using concrete, steel, timber, brick and stone. See materials images for specific application of these materials.
- Utilize historic industrial remnants from the adjacent railroad, industrial area, and/or the historic navy depot, by integrating them into the landscape, if available.
- Materials are encouraged to have a weathered, industrial feeling. This could be done in various ways, such as using rough cut stone or concrete, or by using tumbled stone or brick. The weathered look should help create a feeling of “authenticity.”

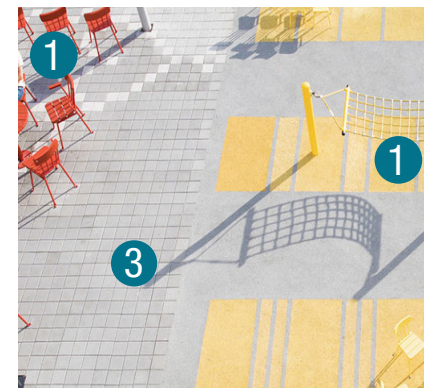
COLOR

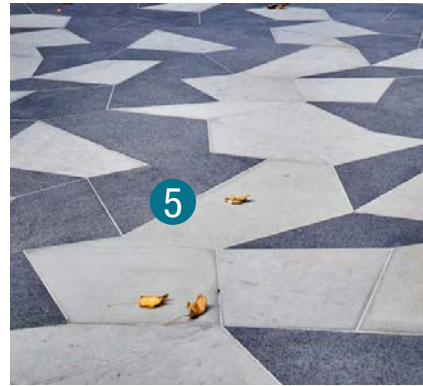
The most prominent color associated with industrial areas is gray, with reds and blacks also playing a large role. These colors should remain as a base for landscape material colors, but should also be supplemented with more modern and interesting colors. Specifically, brighter colors should be strategically added in minimal, but visually prominent ways, to contrast the muted gray tones.

- 1 Pop of Color as an Accent

ACCEPTABLE MATERIALS

- 2 Stone Pavers
- 3 Concrete Pavers
- 4 Broken Industrial Concrete
- 5 Abstract Industrial Broken Concrete
- 6 Decomposed Granite / Crusher Fines
- 7 Rough Cut Stone
- 8 Wood / Timber
- 9 Industrial Remnants (New + Old)
- 10 Steel / Railroad Track
- 11 Asphalt Pavers
- 12 Concrete / Board Form Concrete





Planting

INTENT

To reinforce the unique look and feel of the Station Area by utilizing planting in a way that is complementary to the contemporary industrial theme.

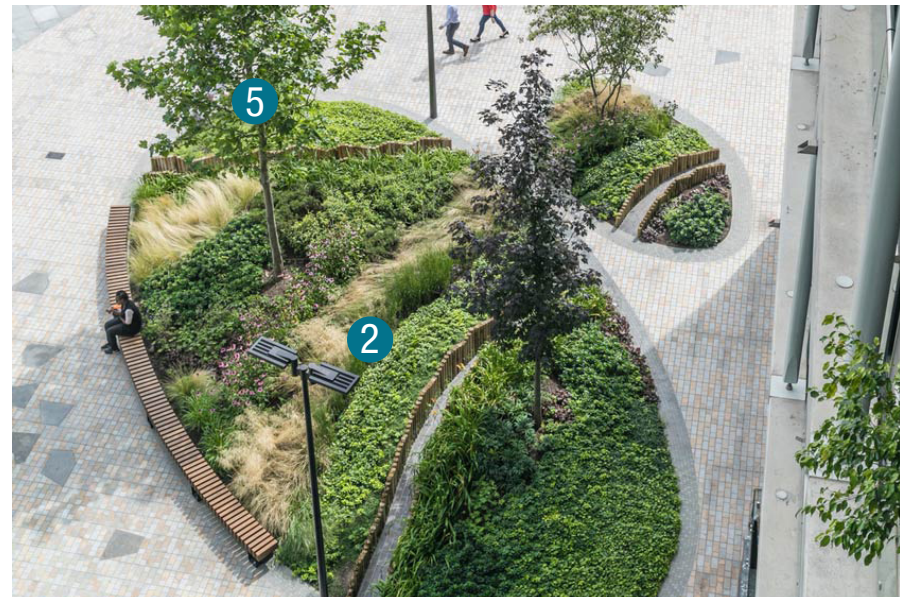
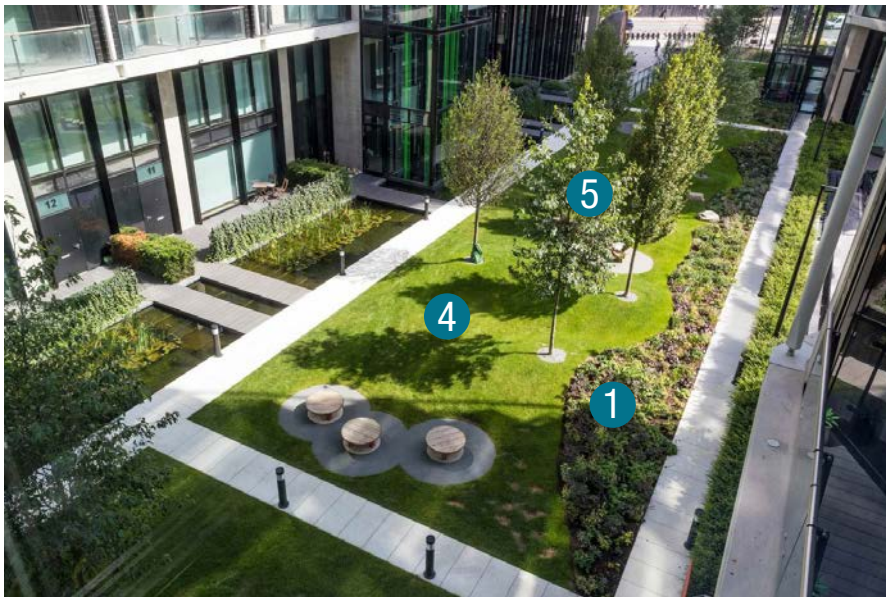
DESIGN GUIDELINES

- Planting areas should generally have an organic feel.
- Planting in groups to create attractive massings is encouraged.
- Lawn areas should be used strategically in areas that will become functional gathering places. Lawn areas should be minimized in other areas, and replaced with more water efficient landscape planting.
- Use perennials, bulbs, and wildflowers to add color to the landscape.
- Choose plants that minimize long-term maintenance costs.

PRECEDENT

- 1 Organic Planting
- 2 Groups of Plants create organized massing
- 3 Naturalized meadows, native grasses, and perennials add color to the landscape.
- 4 Lawn area appropriately sized for gathering space.
- 5 Trees provide shade





Landscape Design Theme

INTENT

To establish a specific “look and feel” to unify the area by developing a landscape “language” that will help brand the neighborhood with a unique aesthetic that also works with the architectural design.

DESIGN THEME - “CONTEMPORARY INDUSTRIAL”

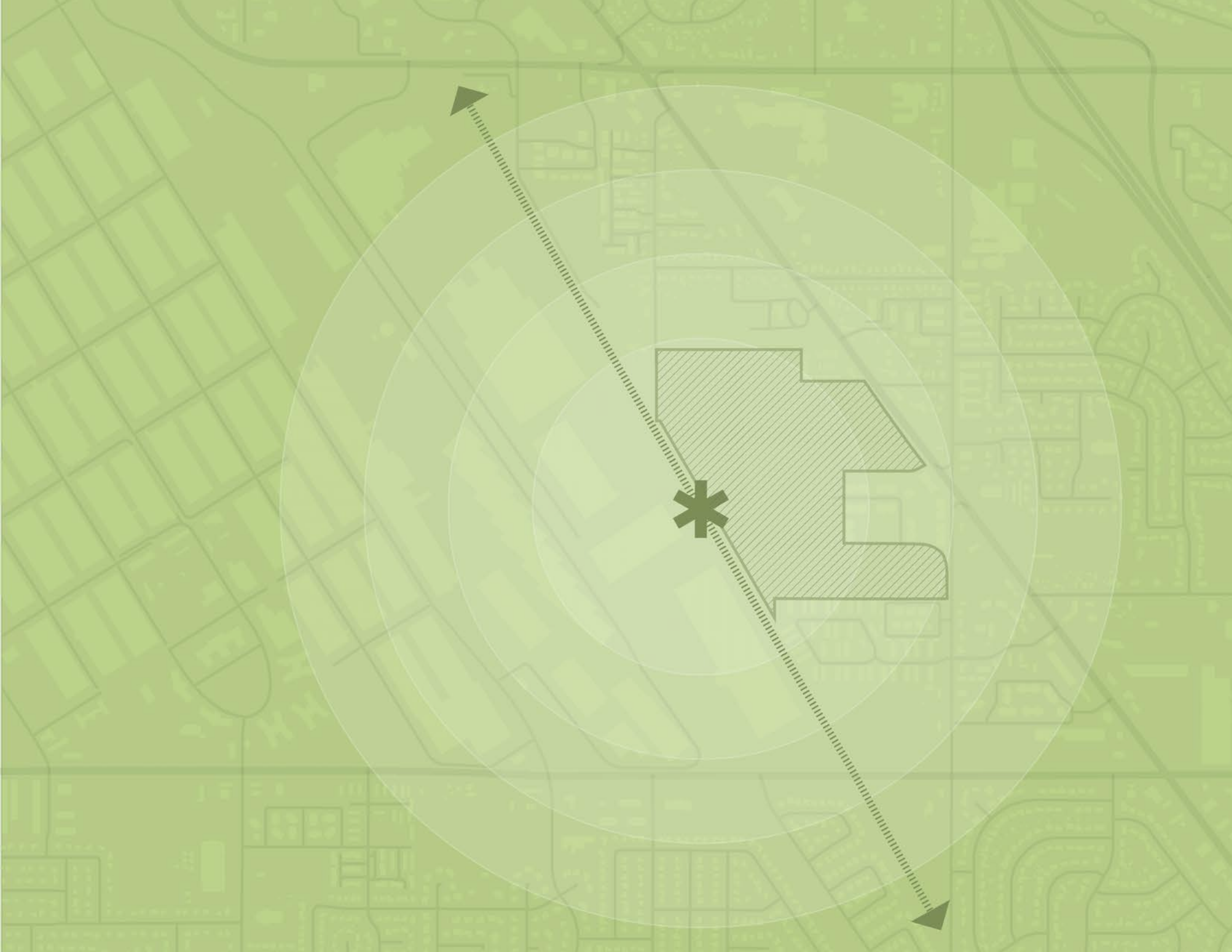
The landscape design theme for the Station Area will mirror the architectural design theme with a contemporary industrial style that is modern, yet rooted in the industrial character that surrounds the area. This industrial character helps to create a brand for the area and provides a common theme that ties the neighborhood together.

LAWN AREAS

Lawn areas should be used strategically in areas that will become functional gathering places. Lawn areas should be minimized in other areas, and replaced with more water efficient landscape planting.



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07

TRANSPORTATION + MOBILITY

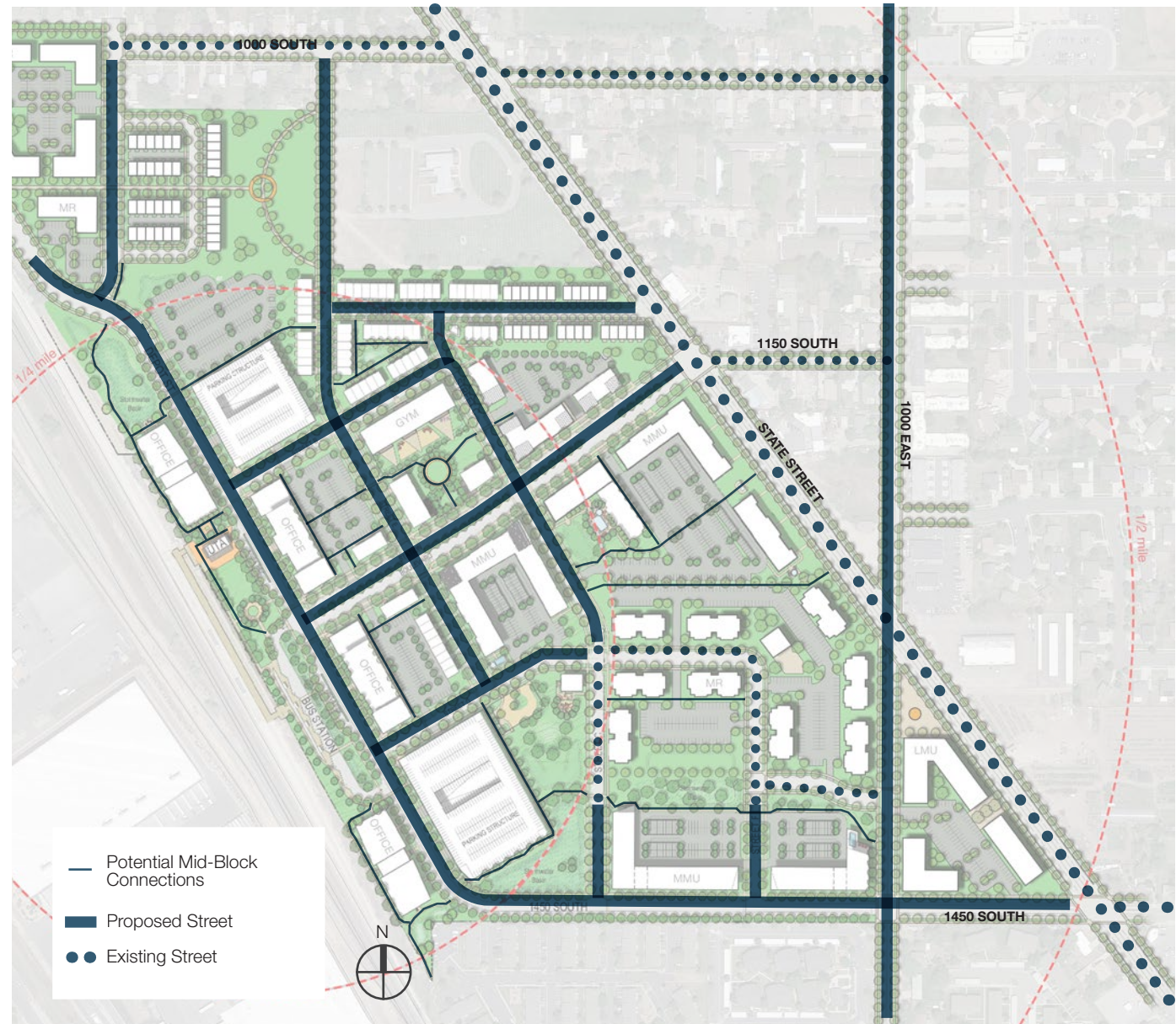
DESIGN GUIDELINES

Streets + Blocks

The street layout of the Clearfield Station District will provide the foundation for the urban form of the area, which will help define the character and performance of the neighborhood. Once established, the street pattern will remain in place as the long-term structure and framework for the area, even as buildings and land-uses may change and evolve over time.

This layout incorporates the following:

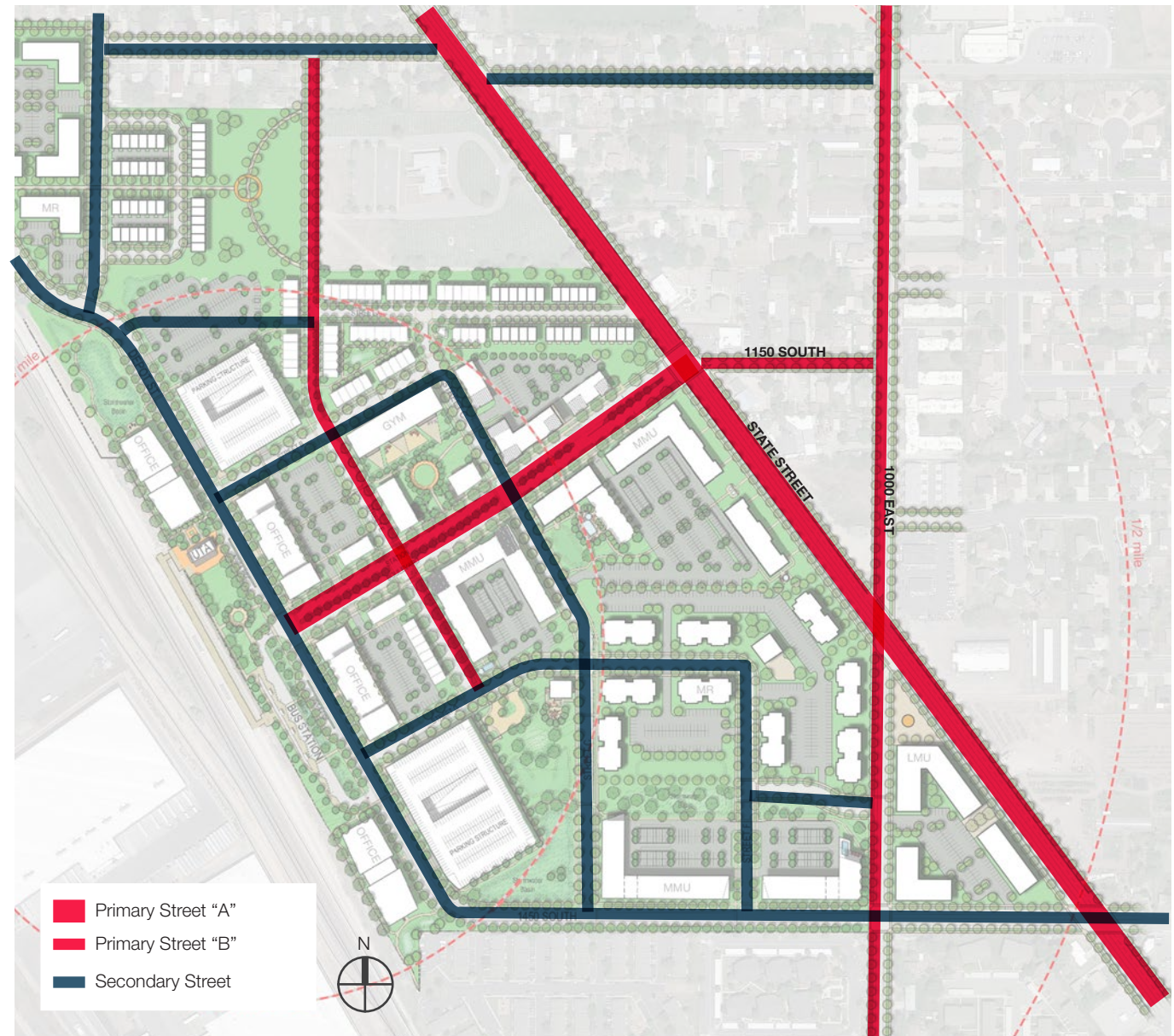
- New streets connect into the existing street pattern to increase connectivity into the MDP site.
- Blocks are between 300' and 350' which is consistent with block sizes in successful, walkable downtowns throughout the country.
- The block size provides a good balance of ensuring good connectivity throughout the area, as well as providing a large enough block to allow for a variety of development options.
- Mid-block connections are encouraged to be designed into each block, if feasible, to further increase connectivity.



Street Hierarchy

A hierarchy of streets has been established in order to define the most prominent and important streets in the neighborhood. It defines the various roles each street will play in regards to traffic volumes, modal choices, and pedestrian experience.

The street hierarchy specifically relates to the ground floor treatment of buildings, which is covered in Section 05 Buildings + Architecture of this document.

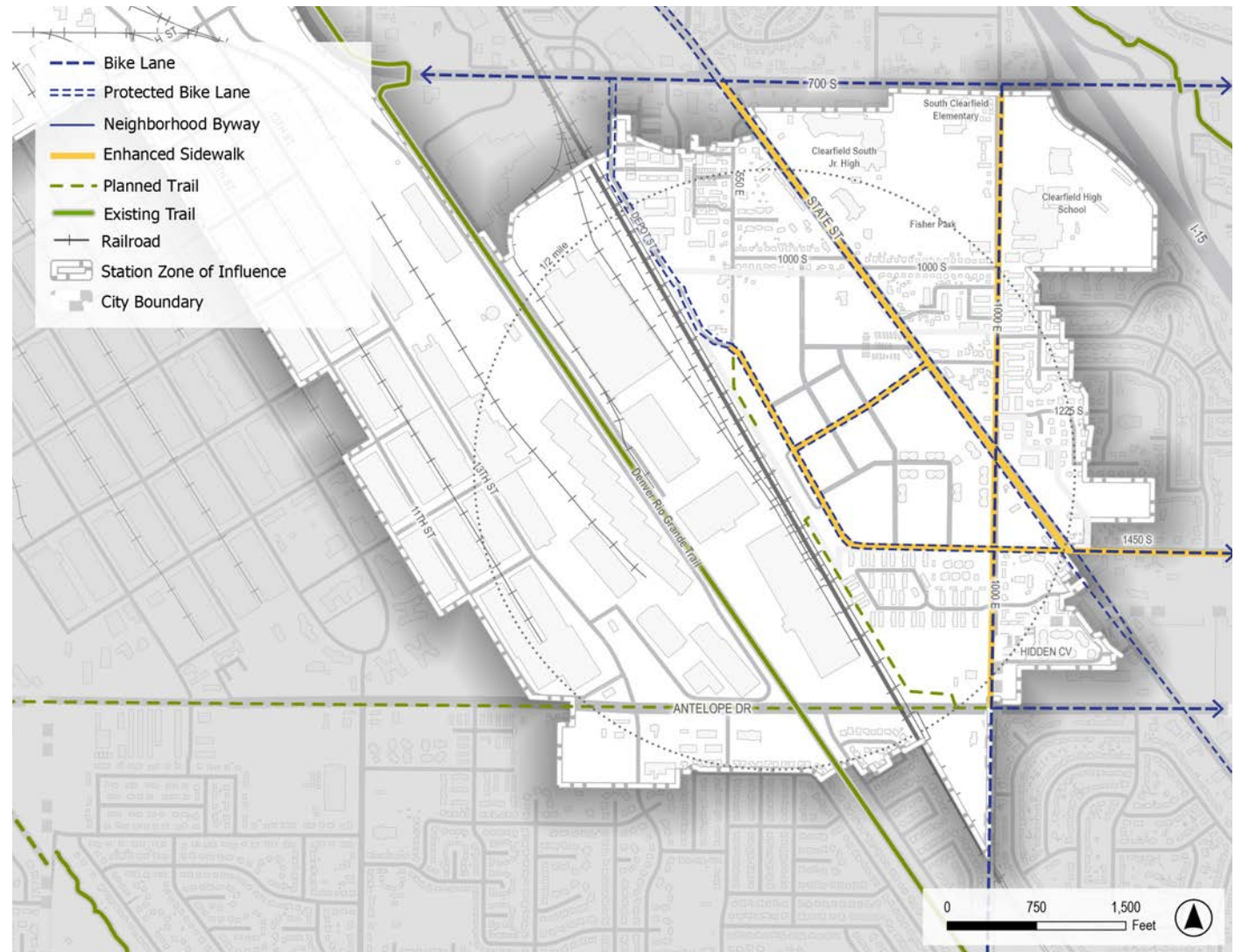


Active Transportation

Active transportation is defined as modes of travel that require physical effort. In Clearfield Station Area, this is specifically manifested as pedestrian and bicycle transportation.

Active transportation is an essential component of a transit-oriented development, as strong pedestrian and bicycle facilities allow transit users to connect from the train/bus to their destination with relative comfort and safety.

Quality active transportation facilities are also important for encouraging healthy lifestyles and reducing vehicle travel and congestion. The map on the right illustrates the proposed active transportation facilities for the station area.



PEDESTRIAN FACILITIES

The station area will specifically focus on providing pedestrian-friendly streets throughout the neighborhood. See street type guidelines on 107-117.

Special attention should be paid to ensuring highly visible and safe street crossings. Crosswalks should be located at all intersections within the area to enhance pedestrian connectivity.

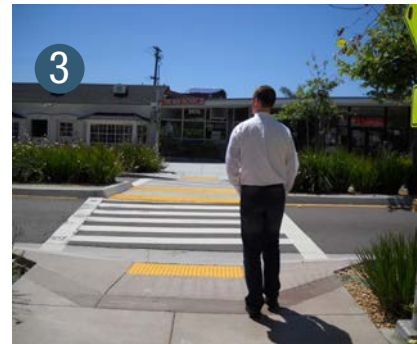
Bulb-outs (or curb extensions) should also be used throughout the neighborhood to calm vehicular traffic and shorten pedestrian crossings. Street trees should be used to increase pedestrian comfort and calm traffic.

CYCLING FACILITIES

Cycling facilities will be provided on primary streets within the Station Area. A protected cycle track will be provided on Station Boulevard. An on-street bike lane will run along Depot Street, through the MDP site, connecting to 1000 East. All other streets in the neighborhood will be designed to allow for a safe mix of cyclists and vehicles in vehicular travel lanes.

PRECEDENTS

- 1 Sidewalk with many elements that add to a comfortable, safe, and interesting pedestrian experience, including street trees, planters, brick pavers, ground floor transparency, pedestrian lighting, bike parking, seating, and outdoor dining.
- 2 Bulb-out helps to calm vehicular traffic and shortens pedestrian crossing lengths.
- 3 Highly visible crosswalk with median refuge and signage.
- 4 On-street bike lane with a painted buffer to increase safety.
- 5 Raised Cycle track separates bikes (and other users, such as scooters, skateboarders, etc.) from vehicular traffic lanes. It also separates these users from the pedestrian sidewalk space.



Transit

The commuter rail is the central feature of the Clearfield Station District, and planned development is arranged to maximize its use as a method for transporting people to and from the station, reducing the need for vehicular trips.

The commuter rail platform and entrances will remain. The bus loading zone will shift slightly to the South of Station Boulevard as seen in the map.

Bus traffic will largely be routed along the boulevard, with an option to exit on 1450 South. Bus routes with connection to Holy Cross Hospital - Davis will likely travel via 1450 South to and from the station platform.

A kiss-and-ride area will be established, as shown, to provide transit users from outside the neighborhood with convenient access to the commuter rail platform.

Transit facilities shall conform to UTA's design standards.



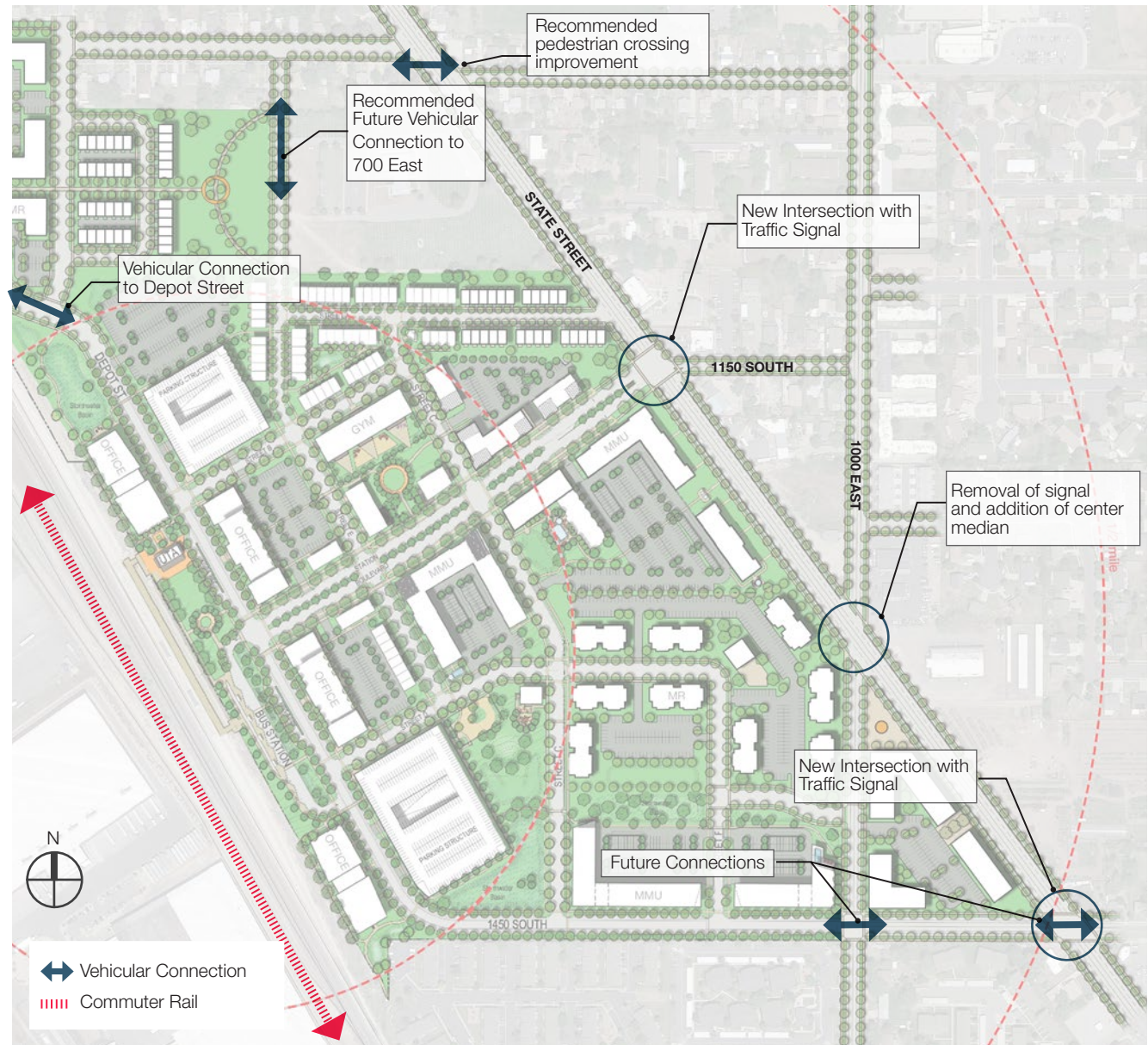
Vehicular Transportation

The Clearfield Station District is intended to be a multi-modal destination, with priority given to pedestrians and cyclists. However, vehicular transportation will still be a fundamental element that must be carefully planned to minimize traffic issues. The increase in development, as outlined in this plan, will have significant impacts on traffic, and traffic mitigation efforts must be carefully considered.

New streets should connect into existing streets to increase connections and to disperse traffic flows in and out of the area as much as possible. A connection to Depot Street should be prioritized. A connection to 700 East is also encouraged.

Improved pedestrian crossings on State Street at 1000 South, 1150 South, 1000 East, and 1450 South are recommended, in addition to a crossing at 1000 East and 1150 South. These connections are intended to overcome active transportation barriers and should prioritize bike and pedestrian safety.

The addition of traffic signals on State Street at Station Boulevard and 1450 South and the removal of the signal at 1000 East and State Street will likely have traffic impacts. The full impact of signal changes and/or removals will require further study and coordination with UDOT.



Traffic Analysis

A traffic impact analysis for the *Clearfield Station Area Plan* identifies the traffic impacts that the proposed land use scenario for the station will have in the surrounding intersections.

This traffic analysis is a 2023 update to the traffic analysis completed in 2019 by Fehr & Peers for the *Clearfield Station Area Plan*. It includes updated land use and trip generation assumptions for the Clearfield Station Area, including all parcels within ½ mile of the station that were not included in the 2019 analysis. All traffic volume growth assumptions and vehicle trip reduction percentages remain consistent with the 2019 analysis.

Trip generation for the project was computed using rates published in the Institute of Transportation Engineers (ITE) Trip Generation, 11th Edition, 2023.

The net external vehicle trips expected to be generated by the Clearfield Station Area, the percent reductions due to trips that start and end within the development, and trips that are done by transit, biking, or walking are shown in Table 5.

The Clearfield Station Area will generate significant traffic at the surrounding intersections, and mitigations will be needed to accommodate the new traffic. This analysis focused on the analysis of four intersections close to the Clearfield Station Area:

- State Street/2000 North
- State Street/1000 East
- State Street/Station Boulevard
- State Street/700 South

The operating performance of these intersections is described by the Level of Service (LOS). LOS is measured quantitatively and reported on a scale from A to F, with A representing the best performance and F the worst. See *Appendix B: Traffic Analysis* for descriptions of each LOS designation.

Using the traffic modeling software Synchro and the HCM 6 delay thresholds introduced above, the existing and existing plus project AM and PM peak hour LOS were computed for each study intersection. The preliminary results of this analysis are reported in Table 6.

TABLE 5: MXD TRIP GENERATION AND REDUCTION ESTIMATES

Time Period	Project Gross Trips	Net External Vehicle Trips	Vehicle Trip Reduction
Daily	30,319	26,226	13.5%
AM Peak Hour	2,002	1,616	19.3%
PM Peak Hour	2,888	2,221	23.1%

TABLE 6: LEVEL OF SERVICE SUMMARY

Intersection			Existing	Existing Plus Project	Existing Plus Project Mitigated
ID	Location	Period	LOS & Sec/Veh ¹	LOS & Sec/Veh ¹	LOS & Sec/Veh ¹
1	State Street / 2000 North	AM	D / 37	D / 37	D / 37
		PM	D / 41	D / 50	D / 50
2	State Street / 1000 East	AM	C / 26	C / 32	C / 26
		PM	D / 52	F / 96	E / 74
3	State Street / Station Boulevard	AM	B / 12	E / 47	E / 47
		PM	C / 19	F / >300	F / >300
4	State Street / 700 South	AM	C / 25	C / 27	D / 27
		PM	E / 63	F / 87	E / 58

1. Overall intersection LOS and average delay (seconds/vehicle) for the signalized intersections and worst movement LOS and average delay for the unsignalized intersections.

ANALYSIS RESULTS

All intersections in the existing conditions operate at acceptable levels during the AM peak hour (LOS D or better); however, **the State Street/700 South intersection operates at LOS E during the PM peak hour.**

With the addition of the proposed land use scenario for the Clearfield Station Area, the development access onto **State Street is LOS E** during the AM peak hour, and **all intersections except Main Street/2000 North operate at LOS E or F during the PM peak hour.**

The existing plus project scenario was also mitigated, i.e., the signals were optimized to provide better results. **This scenario shows significant improvements for the State Street/1000 East and State Street/700 South intersection during the PM peak hour.**

Therefore, it is recommended that the signals are optimized as the station area develops.

MITIGATION STRATEGIES

Other potential mitigations to alleviate the impact of the development on the surrounding area are:

- **Distribute internal traffic to all development accesses.** The main access to the development will be through State Street. However, three other accesses are proposed for this development: a south access onto 1000 East, and two north accesses, one onto 700 South (via Depot Street) and one onto 1000 South (via the recommended connection of 700 E). Encouraging the use of all development access points could alleviate the high traffic impact on State Street. However, a signalized access onto State Street might still be needed.
- **Signalize a secondary major access onto 1000 East.** 1000 East is a local road owned by Clearfield City. Adding a secondary major access onto this road will alleviate the traffic using access onto State Street.
- **Follow TOD best practices on parking supply.** Research conducted by the Utah Transit Authority and the University of Utah's Metropolitan Research Center indicates that mixed-use developments at transit stations generally require significantly less parking than similar developments that lack good transit access. The Utah Transit Authority also released Transit Oriented Development guidelines that provide standards for parking, although these guidelines provide a greater level of parking than the University of Utah research suggests to be necessary.
- **Establish a Transportation Demand Management (TDM) coordinator.** Having a TDM coordinator for the area would help employees and residents find other means of transportation to/ from the TOD beyond driving alone. Examples for TDM measures are incentivizing the use of transit, biking, and walking; having various office hours within the development; etc.
- **Optimize signals to improve PM peak hour LOS along State Street through the Clearfield Station Area.** The traffic analysis results showed significant improvement at signals along State Street when signal optimization was implemented. This strategy should be used at all signals in the station area to improve traffic conditions during peak hours.

Streetscape

INTENT

To create a cohesive, functional, and safe network of streets and walkways that supports a variety of travel modes and connects, attracts, and activates the neighborhood.

DEFINITION

The streetscape is defined in this document as the part of the street between the curb and the building.

DESIGN GUIDELINES

- The streetscape should be considered an important part of the neighborhood open space system, and should provide safe, comfortable travel, as well as interesting places that are desirable to spend time.
- Streets should be designed as outdoor rooms with attractive places to sit, stop, gather, and play.
- Streets should provide opportunities for neighbors and visitors to meet one another and create a vibrant community-oriented neighborhood experience.

- Paving materials and patterns should provide interest and excitement, while also being durable, functional, and easy to maintain.
- Changes in paving should be used to differentiate between streetscape zones.
- Curb radii should be minimized on street corners to slow vehicles making turning movements and maximize pedestrian safety.
- Bulb outs should be used at all intersections and mid-block street crossings to calm traffic and minimize the length of pedestrian crossings.
- Green infrastructure may be incorporated into the streetscape in the street zone with stormwater retention systems or other innovative green systems.

BUILDING ZONE

The building zone is the space between the travel zone and the building facade. This zone can be used to display merchandise, enhance entryways, or provide outdoor seating and dining. It should generally be thought of as an extension of the building into the public realm. This space will typically require some space from a building setback to provide enough usable space.

TRAVEL ZONE

The travel zone is reserved for unobstructed pedestrian travel. It is located between the building zone and the street zone. The National Association of City Transportation Officials (NACTO) recommends 5-7 foot wide sidewalks in residential areas, and 8-12 foot wide sidewalks in downtown areas.

STREET ZONE

The street zone is the space between the travel zone and the street. This area can be landscape or hardscape, and is where trees and street furniture should be located.

STREET TREES

Street trees are required in regular intervals on all streets in the neighborhood. They should be located at least 30 feet apart.

STREET FURNITURE

Street furniture should be provided as part of the general streetscape design for all streets in the neighborhood. The following list includes street furniture that should be included within the Clearfield Station Area. However, not all streets will require all street furniture elements.

- Street Lighting
- Pedestrian Lighting
- Seating / Benches
- Trash / Recycling Receptacles
- Bike Racks
- Wayfinding Signage
- Raised Planters
- Bollards



GENERAL STREETSCAPE ELEMENTS

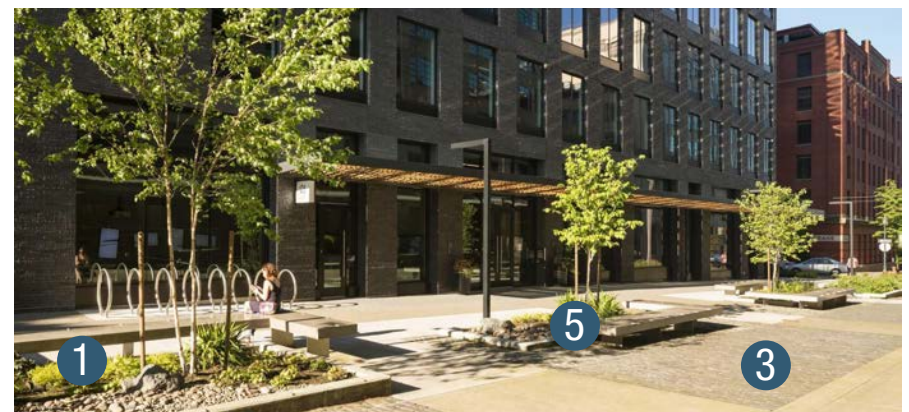
Streetscape design is key in creating an inviting pedestrian environment and a walkable neighborhood.

This graphic demonstrates how the three streetscape zones are broken down, and the simple fundamentals behind effective street design.

- 1 A consistent streetwall on both sides of street, as well as vertical elements such as trees, create a sense of enclosure.
- 2 A consistent row of trees provides a sense of enclosure, protects pedestrians from vehicles, provides shade, and brings nature into the urban environment.
- 3 Street furniture such as lighting, seating, trash receptacles, and bike racks are included in the street zone as pedestrian amenities.
- 4 Seating and outdoor dining is provided in the building zone as an extension of the indoor dining area.

STREETSCAPE PRECEDENTS

- ① Street zone contains trees, plantings and street furniture.
- ② Building zone contains pedestrian amenities such as outdoor dining.
- ③ Interesting paving pattern brings excitement and refinement to the street
- ④ Bioretention strip is built in to the street zone of the streetscape to filter stormwater.
- ⑤ Seating is designed into interesting streetscape planters.



Street Types

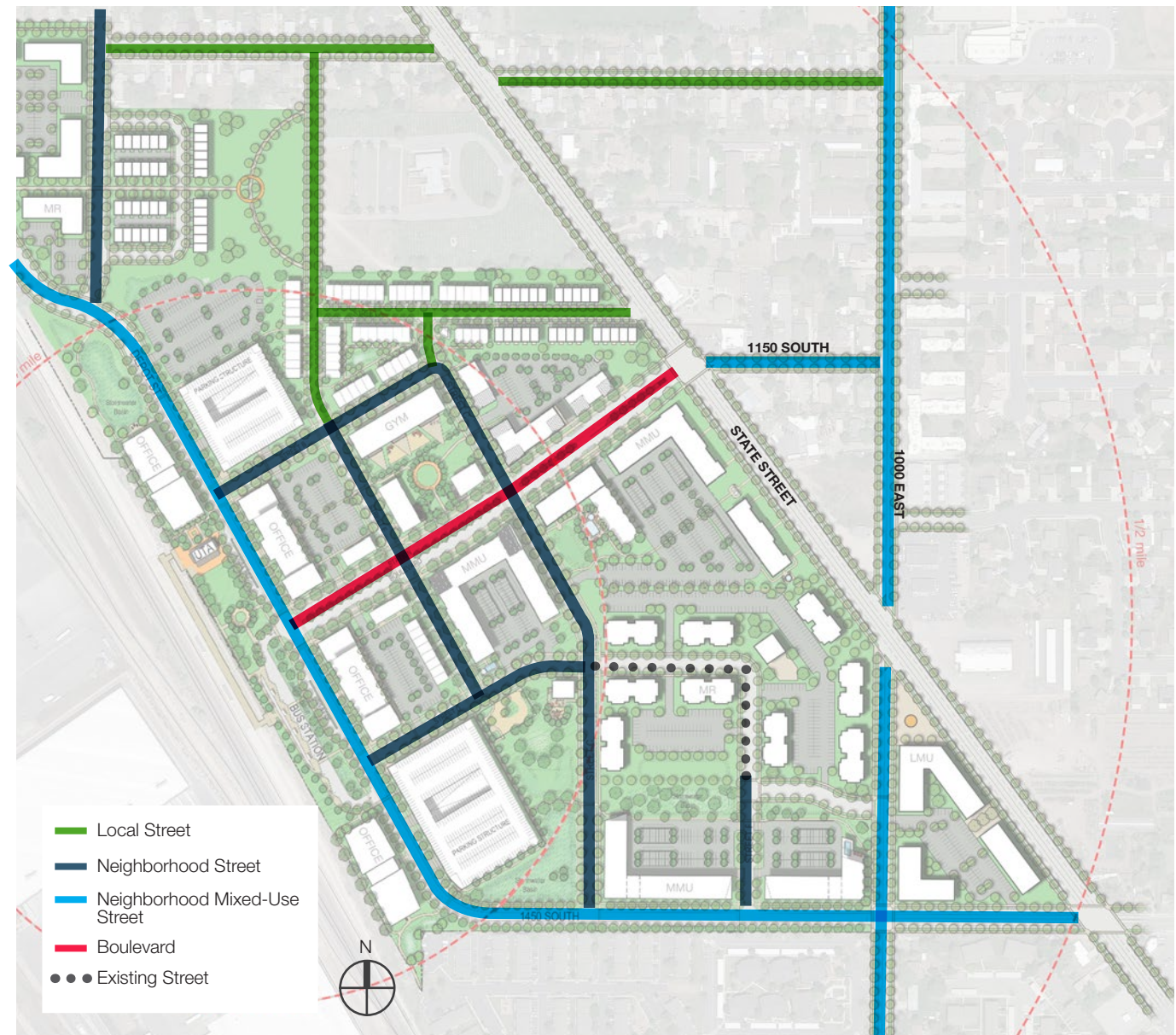
Five street types have been established for the Clearfield Station District.

The **Local Street** is a low-speed and low-volume street for connecting neighborhoods to connector streets.

The **Neighborhood Street** is the default street design, and the most common street in the neighborhood.

The **Neighborhood Street - Mixed-Use** street type is identical to the "Neighborhood Street," but has dedicated on-street bike lanes.

The **Boulevard** street type is established as the primary street in the neighborhood, which connects State Street to the transit station.



Local Street Type

The **Local Street** type is primarily used on residential-only streets within and beyond the Station District. Similar to Neighborhood Streets, Local Streets are intended to provide access for neighborhoods and function as a livable outdoor space but on lower volume, quieter streets.

The local street type includes street trees, plantings, and sidewalks.

This street section is designed for a slow speed, which allows bicycles to safely and comfortably share the vehicular lanes.





Neighborhood Street Type

The *Neighborhood Street* type is the default street type that will be used in the station area and will make up the majority of streets in the neighborhood. It is intended to provide access for neighborhoods and function as a livable outdoor space. The design and layout of the street is a simple, time-tested solution that creates safe, walkable, and livable streets.

The neighborhood street type includes on-street parallel parking, street trees, plantings, lighting, benches, and sidewalks.

This street section is designed for a slow speed, which allows bicycles to safely and comfortably share the vehicular lanes.





Neighborhood Mixed-Use Street Type

The *Neighborhood Mixed-Use* street type is identical to the *Neighborhood Street* type, with the exception of adding on-street dedicated bike lanes.

The buffered bike lanes on these streets will provide safe and convenient access for bicycles on the streets that connect the station area to the rest of the City.



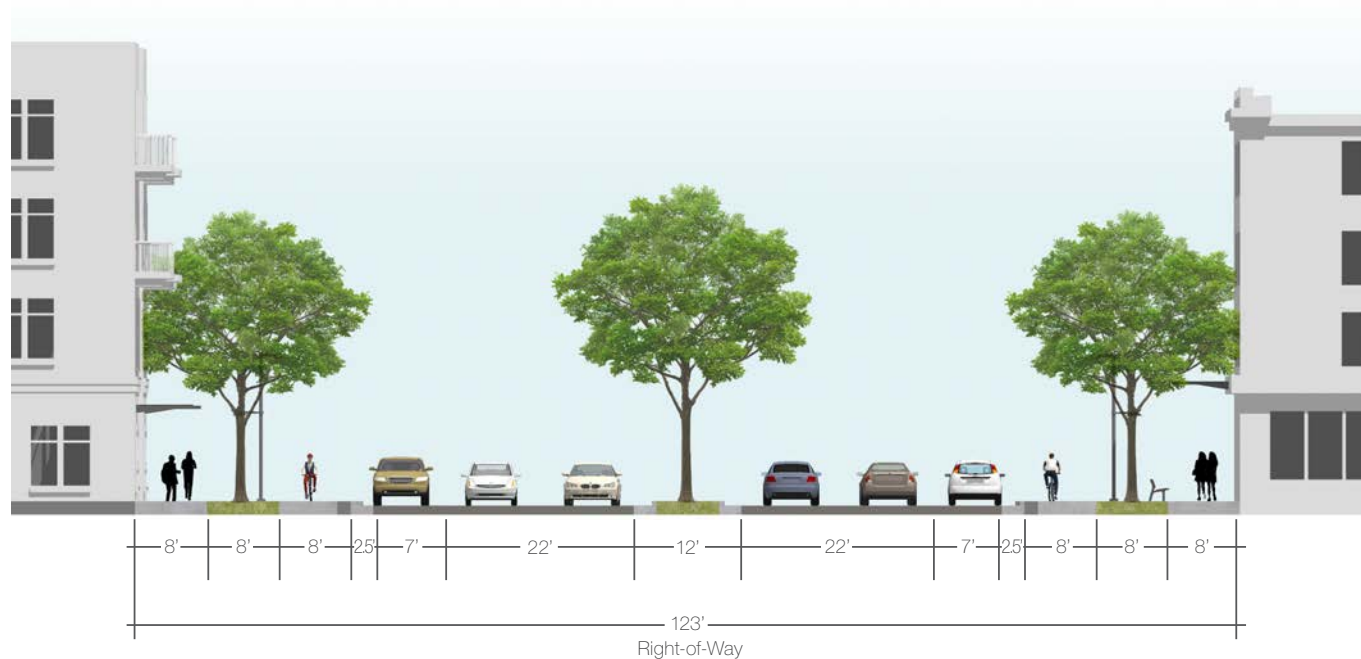


Boulevard Street Type

The *Boulevard* street type is intended to be the “Main Street” for the Clearfield Station District, connecting State Street to the Transit Station.

Station Boulevard should be designed to have a grand, iconic appearance, as it is the main entrance to the neighborhood and the heart of the station area. It should be designed to be functional, safe, and convenient for multiple modes of travel, including vehicles, bus, bicycles, and pedestrians.

The Boulevard street type includes on-street parallel parking, street trees, plantings, a planted median, sidewalks, lighting, benches, and other street furniture.





Parking

OVERVIEW

A comprehensive strategy to deal with parking is one of the most important aspects of creating a successful, walkable, TOD environment. The majority of parking in the Station District will be provided on surface lots and structures with some on-street parking.

Park and ride, visitor, and ADA parking shall be prioritized and located within the shortest distance possible. Landscaping should be used to screen parking from the street where possible.

The parking plan provides about 3,400 parking stalls, which give parking flexibility for future businesses.

INTENT

To arrange parking in a way that promotes walkability, while still providing convenient and accessible parking.

DESIGN GUIDELINES

- Dedicated parking structures will provide parking for park and ride purposes.
- Adequate bike parking should be provided for each building in the neighborhood.

- Parking structures facing Depot Street and 1450 South may have active uses on the ground floor.
- All streets are to include on-street parking where possible.
- Shared parking strategies are encouraged.
- Office parking shall have a minimum of 5% of parking stalls to be Electric Vehicle (EV) hook up ready and at least four stalls per 150,000 SF built.
- EV charging stations to be 220/240 volt minimum (Level 2).

PARKING STRUCTURE PRECEDENTS

- 1 Parking Structure is wrapped by buildings to hide the parking structure from the street and public open spaces.
- 2 Retail uses on the ground level of parking structure activates the street.
- 3 Decorative facade treatment of parking structure adds visual interest to the street.



CONCEPT PLAN PARKING

The plan to the right illustrates a number of parking configurations and strategies that could be used to provide parking in the Station District. Some of these include, but are not limited to:

On-Site Residential Parking:

Surface-level parking located directly adjacent to planned residential uses.

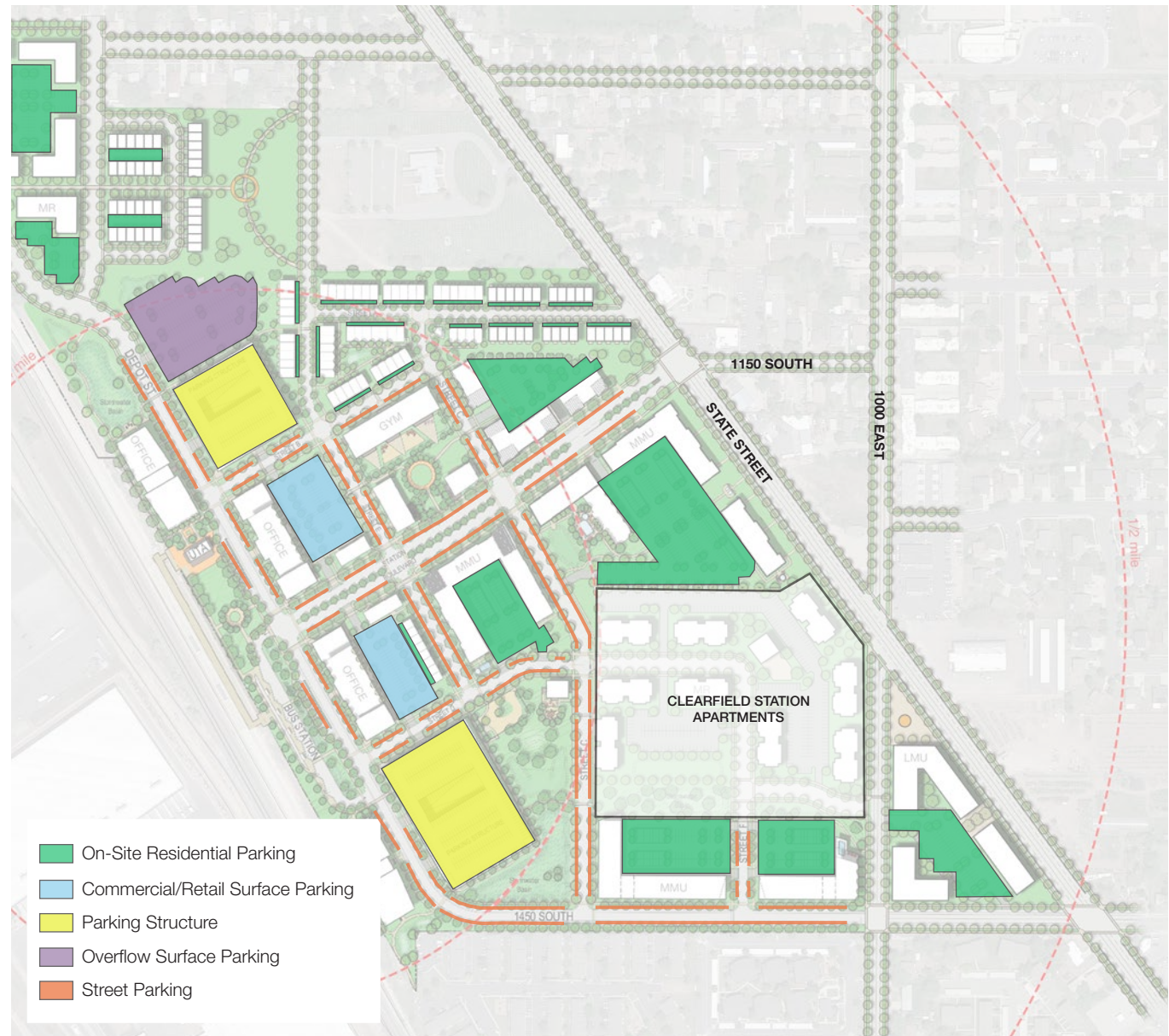
Commercial/Retail Surface

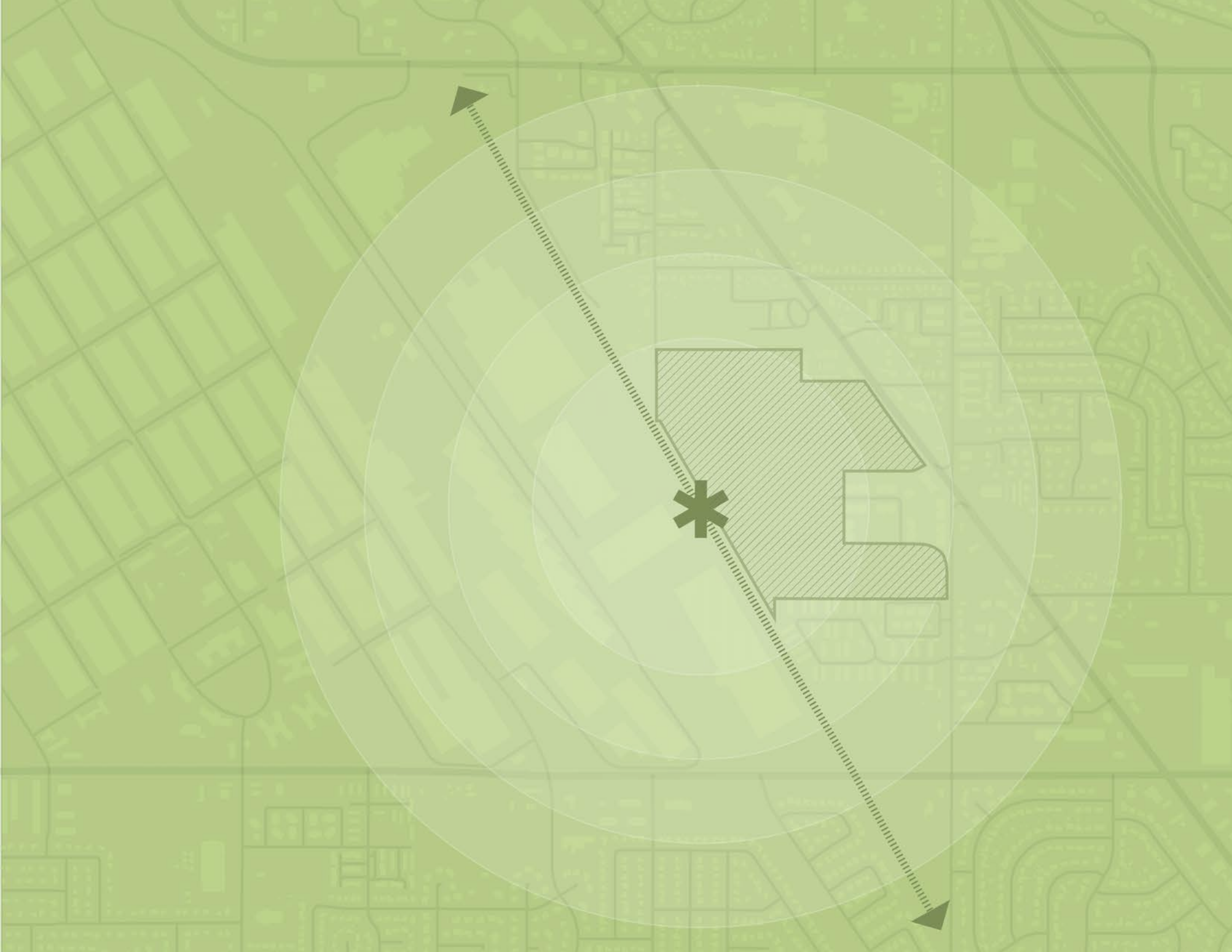
Parking: Surface-level parking located directly adjacent to planned commercial uses.

Parking Structure: Located either above ground with active uses on the ground floor or underground.

Overflow Surface Parking: Surface-level parking utilized when the existing parking supply does not meet demand.

Street Parking: All streets are to include on-street parking where possible.







08

STRATEGIC RECOMMENDATIONS

Implementing the Plan

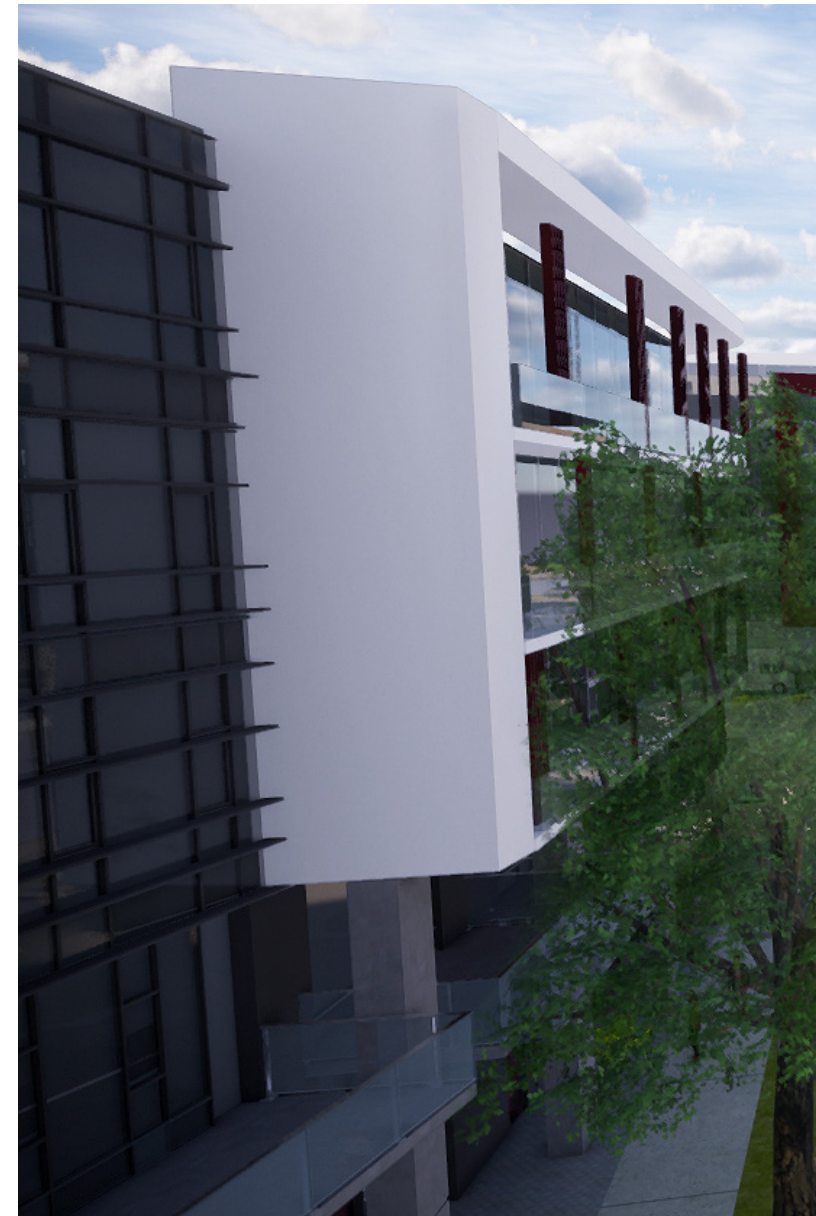
For the vision and objectives laid out in this plan to be realized, it will likely be the result of a long-term process, where residents, City Staff, UTA Staff, and elected officials have championed the vision and ensured the development of the area that they want to see. This plan presents the vision and illustrative plan for the Clearfield Station Area, but for the type of development this plan envisions to be built, more steps will need to be completed.

The strategic recommendations outline the next steps for the Station Area. They are intended to provide the action items that the City, UTA, or other stakeholders must complete to be ready for implementation. Not all steps must be completed before development on the area can begin, but each step will need to eventually be completed to ensure the area reaches its potential as outlined in this plan.

The strategic recommendations are broken down into four categories:

- Policy Updates + Plan Amendments
- Economic Development
- Transportation
- Physical Improvements

The image on the following page illustrates how the project area might look at buildout.





Policy Updates + Plan Amendments

- ☐ Ensure consistency between the Clearfield Station Area Plan and other planning and regulating documents
 - Evaluate the Clearfield General Plan and the city's streets and trails plans to ensure consistency with this plan. Update plans as appropriate.
- ☐ Consider updating the City's Future Land Use Map and Zoning Ordinances to reflect the proposed land uses indicated within this plan (See map on page 37).
- ☐ Investigate creating a form-based code for the Clearfield Station District (see Districts Map on page 33), basing the requirements on the architectural design guidelines established in this plan.
- ☐ Consider updating city transportation policies to include street and transportation related design guidelines as outlined in this plan.
- ☐ Investigate and implement strategies to incentivize or require affordable housing within the Station Area, coordinating closely with the City's Moderate Income Housing Plan.
- ☐ Consider the development of a brand for the area
 - Establish a unique brand for the Station Area that will increase visibility and help the area become more attractive to developers, future residents, and employers/employees.
- ☐ Consider developing refined site plans for undeveloped properties outside of the existing MDP.
 - Site plans should describe the physical location of buildings, accesses, and parking within the proposed developments.

Economic Development

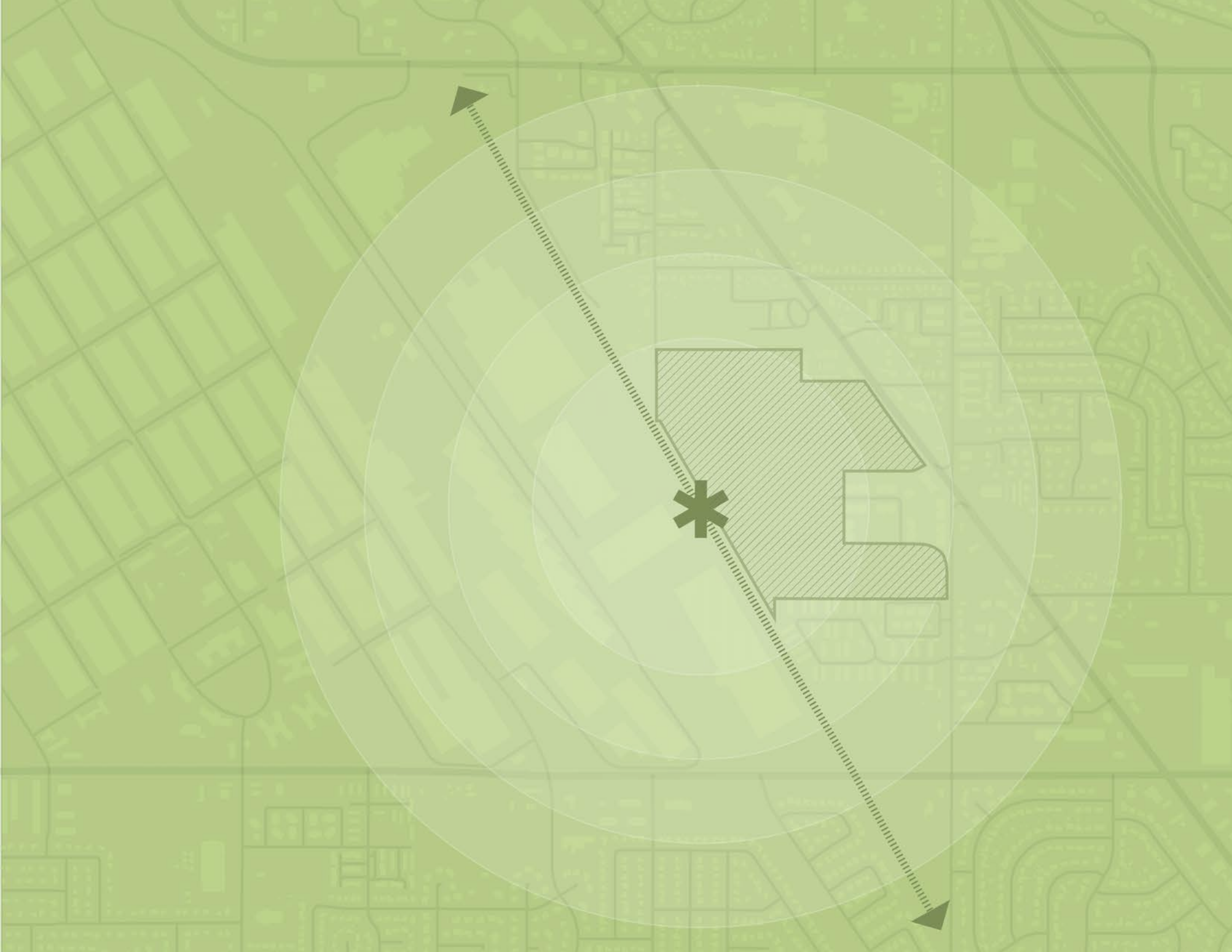
- ☐ Consider formation of a Transportation Reinvestment Zone (TRZ)
 - A TRZ is similar to the existing CDA, in that it is a program that utilizes tax increment financing. However, the advantage to the TRZ is that the majority of the funds can be used for transportation improvements. It also removes the requirement of setting aside ten percent of the increment for affordable housing.
- ☐ Reevaluate retail buying power
 - As new residential product is introduced into the area, the City should consistently reevaluate the retail buying power potential. That actual, or even planned growth, can be translated into specific buying power in terms of real dollars. That information needs to be used in attracting new retailers to the overall area.
- ☐ Reevaluate the fiscal impacts of use types
 - The City should regularly reevaluate the fiscal impacts of use types to reconsider their municipal cost models and make changes as market conditions affect different real estate Sectors.
- ☐ Consider soliciting development partners and commercial tenants
 - UTA and the City should consider actively solicit development partners and commercial tenants who share the vision for the Clearfield Station Area.

Transportation

- ☐ Further Study impacts of the addition of Station Boulevard signal and changes to 1000 East signal
 - The proposed and potential signal changes will impact traffic patterns and delays, but further study is needed to know the full impacts of these intersection changes.
- ☐ Investigate the improvement of the pedestrian crossing at 1000 South and State Street
 - If possible, relocate the existing HAWK signal from its current position north of 1000 South on State street to the crossing between 1000 South and Campbell Heights across State Street.
- ☐ Complete an Operational Analysis and Circulation Plan
 - Due to the high-density development of the Clearfield Station Area, an internal operational analysis should be completed to determine the type of traffic control needed within the development (two-way stop control, four-way stop control, free, roundabouts, traffic circles, etc.).
- ☐ Develop a parking strategy
 - Develop a strategy for parking that takes into account opportunities for shared parking, phasing, and other innovative strategies to provide parking for employees, residents, and visitors.
- ☐ Continue to work with UDOT to improve safety and connectivity across State Street.

Physical Improvements

- ☐ Work with UTA, project stakeholders, and potential developers to implement the Clearfield Station Master Development Plan (MDP).
- ☐ When feasible, implement the active transportation and trail infrastructure proposed in this plan.
 - Safely and efficiently connect the D&RGW Trail to the Clearfield Station.
 - Extend trails and make pedestrian connections to other areas of the city.
- ☐ Consider the design and construction of the proposed public parks indicated in this plan (see page 79).
- ☐ Contemplate enhancing streetscapes within the Station Area through consistent street trees, improved landscaping, street furnishings, and lighting.
- ☐ Consider allocating of tax increment to construct parking structures near station platform to provide park & ride parking for transit users.





09

Appendix A

*Existing Conditions Report:
Land Use & Transportation*

CLEARFIELD CONNECTED

EXISTING CONDITIONS ANALYSIS

BACKGROUND

The Clearfield FrontRunner Station is a place of connections and linkages, where people arrive and depart on their way to destinations near and far. Located in the economic heart of Davis County, the station provides access to many workplace and residential destinations, while facilitating access to countless destinations along the Wasatch Front.

The Clearfield Station is a vital component of the FrontRunner system and Clearfield City's overall infrastructure. Encompassing approximately sixty acres of vacant land, the Clearfield FrontRunner Station TOD site represents a significant opportunity to meet the transit and placemaking needs of Clearfield City and its residents, as well as those of UTA and transit riders throughout the region.

The purpose of the updated Clearfield FrontRunner Station Area Plan (2023) is to establish a clear vision, goals, and urban design principles that will govern development of the Clearfield Station site over the next 10 years and beyond.

CONTEXT

Planning Context

The *Clearfield Connected Station Area Plan (2023)* is an update to the recently adopted *Clearfield Connected Station Area Plan (2019)*, which established a clear vision for the station area. This vision was further refined in 2021 in the *Clearfield Station Master Development Plan (MDP)*, which builds upon the area plan, providing further detail for development. Additionally, in 2021, Clearfield City adopted the *North Davis Active Transportation Plan*, which identifies important pedestrian and cyclist infrastructure improvements around the station area.

Figure 1: Clearfield Station Illustrative Master Plan from the MDP



Recent changes in Utah State planning codes require the *Clearfield Connected Station Area Plan (2019)* be amended to address a wider service area and to incorporate options for affordable housing. This updated plan will embrace previous efforts, translating the energy underpinning those plans into an updated and comprehensive version that also addresses the new elements required by state code.

The updated *Clearfield Connected Station Area Plan* will incorporate the following additions and modifications:

- Assessment of prior studies and the existing conditions of the study area, focusing on the expanded station area “zone of influence,” changing development patterns, and recent demographic and socio-economic changes.
- Incorporation of statewide objectives for moderate-income housing, environmental conditions, and transportation choices and access.
- Updated design guidelines that better align with the MDP.
- Assessment of the market potential of the station area and the synergies of commercial and multi-family residential uses, as part of a mixed-use transit district.
- Assessment of the access to and from the station area for vehicles, transit, and active transportation modes, including pedestrians and bicyclists.

Historical Context

Clearfield was settled in 1877 as an agricultural community. The city’s structure began to change in the 1940’s, when major defense facilities such as Hill Air Force Base and the Clearfield Naval Supply Depot were built within and adjacent to the city. The air force base quickly became a significant employer in the region and has grown to become one of the largest employers in the state.

The Clearfield Naval Supply Depot was constructed adjacent to the railways that line the west edge of the FrontRunner station today. This depot was also a major employer until it was decommissioned in 1962. The depot's remnant facilities eventually became the Freeport Center, which is now a major manufacturing, warehousing, and distribution destination.

The Clearfield Station TOD site has historically been used for light industrial uses. More recently a portion of the site developed into a park-and-ride lot for transit riders.

Demographic Context

Utah is one of the fastest growing states in the country and is expected to grow another 50% by 2040. This growth has led to a lack of housing, which has resulted in skyrocketing housing costs and unprecedented demand for affordable housing in recent years. These conditions have created demand for a wider range of housing options throughout the region, with a particular focus on more compact and efficient multi-family development models. Areas in proximity to transit such as the Clearfield Station site are particularly well-suited for multi-family housing as part of a high-quality, mixed-use development.

Physical Context

Clearfield is located in Davis County, approximately 28 miles north of Salt Lake City, situated between the Great Salt Lake to the west and the Wasatch Mountains to the east. The Clearfield FrontRunner Station is located across the railroad tracks from the Freeport Center near the Clearfield-Layton border. As shown in Figures 2 and 3, the station area is close to Hill Air Force Base (northeast), Davis Medical Center (southeast), Downtown Clearfield (north), the planned Layton City Town Center (south), and a education/recreation district composed of three public schools and a park (northeast).

Figure 2: Regional Context Map

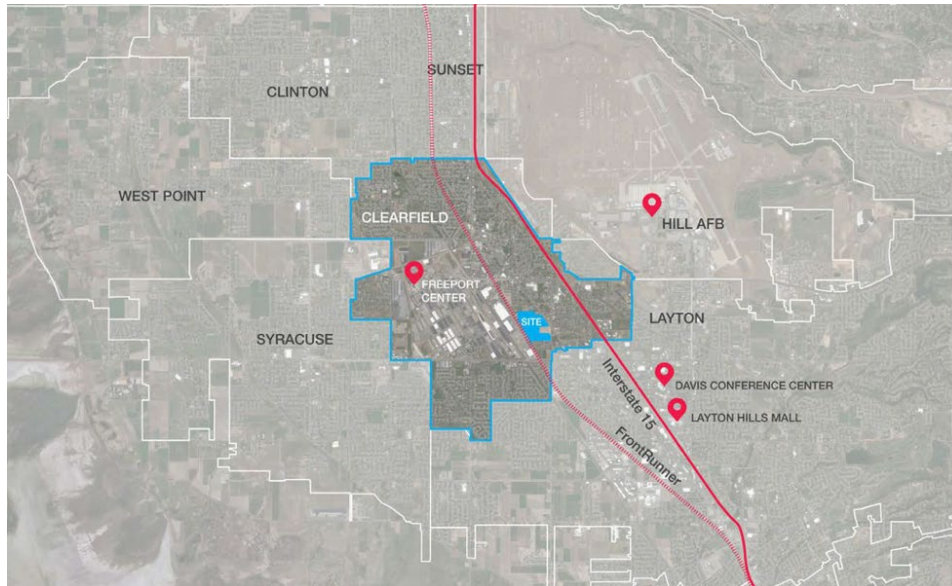
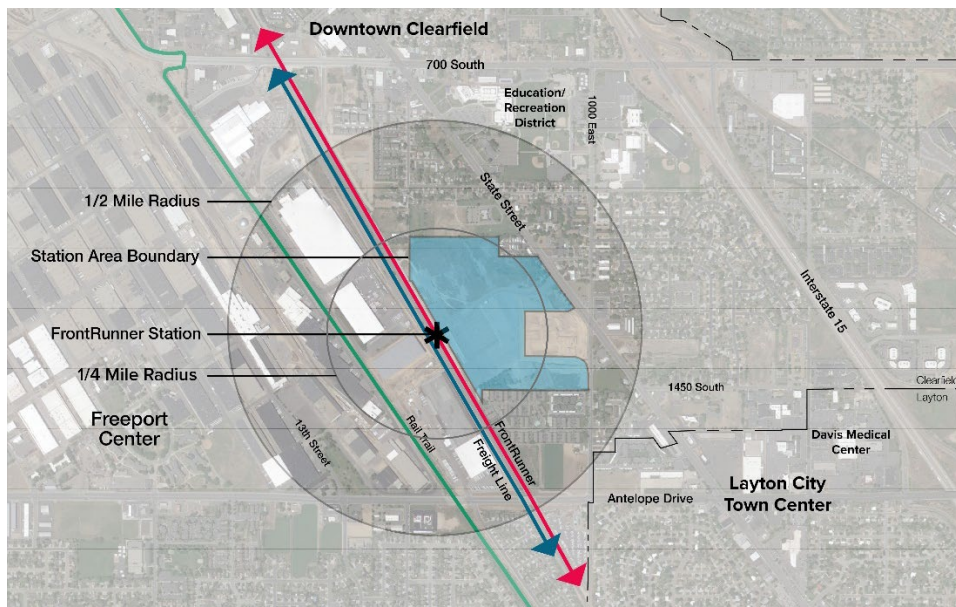


Figure 3: Local Context & Zone of Influence Map



Station Zone of Influence

The State of Utah requires the Station Area Plan to include a half-mile radius “zone of influence” when assessing opportunities and constraints emanating from the station. As shown in Figure 3, this area includes the master-planned Frontrunner TOD property; a large portion of the Freeport Center to the west; commercial properties along State Street to the east; and existing residential neighborhoods to the north, south, and east.

Clearfield Station Site

The boundary for the Clearfield Station Area Plan is shown in Figure 3. The TOD site encompasses 60 acres of land, most of which is undeveloped, and represents the largest area of UTA-owned-vacant-land adjacent to a FrontRunner or TRAX transit station in the entire UTA system. The station is situated between the railroad/FrontRunner tracks to the west and State Street to the east. Currently, the site is used as a park-and-ride lot for transit riders but is otherwise vacant. Since the last station plan was adopted in 2019, significant development activity has taken place in the station area, primarily the road and parking lot design and construction within the site.

Nine apartment buildings consisting of 216 units were built on ten acres on the southwest corner of State Street and 1000 East. This project was incorporated into the station design of the 2019 Station Area Plan, which identified connections between the station site and internal roadway networks. At buildout, the Clearfield Station TOD is envisioned to be a cohesive neighborhood that includes the existing 10-acre apartment site.

LAND USE

A thorough site documentation and analysis process was conducted to ensure the planning and design concepts that emerge are aligned with the opportunities and constraints that currently exist. As described and illustrated below, key land use conditions were reviewed and investigated as part of understanding the structure and relationships between land uses in the study area.

Figure 4 shows the general land-uses of parcels within ½ miles of the station. Current land uses surrounding the site are primarily single family and medium density multifamily residential housing. East of the site is the State Street commercial corridor. West of the site is the Freeport Center that consists of industrial uses, including processing, assembling, manufacturing and warehouse storage. As indicated in Table 1, the total area included within the half-mile zone of influence encompasses 899 acres.

Figure 4: Existing Land Use Map

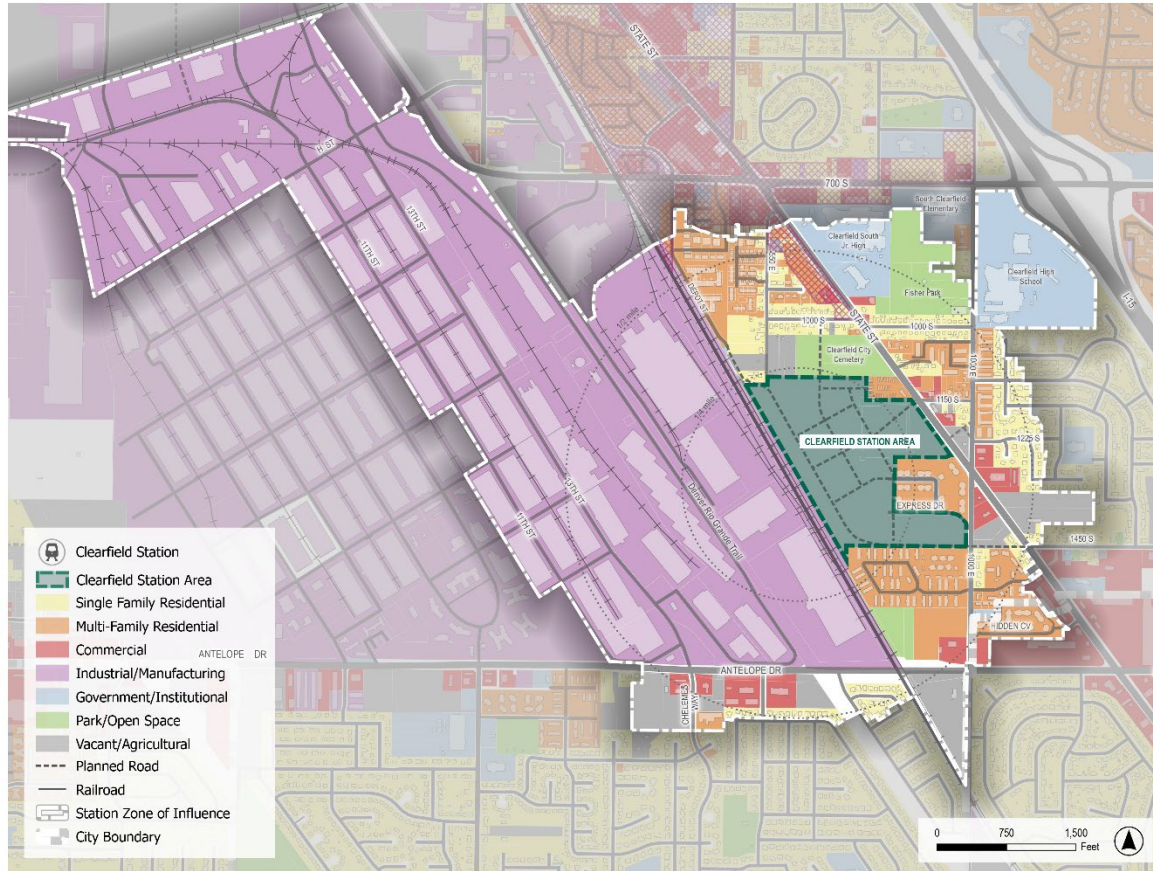


Table 1: Existing Land Use

Name	Acres	Percent
Clearfield Station Area Site	56	6%
Single-Family Residential	41	5%
Multi-Family Residential	88	10%
Commercial	19	2%
Industrial/Manufacturing	534	59%
Government/Institutional	49	5%
Park/Open Space	30	3%
Vacant/Agriculture	44	5%
Roads & Utilities	38	4%
Total	899	

Environmental Conditions

As illustrated in Figure 5, there are no negative environmental conditions known on the site, which provides optimal conditions for development and good access to existing utilities. The primary environmental conditions that impact the site are noise generated by jets taking off from Hill Air Force Base, in addition to noise, vibrations, and emissions resulting from rail lines and major arterial roads adjacent to the site.

The typical slope across the site is approximately 2% which is generally flat and provides adequate surface drainage. An existing detention basin is located on the south end of the site and at present provides adequate storage for surface drainage of the site. Figure 6 indicates that the site is significantly impacted by traffic. High traffic volumes can be beneficial for regional connection and visibility for the station, but can also hinder local access, particularly for pedestrians and cyclists.

Figure 5: Station Site Environmental Conditions

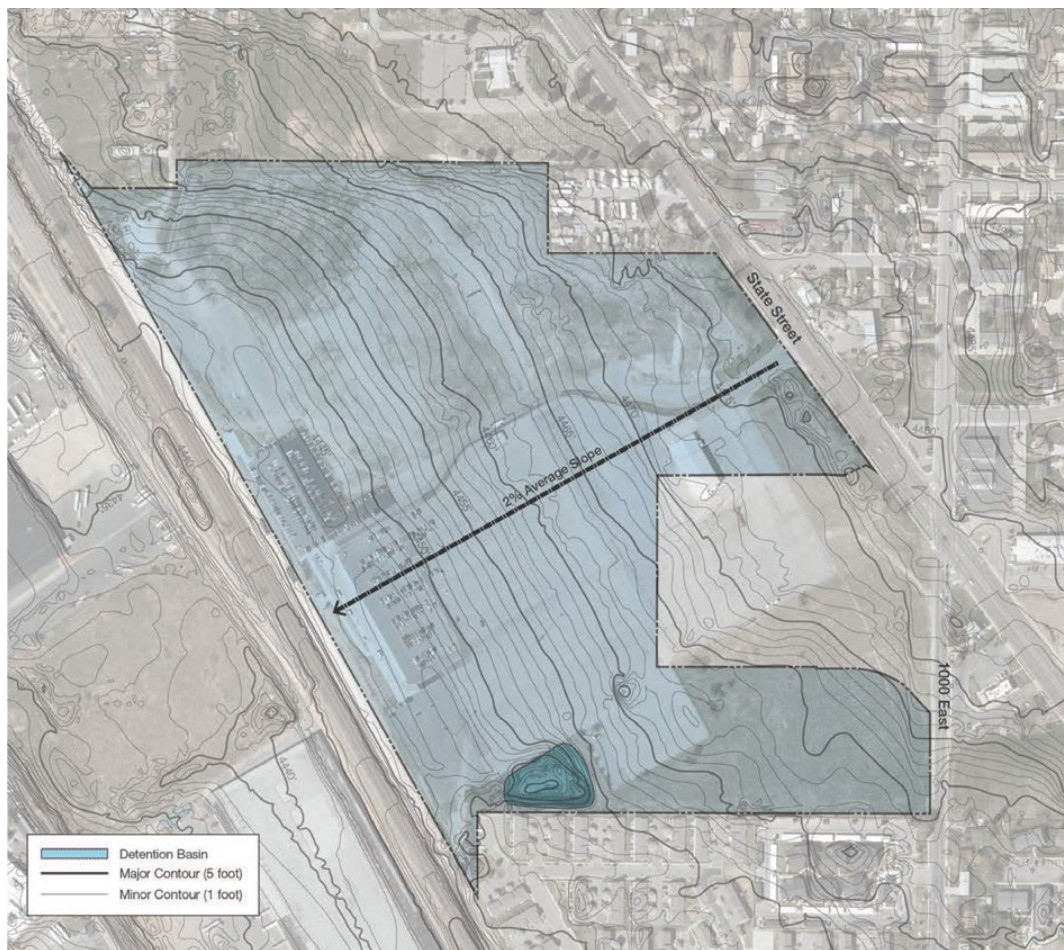
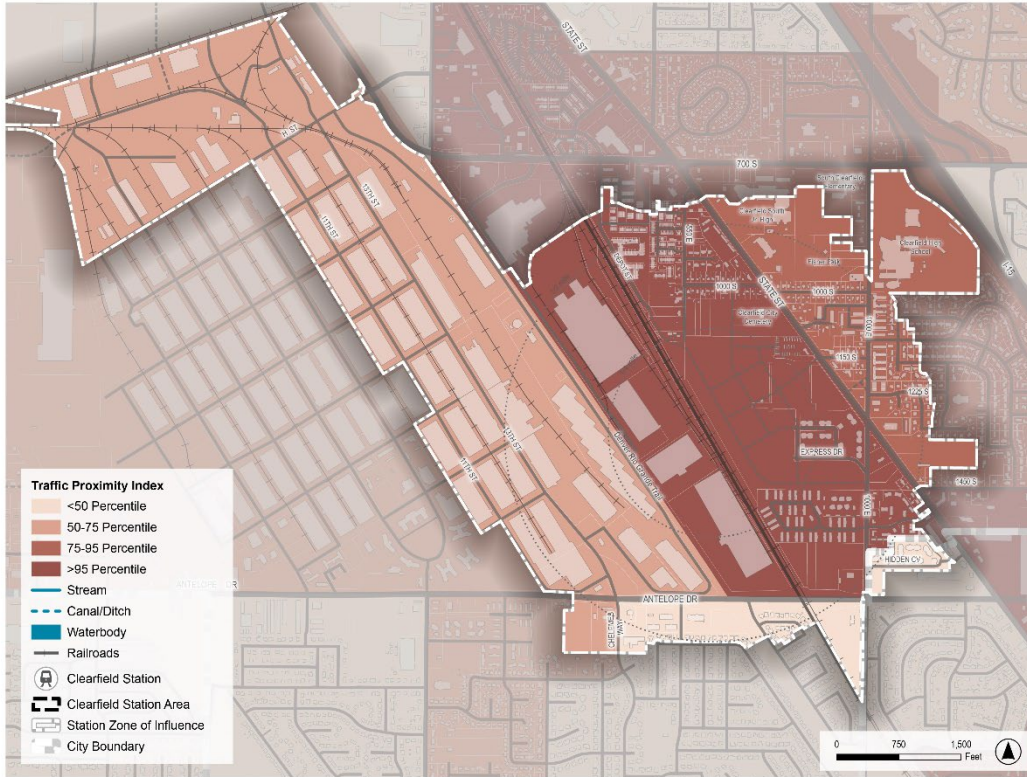


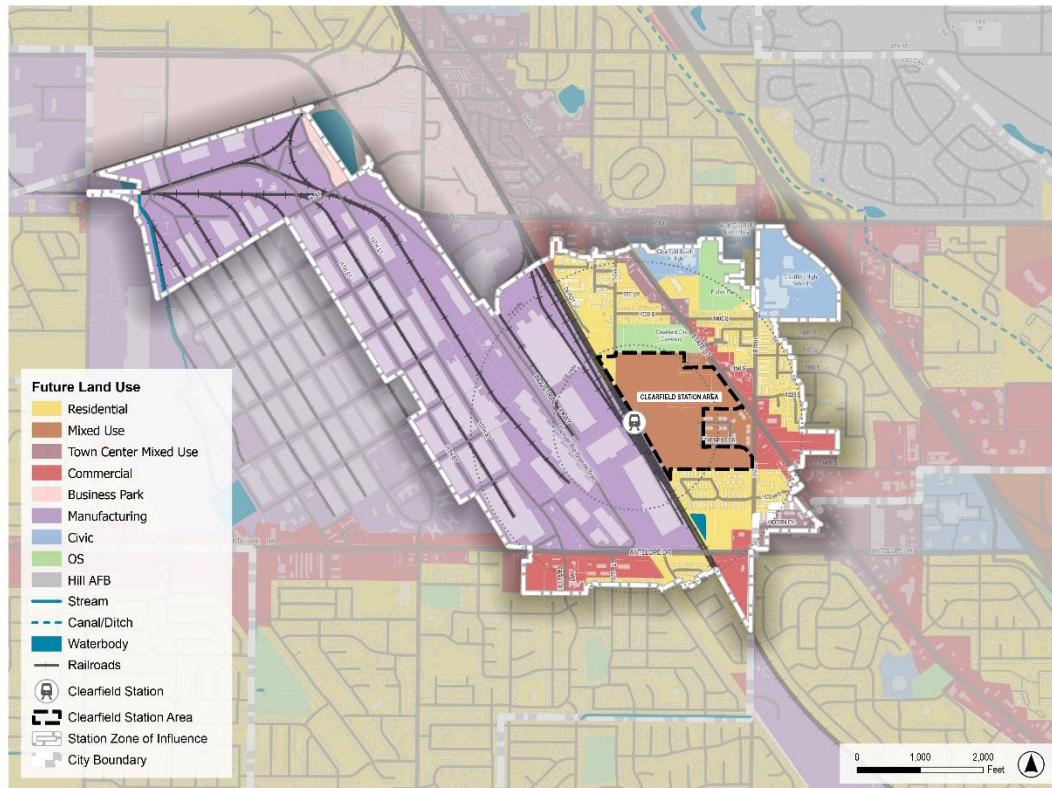
Figure 6: Utah Traffic Proximity Index



Future Land Use

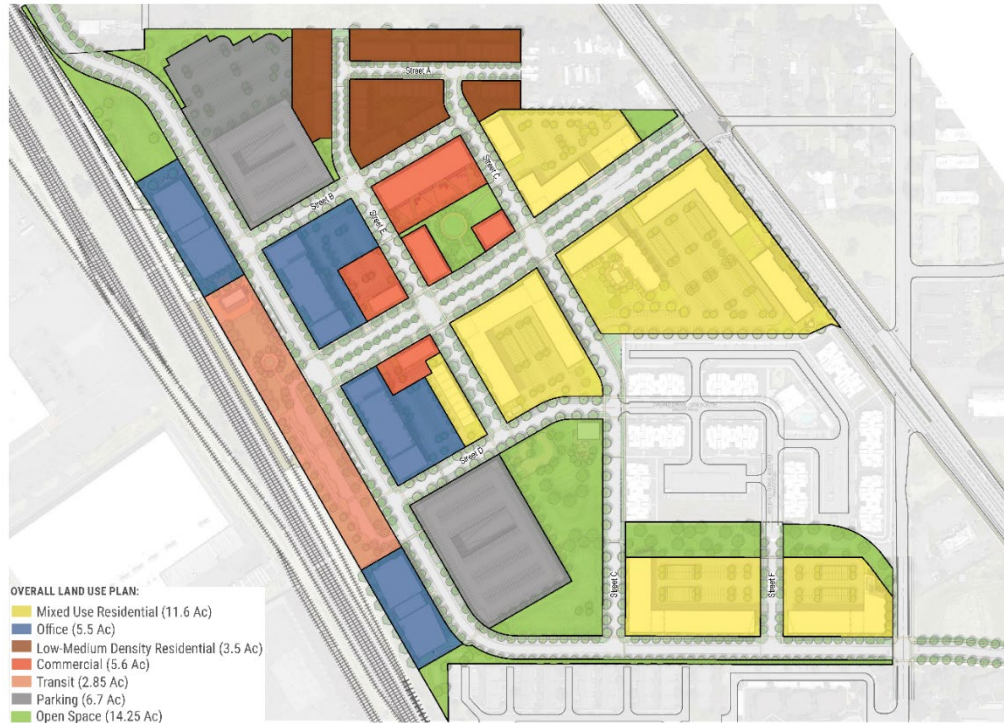
The *Clearfield City General Plan (2017)* identifies future land uses for the station area in a simple and straightforward manner (see Figure 7). The Frontrunner station site is designated as a mixed-use site, with residential uses to the north and south, industrial use to the west, and commercial use dispersed along State Street to the east. The plan also indicates a connection between the station area and downtown Clearfield, as part of an extension of mixed-use development along State Street ending at 1000 South. Since most existing uses on State Street between 700 South and 1000 South are unlikely to change from their civic and residential uses in the short-term, the station area is likely to remain somewhat detached from downtown Clearfield for the next ten years and beyond.

Figure 7: Future Land Use Map



The *Clearfield Station Master Development Plan (2021)* proposes a mix of land uses and new street connections within the Frontrunner Station TOD site (see Figure 8). Proposed land uses include mixed-use residential and retail along Station Boulevard, office development concentrated along Depot Street, additional mixed-use residential along 1450 South, and townhouses along the northern property line – all with accompanying parking areas and a network of connected open spaces.

Figure 8: Clearfield Station Site Future Land Use



TRANSPORTATION

Transit

Clearfield Station is located just west of State Street and north of Antelope Drive. The most recent ridership data from UTA (March 2023) show 434 average daily boardings and 375 average daily alightings. This is similar ridership to that of nearby Layton, Farmington, and Woods Cross stations, and about half that of Ogden Station. Figure 9 shows the transit network within the station area and stop-level ridership.

Clearfield Station is served by four local bus routes:

- **470 | Ogden-Salt Lake Intercity | 30-minute peak service:** Connects downtown Salt Lake City to Ogden Station with a transfer stop at Clearfield Station. The Clearfield Station stop for this route has 86 daily boardings and 86 daily alightings.
- **626 | West Roy – Clearfield Station | 30-minute peak service:** Connects West Roy to Clearfield Station through Syracuse. The Clearfield Station stop for this route has 38 daily boardings and 39 daily alightings.

- **627 | WSU Davis – DTC | 30-minute peak service:** Connects Davis Technical College to Clearfield Station with a transfer stop at Weber State University Davis Campus. The Clearfield Station stop for this route has 48 daily boardings and 39 daily alightings.
- **640 | Layton Hills Mall – WSU Ogden Campus | 30-minute peak service:** Connects Layton Hills Mall to Weber State University with a transfer stop at Clearfield Station. The Clearfield Station stop for this route has 58 daily boardings and 48 daily alightings.

According to UTA's 2019 On-board Survey the primary mode of access/egress to Clearfield Station is walking, following by driving alone and being picked up or dropped off. Combined, the vehicle-oriented modes comprise a majority share of access/egress modes at 61% and 57% respectively. Table 2 shows all modes of access and egress to the station.

Figure 9: Clearfield Station Transit

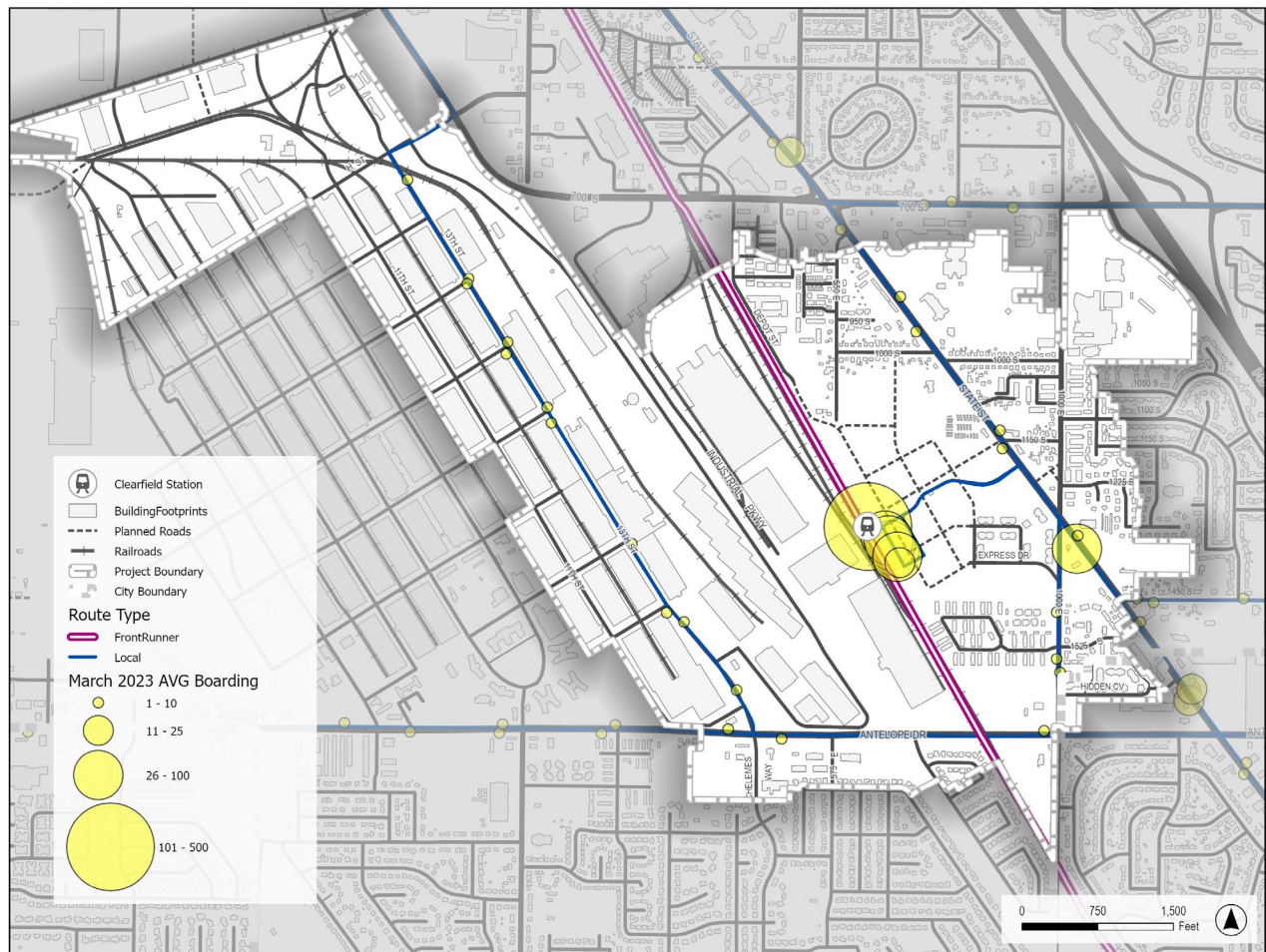


Table 2: Clearfield Station Mode of Access/Egress

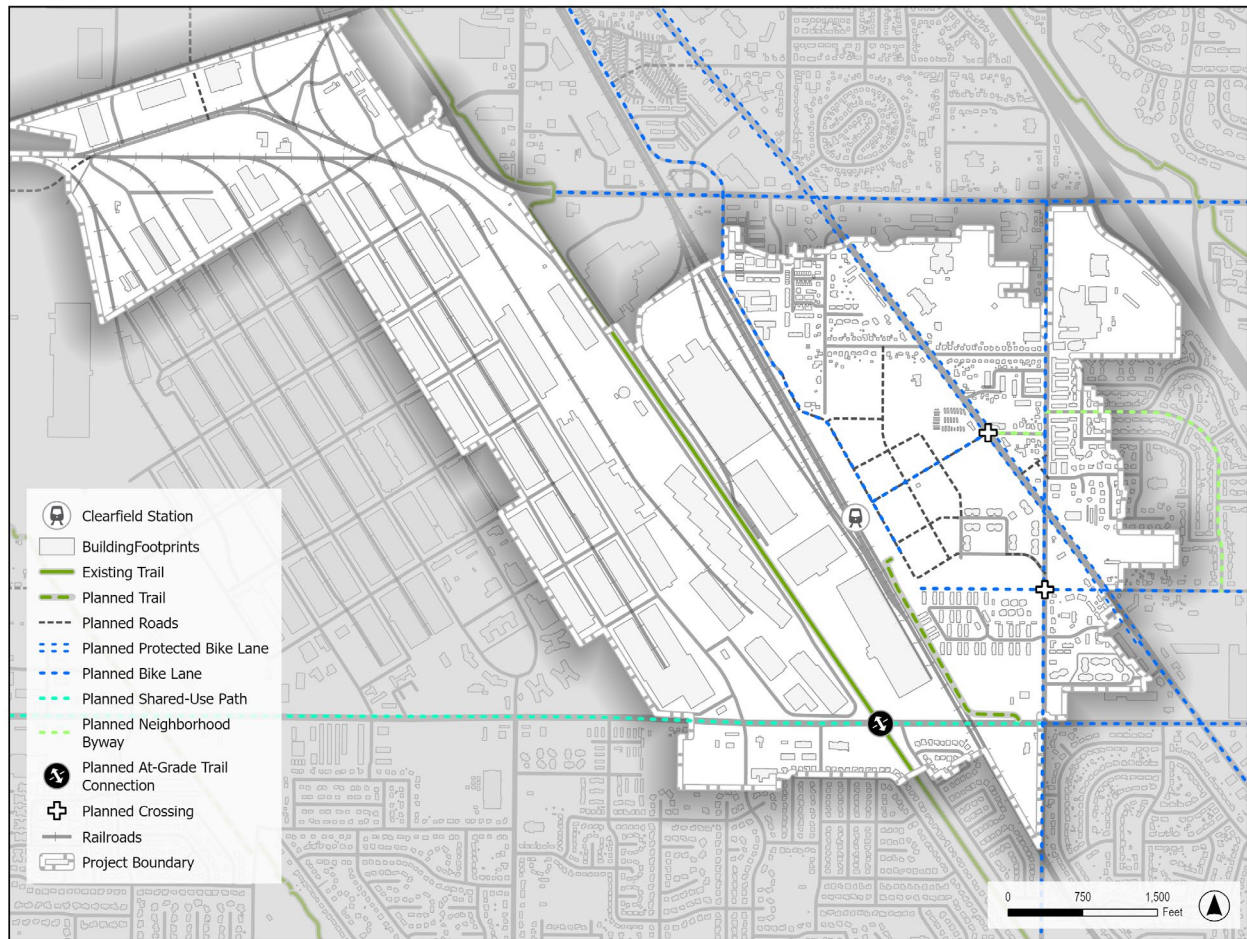
MODE	ACCESS	EGRESS
WALK	35%	41%
DROVE ALONE	34%	31%
PICKED UP/DROPPED OFF BY SOMEONE	22%	21%
DROVE / RIDE WITH OTHERS	5%	3%
PERSONAL BIKE	3%	2%
SKATEBOARD / LONGBOARD	1%	0%
BIKE SHARING (E.G. GREEN BIKE)	1%	0%
SHUTTLE	0%	1%
UBER, LYFT, ETC.	0%	1%

Active Transportation

Existing Facilities

There is only one dedicated active transportation facility within the station area, the Denver and Rio Grande Western Rail Trail. This paved facility is part of the Golden Spoke Route and US Bike Route 77, with connectivity north to Ogden and south all the way to Provo. There are several planned active transportation line and point projects in the area, identified from the North Davis Active Transportation Plan and the 2023 WRFC RTP. Bike lanes are planned for Depot Street, 1000 East, 1450 South, 700 South, on Antelope Drive west of 1000 East, and the future road to the Clearfield FrontRunner Station. Additional planned line projects include a protected bike lane on State Street, a trail connection from the FrontRunner Station south to Antelope Drive, a shared-use path on Antelope Drive west of 1000 East, and neighborhood byways on 1150/1100 South. Planned point projects include at-grade pedestrian/bike crossings at 1150 South State Street and at 1000 East and Antelope Drive, and a planned at-grade trail connection between the Denver and Rio Grande Western Rail Trail and the planned shared-use path on Antelope Drive.

Figure 10: Active Transportation Facilities



Activity

Activity data is derived from self-report trips recorded on the fitness platform Strava. This app is popular with recreational and competitive bicyclists, hikers and runners to track their training progress. Although this group of users tends to be comfortable riding on busier roadways than more casual users, their presence can indicate the frequency of use of certain routes.

Figure 11 shows the recorded run, walk, and hike trips in 2022. The most popular place to log these activities within the study area is the Denver and Rio Grande Western Rail Trail, with close to 3,000 recorded activities. Antelope Drive is also a relatively popular corridor. Few people record these types of trips while accessing the FrontRunner station.

Figure 11: Pedestrian Activity 2022

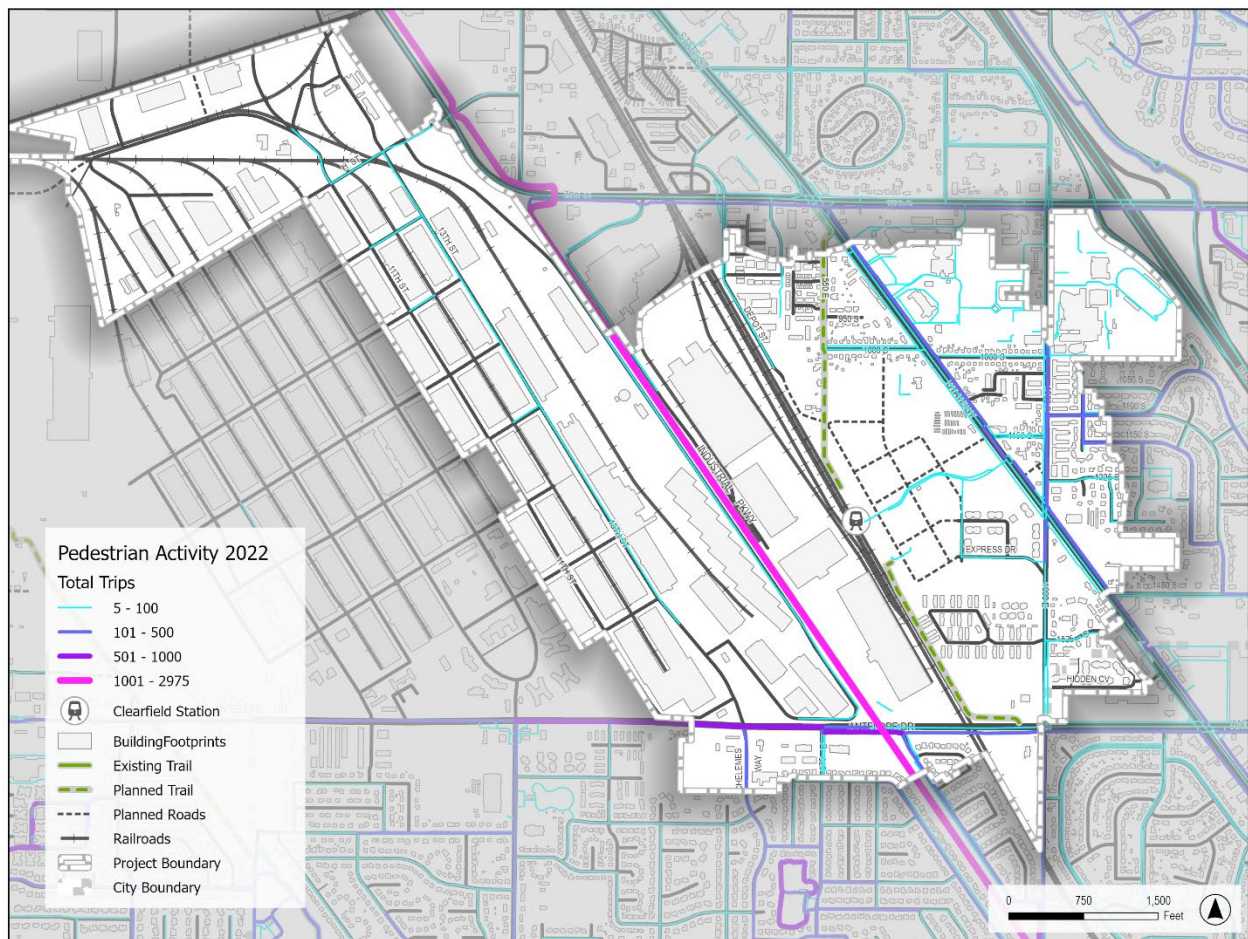
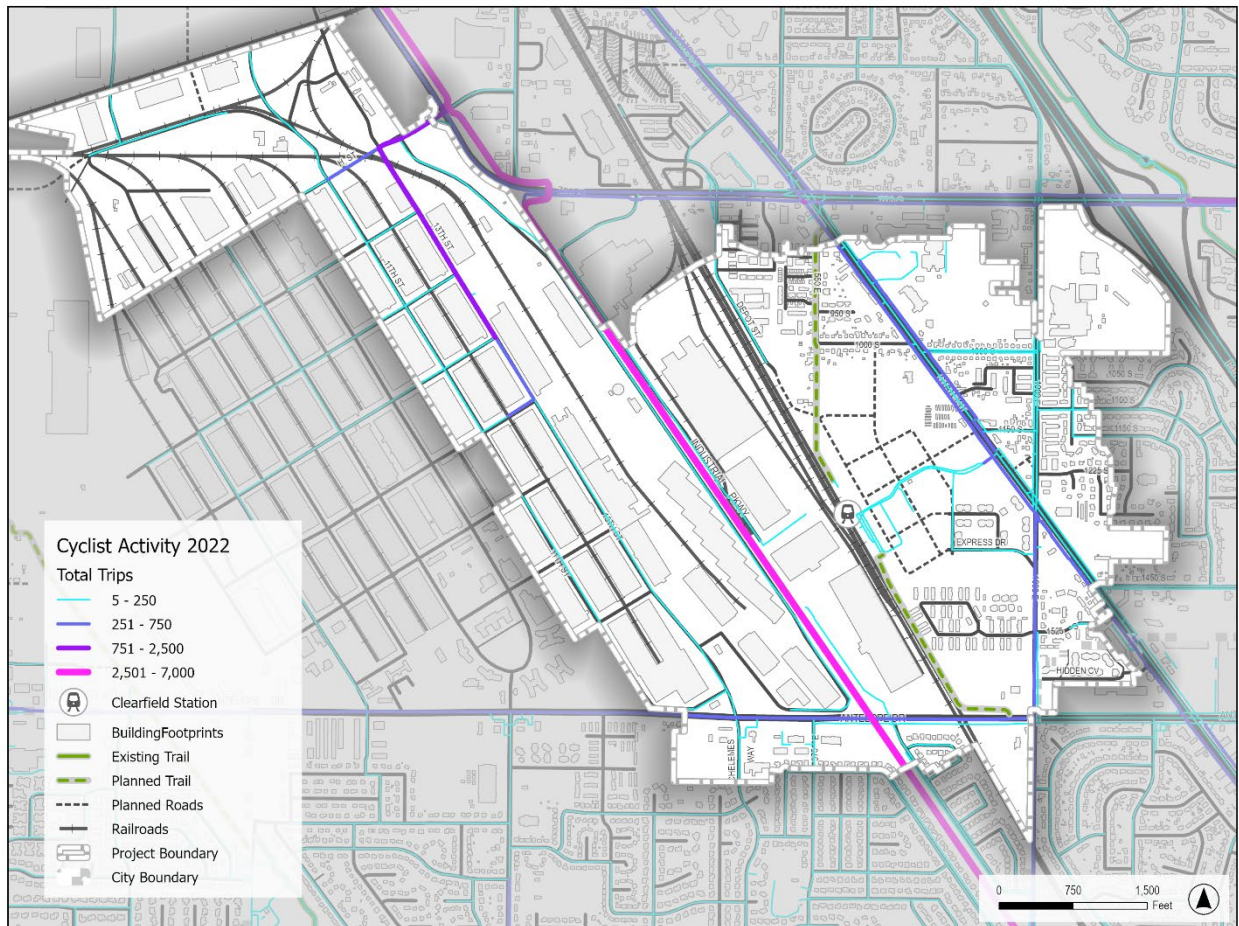


Figure 12 shows the bicycle trips recorded within the city during 2022. These trips largely follow the same pattern found with the pedestrian activity, but with greater magnitude. Here the Denver and Rio Grande Rail Trail has close to 7,000 recorded activities. 13th Street also shows relatively high activity with access from the north on H Street. Few people record these types of Trips while accessing the FrontRunner station.

Figure 12: Cyclist Activity 2022



Planned Improvements

The *North Davis Active Transportation Plan* identifies planned improvements for major corridors, including State Street, Antelope Drive, and 700 S. These improvements include a multi-use path and buffered bike lanes along Antelope Drive (Figure 13 & Figure 14), protected bike lanes on State Street (Figure 15), and enhanced street crossings, including at State Street and 1150 South (Figure 16).

Figure 13: Proposed Street Cross Section for Antelope Drive (1000 W to 1000 E)

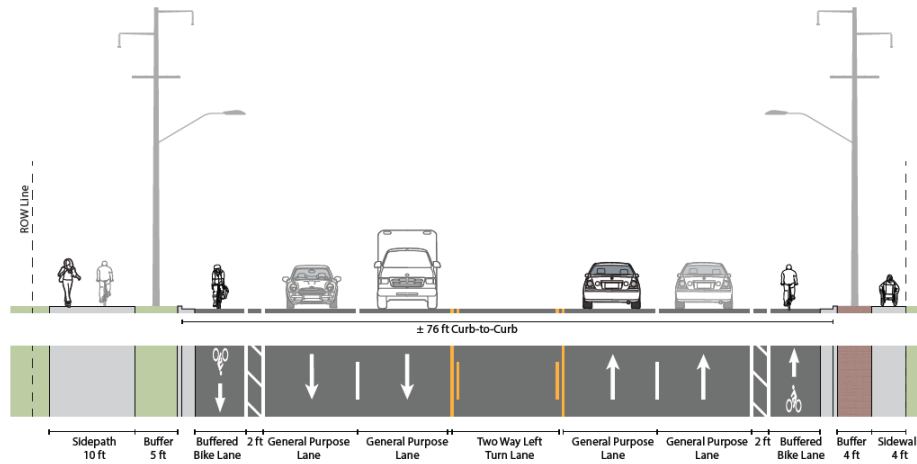


Figure 14: Proposed Antelope Drive & DRGW Trail Connection

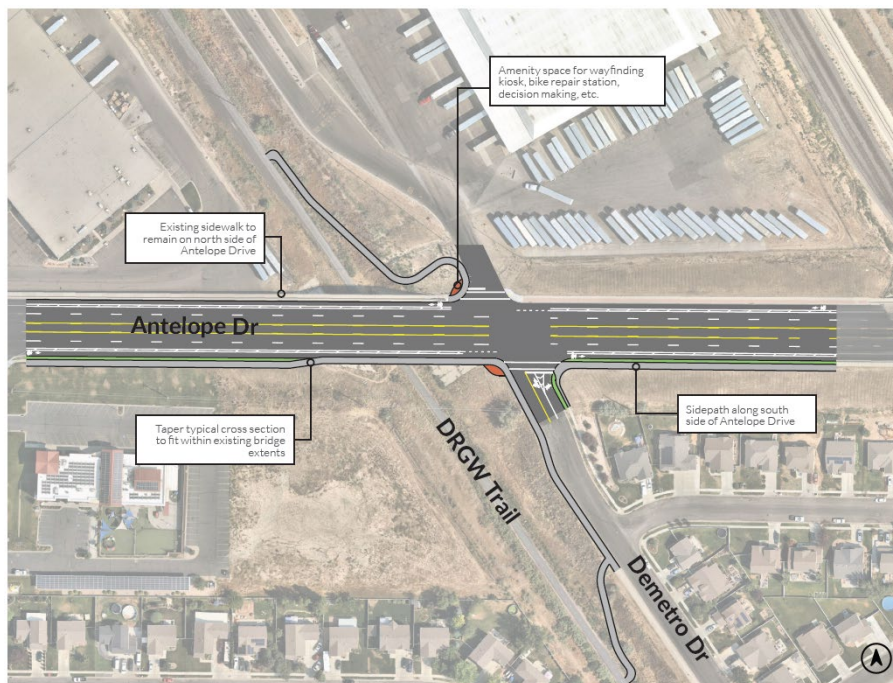


Figure 15: Proposed Street Cross Section for State Street (800 North to 1525 S)

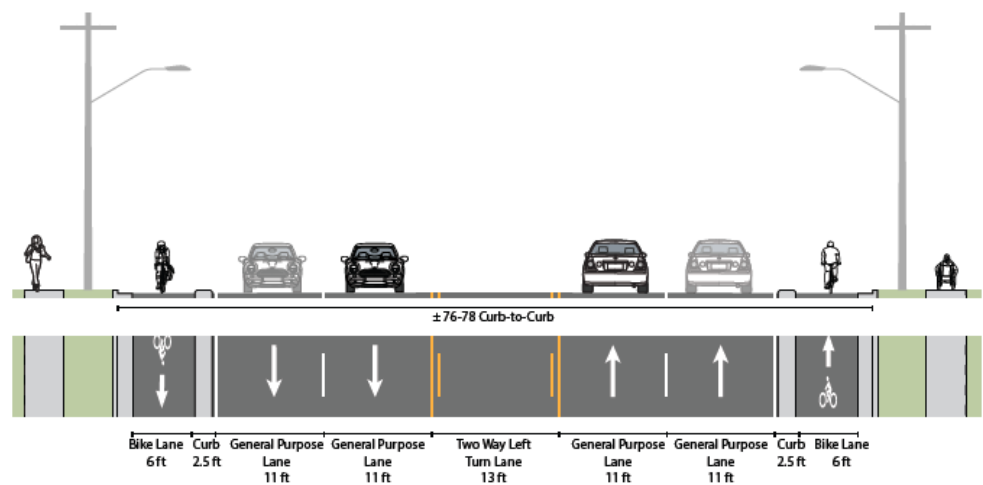
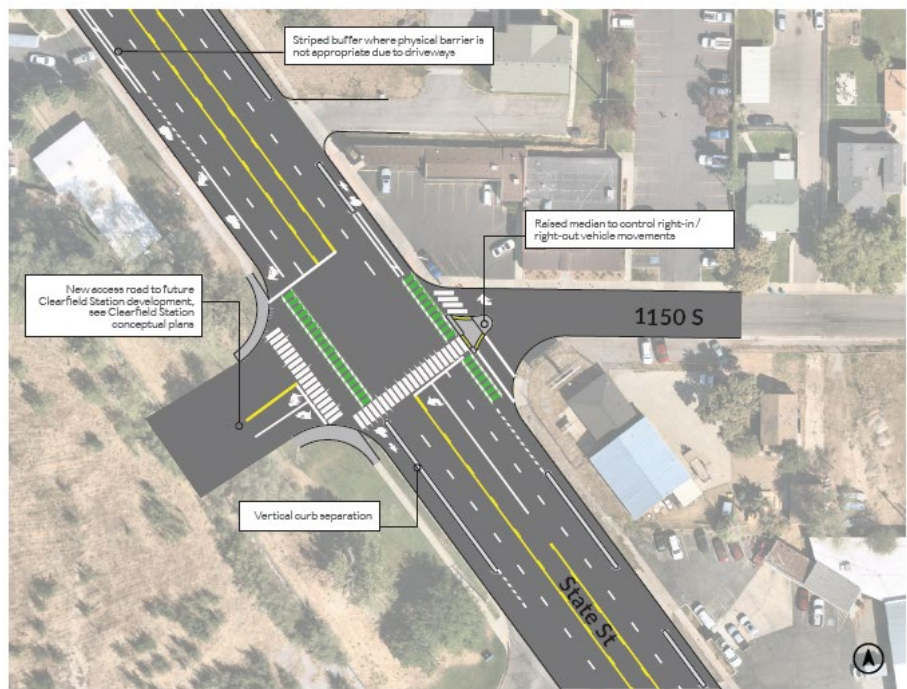


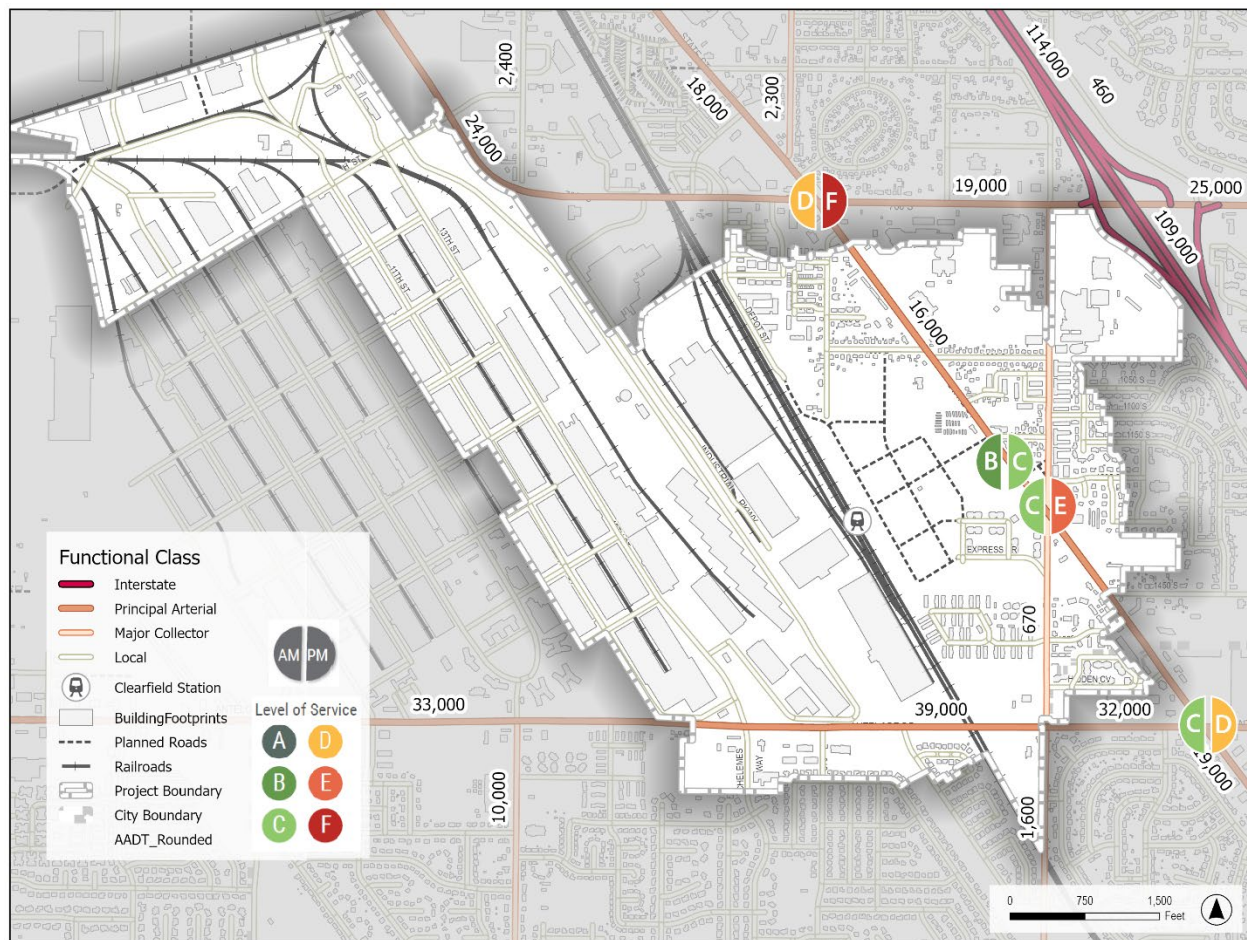
Figure 16: Proposed Intersection Design at State Street and 1150 South



Vehicle Conditions

Figure 17 shows the vehicle classification of roadway in the vicinity of the station area. Additionally, it depicts the 2020 average annual daily traffic volumes (AADT) from UDOT and the intersection level of service (LOS) from the 2018 Clearfield Station Master Plan. State Street is a principal arterial and provides the primary access to the FrontRunner Station. As of 2018, the LOS of the intersection at State Street and the station access was a B in the AM and C in the PM peak periods, indicating a well-functioning intersection. Other intersections along State Street show worse LOS, with the intersection of 700 South having the worst in the area with a PM Peak of F.

Figure 17: Vehicle Conditions



Safety

Figure 18 shows a heat map of all crashes between 2018 and 2022 with fatal and suspected serious injury crashes indicated separately. The largest concentration of all crashes within the station area is at 13th Street and Antelope Drive. Other hot spots occur at 1000 East and Antelope Drive, and 1000 East and State Street. While there are no fatal crashes within the station area, there are a number of suspected serious injury crashes, with four along the State Street corridor. One of these occurs at Station Boulevard, the primary access to the station.

Figure 18: Severe Crashes 2018-2022



Figure 19 shows both pedestrian and cyclist involved crashes between 2018 and 2022. In total there were 22 crashes, 13 pedestrian involved and 9 cyclist involved. The highest concentration of these crashes occurs at 13th Street and Antelope Drive with 3 bicycle involved and 2 pedestrian involved. The Antelope Drive corridor in general has the most of these crashes, with 13 in total. There were no fatal crashes, but three suspected serious injury crashes, all along the 1000 East corridor.

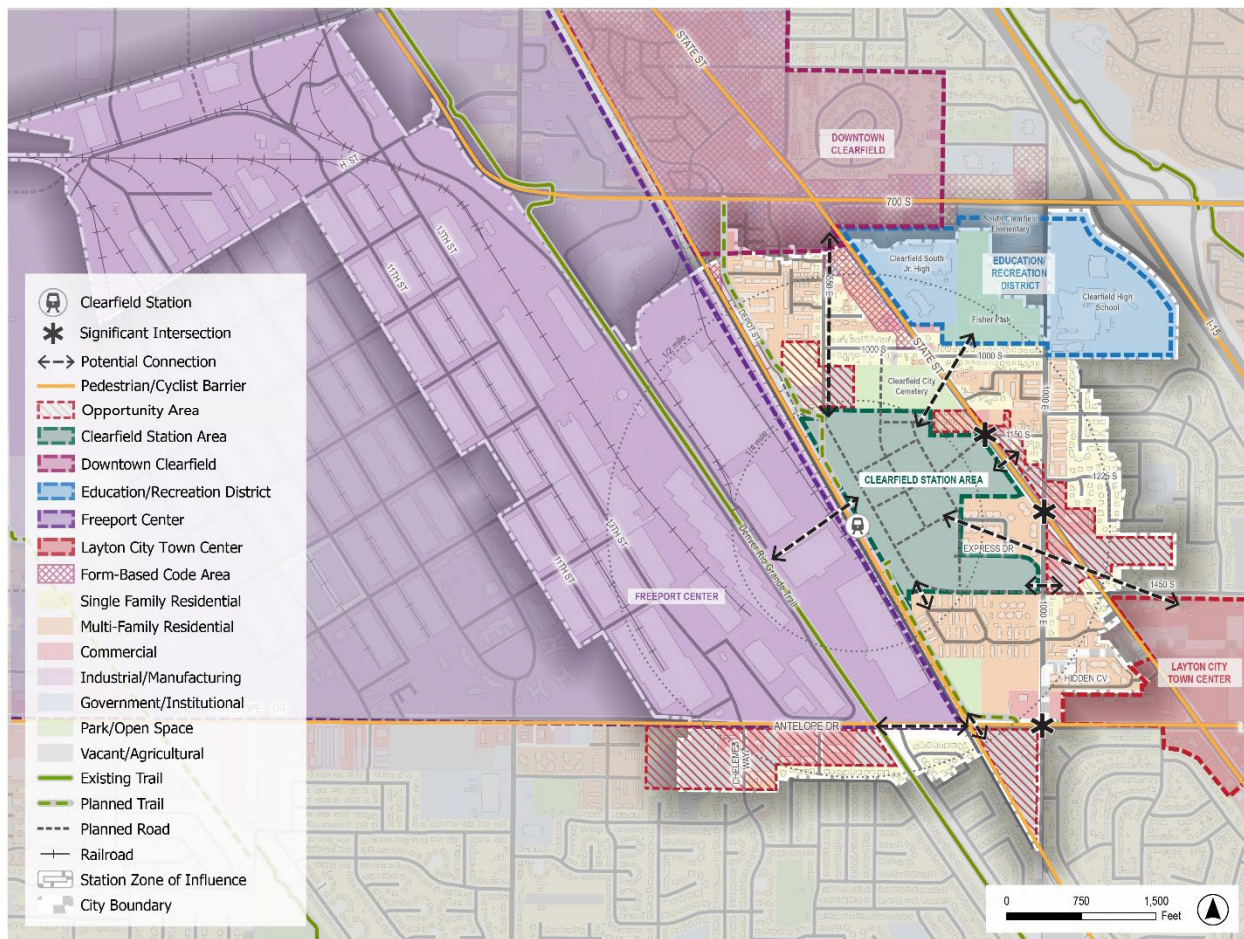
Figure 19: Active Transportation Crashes 2018-2022



SITE ANALYSIS AND IMPLICATIONS

In response to the land use and transportation findings, a site analysis for the station's area of influence (Figure 20) examines the opportunities and constraints for creating a well-connected, integrated, mixed-use station area. Key elements identified include potential connections, significant intersections, pedestrian/cyclist barriers, and potential transformation areas.

Figure 20: Site Analysis Map



Potential Connections

The potential connections identified in Figure 21 indicate destinations in need of a stronger connection to the station. These destinations include:

Adjacent Neighborhoods: The multi-family development to the South is separated by a fence with no connections into the site. The neighborhood north of the site currently does not have any connections to the station, though the planned future Depot Street and trail will allow for vehicular, pedestrian, and bicycle connections to the north.

Freeport Center and Other Areas West of the Tracks: This area has very limited non-motorized access

to the station, as crossing the tracks is only possible along the City's major arterials, which currently include little to no pedestrian or bicycle facilities. However, a multi-use path and buffered bike lanes planned across the Antelope Drive bridge could significantly improve access for these areas.

The Denver & Rio Grande Western Rail Trail (D&RGW) is a multi-use, paved trail that runs 22 miles from West Bountiful through Roy. The trail runs north-south at the west of the station, but is separated by train tracks and warehousing facilities. Currently there is no access from the station to the trail, though the proposed improvements on Antelope Drive would improve access.

The UTA 640 bus route does provide a service connection between Freeport Center and Clearfield Station, but ridership demand is low and the service limited. If a transit connection is desired for Freeport Center, this might be better achieved through flex shuttles or other microtransit options.

Davis Hospital and Neighborhoods to the East: Residential neighborhoods and the Davis Hospital to the east of the corridor represent a significant population of potential ridership. The UTA 640 bus route does provide a transit connection to these areas. However, State Street itself is a significant barrier for any active transportation and will require improved crossings in order to encourage use of the station by these neighborhoods.

Downtown Clearfield and Layton City Town Center: Downtown Clearfield and one of Layton's Town Centers lie just outside of Clearfield Station's zone of influence. Adequately connecting the centers will be important to create a thriving and well-connected mixed-use district.

Clearfield Education and Recreation District: Clearfield High School, North Davis Jr. High, South Clearfield Elementary, the Clearfield Aquatic and Fitness Center, and Fisher Park are clustered together near the northeastern limits of the station's zone of influence. These important community nodes should also have a strong connection to the station area.

Significant Intersections

Figure 21 also identifies significant intersections where key corridors meet within the zone of influence. These intersections should receive special design consideration to ensure they are safe and efficient for all modes of transportation.

Pedestrian/Cyclist Barriers

The rail lines adjacent to the site are significant barriers to users west of the tracks, as they prevent easy linkages to the transit options and placemaking enhancements associated with the station. Similar access and crossing challenges exist along State Street, Antelope Drive, and 700 S due to heavy traffic and minimal bike and pedestrian infrastructure and street crossings. Roadway barriers can be more easily overcome through proactive design and planning than rail barriers can, which would require additional grade separated crossings.

Potential Transformation Areas

The condition and age of existing uses within the station's half-mile zone of influence are variable at best. The Frontrunner station site is largely undeveloped, although a clear vision has been established that supports a significant transformation of the site into a new and important destination for the city and region.

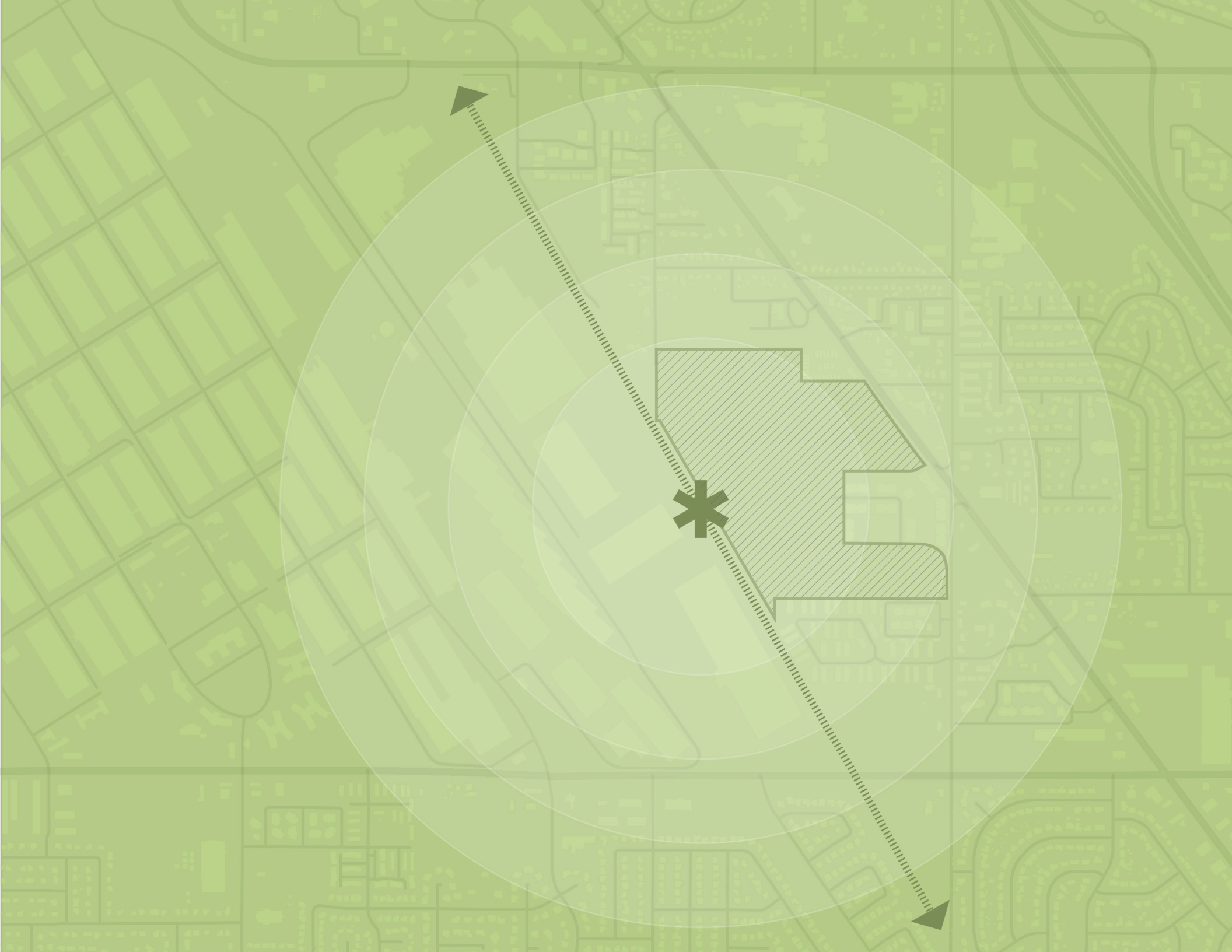
The residential neighborhoods to the north and south include a significant amount of multi-family and townhome residential development, which are aligned with emerging housing demands and TOD profile of the station and its surroundings. Several commercial properties, particularly along State Street, are vacant/abandoned, in disrepair, or include low-land-value uses that typically relocate as an area urbanizes. Figure 21 identifies these areas as “potential areas of transformation”, indicating them as potentially ripe for development or redevelopment in the near future. These properties present an opportunity for additional transit-oriented development that would further support the station area.

CONCLUSION

The Clearfield Station area has a number of opportunities and challenges in creating a well-connected, integrated, mixed-use station area. With a limited amount of vacant land remaining in the station’s area of influence, most development is expected to occur internally to the TOD site. However, a reasonable amount of opportunity exists for meaningful transition land uses at the station’s edges, which may help support the planned station development and/or buffer the station from existing residential neighborhoods.

The Clearfield Station site is currently very auto oriented, with little to no access to the adjacent land uses. Despite this, a high walk access/egress persists, even though there is little infrastructure to support it. Other modes, including bicycles, are not well represented. Of particular concern should be the intersection of 13th Street and Antelope Drive, which holds the highest concentration of both all crashes and bicycle/pedestrian involved crashes.

Planned trails connecting to the north and south of the station should help accommodate first and last mile journeys for active transportation users, while the greatest opportunity in this regard would be a direct connection to the Denver and Rio Grande Rail Trail, which sees the highest active transportation usage in the area and would provide excellent connectivity to surrounding land uses. In addition, overcoming active transportation barriers across State Street through well-planned crossings will be key to providing meaningful connections to areas to the east of the station.





10

Appendix B

*Existing Conditions Report:
Market & Housing*



Clearfield Station Area Plan

Market & Housing Existing Conditions

May 2023



ZIONS PUBLIC FINANCE, INC.

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Background

During the 2022 General Session, the Utah Legislature approved House Bill 462, which required “certain municipalities to develop and adopt station area plans for specified areas surrounding public transit stations.”¹ As Clearfield (“City”) contains a FrontRunner station, the City, to meet the statutory requirements, has begun the process to create a station area plan. In 2019, the City adopted the Clearfield Station Master Development Plan (“MDP”) for this area, and therefore is amending that plan to comply with the new State Code requirements.

As adopted, Utah Code 10-9a-403.1, requires the City to create a plan that promotes the following objectives:

- Increasing the availability and affordability of housing, including moderate income housing;
- Promoting sustainable environmental conditions;
- Enhancing access to opportunities; and
- Increasing transportation choices and connections.

The plan is required to promote these objectives within a 0.5-mile radius around the station area, including any parcel that is partially or completely contained within the radius.

Executive Summary

In total, there are approximately 844 acres of land contained within the proscribed boundary of the station area plan, and the majority of that land is currently developed.

The map and table on the following page show the proposed Clearfield Station Area and a breakdown of the land within the boundary.

¹ <https://le.utah.gov/~2022/bills/hbillenr/HB0462.pdf>

FIGURE 1: PROPOSED STATION AREA BOUNDARY

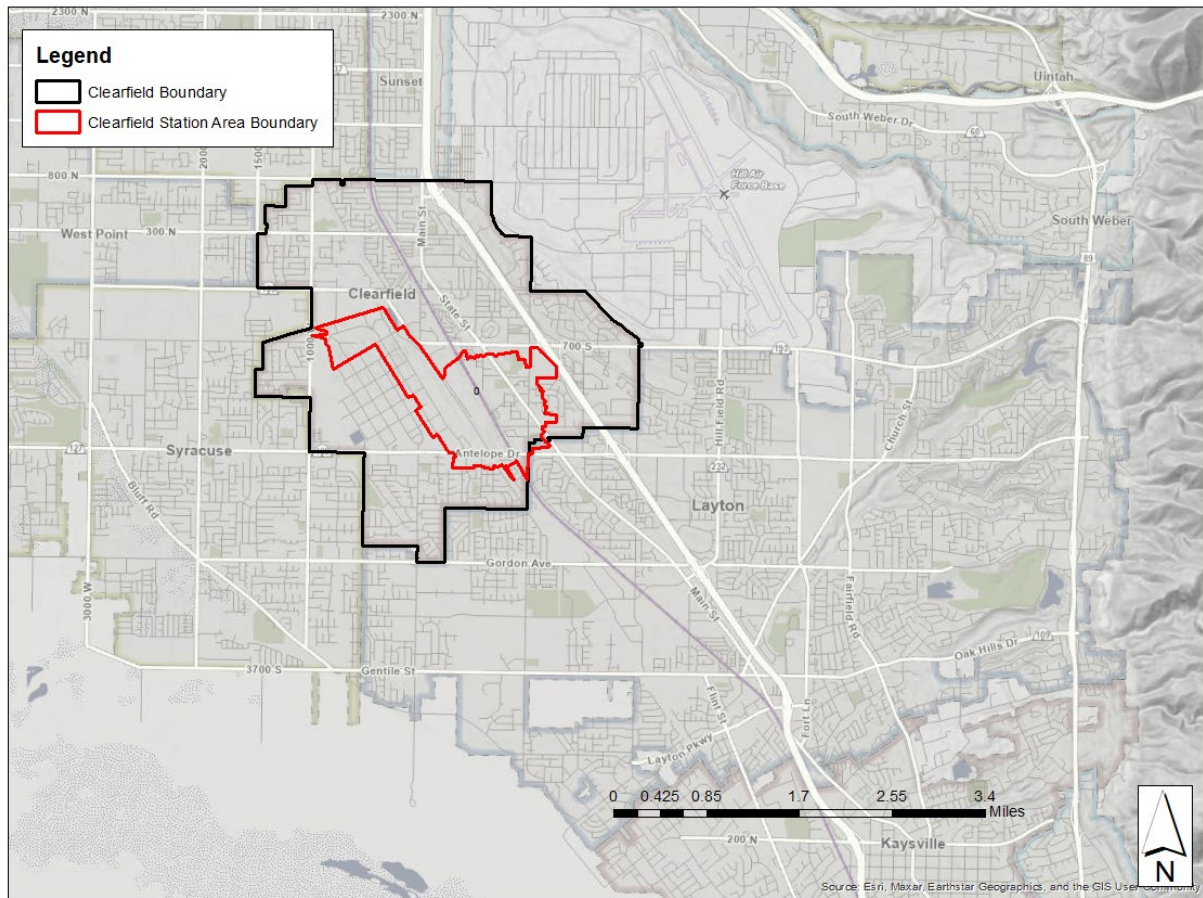


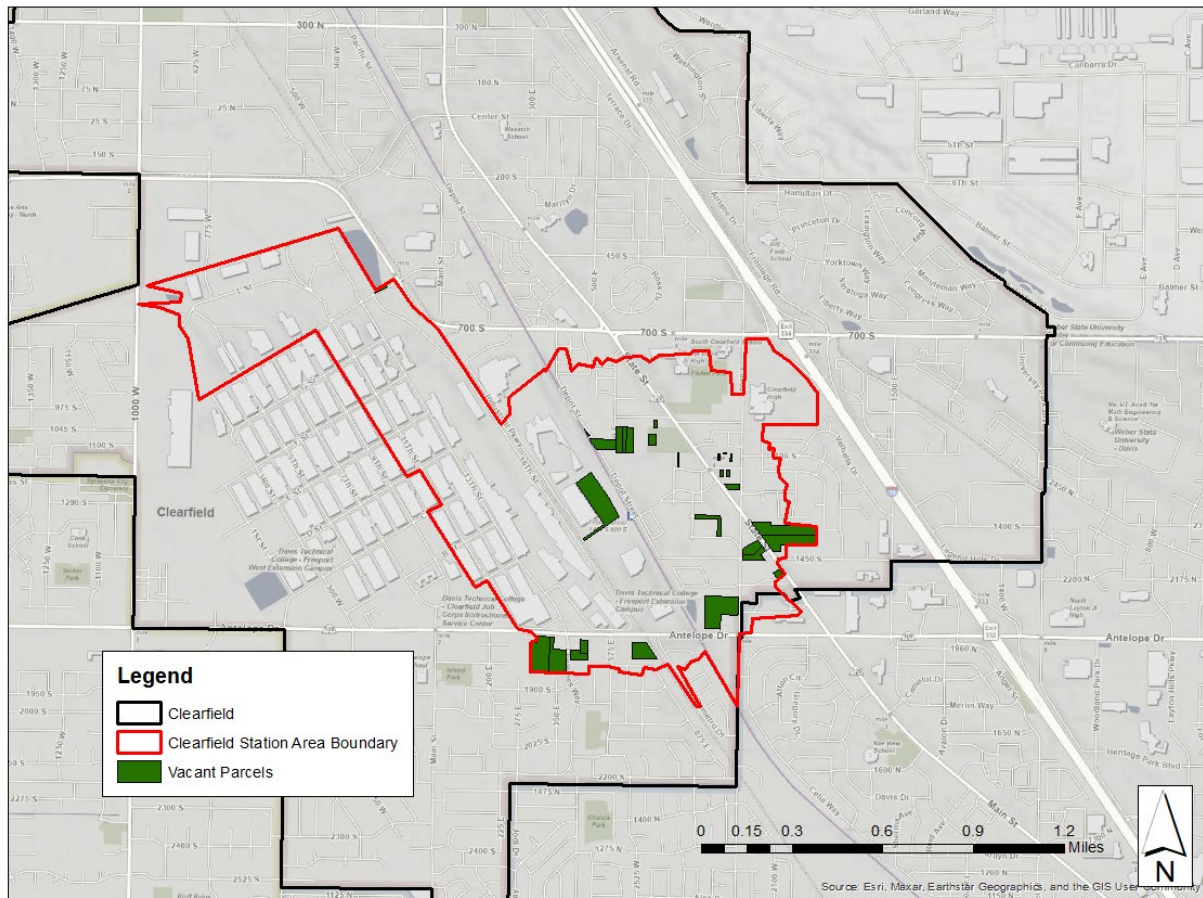
TABLE 1: SUMMARY OF LAND USE

Land Use	Acres
Developed	801.63
Vacant	42.35
Total	843.98

Source: Davis County Assessor's Office

The vacant land in the station area boundary is primarily located on the east or south of the station area boundary, along State Street and Antelope Drive. There is also some available vacant land in the Freeport Center. Some of the parcels that are identified as vacant are not viable for development as they are a part of City's cemetery or are private streets in residential development.

FIGURE 2: VACANT LAND



Strengths of the site include:

- Regional employment center
- Adopted MDP guiding development of east side of study area, providing for increased opportunities in the area across multiple development types
- Proximity to Freeport Center and Falcon Hill National Aerospace Research Park
- Redevelopment opportunities due to age and value of some commercial developments
- Active redevelopment project areas that can be utilized, or expanded, to encourage high-quality development in the area
- Utah Transit Authority (“UTA”) owns major parcels immediately adjacent to FrontRunner Station, thereby aiding a master planning process rather than piecemeal development
- Continued population growth in north Davis County, with anticipated growth of over 56,000 people by 2050. Clearfield is expected to grow by an anticipated 8,000 people.

Obstacles to site development include:

- Lack of vacant land within the station area boundaries
- Redevelopment costs may make redevelopment of older or lower value areas difficult
- Current office market is uncertain with high vacancy rates and negative absorption rates in recent quarters, although some speculative office developments are currently being developed in the City

- Connectivity of west side of site (Freeport Center) to station area and visibility from I-15 potentially limits some development opportunities

Potential Development Scenarios

- Additional commercial growth could occur along State Street and Antelope Drive with filling of vacant land and redevelopment of lower value parcels on the west side of the study area. The current MDP shows 67,500 square feet of retail space located in the interior of the eastern portion of the study area. Retail development would potentially receive greater visibility and access along on State Street rather than inside the MDP, although planned retail inside the development would provide support retail to the planned office development. The City currently has significant retail leakage and would benefit greatly from additional retail development.
- Neighborhood support retail is the most likely retail development type for the eastern portion of the site, especially retail that would complement the housing in the area such as eateries and other convenience shopping. Regional retail does not benefit from transit as large purchases of goods are not easily carried on public transit.
- Adopted MPD anticipates build-out by 2030, with development occurring at approximately 7-10 acres per year. However, this plan identifies 550,000 square feet of office space which may not be feasible in the current market. During the past year, absorption rates in the Davis-Weber office market have been fairly low. In fact, the third and fourth quarters of 2022 saw negative absorption of 186,000 square feet. While the office space planned for and shown in the MDP serves as a good buffer and is a desirable use between the station itself and the planned residential development, it may be difficult to achieve in the near term.
- The western portion of the study area is currently defined primarily by industrial space, which is likely to continue in the future. There is also some office space in that western area. There are not good connections between the west and east sides of the study area and therefore the west side of the study area may not realize all the benefits it otherwise would from the transit stop (i.e., easy access to transit and retail options).

This remainder of this report will explore the following:

- I. Demographics
- II. Economic Opportunities
- III. Housing Opportunities

I. Demographics

The following are key demographics for the City, Davis County, and the State at large.

TABLE 2: DEMOGRAPHICS

Demographic Category	Clearfield	Davis County	State of Utah
Median Age	29.3	32.3	30.7
Average Household Size	3.05	3.24	3.08
Median Household Income	\$64,689	\$92,765	\$79,133
Median Home Value	\$241,300	\$351,400	\$339,700
Median Monthly Housing Costs	\$1,321	\$1,709	\$1,682
Median Gross Rent	\$1,196	\$1,238	\$1,171

Demographic Category	Clearfield	Davis County	State of Utah
Persons in Poverty ²	11.6%	6.4%	8.6%

Source: 2021 ACS 5-Year Estimates

Between 2020 and 2050, this region is expected to grow by over 56,000 people, with Clearfield growing by an anticipated 8,000 people.

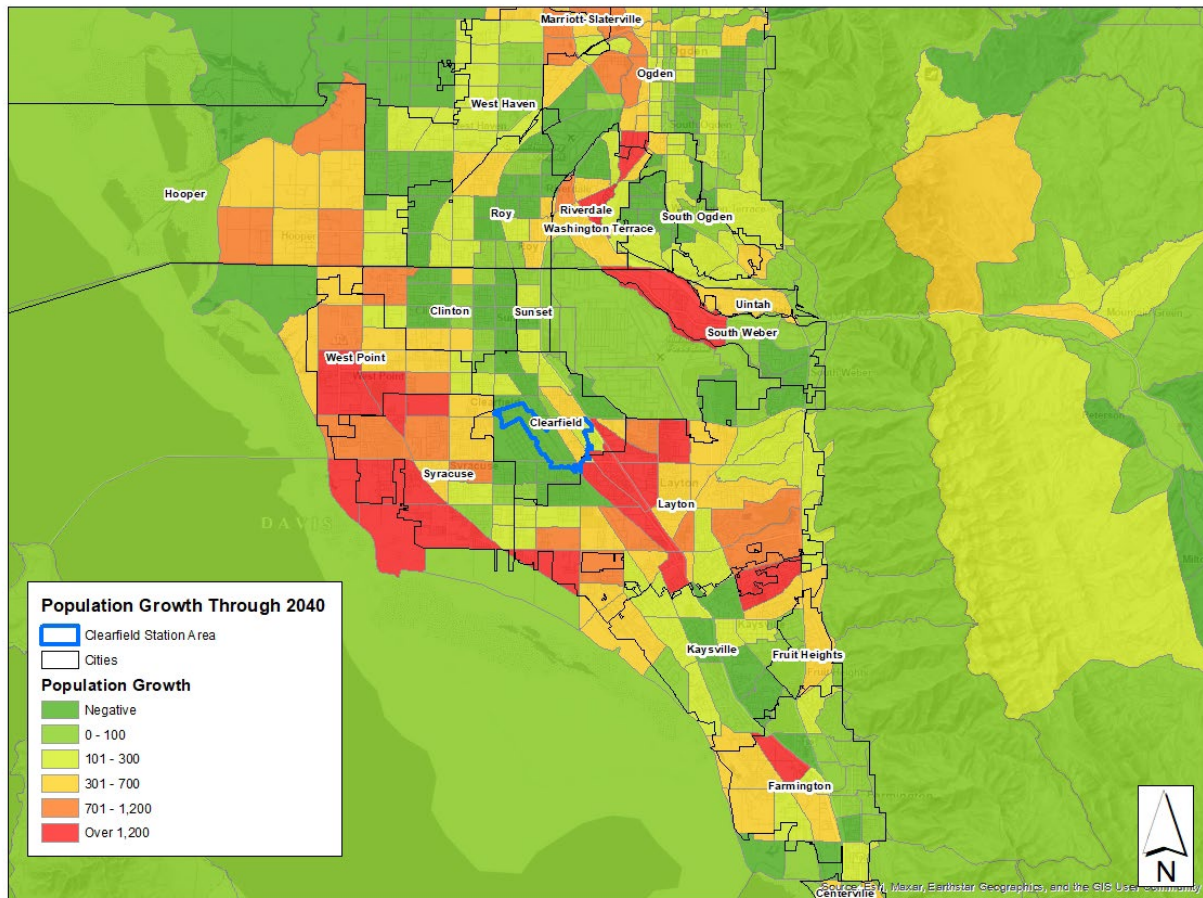
TABLE 3: REGIONAL POPULATION GROWTH PROJECTIONS

City	2020	2030	2040	2050
Clearfield	31,909	33,432	35,999	39,774
Clinton	23,386	23,499	24,824	25,914
Hooper	9,087	12,528	15,470	17,386
Roy	39,306	39,431	40,529	41,826
Syracuse	32,141	39,018	46,682	51,203
West Haven	16,739	22,060	24,598	26,331
West Point	10,963	11,953	14,895	17,341
Total	163,531	181,921	202,997	219,775
Growth from Prior Period		18,390	21,076	16,778
Cumulative Growth		18,390	39,466	56,244

Source: Wasatch Front Regional Council

² The Census Bureau determines persons in poverty by measuring family income against income thresholds based on family size.

FIGURE 3: POPULATION GROWTH PROJECTIONS



II. Economic Opportunities

Utah Code 10-9a-403.1 (7) (a) (iii)

Current Conditions

Workforce

Both the City and Davis County have similar labor force participation rates, although the City does experience slightly higher levels of unemployment. Several of the top industries are shared among the two, but the City, with Freeport Center and Falcon Hill National Aerospace Research Park, sees higher labor force participation in manufacturing than the County as a whole.

TABLE 4: WORKFORCE CHARACTERISTICS

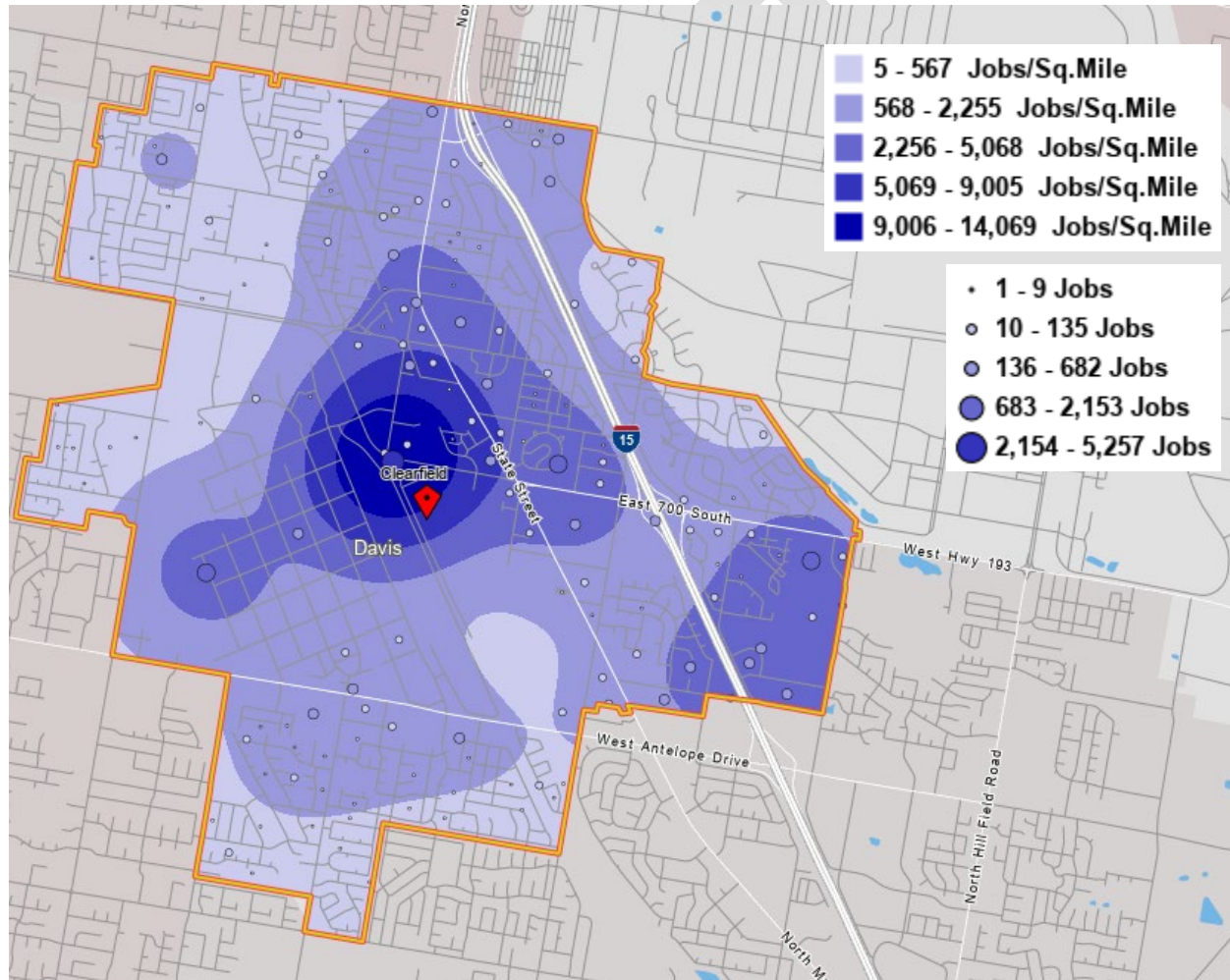
	Clearfield	Davis County
Total Labor Force	15,875	181,737
Labor Force Participation Rate	70.1%	70.6%
Unemployment Rate	2.7%	2.4%
Average Wage	\$3,941	\$4,332
Top Industries	Education & Health Care – 21.0%	Education & Health Care – 21.3%
	Manufacturing – 12.7%	Professional, Scientific, and Management – 12.2%

	Clearfield	Davis County
	Professional Services – 11.1%	Retail Trade – 11.5%
Average Commute	22.1 minutes	22.2 minutes

Source: 2021 ACS 5-Year Estimates

The City has several major areas of job concentration, most notably at the center of the City in proximity to the Freeport Center, and the eastern edge of the City around 1400 South and I-15. The employment center around the Freeport Center is the location of the proposed station area boundary and creates opportunities for additional employment capture in that area. Additionally, as a regional employment center, the City should be able to attract new businesses to the area due to the relatively strong labor market.

FIGURE 4: JOB CONCENTRATION & EMPLOYMENT CENTERS



Currently, the City is a regional employment center, with 16,656 total jobs reported in the City as of 2020. Manufacturing represents over 43 percent of the total jobs held within the City.

TABLE 5: JOB COUNTS BY NAICS INDUSTRY SECTOR IN 2020

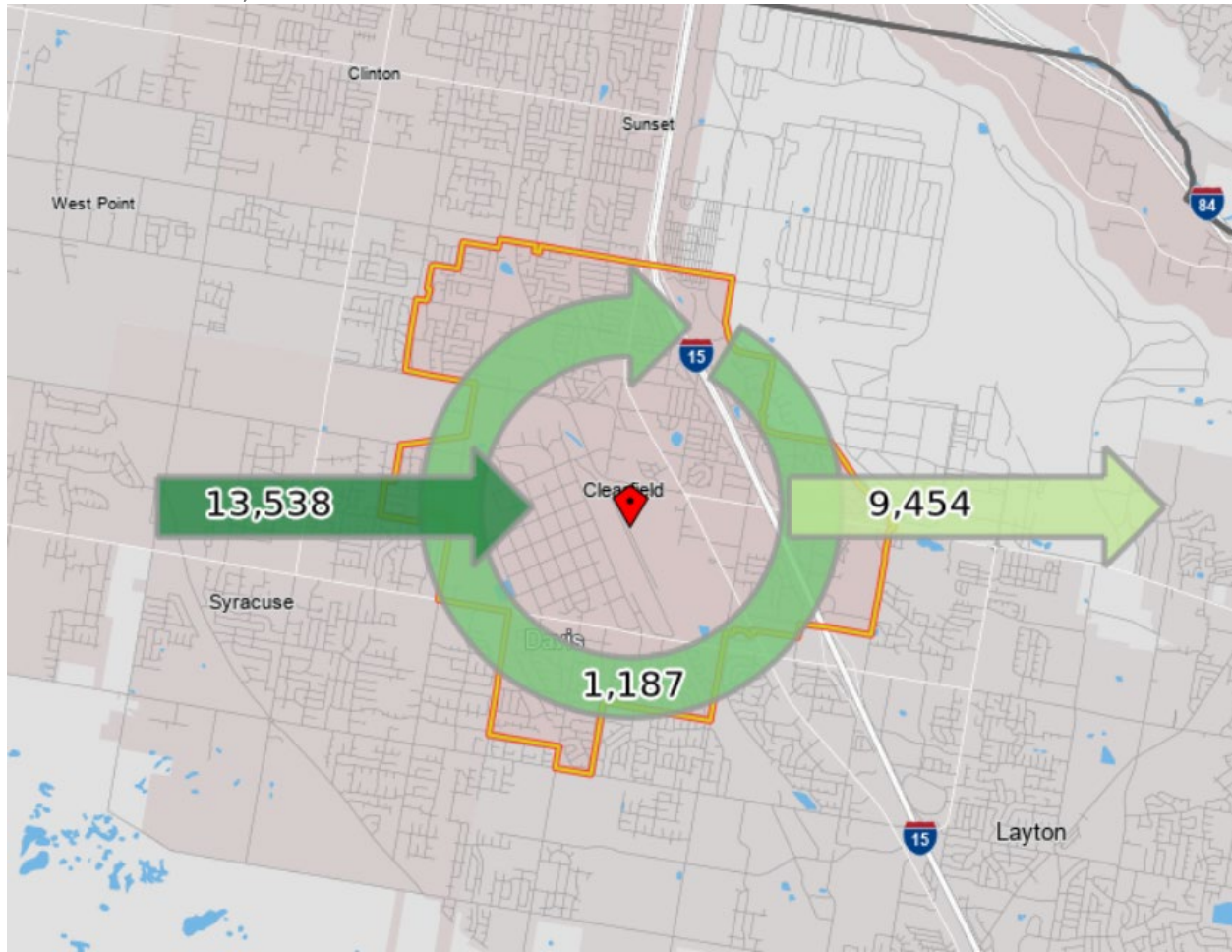
Industry	Count of Jobs	Percent of Total Jobs
Manufacturing	7,218	43.30%
Professional, Scientific, and Technical Services	1,590	9.50%
Administration & Support, Waste Management and Remediation	1,381	8.30%
Health Care and Social Assistance	1,242	7.50%
Educational Services	1,125	6.80%
Retail Trade	727	4.40%
Accommodation and Food Services	687	4.10%
Public Administration	509	3.10%
Finance and Insurance	496	3.00%
Transportation and Warehousing	438	2.60%
Construction	421	2.50%
Real Estate and Rental and Leasing	229	1.40%
Arts, Entertainment, and Recreation	204	1.20%
Wholesale Trade	162	1.00%
Other Services (excluding Public Administration)	147	0.90%
Information	66	0.40%
Management of Companies and Enterprises	13	0.10%
Mining, Quarrying, and Oil and Gas Extraction	1	0.00%
Total Jobs	14,725	100.00%

Source: US Census Bureau

The City is also considered a regional employment center due to the number of people commuting into the City for work, while living elsewhere. One benefit of this inflow of labor is the additional taxable sales that are generated by these individuals. These workers, while not making the majority of their purchases in their work community, will usually generate some taxable sales in close proximity to where they work, generally gasoline and convenience store or other food purchases. This allows the City to experience additional fiscal benefits to companies locating within the City.

In total, approximately 13,500 individuals commute to Clearfield from other communities. This presents opportunities for the City to capture additional sales tax revenue from individuals coming to the area and shopping in the City, and by locating retail along major traffic corridors, the City will be better able to realize the benefits of this inflow of labor.

FIGURE 5: LABOR FLOWS, 2020



There are a variety of employment types within the City's top employers. Due to the proximity of the City to Hill Air Force Base ("Hill"), there are many businesses that are associated with the defense industry. Additionally, various manufacturers have found success by locating in the City.

TABLE 6: CLEARFIELD TOP EMPLOYERS

Employer	Number of Employees	Industry
Air Force Materiel Command (Hill AFB) ³	10,000-14,999	National Security
Lifetime Products Inc.	2,000-2,999	Sporting & Athletic Goods Manufacturing
Northrop Grumman Corp	1,250-2,498	Guided Missile & Space Vehicle Propulsion Manufacturing
AAA	500-999	Telemarketing Bureaus
Utility Trailer Manufacturing Company	500-999	Truck Trailer Manufacturing
Bonnell Aluminum	250-499	Aluminum Rolling, Drawing & Extruding

³ Although outside of the City's boundaries, Hill Air Force Base is attached to Clearfield by the Utah Department of Workforce Service's FirmFind data. The US Census Bureau does not count these numbers in Clearfield's labor pool.

Employer	Number of Employees	Industry
Clearfield Job Corps Center	250-499	Technical & Trade Schools
A Step Forward Home Health	100-249	Home Health Care Services
Americold Logistics, LLC	100-249	Refrigerated Warehousing & Storage
Malnove Incorporates of Utah	100-249	Folding Paperboard Box Manufacturing
North Davis Cabinet, Inc.	100-249	Wood Kitchen Cabinet & Countertop Manufacturing
Parc Community Partnership Foundation	100-249	Vocational Rehabilitation Services
Recommended Building Maintenance LLC	100-249	Janitorial Services
RMC – Clearfield Operating, LLC	100-249	Nursing Care Facilities
Smith Manufacturing	100-249	Sporting & Athletic Goods Manufacturing
Wyle Laboratories, Inc.	100-249	Engineering Services

Source: Utah Department of Workforce Services

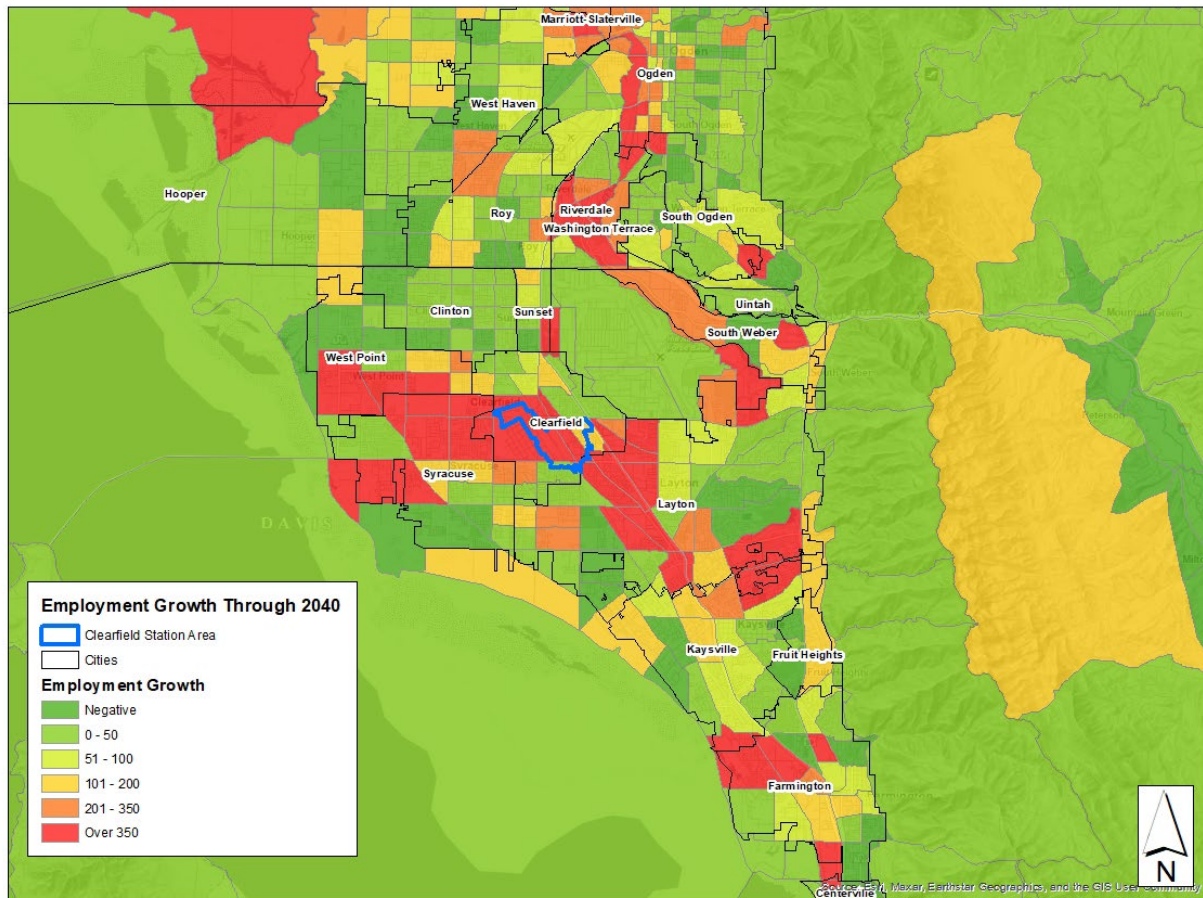
Significant employment growth is also projected for the region, with an increase of over 20,000 jobs by 2040.

TABLE 7: REGIONAL EMPLOYMENT GROWTH PROJECTIONS

City	2020	2030	2040	2050
Clearfield	14,951	17,349	19,754	22,075
Clinton	1,823	1,966	2,135	2,275
Hooper	287	318	310	308
Roy	5,787	6,850	7,404	7,828
Syracuse	2,749	7,243	11,376	14,187
West Haven	2,978	5,595	7,139	8,267
West Point	533	841	1,147	1,590
Total	29,108	40,162	49,265	56,530
Growth from Prior Period		11,054	9,103	7,265
Cumulative Growth		11,054	20,157	27,422

Source: Wasatch Front Regional Council

FIGURE 6: REGIONAL EMPLOYMENT GROWTH PROJECTIONS



Redevelopment Agency

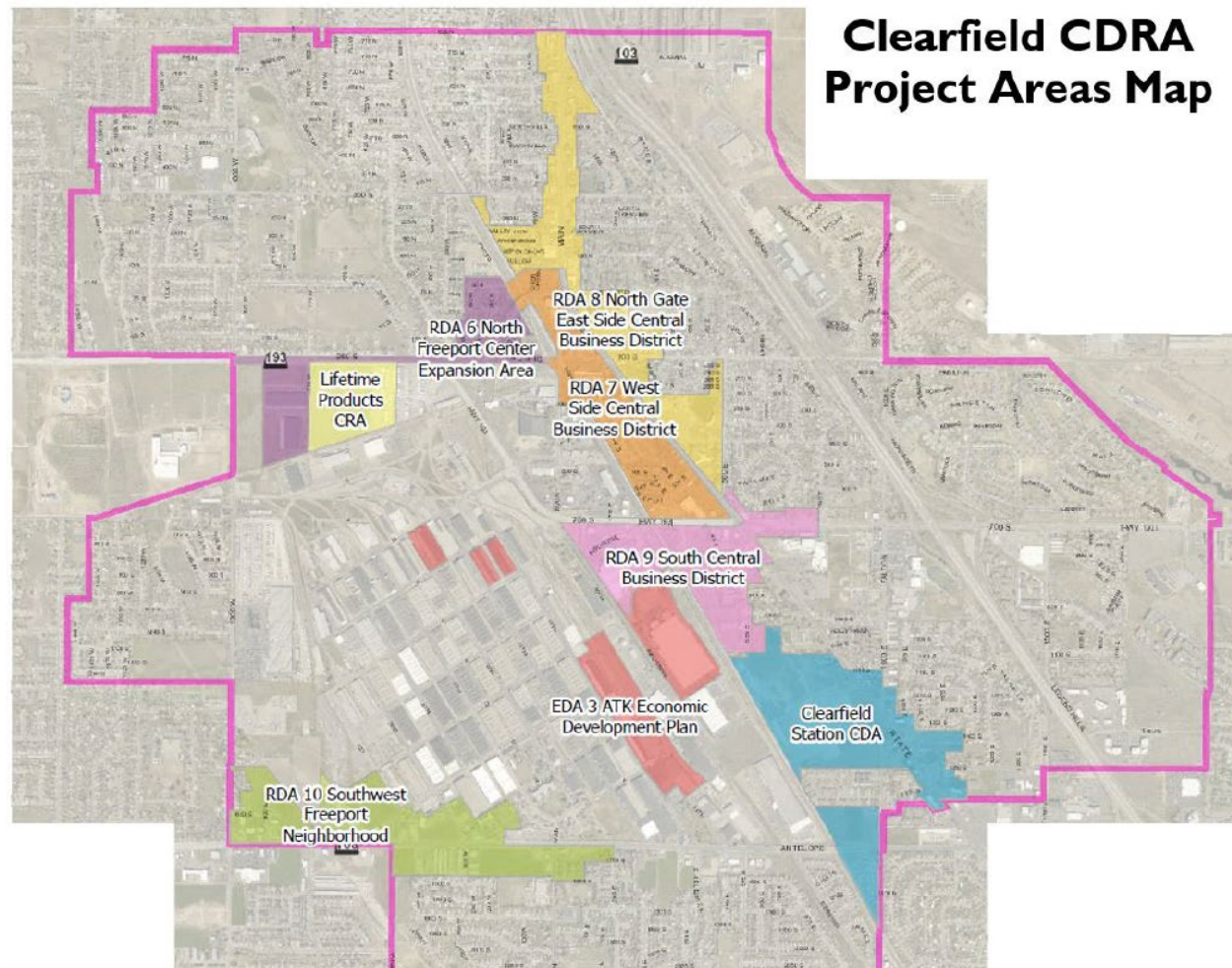
The City currently operates a community development and renewal agency (“CDRA”) that has active project areas within the boundaries of the station area. Of the City’s eight active project areas, three fall within the boundaries of the station area. A summary of these three project areas is provided below:

TABLE 8: ACTIVE RDA PROJECT AREAS

Category	RDA 9 South Central Business District	EDA 3 ATK Economic Development Plan	Clearfield Station CDA
Base Year	1992	2011	2013
Project End Date	2027	2032	2052
Percentage of Tax Increment	60%	82%	75%
Base Year Value	\$11,786,915	\$78,168,767	\$0
Current Assessed Value	\$71,628,571	\$168,192,702	\$19,470,764
Developed Acreage	75.00	96.00	48.10
Undeveloped Acreage	11.00	96.00	77.90
Total Funds Received	\$5,196,143	\$8,788,671	\$554,533
Total Funds Remaining	\$2,174,848	\$8,284,499	\$24,246,035

Source: Utah Governor’s Office of Economic Opportunity RDA Database, 2022 Annual Report

FIGURE 7: CLEARFIELD REDEVELOPMENT AGENCY PROJECT AREAS



Each of these areas may be impacted by development within the station area boundaries. Each area has both time and funds remaining, which, depending on current agreements and obligations, can be utilized to support the development of projects within the station area boundaries.

There are approximate 500 acres of the station area boundaries that are not currently included within a project area. This allows for potential expansion of current project area boundaries, or potential new project areas, to support development in the area. Depending on current agreements with project area participants, there may be available funding within the current areas to help support high-quality development within the station area.

Sales Tax Leakage

Sales tax is one of the City's most important revenue sources. A sales tax leakage model looks at the taxable sales within a community and compares it to expected taxable sales based on average per capita spending statewide and the population of a given community. Capture rates exceeding 100% indicate that consumers are coming to a city from the larger regional area (i.e., outside of the City boundaries) to make retail purchases. A capture rate under 100% indicates that a city has a gap between what it could collect and what it currently is collecting. This is referred to as "leakage" and identifies opportunities for future retail development.

Overall, the City is capturing a total of 41% of the expected taxable sales for its population, indicating that residents are making sales tax purchases in other communities in some retail categories. In total, the City is leaking over \$297 million annually in taxable sales.

The following table shows the sales tax leakage and capture rates for the various sales tax categories. Positive leakage amounts indicate that the City is capturing sales from the larger regional area, based on average per capita spending. Negative leakage amounts, with capture rates less than 100 percent, indicate that the City is leaking taxable sales in a given category.

TABLE 9: SALES TAX LEAKAGE

Sales Tax Category	Leakage Amount	Percent Captured
Gasoline Stations	\$7,335,691	150%
Miscellaneous Store Retailers	(\$1,866,296)	90%
Other Services	(\$4,199,041)	76%
Health and Personal Care Stores	(\$4,498,143)	28%
Arts, Entertainment, and Recreation	(\$4,614,936)	37%
Nonstore Retailers	(\$8,366,645)	85%
Electronics and Appliance Stores	(\$9,276,065)	26%
Sporting Goods, Hobby, Book, and Music Stores	(\$10,228,303)	25%
Furniture and Home Furnishings Stores	(\$11,634,187)	4%
Clothing and Clothing Accessories Stores	(\$13,409,216)	25%
Accommodation	(\$15,212,772)	6%
Food Services and Drinking Places	(\$20,308,730)	59%
Food and Beverage Stores	(\$37,888,344)	35%
Building Material and Garden Equipment and Supplies Dealers	(\$41,222,938)	16%
Motor Vehicle and Parts Dealers	(\$57,060,697)	32%
General Merchandise Stores	(\$64,921,222)	7%
Total	(\$297,371,845)	41%

Source: Utah State Tax Commission, ZPFI

The City currently has only one category (Gasoline Stations) where it is capturing at least its fair share of taxable sales. The other categories, especially those with the highest leakage dollar amount, represent possible areas for the City to specifically focus on to generate the greatest return in the form of increased sales tax revenues.

Opportunities Under Current Conditions

Utah Code 10-9a-403.1 (8) (a) (ii) (A)

Currently, there is little vacant land within the boundaries of the station area plan. Existing development consists of some residential neighborhoods, the Freeport Center on the west side of the FrontRunner tracks, and the currently developing Clearfield Station.

It is likely that, under current conditions, the site will see some measure of residential development mixed with support retail, and business park/industrial development. Because of the City's role as an employment center, there are strong opportunities for increased job growth in the area, and within the boundaries of the station area. It is likely that this will mainly occur within the Freeport Center and the office components of the Clearfield Station, but with commercially viable land on the southern borders of the station area, and along State Street, there may be additional job growth in those areas.

Clearfield Station

In 2019, the City approved a master development plan for land owned by UTA at the Clearfield FrontRunner Station. The area currently consists of parking lots and vacant land. This project, covering 56 acres, is planned to bring a variety of uses to the area. The approved plan calls for approximately 67,500 square feet of commercial space and 550,000 square feet of office space. In addition, there are around 1,000 residential units (townhomes and apartments) planned for Clearfield Station. This is a critical site of development for the City. It allows for better connection to the regional economy and prepares the City to capture benefits of regional growth. In 2022, construction began on required infrastructure such as roads and utilities. It is anticipated that vertical construction will commence in 2023 or 2024.

FIGURE 8: CLEARFIELD STATION MASTER DEVELOPMENT PLAN



This development will provide a significant increase in value to the City as well as bring new residents and businesses to the area.

With current market conditions, there are opportunities to explore possible adjustments to the current plan. This could include relocating or adding additional retail space along State Street to potentially capture additional business traffic and take advantage of the higher visibility roadway. There currently are compatible commercial uses currently along State Street.

Freeport Center

To the west of the FrontRunner Station lies the Freeport Center. This is a key industrial center for northern Utah. This area has four major entities managing the area: Freeport Center Associates, Freeport West, Clearfield Job Corps Center, and Davis School District. The total area encompasses over 1,000 acres of land. The Freeport Center Associates are the majority owner in the area and manage 680 acres and have 7 million square feet of industrial space spread across 78 total buildings.⁴ This rail-served site, is home to seven of Clearfield's top 17 employers. With a mix of manufacturing, distribution, and warehouse users this area is a strength to the area. While it is mostly developed, there may be opportunities to redevelop areas of this property.

Some of the users of the Freeport Center have both manufacturing and office needs and with the planned office space at the Clearfield Station development, there may be opportunities for those users to find office space nearby. Depending on the development of retail uses, there could also be opportunities for additional sales tax capture from employees at the center.

FIGURE 9: FREEPORT CENTER



⁴ <https://www.freeportcenter.com/about-us/>

Falcon Hill National Aerospace Research Park

Hill AFB is a major economic driver for northern Utah, and especially Davis County. One aspect of this is a public-private partnership between the United States Air Force, the State of Utah's Military Installation Development Authority (MIDA), and private developers. The Falcon Hill National Aerospace Research Park is a 550-acre development containing 3.5 miles of I-15 frontage. The first phase alone will contain over 2 million square feet of office space.⁵ The development is planned to contain a mix of Class A commercial office space, research and development space, as well as some small support retail.

Although this park is outside of the station area boundaries, it presents an opportunity for the City to capture some of the overflow or related uses in the station area development.

FIGURE 10: FALCON HILL AEROSPACE RESEARCH PARK



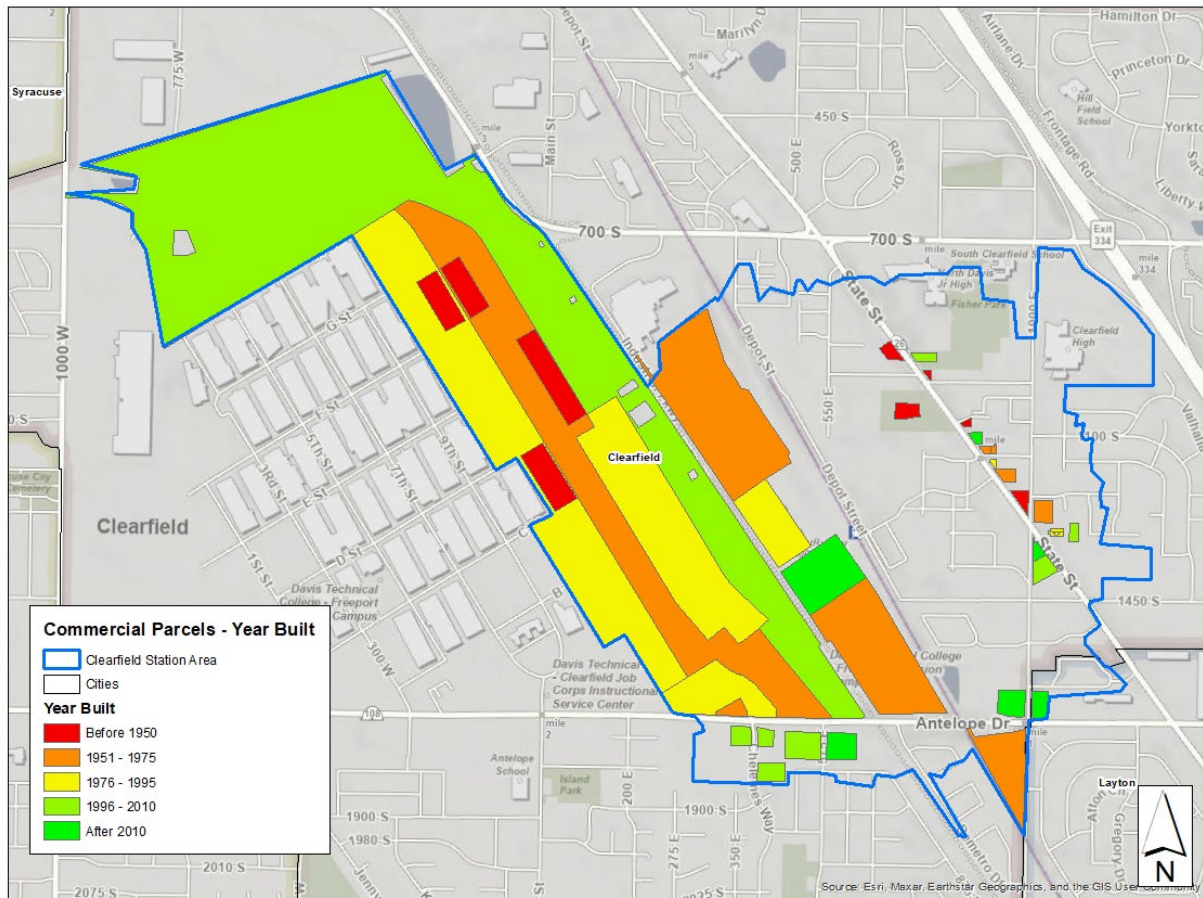
Redevelopment

Although there is little vacant land remaining in the station area boundary, there may be opportunities for the City to explore redevelopment of key areas. This is due to the age of buildings, or low value per square foot of development. This would allow for higher value development to take place.

Within the Freeport Center, there are a number of older buildings that may present opportunities for redevelopment as business needs arise. Additionally, there are a number of commercial buildings along State Street that were built before 1975 that may be candidates for future redevelopment.

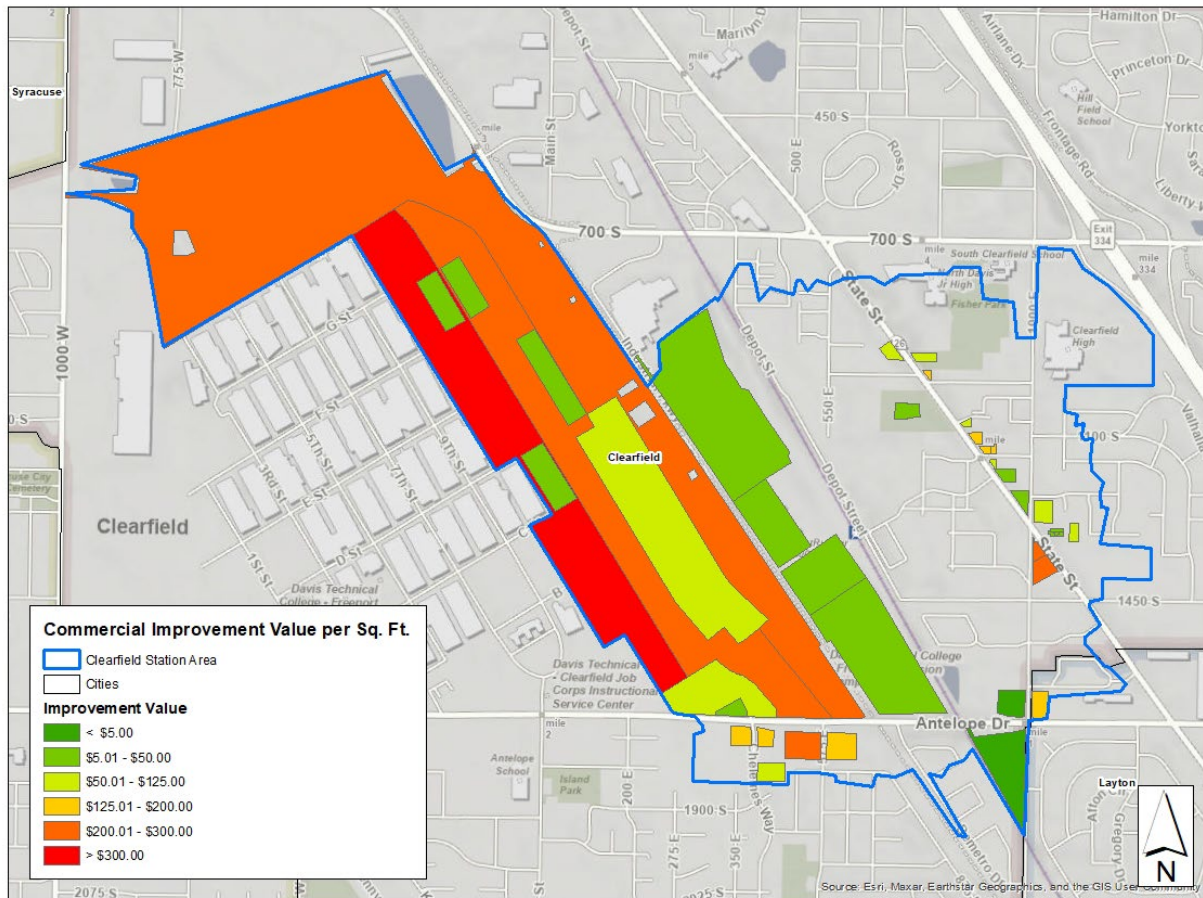
⁵ <https://business.utah.gov/articles/falcon-hill-aerospace-research-park-invests-over-250-million-into-utahs-economy/>

FIGURE 11: COMMERCIAL PARCELS - YEAR BUILT



Some of those same areas have lower improvement values per square foot of development and would bring a higher return to the City if they were redeveloped. Areas on Figure 12 that are designated in green shades indicate that those property currently have low improvement values compared to others in the area. Through redevelopment of those properties, the City could experience higher improvement values, and therefore greater property tax revenues.

FIGURE 12: COMMERCIAL IMPROVEMENT VALUES



Constraints Under Current Conditions

Utah Code 10-9a-403.1 (8) (a) (ii) (B)

There are a number of constraints on development in the area that could impact the possibilities in the station area.

Vacant Land

Approximately 95% of the land contained within the station area boundary is currently developed, thus constraining the available options for development.

Office Market

Within Davis and Weber Counties, the office market is currently experiencing a slowdown, similar to other areas. Vacancy rates have been rising since the end of 2021. At that time, vacancy rates were approximately 5% and they have risen to over 8%. Throughout the Davis-Weber office market, absorption rates have been fairly low. The third and fourth quarters of 2022 saw negative absorption rates, with -186,000 square feet being absorbed.⁶ With negative absorption rates in the area, it may be difficult to attract office development to the area at the levels anticipated in the Clearfield Station Plan.

⁶ Newmark Davis and Weber Counties Office Report, Q4 2022.

The location of the Clearfield station area may also constrain office development in the area, due to the distance and lack of visibility from I-15. Within the County, there are locations directly adjacent to the Interstate that will likely be more attractive to office users.

Redevelopment

While there is potential for redevelopment in areas throughout the station area boundaries, this is often cost prohibitive.

Access

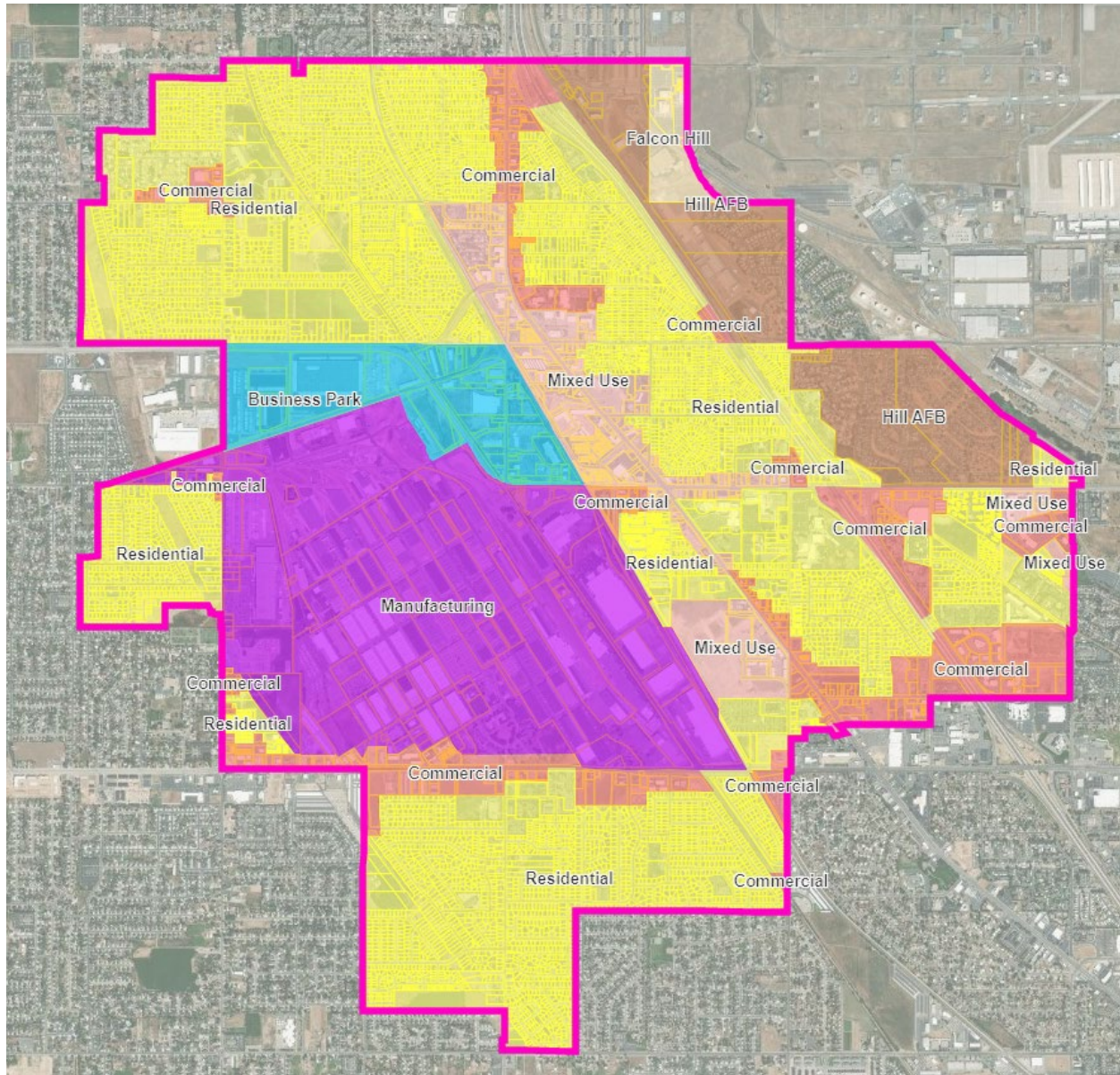
While the Freeport Center is a major employment center in the area, and could serve as an economic driver, access to the center from the FrontRunner Station is limited. FrontRunner riders would need to walk one and a half miles to reach the entrance to the Freeport Center. This could limit the desire for businesses located in the center to acquire office space at the Clearfield Station. It is possible that some form of crossing over the train tracks would help alleviate this concern.

Municipality's Objectives

Utah Code 10-9a-403.1 (8) (a) (ii) (D)

The City's adopted 2017 General Plan contemplates a variety of uses in and surrounding the station area. These include manufacturing, residential, and mixed-use development.

FIGURE 13: CLEARFIELD GENERAL PLAN



The Clearfield Station Plan (discussed previously) is the approved development plan for much of the vacant land in the station area boundaries. The other vacant areas in the boundaries will likely be developed according to the City's General Plan.

Economic Opportunities

Utah Code 10-9a-403.1 (7) (a) (iii)

Highest and Best Use

The purpose of this section is to evaluate the highest-and-best use of the property from the perspective of a developer and the fiscal impacts and benefits to the City from various types of development. It is important to understand how highest and best use works, and, more importantly, how desired development can be achieved. Historically, highest and best use has only been considered as what creates

the greatest return on the land. This is a developer-centric model for highest and best use and relies on an understanding of developer figures and intentions. A wider implementation of highest and best use should consider the following:

- Highest and best use to the developer. This scenario considers the greatest return to the land and has historically been all that has been considered by most municipalities; and
- Highest and best use to the City (fiscal). This consideration addresses the proposed fiscal impacts of development and what revenue and expenses are generated for the City. The impacts may include, but are not limited to, property taxes, sales taxes, municipal energy fees, Class B/C road funds, retail buying power, and costs of services to be provided; and
- Highest and best use to the citizens. This scenario is often less quantitative and relies upon feedback from citizens of what amenities are lacking in the area. This process also requires notable education, as residents will oft resort to desires that are not market feasible. Data is necessary to show, for example, that a certain retailer will not occupy a site until surrounding demographics hit specific metrics. Or residents may be unaware that their transportation costs are higher than those of other communities due to a lack of employment centers, and that adding jobs at a site (instead of an alternative, publicly desired use) may result in notable community benefits.

CAP rates, which are a measure of net operating income (NOI) divided by valuation vary considerably based on location, presumed risk of a project (i.e., vacancy rates, etc.). Lower CAP rates are generally indicative of a more optimistic market with CAP rates rising as market outlooks decline. Generally speaking, developers could see the greatest profit margins with apartments, flex office, and some retail development. It is important to note that profit margins are a general estimate only and are dependent on many factors for the developer such as land costs, interest rates and financing costs, varying construction costs, achievable rents, etc.

TABLE 10: DEVELOPMENT CAP RATES

	Cap Rates	Profit Margins
Apartments		
	4.00%	29%
	4.25%	22%
	4.50%	15%
	5.00%	3%
Office		
	6.00%	1%
	6.50%	-6%
Retail		
	5.00%	15%
	5.50%	5%
Flex Office		
	4.50%	28%
	5.00%	20%
	5.50%	12%

Source: ZPFI

From the perspective of the City, property tax revenues, sales tax revenues and other revenue sources are the best measure of highest-and-best use. Because of the point-of-sale distribution formula in Utah, retail

is the highest revenue generator, on a per acre basis, for cities. However, retail only thrives in certain locations and the supportable amount of retail is dependent on the population and employment in a given area.

TABLE 11: HIGHEST AND BEST USE ANALYSIS— CITY PERSPECTIVE

Summary Comparison	Office	Retail	Multi-Family - 20 units per acre	Multi- Family - 8 units per acre	Flex Office
Property Taxes	\$5,409	\$3,131	\$2,875	\$1,265	\$3,921
Sales Taxes		\$21,780	\$6,690	\$2,676	
Municipal Energy	\$2,086	\$1,372	\$1,177	\$471	\$2,086
Class B/C Road Funds			\$1,803	\$721	
Total Annual Revenue per Acre	\$7,494	\$26,283	\$12,546	\$5,133	\$6,007

Source: ZPFI

Ultimately, these studies show what the market can build, what impacts the City should expect, and what property types are currently not feasible. If the non-feasible (in the market) uses are still desired by the City, various economic development tools may need to be implemented to see that use to fruition.

Market Overview and Opportunities

Industrial Development

There is currently remarkably high demand for industrial space within Davis and Weber Counties, with approximately 2.2 million square feet absorbed in 2022. The industrial vacancy rate also is extremely low at 1.3%, compared with the national average of 4.1%. As of the fourth quarter of 2022, direct vacancy has remained below 2.0% for fourteen straight quarters. Due to the current lack of projects in the construction pipeline, those rates are expected to remain low. Brokers anticipate that Hill AFB will continue to be a major driver of additional industrial space needs in the area.⁷

Because of the Freeport Center, this type of development would likely be able to be developed within the station area boundaries. Industrial flex space is also an area of interest for this area that is popular currently and may be able to fit into the station area, although it may require reworking some of the Clearfield Station Plan.

Office Development

Similar to other areas along the Wasatch Front, the office market in Davis and Weber Counties is struggling. In 2022, there was a negative absorption rate, with approximately -186,000 square feet being absorbed. This means that more commercial space was vacated in the area than what was absorbed by users. Office vacancy rates hit 8.1% throughout the area at the end of 2022.⁸

There is a sizable amount of this development planned for the Clearfield Station Area. Because of the uncertainty of the office market, there is a possibility that this type of development would struggle in the

⁷ Newmark Davis and Weber Counties Industrial Report, Q4 2022.

⁸ Newmark Davis and Weber Counties Office Report, Q4 2022.

near term in the station area. However, there are smaller speculative developments being developed in the City that may show an indication of a need in this area.⁹

Retail Development

Although this area is not planned to be a major commercial center, there are still opportunities to capture some of this growth. Strong population and employment growth are fueling the need for additional retail throughout the County. Slightly offsetting, however, are trends for more online shopping, fueled partially by the COVID pandemic, which has had a significant effect on retail brick-and-mortar space needs per capita. Average retail space needs averaged between 20 and 25 square feet per capita over 10 years ago. Today, Price Waterhouse Coopers suggests that this number has decreased to about 16 square feet.¹⁰ Based on regional growth projections, there could be demand for between 900 thousand – 1.1 million additional square feet of retail space by 2050. There is potential for the station area to capture a portion of this retail growth, both within the Clearfield Station MDP area as well as other areas throughout the boundaries of the station area.

TABLE 12: GROWTH IN RETAIL DEMAND

	2020	2030	2040	2050
Regional Population	163,531	181,921	202,997	219,775
Population Growth from Prior Period		18,390	21,076	16,778
Cumulative Growth		18,390	39,466	56,244
16 sf per capita		294,240	631,456	899,904
20 sf per capita		367,800	789,320	1,124,880

Source: ZPFI

Based on sales tax leakage data, the City has additional capacity to capture a variety of sales tax generating businesses. Although transit is not a major driver for retail, the City's position as a regional employment center creates opportunities to provide retail that supports these use types.

In Utah, the following trends are seen in retail establishments:

- Doing well – Grocery stores, automobile services, eateries, “concept” stores
- Faring poorly – Clothing stores, toy stores, jewelry stores, department stores, anything struggling with competing with online shopping

III. Housing Opportunities

Utah Code 10-9a-403.1 (7) (a) (i)

Current Conditions

Currently, the City has a varied mix of housing types, with increased construction of multi-family housing over the past several years. The following table summarizes residential units built since 2006.

⁹ Newmark Davis and Weber Counties Office Report, Q4 2022.

¹⁰ Byron Carlock, head of U.S. real estate development, Price Waterhouse Coopers

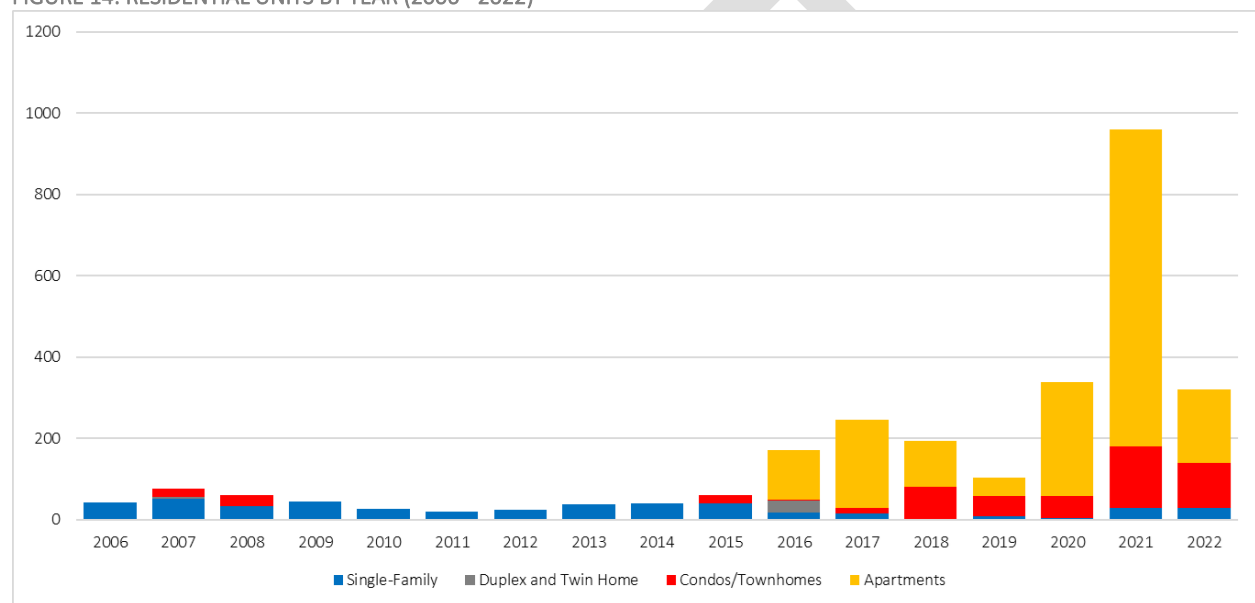
TABLE 13: RESIDENTIAL UNITS PERMITTED SINCE 2006

Building Category	Units Built
Single Family Units	461
Duplex & Twin Home Units	32
Condo & Townhome Units	532
Apartment Units	1,738
Total	2,763

Source: Ivory-Boyer Construction Database

Since 2018, the City has seen large increases in the number of units that are built in the City, with an average of around 383 units per year built in the last five years.

FIGURE 14: RESIDENTIAL UNITS BY YEAR (2006 - 2022)



Affordable Housing

Utah Code 10-9a-403.1 (7) (a) and (b)

One aspect of the station area plans is to assist in efforts to provide for or support affordable housing in the area. To determine how the station area plan may assist in these efforts, it is necessary to understand what affordability levels exist in the City. The following table provides a breakdown of the affordable monthly rent and home value for the “Low-Income” parameters set by HUD.

TABLE 14: AFFORDABLE HOUSING COSTS

	Clearfield	Davis County
Median Household Income	\$64,689	\$93,182
80% Affordability	\$51,751	\$74,546
Rent Affordable after Utilities	\$1,050	\$1,600
Affordable Home Value	\$217,000	\$335,000

Source: 2021 ACS 5-Year Estimates, ZPFI

Based on the City's median household income, a monthly rent of \$1,050 is considered affordable for "Low-Income" households. Homes that are at a price point of \$217,000 are likewise considered affordable for this group. As shown in Tables 2 and 13 above, Clearfield is more affordable than most areas in Davis County and the State.

The City has a number of rental apartment and townhome developments available within the City. According to the U.S. Census Bureau, the City has a total of 11,866 renter occupied units in the City, or approximately 38 percent of the total units in the City.¹¹

Throughout the City, there is variability in the rental rates for these units. On average, throughout the City, the median gross rent is \$1,196. On average, only the 2 bedroom and no bedroom units are considered affordable for "Low-Income" households.

TABLE 15: AVERAGE RENTAL RATES

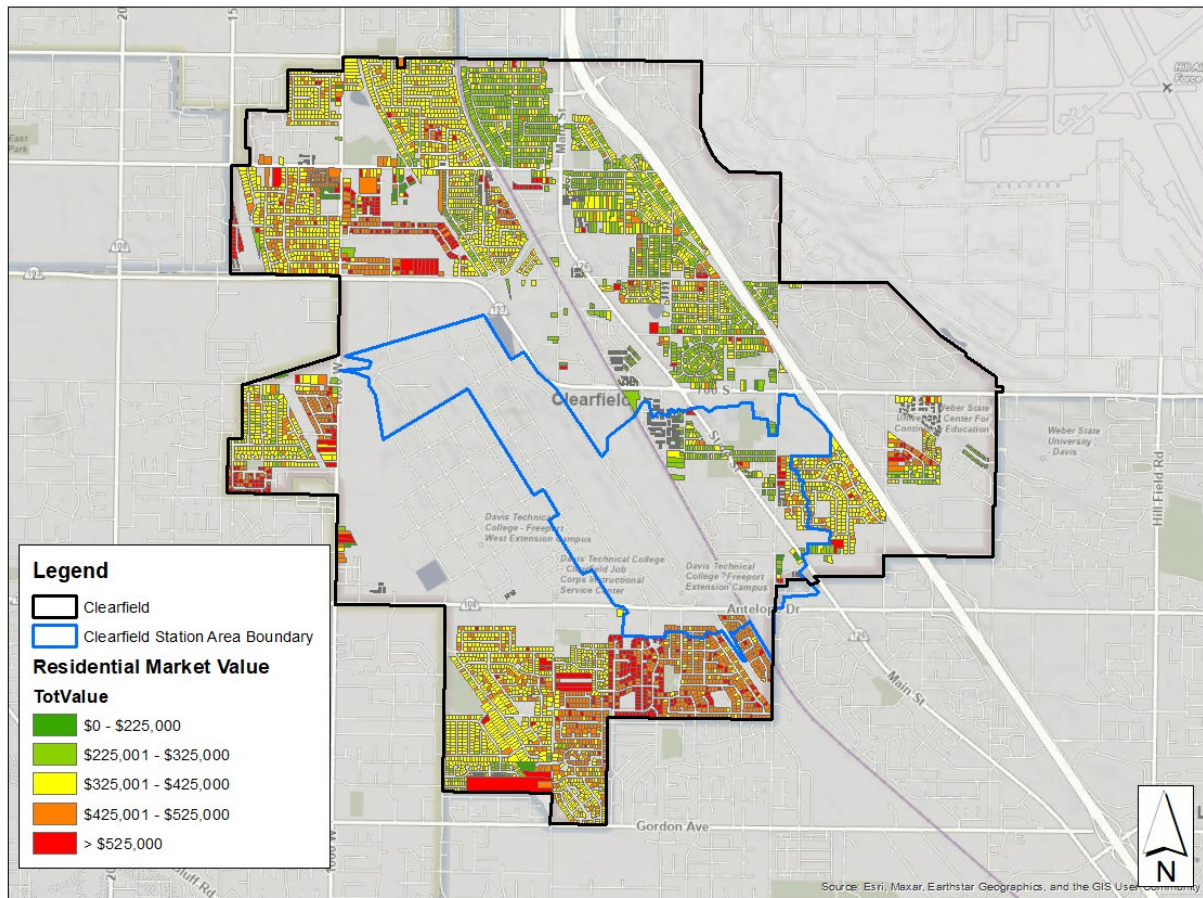
Number of Bedrooms	Median Gross Rent
No bedroom	\$866
1 bedroom	\$1,060
2 bedrooms	\$982
3 bedrooms	\$1,361
4 bedrooms	\$1,477
5 or more bedrooms	\$1,715
Median Gross Rent	\$1,196

Source: 2021 ACS 5-Year Estimates

The City has a varied mix of homes across the affordability spectrum. According to data from the Davis County Assessor's Office, areas in the northeast of the City have lower market value than homes in the south or west areas of the City.

¹¹ 2021 ACS 5-Year Estimates

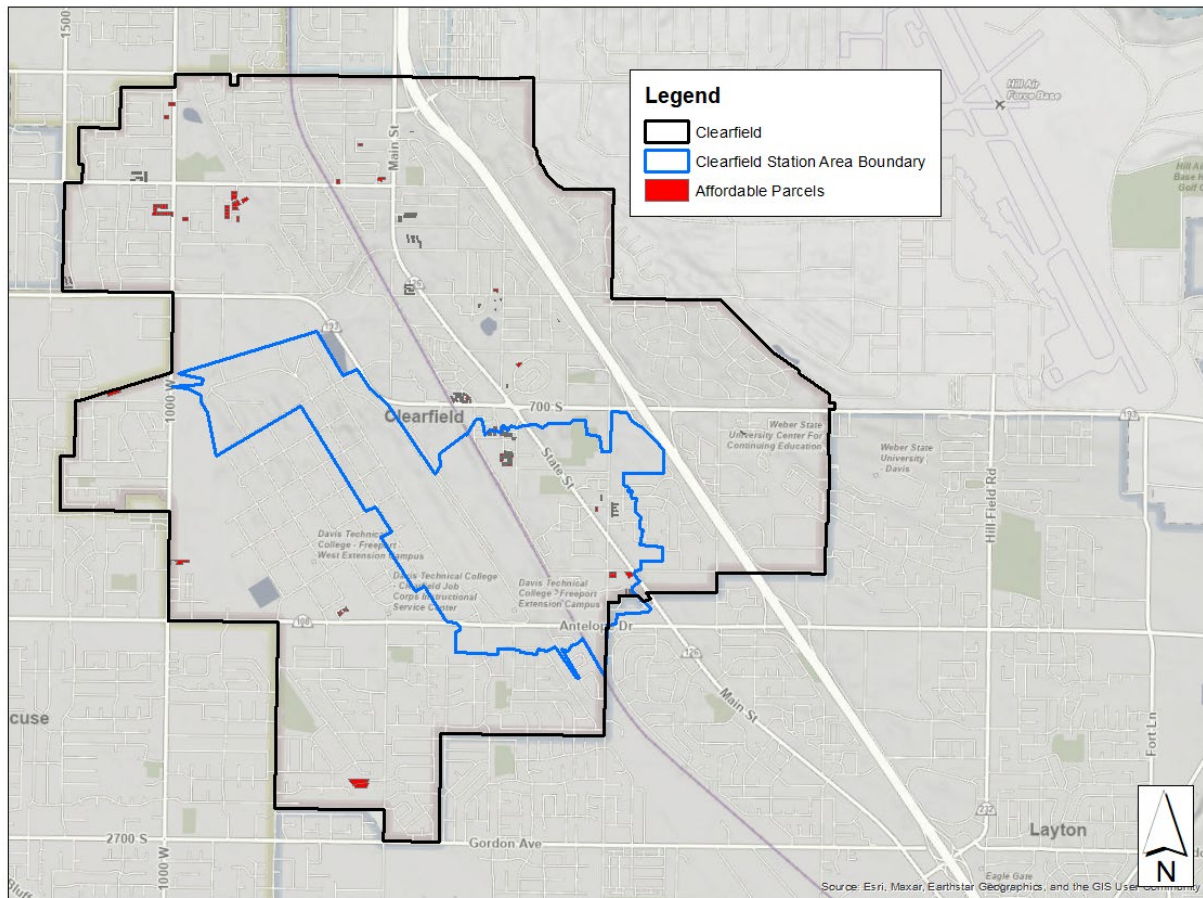
FIGURE 15: RESIDENTIAL MARKET VALUE



Based on HUD thresholds, a total of 433 parcels within the City would likewise be considered affordable. These units are primarily condominiums and townhomes, but there are a total of ten single family homes in the City that are considered affordable.¹²

¹² Based on Davis County Assessor's data

FIGURE 16: AFFORDABLE PARCELS



Currently under development, the Clearfield Station development will bring in additional residential units to the City, and potentially provide for affordable options for “Low Income” households.

Moderate Income Housing Plan

Utah Code 10-9a-403.1 (7) (b) (i) (A)

Each station area plan is required to demonstrate how it aligns with the municipality’s moderate-income housing element of the general plan.

In 2022, the City adopted an update to its Moderate-Income Housing Plan. Three of the City’s strategies relate to efforts within the station area:

- *Action Item #2: Clearfield City will ensure zoning designations allow for higher density and/or moderate-income housing development in the mixed-use Downtown, near Clearfield Station, and adjacent to commercial and employment centers.*
- *Action Item #4: Clearfield City will implement goals and objectives from creating Clearfield Downtown Small Area plan to implement centers and create areas of focus along major transit corridors which include the Downtown Form Based Code area and the Clearfield Station site.*
- *Action Item #11: Clearfield City will update the Station Area Plan for Clearfield Station*

Additionally, the City has a demonstrated commitment to a variety of housing types. Since 2016, the City has approved 464 condo/townhome units and 1,738 apartment units, far outpacing the number of single-family units built.

DRAFT





STAFF REPORT

To: Mayor Shephard and City Council Members
From: Brad McIlrath, Senior Planner
Meeting Date: Tuesday, May 28th, 2024
Subject: Discussion on Planning Commission Appointment

RECOMMENDED ACTION

Based upon the discussion with the Mayor and City Council at the May 14th, 2024 work session, Staff recommends that the City Council appoint Commissioner Brian Swan to complete the term vacancy created with the resignation of Commissioner Lauren DeSpain. Her term expires in February 2026 and consideration for reappointment will occur at that time.

DESCRIPTION / BACKGROUND

In April 2024, Commissioner Lauren DeSpain resigned from the Planning Commission due to a move out of state. Commissioners Swan and Sikes were appointed as Planning Commission Alternates in February 2024. Both commissioners have served well as alternates, and each has had the opportunity to fill in on the dais. Neither has yet to miss a meeting in the first three months of their service and both attended introductory training with Planning Staff. Commissioner Swan has aided the commission with valuable feedback on proper meeting conduct and parliamentary procedures. Commissioner Sikes provided insightful feedback regarding tree regulation changes with a recent text amendment. Both are attentive, respectful, and responsive. When appointed, the person will complete the remainder of Commissioner DeSpain's term which expires in February 2026. The current terms of the alternate commissioners can be seen in the table at the end of this report.

Commissioner DeSpain was serving as the vice-chair at the time of her resignation. As outlined in the bylaws, the Chair Fullmer appointed Commissioner Browning an interim vice-chair on May 1, 2024 until the appointment could be completed. Following the appointment of one of these commissioners, and as outlined in the bylaws, there will be an election for vice-chair for the remainder of the calendar year.

CORRESPONDING POLICY PRIORITY

The appointment from an alternate to a full member of the Planning Commission supports the policy priority of "Providing Quality Municipal Services" by placing interested and experienced community members in a position of responsibility. These residents provide valuable insight and experience that will continue to have a positive impact on the city as they continue to serve on the Planning Commission.



ALTERNATIVES

Each commission member is appointed by the Mayor, subject to the advice and consent of the City Council. The Mayor and City Council may decide to not appoint either alternate commissioner and seek applications from the general public to fill this vacancy.

ATTACHMENTS

TERM EXPRIATION	NAME - Role	STATUS
February 2029	Chad Mortensen	
February 2029	Kathryn Murray	
February 2026	Lauren DeSpain – Vice Chair	Resigned April 2024
February 2025	David Bloomfield	
February 2028	Brogan Fullmer - Chair	
February 2027	Robert Browning	
February 2025	Riley Wheeler	
February 2027	Brian Swan – Alternate	
February 2026	Danielle Sikes – Alternate	
June 2024	Jaylee Bouwhuis – Youth Commission Ambassador	



STAFF REPORT

TO: Mayor Shepherd and City Council Members

FROM: Adam Favero, Public Works Director

MEETING DATE: May 21, 2024

SUBJECT: Third & Fourth phases of the MOC Contract Award _ Project #222

RECOMMENDED ACTION

Approve awarding the bid for the Third & Fourth Phases of the Maintenance & Operations Center (MOC) to Stout Building Contractors, LLC. of Bountiful, Utah for the bid amount of \$10,290,896.00 minus a value engineering amount of \$234,000.00 for a revised bid amount of \$10,056,896.00 with contingency, engineering, and FF & E cost of \$ 868,000.00 for a total project amount of \$10,924,896.00 and authorize the Mayor's signature on all necessary documents.

DESCRIPTION / BACKGROUND


In February of 2022 the city council approved an agreement between Clearfield City and Think Architecture to complete a comprehensive design and plan set to construct the third and fourth phases of the MOC. The phases were identified in a Facility Needs Assessment that was produced in September 2012. The assessment identified four phases for modifying or expanding the existing facilities in the Public Works and Parks operational areas. To date, the City has completed phases one and two. As part of the 2012 Facility Needs Assessment this project will conclude the needs addressed in the study.

The proposed project includes the construction of an Operation Building, Equipment Storage Building, and the site work including a Decant Facility, Material Storage bunkers, and an employee parking lot. The Operation Building is designed to handle all of the day-to-day functions of the Public Works Department's operations including utilities, streets, and fleet.

Staff have strived to be forward-thinking in the use, functionality, and long-term planning of the Public Works Department while designing an efficient facility. The scope of the project includes:

Operations Building

- Approximately 20,000 square feet
- A large break room that is designed to accommodate training sessions holding about 60 chairs
- Office space for 10 work spaces and a conference table

- 
- A water testing room
 - A laundry room
 - A bath/locker room
 - 3 mechanical bays for fleet services
 - 5 operations bays for Public Works
 - A fabrication room
 - Storage for current and future needs
 - Meter and backflow service area
 - Bulk oil storage and distribution room

Storage Building

- Approximately 9,800 square feet
- 7 storage bays


Site

- Employee parking lot
- Expanded visitors parking
- Decant facility
- Material storage bunkers
- Future EV charging area
- Additional yard lighting
- Additional landscaping

Collaboration included input from Public Works Admin and Staff, Community Services Admin, Clearfield City Admin, and the Finance Division.

The project was advertised for bid using the RFP process. Contractors that intended to submit a proposal were required to attend a mandatory pre-proposal meeting. There were 15 companies in attendance at the pre-proposal meeting. Of those 15 companies, 3 submitted a proposal. Of the 3 proposals, only 2 were considered due to a late (therefore, non-responsive) proposal. The 2 companies evaluated were Stout Building Contractors, and Valley Design & Construction.

The selection committee consisted of Adam Favero, Braden Felix, JJ Allen, Eric Howes, Rich Knapp, Brandon Jones, and Jim Poloncic. The proposals were ranked based on the following requirements:



Item	Does Not Meet Requirements	Meets Requirements	Exceeds Requirements	Total Possible Points
Project Team & Key Personnel	0	5-15	16-25	25
Project Experience & References	0	5-15	16-25	25
Proposal Schedule	0	1-3	4-5	5
Project Approach & Innovative Ideas	0	1-3	4-5	5
Risk Mitigation	0	1-5	6-10	10
Project Bid*	0-9	10-19	20-30	30
		Total Points Possible		100

**The points assigned to each offeror's project bid will be based on the lowest bid received from all offers. The offeror with the lowest bid will receive 100% of the cost points. All other offerors will receive a portion of the total cost points based on what percentage higher their bid price is than the lowest bid. The formula to compute the points is: Project Bid Points x (Lowest Bid/Project Bid).*

Based on the evaluation table requirements the committee scored Stout Building Contractors at 91.2 and Valley Design & Construction at 86.5. The selection committee's proposal evaluation results are an attachment to this staff report. Breaking down the individual scores from the selection committee members, the two companies score relatively close with a few minor exceptions. The score with the greatest discrepancy was the Project Bid. Stout Building Contractors bid came in about \$2.125 million less than Valley Design & Construction. Both companies were more than professional during the selection process and the project team has enjoyed working with each of them. Considering the results of the selection process, staff recommends awarding the contract to Stout Building Contractors.

CORRESPONDING POLICY PRIORITIES

- Providing Quality Municipal Services
 - It is imperative that the city is planning for current and future needs. To provide the quality services that the community depends on every day, the city needs to make sure the facilities are designed to meet the city's needs.
- Maintaining a Highly Motivated and Well-Trained Workforce
 - Providing our community and employees with equipment and facilities that are safe and adequate are key to creating a place where city residents and staff want to be.

HEDGEHOG SCORE



FISCAL IMPACT

In the FY 25 budget process, the City Council has been working closely with staff to provide funding for the project. There have been multiple meetings between everyone involved in this project including staff, city council, outside engineering, the project architect, and the contractor. Staff recognizes the importance and value of this project and understands the financial impact it has on the community. For these reasons, it has been a top priority to design and work through this process with a conservative mindset.

Contractor's Bid Amount	\$ 10,290,896.00
Design and Engineering	\$ 443,000.00
Contingency	\$ 300,000.00
Furniture, Fixtures & Equipment	\$ 110,000.00
3rd Party Utility Connections	\$ 15,000.00
Value Engineering (to date)	\$ (234,000.00)
Total Project Cost	\$ 10,924,896.00
Project Budget	\$ 10,925,000.00

SCHEDULE / TIME CONSTRAINTS

The project team is currently working with Stout Building Contractors to ensure the project stays on schedule to avoid unnecessary costs associated with winter conditions.

LIST OF ATTACHMENTS

- Third & Fourth Phase of the MOC Construction Power Point
- PW Complex General Contractor RFP Evaluations

Third Phase of the MOC Design Update





Third & Fourth Phase of the MOC Construction

- **Needs and Challenges of Current Facilities**
- **2- Building Design Features**
- **Site Plan**
- **Contractor Selection**
- **Budget**
- **Schedule**



Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current facilities-

Mechanics Shop

- Building is not aging well. Cracks in the brick wall.
- Not adequately sized for current equipment.
- Electrical power is fully built out. Does not meet current code.
- Several leaks in the roof throughout the building.
- The floor pit does not meet OSHA requirements.
- Overall storage needs, currently storing items on the floor.
- The floor drains are not properly connected to the sanitary sewer system.



Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current Facilities-

Operations Building

- The department has outgrown the current operation building. (vehicle storage, operational needs, parts storage, office space, etc.)
- The bathroom/laundry/locker/changing room.
- The floor drains are not properly connected to the sewer system.
- The intent of the building was temporary as it was a used building when reconstructed over 30 years ago. Adjustments were made to make it work.
- The additional space will provide for more training opportunities, increase efficiency, and provide the facility needed to perform the basic and complex function required of the department (meter testing, manufacturing of street signs, water quality functions, fabrication needs, etc.)



Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current Facilities- Site

- Employee and visitor parking (additional and separated)
- Separation of “public” vs “private” operation areas.
- Provide a planned approach to site. (see video)
- Additional yard lighting
- Provide Decant Facility and Storage as required by our audit by the Division of Water Quality in 2019
- Future EV charging area



Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current Facilities-



Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current Facilities-



Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current Facilities-



Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current Facilities-



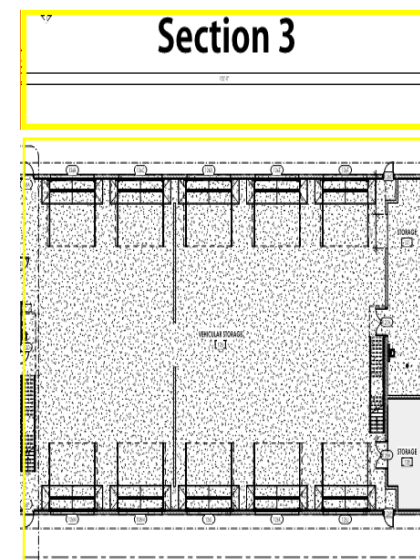
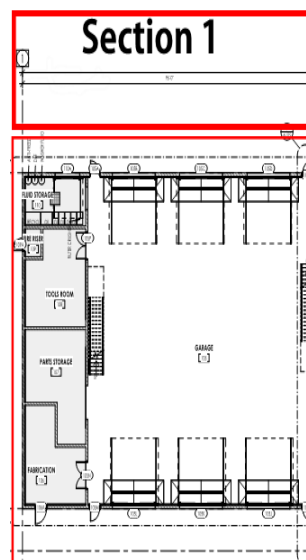
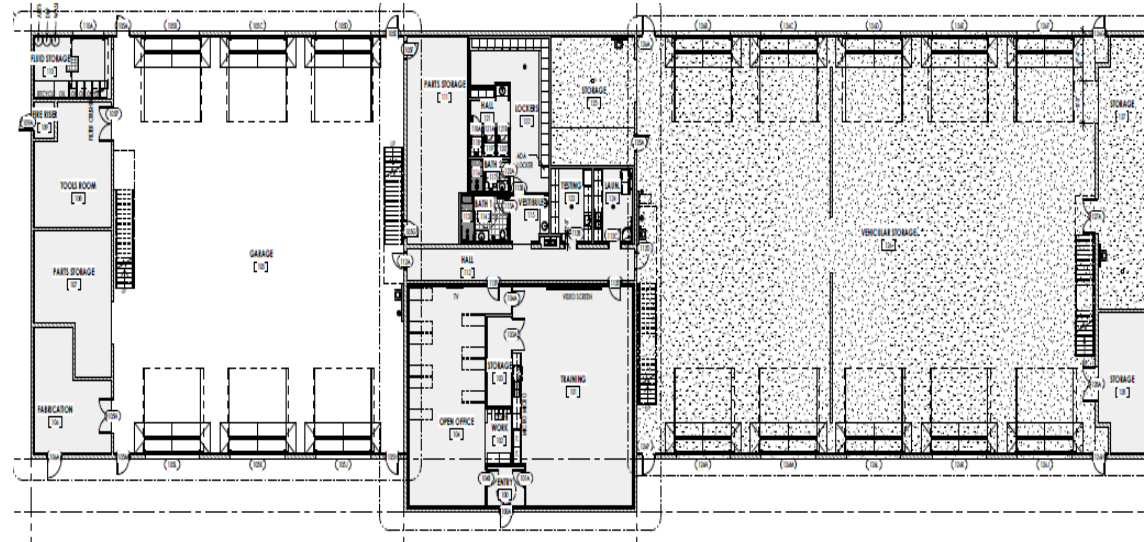
Third & Fourth Phase of the MOC Construction

Needs and Challenges of Current Facilities-



Third & Fourth Phase of the MOC Construction

Building Design Features-



Third & Fourth Phase of the MOC Construction

Building Design Features-

Fluid Storage: Designed to comply with EPA Standard and to provide employees a safe work area.

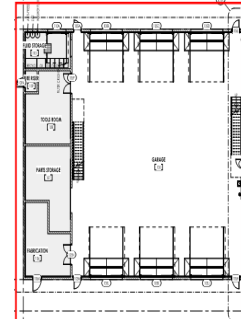
Mechanic Bays: Design to provide our mechanics with an economical and functional work area. Planning and design easy access to frequently need equipment and supplies is key to this area.

Key Features: 4 lifts, Charging Station, Quick lubrication, Floor drains, Part storage, Radiant heating, Multi purpose unit, Compressed air supplies, adequate power supplies, electric/manual over head doors, etc.

Metal Fabrication: Designed to comply with Building Codes and to provide employees a safe work area.

Sink and Emergency Eye Wash Station

Section 1



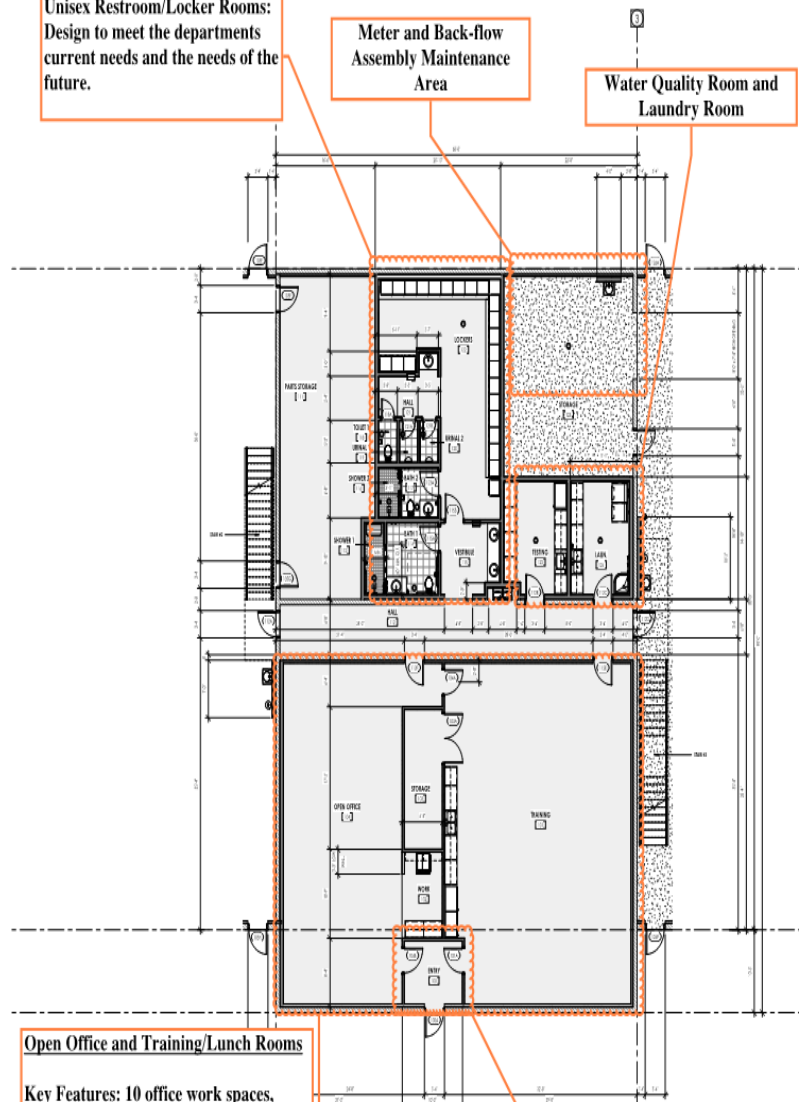
Third & Fourth Phase of the MOC Construction

Building Design Features-

Unisex Restroom/Locker Rooms:
Design to meet the departments current needs and the needs of the future.

**Meter and Back-flow
Assembly Maintenance
Area**

**Water Quality Room and
Laundry Room**

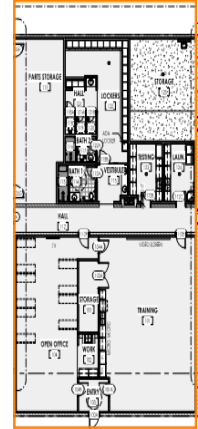


Open Office and Training/Lunch Rooms

Key Features: 10 office work spaces, Collaboration area, Large TV monitors, printer and doc. storage area, kitchenette, soda machine, 60 persons training area, etc.

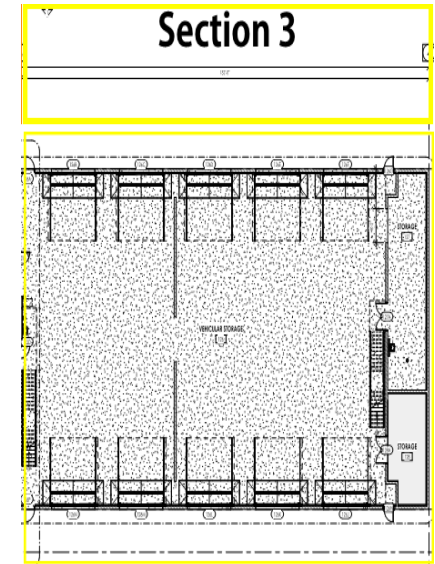
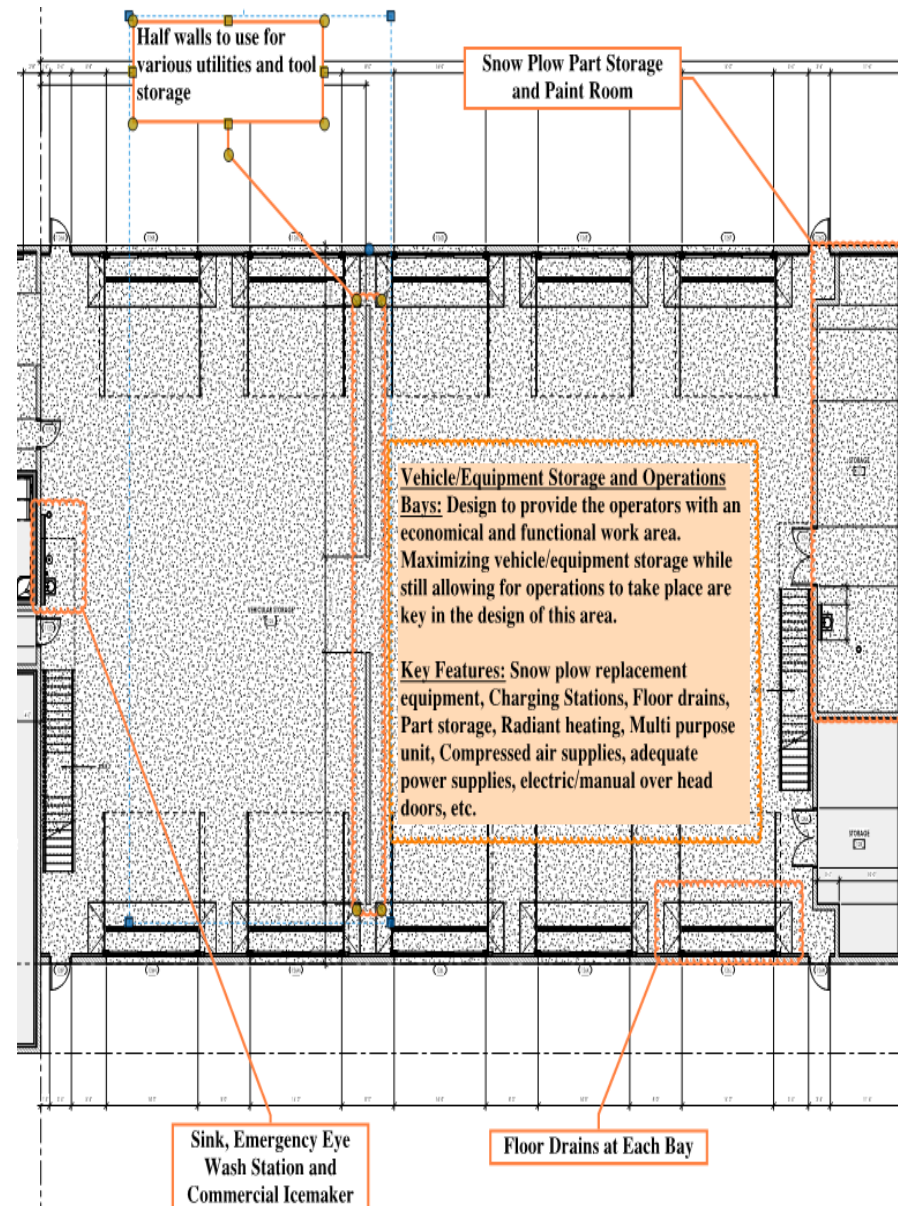
Vestibule: Added to reduce heat and cool air loss. It will also add aesthetics to the look of the building to tie the look into the existing building.

Section 2



Third & Fourth Phase of the MOC Construction

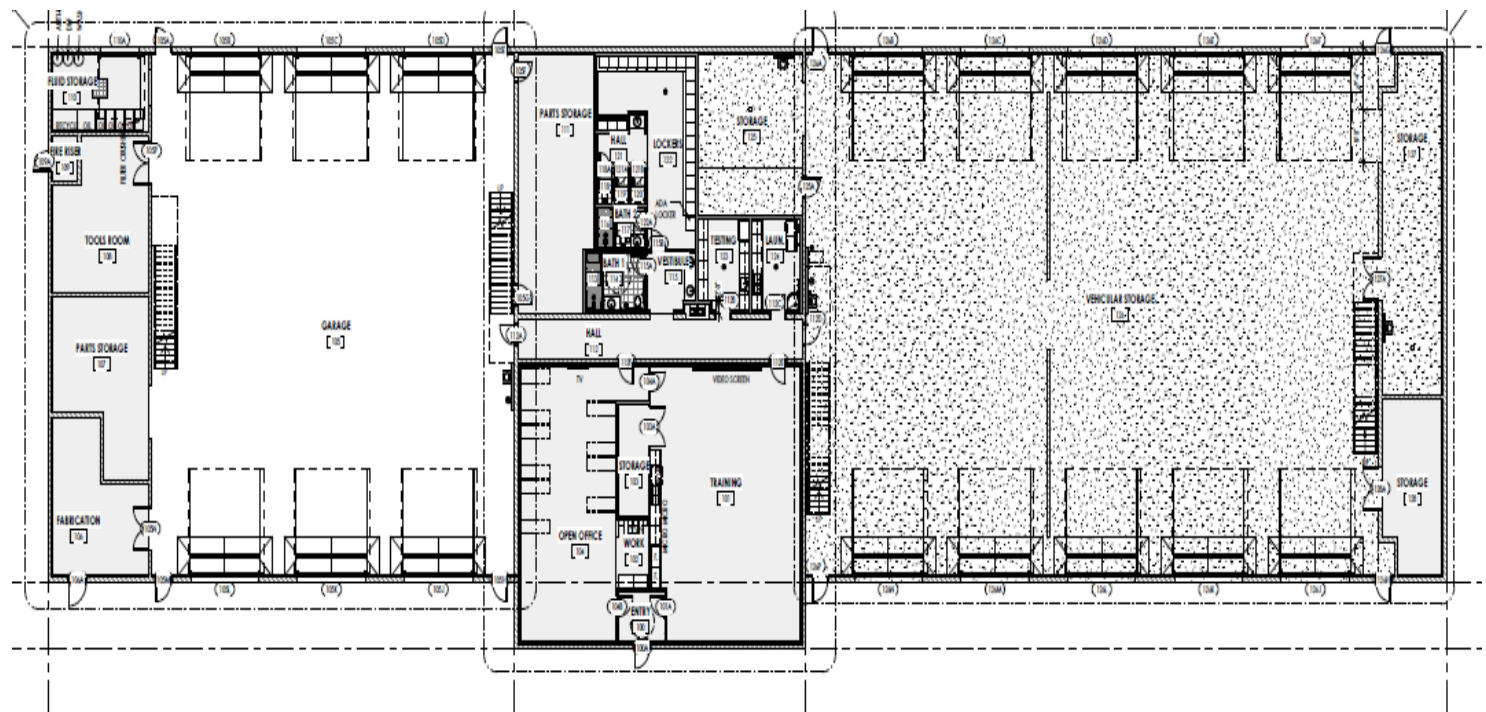
Building Design Features-



Third & Fourth Phase of the MOC Construction

Building Design Features-

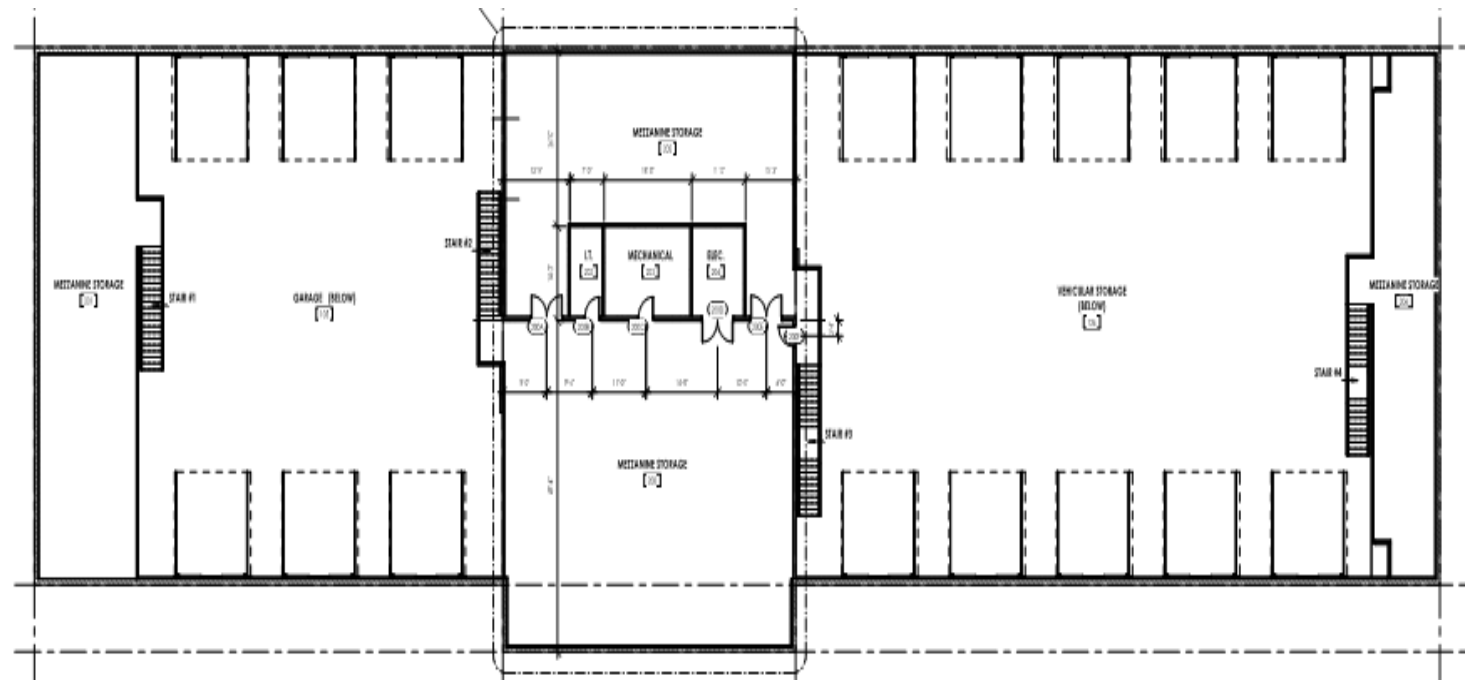
First Level



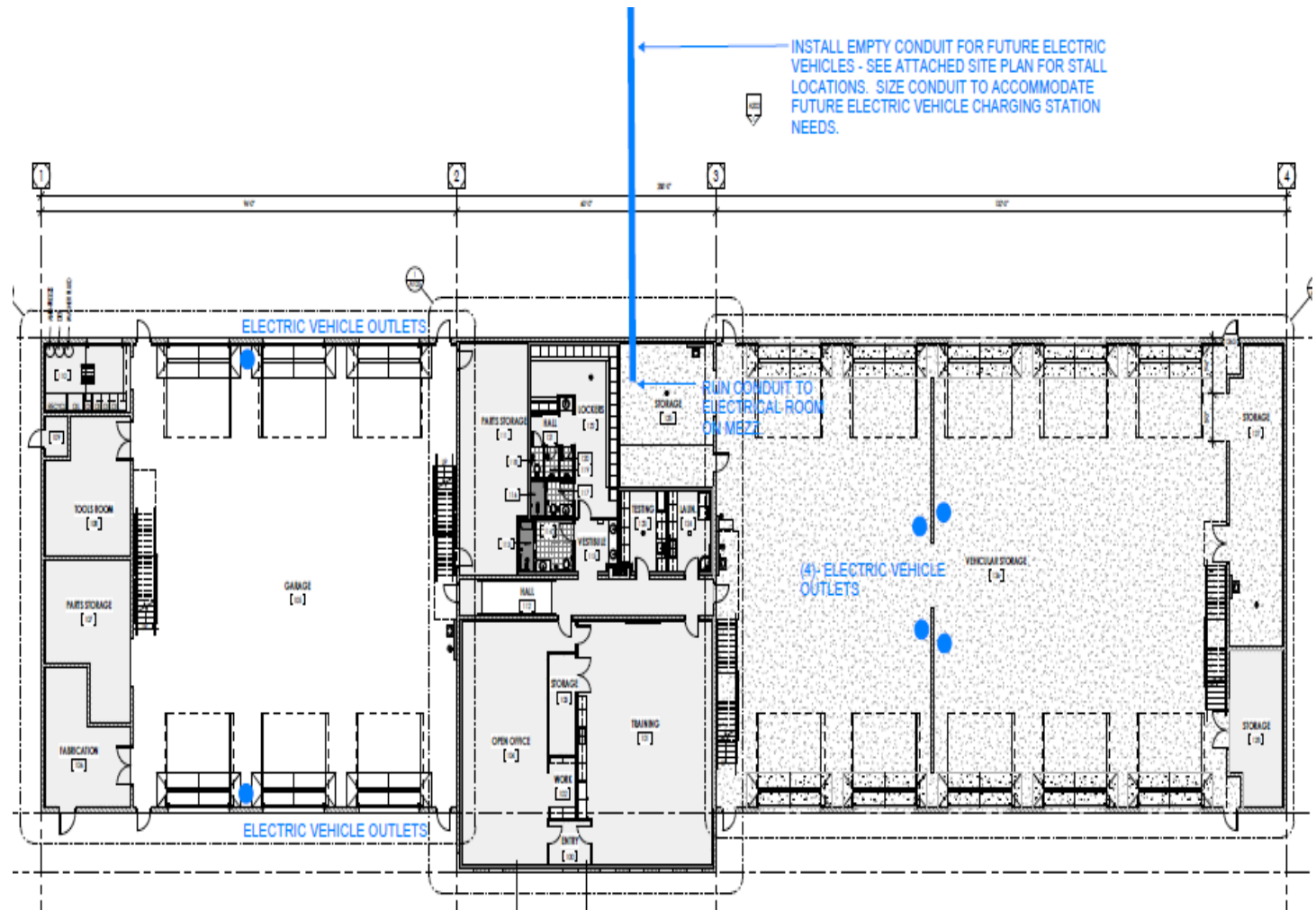
Third & Fourth Phase of the MOC Construction

Building Design Features-

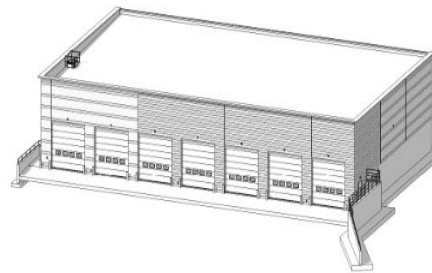
Second Level



Building Design Features-



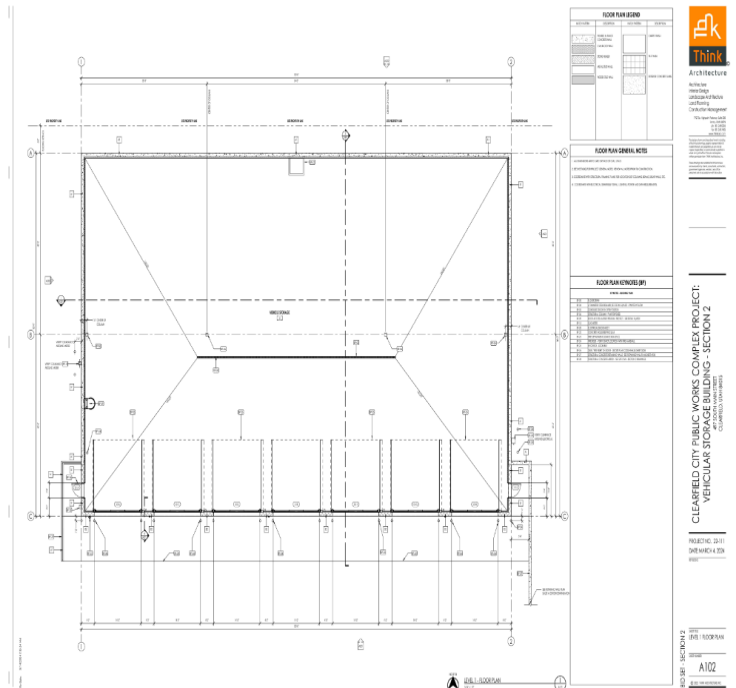
Building Design Features-



CLEARFIELD CITY PUBLIC WORKS COMPLEX PROJECT
VEHICULAR STORAGE BUILDING - SECTION 2

QUARTER 1			
ITEM		REVENUE	
1	ITEM 1	1	REVENUE 1
2	ITEM 2	2	REVENUE 2
3	ITEM 3	3	REVENUE 3
4	ITEM 4	4	REVENUE 4
5	ITEM 5	5	REVENUE 5
6	ITEM 6	6	REVENUE 6
7	ITEM 7	7	REVENUE 7
8	ITEM 8	8	REVENUE 8
9	ITEM 9	9	REVENUE 9
10	ITEM 10	10	REVENUE 10
11	ITEM 11	11	REVENUE 11
12	ITEM 12	12	REVENUE 12
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95	ITEM 95	95	REVENUE 95
96	ITEM 96	96	REVENUE 96
97	ITEM 97	97	REVENUE 97
98	ITEM 98	98	REVENUE 98
99	ITEM 99	99	REVENUE 99
100	ITEM 100	100	REVENUE 100

PROJECT TEAM		SKL	MEMBERING BUILDING CODES & INFORMATION	ABBREVIATIONS	GRAPHIC SYMBOLS MATERIAL LEGEND	NOTES
	ARCHITECT THEIR ARCHITECTURE 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.THEIRARCHITECTURE.COM		STRUCTURAL ENGINEER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM	MECHANICAL ENGINEER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM	ELECTRICAL ENGINEER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM	CIVIL ENGINEER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM
	MECHANICAL ENGINEER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM		ELECTRICAL ENGINEER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM	CIVIL ENGINEER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM	LANDSCAPE ARCHITECT KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM	OTHER KIMLEY-HORN 10000 10TH AVENUE DENVER, CO 80202 TEL: 303.733.1000 WWW.KIMLEY-HORN.COM

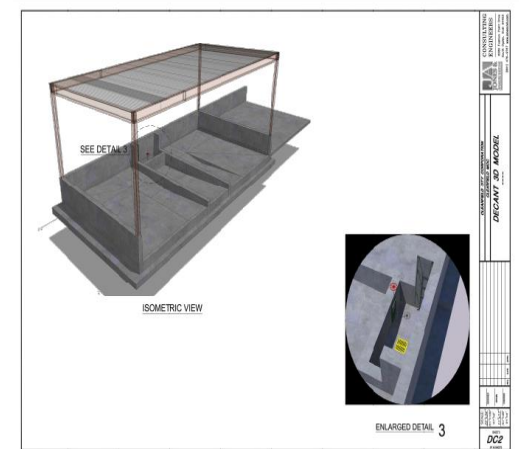
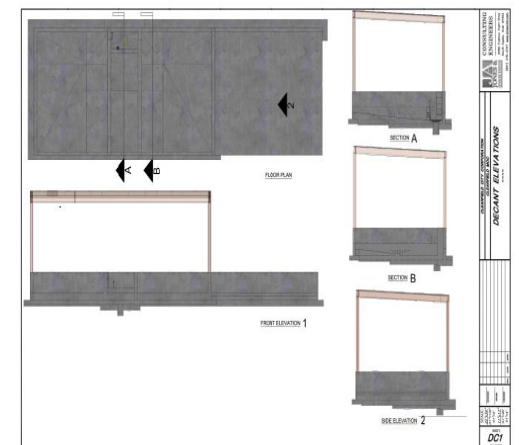


Site Plan-



Third & Fourth Phase of the MOC Construction

Site Plan-





Third & Fourth Phase of the MOC Construction

Contractor Selection-

- We decided to do a Request for Proposal (RFP) process to better select a proficient contractor
 - The evaluation was based on Personnel, Experience, Schedule, Project Approach, Risk Mitigation, and Bid Cost
- Pre-proposal meeting
 - 15 contractors were in attendance 3 submitted proposals and 2 were evaluated.
- Selection Committee
 - Adam Favero, Eric Howes, Braden Felix, JJ Allen, Rich Knapp, Jim Poloncic, & Brandon Jones
- After receiving initial proposals, both over budget, we met with each of them to discuss suggestions for cost savings
 - Contractors indicated that the project was economically designed
 - Contractors received 300 - 400 subcontractor bids for this project
- Contractor Evaluation
 - Stout Building Contractors 91.2
 - Valley Design & Construction 86.5

Third & Fourth Phase of the MOC Construction

Contractor Selection-

Clearfield City

Proposals Deadline: April 23, 2024

Public Works Complex - General Contractor Services RFP

Proposal Evaluation Summary

		Stout	VDC
Project Team & Key Personnel (25)	Reviewer #1	25	23
	Reviewer #2	17	19
	Reviewer #3	23	25
	Reviewer #4	22	20
	Reviewer #5	24	22
	Reviewer #6	23	22
	Average =	22.3	21.8
Project Experience & References (25)	Reviewer #1	25	23
	Reviewer #2	18	20
	Reviewer #3	23	25
	Reviewer #4	22	23
	Reviewer #5	23	23
	Reviewer #6	20	15
	Average =	21.8	21.5
Proposed Schedule (5)	Reviewer #1	4	5
	Reviewer #2	5	5
	Reviewer #3	5	5
	Reviewer #4	5	4
	Reviewer #5	5	5
	Reviewer #6	5	4
	Average =	4.8	4.7
Project Approach & Innovative Ideas (5)	Reviewer #1	5	5
	Reviewer #2	4	3
	Reviewer #3	5	5
	Reviewer #4	4	5
	Reviewer #5	5	5
	Reviewer #6	5	5
	Average =	4.7	4.7
Risk Mitigation (10)	Reviewer #1	6	8
	Reviewer #2	6	8
	Reviewer #3	10	8
	Reviewer #4	10	10
	Reviewer #5	8	9
	Reviewer #6	5	10
	Average =	7.5	8.8
Project Bid (30)	Reviewer #1	30	25
	Reviewer #2	30	25
	Reviewer #3	30	25
	Reviewer #4	30	25
	Reviewer #5	30	25
	Reviewer #6	30	25
	Average =	30.0	25.0
Total Average Points		91.2	86.5
RANK		1	2

Third & Fourth Phase of the MOC Construction

Budget-

Contractor's Bid Amount	\$	10,290,896.00
Design and Engineering	\$	443,000.00
Contingency	\$	300,000.00
FF & E	\$	110,000.00
3rd Party Utility Connections	\$	15,000.00
Value Engineering (to date)	\$	(234,000.00)
Total Project Cost	\$	<u>10,924,896.00</u>
Project Budget	\$	<u>10,925,000.00</u>



Third & Fourth Phase of the MOC Construction

Schedule-

- **City Council Work Session:** **5/21/2024**
- **City Council Policy Session:** **5/28/2024**
- **Groundbreaking Ceremony** **?????**
- **Construction Phase:** **Estimated 294 days (10 months plus)**
- **Completion Date:** **Summer of 2025**

** Construction and completion dates are subject to change and are estimates only.*

Public Works Complex - General Contractor Services RFP

Proposal Evaluation Summary

		Stout	VDC
Project Team & Key Personnel (25)	Reviewer #1	25	23
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	Reviewer #3	23	25
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	Reviewer #3	30	25
	Reviewer #4	30	25
	Reviewer #5	30	25
	Reviewer #6	30	25
	Average =	30.0	25.0
Total Average Points		91.2	86.5
RANK		1	2

CLEARFIELD CITY ORDINANCE 2024-09

AN ORDINANCE APPROVING AN UPDATE TO THE “CLEARFIELD CONNECTED STATION AREA PLAN AND DESIGN GUIDELINES” TO ADDRESS NEW LEGISLATION FROM 2022 REQUIRING THE PLAN TO ADDRESS FUTURE DEVELOPMENT VISIONS FOR PROPERTIES WITHIN ONE-HALF MILE OF THE TRANSIT ORIENTED DEVELOPMENT LOCATED AT APPROXIMATELY 1250 SOUTH STATE STREET IN CLEARFIELD, DAVIS COUNTY, UTAH

WHEREAS, Clearfield City created the “Clearfield Connected Station Area Plan and Design Guidelines” in 2019 to address transit oriented development in and around the current Clearfield Station Frontrunner property located at approximately 1250 South State Street; and

WHEREAS, pursuant to changes made by the Utah Legislature in 2022, requiring cities to address the development of property withing one-half mile of such developments; and

WHEREAS, Clearfield City hired a consultant with the help of Wasatch Regional Council to assist in the efforts to update the “Clearfield Connected Station Area Plan and Design Guidelines” in compliance with the new State Law; and

WHEREAS, after a public hearing on the matter, the Clearfield City Planning Commission recommended to the Clearfield City Council that updated “Clearfield Connected Station area Plan and Design Guidelines” be approved; and

WHEREAS, following proper notice, as set forth by state law, the City Council held a public hearing on the updated plan and allowed for public comment thereon; and

WHEREAS, after the public hearing, the City Council carefully considered any comments made during the public hearing, the requirements of 2022 Legislative action, as well as the Planning Commission’s recommendations regarding the proposed updated plan; and

WHEREAS, following its public deliberation, the City Council has determined that the proposed updates to the “Clearfield Connected Station Area Plan and Design Guidelines” is in the best interests of Clearfield City and its residents and will most effectively implement the City’s planning efforts while allowing the subject properties to be put to their highest and best use;

NOW THEREFORE BE IT ORDAINED by the Clearfield City Council that:

Section 1. Development Agreement: The updated “Clearfield Connected Station Area Plan and Design Guidelines” is approved and attached as Exhibit ‘A’.

Section 2. Effective Date: This Ordinance shall become effective upon being posted in three public places within Clearfield.

Dated this 28th of May, 2024, at the regularly scheduled meeting of the Clearfield City Council.

CLEARFIELD CITY CORPORATION

Mark R. Shepherd, Mayor

ATTEST

Nancy R. Dean, City Recorder

VOTE OF THE COUNCIL

AYE:

NAY: