



STAFF REPORT

To: Summit County Council (SCC)
From: Amir Caus, County Planner
Date of Meeting: July 16, 2014
Type of Item: Newpark Development Agreement Amendment - Public Hearing, Possible Action
Process: Legislative Review

RECOMMENDATION: Staff has reviewed the application for compliance with all standards in the Snyderville Basin Development Code and the Newpark Development Agreement (Development Agreement) and has found that it meets the minimum required for approval. Accordingly, staff recommends that the Summit County Council review the proposed Development Agreement Amendment, conduct a public hearing and vote to approve the proposed Development Agreement to delete a portion of Center Drive, pursuant to the findings of fact, conclusions of law and conditions of approval found in this staff report.

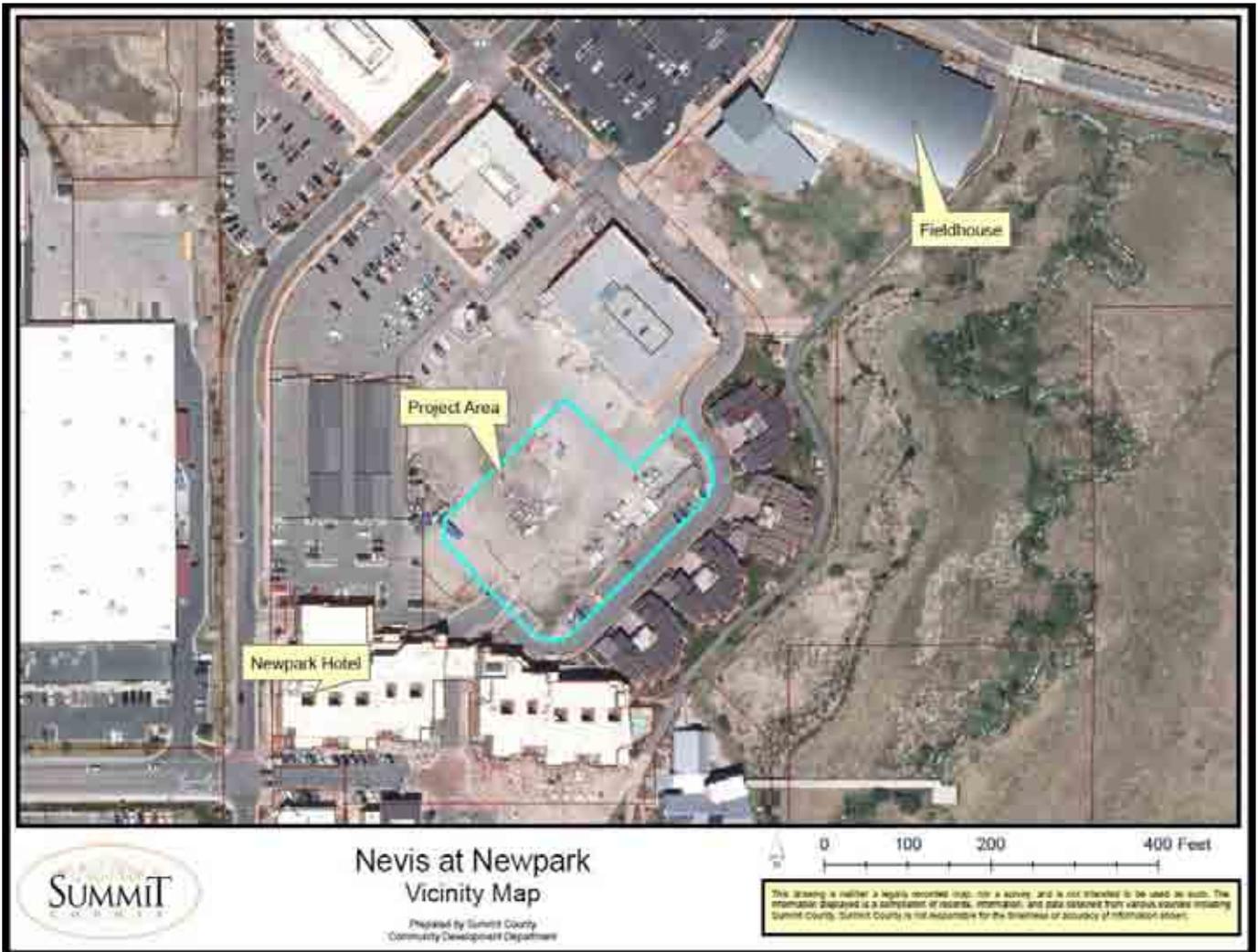
Project Description

Project Name: Newpark Development Agreement Amendment
Applicant(s): Michael Brodsky
Property Owner(s): Cottonwood Partners
Location: Parcel P-2, Newpark, Summit County, Utah
Zone District: Town Center (TC)
Parcel Number and Size: Parcel # NPRK-P-2, 1.20 Acres
Type of Process: Legislative (Development Agreement Amendment)
Administrative (Final Plat and Final Site Plan)
Final Land Use Authority: Summit County Council (Development Agreement Amendment)
Summit County Manager (Final Plat and Final Site Plan)

Proposal

The applicant is requesting that the SCC approve the Newpark Development Agreement Amendment to delete the portion of Center Drive designated on Parcel NPRK-P-2 to allow for the Nevis at Newpark Final Plat and Final Site Plan. The proposed Final Plat and Final Site Plan would allocate 29,041 sq. ft. of residential density from the overall Newpark density pool onto Parcel NPRK-P-2, Newpark, Summit County, UT.

Vicinity Map



Background

The Newark Specially Planned Area (SPA) and Newark Development Agreement were approved in October, 2001 and amended in December, 2002. The SPA resulted in the approval of 819,360 sq. ft. of density on the ~37 acre site.

The development perimeters for this project are specifically set forth in the Development Agreement.

The proposed project consists of 29,041 sq. ft. of residential density, configured in 23 units, a parcel used for Cottonwood III parking, and a common area parcel. There are 19 proposed 1,255 sq. ft. units and 4 proposed at 1,299 sq. ft.

Analysis and Findings

There is 76,360 sq. ft. of remaining density for the Newpark Town Center. If the current 29,041 sq. ft. proposal is approved and recorded, the remaining density would be 47,319 sq. ft., making the project over 94% complete.

Parking and Pedestrian Walkability

Each new development in the Newpark Town Center is required to submit a parking study to the Summit County Engineering Department (Engineering Department) to ensure that public health, safety, and welfare is protected.

The Engineering Department did not raise any issues with the submitted parking study. Each unit will include a single-car garage and a driveway parking space. Parcel P-3 is planned for additional parking to be used by the tenants of Cottonwood III. 12 additional spaces will be provided for the Newpark Townhomes as per a parking agreement recorded in 2012. The parking study can be found in Exhibit B

The pedestrian walkability and connection was of a greater concern with the lack of direct connection from the Rosignol and Cottonwood III buildings towards Newpark Hotel, however an updated study has been provided and the Engineering Department is satisfied with the proposed solution. The Engineering Department Memo can be found in Exhibit D for further analysis.

Design Review Committee (DRC)

According to the DA, Final Site Plans and Final Subdivision/Condominium Plats are required prior to the development of each parcel and shall first be reviewed by the DRC. It is required that the DRC be made up of County Planning Staff, SBPC members chosen to represent the Planning Commission, and representatives of the Developer. The DRC was established to allow a more detailed, intense, and interactive review of the projects.

The DRC met on multiple occasions with regard to the subject proposal and has reviewed the proposed Condominium Plat and Final Site Plan which includes items such as, but not limited to design, height, lighting, parking, landscaping, and materials.

The DRC voted unanimously to forward a positive recommendation to the SBPC.

Additionally, Staff has found that the proposed project complies with the DA.

Attorney's Office

During the review, the Summit County Attorney's Office was concerned on whether the removal of the portion of the Center Drive would take away a public benefit, or impact the overall transportation plan for the Snyderville Basin. The applicant's transportation engineer

has submitted a revised report. The Summit County Engineer has met with the transit authority and there were no concerns over the removal of the subject portion of Center Drive, which hasn't been improved yet. The Summit County Engineer is also satisfied with the report and does not find a significant impact will be made. It should be noted that the original retail and restaurant uses that were intended to go north of the Newpark Hotel were substituted by office uses and the necessity for an additional thoroughfare does not exist.

Development Agreement Requirements

The Development Agreement delegates decision making powers to the Board of County Commissioners for all Final Plats, Final Site Plans, and Substantial Development Agreement Amendments. With the change of government that took place, the former Board of County Commissioner's administrative duties diverted to the County Manager and the legislative duties diverted to the County Council. The Final Plat and Final Site Plan are considered administrative duties while a Substantial Development Agreement is considered a legislative one.

On May 27, 2014, the Snyderville Basin Planning Commission forwarded a positive recommendation to the Summit County Council for the proposed Development Agreement Amendment and to the Summit County Manager for the proposed Nevis at Newpark Final Plat and Final Site Plan.

Recommendation

It is staff's finding that the amendment request meets the applicable standards in the Snyderville Basin Development Code and the Newpark Development Agreement. Staff recommends that the Summit County Council review and vote to approve the proposed Newpark Development Agreement Amendment to eliminate a portion of Center Drive to accommodate for the proposed Nevis at Newpark development, according to the following findings of fact, conclusions of law and conditions of approval:

Findings of Fact:

1. The Newpark Development Agreement was approved on October 18, 2001 and was subsequently amended in December 2002. It provided for 819,360 sq. ft. of density on approximately 37 acres.
2. Cottonwood Partners is the owner of record of parcel NPRK-P-2.
3. The development parameters for this project are specifically set forth in the Newpark Development Agreement.
4. The proposed Final Plat and Final Site Plan are legally described as Nevis at Newpark.
5. There is 76,360 sq. ft. of remaining density for the Newpark Town Center.
6. The proposed project consists of 29,041 sq. ft. of residential density, configured in 23 units, a parcel used for Cottonwood III parking, and a common area parcel.
7. There are 19 proposed 1,255 sq. ft. units and 4 proposed at 1,299 sq. ft.
8. The density is established by the Newpark Development Agreement pool of density.
9. If approved, the remaining density for Newpark Town Center would be 47,319 sq. ft.

10. Parking is regulated and accepted by the Engineering Department.
11. The Design Review Committee reviewed the Nevis at Newpark Final Plat and the Final Site Plan, and positively recommended to the Snyderville Basin Planning Commission.
12. The Newpark Development Agreement establishes that the Summit County Council “is the Land Use Authority for [Substantial Development Agreement Amendments].”
13. Public notice of the public hearing was published in the July 12, 2014 issue of *The Park Record*.
14. Postcard notices announcing the public hearing were mailed to property owners within 1,000 feet of the subject parcels on July 1, 2014.
15. Service providers have reviewed the plat for compliance with applicable standards and no project issues have been identified that could not be mitigated.
16. Staff has reviewed the proposed plat and final site plan for compliance with applicable Development Code standards.
17. Staff has reviewed the proposed plat and final site plan for compliance with Newpark Development Agreement standards.
18. On May 27, 2014, the Snyderville Basin Planning Commission forwarded a positive recommendation to the Summit County Council for the proposed Development Agreement Amendment.
19. On May 27, 2014, the Snyderville Basin Planning Commission forwarded a positive recommendation to the Summit County Manager for the proposed Nevis at Newpark Final Plat and Final Site Plan.

Conclusions of Law:

1. The Summit County Engineer’s Office has accepted the proposed deletion of Center Drive.
2. The applicable service providers have accepted the proposed deletion of Center Drive.
3. The proposal meets the terms of the Newpark Development Agreement.
4. The proposal meets the applicable standards of the Snyderville Basin Development Code.

Conditions of Approval:

1. This approval will remain valid only if the Summit County Manager approves the Final Plat and Final Site Plan for Nevis at Newpark.
2. All necessary permits must be obtained and fees shall be paid prior to the commencement of any construction activity, including but not limited to the Summit County Engineering and the Summit County Building Departments.

Public Notice, Meetings and Comments

This item was publicly noticed as a public hearing with possible action by the Snyderville Basin Planning Commission. Notice of the public hearing was published in the issue of *The Park Record*. Courtesy postcards were mailed to all property owners within 1,000 feet of the subject Parcel.

As of the date of this report, multiple negative public comments have been received opposing the deletion of Center Drive (Exhibit E).

Attachments

- Exhibit A – Proposed Plans and Elevations
- Exhibit B – Parking and Traffic Study
- Exhibit C – Pedestrian Circulation
- Exhibit D – Engineering Department Memo
- Exhibit E – Public Comments

NEVIS AT NEWPARK SUBDIVISION

AMENDING ALL OF LOT P-2, NEWPARK PARCEL P SUBDIVISION,
LYING WITHIN THE NORTHEAST QUARTER (NE 1/4) OF SECTION 19, TOWNSHIP 1 SOUTH, RANGE 4 EAST, SALT LAKE BASE AND MERIDIAN

SURVEYOR'S CERTIFICATE

I, TYLER E. JENKINS DO HEREBY CERTIFY THAT I AM A PROFESSIONAL LAND SURVEYOR, AND THAT I HOLD CERTIFICATE NO. 4938730 AS PRESCRIBED BY THE LAWS OF THE STATE OF UTAH. I FURTHER CERTIFY THAT BY THE DIRECTION OF HAMLET DEVELOPMENT, I HAVE MADE A SURVEY OF THE TRACT OF LAND SHOWN ON THIS PLAT AND DESCRIBED HERewith AND THE MONUMENTS DEPICTED ON THE PLAT ARE OF THE CHARACTER SHOWN, OCCUPY THE POSITIONS INDICATED, AND ARE OF SUFFICIENT NUMBER AND DURABILITY.



DATE _____ TYLER E. JENKINS
P.L.S. NO. 4938730

LEGAL DESCRIPTION

ALL OF LOT P-2, NEWPARK PARCEL P SUBDIVISION, RECORDED AT ENTRY NO. 939829, ON FILE AT THE SUMMIT COUNTY, UTAH RECORDER'S OFFICE.

OWNER'S DEDICATION

KNOW THAT ALL PEOPLE BY THESE PRESENTS THAT WE, THE UNDERSIGNED OWNER OF THE ABOVE DESCRIBED TRACT OF LAND, HAVING CAUSED THE SAME TO BE SUBDIVIDED INTO LOTS AND STREETS TO BE HEREAFTER KNOWN AS:

NEVIS AT NEWPARK

DO HEREBY GRANT UNTO EACH PRIVATE UTILITY COMPANY AND PUBLIC UTILITY AGENCY PROVIDING UTILITY SERVICES TO THIS PROJECT, A PERPETUAL NON-EXCLUSIVE EASEMENT IN ALL AREAS SHOWN HEREON TO INSTALL, USE, KEEP, MAINTAIN, REPAIR AND REPLACE, AS REQUIRED, UNDERGROUND UTILITY LINES, PIPES AND CONDUITS OF ALL TYPES AND APPURTENANCES THERETO SERVING THIS PROJECT.

OWNER:

BY: _____
NAME: MICHAEL BRODSKY
TITLE: CHAIRMAN OF HAMLET HOMES CORPORATION, MANAGING MEMBER OF NEVIS AT NEWPARK LLC
FOR: NEVIS AT NEWPARK LLC

LIMITED LIABILITY COMPANY ACKNOWLEDGEMENT

ON THE _____ DAY OF _____ A.D., 2014, MICHAEL BRODSKY PERSONALLY APPEARED BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, IN AND FOR SAID COUNTY OF SALT LAKE IN THE STATE OF UTAH, WHO AFTER BEING DULY SWORN, ACKNOWLEDGED TO ME THAT HE IS THE CHAIRMAN OF HAMLET HOMES CORPORATION, MANAGING MEMBER OF NEVIS AT NEWPARK LLC, A UTAH LIMITED LIABILITY COMPANY AND THAT HE SIGNED THE OWNER'S DEDICATION FREELY AND VOLUNTARILY FOR AND IN BEHALF OF SAID LIMITED LIABILITY COMPANY FOR THE PURPOSES THEREIN MENTIONED.

MY COMMISSION EXPIRES _____ NOTARY PUBLIC
RESIDING IN _____ COUNTY

OWNER:

BY: _____
NAME: LARRY BURTON
TITLE: PRESIDENT
FOR: US TITLE COMPANY OF UTAH

CORPORATE ACKNOWLEDGEMENT

ON THE _____ DAY OF _____ A.D., 2014, LARRY BURTON PERSONALLY APPEARED BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, IN AND FOR SAID COUNTY OF SALT LAKE IN THE STATE OF UTAH, WHO AFTER BEING DULY SWORN, ACKNOWLEDGED TO ME THAT HE IS THE PRESIDENT OF US TITLE COMPANY OF UTAH, A CORPORATION IN THE STATE OF UTAH, AND THAT HE SIGNED THE OWNER'S DEDICATION FREELY AND VOLUNTARILY FOR AND IN BEHALF OF SAID CORPORATION FOR THE PURPOSES THEREIN MENTIONED.

MY COMMISSION EXPIRES _____ NOTARY PUBLIC
RESIDING IN _____ COUNTY

OWNER:

BY: _____
NAME: _____
TITLE: _____
FOR: BANK OF UTAH

CORPORATE ACKNOWLEDGEMENT

ON THE _____ DAY OF _____ A.D., 2014, _____ PERSONALLY APPEARED BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, IN AND FOR SAID COUNTY OF SALT LAKE IN THE STATE OF UTAH, WHO AFTER BEING DULY SWORN, ACKNOWLEDGED TO ME THAT HE IS THE _____ OF BANK OF UTAH, A CORPORATION IN THE STATE OF UTAH, AND THAT HE SIGNED THE OWNER'S DEDICATION FREELY AND VOLUNTARILY FOR AND IN BEHALF OF SAID CORPORATION FOR THE PURPOSES THEREIN MENTIONED.

MY COMMISSION EXPIRES _____ NOTARY PUBLIC
RESIDING IN _____ COUNTY

PLAT NOTES

- THIS PLAT IS APPROVED SUBJECT TO THE CONDITIONS CONTAINED IN THE NEWPARK MASTER DEVELOPMENT PARCEL PLAT, DATED APRIL 14, 2004, AND RECORDED AS ENTRY NO. 654674.
- THIS PLAT IS SUBJECT TO THE FOURTH AMENDED AND RESTATED DECLARATION OF COVENANTS, CONDITIONS, AND RESTRICTIONS OF NEWPARK OWNERS ASSOCIATION ("MASTER ASSOCIATION") RECORDED IN BOOK 1814, PAGE 1035-1063 IN THE OFFICE OF THE SUMMIT COUNTY RECORDER AS AMENDED FROM TIME TO TIME ("MASTER COVENANTS"), WHICH SHALL ENCUMBER ALL OF NEWPARK TOWN CENTER.
- ALL COMMON AREAS ARE SUBJECT TO A NON-EXCLUSIVE PUBLIC AND PRIVATE UTILITY AND DRAINAGE EASEMENT FOR THE PURPOSE OF PROVIDING ACCESS FOR UTILITY INSTALLATION, USE, MAINTENANCE AND EVENTUAL REPLACEMENT.
- THE COMMON AREA INCLUDES ALL REAL PROPERTY AND IMPROVEMENTS WITHIN THE PROJECT, OTHER THAN PARCELS OWNED BY THIRD PARTIES IN FEE. THE COMMON AREA INCLUDES, WITHOUT LIMITATION, ALL LANDSCAPED AREAS, PRIVATE ROADWAYS, WALKWAYS OR PEDESTRIAN CORRIDORS, PARKING AREAS, PARKS, PLAZAS, ETC. OWNED OR MAINTAINED THRU EASEMENT BY THE NEWPARK OWNERS ASSOCIATION FOR THE COMMON USE AND ENJOYMENT OF ALL OWNERS, UNLESS SPECIFICALLY LIMITED BY RECORDED DOCUMENTS.
- THE MASTER ASSOCIATION SHALL, PURSUANT TO THE MASTER DECLARATION, MAINTAIN, REPAIR AND REPLACE ALL COMMON AREAS AND COMMON IMPROVEMENTS OF THE MASTER ASSOCIATION WITHIN NEVIS AT NEWPARK AND WITHIN NEWPARK TOWN CENTER THAT ARE CREATED PURSUANT TO THE MASTER DECLARATION AND RELATED PLAT(S), INCLUDING BUT NOT LIMITED TO PEDESTRIAN CORRIDORS, PRIVATE ROADWAYS, LANDSCAPE, PARKING AREAS, UTILITY INFRASTRUCTURE AND OTHER COMMON FACILITIES. RESPONSIBILITY FOR THE COST OF MAINTAINING SUCH COMMON IMPROVEMENTS SHALL BE SHARED BY THE PARCEL OWNERS IN NEWPARK TOWN CENTER IN ACCORDANCE WITH THE MASTER COVENANTS.
- THE MASTER ASSOCIATION IS GRANTED THE RIGHT TO REMOVE SNOW OVER ALL MASTER ASSOCIATION COMMON AREAS OF NEWPARK TOWN CENTER AND COMMON AREAS OWNED BY NEVIS AT NEWPARK.
- THE TOWNHOMES WITHIN THE NEVIS AT NEWPARK ARE DESIGNED WITH ZERO LOT LINES. BUILDINGS AND OTHER IMPROVEMENTS MAY BE LOCATED ON THE PROPERTY LINE. ADJOINING BUILDINGS MAY HAVE A COMMON FOOTING, FOUNDATION, OR WALL, WHICH MAY BE CENTERED ALONG THE PROPERTY LINE.
- PUBLIC TRANSPORTATION AND PARKING STRUCTURES ARE CONTEMPLATED IN THE NEWPARK TOWN CENTER TO FACILITATE TRANSIT ORIENTED DEVELOPMENT GRANTS OR OTHER FORMS OF FINANCIAL ASSISTANCE FOR PUBLIC TRANSPORTATION AND/OR PARKING IMPROVEMENTS. THE MASTER ASSOCIATION MAY CONVEY EASEMENTS OR FEE TITLE TO COMMON AREAS WITHIN NEWPARK TOWN CENTER THAT ARE DESIGNATED FOR PARKING TO A PUBLIC AGENCY IN CONNECTION WITH SUCH GRANTS OR LOANS. ANY SUCH CONVEYANCE SHALL BE SUBJECT TO AN APPROPRIATE RESERVATION TO ADDRESS THE REASONABLE PARKING DEMANDS OF OWNERS, TENANTS AND GUESTS WITHIN THE NEWPARK TOWN CENTER.
- EACH PARCEL OF LAND IN NEWPARK TOWN CENTER IS ENTITLED TO UTILIZE ANY PUBLIC UTILITIES SERVING THAT PARCEL UPON THE APPROVAL OF THE UTILITY PROVIDER AND SUBJECT TO THE TERMS AND CONDITIONS OF SAID UTILITY PROVIDER IN ACCORDANCE WITH THE RESTRICTIVE COVENANTS AND NEWPARK OWNERS ASSOCIATION BYLAWS AND ARTICLES OF INCORPORATION.
- A NON-EXCLUSIVE 20.00 FOOT WIDE PUBLIC UTILITY EASEMENT IS HEREBY DEDICATED ALONG ALL PUBLIC RIGHT OF WAYS, AND IS SUBJECT TO FOUNDATIONS AND STRUCTURES TO BE LOCATED WITHIN SAID EASEMENT.
- UTILITY PROVIDERS HAVE THE RIGHT TO INSTALL, OPERATE AND MAINTAIN THEIR EQUIPMENT AND OTHER RELATED FACILITIES ABOVE AND BELOW GROUND LEVEL WITHIN THE PUBLIC UTILITY EASEMENTS, AS MAY BE NECESSARY OR REASONABLE IN SERVING THE PARCELS, INCLUDING THE RIGHT OF ACCESS TO SUCH FACILITIES AND THE RIGHT TO REQUIRE REMOVAL OF ANY OBSTRUCTIONS. INCLUDING TREES AND VEGETATION THAT MAY BE PLACED WITHIN THE EASEMENT AT THE OWNER'S EXPENSE EXCEPT AS DEPICTED ON THIS PLAT, NO PERMANENT STRUCTURES OR OBSTRUCTION MAY BE PLACED IN THE EASEMENTS THAT INTERFERE WITH THE USE OF THE EASEMENTS WITHOUT THE PRIOR WRITTEN APPROVAL OF THE AFFECTED UTILITY PROVIDER.
- ALL PARCELS ARE SUBJECT TO PARK CITY FIRE SERVICE DISTRICT REVIEW AND REQUIREMENTS, INCLUDING STANDARDS FOR ACCESS AND WATER SUPPLY FOR FIRE PROTECTION AND FULL PAYMENT OF SUBSEQUENT IMPACT AND PERMIT FEES. AN ALL WEATHER FIRE DEPARTMENT ACCESS ROAD IS REQUIRED TO BE INSTALLED AND MADE SERVICEABLE PRIOR TO THE ISSUANCE OF A BUILDING PERMIT AUTHORIZING COMBUSTIBLE CONSTRUCTION. THE ALL WEATHER FIRE DEPARTMENT ACCESS ROAD IS TO BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. IF THE ALL WEATHER ACCESS ROAD IS NOT MAINTAINED, THE FIRE DISTRICT RESERVES THE RIGHT TO STOP WORK UNTIL REQUIRED ROADS ARE REOPENED. WATER SUPPLIES REQUIRED FOR FIRE PROTECTION ARE TO BE INSTALLED AND MADE SERVICEABLE PRIOR TO THE ISSUANCE OF A BUILDING PERMIT AUTHORIZING CONSTRUCTION OF COMBUSTIBLE IMPROVEMENTS. IF THE FIRE PROTECTION WATER SUPPLY IS NOT MAINTAINED, THE FIRE DISTRICT RESERVES THE RIGHT TO STOP WORK UNTIL THE REQUIRED WATER SUPPLY FOR FIRE PROTECTION IS PLACED BACK IN SERVICE. WATER SUPPLIES FOR FIRE PROTECTION MUST BE CLEARLY IDENTIFIED IN A MANNER TO PREVENT OBSTRUCTIONS. EACH WATER SUPPLY FOR FIRE PROTECTION MUST BE MARKED WITH AN APPROVED FLAG TO IDENTIFY ITS LOCATION DURING WINTER CONDITIONS ROOFING MATERIALS MUST BE NON-COMBUSTIBLE AND APPROVED BY THE PCFSD. NO WOOD SHAKE ROOFING MATERIAL WILL BE PERMITTED.
- SEWER SERVICE TO NEWPARK TOWN CENTER AND THIS PROJECT SHALL BE THROUGH THE SNYDERVILLE BASIN WATER RECLAMATION DISTRICT (SBWRD), AND MUST COMPLY WITH ITS RULES AND REGULATIONS. AT THE TIME OF ANY RESURFACING OF ASPHALT PAVEMENT WITHIN THE PUBLIC AND PRIVATE ROADS SHOWN HEREON, THE MASTER ASSOCIATION SHALL BE RESPONSIBLE TO RAISE SEWER MANHOLES TO GRADE ACCORDING TO SNYDERVILLE BASIN WATER RECLAMATION STANDARDS.
- WATER SERVICES TO NEWPARK TOWN CENTER AND THIS PROJECT SHALL BE THROUGH THE MOUNTAIN REGIONAL WATER SPECIAL SERVICE DISTRICT (MRWSSD), AND MUST COMPLY WITH ITS RULES AND REGULATIONS.
- IN ACCORDANCE WITH PLAT NOTE 17 OF THE MASTER DEVELOPMENT PARCEL PLAT, THE FOLLOWING DENSITY HAS BEEN PLATTED OR CONSTRUCTED TO DATE: THE BUILDING PLAN ON PARCEL P (NEVIS AT NEWPARK) INCLUDES 29,041 SQ.FT. OF RESIDENTIAL SPACE. THE TABLE REFLECTS THESE CHANGES IN SQUARE FEET:

BUILDING DENSITY	RETAIL/COMMERCIAL	RESIDENTIAL	OFFICE	INSTITUTIONAL	ALLOCATION FOR FUTURE DEVELOPMENT
PREVIOUS PLATS	164,557	228,750	155,166	121,880	
NEVIS AT NEWPARK (AMENDED PARCEL P-2)		29,041			209
LOT P-3 (AMENDED PARCEL P-2)					
TOTAL PLATTED TO DATE:			TOTAL REMAINING DENSITY:		

- THE REAL PROPERTY LOCATED GENERALLY OUTSIDE OF THE AREA WITHIN THE FOOTPRINT OF THE BUILDING(S) ON PARCEL P SHALL BE SUBJECT TO A PARKING AND MAINTENANCE AGREEMENT IN FAVOR OF THE NEWPARK OWNERS ASSOCIATION ("NOA") IN ACCORDANCE WITH, AND SUBJECT TO THE TERMS AND PROVISIONS OF SAID AGREEMENT. THE AGREEMENT WILL PROVIDE FOR LANDSCAPED AREAS, WALKWAYS OR PEDESTRIAN CORRIDORS, CROSS PARKING AREAS WITH ASSOCIATED COSTS AND OBLIGATIONS TO BE ALLOCATED IN THE SAME MANNER AS PRESCRIBED FOR COMMON AREAS UNDER THE MASTER COVENANTS FOR THE PROJECT. THE PARKING AREAS ON THIS PARCEL SHALL ALSO BE SUBJECT TO THE PARKING AND MAINTENANCE AGREEMENT FOR THE PURPOSE OF ESTABLISHING CROSS PARKING RIGHTS IN ACCORDANCE WITH, AND SUBJECT TO, THE TERMS AND PROVISIONS OF THE SAID AGREEMENT. IN ADDITION TO BENEFITTING THE BUILDING(S) LOCATED ON PARCEL P, THE PARKING ON PARCEL P WILL ALSO BENEFIT THE NEWPARK TOWN CENTER IN GENERAL DURING OFF-PEAK HOURS. REFERENCE IS MADE TO THE PARKING AND MAINTENANCE AGREEMENT, THE TERMS OF WHICH SHALL CONTROL.
- THE FIRE IMPACT FEE REQUIRED BY THE PARK CITY FIRE DISTRICTS FOR PARCEL P WAS NOT PAID AT TIME OF PLAT RECORDATION. HOWEVER THE FIRE IMPACT FEE MUST BE PAID TO THE PARK CITY FIRE DISTRICT PRIOR TO ISSUANCE OF A BUILDING PERMIT FOR CONSTRUCTION ON PARCEL P.
- THE FINANCIAL ASSURANCES TO INSURE THE DEVELOPER'S PERFORMANCE REQUIRED BY SUMMIT COUNTY UNDER THE DEVELOPMENT IMPROVEMENT AGREEMENT FOR PARCEL P2 WAS NOT PROVIDED AT THE TIME OF PLAT RECORDATION. HOWEVER THE FINANCIAL ASSURANCES MUST BE PROVIDED TO SUMMIT COUNTY PRIOR TO ISSUANCE OF A BUILDING PERMIT FOR CONSTRUCTION ON PARCEL P2.
- SNOW STORAGE IN CONNECTION WITH THE PROJECT WILL BE LOCATED BOTH ON-SITE AND OFF-SITE, BASED UPON ACTUAL CONDITIONS AND CIRCUMSTANCES WHICH MAY EXIST FROM TIME TO TIME. TO THE EXTENT THAT ON-SITE SNOW STORAGE SIGNIFICANTLY INTERFERES WITH ON-SITE PARKING AND/OR VISIBILITY IN CONNECTION WITH ON-SITE VEHICULAR CIRCULATION, SNOW SHALL BE TRANSPORTED OFF-SITE FOR STORAGE IN APPROPRIATE LOCATIONS.

LOT TOTAL

THE TOTAL NUMBER OF PRIVATE LOTS AND PARCELS SUBDIVIDED PER THIS PLAT MAP IS 24.
THE TOTAL NUMBER OF COMMON AREA LOTS PER THIS PLAT IS 1.

NOT TO SCALE
DRAWN BY: B. YATES
CHECKED BY: T. JENKINS
TAX ID NO.
UTAH
SUMMIT COUNTY
PARK CITY

NEVIS AT NEWPARK SUBDIVISION
AMEND. LOT P-2, NEWPARK PARCEL P SUB
HAMLET DEVELOPMENT
(NE 1/4) SEC. 19, T. 1 S., R. 4 E., S.L.B.M.

REDCON, INC.
LAND SURVEYORS
90 NORTH MAIN STREET
BOUNTIFUL, UTAH 84010
(801) 298-2401 FAX (801) 298-2024
REDCON.COM

COUNTY PLANNING COMMISSION
REVIEWED FOR CONFORMANCE TO:
THE SNYDERVILLE BASIN PLANNING COMMISSION.
APPROVED THIS _____ DAY OF _____ 2014,

PLANNING COMMISSION CHAIRMAN

UTILITY EASEMENT APPROVAL
APPROVED THIS _____ DAY OF _____ 2014.
BY: ROCKY MOUNTAIN POWER, A DIVISION OF PACIFIC CORP.,
AUTHORIZED AGENT

WATER DISTRICT
APPROVED THIS _____ DAY OF _____ 2014.
BY: MOUNTAIN REGIONAL WATER SPECIAL SERVICE DISTRICT
AUTHORIZED AGENT

SNYDERVILLE BASIN SPECIAL RECREATION DISTRICT
APPROVED THIS _____ DAY OF _____ 2014.
BY: SNYDERVILLE BASIN SPECIAL RECREATION DISTRICT
AUTHORIZED AGENT

PARK CITY FIRE SERVICE DISTRICT
APPROVED THIS _____ DAY OF _____ 2014.
BY: PARK CITY FIRE SERVICE DISTRICT
FIRE MARSHAL

SUMMIT COUNTY PUBLIC WORKS
APPROVED THIS _____ DAY OF _____ 2014.
BY: SUMMIT COUNTY PUBLIC WORKS DEPARTMENT,
PUBLIC WORKS DIRECTOR

WATER RECLAMATION DISTRICT
REVIEWED FOR CONFORMANCE TO:
THE SNYDERVILLE BASIN WATER RECLAMATION DISTRICT'S
STANDARDS.
APPROVED THIS _____ DAY OF _____ 2014,

SNYDERVILLE BASIN WATER RECLAMATION DISTRICT

COUNTY ASSESSOR
REVIEWED AND ACCEPTED BY:
THE OFFICE OF THE SUMMIT COUNTY ASSESSOR.
APPROVED THIS _____ DAY OF _____ 2014,

SUMMIT COUNTY ASSESSOR

APPROVAL AS TO FORM
APPROVED THIS _____ DAY OF _____ 2014.

SUMMIT COUNTY ATTORNEY

COUNTY ENGINEER
I HEREBY CERTIFY THAT I HAVE HAD THIS PLAT REVIEWED BY THIS OFFICE
AND IT IS CORRECT IN ACCORDANCE WITH AVAILABLE INFORMATION ON
FILE IN THIS OFFICE.
APPROVED THIS _____ DAY OF _____ 2014,

SUMMIT COUNTY ENGINEER

COUNTY MANAGER
PRESENTED TO THE SUMMIT COUNTY MANAGER
THIS _____ DAY OF _____ 2014, AT WHICH TIME THIS
SUBDIVISION PLAT WAS APPROVED AND ACCEPTED.

SUMMIT COUNTY MANAGER

ENTRY NO: _____
STATE OF UTAH, COUNTY OF SUMMIT, RECORDED AND FILED AT
REQUEST OF: _____
DATE: _____ TIME: _____ BOOK: _____ PAGE: _____
FEES _____ SUMMIT COUNTY RECORDER

JOB NUMBER
413000-35
DWG: 413000-35 PLAT
DATE: 5/16/2014
SHEET 1 OF 2

NEVIS AT NEWPARK SUBDIVISION

AMENDING ALL OF LOT P-2, NEWPARK PARCEL P SUBDIVISION,
LYING WITHIN THE NORTHEAST QUARTER (NE 1/4) OF SECTION 19, TOWNSHIP 1 SOUTH, RANGE 4 EAST, SALT LAKE BASE AND MERIDIAN

S89°47'32"E 5331.28' (S89°47'32"E 5331.20' RECORD) (BASIS OF BEARING)

FOUND NORTHWEST CORNER OF SECTION 19, TOWNSHIP 1 SOUTH RANGE 4 EAST, SALT LAKE BASE AND MERIDIAN

FOUND NORTHEAST CORNER OF SECTION 19, TOWNSHIP 1 SOUTH RANGE 4 EAST, SALT LAKE BASE AND MERIDIAN

PARKING NOTE

TWELVE (12) STALL PARKING AGREEMENT FOR THE TENANTS OF NEWPARK RESORT RESIDENCES, ALSO KNOWN AS NEWPARK TOWNHOMES PER ENTRY NO. 939968, 4(a) ON SHEET 3 OF 15.

4(a). TWELVE (12) PARKING STALLS LOCATED ALONG THE EASTERLY BORDER OF PARCEL P AND LOCATED ON PARK LANE NORTH SHALL BE AVAILABLE FOR USE BY THE ASSOCIATION TO PROVIDE PARKING 24 HOURS A DAY AND 365 DAYS A YEAR FOR THE NEWPARK TOWNHOMES AND OTHER NOA MEMBERS, AS DETERMINED AND REGULATED FROM TIME TO TIME BY THE ASSOCIATION.

SOUTH 1430.63'

N89° 59' 42"E 2885.38'

HIGHLAND DRIVE
(PUBLIC RIGHT-OF-WAY)

CENTER DRIVE
(PRIVATE)

CROSS ACCESS EASEMENT
PER ENTRY NO. 654674,
720981 AND 783595
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

COTTONWOOD NEWPARK THREE LLC
LOT P-1 NEWPARK PARCEL P SUBDIVISION
ENTRY NO. 939829 ON FILE AT THE
SUMMIT COUNTY, UTAH RECORDERS OFFICE
1389 CENTER DRIVE
PARCEL NUMBER NPRK-P-1
NOT A PART

WIDTH VARIES
ALL EASEMENTS, NOTES, PLAT NOTES AND AMENDED PLAT NOTES AS
DEPICTED, AND/OR SET FORTH ON THE PLAT FOR CENTER DRIVE
PER ENTRY NO. 794333
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

LOT V-1
NOT A PART

20.00' WIDE WASTEWATER EASEMENT
PER ENTRY NO. 692436
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

CROSS ACCESS EASEMENT
PER ENTRY NO. 939829
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

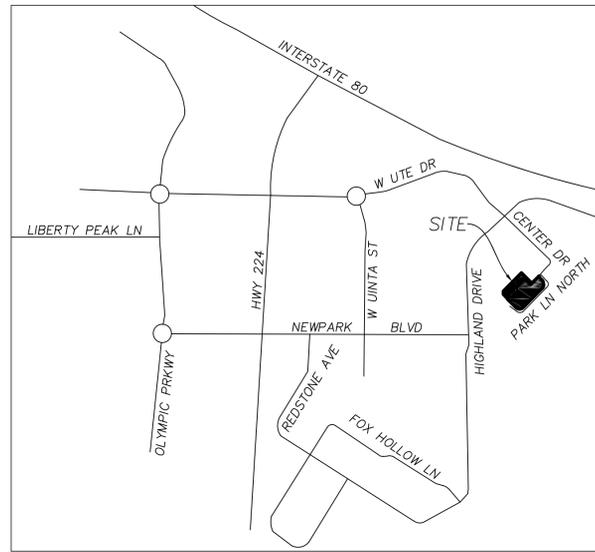
CROSS ACCESS EASEMENT
PER ENTRY NO. 654674,
720981 AND 783595
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

COTTONWOOD NEWPARK THREE LLC
LOT P-1 NEWPARK PARCEL P SUBDIVISION
ENTRY NO. 939829 ON FILE AT THE
SUMMIT COUNTY, UTAH RECORDERS OFFICE
1389 CENTER DRIVE
PARCEL NUMBER NPRK-P-1
NOT A PART

LINE TABLE		
LINE #	LENGTH	DIRECTION
L1	10.48'	N45°00'00"E
L2	12.00'	N45°00'00"E
L3	8.09'	N45°00'00"E

CURVE TABLE					
CURVE #	LENGTH	RADIUS	DELTA	CHORD BRG	CHORD LENGTH
C1	2.16'	38.00	3°15'14"	N43°22'23"E	2.16'
C2	13.18'	38.00	19°52'00"	N54°56'00"E	13.11'

LOT V-2
NOT A PART



VICINITY MAP

LEGEND

- MAP BOUNDARY LINE
- LOT LINE
- TIE LINE
- EASEMENT LINE
- ADJOINING PROPERTY LINE
- QUESTAR GAS EASEMENT HEREBY VACATED
- COMMON AREA
- CROSS ACCESS EASEMENT
- FOUND MONUMENTATION AS SHOWN AND DESCRIBED
- PROPERTY CORNER SET WITH 5/8" BAR AND CAP STAMPED "REDCON"
- FIRE HYDRANT

BASIS OF BEARING

SOUTH 89°47'32" EAST, BEING THE BEARING OF THE SECTION LINE BETWEEN THE NORTHWEST CORNER AND THE NORTHEAST CORNER OF SECTION 19, TOWNSHIP 1 SOUTH, RANGE 4 EAST, SALT LAKE BASE AND MERIDIAN, SUMMIT COUNTY, UTAH.

REFERENCES

NEWPARK PARCEL P SUBDIVISION, RECORDED AT ENTRY NO. 939829, ON FILE AT THE SUMMIT COUNTY, UTAH RECORDER'S OFFICE.

S89° 59' 42"E
10.98'

N89° 59' 42"E
19.50'

S00° 00' 18"E 143.51'

CROSS ACCESS EASEMENT
PER ENTRY NO. 939829
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

20.00' WIDE QUESTAR RIGHT-OF-WAY
AND EASEMENT GRANT
PER ENTRY NO. 00701006
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.
(A PORTION HEREBY VACATED)

PARCEL A
NOT A PART

SEE PARKING NOTE

L=59.69
90°00'00"
R=38.00

S00° 00' 18"E 143.51'

N89° 59' 42"E
19.50'

S00° 00' 18"E 143.51'

CROSS ACCESS EASEMENT
PER ENTRY NO. 939829
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

20.00' WIDE QUESTAR RIGHT-OF-WAY
AND EASEMENT GRANT
PER ENTRY NO. 00701006
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.
(A PORTION HEREBY VACATED)

PARCEL A
NOT A PART

SEE PARKING NOTE

L=59.69
90°00'00"
R=38.00

S89° 59' 42"E
10.98'

N89° 59' 42"E
19.50'

S00° 00' 18"E 143.51'

CROSS ACCESS EASEMENT
PER ENTRY NO. 939829
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

20.00' WIDE QUESTAR RIGHT-OF-WAY
AND EASEMENT GRANT
PER ENTRY NO. 00701006
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.
(A PORTION HEREBY VACATED)

PARCEL A
NOT A PART

SEE PARKING NOTE

L=59.69
90°00'00"
R=38.00

L=25.69
127°00'25"
R=54.50

S45° 00' 00"E
14.13'

L=23.10
134°49'54"
R=38.00

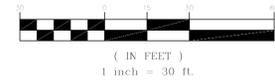
S10° 10' 06"E
13.25'

L=36.59
55°10'06"
R=38.00

TRASH ENCLOSURE AGREEMENT
PER ENTRY NO. 939968
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

20.00' WIDE WASTEWATER EASEMENT
PER ENTRY NO. 692436
ON FILE AT THE SUMMIT COUNTY,
UTAH RECORDERS OFFICE.

SEE PARKING NOTE



ENTRY NO: _____

STATE OF UTAH, COUNTY OF SUMMIT, RECORDED AND FILED AT
REQUEST OF: _____

DATE: _____ TIME: _____ BOOK: _____ PAGE: _____

FEES: _____ SUMMIT COUNTY RECORDER

NEVIS AT NEWPARK SUBDIVISION
AMEND. LOT P-2, NEWPARK PARCEL P SUB
HAMLET DEVELOPMENT
(NE 1/4) SEC. 19, T. 1 S., R. 4 E., S.L.B.M.

SCALE (H): 1" = 30'
DRAWN BY: B. YATES
CHECKED BY: T. JEWINS
TAX ID NO. _____

REDCON, INC.
LAND SURVEYORS
90 NORTH MAIN STREET
BOUNTIFUL, UTAH 84010
(801) 298-2401 FAX (801) 298-2024
REDCON.COM

JOB NUMBER
413000-35
DWG: 413000-35 PLAT
DATE: 5/16/2014
SHEET 2 OF 2

UTAH
SUMMIT COUNTY
PARK CITY

NEVIS AT NEWPARK FINAL SITE PLAN

AMENDING ALL OF LOT P-2, NEWPARK PARCEL P SUBDIVISION, LYING WITHIN THE NORTHEAST QUARTER (NE 1/4) OF SECTION 19, TOWNSHIP 1 SOUTH, RANGE 4 EAST, SALT LAKE BASE AND MERIDIAN

SURVEYOR'S CERTIFICATE

I TYLER E. JENKINS DO HEREBY CERTIFY THAT I AM A PROFESSIONAL LAND SURVEYOR, AND THAT I HOLD CERTIFICATE NO. 4938730 AS PRESCRIBED BY THE LAWS OF THE STATE OF UTAH. I FURTHER CERTIFY THAT BY THE DIRECTION OF HAMLET DEVELOPMENT, I HAVE MADE A SURVEY OF THE TRACT OF LAND SHOWN ON THIS PLAT AND DESCRIBED HERewith AND THE MONUMENTS DEPICTED ON THE PLAT ARE OF THE CHARACTER SHOWN, OCCUPY THE POSITIONS INDICATED, AND ARE OF SUFFICIENT NUMBER AND DURABILITY:



DATE _____ TYLER E. JENKINS
P.L.S. NO. 4938730

LEGAL DESCRIPTION

ALL OF LOT P-2, NEWPARK PARCEL P SUBDIVISION, RECORDED AT ENTRY NO. 939829, ON FILE AT THE SUMMIT COUNTY, UTAH RECORDER'S OFFICE.

OWNER'S DEDICATION

KNOW THAT ALL PEOPLE BY THESE PRESENTS THAT WE, THE UNDERSIGNED OWNER OF THE ABOVE DESCRIBED TRACT OF LAND, HAVING CAUSED THE SAME TO BE SUBDIVIDED INTO LOTS AND STREETS TO BE HEREAFTER KNOWN AS:

NEVIS AT NEWPARK

DO HEREBY GRANT UNTO EACH PRIVATE UTILITY COMPANY AND PUBLIC UTILITY AGENCY PROVIDING UTILITY SERVICES TO THIS PROJECT, A PERPETUAL NON-EXCLUSIVE EASEMENT IN ALL AREAS SHOWN HEREON TO INSTALL, USE, KEEP, MAINTAIN, REPAIR AND REPLACE, AS REQUIRED, UNDERGROUND UTILITY LINES, PIPES AND CONDUITS OF ALL TYPES AND APPURTENANCES THERETO SERVING THIS PROJECT.

OWNER:

BY: _____
NAME: MICHAEL BRODSKY
TITLE: CHAIRMAN OF HAMLET HOMES CORPORATION, MANAGING MEMBER OF NEVIS AT NEWPARK LLC
FOR: NEVIS AT NEWPARK LLC

LIMITED LIABILITY COMPANY ACKNOWLEDGEMENT

ON THE _____ DAY OF _____ A.D., 2014, MICHAEL BRODSKY PERSONALLY APPEARED BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, IN AND FOR SAID COUNTY OF SALT LAKE IN THE STATE OF UTAH, WHO AFTER BEING DULY SWORN, ACKNOWLEDGED TO ME THAT HE IS THE CHAIRMAN OF HAMLET HOMES CORPORATION, MANAGING MEMBER OF NEVIS AT NEWPARK LLC, A UTAH LIMITED LIABILITY COMPANY AND THAT HE SIGNED THE OWNER'S DEDICATION FREELY AND VOLUNTARILY FOR AND IN BEHALF OF SAID LIMITED LIABILITY COMPANY FOR THE PURPOSES THEREIN MENTIONED.

MY COMMISSION EXPIRES _____ NOTARY PUBLIC
RESIDING IN _____ COUNTY

OWNER:

BY: _____
NAME: LARRY BURTON
TITLE: PRESIDENT
FOR: US TITLE COMPANY OF UTAH

CORPORATE ACKNOWLEDGEMENT

ON THE _____ DAY OF _____ A.D., 2014, LARRY BURTON PERSONALLY APPEARED BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, IN AND FOR SAID COUNTY OF SALT LAKE IN THE STATE OF UTAH, WHO AFTER BEING DULY SWORN, ACKNOWLEDGED TO ME THAT HE IS THE PRESIDENT OF US TITLE COMPANY OF UTAH, A CORPORATION IN THE STATE OF UTAH, AND THAT HE SIGNED THE OWNER'S DEDICATION FREELY AND VOLUNTARILY FOR AND IN BEHALF OF SAID CORPORATION FOR THE PURPOSES THEREIN MENTIONED.

MY COMMISSION EXPIRES _____ NOTARY PUBLIC
RESIDING IN _____ COUNTY

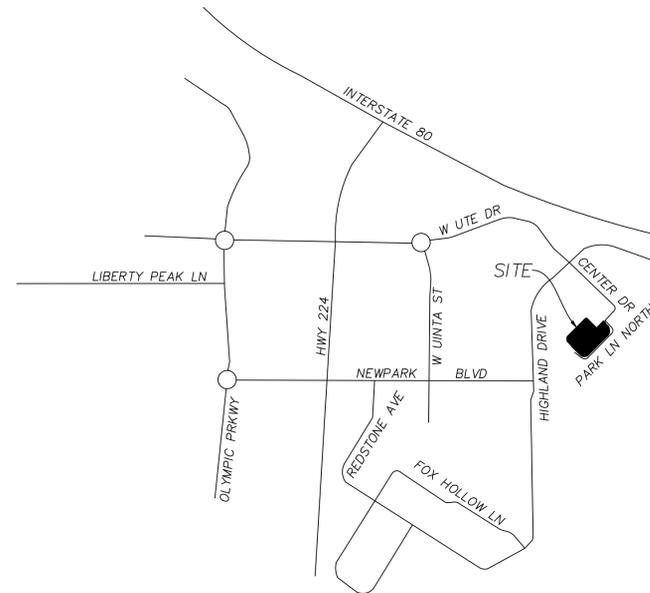
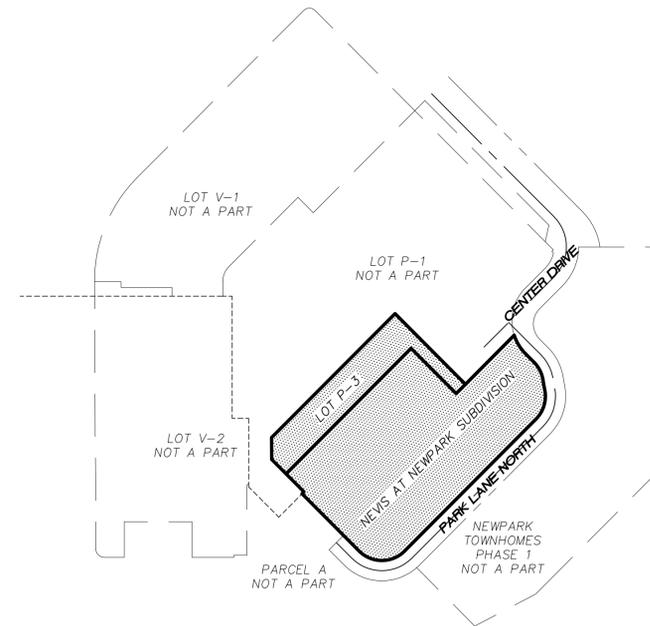
OWNER:

BY: _____
NAME: _____
TITLE: _____
FOR: BANK OF UTAH

CORPORATE ACKNOWLEDGEMENT

ON THE _____ DAY OF _____ A.D., 2014, _____ PERSONALLY APPEARED BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, IN AND FOR SAID COUNTY OF SALT LAKE IN THE STATE OF UTAH, WHO AFTER BEING DULY SWORN, ACKNOWLEDGED TO ME THAT HE IS THE _____ OF BANK OF UTAH, A CORPORATION IN THE STATE OF UTAH, AND THAT HE SIGNED THE OWNER'S DEDICATION FREELY AND VOLUNTARILY FOR AND IN BEHALF OF SAID CORPORATION FOR THE PURPOSES THEREIN MENTIONED.

MY COMMISSION EXPIRES _____ NOTARY PUBLIC
RESIDING IN _____ COUNTY



VICINITY MAP

<p>COUNTY PLANNING COMMISSION</p> <p>REVIEWED FOR CONFORMANCE TO: THE SNYDERVILLE BASIN PLANNING COMMISSION. APPROVED THIS _____ DAY OF _____ 2014.</p> <p>PLANNING COMMISSION CHAIRMAN</p>	<p>UTILITY EASEMENT APPROVAL</p> <p>APPROVED THIS _____ DAY OF _____ 2014.</p> <p>BY: ROCKY MOUNTAIN POWER, A DIVISION OF PACIFIC CORP., AUTHORIZED AGENT</p>	<p>WATER DISTRICT</p> <p>APPROVED THIS _____ DAY OF _____ 2014.</p> <p>BY: MOUNTAIN REGIONAL WATER SPECIAL SERVICE DISTRICT AUTHORIZED AGENT</p>	<p>SNYDERVILLE BASIN SPECIAL RECREATION DISTRICT</p> <p>APPROVED THIS _____ DAY OF _____ 2014.</p> <p>BY: SNYDERVILLE BASIN SPECIAL RECREATION DISTRICT AUTHORIZED AGENT</p>	<p>PARK CITY FIRE SERVICE DISTRICT</p> <p>APPROVED THIS _____ DAY OF _____ 2014.</p> <p>BY: PARK CITY FIRE SERVICE DISTRICT FIRE MARSHAL</p>	<p>SUMMIT COUNTY PUBLIC WORKS</p> <p>APPROVED THIS _____ DAY OF _____ 2014.</p> <p>BY: SUMMIT COUNTY PUBLIC WORKS DEPARTMENT, PUBLIC WORKS DIRECTOR</p>
<p>WATER RECLAMATION DISTRICT</p> <p>REVIEWED FOR CONFORMANCE TO: THE SNYDERVILLE BASIN WATER RECLAMATION DISTRICT'S STANDARDS. APPROVED THIS _____ DAY OF _____ 2014.</p> <p>SNYDERVILLE BASIN WATER RECLAMATION DISTRICT</p>	<p>COUNTY ASSESSOR</p> <p>REVIEWED AND ACCEPTED BY: THE OFFICE OF THE SUMMIT COUNTY ASSESSOR. APPROVED THIS _____ DAY OF _____ 2014.</p> <p>SUMMIT COUNTY ASSESSOR</p>	<p>APPROVAL AS TO FORM</p> <p>APPROVED THIS _____ DAY OF _____ 2014.</p> <p>SUMMIT COUNTY ATTORNEY</p>	<p>COUNTY ENGINEER</p> <p>I HEREBY CERTIFY THAT I HAVE HAD THIS PLAT REVIEWED BY THIS OFFICE AND IT IS CORRECT IN ACCORDANCE WITH AVAILABLE INFORMATION ON FILE IN THIS OFFICE. APPROVED THIS _____ DAY OF _____ 2014.</p> <p>SUMMIT COUNTY ENGINEER</p>	<p>COUNTY MANAGER</p> <p>PRESENTED TO THE SUMMIT COUNTY MANAGER THIS _____ DAY OF _____ 2014, AT WHICH TIME THIS SUBDIVISION PLAT WAS APPROVED AND ACCEPTED.</p> <p>SUMMIT COUNTY MANAGER</p>	<p>ENTRY NO: _____</p> <p>STATE OF UTAH, COUNTY OF SUMMIT, RECORDED AND FILED AT REQUEST OF: _____</p> <p>DATE: _____ TIME: _____ BOOK: _____ PAGE: _____</p> <p>FEES: _____ SUMMIT COUNTY RECORDER</p>

NEVIS AT NEWPARK FINAL SITE PLAN
AMEND. LOT P-2, NEWPARK PARCEL P SUB
HAMLET DEVELOPMENT
(NE 1/4) SEC. 19, T. 1 S., R. 4 E., S.L.B.M.

REDCON, INC.
LAND SURVEYORS
90 NORTH MAIN STREET
BOUNTIFUL, UTAH 84010
(801) 298-2401 FAX (801) 298-2024
REDCON.COM

JOB NUMBER
413000-35
DWG: 413000-35 SP
DATE: 5/19/2014
SHEET 1 OF 12



PO Box 522056 Salt Lake City, UT 84152-2056
(801) 201-7494 www.edmlc.net

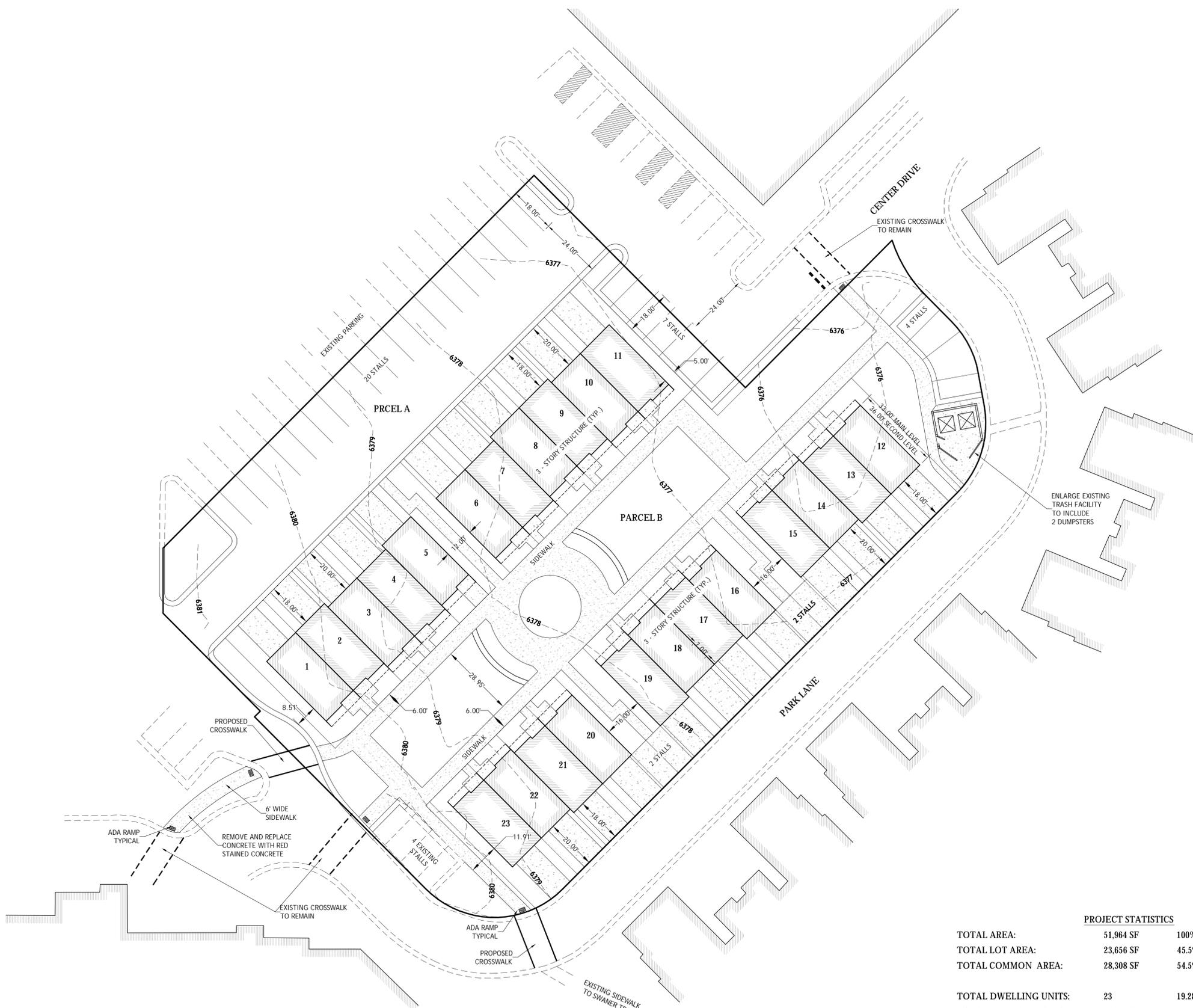


SCALE: 1" = 20'



DEVELOPER:
Hamlet Development
308 East 4500 South, Suite 200
Murray, UT 84107
801-281-2223

NOTES:



PROJECT STATISTICS		
TOTAL AREA:	51,964 SF	100% OF TOTAL
TOTAL LOT AREA:	23,656 SF	45.5% OF TOTAL
TOTAL COMMON AREA:	28,308 SF	54.5% OF TOTAL
TOTAL DWELLING UNITS:	23	19.28 UNITS PER ACRE
TOTAL HABITABLE SPACE:	29,041 SF	
PARKING SPACES:	23 COVERED (ONE PER UNIT)	
	62 UNCOVERED (4 EXISTING STALLS)	

Nevis at Newpark
1389 Center Drive
Site Plan

PROJECT:	1401
DRAWN BY:	NMM
REVIEWED BY:	NMM
REVISIONS:	
No. DATE	REMARKS

DATE: May 19, 2014

SHEET NUMBER: **C-1**



LANDSCAPE ARCHITECTURE & LAND PLANNING

1473 SOUTH 1100 EAST
S U I T E B
SALT LAKE CITY, UTAH 84105
8 0 1 . 5 5 4 . 6 1 4 6
STBDESIGNLLC.COM

Drawn By: Scott B.

ISSUE DESCRIPTION	DATE
	05.8.2014

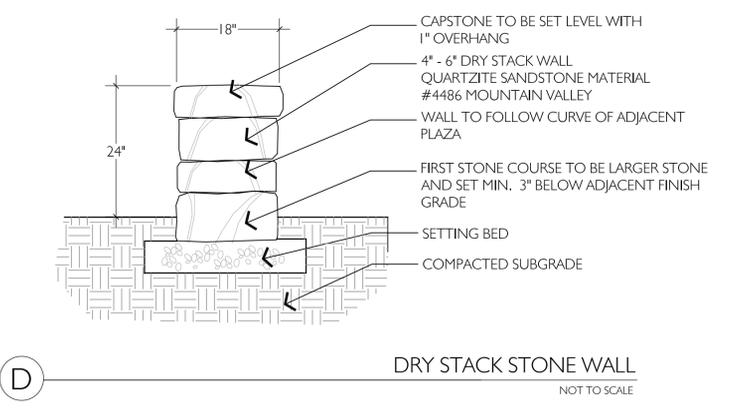
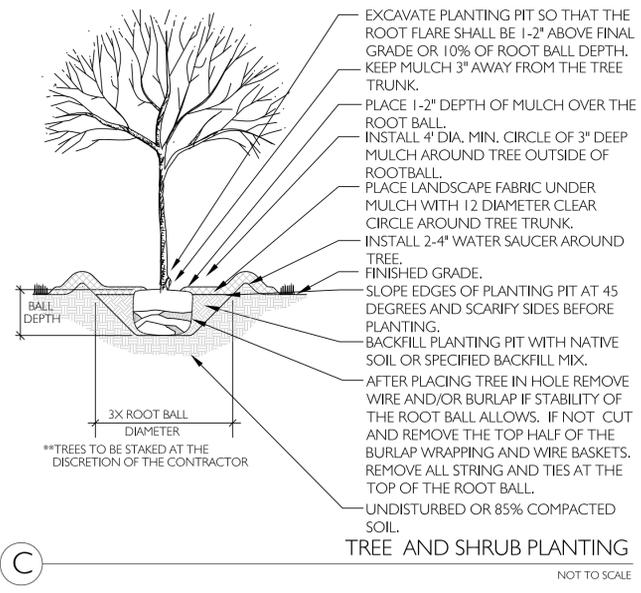
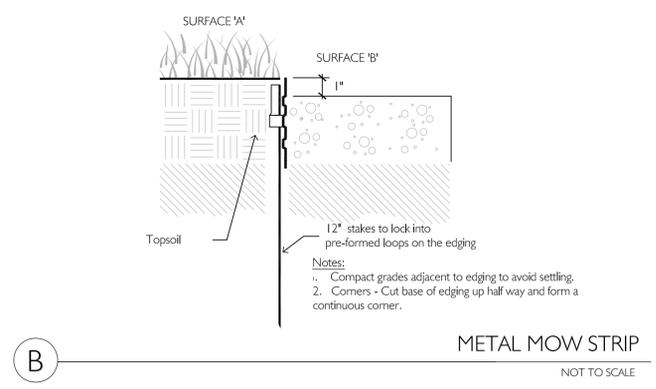
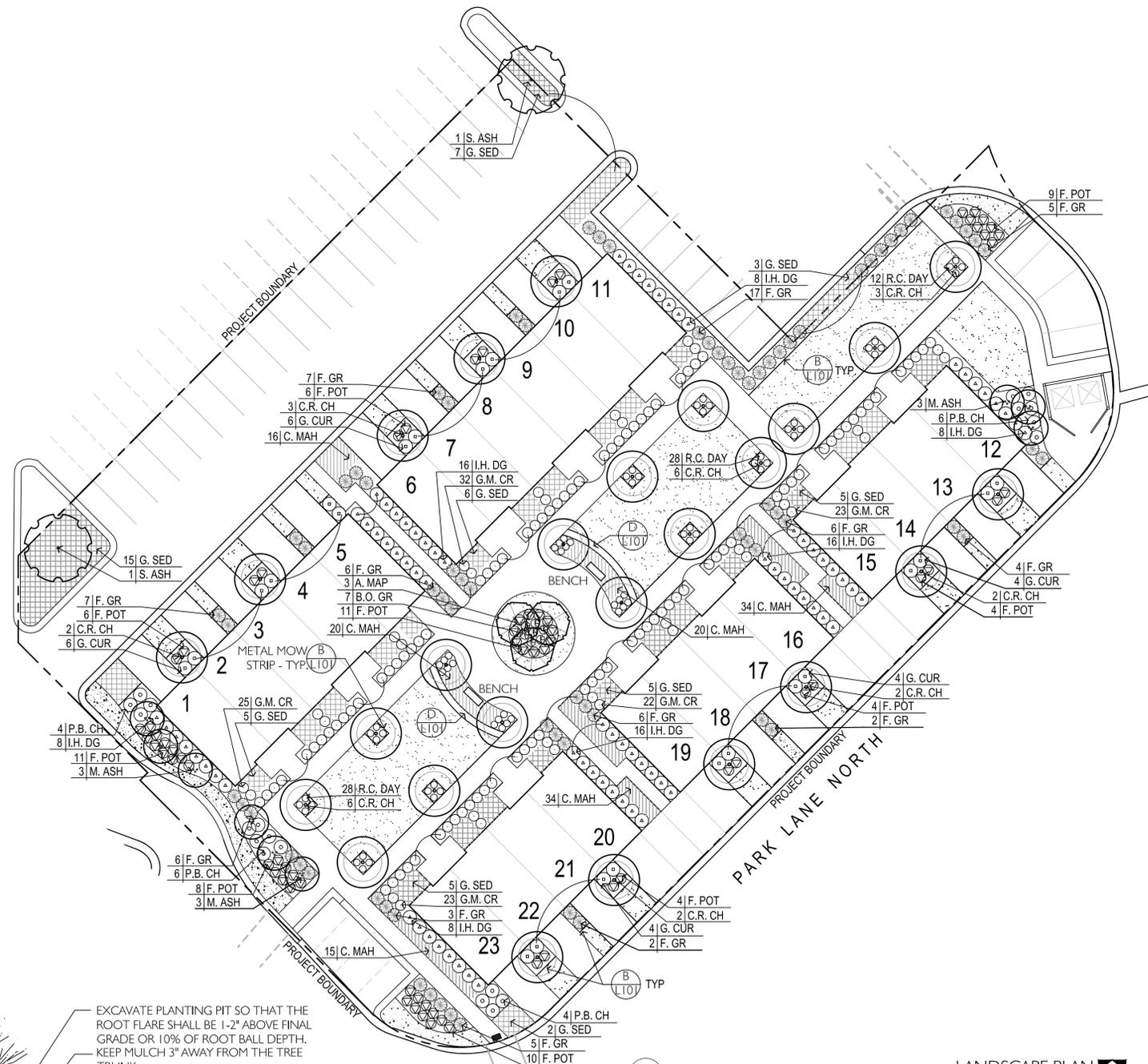
#	REV. DESCRIPTION	DATE
▲		
▲		
▲		
▲		

LANDSCAPE SCHEDULE

Sym.	Qty.	Botanical Name	Common Name	Size	Spacing
DECIDUOUS TREES					
A. MAP	3	Acer Ginnala	Amur Maple	2" Cal.	As Shown
S. ASH	2	Fraxinus pennsylvanica 'Summit'	Summit Ash	2" Cal.	As Shown
C.R. CH	26	Prunus virginiana 'Canada Red'	Canada Red Chokecherry	2" Cal.	As Shown
M. ASH	9	Sorbus aucuparia 'Fastigiata'	Pyramidal Mountain Ash	2" Cal.	As Shown
SHRUBS					
I.H. DG	80	Cornus alba 'Bailhalo'	Ivory Halo Dogwood	2 Gal.	4' o.c.
F. POT	73	Potentilla fruticosa 'Frosty'	Frosty Shrubby Cinquefoil	2 Gal.	3' o.c.
P.B. CH	20	Prunus besseyi 'Pawnee Buttes'	Pawnee Buttes Sand Cherry	2 Gal.	4' o.c.
G. CUR	24	Ribes aureum	Golden Currant	2 Gal.	4' o.c.
G.M. CR	125	Ribes alpinum 'Green Mound'	Green Mound Currant	2 Gal.	3' o.c.
ORNAMENTAL GRASSES, PERENNIALS, AND GROUNDCOVERS					
R.C. DAY	68	Hemerocallis 'Rocket City'	Rocket City Daylily	1 Gal.	24" o.c.
B.O. GR	7	Helictotrichon spempervirens	Blue Oat Grass	1 Gal.	30" o.c.
C. MAH	139	Mahonia repens	Creeping Mahonia	1 Gal.	24" o.c.
F. GR	76	Miscanthus sinensis purpurascens	Flame Grass	1 Gal.	4' o.c.
G. SED	46	Sedum acre 'Matrona'	Goldmoss Stonecrop	Flat / 36	12" o.c.
LAWN					
5,930 S.F.		Water Wise Bluegrass		Sod	
MULCH					
9,038 S.F.		Medium Wood Mulch		Min. 3"	
747 S.F.		Gravel, Crushed, 1 1/2"			
MULCH					
		Metal, Min, 3/16" x 4" - All planter bed / turf / gravel mulch interfaces			

LANDSCAPE GENERAL NOTES

- All alterations to these drawings during construction shall be approved by the Project Representative and recorded on "as Built" drawings by the Contractor.
- All plant materials shall conform to the minimum guidelines established by the American Standard for Nursery Stock, published by the American Nursery Association, Inc.
- All plants to be balled and burlapped or container grown, unless otherwise noted on the plant list.
- The contractor shall supply all plant material in quantities sufficient to complete the planting shown on the drawings.
- Any proposed substitutions of plant species shall be made with plants of equivalent overall form, height, branching habit, flower, leaf color, fruit and culture only as approved by the Landscape Architect.
- The Contractor shall locate and verify all existing utility lines prior to planting and shall report any conflicts to the Landscape Architect.
- Stake location of all proposed planting for approval by the Landscape Architect prior to commencement of planting.
- All turf areas shall receive four inches (4") of topsoil prior to planting. All shrub, groundcover, and perennial beds shall receive four inches (4") of topsoil prior to planting.
- Submit topsoil report prepared by a qualified soil testing laboratory prior to soil placement. topsoil shall meet the following mechanical analysis:
Sand (0.05 - 2.0 mm Dia.) 20 - 70%
Clay (0.002 - 0.05 mm Dia.) 20 - 70%
The max. retained on a #10 sieve will be 15 percent. the topsoil shall meet the following analysis criteria:
pH Range of 5.5 to 8.2, a min. of 4% and max. of 8% organic matter content and free of stones 3/4" or larger. Soluble salts <2 dS/m or mmho/cm and sodium absorption ration (sar) <6.
- All tree rings and plant beds to receive mulch as specified in the Landscape Schedule.
- Prune trees in accordance with current horticultural practices.
- All landscape areas to be watered by pop-up spray heads, rotors or drip irrigation. pop-up spray heads, rotors and drip irrigation to be placed on separate irrigation zones.
- All shrubs, groundcover and perennial plants to be watered on zones separate from turf.



NEVIS AT NEWPARK
NORTH PARK LANE
PARK CITY, UTAH

LANDSCAPE PLAN

L101



LANDSCAPE ARCHITECTURE & LAND PLANNING

1473 SOUTH 1100 EAST
SUITE B
SALT LAKE CITY, UTAH 84105
801.554.6146
STBDESIGNLLC.COM

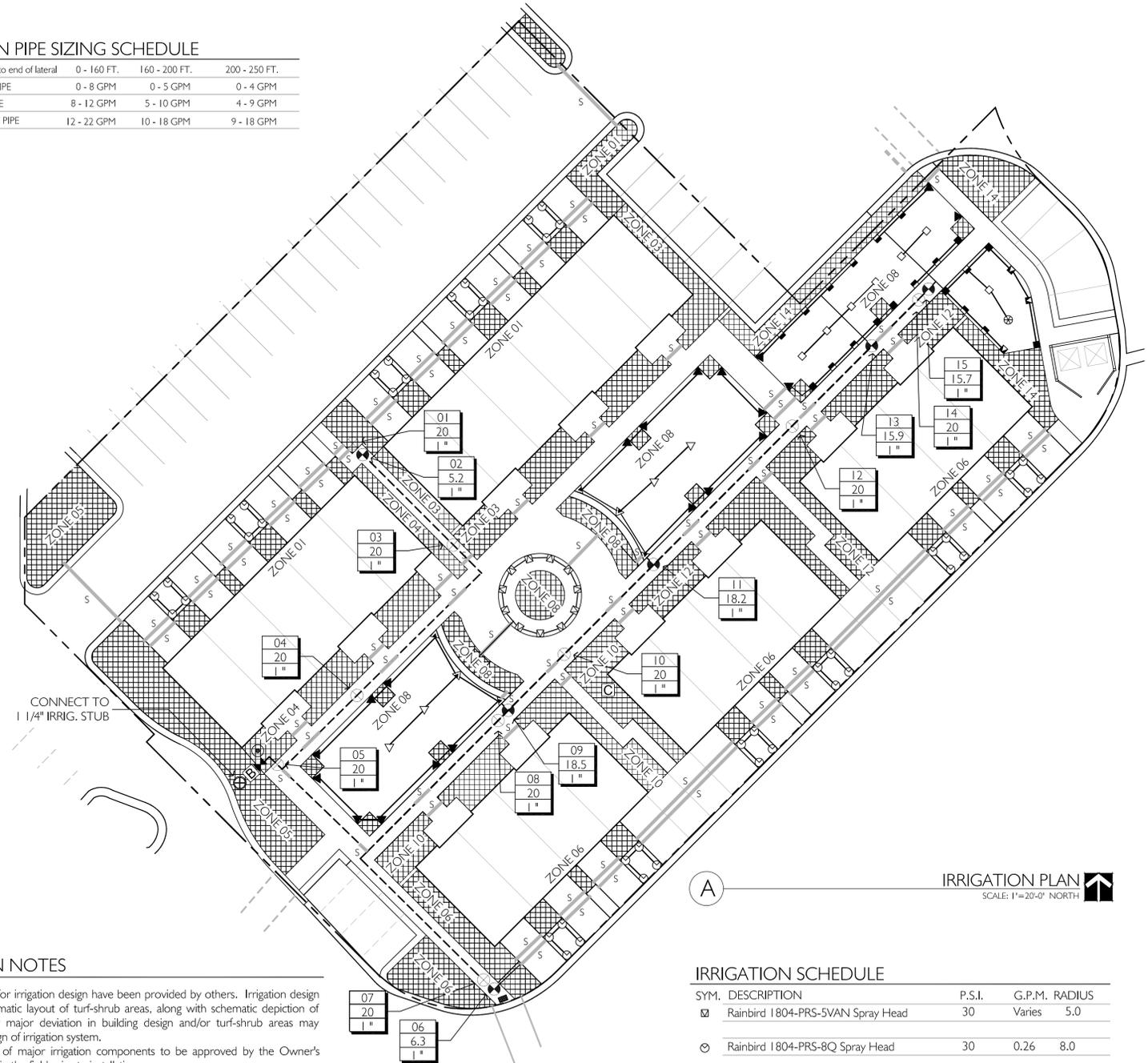
Drawn By: Scott B.

ISSUE DESCRIPTION	DATE
---	05.08.2014

#	REV. DESCRIPTION	DATE
▲		
▲		
▲		
▲		

IRRIGATION PIPE SIZING SCHEDULE

Distance from valve to end of lateral	0 - 160 FT.	160 - 200 FT.	200 - 250 FT.
3/4" SCH. 40 PVC PIPE	0 - 8 GPM	0 - 5 GPM	0 - 4 GPM
1" SCH. 40 PVC PIPE	8 - 12 GPM	5 - 10 GPM	4 - 9 GPM
1-1/4" SCH. 40 PVC PIPE	12 - 22 GPM	10 - 18 GPM	9 - 18 GPM



IRRIGATION NOTES

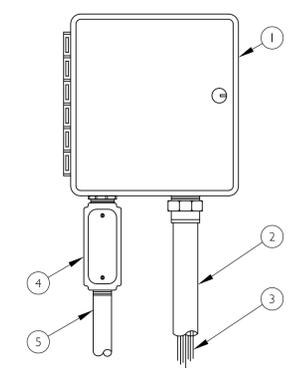
- Base drawings for irrigation design have been provided by others. Irrigation design based on schematic layout of turf-shrub areas, along with schematic depiction of buildings. Any major deviation in building design and/or turf-shrub areas may require re-design of irrigation system.
- Exact locations of major irrigation components to be approved by the Owner's Representative in the field prior to installation.
- Contractor is responsible to verify material counts and square footages. Irrigation table quantities provided as a courtesy. In the event of a discrepancy, plan quantities take precedence over table quantities.
- Contact the local underground utility services for utility location and identification.
- Perform excavation in the vicinity of underground utilities with care and if necessary, by hand. The Contractor bears full responsibility for this work and disruption or damage to utilities shall be repaired immediately at no expense to the Owner.
- Irrigation main line and/or other components are shown schematically in landscapes for graphic clarity only. All Irrigation components shall be located in landscaped areas.
- Place remote control valves in logical groupings as field conditions permit. All remote control valves and quick coupler valves shall be isolated from the main line via an isolation valve as shown in details.
- Quick coupler valves in landscaped areas shall be installed as close as possible to plan locations. Quick coupler valve spacing shall not exceed 200 feet apart to allow for hand watering of plant material.
- Sprinklers are placed at various percentages of manufacturers published radii. see Irrigation table for specific spacing. Spray heads typically shown at 90% of manufacturer's published coverage radius. Rotor heads typically shown at 90% of manufacturer's published coverage radius.
- Spray sprinklers are designed for 30 PSI at the head. Rotor sprinklers are designed for 50 PSI at the head. If operational pressure varies, coordinate with Landscape Architect.
- Not all sleeving necessary to complete this project is shown on plan. Portions of irrigation sleeving may have been previously installed by others. Coordinate location and usage with Owner's Representative.
- Rotor zones may be shown with same nozzles for half circle and full circle heads, full circle head zones shall need double run time on controller. Rotor zones using the same nozzle for half circle and full circle heads shall be placed on separate zones.
- POCs and Main line are designed for one zone to be operated at a time, per POC & Controller.

IRRIGATION SCHEDULE

SYM.	DESCRIPTION	P.S.I.	G.P.M.	RADIUS
☒	Rainbird 1804-PRS-5VAN Spray Head	30	Varies	5.0
☉	Rainbird 1804-PRS-8Q Spray Head	30	0.26	8.0
■	Rainbird 1804-PRS-10Q Spray Head	30	0.39	10.0
■	Rainbird 1804-PRS-10H Spray Head	30	0.79	10.0
□	Rainbird 1804-PRS-10F Spray Head	30	1.58	10.0
▼	Rainbird 1804-PRS-10 VAN Spray Head	30	Varies	10.0
▼	Rainbird 1804-PRS-15Q Spray Head	30	0.92	15.0
▼	Rainbird 1804-PRS-15H Spray Head	30	1.85	15.0
▼	Rainbird 1804-PRS-15F Spray Head	30	3.70	15.0
▼	Rainbird 1804-PRS-15 VAN Spray Head	30	Varies	15.0

- ☒ Inline Drip Line - Netafim-09-18-xxx
- ⊕ Rainbird PEB Automatic Valve
- ⊕ Rainbird XCCZ-100-PRBCOM
- ⊕ Isolation Gate Valve
- ⊕ Quick Coupling Valve Assembly
- ⊕ Rainbird ESP-16LXME Controller, 16 Stations
- ⊕ Backflow Preventer - 1"
- ⊕ Stop and Waste - 1"
- Lateral Pipe - Schedule 40 PVC
- 1 1/4" Sch 40 PVC Mainline
- Irrigation Sleeving (See Plan)

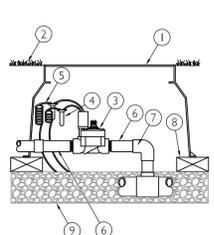
A-01	Valve #
26.7	GPM
1"	Valve Size



- CONTROLLER: OUTDOOR WALL MOUNT
- 1.5-INCH PVC SCH 40 CONDUIT AND FITTINGS
- WIRES TO REMOTE CONTROL VALVES
- JUNCTION BOX
- 1-INCH PVC SCH 40 CONDUIT TO POWER SUPPLY

OUTDOOR CONTROLLER MOUNT

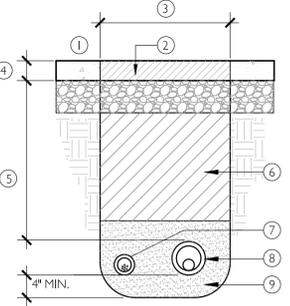
NOT TO SCALE



- STANDARD VALVE BOX
- FINISH GRADE
- REMOTE CONTROL VALVE (SEE EQUIPMENT SCHEDULE)
- WATERPROOF CONNECTORS (2)
- 18-24" COILED WIRE
- SCH 80 T.O.E. NIPPLE
- MAIN LINE PIPE & FITTINGS
- BRICK SUPPORTS (4)
- 3/4" MINUS WASHED GRAVEL

REMOTE CONTROL VALVE ASSEMBLY

NOT TO SCALE

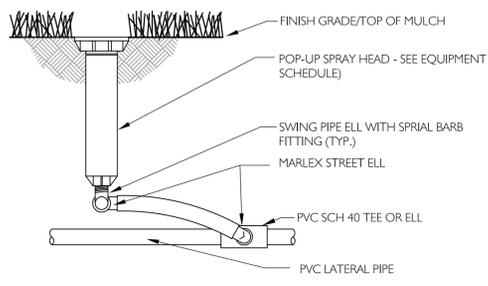


- EXISTING CONCRETE / ASPHALT
- SAWCUT & PATCH CONCRETE/ ASPHALT AS REQUIRED
- WIDTH AS REQUIRED
- DEPTH TO MATCH EXISTING
- DEPTH AS REQUIRED TO PROVIDE 2" MINIMUM COVER OVER SLEEVE
- COMPACTED BACKFILL
- CONTROL WIRES 6" TO EITHER SIDE OR 6" UNDER MAINLINE.
- PVC MAIN LINE/LATERAL
- PROVIDE MORTAR SAND BEDDING 4" AROUND SLEEVES

- NOTE: 1. SLEEVE TO BE 2" LARGER THAN PIPE TO BE SLEEVED.
2. WIRE SLEEVE SHALL BE TWICE THE DIAMETER OF THE WIRE BUNDLE.
3. SLEEVE TO EXTEND 12" INTO PLANTING AREA.

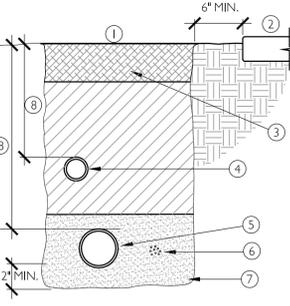
PIPE SLEEVING

NOT TO SCALE



4" POP-UP SPRAY HEAD DETAIL

NOT TO SCALE

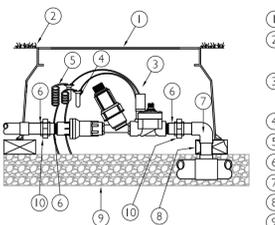


- IF LAWN IS EXISTING, REPLACE SOD AS PER OWNER'S SPECIFICATIONS
- ADJACENT HARD SURFACE
- TOPSOIL
- NON-PRESSURE LATERAL LINE
- PRESSURE MAIN LINE
- DIRECT BURIAL, LOW VOLTAGE CONTROL WIRES: TAPE AND BUNDLE AT 10" O.C. PLACE 6" EITHER SIDE OF PIPE OR 6" BELOW.
- MORTAR SAND BEDDING 2" BELOW AND ABOVE PIPE IN AREAS OF ROCK LADEN SOIL
- PIPE DEPTH: MAIN LINE: 18 - 30" COVER LATERAL LINE: 8 - 14" COVER

NOTE: SEE SLEEVING DETAIL FOR TRENCHING IN PAVED AREAS.

PIPE TRENCH

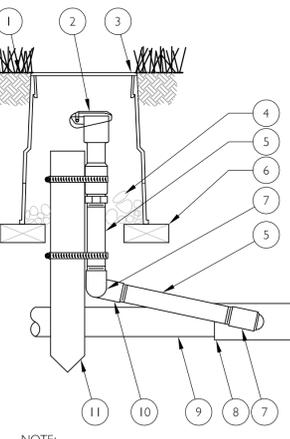
NOT TO SCALE



- JUMBO VALVE BOX
- FINISH GRADE
- DRIP ZONE CONTROL ZONE ASSEMBLY (SEE EQUIPMENT SCHEDULE)
- WATERPROOF CONNECTORS (2)
- 18-24" COILED WIRE
- SCH 80 T.O.E. NIPPLE
- MAIN LINE PIPE & FITTINGS
- BRICK SUPPORTS (4)
- 3/4" MINUS WASHED GRAVEL
- PVC SLIP UNIONS

DRIP IRRIGATION VALVE ASSEMBLY

NOT TO SCALE

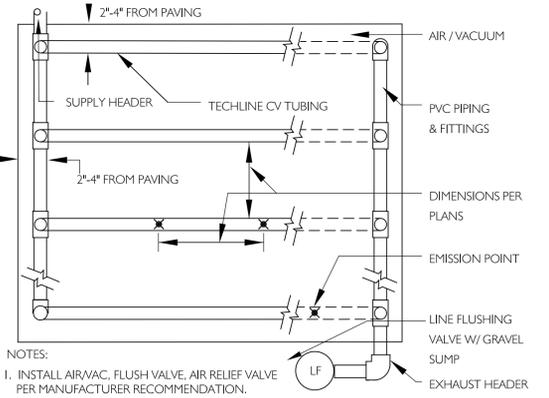


- FINISH GRADE/TOP OF MULCH
- QUICK-COUPLING VALVE:
- VALVE BOX WITH COVER:
- 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL
- PVC SCH 80 NIPPLE (LENGTH AS REQUIRED)
- BRICK (1 OF 2)
- PVC SCH 40 STREET ELL
- PVC SCH 40 TEE OR ELL
- PVC MAINLINE PIPE
- PVC SCH 40 ELL
- 2" x 2" REDWOOD STAKE WITH STAINLESS STEEL GEAR CLAMPS OR EQUIVALENT SUPPORT SYSTEM

NOTE: FURNISH FITTINGS AND PIPING NOMINALLY SIZED IDENTICAL TO NOMINAL QUICK COUPLER VALVE INLET SIZE.

QUICK COUPLER VALVE

NOT TO SCALE



- NOTES:
1. INSTALL AIR/VAC, FLUSH VALVE, AIR RELIEF VALVE PER MANUFACTURER RECOMMENDATION.

IN LINE DRIP TUBING SAMPLE LAYOUT

NOT TO SCALE

NEVIS AT NEWPARK
NORTH PARK LANE
PARK CITY, UTAH

IRRIGATION PLAN

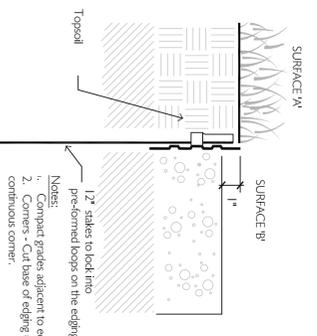
L102

LANDSCAPE SCHEDULE

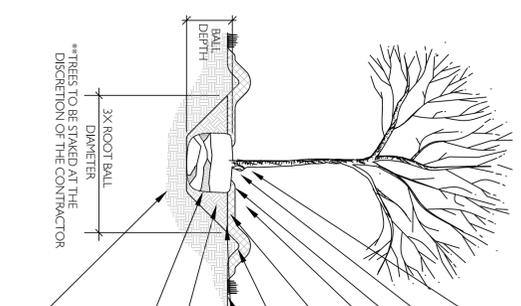
Sym.	Qty.	Botanical Name	Common Name	Size	Spacing
DECIDUOUS TREES					
A	MAP	3	Acer Ginnala	2 1/2 Cal.	As Shown
5	ASH	2	Fraxinus pennsylvanica Summit'	2 1/2 Cal.	As Shown
C	R. CH	26	Prunus virginiana Canada Red'	2 1/2 Cal.	As Shown
M	ASH	9	Sorbus aucuparia Fastigiat'	2 1/2 Cal.	As Shown
SHRUBS					
4	I.H. DG	80	Cornus alba Balliald'	2 Gal.	4' o.c.
F	POT	73	Potentilla fruticosa Frosty'	2 Gal.	3' o.c.
P	B. CH	20	Prunus besseyi 'Fawnree Buttes'	2 Gal.	4' o.c.
G	CUR	24	Ribes aureum Golden Currant	2 Gal.	4' o.c.
G	M. CR	121	Ribes alpinum Green Mound'	2 Gal.	3' o.c.
ORNAMENTAL GRASSES, PERENNIALS, AND GROUNDCOVERS					
R	C. DAY	68	Hemerocallis Rocket City'	1 Gal.	24" o.c.
B	O. GR	7	Helictotriton spemperiens Blue Oat Grass	1 Gal.	30" o.c.
C	MAH	139	Malonia repens Creeping Malonia	1 Gal.	24" o.c.
F	GR	75	Miscanthus sinensis purpureus Flame Grass	1 Gal.	4' o.c.
G	SED	46	Sedum acre 'Marston'	Flat / 36	12" o.c.
LAWN					
5	930 S.F.		Water Wise Bluegrass	Sod	
MULCH					
9	9,038 S.F.		Medium Wood Mulch	Min. 3"	
4	747 S.F.		Gravel, Crushed, 1 1/2"		
MULCH					
			Metal, Min. 3/16" x 4" - All planter bed / turf / gravel mulch interfaces		

LANDSCAPE GENERAL NOTES

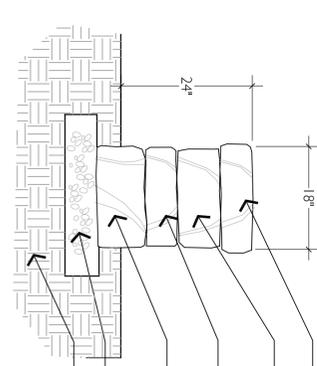
- All alterations to these drawings during construction shall be approved by the Project Representative and recorded on "as Built" drawings by the Contractor.
- All plant materials shall conform to the minimum guidelines established by the American Standard for Nursery Stock, published by the American Nursery Association, Inc.
- All plants to be balled and burlapped or container grown, unless otherwise noted on the plant list.
- The contractor shall supply all plant material in quantities sufficient to complete the planting shown on the drawings.
- Any proposed substitutions of plant species shall be made with plants of equivalent overall form, height, branching habit, flower, leaf color, fruit and culture only as approved by the Landscape Architect.
- The Contractor shall locate and verify all existing utility lines prior to planting and shall report any conflicts to the Landscape Architect.
- Stake location of all proposed planting for approval by the Landscape Architect prior to commencement of planting.
- All turf areas shall receive four inches (4") of topsoil prior to planting. All shrub, groundcover, and perennial beds shall receive four inches (4") of topsoil prior to planting.
- Submit topsoil report prepared by a qualified soil testing laboratory prior to soil placement. topsoil shall meet the following mechanical analysis:
Sand (0.05 - 2.0 mm Dia.) 20 - 70%
Clay (0.002 - 0.05 mm Dia.) 20 - 70%
The max. retained on a # 10 sieve will be 15 percent. the topsoil shall meet the following analysis criteria:
pH Range of 5.5 to 8.2, a min. of 4% and max. of 8% organic matter content and free of stones 3" or larger. Soluble salts < 2 dS/m or mmho/cm and sodium absorption ration (SAR) < 6.
- All tree rings and plant beds to receive mulch as specified in the Landscape Schedule.
- Prune trees in accordance with current horticultural practices.
- All landscape areas to be watered by pop-up spray heads, rotors or drip irrigation. pop-up spray heads, rotors and drip irrigation to be placed on separate irrigation zones.
- All shrubs, groundcover and perennial plants to be watered on zones separate from turf.



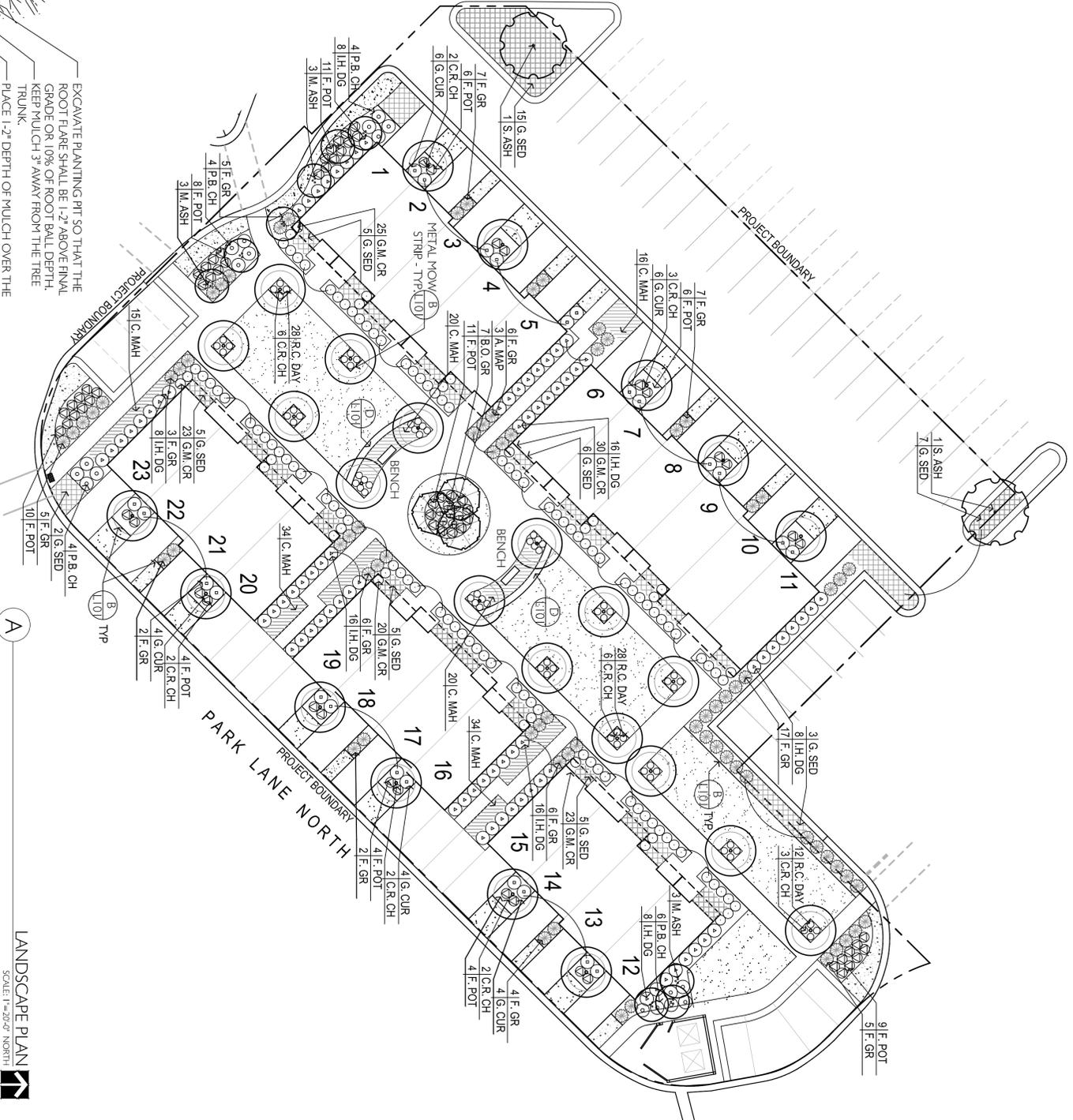
METAL MULCH STRIP
NOT TO SCALE



TREE AND SHRUB PLANTING
NOT TO SCALE



DRY STACK STONE WALL
NOT TO SCALE



LANDSCAPE PLAN
SCALE 1" = 20'-0" NORTH

ESTB
DESIGN
LANDSCAPE ARCHITECTURE
& LAND PLANNING
1473 SOUTH 1100 EAST
S U I T E B
SALT LAKE CITY, UTAH 84105
801.554.6146
STBDESIGNLLC.COM

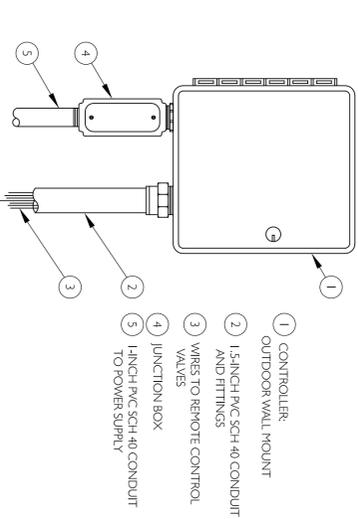
Drawn By: Scott B.

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	02.22.2014
# REV. DESCRIPTION	DATE

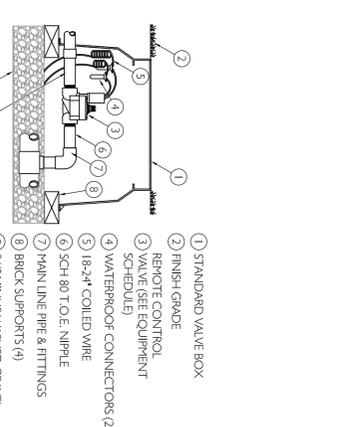
NEVIS AT NEWPARK
NORTH PARK LANE
PARK CITY, UTAH

LANDSCAPE PLAN

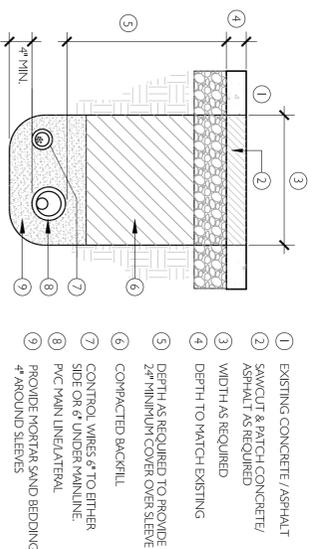
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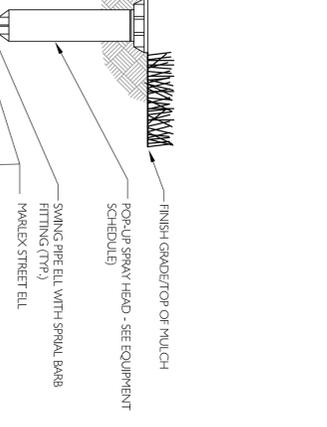
A OUTDOOR CONTROLLER MOUNT
NOT TO SCALE



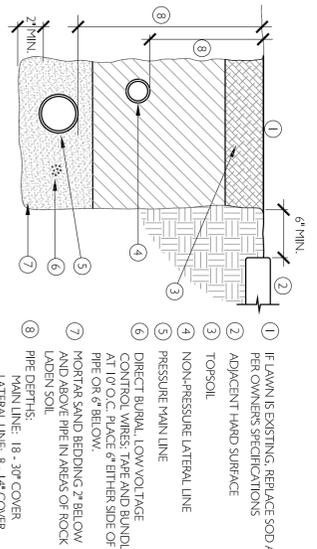
E REMOTE CONTROL VALVE ASSEMBLY
NOT TO SCALE



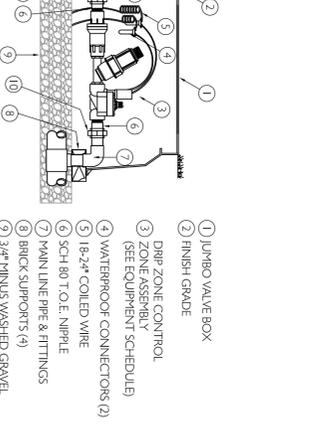
B PIPE SLEEVING
NOT TO SCALE



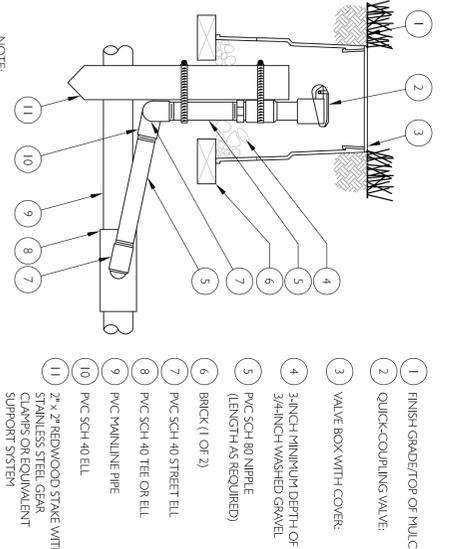
F 4" POP-UP SPRAY HEAD DETAIL
NOT TO SCALE



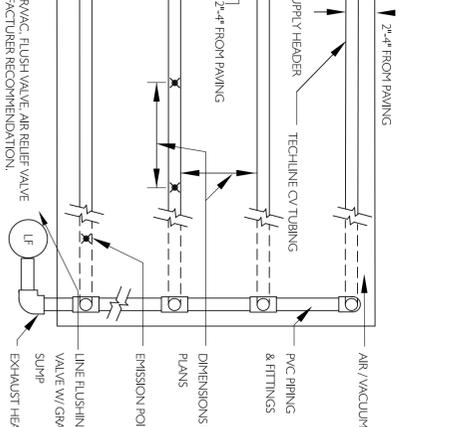
C PIPE TRENCH
NOT TO SCALE



G DRIP IRRIGATION VALVE ASSEMBLY
NOT TO SCALE



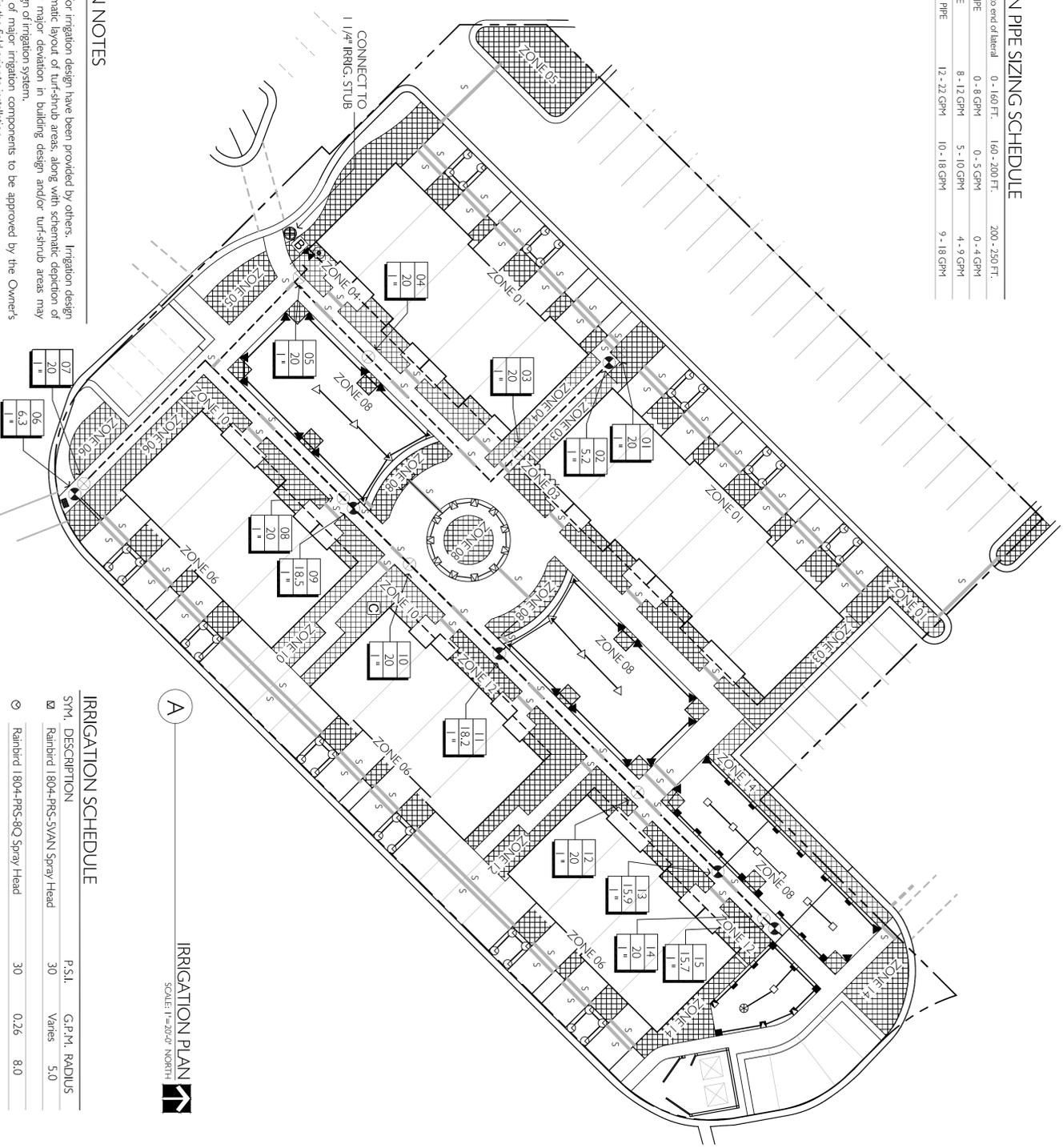
D QUICK COUPLER VALVE
NOT TO SCALE



H IN LINE DRIP TUBING SAMPLE LAYOUT
NOT TO SCALE

IRRIGATION PIPE SIZING SCHEDULE

Distance from valve to end of lateral	0 - 160 FT.	160 - 200 FT.	200 - 250 FT.
3/4" SCH. 40 PVC PIPE	0 - 8 GPM	0 - 5 GPM	0 - 4 GPM
1" SCH. 40 PVC PIPE	8 - 12 GPM	5 - 10 GPM	4 - 9 GPM
1 1/4" SCH. 40 PVC PIPE	12 - 22 GPM	10 - 18 GPM	9 - 18 GPM



IRRIGATION PLAN
SCALE: 1" = 20' NORTH

IRRIGATION NOTES

1. Base drawings for irrigation design have been provided by others. Irrigation design based on schematic layout of turf-stand areas, along with schematic depiction of buildings. Any major deviation in building design and/or turf-stand areas may require re-design of irrigation system.
2. Exact locations of major irrigation components to be approved by the Owner's Representative in the field prior to installation.
3. Representative in the field to verify material counts and square footages. Irrigation table quantities provided as a courtesy. In the event of a discrepancy, plan quantities take precedence over table quantities.
4. Contact the local underground utility services for utility location and identification.
5. Perform excavation in the vicinity of underground utilities with care and if necessary, by hand. The Contractor bears full responsibility for this work and disruption or damage to utilities shall be repaired immediately at no expense to the Owner.
6. Irrigation main line and/or other components are shown schematically in landscaped areas for graphic clarity only. All Irrigation components shall be located in landscaped areas.
7. Place remote control valves in logical groupings as field conditions permit. All remote control valves and quick coupler valves shall be isolated from the main line via an isolation valve as shown in details.
8. Quick coupler valves in landscaped areas shall be installed as close as possible to plan locations. Quick coupler valve spacing shall not exceed 200 feet apart to allow for hand watering of plant material.
9. Sprinklers are placed at various percentages of manufacturers published radii, see Irrigation table for specific spacings. Spray heads typically shown at 90% of manufacturer's published coverage radius. Rotor heads typically shown at 90% of manufacturer's published coverage radius.
10. Spray sprinklers are designed for 30 PSI at the head. Rotor sprinklers are designed for 50 PSI at the head. If operational pressure varies, coordinate with Landscape Architect.
11. Not all sleeving necessary to complete this project is shown on plan. Portions of irrigation sleeving may have been previously installed by others. Coordinate location and usage with Owner's Representative.
12. Rotor zones may be shown with same nozzles for half circle and full circle heads, full circle head zones shall need double run time on controller. Rotor zones using the same nozzle for half circle and full circle heads shall be placed on separate zones.
13. POC's and Main line are designed for one zone to be operated at a time, per POC & Controller.

IRRIGATION SCHEDULE

SYM.	DESCRIPTION	P.S.I.	G.P.M.	RADIUS
☒	Rainbird 1804-PRS-SVAN Spray Head	30	Varies	5.0
☉	Rainbird 1804-PRS-8Q Spray Head	30	0.26	8.0
■	Rainbird 1804-PRS-10Q Spray Head	30	0.38	10.0
■	Rainbird 1804-PRS-10H Spray Head	30	0.79	10.0
■	Rainbird 1804-PRS-10F Spray Head	30	1.88	10.0
■	Rainbird 1804-PRS-10 VAN Spray Head	30	Varies	10.0
▼	Rainbird 1804-PRS-15Q Spray Head	30	0.92	15.0
▼	Rainbird 1804-PRS-15H Spray Head	30	1.85	15.0
▼	Rainbird 1804-PRS-15F Spray Head	30	3.70	15.0
▼	Rainbird 1804-PRS-15 VAN Spray Head	30	Varies	15.0

- Line Drip Line - Netalim-09-18-xxx
- Rainbird RBZ Automatic Valve
- Rainbird X/CZ-100-PRR/COM1
- Isolation Gate Valve
- Quick Coupling Valve Assembly
- Rainbird ESP-16LX/VE Controller, 16 Stations
- Backflow Preventer - 1"
- Stop and Waste - 1"
- Lateral Pipe - Schedule 40 PVC
- 1 1/4" Sch. 40 PVC Mainline
- Irrigation Sleeving (See Plan)

ISSUE DESCRIPTION	DATE	
---	05.22.2014	
#	REV. DESCRIPTION	DATE



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NEVIS AT NEWPARK
NORTH PARK LANE
PARK CITY, UTAH

SITE PLAN

L100

A LANDSCAPE PLAN
SCALE: 1"=20'-0" NORTH









05.6.2014

Memo

To:

Michael Brodsky
Chairman
Hamlet Homes

From:

Steven Lord
Project Manager

Re:

Nevis at Newpark Traffic
Review

Purpose and Introduction

The purposed of this memorandum is to update a previously completed traffic impact study for the Newpark development in Summit County, Utah. The study relates specifically to the parcel P-2 shown in the attached study and site plan.

History

Several studies have been completed in the Newpark development by Horrocks Engineers over the past few years. The studies relevant to this project began with the ***Newpark Flats Trip Generation and Parking Demand*** study and report dated May 14, 2010. This study presented a change in the development plan for one of the Newpark parcels and outlined the difference in trip generation and parking demand from the original site plan (30 condos, 28 townhomes, and 5,000 square feet of office space) to a new site plan consisting of 100 apartments. The study concluded that the trip generation would decrease with the new site plan and parking generation would increase.

On August 4th, 2011 Horrocks prepared a memo titled ***Cottonwood Three Office Building – Newpark Development Traffic Review***. This report discussed the traffic impacts of the proposed 60,000 square foot Cottonwood Three office building to be built on Lot P-1 of the Newpark development. The study concluded that the existing roads and intersections could accommodate the additional traffic generated by the office building without degrading beyond level of service (LOS) A.

The August 18th, 2011 addendum to the August 4th report, ***Cottonwood Three Office Building – Newpark Development Traffic Review Addendum***, discussed the effects of the adjacent parking lot on the traffic distribution assumed in the original report. This addendum determined that due to the proximity of the parking, very few vehicles would use Park Lane north to access the Cottonwood Three building. The study concluded that Park Lane North and the adjacent intersection would not be adversely affected by the traffic from Cottonwood Three. The Cottonwood Three addendum will form the basis for this memorandum and will be the starting point for all analysis discussed herein.

The last report was submitted on August 22, 2011 and was titled, ***Cottonwood Three Office Building – 2030 Traffic Analysis***. This report provided a future projected condition based on the Cottonwood Three office building traffic and background growth in the area. The study

Nevis at Newpark Traffic Review

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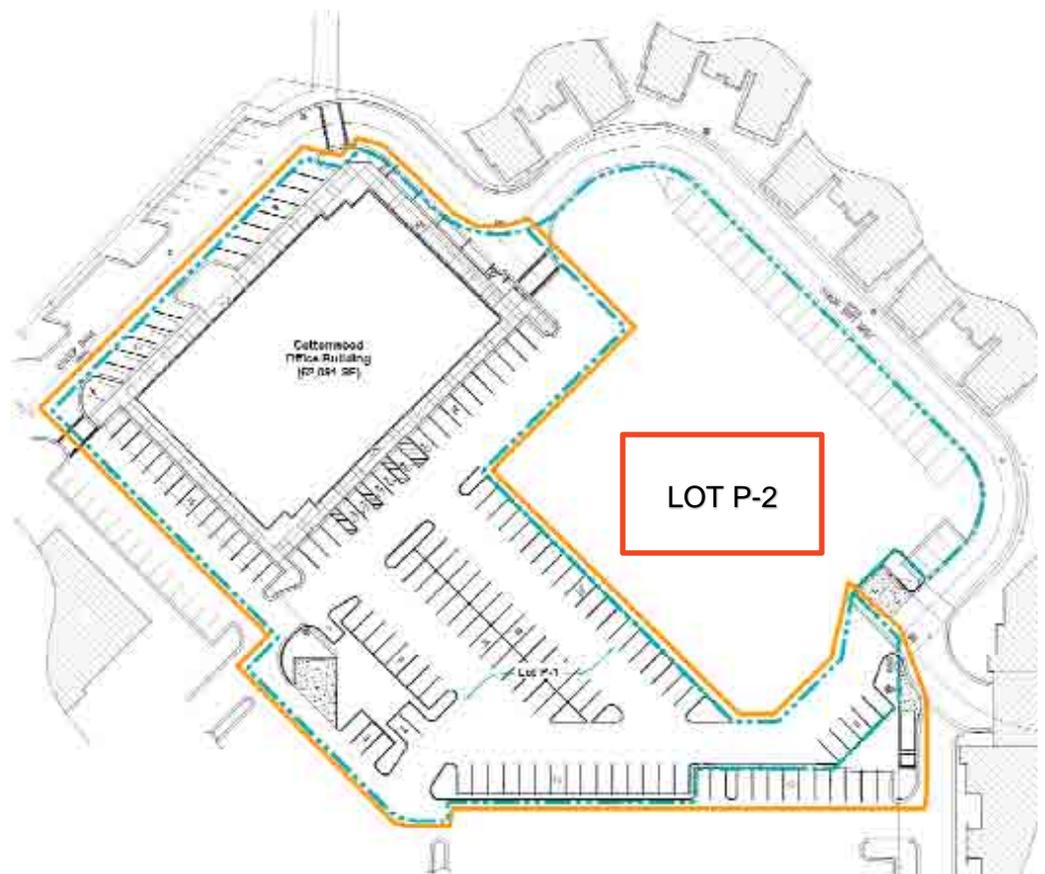


concluded that the intersections in the Newpark Development and Park Lane North will both accommodate traffic in the year 2030.

Nevis at Newpark

The development studied in this report is a townhome development on the P-2 parcel shown in Figure 1.

Figure 1 Lot P-2, Newpark



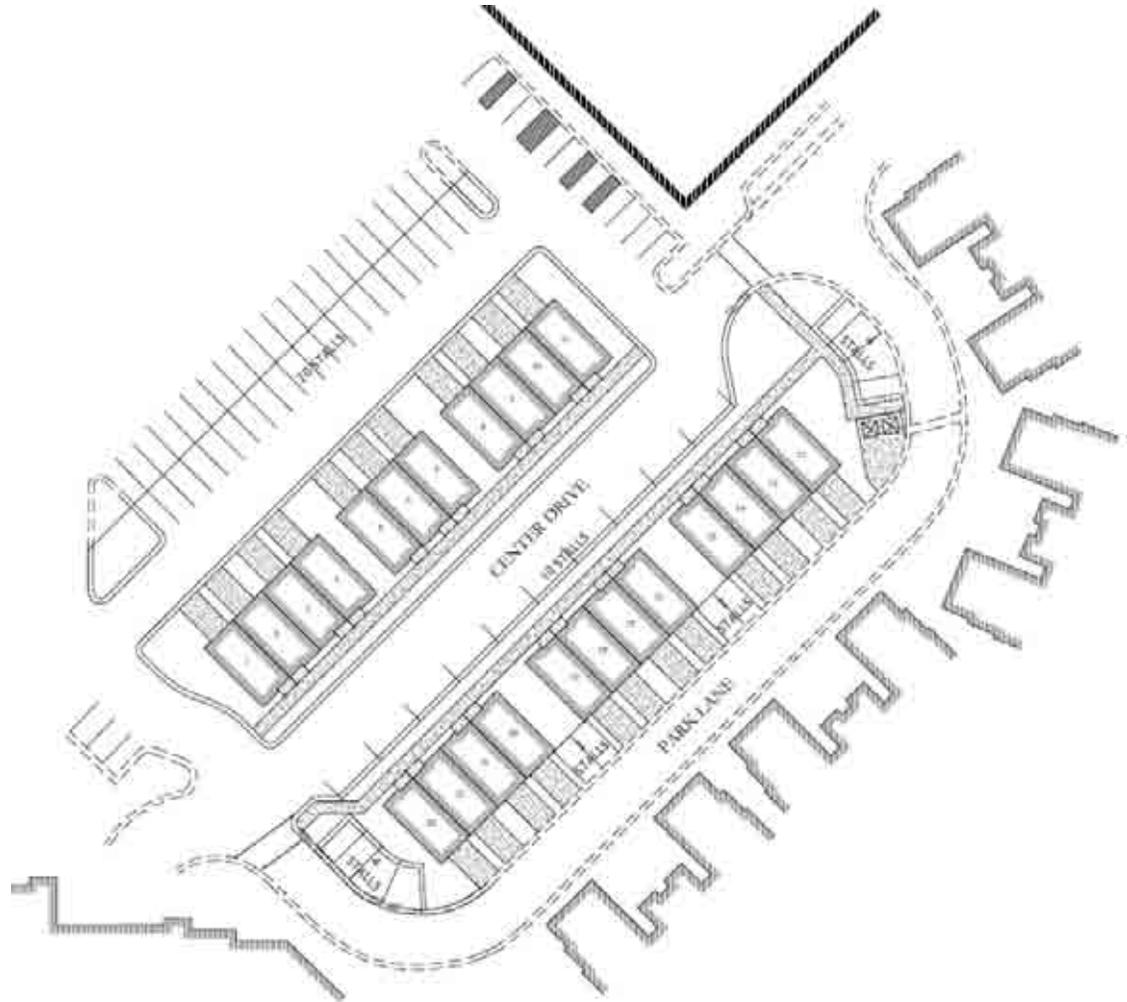
05.6.2014

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The development includes 23 townhomes, parking, green space and landscaping. Two site plans have been proposed. The first site plan (Figure 2) includes Center Drive per the original Newpark Development agreement.

Figure 2 Site Plan 1



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The second site plan (Figure 3) removes Center Drive and replaces it with a pedestrian plaza.

Figure 3 Site Plan 2



Existing Traffic Conditions

The previous studies indicated that Park Lane North carried approximately 200 vehicles per day. Since that time new data has been collected which shows that traffic has decreased on Park Lane North to approximately 120 vehicles per day. Center Drive southwest of the proposed development is carrying approximately 477 vehicles per day. April is the month of the year where the Newport Resort Hotel sees some of its lowest occupancy rates so the traffic volumes on each of the roadways are lower than would be expected at the peak times of the year. The Newport Resort Hotel provided occupancy data for the hotel over the past year. In April 2013 the Hotel sold 1232 of 4410 available rooms for an occupancy rate of 28%. The peak month was July where the Hotel sold 2516 of 4464 available rooms for an occupancy rate of 56%. As

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such, the traffic volumes on Park Lane North and Center Drive were doubled for analysis purposes so that the effective traffic volumes were 240 vehicles per day on Park Lane North and 944 vehicles per day on Center Drive.

The area where the future Center Drive would be constructed is currently a dirt lot with new landscaping around the outer edge. There is no evidence that vehicles are cutting through the dirt to connect from Center Drive on the northeast to Center Drive on the southwest.

As there is little increase in Park Lane North traffic and after discussion with the Summit County Engineer, the basis for intersection operations is taken from the previous Cottonwood Three Office Building reports.

The Cottonwood Three Office Building reports studied the following three intersections:

1. Highland Drive/Newpark Boulevard
2. Highland Drive/Ute Boulevard
3. Park Lane North/Center Drive (southwest)

The PM peak hour operating conditions after Cottonwood Three is built for each of the intersections are shown in the table below. Each of the intersections was operating well below capacity.

Table 1 Existing Intersection Operations

Intersection	Delay (seconds/vehicle)	Level of Service
Highland Drive / Newpark Boulevard	8.0	A
Highland Drive / Ute Boulevard	8.1	A
Park Lane North / Center Drive	6.9	A

Source: Cottonwood Three Office Building – Newpark Development Traffic Review Addendum

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Development Traffic

Trip Generation

The Institute of Traffic Engineers' *Trip Generation Manual 9th Edition* was used to predict the number of trips that the Nevis at Newpark development will generate. According to the aforementioned manual, 23 townhomes will generate 134 daily trips. During the PM peak hour, the development will generate 8 inbound trips and 4 outbound trips.

Trip Distribution

Twelve of the townhome units have driveway access directly onto Park Lane North. The other units will be accessed from parking on the opposite side of the unit and will therefore not likely affect Park Lane North. Trips will likely be distributed evenly between Center Drive northeast and Center Drive southwest. This equates to 6 additional vehicles at each of the study intersections.

Future Traffic Conditions

As only 12 of the units have frontage onto Park Lane North the project increase in traffic is unlikely to use Park Lane North, this will be approximately 70 daily trips and 6 trips during the PM peak hour. The resulting number of daily vehicles that can be expected on Park Lane North is 310, far below the comfortable threshold of 1,000 vehicles per day that can be accommodated on such a local street.

The increase of traffic at each of the study intersections of 6 vehicles during the PM peak hour would result in an increase of approximately 2%. Therefore each of the study intersections will continue to operate at acceptable levels of service (C or better).

Special Events

The Newpark developments sees a myriad of special events throughout the year including concerts, weddings, holiday shopping and seasonal recreation. Each of these present unique traffic and operations challenges. Parking will be discussed later in this report but the timing of the parking demand of special events (evening and weekend) does not coincide with parking

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demand of offices (daytime) and residential uses (overnight), therefore parking for high demand special events can be accommodated by the existing and proposed parking supply.

As has been identified there are not expected to be in excess of 310 vehicles per day traveling on Park Lane North after the townhomes are developed and occupied. The comfortable threshold for daily traffic on Park Lane North is 1,000 vehicles per day, an additional 690. To put that into context 690 vehicles per day is the equivalent of another 123 townhomes or 69 single family homes. A special event would have to generate a highly unlikely amount of traffic to cause Park Lane North to exceed its comfortable threshold especially as Park Lane North is not the most direct or attractive route to take to access the retail area of Newpark or the Newpark Resort Hotel.

Removal of Center Drive

There are a number of arguments which can be made for the removal of the proposed Center Drive. These arguments are listed below:

1. The amount of traffic on Park Lane North after the construction of the Nevis Townhomes is less than 400 vehicles per day. This is far below the threshold and therefore this street will not benefit from an "alternate" parallel route such as Center Drive.
2. Replacing Center Drive with a pedestrian plaza will add the overall pedestrian feel of the Newpark Development helping to preserve the existing pedestrian friendly atmosphere.
3. The addition of green space over what essentially will be unnecessary roadway is generally preferred within sustainable development practices.
4. Center Drive is predominantly used by those accessing the condominiums currently there and the future Nevis at Newpark Townhomes and therefore is not likely to be subject to significant background growth or development growth in the surrounding area. The Newpark Resort Hotel is the closest land use that could affect the future Center Drive but it is unlikely that many, if any, visitors to the hotel will use Center Drive as the most logical entrance to the Hotel is from Newpark Boulevard. The same is true of the exiting Cottonwood Three Office Building. A survey of the most popular driving direction software (google, bing, mapquest, apple maps) all direct drivers to use Newpark Boulevard to access the hotel. This is also logical as the hotel parking is north of the hotel main entrance so approaching the main entrance from the south (Newpark

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Boulevard) will allow vehicles to continue straight into the parking area whereas approaching from the north (Center Drive) would require a U-Turn to access the hotel parking area.

Parking

Parking for the Nevis at Newpark development is provided in three ways. Each unit will have a one-car garage for indoor parking as well as driveway depth for one additional parked vehicle. Supplementary to the parking for each unit there will be 12 allocated surface parking stall for the existing Newpark townhomes and 27 surface parking stalls allocated for use by the Cottonwood Three building during the day. These 27 stalls will be available to visitors at the Townhomes during the evening and weekend hours. The total number of parking spaces for the Nevis at Newpark townhomes will therefore be 46 or 2 spaces per unit.

The ITE Parking Generation Manual 4th edition takes empirical data and determines average and 95th percentile parking demand. According to ITE, the peak parking demand for townhomes occurs during the night between 11pm and 5am and equates to an average demand of 1.38 vehicles per unit and a 95th percentile demand of 1.52 vehicles per unit. As indicated above, the Nevis townhomes parking supply is 2 stalls per unit and thus exceeds the minimum recommended parking demand by approximately 1 stall for every 2 units.

Pedestrian Circulation

The Newpark development is known for its pedestrian friendly environment. The wide walkways, narrow roads and appealing architecture and landscaping provide an attractive place for pedestrian use. The Nevis townhomes have sought to keep in step with this pedestrian theme by providing a wide, landscaped pedestrian plaza between the two rows of townhomes. Each of the townhomes front this plaza and it makes for an attractive beginning or end to any walking trip generated by the development.

Pedestrian connectivity is essential to sustainable development design. Currently the only pedestrian connection between the retail/restaurant area to the south of the Newpark Resort Hotel and the commercial/office complex to the North is through the parking lot via a striped area (shown in blue in Figure 4). This was always intended as a temporary solution until the Nevis

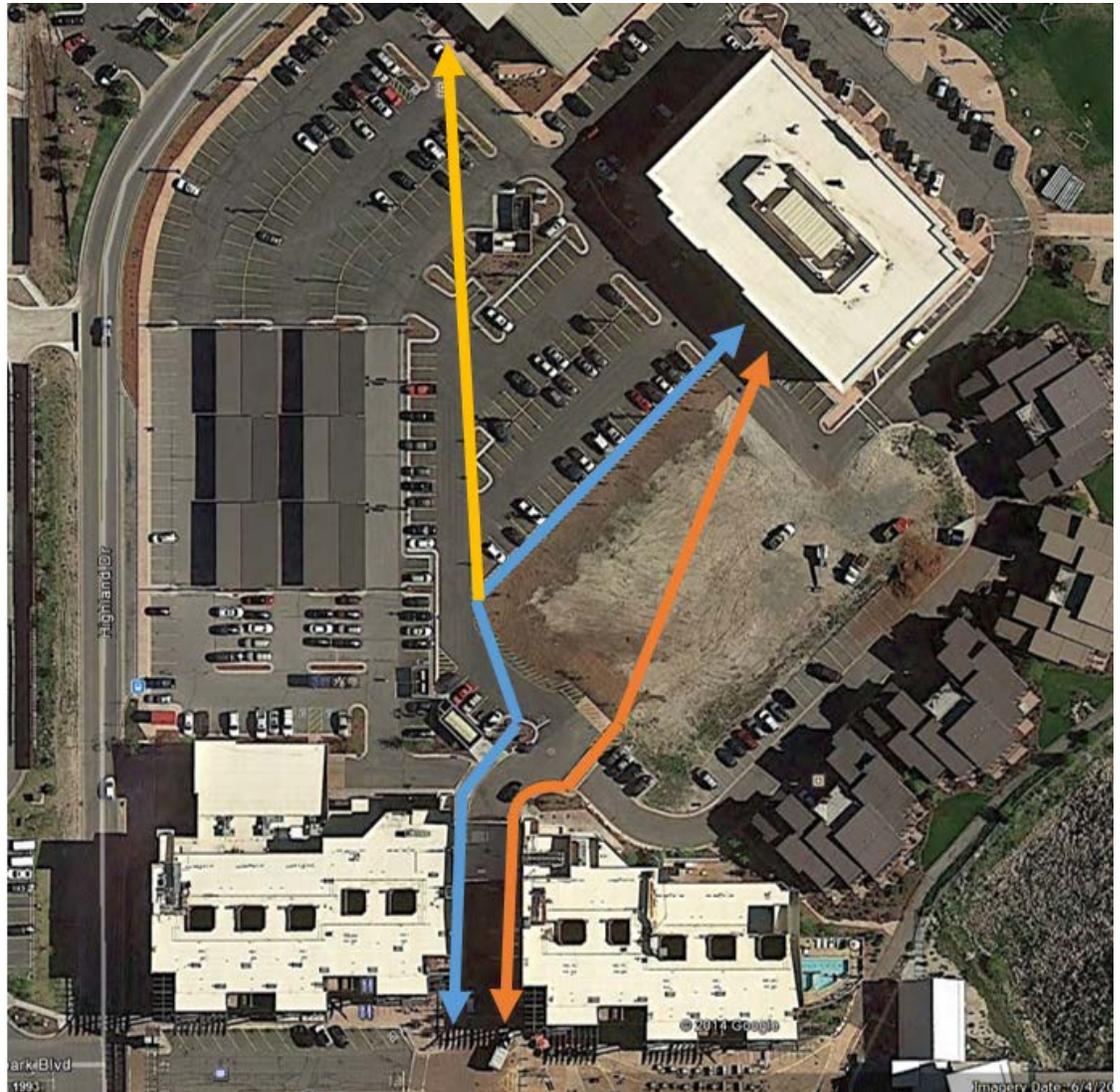
05.6.2014

Memo

Pg.09

area developed. Pedestrian traffic was also observed crossing the parking lot and the dirt area to other sites on the North end of the development (yellow and orange) without using the designated pedestrian area.

Figure 4 Existing North/South Pedestrian Paths



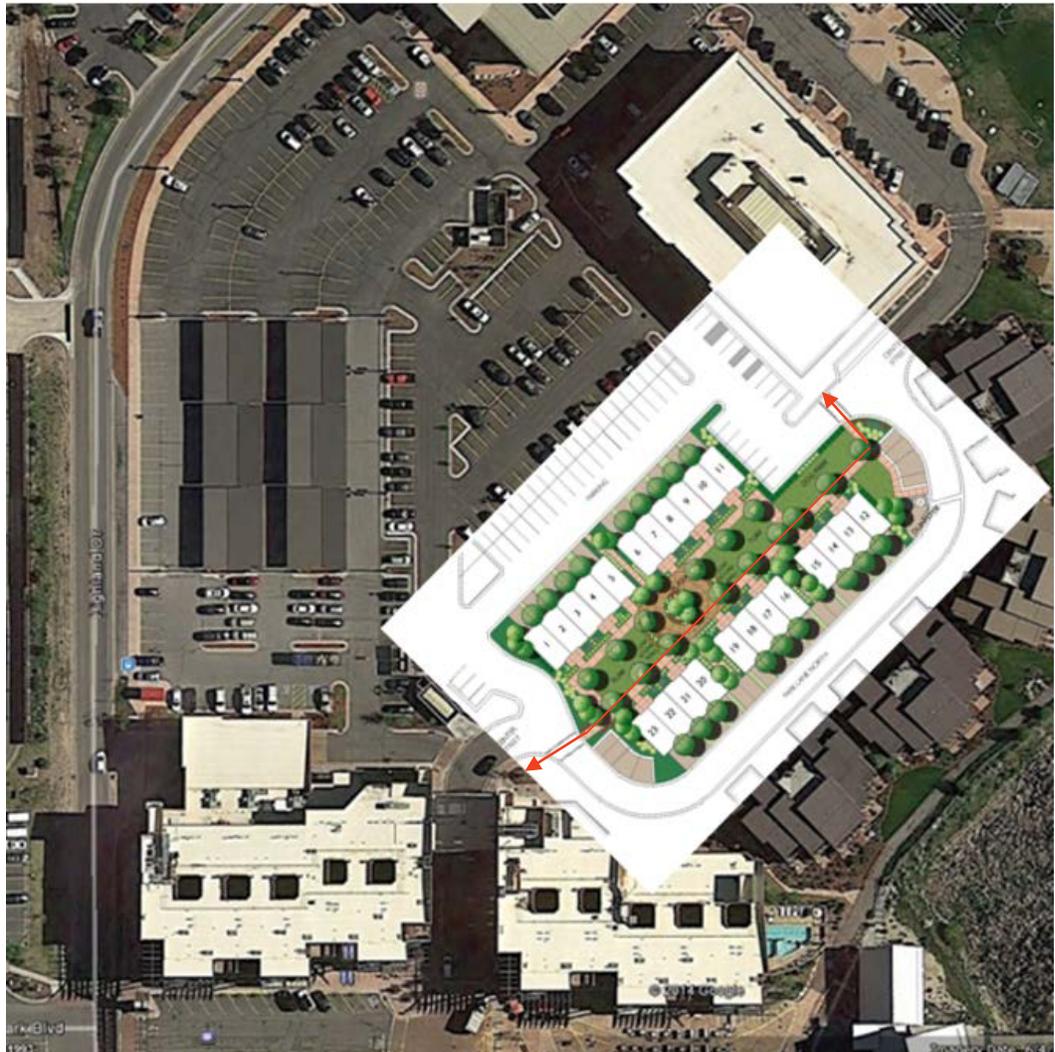
05.6.2014

Memo

Pg.10

The site plan proposed for the townhomes removes the temporary striping and mitigates the lack of pedestrian connectivity by providing a permanent direct connection between the sidewalk on the east side of Center Drive by the Hotel and the sidewalk on the south corner of the Cottonwood office building (shown below in red in Figure 5).

Figure 5 Future Permanent Pedestrian Connection



05.6.2014

Memo

Pg.11

The one potential area of concern for pedestrian traffic remains the connection from the west side of Center Drive by the hotel (Figure 6). There is a beaten path (shown in green) through the landscaping where it is obvious that pedestrians are cutting through the parking island and across to the striped paved area in the parking lot. This landscaped island is a less than ideal location for a sidewalk connection as the island contains trees, a transformer, a fire hydrant and passes directly in front of a trash enclosure. Any attempt to connect a sidewalk in this area will likely require the removal and replacement of some if not all of these amenities and would result in undue cost for little benefit. If this sidewalk connection were to be made it would only result in encouraging pedestrians to then walk through the parking lot rather than on the established trail/sidewalk network, a practice that should be discouraged rather than made more inviting. One possible solution to this problem would be to eliminate or discourage the pedestrian crossing on the west side of Center Drive and encourage pedestrian use of the existing crossing on the East side and the Nevis pedestrian plaza.

Figure 6 Pedestrian Connectivity



05.6.2014

Memo

Pg.12

Pedestrian access to the transit stop on Highland Drive from the Nevis development or the townhomes at Newport has been made virtually impossible via sidewalks and trails because of the parking lot directly north of the hotel. Wholesale changes to the configuration of the parking lot would be needed to provide more direct connection between the north and south areas within Newport. Had a pedestrian walkway been added between the Newport Hotel building and parking to the North, this connection could have been made for pedestrians.

Conclusions

- The roadways and intersections around the proposed Nevis at Newport development are currently under capacity.
- The Nevis at Newport development will add 134 daily trips and 12 PM peak hour trips.
- The addition of traffic from the townhome development will not result in a significant degradation to traffic operating conditions on either Park Lane North or the study intersections.
- Park Lane North is expected to remain below the comfortable volume threshold during special events such as weddings, concerts, and holidays.
- The parking supply is sufficient to meet the expected parking demand.
- The Nevis townhomes solve the existing pedestrian connectivity problems between the north office complex and the south retail area.
- Replacing Center Drive with a pedestrian plaza will not have an adverse effect on traffic operations in the Newport Development and will enhance the pedestrian feel of the development.

Sincerely,



Steven Lord, Project Manager



PO Box 522056 Salt Lake City, UT 84152-2056
(801) 201-7494 www.edmlc.net



SCALE: 1" = 20'



DEVELOPER:
Hamlet Development
308 East 4500 South, Suite 200
Murray, UT 84107
801-281-2223

NOTES:



C:\Users\NMM\Desktop\Projects\Nevis at Newport\Drawings\Site Plan.dwg

Nevis at Newport
1389 Center Drive
Site Plan

PROJECT STATISTICS		
TOTAL AREA:	51.90 SF	100% OF TOTAL
TOTAL LOT AREA:	23,836 SF	45% OF TOTAL
TOTAL COMMON AREA:	28,308 SF	55% OF TOTAL
TOTAL DWELLING UNITS:	22	100 UNITS PER ACRE
TOTAL HABITABLE SPACE:	23,866 SF	1,55 SF PER UNIT
PARKING SPACES:	22 COVERED	(ONE PER UNIT)
	02 UNCOVERED	(4 EXISTING STALLS)

PROJECT:	1401
DRAWN BY:	NMM
REVIEWED BY:	NMM
REVISIONS:	
No. DATE	REMARKS

DATE: May 19, 2014
SHEET NUMBER: **C-1**

County Engineer



Leslie Crawford, P.E.

MEMORANDUM**Date: June 18, 2014****To: Robert Jasper, County Manager****From: Leslie Crawford, County Engineer****Re: Newpark Specially Planned Area
Nevis Townhomes**

Summit County Engineering has reviewed the pedestrian plaza that is proposed for the Nevis Townhomes at the Newpark Specially Planned Area (SPA). The project has been discussed with the Hamlet Homes' Engineer of Record and the transportation engineer. Summit County has also met with Park City Transit to discuss any changes that will need to be made to existing transit patterns. Additionally, Summit County Engineering has reviewed the proposed changes to the Development Agreement for the Newpark SPA.

As a result of these discussions, Summit County Engineering has learned that a pedestrian plaza in this location will adversely impact transit patterns and will not adversely affect traffic patterns in this area. Therefore, this project and the pedestrian plaza is recommended for approval with the following conditions:

1. "No Parking" signs will be placed along Park Lane;
2. Sight distance will be reviewed at final site plan phase; and
3. The Park City Fire District provides approval of the new layout.

If you have any questions and/or concerns, please contact me.

cc: Derrick Radke, Public Works Administrator
file (S:\Projects\2014\cd14\Newpark Nevis townhomes\Memo - traffic 06-18-14.docx)

From: davidkrause@hotmail.com on behalf of [David Krause](#)
To: [Amir Caus](#)
Subject: May 27th Public Hearing on Townhouse Construction on Parcel P-2 Newpark
Date: Monday, May 19, 2014 11:22:01 AM

To whom it may concern:

The proposed approval of this amendment would eliminate construction of Center Drive, effectively putting all traffic on the north side of the Newpark development onto the very small Park Lane North. Without completion of Center Drive, all traffic from the south will be forced to use Park Lane North, a roadway that was not designed as nor ever intended to be a primary route for through traffic. In fact, Park Lane North is configured with sharp almost 90 degree turns on each end, consistent with its designed purpose as a residential street providing access to the Newpark Townhome Residences.

It is important to note that the originally approved Newpark Master Plan calls for a townhouse development on Parcel P-2 **that includes the completion of Center Drive**. There is no justification for now constructing townhomes on this parcel as originally envisioned and approved while allowing the developer to ignore a previously agreed to and important infrastructure requirement.

As a homeowner at the Newpark Townhomes, I urge you to not accept Michael Brodsky's amendment and follow through with the original Master Plan allowing for construction of Center Drive.

Dave Krause, CMT
Managing Member
Comtrade Commodities, LLC

6312 North Park Lane N, #12
Park City, UT 84098
816-729-5873

From: [Rick Hall](#)
To: [Amir Caus](#)
Subject: Newpark Master Plan
Date: Tuesday, May 20, 2014 3:31:23 PM

Summit County Planning Commission,

We are homeowners at 6296 Park Lane North, Unit 18, and would like to offer comments on the upcoming public hearing for the 23 townhomes on Parcel P-2 at Newpark.

We understand that the developer wants to eliminate the construction of Center Drive which was included in the approved Newpark Master Plan and this will be discussed at the public hearing on May 27th. We will not be in town on that date.

There was a reason that Center Drive was in the initial approved plan - SAFETY and proper traffic flow. There is a huge safety issue in eliminating Center Street. This area is growing by leaps and bounds and the last thing we need to do is cause more safety hazards than we already have caused. It makes no sense to funnel traffic onto our road, Park Lane North for several reasons. I have lived in Park City and Summit County over 25 years and have seen a lot of progress, construction and in hindsight, bad decisions. You don't want to wait until a stupid change is made and then realize it was a mistake. I think we're a lot smarter now in Park City and Summit County since the expansion has been so dramatic over the last few years. It may or may not cost a little more in the beginning but the bottom line should be safety. Park Lane North was never designed to be a thoroughfare. Center Drive makes perfect sense - it's in the right spot, a straight line and the closest point between 2 destinations. No brainer.

Thanks for your time,

Brenda and Rick Hall
609-709-9212

From: [Debra Cronshaw](#)
To: [Amir Caus](#)
Subject: Nevis at Newpark Proposed Change
Date: Thursday, May 22, 2014 9:25:06 AM

We are opposing the change being consider to the previously approved master plan to Parcel P-2 Newpark, Summit County, Utah.

Nevis at Newpark

Development Agreement Amendment, Plat and Final Site Plan

Parcel P-2 Newpark, Summit County, Utah

Parcel NPRK-P-2 1.20 acres

As a condition of approval, the applicant, Michael Brodsky, should be required to complete construction of the missing section of Center Drive. This street is included in the approved Newpark Master Plan and failure to require its completion at this time would obviate an important aspect of the planning commission's previous requirements for efficient traffic movement. Without completion of Center Drive, all traffic from the south will be forced to use Park Lane North, a roadway that was not designed as nor ever intended to be a primary route for through traffic. In fact, Park Lane North is configured with sharp almost 90 degree turns on each end, consistent with its designed purpose as a residential street providing access to the Newpark Townhome Residences. It is only approximately 20 feet wide, with no sidewalks. In addition, due to the configuration of the driveways serving Newpark Townhome units 1-24, which are smaller than those on the south side of the development, many residents find it necessary to back out of their garages onto Park Lane North. Routing virtually all development traffic north of the Newpark Hotel onto Park Lane North would result in unnecessary congestion and hazards, all of which are eliminated with the construction of the planned Center Drive. In contrast, Center Drive is designed to be a through street, with wider traffic lanes and sidewalks. With development of Lot P-2, the project will have reached full buildout on the north side with projected traffic densities which due to the previously allowed conversion of entitlements to increased commercial use are certain to exceed original, planned and approved volumes. As such, all roadways, including the currently missing section of Center Drive, need to be completed and open for the proper functioning of the Master Plan. It is important to note that the originally approved Newpark Master Plan calls for townhouse development on Parcel P-2 that includes the completion of Center Drive. There is simply no justification for now constructing townhomes on this parcel as originally envisioned and approved while allowing the developer to ignore a previously agreed to and important infrastructure requirement.

Thank you for your consideration of our position.

Debbie Cronshaw
6292 Park Lane North, #14
Park City, UT 84098
801-949-0925

Supplemental Letter of Opposition to Nevis at Newport Project:

Nevis at Newport
Development Agreement Amendment, Plat and Final Site Plan
Parcel P-2 Newport, Summit County, Utah
Parcel NPRK-P-2 1.20 acres

Snyderville Basin Planning Commission Hearing, May 27, 2014.

We are the owners of the Newport Townhome Residence located at 6496 Park Lane North #13 and as such we are directly and materially affected by the proposed Nevis at Newport development (hereinafter Nevis Project). We oppose Commission approval of the Nevis Project as submitted. ***We urge the Commission to require the applicant, Michael Brodsky, to complete construction of the missing section of Center Drive and to install a sidewalk on Park Lane North.***

Relative to the need to provide “walkability” to the existing residents of the 24 units of the Newport Townhome Residences, the staff report is deficient in completely excluding these residents from its “walkability” analysis. The staff report goes to great length to discuss the need to ensure walkability for the tenants of the Cottonwood III building and the residents of the proposed Nevis at Newport development while ignoring completely the larger number of residents already living in the north section of the Newport Townhome Residences. It is instructive to look at Figure 4 on Exhibit B.9. One cannot help but be struck by the depiction of existing “north/south pedestrian paths.” Note that no “paths” or sidewalks exist for pedestrians from the Newport Townhome Residences and the Exhibit does not even recognize that there is also pedestrian traffic generated from these units.....We do not get our own arrow showing the existing, inadequate pedestrian access along Park Lane North. The Commission should not make an already bad and UNSAFE situation much worse by increasing traffic on Park Lane North. The proposed Nevis at Newport project should not be approved unless and until SAFE pedestrian access for the current residents of the Newport Townhome Residences is appropriately addressed.

Respectfully submitted.

Janet and David Thomas
Newport Townhome Residences
6496 Park Lane North #13
Park City, Utah 84098

From: [Robert Behncke](#)
To: [Amir Caus](#)
Subject: Parcel P-2 Newport, Summit County , Utah
Date: Saturday, May 24, 2014 4:43:08 AM

The purpose of this email is to submit my input with regard to the development of the remaining parcel fronting Newport units 1-24 on Park Lane North.

I fully support the completion of construction of units facing my townhouse on the subject parcel, but not without the completion of construction of the missing section of Center Drive. Park Lane North is not configured to be a through street and is more like an alley which accesses to the townhouses garages. The garages are configured in such a way that there is no parking for the six vehicles in the driveway, and cars must back out onto a narrow street with no sidewalks and sharp turns. The Master Plan anticipated this with the construction of Center Drive, and without its completion, Park Lane North would become a through street for traffic which reaches the Recreation Center or the Hotel. I bought my townhouse in 2005 with the understanding that whatever was built across from my townhouse would include a major access road- Center Drive. It is a critical part of the Newport Master Plan and I strongly recommend that the Plan be implemented in the language and spirit it was intended.

Robert H. Behncke
Newport Owner Unit # 10

Statement of Opposition to the Approval of Nevis at Newport

Nevis at Newport
 Development Agreement Amendment, Plat and Final Site Plan
 Parcel P-2 Newport, Summit County, Utah
 Parcel NPRK-P-2 1.20 acres

Snyderville Basin Planning Commission Hearing, May 27, 2014.

Overview:

We are the owners of the Newport Townhome Residence located at 6496 Park Lane North #13 and as such we are directly and materially affected by the proposed Nevis at Newport development (hereinafter Nevis Project). We oppose Commission approval of the Nevis Project as submitted. ***We urge the Commission to require the applicant, Michael Brodsky, to complete construction of the missing section of Center Drive and to install a sidewalk on Park Lane North.*** Center Drive is included in the approved Newport Master Plan and failure to require its completion at this time would obviate an important aspect of the Commission's previous requirements for efficient traffic movement throughout the planned development. Without completion of Center Drive, all traffic from the south will be forced to use Park Lane North, a roadway that was not designed as nor ever intended to be a primary route for through traffic. In fact, Park Lane North is configured with sharp, almost 90 degree turns on each end, consistent with its designed purpose as a secondary, residential street providing limited access to local traffic only for the Newport Townhome Residences. It is only approximately 20 feet wide, with no sidewalks. In addition, due to the configuration of the driveways serving Newport Townhome units 1-24, many residents find it necessary to back out of their garages onto Park Lane North. Routing virtually all development traffic north of the Newport Hotel onto Park Lane North would result in unnecessary congestion and hazards, all of which are eliminated with the construction of the planned Center Drive. In contrast, Center Drive is designed to be a through street, with wider traffic lanes and sidewalks. With development of Lot P-2, the overall Newport project will have reached full buildout on the north side with projected traffic densities which due to the previously allowed conversion of entitlements to increased commercial and hotel use are certain to exceed original, planned and approved volumes. As such, all roadways, including the currently missing section of Center Drive, need to be completed and open for the proper functioning of the Master Plan and to provide safety for both pedestrians and vehicular traffic. It is important to note that the originally approved Newport Master Plan envisioned townhouse development on Parcel P-2 that includes the completion of Center Drive. There is simply no justification for now constructing townhomes on this parcel, a use that was originally envisioned and approved, while allowing the developer to ignore a previously agreed to and important infrastructure requirement.

Specific Comments:

1. **The public has not been provided adequate notice or time to review and respond to the proposed project.** The timing of this hearing is very inconvenient for anyone wishing to comment on the proposed Nevis Project. The staff report was only made available to the public on Friday afternoon of Memorial Day weekend. The staff report is long and involved, requiring time to review properly. Interested parties have only been provided the holiday weekend to review the report and to provide comments. The hearing is scheduled the day after the Memorial Day weekend, making it difficult to attend. This timing is unfair to the public. The developer had months to prepare its plans and to negotiate with the staff, and the public has only been given three days of a holiday weekend. The timing certainly discourages any real public participation in the process.

2. **While “walkability” is an important and appropriate aspect of the overall Newport development, the proposed Nevis Project will not meet the walkability goal efficiently and will significantly degrade existing pedestrian access on Park Lane North.** The proposed Nevis Project and staff report go to great lengths to highlight “walkability” within Newport and we agree that the ability to “live, work and play” within a planned mixed-use development is one of the great things about Newport. However, as proposed, the Nevis Project will negatively impact walkability from the existing Newport Townhome Residences. The staff study states that eliminating Center Drive and replacing it with a courtyard will improve walkability for Cottonwood III tenants and the proposed Nevis townhouses. We disagree. The proposed courtyard will primarily serve as a green space amenity for the Nevis townhouses. As planned, it will only tenuously connect with other parts of the overall development, including the Newport Hotel and Cottonwood offices. Completion of Center Drive, as planned with sidewalks will provide much more direct and efficient pedestrian access through the Nevis Project to other portions of the overall Newport development. We walk everywhere within Newport, Redstone, and the Smiths area. In the last couple of years walkability has improved a great deal. The one area where it has not improved is on Park Lane North. There are no sidewalks on Park Lane North and the Nevis Project will add dense development on the now open side of the street without adding any sidewalks or other pedestrian accommodation. In fact, the current poor walkability along Park Lane North and pedestrian access to and from the Newport Townhome Residences will be significantly degraded if Center Drive is eliminated with the attendant additional traffic on Park Lane North. Currently, we and other residents, guest and visitors to the Newport Townhome Residences are forced to walk on the street. The proposed Nevis Project will exacerbate an already unsafe condition for pedestrians by increasing vehicle traffic on Park Lane North and having residents of the Nevis townhouses back directly onto Park Lane North. As proposed, the Nevis Project will essentially cut off pedestrian access to the Newport Townhome Residences. Construction of a sidewalk along the full length of Park Lane North should be required by the Commission as part of any approval of the Nevis Project.

3. **The approved Newport Master Plan carefully balanced increased development densities with developer agreement to complete certain infrastructure improvements. Developers simply should not be allowed the benefits of increased density while eliminating previously agreed to infrastructure.** As originally approved in October 2001, the Newport Master Plan allowed for the development of 819,360 square feet on approximately 37 acres. To date, all but 76,360 square feet have been developed. The Nevis Project proposed to add over 29,000 square feet of development on the last remaining parcel on the north side of the project. The density of development approved as part of the Master Plan is predicated on the completion of all infrastructure elements. The Master Plan process is in essence a quid pro quo exercise wherein the developer agrees to provide certain infrastructure improvements in return for increased development rights. With the construction of the Nevis Project, all envisioned development will have been completed on the north portion of the project with only a south parcel remaining which can easily accommodate the remaining 47,000 square feet of development rights. When extra density is allowed in exchange for specific infrastructure improvements, the developer is simply not entitled to a “pass” – and not held to the infrastructure obligations that everyone understood would be required to be completed. In this case, the developer has even provided an alternative plan to retain completion of Center Drive, clearly demonstrating that the project can co-exist with the street.

4. **The traffic study used to justify the elimination of Center Drive is fatally flawed, based on inappropriate data and fails to consider the full impact of previously approved development in the overall Newport project and adjacent uses.** The proposed project attempts to justify the elimination of Center Drive primarily based on a flawed traffic study completed by Horrocks Engineers for a somewhat different project than the one currently proposed. The Horrocks Engineers’ report traffic numbers were based on April 2013 room occupancy levels at the existing Newport Hotel. The numbers were purportedly adjusted for presumed July hotel room occupancy rates. This methodology is fatally flawed in three respects: (1) anyone who knows anything about Park City occupancy rates knows that such rates peak in

late December, for the holidays, and in mid-January during the Sundance Film Festival. To use April numbers, after ski season, during the well known “mud season,” whether or not “adjusted” is erroneous and will clearly understate maximum traffic volumes; (2) furthermore, looking at Newport Hotel occupancy completely ignores the 24 units on the north side of the existing Newport Townhome Residences development. Whether owner occupied or on the rental market, the traffic generated by these units must also be considered; and (3) it is specious, at best, to suggest that traffic is “less than previously predicted” when the retail component of the Newport project, located on Center Drive in the south portion of the development, remains virtually empty. Any traffic estimates must take into account the traffic that will be generated on Center Drive heading north once those retail venues are filled AND the newly approved Metro townhouse project on the top of the existing south parking garage is completed. No credible traffic study or attempt to bootstrap an existing flawed traffic study prepared at a different time for a different project can support the elimination of Center Drive without full consideration of the traffic generated by all current development, including the Newport Hotel, the Newport Townhome Residences, the three office buildings, full occupancy of the retail area on Center Drive, the completion and occupancy of the Metro townhouse project, the new development in the Smiths area AND the newly expanded Basin Recreation Center. All of these developments do or will contribute traffic on the north side of the Newport project. Center Drive is needed to ensure smooth and SAFE traffic flow on the north.

5. **Previous Commission amendments to the Newport Master Plan, especially the lack of dedicated, off-street service areas at both the Newport Hotel and Cottonwood III building have resulted in a material reduction in the traffic capacity of Park Lane North which would be further exacerbated by adding any additional through traffic.** The service area for the Newport Hotel is on the north side of the hotel. Almost every day various hotel service vehicles (e.g., laundry trucks, FedEx delivery trucks, etc.) are parked in one of the traffic lanes of Park Lane North, effectively reducing this road to a single lane. The Newport Hotel was permitted as a change in the Master Plan without a loading area or service apron, leaving these service vehicles nowhere else to park while picking up or delivering items to the hotel. The street is not wide enough to allow parking on the street, but we have it. Driving around these vehicles is dangerous and walking around them is even more so. At the other end, frequently, vehicles that are servicing the new Cottonwood III building park on Park Lane North, usually at the right angle bend. Absent the construction of Center Drive, this loss of traffic capacity due to the presence of parked service vehicles, while currently a nuisance, will become an on-going safety hazard if Park Lane North becomes the sole through-traffic street on the north side of the Newport project.
6. **Both the existing Newport Townhome Residences and the proposed Nevis Project Townhouses require that residences back out of their garages onto Park Lane North. Having vehicles back out onto Park Lane North with obstructed views/limited visibility will create hazardous conditions for both vehicular traffic and pedestrians.** As noted previously, due to the construction of the common driveways serving the existing Newport Townhome Residences, many residents and guests must back out of their garages onto Park Lane North. Their vision is limited by the construction of the buildings. Increased traffic will only increase the hazard posed for existing residents. But, even more problematically, the design of the Nevis Project calls for single garages facing Park Lane North for the buildings on the south half of the parcel. Residents of these buildings will have no option but to back out of their garages onto Park Lane North. In fact, the Nevis Project calls for each unit to have an outdoor parking space directly behind each garage. These spaces, in turn, are flanked on each side by additional, common parking spaces. As a result, Nevis Project residents will have to back up with their vision obstructed by cars on both sides onto Park Lane North. Thus, cars will be backing onto Park Lane North from both directions. It is certain that this hazard will result in needless accidents that could largely be avoided with the construction of Center Drive.

For all of the above reasons, we request that the Commission require the completion of Center Drive and the installation of a sidewalk along the entire length of Park Lane North as a condition of approval of the Nevis Project.

Respectfully submitted.

Janet and David Thomas
Newpark Townhome Residences
6496 Park Lane North #13
Park City, Utah 84098

URGENT IMPORTANCE

May 25, 2014

Summit County Department of Community Development
c/o Amir Caus
60 North Main Street
P.O. Box 128
Coalville, Utah 84107

Re: Nevis at Newpark; Opposition and Objection to Proposed Development Agreement Amendment, Plat and Final Site Plan; Parcel P-2, Parcel : NPRK-P 2

Dear Mr. Caus,

I am president of the Newpark Resort Residences Owners Association, commonly referred to as the Newpark Townhomes. I recently received notice of the upcoming public hearing to be held on May 27, 2014 for the purpose of discussing a proposed Amendment to the Newpark Development plan and plat. On behalf of our Townhome Association and its 95 members, I am writing to urge the Planning Commission to reject the proposed amendment. If approved, our association and its members will suffer significant adverse consequences. It will likewise adversely impact visitors and patrons of the surrounding businesses.

As you are likely aware, the Newpark Townhomes are directly adjoined to the land to be developed by the applicant, Mr. Michael Brodsky. The proposed amendments are a substantial deviation from the previously approved Development plan. The Association urges the Commission to uphold its requirement to complete Center Drive.

Center Drive is a critical component to the infrastructure as contemplated in the previously approved Development plan and plat map. Left uncompleted, it is impossible to achieve the Commission's requirement and objective of efficient traffic movement. If eliminated from the plan, all traffic from the south will be forced to use Park Lane North. This roadway was not designed or intended to be a primary thoroughfare.

Its current approved design and configuration are suitable for simple ingress and egress of driveways of residential units, but cannot sustain the burden of any increased traffic. To approve the pending amendment would create a significant problem for the health, safety and general welfare of the residents and visitors of the Newpark Development.

Park Lane North is narrow, will contain blind spots, and there are no abutting sidewalks, thus increased traffic presents a health and safety concern for pedestrians in the community. There are multiple short driveways that run perpendicular to Park Lane North, which require the residents to back out of their garages directly onto the roadway. The inevitable resultant congestion is counterproductive to efficient traffic movement, and also would be hazardous to both vehicles and pedestrians if this change were to occur.

Our Association members purchased in Newpark relying on the implementation of the intended design and traffic plan set forth in the Development Agreement and Master Plan. The Development Agreement is replete with statements seeking to “ensure seamless access and continuity” and “seamless pedestrian and vehicular flows.” The Agreement, as previously approved, seeks to be “pedestrian friendly” and to be development “where pedestrians feel welcome and desire to explore all the Town Center has to offer.” The Development Agreement again boasts that the plan “promotes seamless pedestrian and vehicular flows by way of a common retail “Main Street.” This Main Street is depicted as what is now known as “Center Drive”.

The included Circulation Map proposes that Main Street (Center Drive) will be complete and at a safe distance from the Newpark Townhomes. Notably, the area where Park Lane North now sits is not even contemplated as a “Secondary Vehicular Circulation route.”

Notably, Summit County Ordinance 817 adopted the Snyderville Basin General Plan on February 27, 2014. It references the extensive workshops and open houses conducted by the Planning Commission to understand the community’s goals. Among the top priorities identified were “Walkability”, “Less Density” and “Traffic”. In reference to the “Mixed Use Centers” of the Basin, the General Plan sets forth that they “should benefit, not detract from, the general health, safety and welfare of the entire community. Increases in density for Town and Resort Centers should only occur in instances where such increases result in significant benefit to the community at large . . .”

The General Plan also emphasizes in “Objective A”, Policy 2.2 the “following sustainable pattern of development: [. . .] Commercial, residential, resort, and other mixed-use development that contains multi-modal streets *that are not exclusively oriented to the automobile use and that emphasize pedestrian accessibility.*”

Finally, the Association has sought legal counsel and is informed that developers have a fiduciary duty to the members of the communities that they develop. Our association wishes to emphasize that it is not in our best interest to adopt the amendments proposed by Mr. Brodsky, whom has not even reached out to our board.

For these reasons the Commission must uphold the previously approved Master Plan. As designed, Center Drive is large enough to accommodate traffic lanes, signage, and sidewalks. It is suited to handle the burden of the increased traffic that will necessarily result from the further development on Lot P-2.

To abandon completion of Main Street/Center Drive, would amount to a bait and switch for the residents who relied upon the well thought out Development Plan as previously adopted by the Planning Commission. Furthermore, it is counterproductive to the health, welfare and safety of the Newpark Townhome residents and visitors to the community.

We trust that the Planning Commission will uphold the Development Plan as originally conceived and require the completion of construction of Center Drive.

On behalf of our Board of Directors and members



Robert P. Franke
President
Newpark Resort Residences Owners Association
Park City, UT

rpfranke@gmail.com
630-205-6100