



All agenda items
in this packet are
preliminary, until
approved by the
Layton City
Council.

**REGULAR MEETING AGENDA OF THE
CITY COUNCIL OF LAYTON, UTAH**

PUBLIC NOTICE is hereby given that the City Council of Layton, Utah, will hold a public meeting in the Council Chambers of the City Center Building, 437 North Wasatch Drive, Layton, Utah, commencing at **7:00 PM on March 21, 2024.**

AGENDA ITEMS:

1. CALL TO ORDER, PLEDGE, OPENING CEREMONY, RECOGNITION, APPROVAL OF MINUTES:

A. Minutes of Layton City Council Meeting - February 1, 2024

2. MUNICIPAL EVENT ANNOUNCEMENTS:

3. VERBAL PETITIONS AND PRESENTATIONS:

4. CITIZEN COMMENTS:

5. CONSENT ITEMS: (These items are considered by the City Council to be routine and will be enacted by a single motion. If discussion is desired on any particular consent item, that item may be removed from the consent agenda and considered separately.)

- A. Federal Aid Supplement Agreement between Layton City and Utah Department of Transportation for the Participation in the Cost for a Pedestrian Safety Project – Resolution 24-11 – Along Gentile Street between Approximately 3475 West and 3200 West
- B. Accept a Proposal for an Agreement Between Layton City and Hogan & Associates Construction for Construction Management/General Contractor Services for a New 9-1-1 Emergency Communications Center and Emergency Operations Center – Resolution 24-10
- C. Annexation Request – Layton City Economic Development Project Plan Area (EDA) Properties Annexation – Acceptance and Certification of the Petition – Resolution 24-02 and Resolution 24-06 – Approximately 3925 North Fairfield Road and 3945 North Fairfield Road
- D. Final Plat – Adams Oak Hills Subdivision – 2201 East 350 North

6. PUBLIC HEARINGS:

A. Gordon Avenue and Highway 89 Town Center Master Plan, Amending and Updating the General Plan – Ordinance 24-06

7. UNFINISHED BUSINESS:

ADJOURN:

Notice is hereby given that:

- A Work Meeting will be held at 5:30 PM to discuss miscellaneous matters.
- This meeting will also be live streamed via laytoncitylive.com and facebook.com/Laytoncity
- In the event of an absence of a full quorum, agenda items will be continued to the next regularly scheduled meeting.
- This meeting may involve the use of electronic communications for some of the members of this public body. Elected Officials at remote locations may be connected to the meeting electronically.
- By motion of the Layton City Council, pursuant to Title 52, Chapter 4 of the Utah Code, the City Council may vote to hold a closed meeting for any of the purposes identified in that chapter.

Date: _____ By: _____
Kimberly S Read, City Recorder

This public notice is posted on the Utah Public Notice website www.utah.gov/pmn/, the Layton City website www.laytoncity.org, and at the Layton City Center.

In compliance with the Americans with Disabilities Act, persons in need of special accommodations or services to participate in this meeting shall notify the City at least 24 hours in advance at 801-336-3826 or 801-336-3820.

Citizen Comment Guidelines

For the benefit of all who participate in a PUBLIC HEARING or in giving PUBLIC COMMENT during a City Council meeting, we respectfully request that the following procedures be observed so that all concerned individuals may have an opportunity to speak.

Electronic Information: An electronic or hard copy of any electronic information presented to the City Council must be submitted to the City Recorder by the end of the meeting.

Time: If you are giving public input on any item on the agenda, please limit comments to three (3) minutes. If greater time is necessary to discuss the item, the matter may, upon request, be placed on a future City Council agenda for further discussion.

New Information: Please limit comments to new information only to avoid repeating the same information multiple times.

Spokesperson: Please, if you are part of a large group, select a spokesperson for the group.

Courtesy: Please be courteous to those making comments by avoiding applauding or verbal outbursts either in favor of or against what is being said.

Comments: Your comments are important. To give order to the meeting, please direct comments to and through the person conducting the meeting.

Thank you.

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MINUTES OF LAYTON CITY COUNCIL MEETING

FEBRUARY 1, 2024; 7:11 P.M.

MAYOR AND COUNCILMEMBERS PRESENT:

MAYOR JOY PETRO, ZACH BLOXHAM, CLINT MORRIS, TYSON ROBERTS, BETTINA SMITH EDMONDSON, AND DAVE THOMAS

STAFF PRESENT:

ALEX JENSEN, CLINT DRAKE, CHAD WILKINSON, MORGAN CLOWARD, AND KIM READ

The meeting was held in the Council Chambers of the Layton City Center.

Mayor Petro opened the meeting and welcomed the public. Councilmember Morris offered the invocation and led the Pledge of Allegiance.

Mayor Petro acknowledged Youth Councilmember Kaycen Ingebretsen sitting with her on the dais.

MINUTES:

MOTION: Councilmember Thomas moved and Councilmember Morris seconded to approve the minutes of:

**Layton City Council Work Meeting – December 21, 2023;
Layton City Council Meeting – January 4, 2024; and
Layton City Council Special Meeting – January 9, 2024.**

The vote was unanimous to approve the minutes as written.

MUNICIPAL EVENT ANNOUNCEMENTS:

Councilmember Roberts announced Parks and Recreation's Family Recreation Activity would be the Valentine's Dance scheduled for Friday, February 9, 2024, from 6:30-8:30 PM at Central Davis Jr. High school gymnasium. He mentioned there would be a live band and refreshments. He also mentioned the Recreation Activity planned for Friday, March 22, 2024 at the Davis County Library, Mermaid Scales and Pirate Tales.

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Councilmember Smith Edmondson announced the month of February was designated Black History Month and invited the public to learn more about the history of African Americans and their contributions to the United States of America.

Alex Jensen, City Manager, recognized Liz Mumford in the audience. He announced she currently served as the President of the Davis School Board and mentioned the City's positive long standing relationship with Davis School District and acknowledged her contributions with that partnership. He expressed appreciation for her support of Layton City in that capacity.

Mayor Petro also expressed her appreciation to Ms. Mumford and Davis School District for their support of Layton City.

PRESENTATIONS:

There were no presentations.

CITIZEN COMMENTS:

Laura Moore, 283 Glen Avenue, announced she had gathered signatures to petition the Council to improve the crosswalk located at Gordon Avenue and Hill Boulevard. She pointed out the crosswalk was oftentimes used by residents in adjacent neighborhoods going to and from the church outside times when the school crossing guard was present to stop vehicular traffic. She shared photos which she believed illustrated the poorly lit crosswalk when dark and shared her son's recent experience using the crosswalk after sundown and showed his broken skateboard which was hit by a car when he jumped off. She reported she had collected approximately 1,500 signatures and presented it to the City Recorder, Kimberly Read.

Mayor Petro expressed appreciation to Ms. Moore for her presentation and requested the City Manager provide an update regarding the crosswalk.

Alex Jensen, City Manager, informed Ms. Moore the City was aware of the issue and explained the challenge in identifying the right tool to address the situation and also explained the process which dictated the allowance and/or installation of crosswalks. He mentioned this particular crossing was a designated school crossing and identified the other options for a pedestrian crosswalk and the challenges associated with the implementation of a HAWK (High-Intensity Activated Crosswalk) crossing at that location. He continued to review the

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processes currently being considered by the City's Engineering Department to identify the best solution for the crosswalk at that location. Mr. Jensen mentioned the lighting at that location was a separate issue and would have Staff investigate that issue beginning tomorrow.

Liz Mumford, Bountiful resident, expressed appreciation for the warm introduction and announced she had declared candidacy as a republican candidate for the Davis County Commission. She mentioned she had previously worked with Mayor Petro and some of the Council on a number of different items. She expressed her desire for continued collaboration with Layton City and mentioned just a few: housing, public safety, and economic development. She pointed out during her time serving on the Board of Education she had been an advocate of tax increment financing and interlocal agreements which benefitted the entire County residents and expressed a desire for this to continue.

Claude Young, 1198 North 100 East, mentioned the recent news story which reported the City had ignored results from a \$500,000 study it had commissioned concerning noise associated with HAFB (Hill Air Force Base). He pointed out his concern wasn't with funding of the study; rather, that the conclusion and/or recommendations were being ignored. He questioned whether the study didn't provide the City with its desired results.

Mr. Jensen responded the representations made by the media were not entirely accurate and explained every city within Davis County, Davis County, and some cities in Weber County were involved in petitioning the Federal Government and HAFB to complete a Compatible Use Study. He added one of those entities had to be the 'host' entity, and due to the City's proximity to the Base, agreed to be the host city. He emphasized it wasn't Layton City's study. He reported a series of recommendations which came from a variety of sources and addressed ways to improve compatibility in a variety of areas and mentioned all were suggestions; nothing was mandatory. He indicated Layton City had continued to be an active participant and added a meeting had taken place earlier today regarding this issue. He reported the City had engaged in discussions with the group of Layton citizens with strong views regarding the jet noise associated with HAFB. He stated the narrative that the City had a study and was choosing to not follow the results because it disagreed with the findings was inaccurate. He indicated many of the issues the City had no control or influence over and added this was a working document. He clarified Layton City did not pay for the study, it was funded through the Federal Government applied for collectively by all participants.

Rachel Black, 1663 North 2800 East, stated she lived in the area near the proposed Gordon Avenue Town Center. She indicated two planning meetings had taken place since November and informed the Council a number of residents were concerned with the high density and traffic associated with the proposed plan. She

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understood UDOT (Utah Department of Transportation) owned the property and was hopeful the City would consider other options: keep it the way it currently is or possibly a park/greenspace. She didn't believe the Planning Commission had considered other options or what had been discussed during the meeting. She reported the residents would be having their own meeting on Saturday and would like to provide more information at that time.

Mr. Jensen responded the proposed town center had been discussed for a number of years and Staff had identified the need for certain services in the eastern portion of the City. He reported UDOT and the City had a good working relationship and it agreed to not sell the property for the highest dollar to a developer; rather, it was allowing development to take place with a methodical approach. He believed it was the City's responsibility to build a community and provide services to its residents.

Ms. Black disagreed with results from the previous study and believed the majority of the East Layton residents were opposed to the town center development and suggested a new study should be completed.

Chad Wilkinson, Community and Economic Development Department, stated he and/or his Staff would be happy to review the process in detail with Ms. Black. He reported the General Plan was adopted in 2019.

CONSENT AGENDA:

There were no consent agenda items.

PUBLIC HEARINGS:

COMMUNITY DEVELOPMENT BLOCK GRANT ANNUAL ACTION PLAN FOR FISCAL YEAR 2024-2025

Layton City had been designated as an entitlement City for Housing and Urban Development's (HUD) Community Development Block Grant (CDBG) and was required to develop an Annual Action Plan. The Plan identified needs within the City and proposed strategies to meet those needs using the allotment of CDBG funds during the upcoming Program Year, July 1, 2024 to June 30, 2025. HUD regulations required two public hearings during the preparation of the Plan. This first public hearing would gather information from the public concerning the needs within Layton City and community organizations were allowed an opportunity to present requests for assistance.

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Morgan Cloward, CDBG Coordinator, explained this was the first public hearing to accept public comment for the Program Year 2024-2025. He stated the Annual Action Plan identified how CDBG funds would be allotted and reviewed the current 2023-2024 CDBG Activities and Funding. He reported he recently concluded the annual monitoring visits with the public service partners: Safe Harbor, Open Doors, and Youth Court, and reported all were compliant and using funds appropriately.

He informed the Council of previous discussions with Davis School District for possibly acquiring approximately two acres of property, south of East Layton Elementary, to allow for the construction of homes in conjunction with Have A Heart, to benefit families of low to moderate income. He shared an illustration which identified the location of the parcels which could accommodate four homes. The District was now ready to proceed with the property acquisition and explained how the CDBG funding would be recognized for the property; \$410,000 consisting of previous years' program funds. He mentioned this was an exciting project and indicated HUD was extremely interested in providing housing assistance.

Mayor Petro's expressed her opinion this was a great program to partner with Have A Heart and other contributions from others within the construction and real estate community. She pointed out the difficulty in locating vacant parcels of property which could be used for this purpose and expressed appreciation to the School District for approaching the City. Mr. Cloward indicated the City had identified a decrease in Down Payment Assistant and Home Repair requests which allowed the re-programming of that funding to be used in conjunction with the new grant funding.

Councilmember Bloxham expressed appreciation to Mr. Cloward for his efforts and expressed concern that a significant portion of funds would be benefitting such a limited few. He requested Mr. Cloward speak to his concern of not spreading the breadth across the City.

Mr. Cloward mentioned the City had granted Down Payment Assistance funding in the past for approximately 12-15 applicants and reported only four or five grant requests had been received for this past year's programming. He continued to explain that due to the current market conditions: price of homes, interest rates, and the requirements for accessing the funding, it was just financially not feasible. He indicated the same scenario associated with the home repair/rehab grant funding with few submissions. He also mentioned the timeliness test associated with HUD grant funding and pointed out the timing was appropriate for purchasing the property from the School District. Councilmember Bloxham inquired whether the City had to market the program to inform the public of the available funding and Mr. Cloward identified the different avenues used to publicize the program and available grant funding.

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Councilmember Morris asked about other communities offering similar programs with Have A Heart and Mr. Cloward responded Clearfield City was the only other City he was aware of partnering with Have A Heart.

Councilmember Smith Edmondson expressed appreciation to Mr. Cloward administering the CDBG Program.

Mayor Petro opened the public hearing at 7:53 p.m.

Mayor Petro called for public comment.

Denene Adams, 1360 East 1450 South, Executive Director from Open Doors, informed the Council of the results from The Point In Time count last week and reported over 100 homeless individuals were identified. She also indicated 130 homeless people were on a waiting list for the rapid re-housing program. She stated demand for food from the Food Bank had increased by 68% at the same time it was experiencing a decrease in food donations. She announced the results from a recent inventory which identified approximately 127,000 pounds of available food and reported the food bank generally operated at 800,000 pounds of food. She expressed her opinion the focus should be on assisting people as opposed to ‘programs’ and informed the Council a local trailer park consisting of 47 trailers was currently on the market to be sold which could easily contribute to more homeless within the community. She mentioned the Circles program had been successful with participants. She concluded she had worked with Open Doors for 14 years and had not witnessed so much ‘need’ within the community.

Mayor Petro requested she speak to Code Blue. Ms. Adams responded this required shelters to increase capacity when temperatures decrease below 15 degrees. Since Davis County had no homeless shelter or warming center, Open Doors was responsible to provide shelter, usually through hotel vouchers, which was extremely expensive. She reported she only had nine available rooms and every Code Blue those were booked. She explained the challenges associated with the review process for hotel placement which was a significant drain on Staff resources. Mayor Petro mentioned Open Doors had depleted its hotel voucher funding by Christmas.

Councilmember Morris requested clarification regarding those individuals identified in the Point in Time count and Ms. Adams explained how that was conducted and reported all of these identified individuals were locals from Davis County and not from Ogden or Salt Lake City. She identified resources provided at the teen resource centers in the high schools, however, those weren’t geared to provide homeless services. Ms. Adams reported Layton City had been a great support for Open Doors and stated it also received CDBG funding from Davis County and Clearfield City, as well as funding from the State of Utah and private donors. She also

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informed the Council what could be used as a ‘warming center’.

Timothy Schafermeyer, Layton Youth Court, expressed appreciation for the CDBG funding received by the Youth Court. He informed the Council this was the 26th year for the Layton Youth Court program with 25 youth currently trained to be judges. He reported the Court was halfway through its operating year and had received 61 cases thus far; last year the Court received a total of 62 cases and suggested the number of cases could easily double. He announced 100% of offenders had completed their cases for the previous two years and indicated the majority of those were marijuana possession followed by retail theft. He also mentioned the ‘Learning to Breath’ class offered in conjunction with Davis Behavioral Health and mentioned those participants weren’t just from Layton City.

Councilmember Smith Edmondson expressed appreciation for the Youth Court representation and was thrilled to be able to offer these diversion programs. She mentioned the ‘Learning to Breath’ program was made available with Communities that Care partnered with the Davis Behavioral Health and Davis Schools and was offered in all sixth grade elementary schools. She added this was a tool to provide coping skills to youth and was also attended by adults.

ANNEXATION AND REZONE REQUEST – BARNEY ANNEXATION AND REZONE – A (AGRICULTURE) TO R-1-10 (SINGLE FAMILY RESIDENTIAL) – ORDINANCE 24-04 AND ORDINANCE 24-05 – APPROXIMATELY 3041 WEST GENTILE STREET

Chad Wilkinson, Community and Economic Development Director, shared a visual illustration and identified the location of the parcel. The applicant, John Barney, was requesting to annex and rezone approximately .59 acres of property located at approximately 3041 West Gentile Street from A to R-1-10, anytime a property was annexed it was zoned Agriculture. The rezone was consistent with the City’s General Plan. The purpose for the annexation and rezone was to accommodate a future single-family development which would need to front a future public street. It was anticipated that the property would be combined with property owned by the applicant which was currently within the City in order to develop a few single-family homes. The southern portion of the property could be developed in conjunction with the recently approved Jenkins PRUD (Planned Residential Unit Development). The property was adjacent to the Jenkins PRUD, which proposed a public street stub to the western boundary of the proposed annexation area. Property to the west and south was recently annexed into the City and zoned R-1-10 PRUD for future single-family development. The subject property might gain public street frontage with the PRUD development to the west. Development of the subject property cannot occur until the public street frontage exists, per City ordinance. Annexing the property met the City's Annexation Plan.

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The General Plan identified this property and surrounding properties as Low Density Residential with the Neighborhood Ag Heritage overlay, which permitted a maximum density of R-1-10. The proposed rezone was compatible with the existing uses in the vicinity and consistent with the General Plan. The Planning Commission reviewed the proposed rezone during its meeting on Tuesday, January 9, 2024, and unanimously recommended approval.

Mayor Petro called for public comment.

There was no public comment.

Councilmember Bloxham inquired whether Council approval would be required if there was a request for this parcel to be included with the Jenkins PRUD overlay and Mr. Wilkinson responded the request would be for an amendment only for this parcel and confirmed that could be an option.

MOTION: Councilmember Bloxham moved to approve the Annexation and Rezone Request located at approximately 3041 West Gentile Street, the Barney property, from A (Agriculture) to R-1-10 (Single Family Residential) - as presented, Ordinance 24-04 and Ordinance 24-05. Councilmember Thomas seconded the motion. The motion passed with the following vote: **Voting AYE – Councilmembers Roberts, Bloxham, Morris, Smith Edmondson, and Thomas. Voting NO – None.**

PROPOSED AMENDMENTS TO LAYTON CITY MUNICIPAL CODE, TITLE 19 “ZONING”, CHAPTER 19.02 “DEFINITIONS”, 19.05 “GENERAL AND SPECIFIC DEVELOPMENT REGULATIONS”, 19.06 “LAND USE REGULATIONS”, AND 19.12 “OFF STREET PARKING” TO AMEND REQUIREMENTS AND ADD CLARIFICATION FOR IMPERVIOUS SURFACE COVERAGE WITHIN RESIDENTIAL ZONES – ORDINANCE 24-02

Mr. Wilkinson explained the City had received a Zoning Ordinance text amendment petition from Jeremy Roberts of RPM Construction, on behalf of a Layton City resident. The applicant had submitted a proposal to modify and increase the impervious surface coverage percentage applied to the rear yards in single-family residential zones. The City had received an increasing number of requests for pool installations; however, many struggle and become frustrated with restrictions on the location of the pool because of Code constraints specifically related to maximum impervious area coverage requirements. Impervious surfaces include any surface which prevents, delays, hinders, or alters the natural absorption of water into the soil, or that causes water to run off the surface in greater quantities or faster than natural conditions. Examples of impervious

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surfaces include, but are not limited to structures, concrete or asphalt, walkways, decks, patios, driveways, parking lots, sports courts, pools, and/or other similar surfaces. Impervious surface coverage was an important consideration when developing lots within subdivisions as it determined sizing for stormwater systems and provided for open areas to allow for water to naturally drain into the ground.

He explained the proposal included two main purposes: one to standardize and clarify definitions for a rear yard and front yard; and to also clarify how much of yard area could be covered with these impervious surfaces.

Staff had reviewed the codes and determined the City's Code might be too restrictive related to the maximum amount of impervious surface allowed. He shared a visual illustration and mentioned the City Engineer determined the maximum impervious surface which could be permitted in a rear yard. The maximum lot coverage allowed and maximum coverage of structures in the rear yard setback in single-family zones would not change; however, what counted as a structure, would be clarified. Swimming pools, patios, sports courts, etc., would no longer count as a structure when calculating coverage and would amend Chapter 19.06, "Land Use Regulations." Pools, patios, sports courts, etc., would continue to be considered impervious surfaces and would count towards the overall maximum lot coverage. The Engineering Department had determined the proposed increase in impervious surface area in the rear yard setback would not negatively impact existing storm drain systems as the overall lot coverage would not change.

Additionally, there was potential ambiguity when determining in which yard area impervious surfaces were attributed, especially when structures overlap setback lines. To address the ambiguity the proposed ordinance provided clarification when determining yard areas. The purpose of the amendments was to clarify yard definitions found in Chapter 19.02 "Definitions," with a graphic in Diagram A-2 to illustrate these definitions. Clarifying language would also be added to Chapter 19.12, "Off Street Parking," in reference to impervious surface coverage in the front yard in single-family zones.

Impervious surface would be added as a definition, along with clarifications to the definition of hard surface, front yard area, corner side yard, and corner side yard area. Additionally, graphics would be added to help illustrate definitions.

He shared an illustration which proposed a new table reflecting the new percentages for lot coverages. The Planning Commission reviewed this item during its meeting on Tuesday, January 9, 2024 and unanimously recommended approval. Staff concurred with the recommendation.

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Councilmember Smith Edmondson expressed appreciation to Staff for working with a resident which had expressed a concern which prompted the change.

Mayor Petro called for public comment.

There were no comments.

MOTION: Councilmember Roberts moved to close the public hearings at 8:19 and approve the Proposed Amendments to Layton City Municipal Code, Title 19 “Zoning”, Chapter 19.02 “Definitions”, 19.05 “General and Specific Development Regulations”, 19.06 “Land Use Regulations”, and 19.12 “Off Street Parking” add clarification for Impervious Surface Coverage within a Residential Development - Ordinance 24-02. Councilmember Bloxham seconded the motion. The motion passed with the following vote: **Voting AYE – Councilmembers Thomas, Smith Edmondson, Morris, Bloxham, and Roberts. Voting NO – None.**

UNFINISHED BUSINESS:

There was no unfinished business.

The meeting adjourned at 8:20 p.m.

Kimberly S Read, City Recorder

**LAYTON CITY COUNCIL MEETING
AGENDA ITEM COVER SHEET**

Item Number: 5.A.

Subject:

Federal Aid Supplement Agreement between Layton City and Utah Department of Transportation for the Participation in the Cost for a Pedestrian Safety Project – Resolution 24-11 – Along Gentile Street between Approximately 3475 West and 3200 West

Background:

Resolution 24-11 authorizes the execution of an agreement between Layton City (City) and Utah Department of Transportation (UDOT) for a Federal Aid Supplement Agreement outlining the participation in the cost for a pedestrian safety project (Project) along Gentile Street as a project that proposes approximately 1,300 feet of sidewalk to improve the safety of students, well-being of the citizens of the City, and the general public.

The original agreement for this project was approved by Resolution 23-39 on August 17, 2023. Project costs have exceeded the original estimate. UDOT and the City have agreed to participate in the additional costs. The new estimated total cost of the improvements are \$450,800. UDOT has agreed to participate with an estimated amount of \$407,550 of the project and the City will participate with an estimated amount of \$43,250 minimum. The maximum UDOT participation will be \$407,550.

Alternatives:

Alternatives are to: 1) Adopt Resolution 24-11 authorizing the execution of the Cooperative Agreement between Layton City and Utah Department of Transportation for the participation in the cost for a pedestrian safety project; 2) Adopt Resolution 24-11 with any amendments the Council deems appropriate; or 3) Not Adopt Resolution 24-11 and remand to Staff with directions.

Recommendation:

Staff recommends the Council adopt Resolution 24-11, authorizing the Mayor or City Manager to execute the Cooperative Agreement between Layton City and Utah Department of Transportation for the participation in the cost for a pedestrian safety project.

RESOLUTION 24-11

A RESOLUTION ADOPTING AND APPROVING A FEDERAL AID SUPPLEMENT AGREEMENT BETWEEN LAYTON CITY AND UTAH DEPARTMENT OF TRANSPORTATION FOR THE PARTICIPATION IN THE COST FOR A PEDESTRIAN SAFETY PROJECT

WHEREAS, Layton City (City) and Utah Department of Transportation (UDOT) desire to cooperate in constructing a pedestrian safety project (Project) along Gentile Street described as a project that proposes sidewalk to improve the safety for students and wellbeing of the citizens of Layton City and the general public; and

WHEREAS, the City entered into an agreement with UDOT through Resolution 23-39 on August 17, 2023 for grant funding to install sidewalk along Gentile Street at approximately 3475 West and 3200 West; and

WHEREAS, project costs have exceeded the original estimates, UDOT and the City have agreed to provide additional funding for the project; and

WHEREAS, the parties have agreed to the terms and conditions contained in the Agreement; and

WHEREAS, it is deemed to be in the best interest of the citizens of Layton City to adopt and approve the Federal Aid Supplement Agreement with the State of Utah Department of Transportation for the participation in the cost of a pedestrian safety project.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF LAYTON, UTAH:

1. That the Federal Aid Supplement Agreement with the State of Utah Department of Transportation, which is attached hereto and incorporated herein by this reference, be adopted and approved.
2. That the Mayor, or City Manager, is authorized to execute the necessary documents.
3. This Resolution shall become effective immediately upon adoption by the City Council.

PASSED AND ADOPTED by the City Council of Layton, Utah, this **21st day of March, 2024**.

JOY PETRO, Mayor

ATTEST:

KIMBERLY S READ, City Recorder

APPROVED AS TO FORM:


CLINTON R. DRAKE, City Attorney


TERRY COBURN, Department Director

State of Utah
Department of Transportation

Federal Aid Agreement Supplement for Local Agency Project CFDA No. 20.205	Layton City - Mindy Hunsaker	Maximum Project Value Authorized \$450,800
PIN Number 19627 FINET Number 55870 FMIS Number F016630 DUNS Number 051613032	Project Number F-1458(1)0 PIN Description Gentile Street SRTS; 3475 W - 3200 W	Agreement Number (Original Agreement No.) 248184 Mod No. 1 Date Executed

The Local Agency(ies) desires to supplement the agreement entered into and executed on **8/21/2023**. All provisions in the basic agreement remain in effect except as expressly modified by this supplement. The changes to the agreement are described as follows:

Updated funding for this Federal Aid Agreement Modification #1:

State Wide Transportation Improvement Program STIP 2024 - 2027

Fund	Prior	2024	2025	2026	2027	Total	Fed Aid	State	Other	Pct
LOCAL GOVT	\$0	\$13,655	\$0	\$0	\$0	\$13,655	\$0	\$0	\$13,655	100.00%
STP_URB_O/L	\$10,000	\$427,145	\$0	\$0	\$0	\$437,145	\$407,550	\$0	\$29,595	6.77%
Total:	\$10,000	\$440,800	\$0	\$0	\$0	\$450,800	\$407,550	\$0	\$43,250	9.59%

Funding from Base Federal Aid Agreement:

State Wide Transportation Improvement Program STIP 2023 - 2026

Fund*	Prior	2023	2024	2025	2026	Total	Fed Aid	State	Other	Pct
LOCAL GOVT	\$0	\$0	\$0	\$0	\$0	\$500	\$0	\$0	\$500	100.00%
STP_URB_O/L	\$0	\$10,000	\$0	\$0	\$0	\$174,300	\$162,500	\$0	\$11,800	6.77%
Total:	\$0	\$10,000	\$0	\$0	\$0	\$174,800	\$162,500	\$0	\$12,300	7.04%

Upon signing this agreement, the Local Agency(ies) agrees to pay its estimated matching share in phases when requested by UDOT. Phases typically include environmental, design, right-of-way and construction. The local match for this project is represented by the percentages of the Total Project Value shown above. In addition, the Local Agency(ies) agrees to pay 100% of the overruns that exceed **\$450,800** and any ineligible costs when requested by UDOT.

UDOT will request payment of matching shares and overruns through an email that will be sent to **Mindy Hunsaker** at **MHUNSAKER@LAYTONCITY.ORG**, Local Agency(ies) Contact. The Local Agency(ies) shall pay within 30 days after each payment request. The Local Agency(ies) shall make the check payable to the Utah Department of Transportation referencing the project number above and mail to UDOT Comptroller's Office, Box 141510, 4501 South 2700 West, Salt Lake City, Utah 84119-5998.

As part of this Federal Aid Agreement modification, the following additional requirements apply to section VI. Federal Aid Project Compliance of the original Federal Aid Agreement:

VI. Federal Aid Project Compliance. Local Agency shall comply with Title 23, USC, 23 CFR, 2 CFR Part 200, UDOT Local Government and State Aid Project Guide, UDOT's Right of Way Operational Manual and the Federal Aid Project Agreement between UDOT and Federal Highway Administration concerning federal aid projects. They will also follow the Local Government Design and Process Manuals.

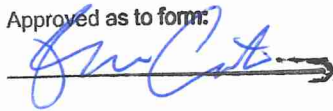
VII. **Compliance with the John S. McCain National Defense Authorization Act:** The Local Agency certifies conformance and continued conformance with Public Law 115-232, § 889 and 2 CFR § 200.216.

LOCAL AGENCY

By _____

Date _____
Layton City Official

Approved as to form:



Utah Department of Transportation

By _____
Region Director

Date _____

UDOT Comptroller

By _____
Comptroller's Office

Date _____



**Consultant Services
Federal Aid Agreement Review/Approval Routing Form**

**STATE OF UTAH
UTAH DEPARTMENT OF TRANSPORTATION
CONSULTANT SERVICES**

TODAY'S DATE 2/28/2024
PM REQUEST DATE 2/28/2024

FEDERAL AID 248184
AGREEMENT NO.
MOD NO. 1

Project No.: F-1458(1)0
PIN Description: Gentile Street SRTS; 3475 W - 3200 W

PIN No.: 19627
FINET Prog Code No.: 55870

UDOT Project Manager	UDOT Contract Administrator
David Adamson 166 West Southwell Street Ogden, UT 84404 (801)620-1684 deadamson@utah.gov	Michael R. Butler (Acting as UDOT) PO Box 148490 Salt Lake City Utah 84114-8490 (801)815-4367 michael.butler+udot@wcg.us

Local Government
Layton City 437 N WASATCH DR Layton, UT 84041-3196 Mindy Hunsaker, (801) 336-3700 MHUNSAKER@LAYTONCITY.ORG

Project Value	\$450,800
Federal Match	\$407,550
Local Government Match	\$43,250
State Match	\$0

This Federal Aid Agreement will follow the current Consultant Services electronic signature process. Please follow the email instructions for processing the Federal Aid Agreement. If legal reviews are required by your entity, the contract will still need to ultimately follow the electronic signature process.

**LAYTON CITY COUNCIL MEETING
AGENDA ITEM COVER SHEET**

Item Number: 5.B.

Subject:

Accept a Proposal for an Agreement Between Layton City and Hogan & Associates Construction for Construction Management/General Contractor Services for a New 9-1-1 Emergency Communications Center and Emergency Operations Center – Resolution 24-10

Background:

Layton City has had ongoing discussions with Davis County and other city administrators, police chiefs, and fire chiefs since early 2021 about the countywide provision of dispatch services. It was determined that the best system design was two dispatch centers, one in Bountiful, and one serving the north end of the County. Upon further discussion in the ensuing months, Layton was selected as the agency to provide dispatch services for the agencies located in the northern part of the County. To accommodate this need, plans were developed for a new building. The new building includes a 9-1-1 Emergency Communications Center (ECC) capable of serving multiple agencies, a larger, dual-purpose Emergency Operations Center (EOC)/training room, and evidence storage capable of meeting the City's needs for many years to come.

A Request for Proposal (RFP) for construction management / general contractor services for the new building was issued 6 February 2024 with responses required by 5 March 2024. Included in the RFP was a scope of work including pre-construction services, cost estimates, schedule, fees, and management approach. The RFP was advertised on the City's website, the Utah Public Notice website, and Utah's "U3P" public procurement website, and QuestCDN, in accordance with City practice. Six companies submitted proposals.

The proposals were evaluated by eight City employees using a predetermined, weighted scoring criteria. Galloway, our contracted architectural and engineering firm, assisted in the selection process through their experiences with the candidate firms. The technical proposal was evaluated in five categories with the following point values: Overall Qualifications (10), Project Team (15), Relevant Construction Experience (15), Management Plan (30), and Schedule and References (25). A separate, sealed fee proposal was evaluated at a 5 point value. Based on the predetermined criteria, three companies were selected to participate in a follow-up interview on 13 March 2024. Hogan & Associates Construction received the highest overall rating and was recommended by the selection team.

Alternatives:

Alternatives are to: 1) Adopt Resolution 24-10 accepting the proposal for an agreement between Layton City and Hogan & Associates Construction for construction management / general contractor services for a new 9-1-1 Emergency Communications Center and Emergency Operations Center conditionally approving the selection team's recommendation, subject to successful negotiation of an agreement acceptable to the City, authorizing the City Manager to conduct said negotiations, and execute an agreement between Layton City and Hogan & Associates Construction for construction management / general contractor services for the new building; 2) Adopt Resolution 24-10 with any amendments the Council deems appropriate; or 3) Not adopt Resolution 24-10 and remand to Staff with directions.

Recommendation:

Staff recommends the Council adopt Resolution 24-10 accepting the proposal for an agreement between Layton City and Hogan & Associates Construction for construction management / general contractor services for a new 9-1-1 Emergency Communications Center and Emergency Operations Center, authorizing the City Manager to conduct negotiations and execute an agreement between Layton City and Hogan & Associates for construction management / general contractor services for the new 9-1-1 Emergency Communications Center and Emergency Operations Center.

RESOLUTION 24-10

A RESOLUTION ACCEPTING THE PROPOSAL FOR AN AGREEMENT BETWEEN LAYTON CITY AND HOGAN & ASSOCIATES CONSTRUCTION FOR CONSTRUCTION MANAGEMENT / GENERAL CONTRACTOR SERVICES FOR A NEW 9-1-1 EMERGENCY COMMUNICATIONS CENTER AND EMERGENCY OPERATIONS CENTER

WHEREAS, Layton City has a desire to improve Public Safety services to Layton City residents and visitors; and

WHEREAS, Layton City is working with Davis County and cities within northern Davis County for the future provision of regional 9-1-1 dispatch services to improve service and efficiencies; and

WHEREAS, City Staff has reviewed and evaluated the response from Hogan & Associates Construction (Hogan), and has found it to be in the best interest of Layton City to conditionally select Hogan as the construction management / general contractor firm for the new 9-1-1 Emergency Communications Center and Emergency Operations Center.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF LAYTON, UTAH:

1. That Hogan is conditionally selected as the firm with whom the City Manager should conduct negotiations to provide construction management / general contractor services.
2. That the City Manager is directed to conduct negotiations for an Agreement with Hogan for construction management / general contractor services for a new 9-1-1 Emergency Communications Center and Emergency Operations Center. The Agreement shall include the scope of work, preconstruction services, cost estimate schedule, construction phase services, price, termination, liquidated damages, and other responses as contained in the proposal submitted by Hogan. The Agreement shall include such other provisions as are deemed necessary to accomplish the purposes of the City in entering into an Agreement to provide construction management / general contractor services to the City. This Resolution shall become effective immediately upon adoption by the City Council.
3. That at such time as the Agreement is in a form acceptable to the City Manager and City Attorney, and Hogan has properly executed said Agreement, the City Manager is authorized to execute the Agreement on behalf of the City. Execution of the Agreement by Hogan shall formalize Hogan's, offer for construction management / general contractor services pursuant to the terms and conditions of the Agreement. Execution of the Agreement by the City Manager shall constitute the City's acceptance of Hogan's offer and formal award of the contract to Hogan for construction management / general contractor services pursuant to the terms and conditions of the Agreement.

PASSED AND ADOPTED by the City Council of Layton, Utah, this **21st** day of **March, 2024**.

JOY PETRO, Mayor

ATTEST:

KIMBERLY S READ, City Recorder

APPROVED AS TO FORM:



CLINTON DRAKE, City Attorney

SUBMITTING DEPARTMENT:



ALLEN A. SWANSON, Chief of Police

**LAYTON CITY COUNCIL MEETING
AGENDA ITEM COVER SHEET**

Item Number: 5.C.

Subject:

Annexation Request – Layton City Economic Development Project Plan Area (EDA) Properties Annexation – Acceptance and Certification of the Petition – Resolution 24-02 and Resolution 24-06 – Approximately 3925 North Fairfield Road and 3945 North Fairfield Road

Background:

The proposal is to annex 24.3 acres located at approximately 3925 North Fairfield Road and 3945 North Fairfield Road. The petitioner is Layton City Corporation.

On January 16, 2024, Layton City purchased property within the EDA from Wasatch Waste Management. The property is currently located in Davis County adjacent to the Layton municipal boundary. The City was provided an option to purchase the property when Fairfield Road was constructed and in November 2023, the City initiated the process to execute that agreement. The purpose of the option agreement was to provide the City with control over the development of the property for the following reasons; 1) to secure the future alignment and construction of the extension of Fairfield Road and appurtenant utilities; 2) to preserve and guide development of the property for a manufacturing use that would support Hill Air Force Base and its operations or a similar Department of Defense mission; 3) to provide family sustaining career opportunities in Layton; and 4) to achieve the objectives of the Development Plan, generally, which has been determined is in the vital and best interests of the City, and in the best interest of the health, safety, morals and welfare of City residents. Should the Council adopt the attached Resolutions, Staff will initiate the necessary annexation procedures outlined by State Law.

Alternatives:

Alternatives are to: 1) Adopt Resolutions 24-02 and 24-06 accepting and certifying the petition for annexation and directing Staff to initiate the necessary annexation procedures; or 2) Not adopt Resolutions 24-02 and 24-06 denying the petition for annexation.

Recommendation:

Staff recommends the Council adopt Resolutions 24-02 and 24-06 accepting and certifying the petition for annexation and directing Staff to initiate the necessary annexation procedures.

RESOLUTION 24-02
(Layton City Corporation Annexation Petition)

**A RESOLUTION ACCEPTING A PETITION FOR ANNEXATION OF
PROPERTIES LOCATED AT APPROXIMATELY 3925 NORTH FAIRFIELD
ROAD AND 3945 NORTH FAIRFIELD ROAD**

WHEREAS, a petition has been received by Layton City, pursuant to Utah Code Annotated section 10-2-403, petitioning for annexation of certain properties located at approximately 3925 North Fairfield Road and 3945 North Fairfield Road; and

WHEREAS, these properties are located within an area where Layton City can provide urban services; and

WHEREAS, these properties are identified in the Layton City Annexation Policy Plan, Expansion Area Two, adopted by the City Council on December 5, 2002; and

WHEREAS, the City Council determines it to be in the best interest of the citizens of Layton City to accept the petition for annexation and direct staff to certify the annexation petition and provide notice thereof relative to the proposed annexation; and

WHEREAS, the property owners wish to proceed with the petition for annexation to the City.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF LAYTON, UTAH:

1. The City has received a petition for annexation of 24.3 acres (1,057,305.8 s.f.) located at approximately 3925 North Fairfield Road and 3945 North Fairfield Road and otherwise referred to as Parcel 090120031 and Parcel 090120032; and

2. The above properties, more particularly described in 'Exhibit A' which is attached hereto and made a part hereof by reference, represents one hundred percent (100%) of the public land and constitutes one hundred percent (100%) of the value of all private property within the area proposed for annexation.

3. The petition is hereby accepted by the City, and staff is hereby directed to certify the petition for the proposed annexation pursuant to Utah Code Annotated and initiate the necessary annexation procedures.

PASSED AND ADOPTED by the City Council of Layton, Utah, this 21st day of March 2024.

JOY PETRO, Mayor

ATTEST:

KIMBERLY S READ, City Recorder

APPROVED AS TO FORM:


CLINTON R. DRAKE, City Attorney

SUBMITTING DEPARTMENT:


CHAD WILKINSON, Director
Community & Economic Development

EXHIBIT "A" LEGAL DESCRIPTION

PROPERTY BOUNDARY DESCRIPTION

BEGINNING AT THE NORTH EAST CORNER OF SECTION 4 T4NR1W, SAID CORNER LIES
N00°29'22"E 2626.25' ALONG THE SECTION LINE FROM THE EAST 1/4 CORNER OF SECTION 4.

THENCE AROUND THE BOUNDARY OF THE WASATCH INTEGRATED ANNEXATION THE FOLLOWING 4
COURSES.

1.) N 89°36'23" W 1976.45' ALONG THE NORTH LINE OF SECTION 4 TO THE BOUNDARY OF HILL AIR
FORCE BASE.

2.) THENCE S00°35'17"W 533.20' ALONG THE BOUNDARY OF HILL AIR FORCE BASE TO THE LAYTON CITY
CORPORATION BOUNDARY DESCRIBED BY ENTRY NO. 817331, BOOK 1221, PAGE 260.

3.) THENCE S89°29'51"E 1977.37' ALONG THE LAYTON CITY CORPORATION BOUNDARY TO THE EAST
SECTION LINE OF SECTION 4 T4NR1W.

4.) THENCE N00°29'21" E 536.95' ALONG THE SECTION LINE TO THE POINT OF BEGINNING.

CONTAINS: 1,057,305.8 SF
24.3 ACRES MORE OR LESS

RESOLUTION 24-06
(Layton City Corporation Annexation Petition Certification)

**A RESOLUTION ACKNOWLEDGING THE RECEIPT OF CERTIFICATION OF
THE PETITION FOR ANNEXATION OF PROPERTIES LOCATED AT
APPROXIMATELY 3925 NORTH FAIRFIELD ROAD AND 3945 NORTH
FAIRFIELD ROAD**

WHEREAS, a petition for annexation was received and accepted by the Layton City Council pursuant to Utah Code Annotated section 10-2-403, for properties located at approximately 3925 North Fairfield Road and 3945 North Fairfield Road on March 21, 2024 (Resolution 24-6); and

WHEREAS, the City Recorder has certified that the petition for annexation complies with the statutory requirements of Utah State Code section 10-2-403 and said certification was received by the City Council on March 21, 2024; and

WHEREAS, notices of the proposed annexation must be advertised in the newspaper and sent to entities prescribed in Utah State Code section 10-2-406.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF LAYTON, UTAH:

1. Staff is hereby directed to publish the prescribed notice of the proposed annexation at least once a week for three (3) successive weeks with the first notice being published within ten (10) days of the Council's receipt of the notice of certification.

2. Staff is hereby directed to mail written notice of the proposed annexation to the Davis County Commission, to the board of any special or independent service district that services any of the properties within the subject annexation area, the city council of any city whose boundaries are within one-half mile of the subject area, and to the Davis County School District.

3. The published and mailed notice shall contain the prescribed information, as outlined in Utah Code Annotated. Said notice shall provide the necessary information relative to the filing of protests and deadlines for filing such protests.

PASSED AND ADOPTED by the City Council of Layton, Utah, this 21st day of March 2024.

JOY PETRO, Mayor

ATTEST:

KIMBERLY S READ, City Recorder

APPROVED AS TO FORM:



CLINTON R. DRAKE, City Attorney

SUBMITTING DEPARTMENT:



CHAD WILKINSON, Director
Community & Economic Development



Community • Prosperity • Choice

Mayor • Joy Petro
City Manager • Alex R. Jensen
Asst. City Manager • Steven L. Garside

• Council Members •
Zach Bloxham
Clint Morris
Tyson Roberts
Bettina Smith Edmondson
Dave Thomas

March 21, 2024

To the Layton City Council

I hereby certify the petition for annexation commonly known as the Layton City East Gate Annexation located at approximately 3925 and 3945 North Fairfield Road. The proposed annexation includes approximately 24.3 acres as drawn and described in the survey filed with the petition.

In my opinion, the annexation meets the statutory requirements of Utah State Code sections 10-2-403(3), (4) and (5).

Respectfully,

Kimberly S. Read, MMC
City Recorder



**LAYTON CITY COUNCIL MEETING
AGENDA ITEM COVER SHEET**

Item Number: 5.D.

Subject:

Final Plat – Adams Oak Hills Subdivision – 2201 East 350 North

Background:

The applicant, Luke Martineau, representing J. Fisher Companies, is requesting final plat approval for the Adams Oak Hills Subdivision. The proposed subdivision is adjacent to similar single-family uses to the east and north. Vacant agricultural zoned property is to the west, which slopes down into the hollow. To the south is Oak Hills Drive, which is owned by the Utah Department of Transportation (UDOT).

The proposed subdivision is within a Sensitive Lands area of the City, which requires geotechnical and geological hazard studies to be done on the property. The applicant has submitted the required reports which include mitigation measures for construction on the property. These reports have been reviewed and proposed mitigations accepted by Staff and approved by the Commission and Council as part of the preliminary plat approval for the subdivision. The plat consists of two single-family lots that will back or side onto Oak Hills Drive. Both lots meet the frontage and area requirements of the A (Agriculture) zone.

Alternatives:

Alternatives are to: 1) Grant final plat approval to Adams Oak Hills Subdivision subject to meeting all Staff requirements as outlined in Staff memorandums; or 2) Deny the proposed final plat to Adams Oak Hills Subdivision.

Recommendation:

On January 4, 2024, the Planning Commission unanimously recommended the Council grant final plat approval to Adams Oak Hills Subdivision subject to meeting all City requirements as outlined in Staff memorandums.

Staff supports the recommendation of the Planning Commission.



**COMMUNITY AND ECONOMIC
DEVELOPMENT DEPARTMENT
PLANNING DIVISION**

STAFF REPORT

To: City Council

From: Kem Weaver, Planner II

A handwritten signature in blue ink, appearing to read "Kem Weaver", is written over a horizontal line.

Date: March 21, 2024

Re: Adams Oak Hills Subdivision – Final Plat

Location: 2201 East 350 North

Zoning: A (Agriculture)

Description:

The applicant, Luke Martineau, representing J Fisher Companies, is requesting final plat approval for the Adams Oak Hills Subdivision. The proposed subdivision is adjacent to similar single-family uses to the east and north. Vacant agricultural zoned property is to the west, which slopes down into the adjacent hollow. To the south is Oak Hills Drive, which is owned by the Utah Department of Transportation (UDOT).

Background:

Ordinance 18.01.070 designates the Council as the land use authority for a final plat within Sensitive Land areas. The proposed two-lot subdivision does not meet the requirements of the Small Subdivision ordinance due to the proposed cul-de-sac in the subdivision being dedicated to the City, which does require a final plat review. The Sensitive Lands ordinance requires an approval from the Council for a final plat.

The proposed two-lot subdivision is within a Sensitive Lands area of the City, which requires geotechnical and geological hazard studies to be done on the property. The submitted reports require a mitigation measure for a home to be built on each lot. The mitigations are for slope stability and are necessary to achieve a factor of safety so that the homes will not slide during a seismic event.

The mitigation is to either create a 12-inch thick concrete mat slab as the basement slab or apply a 12-inch cement treatment base underneath the concrete basement slab, these areas

are shown as hatched areas on the preliminary plat. Depending on the size of the future home for each lot, there is a possibility that the home could be placed outside these hatched areas and not have to construct a mat slab or apply a cement treatment base but still be located in the buildable area.

The final plat consists of two single-family lots within the A (Agriculture) zone, which requires each lot to be a minimum one acre. Due to the geological restraints of the property, proposed Lot 2 is 4.5 acres and Lot 1 is smaller with 1.34 acres. The applicant was proposing a much denser subdivision under the R-1-10 (Single Family Residential) zone, but the geotechnical and geological constraints and mitigations caused the property to be reduced to two lots. The two lots will be accessed by extending 350 North to the west and terminating in a cul-de-sac to meet the Fire Department turnaround requirements.

The preliminary plat contained a landscape buffer easement and landscape plan, the final plat does not include this same easement or a corresponding landscaping plan because it is not required by ordinance. A landscape buffer is only required along arterial and collector streets in single-family residential zones. This proposed subdivision is in an A zone, which does not require landscaping along Oak Hills Drive.

Recommendation:

On February 27, 2024, the Planning Commission voted unanimously to recommend the Council approve the final plat for Adams Oak Hills Subdivision subject to meeting all City requirements.

Staff supports the recommendation of the Planning Commission.



Attention Engineers & Developers: Please do not resubmit plans until you have received comments from Layton City Fire Department, Parks Department, Engineering Division and Planning Division. You may expect to receive comments within 15 business days of a preliminary submittal and within 20 business days of a final submittal. Thank you.

MEMORANDUM

TO: Brock Loomis; brock@jfisherco.com
Luke Martineau; luke@jfisherco.com
Cam Preston; spreston@ensignutah.com

CC: CED/Fire Marshal/Legal

FROM: Shannon Hansen, Assistant City Engineer - Development

DATE: February 15, 2024

SUBJECT: Adams Oak Hills Subdivision
Final Plans – 2nd Submittal 2nd Cycle
2201 West 350 North

I have reviewed the dedication plat, title report, and construction plans submitted on February 7, 2024 for the Adams Oak Hills subdivision located at approximately 2201 West 350 North. The plan has been stamped "Approved as Corrected." The following comments and corrections will need to be addressed prior to scheduling a preconstruction meeting. Municipal Code (MC) and Development Guideline and Design Standard (DG) references provided in parenthesis. Items that have been addressed have strikethrough and new comments based on changes to the drawings are in red.

General Notes –

1. Water Exaction – The water exaction amount for a 1" meter is 2-acre foot for a total of 4-acre feet for the two meters. Water shares from Kays Creek Irrigation, Holmes Creek Irrigation, or DWCCC will need to be submitted **prior to scheduling a preconstruction meeting.** (MC 19.23.010).
2. Street Lights – Street light location is anticipated to be located as indicated on the drawing. The developer will be required to pay for the lights and installation. The lights will be purchased by the City and the installation will be done by the City's contractor. The cost estimate for the light is \$7,253.00. The developer will be responsible to install any transformers that may be needed for the lights. (MC 18.50.075 and DG 10.02)
3. ~~The development will need to comply with Low Impact Development requirements in Section 6 of the City Guidelines and Design Standards including the submittal of a Storm Water Quality Report. **Future submittals will not be reviewed without this document.**~~
4. Bonding – A cost estimate from a contractor will need to be submitted for review. The cost estimate will be used to determine the bonding amount. Bonding or a letter in lieu will need to be signed for all on-site infrastructure. **Bonding will need to be in place for the work within Oak Hills.** Dena Hyatt in the Engineering office (801-336-3700) will need to be contacted for bonding requirements.

5. ~~A permit from Phillips 66 for the laterals to be installed through the gas line easement will need to be submitted for final approval. Any requirements of the gas companies will need to be addressed on the final plans.~~
6. An encroachment permit from UDOT for all work within the Oak Hills right of way will need to be submitted for final approval.
7. ~~Legal descriptions for the Public Storm Drain Easement and the Private Sewer Easement will need to be submitted for review. Once approved,~~ **The legal descriptions for the Public Storm Drain easement and Private Sewer Easement have been approved and** the private easement can be recorded. The storm drain easement legal description will need to be attached as Exhibit A to the City's easement form (previously provided). The original, signed and notarized document will need to be submitted to the City for City signatures for recording at the County **prior to scheduling a preconstruction meeting.**
8. A Notice of Intent (NOI) from the State of Utah, Department of Environmental Quality, Division of Water Quality will need to be submitted before scheduling a preconstruction meeting.
9. A SWPPP permit that meets State requirements will need to be submitted for review by Engineering. The permit will need to be submitted in PDF format. The erosion control plan review cannot be completed until the permit has been submitted.
10. Upon approval from all City Departments, the stamped and signed complete sets of drawings in PDF form will need to be submitted for a preconstruction meeting. The plans will need to address the comments in the most recent Engineering memo as well as all comments from other City departments and utility companies.
11. **An approval letter from Weber Basin for the secondary water design will need to be submitted before scheduling a preconstruction meeting.**

Holly Energy Permit

1. ~~The legal description in the permit is incorrect. The description should be from the Center Quarter Corner rather than from the West Quarter Corner of Section 23.~~

Dedication Plat

1. ~~Sheet 1 – The 4th call after POB is included twice in the Boundary Description.~~
2. ~~Sheet 1 – General Note 3 – The rear PU&DE can be reduced to 5 feet. (DG 9.02.A.12.a)~~
3. Sheet 2 – The lengths and bearings provided to establish the area of slope failure on Lot 2 are to the buildable area rather than the slope failure area. **This has not been addressed.** The length along the east boundary of this area is incorrect at 93.45 feet. This line measures approximately 177.63 feet. The length along the northwestern boundary of this area is incorrect at 350.51 feet. This line measures approximately 132.92 feet.
4. ~~The private sanitary sewer easement will need to be expanded to cover the existing sewer lateral from the Deniece Adams parcel 11-021-0062.~~
5. ~~Sheet 3 – Sanitary Sewer Easement – The curve between L7 and L8 along 2250 North will need to be labeled.~~
6. The Holly Energy Gas Easement is dimensioned at 105 feet in the drawings and 100 feet in the title of the detail on sheet 4. The legal description in the permit is for a width of 100 feet. The lengths in the detail have not been changed to reflect a 105 foot wide easement. The easement has a failure to close of 1.10 feet.
7. ~~The lengths and bearings for the eastern and western line of the Phillips 66 Gas Easement will need to be added to the detail on Sheet 4.~~
8. ~~The dedication plat will need to include the following note and developer shall prepare a notice to be recorded against every lot.~~

~~“Notice to all Lot Owners—The developer has received a waiver on this subdivision to eliminate the land drain system and footing/foundation drain requirements of Section 18.40.020 of the Layton City Municipal Code. A copy of the geotechnical report is available for review with the Layton City Engineering or Community Economic Development Departments.~~

9. The PC elevations added to the lot corners will be too small to read when printed to scale. The text size will need to be increased.
10. The curve information for C3 and C4 has changed from the previous submittal. With this change, the lots have a failure to close of 0.89 feet. This will need to be reduced to be 0.015 feet. (DG 9.02.B.1.c)
11. With the removal of the landscape easement, Note 14 can be removed from sheet 1.
12. The hatching for the Holly Energy Gas Easement has been removed from within the boundary in the easement legend on all sheets.
13. The hatching for the Phillips 66 Gas Easement in the easement legend is different from the hatching seen on the plat on sheet 2.



Construction Plans

C-100 – Site Plan

- ~~1. The boundary is located from the Center Quarter Corner rather than the West Quarter Corner as indicated in the Property Description.~~
- ~~2. General Note 3—The rear PU&DE can be reduced to 5 feet. (DG 9.02.A.12.a)~~
- ~~3. Typically, the Section information and bearings are capitalized in a property description. That is not the case for the qualifying information for the basis of bearing.~~
- ~~4. Key Note 3 is conflicting with instructions to install the street light as well as for the street light to be installed by the City's contractor.~~
1. The leader notes for the area of slope failure on both lots refer to Note 11. The General Notes have changed and Note 11 now references the landscaping requirements. The leader notes should refer to General Note 12.

C-200 – Grading and Drainage Plan

- ~~1. The storm drain line shall be installed within steel casing from ROW to Box. (DG 6.06.E.4) Storm drain pipe within casing shall be manufactured locking joint, internal or external restraint, or manufactured bell restraint pipe. (DG 6.10.I.5) General Note 14 indicates the casing to be locking rather than the pipe.~~

2. ~~It is recommended that the pipe be design to avoid a box on the lot which requires a drivable surface. Storm drain lines that are approved for side lot or rear lot installation shall provide for vehicular access to all cleanouts or manholes. Vehicular access shall have a maximum slope of 10% and a minimum 10-foot-wide drivable surface capable of handling 65,000 lbs. (DG 6.06.E.3)~~
3. ~~"See Note 14" will need to be added to the leader note for the storm drain pipe between SDCO #320 and SDCO on Exist LD Line #102.~~
4. ~~Notes to plug and block the existing land drain line into SDCO #4 will need to be added.~~
5. ~~The storm drain line will not be able to connect to the existing box as shown. The flow line of the 15-inch matches the top of the inside of the existing 24-inch pipe into Exist. SDCO #4. Also, the box will be destroyed with a connection to the corner of the box. It is recommended that a new box be added to the 24-inch storm drain between SDCB #3 and SDCO #4.~~
6. ~~There is a note of "Area not to be disturbed" on Lot 2. Will this be limited to the construction of the improvements or is this a long term constraint on the lot which will require that notes be added to the dedication plat?~~

C-300 – Utility Plan

1. ~~The standard drawing for the 1" meter is ST-WL 01 rather than ST-WL 10 as indicated in the leader notes for the meters.~~
2. ~~The minimum slope of a 4-inch sewer lateral can be reduced to 1.0%. (DG 5.08.B.4)~~

C-400 – Details

1. ~~The Standard Trail Section should have 2 feet of road base on both sides of the trail.~~

Storm Water Quality Report

1. The watershed information will need to be updated to list the receiving water as Holmes Creek, which is impaired with no approved TDML for pH, Copper, E. Coli.
2. This report states that retention is prohibited on-site due to the drinking water protection zone. This site is within a Zone 4 water source protection area. Infiltration BMPs are allowed in Zones 3 and 4 water source protection zone areas. (DG 6.14.D.3)
3. Bioretention and infiltration will not be feasible on-site for soils that are part of the Hydrologic Soil Group C or D.
4. The final SWQR will need to be signed and stamped by a Professional Engineer.



Community • Prosperity • Choice

Mayor • Joy Petro
City Manager • Alex R. Jensen
Asst. City Manager • Steve Garside

• Fire Department •
Kevin Ward • Fire Chief
Telephone: (801) 336-3940
Fax: (801) 546-0901

Attention Engineers & Developers: Please do not resubmit plans until you have received comments from Layton City Fire Department, Parks Department, Engineering Division and Planning Division. You may expect to receive comments within 7-10 business days of a submittal and within 7 business days of a resubmittal. Thank you.

MEMORANDUM

TO: Community Development

FROM: Gavin Moffat, Deputy Fire Marshal 

RE: Adams Oak Hills Subdivision

CC: 1) Engineering
2) Cam Preston, cpreston@ensignutah.com
3) Brock Loomis, brock@jfisherco.com
4) Luke Martineau, luke@jfisherco.com

DATE: January 30, 2024

I have reviewed the plat submitted on January 18, 2024 for the above referenced project. The Fire Prevention Division of this department has the following comments/concerns.

1. The minimum fire flow requirement is 1,000 gallons per minute for 60 consecutive minutes for residential one and two family dwellings. Fire flow requirements may be increased to a maximum of 2,000 gallons per minute for residential one and two family dwellings with a building footprint equal to or greater than 3,600 square feet, or for buildings other than one and two family dwellings. Per the Layton City Engineering Water Model, the anticipated fire flow is 2,400 gallons per minute at 55 PSI.



Adam Oak Hills Subdivision S24-004
January 30, 2024
Page 2

These plans have been reviewed for Fire Department requirements only. Other departments must review these plans and will have their requirements. This review by the Fire Department must not be construed as final approval from Layton City.

CF#1 subdivision site plan:sh
Plan #S24-004 District #33
Project Tracker #LAY2308293293
ERS #12766



Memorandum

To: Brock Loomis, Luke Martineau, Cam Preston
CC: Community Development, Fire, & Engineering
From: JoEllen Grandy, City Landscape Architect – Parks & Recreation
Date: February 9, 2024
Re: Adams Oak Hills Subdivision, Final Approval – 2215 E. 350 N.
Review: 2nd Cycle, 2nd Review

Adams Oak Hills Subdivision, located at 2215 East 350 North, lies within the future neighborhood park service area of Boynton Park.

The Parks & Recreation Department has reviewed the plans transmitted on February 7th and has no other comments or concerns regarding Adams Oak Hills Subdivision.

Attention Engineers & Developers: Please do not resubmit plans until you have received comments from Layton City Fire Department, Parks Department, Engineering Division and Planning Division. You may expect to receive comments within 7-10 business days of a submittal and within 7 business days of a resubmittal. Thank you.



ADAMS OAK HILLS
SUBDIVISION

2201 EAST
350 NORTH

FINAL PLAT

Legend



City Boundary



Rail Lines



APZ



Interstate 15



Lakes



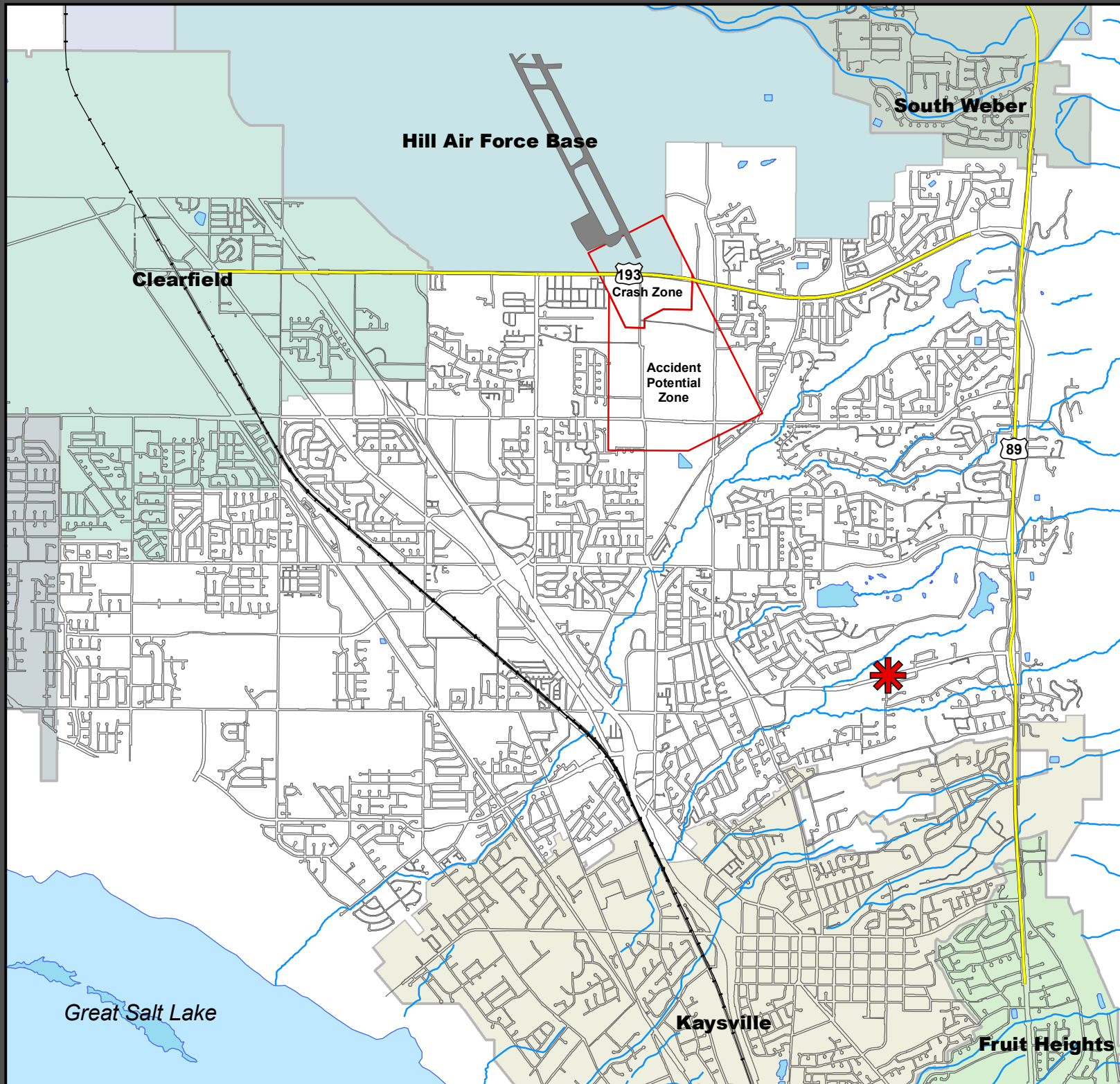
Streams



- Project Site



Map 1





ADAMS OAK HILLS SUBDIVISION

2201 EAST
350 NORTH

FINAL PLAT

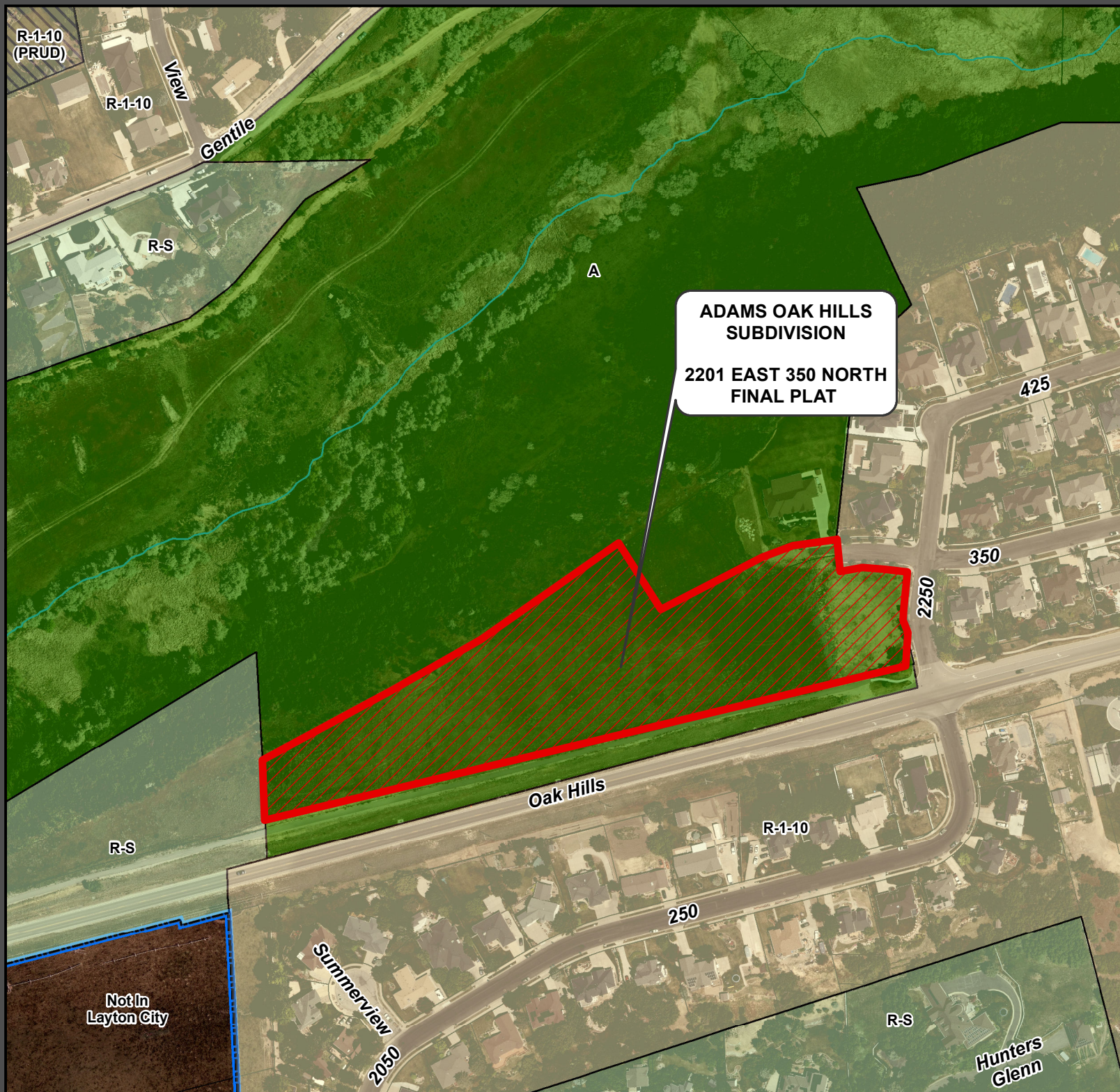
Legend

- Interstate Highways
- City Boundary
- Highways
- Lakes
- Streams

- Project Area



Map 2










ADAMS OAK HILLS SUBDIVISION

2201 EAST
350 NORTH

FINAL PLAT

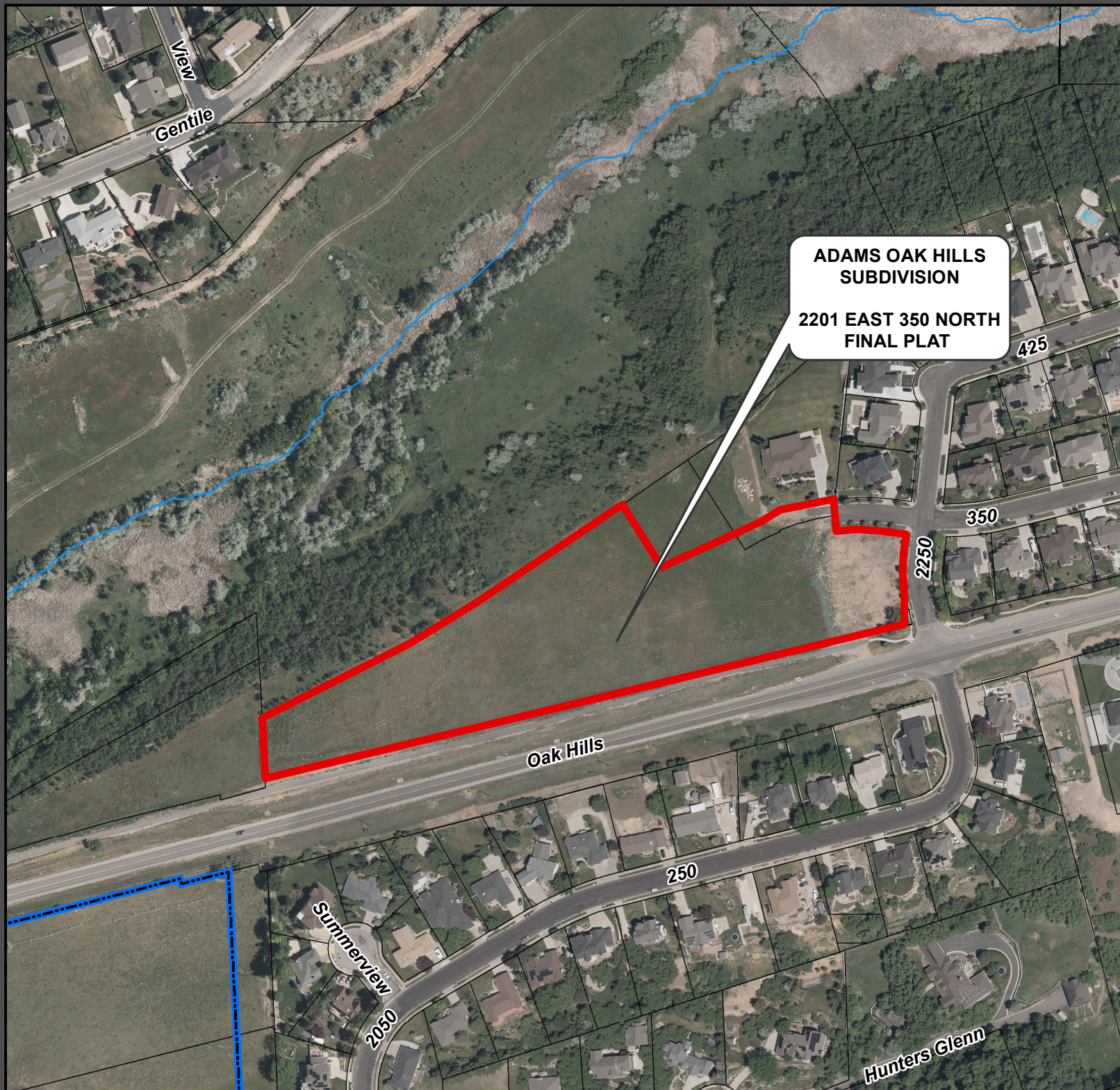
Legend

-  Interstate Highways
-  City Boundary
-  Highways
-  Lakes
-  Streams

 - Project Area



Map 3



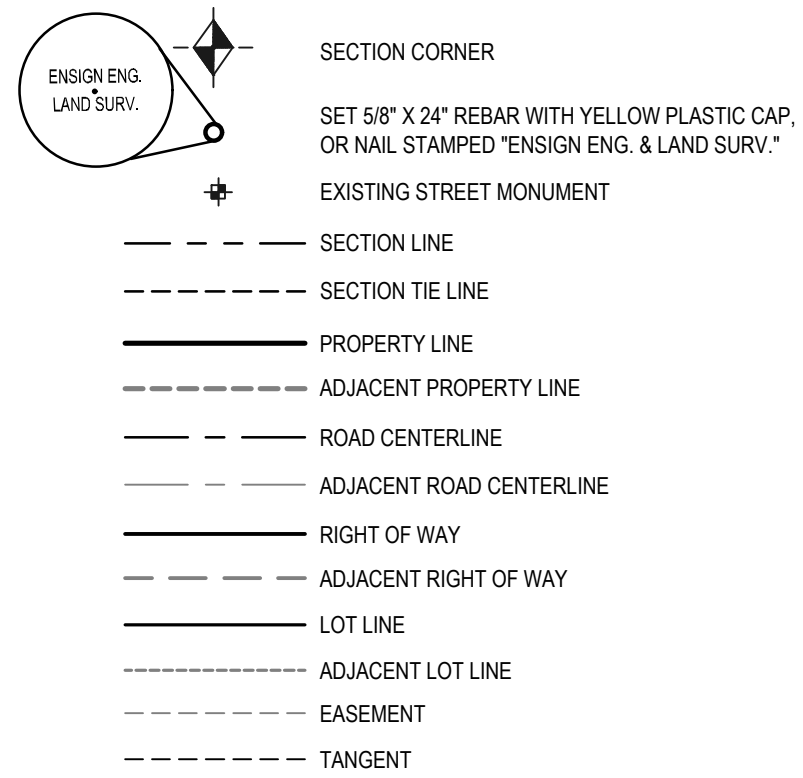
ADAMS OAK HILLS SUBDIVISION

LOCATED IN THE SOUTHWEST QUARTER
OF SECTION 23
TOWNSHIP 4 NORTH, RANGE 1 WEST
SALT LAKE BASE AND MERIDIAN
LAYTON CITY, DAVIS COUNTY, UTAH
FEBRUARY 2024

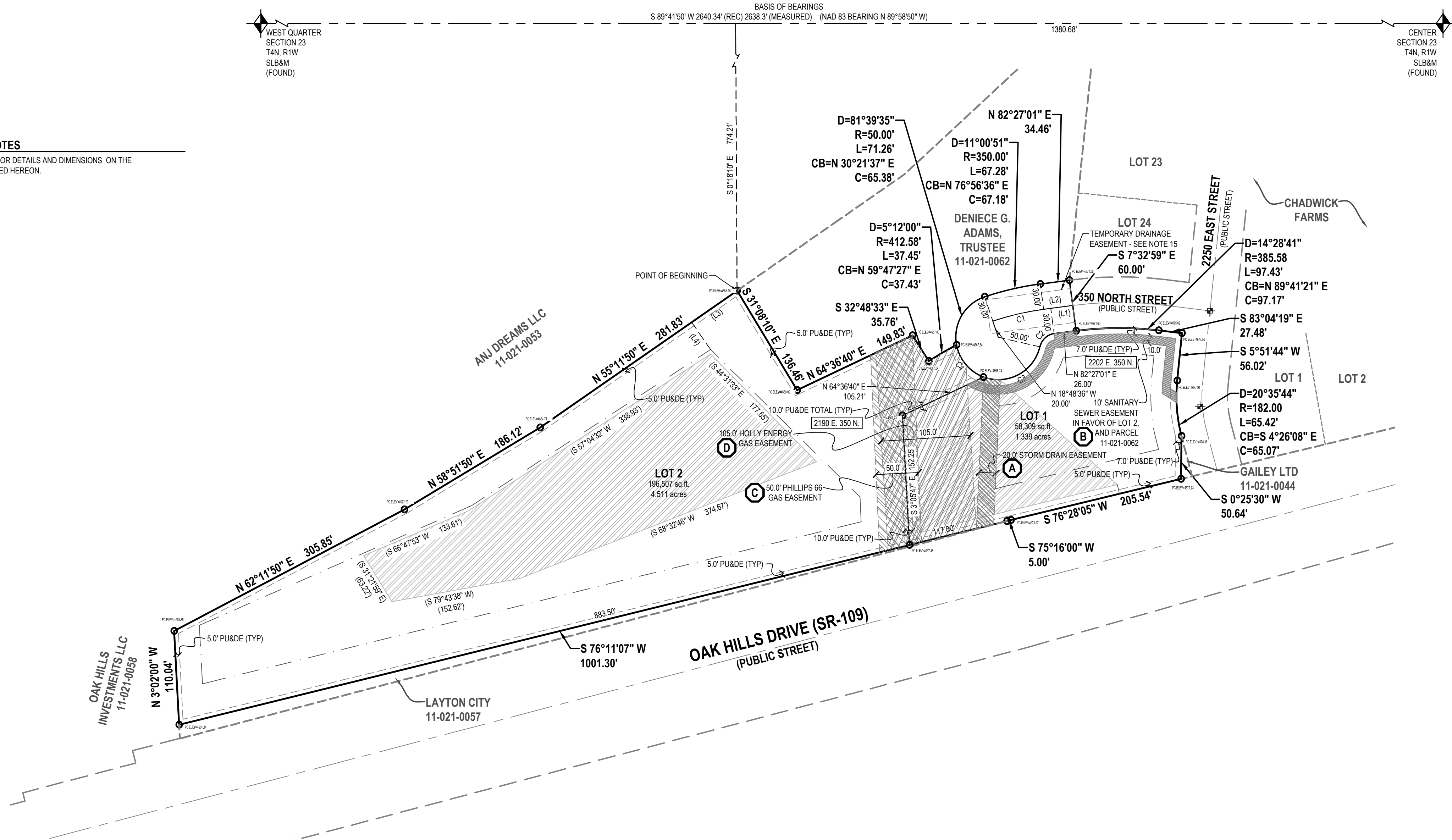
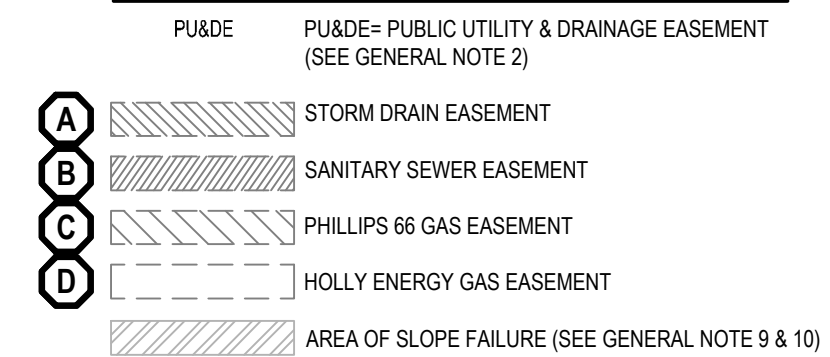
EASEMENT NOTES

1. SEE SHEETS 384 FOR DETAILS AND DIMENSIONS ON THE
EASEMENTS DEPICTED HEREON.

LEGEND



EASEMENT LEGEND



CURVE TABLE					
CURVE	RADIUS	LENGTH	DELTA	BEARING	CHORD
C1	320.00'	61.39'	10°59'28"	S76°57'17"W	61.29'
C2	20.00'	24.26'	69°29'59"	S47°42'02"W	22.80'
C3	50.00'	85.97'	98°30'56"	S62°12'30"W	75.77'
C4	50.00'	50.67'	58°03'53"	N39°30'05"W	48.53'

LINE TABLE		
LINE	BEARING	LENGTH
L1	S82°27'01"W	26.00'
L2	S82°27'01"W	34.46'
L3	S55°11'50"W	65.04'
L4	S34°48'10"E	39.04'

DEVELOPER
JF CAPITOL
1216 W LEGACY CROSSING BLVD, SUITE 300
CENTERVILLE, UTAH 84014
801-513-9537



LAYTON
919 North 400 West
Layton UT 84041
Phone: 801.547.1100
Fax: 801.593.6315
WWW.ENSIGNENG.COM

SALT LAKE CITY
Phone: 801.255.0529
TOOELE
Phone: 435.843.3590
CEDAR CITY
Phone: 435.863.1433
RICHFIELD
Phone: 435.896.2983

ADAMS OAK HILLS SUBDIVISION

LOCATED IN THE SOUTHWEST QUARTER
OF SECTION 23
TOWNSHIP 4 NORTH, RANGE 1 WEST
SALT LAKE BASE AND MERIDIAN
LAYTON CITY, DAVIS COUNTY, UTAH

SHEET 2 OF 4

PROJECT NUMBER : 10128
MANAGER : C.PRESTON
DRAWN BY : M.ELMER
CHECKED BY : T.WILLIAMS
DATE : 2/1/24

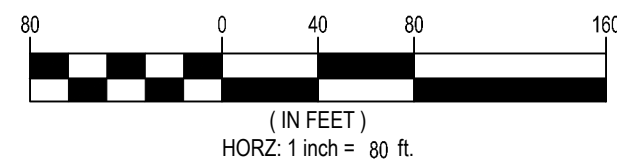
DAVIS COUNTY RECORDER

ENTRY NO. _____ FEE
PAID _____ FILED FOR RECORD AND
RECORDED THIS _____ DAY OF _____ 20____
AT _____ IN BOOK _____ OF OFFICIAL RECORDS
PAGE _____

DAVIS COUNTY RECORDER

BY _____ DEPUTY RECORDER

HORIZONTAL GRAPHIC SCALE



**LAYTON CITY COUNCIL MEETING
AGENDA ITEM COVER SHEET**

Item Number: 6.A.

Subject:

Gordon Avenue and Highway 89 Town Center Master Plan, Amending and Updating the General Plan – Ordinance 24-06

Background:

The Layton General Plan was adopted in September of 2019 after a 5-year public process involving substantial input from residents. One of the key components of the General Plan was the creation of town and urban centers, versus a standard commercial center. A Town Center is a walkable designed oriented mixed-use center that integrates a variety of land uses, such as residential, commercial, and recreational, in a compact environment. Within a Town Center, buildings are brought closer to the street with shops and services on the ground floor and residential units and/or offices above. Town Centers create vibrant communities where people can live, work, and play in close proximity. On the other hand, a standard commercial center is typically designed primarily for automobile access, with little emphasis on the pedestrian or a mix of uses and often consist of standalone retail stores surrounded by parking.

The General Plan has identified six Town Centers distributed throughout Layton. Town Centers are the local centers for everyday life that provide convenience access to goods and services with diverse housing options, shopping, and jobs that are closer to where people live. As housing costs continue to rise and demand for more compact housing increases, Town Centers provide variety and a sense of community. The development focuses on creating a viable mix of commercial, office, and residential uses. While the Gordon Avenue and Highway 89 Town Center is surrounded by development, it has yet to be developed. The City has a rare opportunity to plan a Town Center to match the needs of the surrounding community.

The Town Center area contains approximately 44 acres, all of which are owned by the Utah Department of Transportation (UDOT). UDOT has used the site as a staging area for the Highway 89 construction project over the past several years. This project included creating an off-ramp to Gordon Avenue, which previously did not have a connection to Highway 89. It is expected that UDOT will sell the property and in anticipation, the City has been working with a team of consultants that includes Psomas, LRB Public Finance Advisors, and Wall Consultant Group (WCG) to help put together a market study and traffic impact study (TIS) to better understand the land use issues, market demand, and traffic impacts.

In July of 2023, the City held an open house at Fire Station 54, to receive public comment on how the Town Center should be developed. The City used a preference survey to help collect feedback on the development of the Center. In addition, the City mailed out an online preference survey to every home within 1,500 feet of the Town Center. It is estimated that over 300 residents attended the open house, with over 4,400 aggregated responses received during the event and 229 individuals participating in the online survey. Based on the in-person and online preference survey the most selected land uses for the Town Center included: mixed-use development, multi-use paths, pedestrian bridge/tunnel across Gordon Avenue, restaurants, grocery store, gas station, retail space, green space, day-lighted stream, outdoor seating, and plaza space.

Under the framework of the existing General Plan designation of a Town Center, Staff has worked with the project consultants to compile the feedback received from the community, the market study, and traffic impact study. The information was then used to create a Town Center Plan that can be adopted as an addendum to the General Plan and provide more specific direction on how the area can develop. The Plan was presented to the public for review during the November 14, 2023 Planning Commission meeting. During the public comment, residents expressed concern regarding the proposed density. In particular, residents of the neighborhood expressed concern over the two and three unit attached dwellings proposed abutting existing single-family homes on the west boundary of the town center. Residents also expressed concern over the proposed height of one of the mixed-used buildings, which was shown in the plan with a maximum of 4-stories. The updated plan includes replacing the two and three unit dwellings that border homes fronting onto 2575 East with single-family lots and decreasing the 4-story mixed-use building to a maximum of 3-stories. Additional questions and topics

were raised during the public meeting, which Staff will address during this public hearing.

As a part of the Town Center Plan an assessment was conducted on the City's existing zones that could be used within a Town Center. It was identified that the current municipal code provides for a variety of zones that allow for various residential densities and commercial uses. However, the majority of both residential and commercial zones have minimum design standards, and require large setbacks, making it difficult to establish a cohesive pedestrian-friendly development.

To create a cohesive Town Center, the Town Center Plan recommends the City create an overlay zone that would regulate building design, height, densities, and setback standards. The overlay zone will allow for creativity in the development and highlight the unique natural characteristics of the site and surrounding area. The overlay zone will also focus on providing safe pedestrian connections through the development and to surrounding outdoor amenities. The appendices of the proposed Town Center Plan include design guidelines and recommendations the City can use for a basis in creating a new overlay zone.

Alternatives:

Alternatives are to: 1) Adopt Ordinance 24-06 approving the Gordon Avenue and Highway 89 Town Center Master Plan and updating and amending the General Plan; or 2) Adopt Ordinance 24-06 approving the Gordon Avenue and Highway 89 Town Center Master Plan with modifications; or 3) Not adopt Ordinance 24-06, denying the Town Center Master Plan and not updating and amending the General Plan.

Recommendation:

On January 20, 2024, the Planning Commission voted unanimously to forward a positive recommendation to the City Council to approve the Gordon Avenue and Highway 89 Town Center Master Plan, updating and amending the General Plan.

Staff supports the Planning Commission's recommendation.

ORDINANCE 24-06

(General Plan Amendment, Gordon Avenue and Highway 89 Town Center Master Plan Addendum)

AN ORDINANCE AMENDING GENERAL PLAN TO ADD THE GORDON AVENUE AND HIGHWAY 89 TOWN CENTER MASTER PLAN AS AN ADDENDUM AND PROVIDING FOR SEVERABILITY, REPEALER, AND AN EFFECTIVE DATE

WHEREAS, the purpose of the General Plan is to provide a vision of orderly growth to guide public and private investment for the positive development of our community; and

WHEREAS, the General Plan has classified the subject area as a Town Center; and

WHEREAS, the General Plan states that greenfield Town Centers should be master planned; and

WHEREAS, the City has conducted a study and engaged in a public input process to create a Gordon Avenue and Highway 89 Town Center Master Plan; and

WHEREAS, the Planning Commission held two public hearings on November 14, 2023 and January 30, 2024 to receive public input on the Plan; and

WHEREAS, after receiving and considering public input the Planning Commission voted unanimously to recommend approval of the Plan to the City Council; and

WHEREAS, the Council has reviewed the Planning Commission's recommendation and has received and considered pertinent information in a public hearing regarding the proposal; and

WHEREAS, at the conclusion of the public hearing and upon making the necessary reviews, the Council has determined that this amendment is rationally based, reasonable, and consistent with the intent of the City's General Plan, which is in furtherance of the general health, safety, and welfare of the citizenry.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF LAYTON, UTAH:

SECTION 1. Amendment. "Gordon Avenue and Highway 89 Town Center Master Plan" is hereby adopted to be included as an addendum to the Layton City General Plan as attached hereto.

SECTION 2. Repealer. If any provision of Layton City's ordinance that is deemed to be inconsistent with the amendment is hereby repealed.

SECTION 3. Severability. If any part of this ordinance is found to be invalid by a court of competent jurisdiction, the remaining language shall remain in full force and effect.

SECTION 4. Effective Date. This ordinance amendment shall become effective immediately upon posting.

****Signatures on next page****

PASSED AND ADOPTED BY THE LAYTON CITY COUNCIL ON THIS 21st DAY OF MARCH, 2024.

	AYE	NAY	ABSENT	ABSTAIN
Joy Petro	_____	_____	_____	_____
Zach Bloxham	_____	_____	_____	_____
Clint Morris	_____	_____	_____	_____
Tyson Roberts	_____	_____	_____	_____
Bettina Smith Edmondson	_____	_____	_____	_____
Dave Thomas	_____	_____	_____	_____

JOY PETRO, Mayor

ATTEST

KIMBERLY S READ, City Recorder



CLINTON R. DRAKE, City Attorney



CHAD WILKINSON, Community &
Economic Development Director



GORDON AVENUE AND HIGHWAY 89 – TOWN CENTER MASTER PLAN

March 11, 2024

PSOMAS



Gordon Avenue and Highway 89

Town Center Master Plan

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Recommended Design Guidelines	Appendix-F
Market Study*	Appendix-G
Traffic Impact Study*	Appendix-H
Transportation Reinvestment Zone*	Appendix-I

*Incorporated by Reference (Available for review in the Community and Economic Development Department)

Gordon Avenue and Highway 89

Town Center Master Plan

ACKNOWLEDGMENTS

Planning Commission

Trevor Steenbilk
Lindsey Hamilton
Scott Carter
Julie Pierce
Justin Whitworth
George Wilson
Wesley Felice
Bret Nielson
Peter McDonough

City Council

Mayor Joy Petro
Zach Bloxham
Bettina Smith Edmondson
Clint Morris
Tyson Roberts
Dave Thomas

City Staff

Chad Wilkinson
Lon Crowell
Weston Applonie
Zachary Kadin
Stephen Jackson
Shannon Hansen

Property Owners

Utah Department of
Transportation

Wasatch Front Regional Council (WFRC)

Christy Dahlberg

Utah Department of Transportation (UDOT)

Chris Chestnut

PROJECT TEAM

Psomas

Chris Hupp
Christian Kirkham
Dylan Cindrich
Tanner Snow
Trevor Allen

Wall Consultant Group (WCG)

Jeremy Searle
Shawn Seager
Scott Johnson

Lewis Robertson Burningham LRB

Fred Philpot
Logan Loftis

SPECIAL THANKS

Survey and Public Engagement Participants

Thank you to all the residents, property owners, and interested parties that participated with the interviews, public open house, and surveys. We appreciate the feedback and have incorporated it into all aspects of this project.

Existing Conditions

Executive Summary

The purpose of the Gordon Avenue and Highway 89 Town Center Master Plan study is to establish a community vision and a supportive yet feasible scenario for this planned town center area that fits within the parameters already established by the General Plan. In addition to the master plan study, the project team was tasked to complete a market study, craft a Transportation Impact Study, and look at the creation of a Transportation Investment Zone.

The property is currently owned by the Utah Department of Transportation (UDOT), and has in recent history been used for construction staging for the newly created Gordon Avenue Interchange along Highway 89. Prior to UDOT's ownership, the land was used for farming and was home to an orchard.

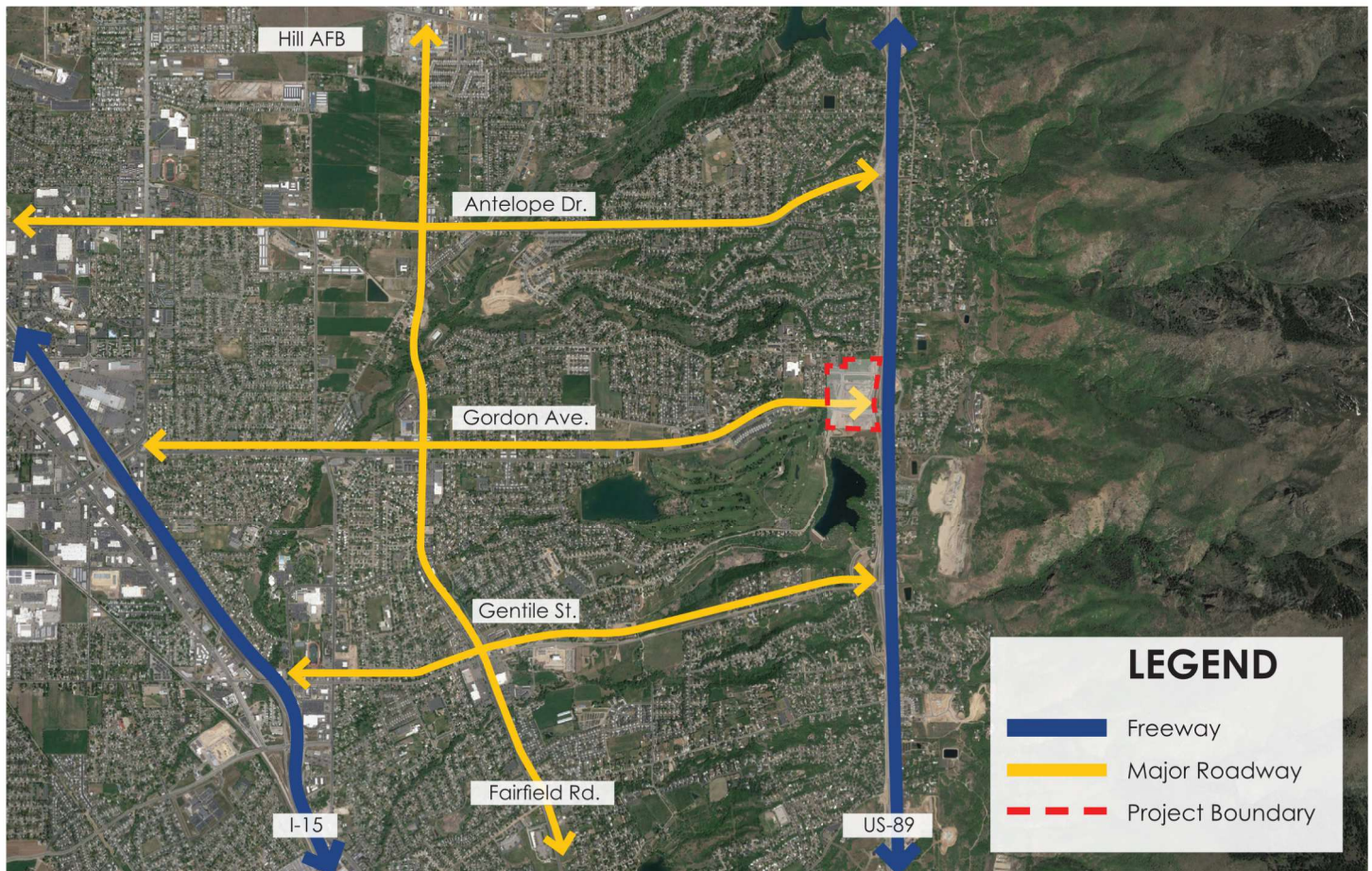
During the 2019 Layton Forward General Plan update the City formed six town center areas, with the purpose of creating "local centers for everyday life that provide convenient access to goods and services with diverse

housing options, shopping, and jobs that are closer to where people live." (Layton Forward General Plan). These six Town Centers were strategically selected to help increase access to various areas within the city. The Town Centers established as part of the general plan were:

- Church & Highway 193
- Antelope & Main
- Gordon & Fairfield
- Gentile & Fairfield
- Highway 89 & Gordon Ave (the focus of this study)
- West Layton

Given the recent State investment in the interchange, and updated State regulations for areas surrounding transportation investments, this was one of the first projects to have a more in-depth study completed, offering much needed services to this highly residential area with incredible access to recreational opportunities.

A vicinity map of the project area can be seen below:



Gordon Avenue and Highway 89

Town Center Master Plan

Existing Plans

Layton Forward

This General Plan update was a five year public process that looked to the future projected growth of the City and created a few guiding principles for the City as a whole. These major principles were as follows:

- **Establish a Balanced Growth Strategy** – which distinguished uses into three land use categories mixed-use, non-residential, and residential.
- **Seek Redevelopment within Urban Districts** – helping to create better pedestrian-oriented places and increasing the quality of these sites.
- **Create Town Centers** – to help facilitate strategic locations throughout the city that aid in providing services and access to recreation within close proximity.
- **Preserve Key Locations for Future Employment** – which provide for the future growth of the City and economic vitality and affordability for Layton residents.
- **Add Variety and Amenities to Residential Areas** – which help increase access and quality of life for Layton residents.

The Gordon Avenue and Highway 89 Town Center area was created as a part of the Layton Forward General Plan process and is one of the multiple Town Centers identified throughout the City.

Layton Active Transportation Plan (ATP)

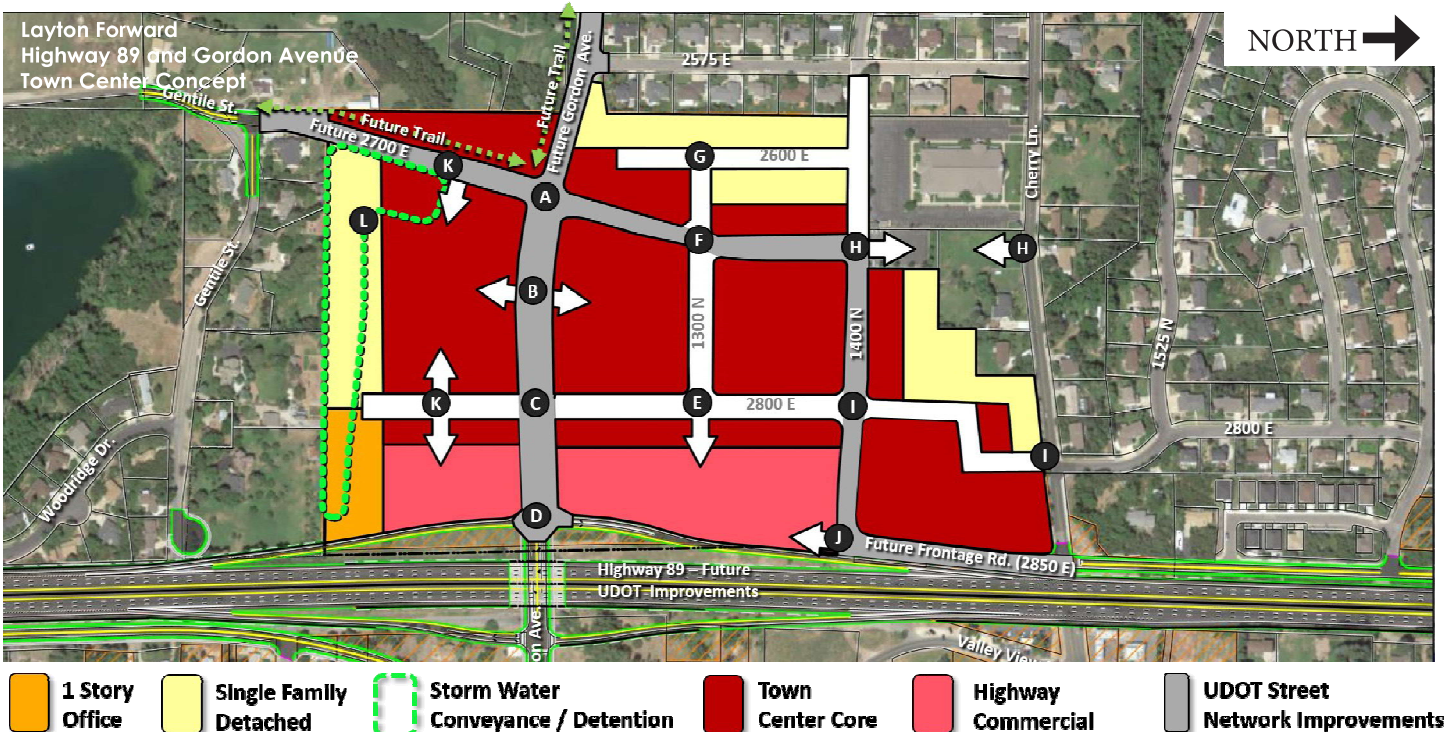
Several active transportation facilities lie within or are adjacent to the study area. These facilities include:

- Hobbs Creek Drive Bike Route
- Gordon Avenue Multi-Use Trail
- Gentile Street Side Path
- Valley View Drive Bike Route
- Cherry Lane Bike Route
- Gentile Street Bike Route
- Bonneville Shoreline Trail (BST) connection from Shay Lane

Layton Parks and Recreation Master Plan

In the Parks and Recreation Master Plan a few facilities are shown passing through the Gordon Avenue and Highway 89 Town Center area. These facilities include:

- A proposed pedestrian bridge, which was built in connection with the interchange.
- A proposed multi-use paved trail, which has been substantially completed and is within 500 ft of the subject town center.
- A proposed bike lane along Highway 89, which has not been built or recommended by the ATP.
- A proposed multi-use paved trail connecting to the Holmes Reservoir Trail.
- Future trailheads along the BST and just west of where 2575 E and 1525 N intersect.

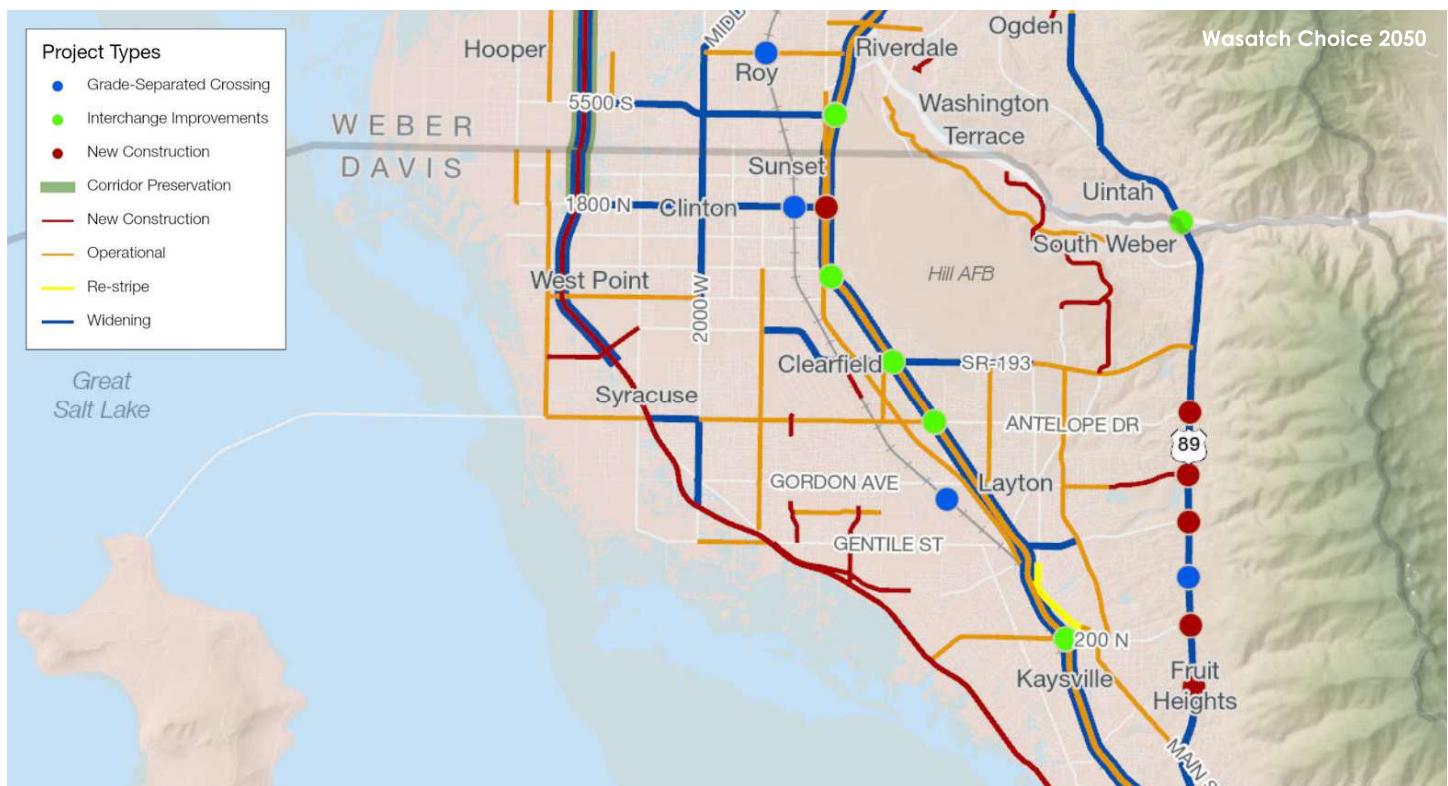


Layton Moderate Income Housing Plan (MIHP)

One of the goals of the MIHP is to “integrate moderate income households into the fabric of the community by mixing into appropriately designed areas, including town centers and mixed-use areas/corridors.” Furthermore, the MIHP seeks to “encourage rezoning of properties within the General Plan’s Urban Districts, Town Centers and Mixed-Use Corridors to allow medium to high density residential housing.” These are key points for this document to integrate and consider.

Wasatch Choice 2050 Plan

As shown in the map below several facilities were planned to be completed around Layton including operational improvements and new construction improvements to Gordon Avenue, connecting Gordon Avenue to Highway 89, and road widening of Highway 89. These, as previously mentioned, have been completed.



Gordon Avenue and Highway 89

Town Center Master Plan

Master Development Plan Process

Introduction and Background

The east side of Layton is predominately made up of single family development, with some public and private open space facilities, trails, civic uses, and intermittent agricultural properties. Residents must travel a few miles to receive basic commercial services. The nearest commercial development along Highway 89 is approximately six miles to the north of the town center, and over five miles to the south. Along Gordon Avenue the closest commercial development is over two miles away. This location has great access and visibility and has had significant investment over the last few years. A few of these investments include the Gordon Avenue Interchange, Layton Fire Station 54, and the future public park east of the site. This future Town Center is approximately 44 acres, most of which is vacant land, and as the City's General Plan states it has "direct access to central Layton and regional access to Weber County and South Davis County, making the center compelling for retail and services for surrounding residential areas and increasing highway corridor travel."



The Layton Forward plan established several land use placemaking strategies which:

- Explore a Town Center tied to recreational opportunities.
- Building orientation should take advantage of views of both the mountains and lake.
- Uses should be pedestrian oriented with off-street parking shielded from streetscapes and plaza areas
- Smaller boutique retail and restaurant uses and live/work uses that are oriented to the plazas

- Buildings adjacent to single family homes should be no more than two stories and no more than four stories in the Town Center
- Multi-family residential may be integrated using the following design standards and conditions:
 - Townhome-style, main street building, or mansion house building forms.
 - Multi-family housing may be integrated, with housing units above live/work units and commercial services.
 - The Town Center should be governed by a Town Center Home Owners Association or Community Association board.
 - Multi-family amenities should be shared by the Town Center community, and accessible from public streets rather than internally oriented.
 - Explore potential for multi-family residential built over podium (structured) parking.

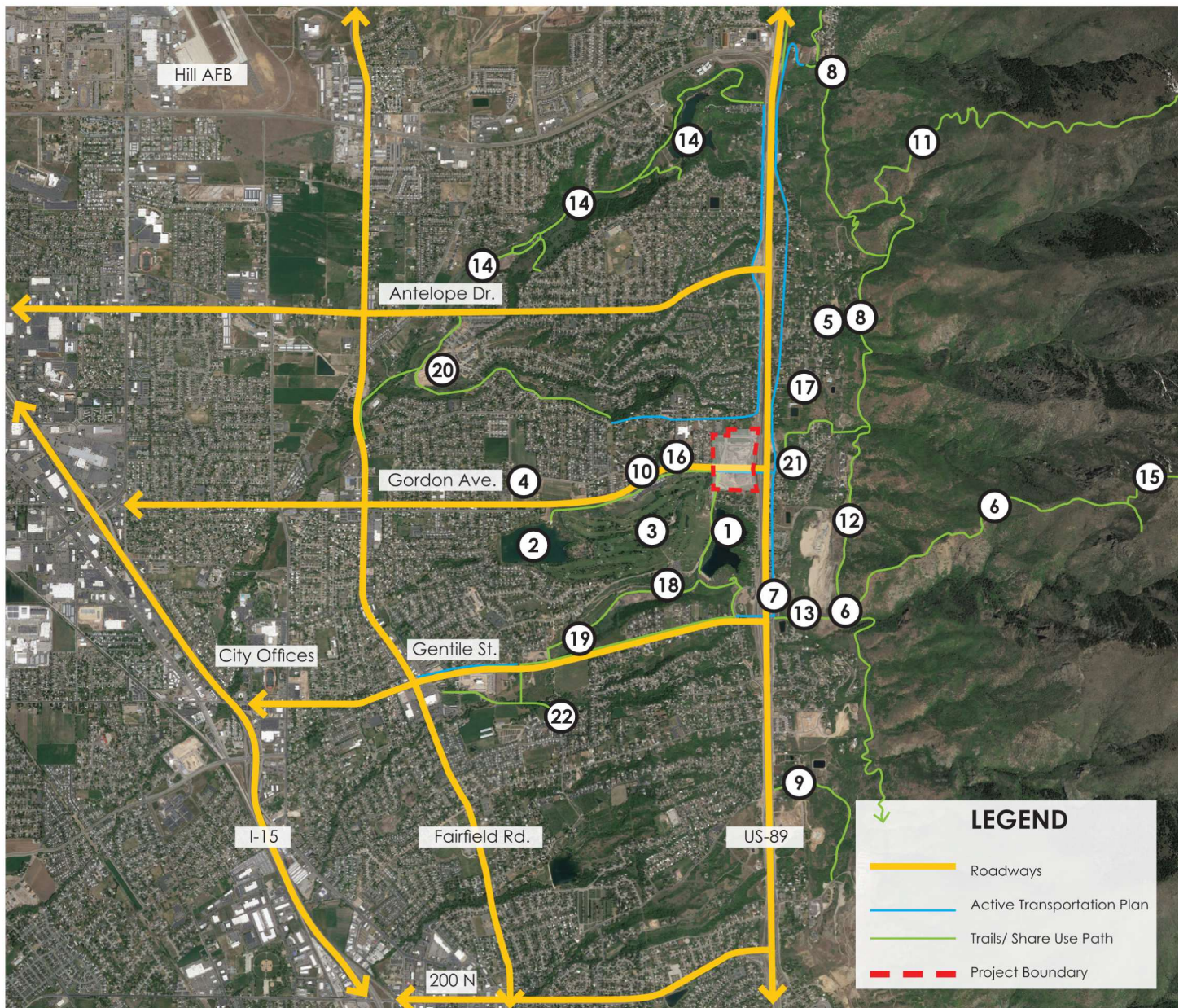


The public feedback received for this study confirmed many of the ideas captured in the Layton Forward plan, and are supported in the preferred concept for this Town Center. One of the main reasons to explore a Town Center tied to recreational opportunities lies in the proximity of this site to a wide range of great regional and local open space facilities, outlined on the following page.

Local Open Space Amenities

There is a wide range of local amenities surrounding the Gordon Avenue and Highway 89 Town Center. The development has access to the following:

1. Holmes Creek Reservoir
2. Adams Reservoir
3. Valley View Golf Course
4. Andy Adams Park
5. Fernwood Recreation Site
6. Adams Canyon Cabin Trailhead/Trail
7. Adams Canyon Waterfall and Cabin Hike
8. Bonneville Shoreline Trailhead/Trail
9. East Mountain Wilderness Trail
10. Gordon Avenue Trail
11. Great Western Trail
12. Fernwood Trailhead
13. Lower Water Fall Trailhead/Trail
14. Kay's Creek Parkway Trail/Trailhead, Hobbs Reservoir and Welker Trailhead
15. Thurston Peak Trail
16. East Gordon Trail
17. Snow Canyon Trail
18. Holmes Creek Reservoir Trail
19. Oak Forrest Trail
20. Mid Fork Trail
21. Future Snow Canyon Park
22. Future Boynton Park



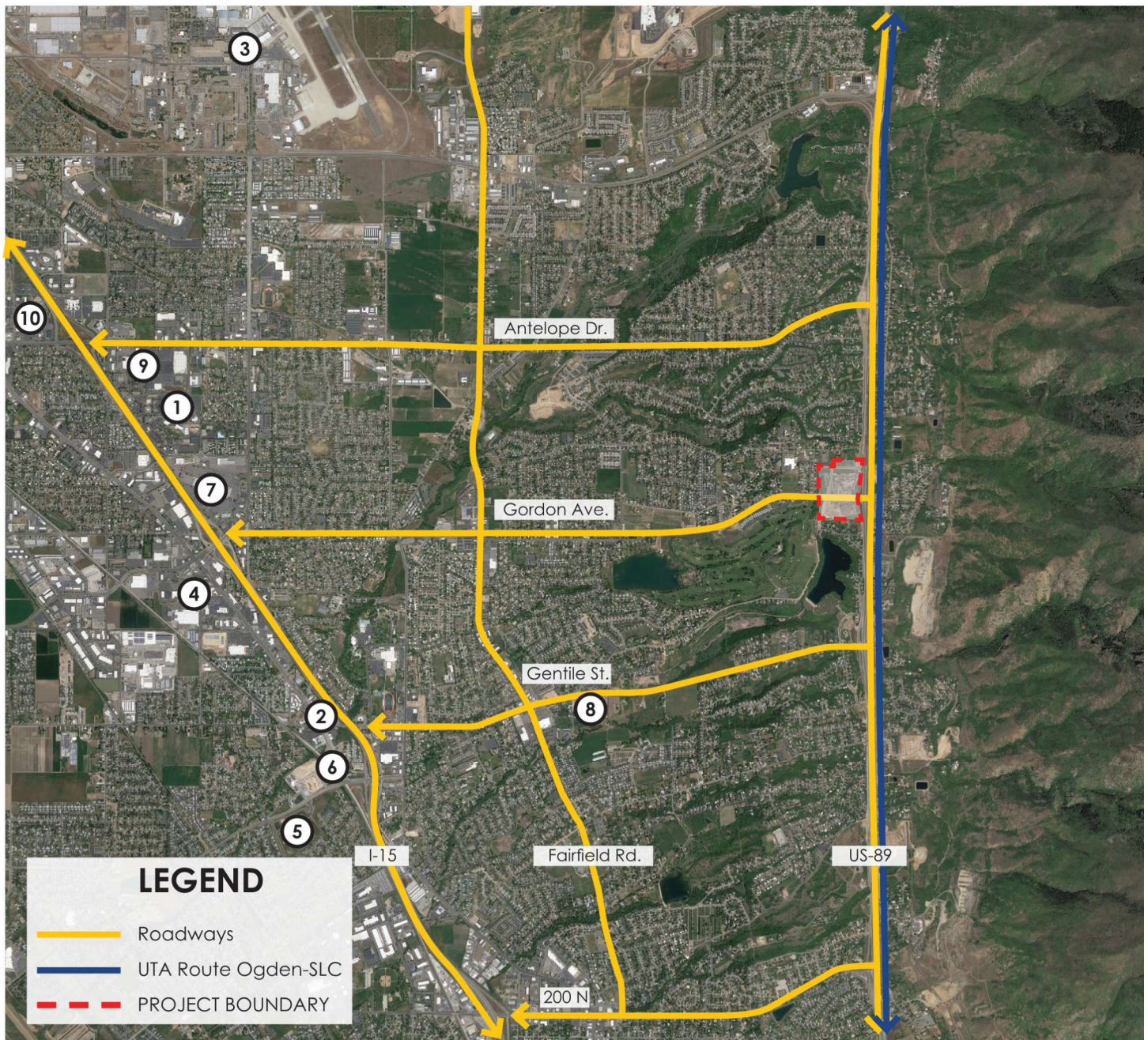
Gordon Avenue and Highway 89

Town Center Master Plan

Commercial Services and Amenities

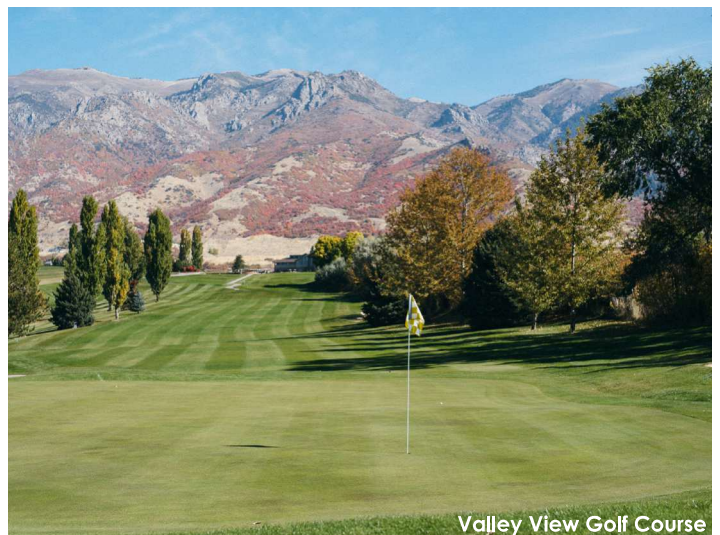
There are also several non open space amenities within the larger region of the Gordon Avenue and Highway 89 Town Center. These amenities include:

1. Davis Conference Center
2. Downtown Layton
3. Hill Air Force Base
4. Hill Field Road Commercial Center
5. IH Hospital
6. Layton FrontRunner Station
7. Layton Hills Mall
8. Layton Temple
9. Midtown Commercial Area
10. Tanner Clinic and Davis Hospital



Gordon Avenue and Highway 89

Town Center Master Plan



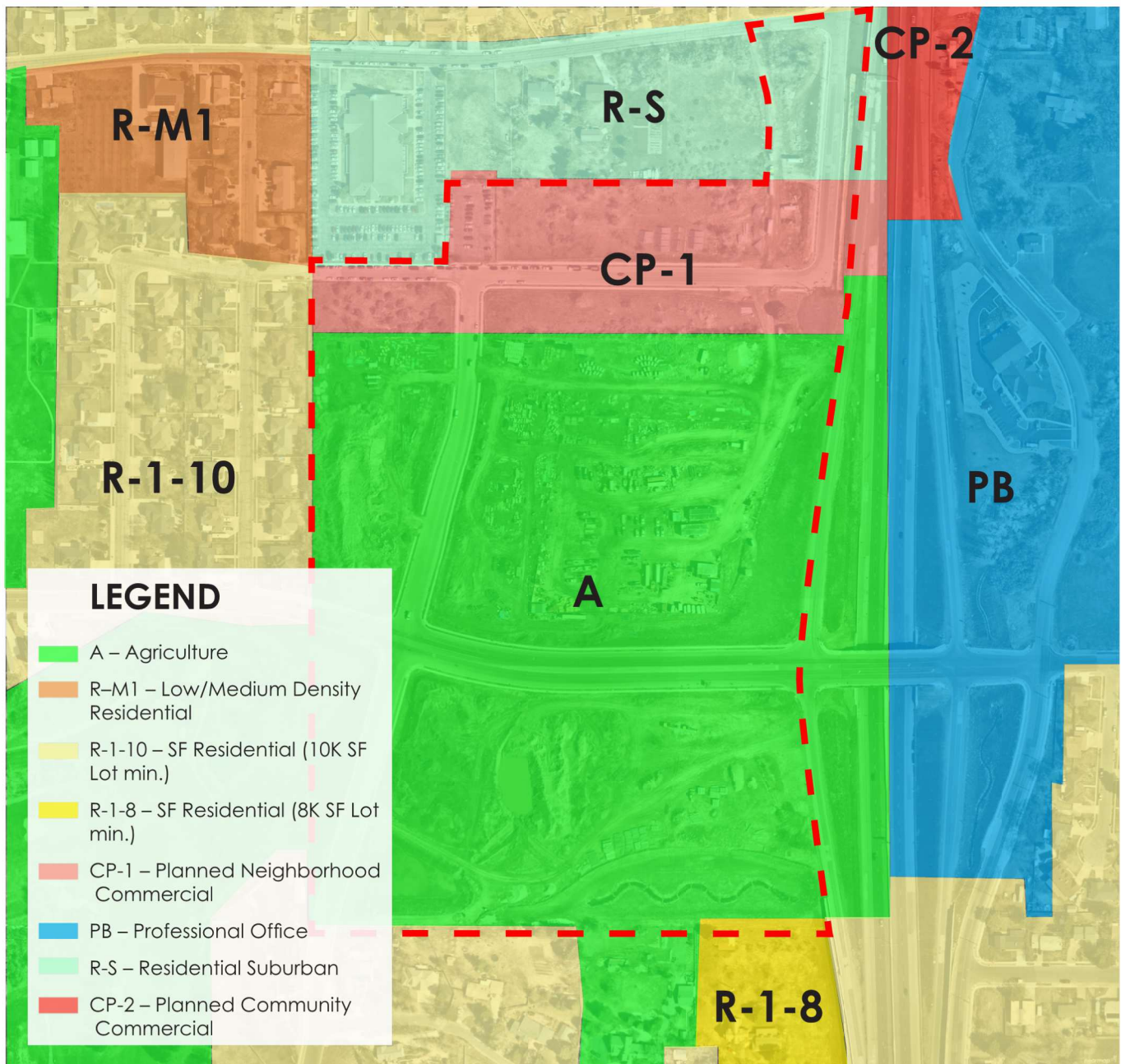
Gordon Avenue and Highway 89

Town Center Master Plan

Zoning and Land Uses

The majority of the project site is zoned A (Agricultural). The site contains approximately 37.02 acres zoned A (Agricultural), approximately 2.15 acres zoned R-S (Residential Suburban), and approximately 7.03 acres zoned CP-1 (Planned Neighbor Commercial). The commercially zoned areas are broken up into parcels that are not large enough to provide a viable commercial space. Although the area to the east of the

town center is zoned PB (Professional Office), the majority of the property is owned by Layton City or UDOT and will not be developed as professional office. The vicinity of the site is bordered by single-family residences, a golf course, and a church. The newly constructed frontage road for Highway 89 travels directly through the project site.



Zoning Assessment

The tables below provide an assessment of the existing zones that could be used within the town center.

Assessment of Residential Zones

	R-1-6 (Single Family Residential)	R-2 (Single and Two Family Residential)	C-TH (Condominium/Townhouse)	RM-2 (Medium Density Residential)	R-H (High Density Residential)	MU (Mixed-Use)
Land-Use	Single-Family Residential	Single-family attached, duplex	Townhomes, duplex, mansion homes	Multi-Family Residential	Multi-Family Residential	Commercial, Retail, Office, and/or Multi-family Residential
Units Per Acre	5.5	10.89	16	19.17	25.97	Density is based on design, height, parking, landscaping and setback requirements
Maximum Height	30'	30'	35'	35'	35'	35'
Design Standards	None	None	Yes, but updates are recommended	None	None	Yes, but updates are recommended
Considerations	Large setback requirements	Large setback requirements	Design focused development	Large setback requirements	Large setback requirements	Allows for commercial and residential uses

Assessment of Commercial Zones

	CP-1 (Planned Neighborhood Commercial)	CP-2 (Planned Community Commercial)	PB (Professional Office)	MU (Mixed-Use)
Land-Use	Commercial services for basic trade and personal services which occur regularly or frequently. It services both pedestrian and vehicular traffic	Provide a range of commercial uses greater than CP-1 but lower intensity than a regional commercial area. The zone focuses on vehicular-oriented services	Provide area for offices and institutions in which the intensity of use, in terms of hours of operation and number of customers, is less than that of a commercial zoning district.	Provide areas that may combine commercial or retail uses with multi-family residential uses within buildings, or within close proximity to other uses along a corridor or within a walkable district.
Maximum Height	40'	40'	35'	35'
Design Standards	Minimal	Minimal	Minimal	Yes, but updates are recommended
Considerations	Large front setback requirements, Zone is limited to commercial uses	Large front setback requirements, Zone is limited to commercial uses	Large front setback requirements, Zone is limited to small office uses with limited commercial	Allows for commercial, retail, office, multi-family but would need additional restrictions to regulate the amount of each use

Current municipal code provides for a variety of zones that allow for various residential density options. The residential zones are limited to 35' in height; providing increased height would allow for a broader range and design of residential units that could be constructed. The commercial zones are designed for pedestrian and

vehicular traffic, and can facilitate a broad range of commercial, office, and retail land-uses. The majority of the residential and commercial zones require a large setback and have minimal design standards, making it difficult to establish a cohesive pedestrian friendly development.

Gordon Avenue and Highway 89

Town Center Master Plan

Land Use Classification

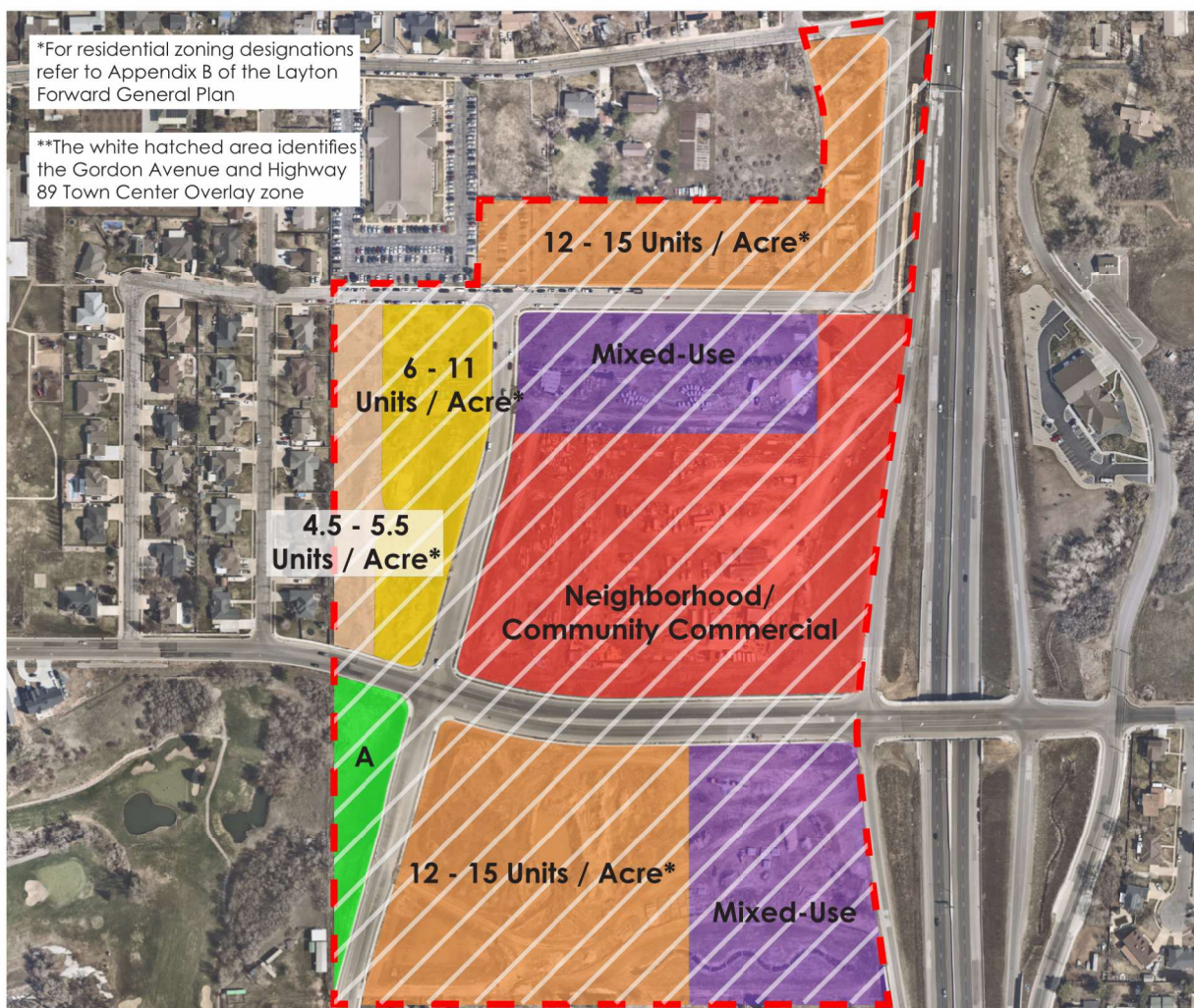
The preferred zoning map below seeks to provide applicable residential zones based on densities outlined in Appendix B of the General Plan. The commercial area has a designation of neighborhood/community commercial. Identifying the scale of commercial uses should be focused on local retail services.

Land-Use / Density	Approximate Size
4.5-5.5 Units per Acre / Detached Single Family Homes	1.97 Acres
6-11 Units per Acre / Residential Twin Homes, Mansion Homes	3.15 Acres
12-15 Units per Acre / Residential Townhomes	12.48 Acres
Mixed-Use Areas / Retail, Office and/or Residential	8.63 Acres
Neighborhood-Community Commercial / Commercial	11.72 Acres
Agriculture / Agriculture	1.59 Acres

Note: The size and location for each land-use classification outlined herein is general to allow for flexibility in development.

A Town Center Overlay Zone is recommended over the project site to create a cohesive development that can regulate design, and establish compatible building heights, setbacks, densities, and potential

land uses for the town center. The overlay zone should include flexible standards that allow for creativity in the development, while also focusing on pedestrian connections throughout the site and to surrounding outdoor amenities. The design standards should highlight the unique natural characteristics of the site and the surrounding area. To establish the proposed mix of commercial, retail, and residential uses will require the properties within the town center to be rezoned. A rezone process will require support of the property owner, UDOT. It is anticipated that UDOT will sell the property within a short time after completing all improvements needed to the Highway 89 road construction project. The City should partner with UDOT to establish an entitlement process either through a development agreement that outlines the requirements and process for development or by modifying the standards applicable to the property through a rezone process. The latter would require the City to create a new overlay zone that includes the design standards referenced herein.



Property Ownership and Land Features



Gordon Avenue and Highway 89

Town Center Master Plan

Project Process Overview

This project has been sectioned into five major phases which are: 1) kick-off and existing conditions, 2) public visioning and engagement, 3) alternatives analysis, 4) preferred scenario, and 5) public hearings. These phases help the City, its residents, property owners, stakeholders, and the project team work together to best understand the needs of the community in terms of culture, vision, development potential, fiscal and infrastructure needs, and best practices for a sustainable town center development.

Kick-Off and Existing Conditions

The process for this project began with meetings with elected officials and City staff to better understand some of the opportunities, constraints, and vision for the project area. In addition to this meeting, stakeholder interviews were held to further detail this vision. An initial market analysis was then conducted for the study area to better understand viable land uses and land use mix.

Following this, the project team compiled and analyzed relevant plans, City Code, existing zoning and future land use maps, UDOT and Utah Transportation Authority (UTA) plans, utility and infrastructure files, aerial photography, demographics, property ownership, and topography to better understand the opportunities and constraints provided in the kick-off and stakeholder interview meetings.

Public Visioning and Engagement

During this phase of the plan, input was gathered on how

the various stakeholders (residents, staff, elected officials, property owners, and public agencies) would like the area to develop, what they would like to see happen, and any other feedback. This process incorporated the opportunity to see potential development types and leave specific feedback for what is desired for this area. Feedback was received through one-on-one stakeholder interviews, a public open house at a Layton fire station, a visual preference survey, and a digital survey.

Alternatives Analysis

Based on the findings and vision established in the previous phases, three concept scenarios were developed. These were established around varied land use and transportation networks to better focus on the right street network and mix of land uses. These were submitted to City Staff for review, then a preferred scenario was created.

Preferred Scenario

Portions of each of the three alternative scenarios were selected by city staff, and a preferred scenario was then developed with a supportive traffic impact study, transportation reinvestment zone study, and updated market study.

Public Hearings

Public hearings were held by the Planning Commission and City Council to consider official adoption of the plan.



View looking east from the Gordon Avenue and Highway 89 Town Center

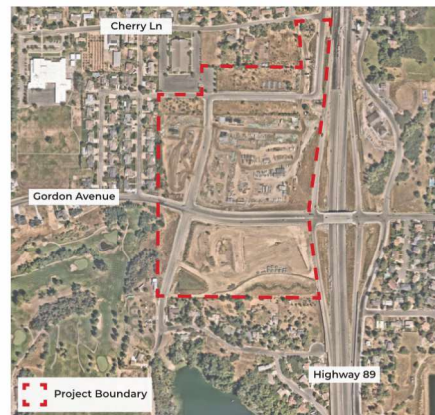
Public Engagement

To initiate the public engagement, 10 small group stakeholder interviews were held to gather targeted feedback on the property. This feedback included vision for the Town Center, area opportunities and constraints, elements to preserve within the Town Center, and how this Town Center supports the local residents and the other Town Centers within Layton.

Following the stakeholder interviews an open house was held at Fire Station 54 on June 29th from 6 - 8 PM.



It is estimated that over 300 residents were in attendance at the open house, and over 4,400 aggregate responses were received during the event. A digital survey was also created which gathered the same information, and was sent to all residents via a mailer following the open house event. There were 229 participants who completed the digital survey.



For additional information about the project, please contact the Layton City Planning Department at:

Planning@laytoncity.org

(801) 336-3780



The purpose of the open house was created to receive input from residents for the town center in four subject areas. These areas were:

- **General Vision and Overview** – which allowed residents to give open ended feedback about their vision for the area
- **Land Use and Community Character** – which discussed residential and non-residential development types
- **Open Space and Recreation** – which asked which amenities residents would most like to see
- **Transportation** – which addressed multimodal facilities and potential transportation improvements to increase access and safety

A brief synopsis of the open house feedback is provided below with more detailed public engagement results in the appendix. During this process the project team also looked into the preliminary branding of the site which included the orchard/farm theme with rustic elements, and urban orchards throughout the development. The logo created during this phase is shown in the appendix under the public engagement section.

Gordon Avenue and Highway 89

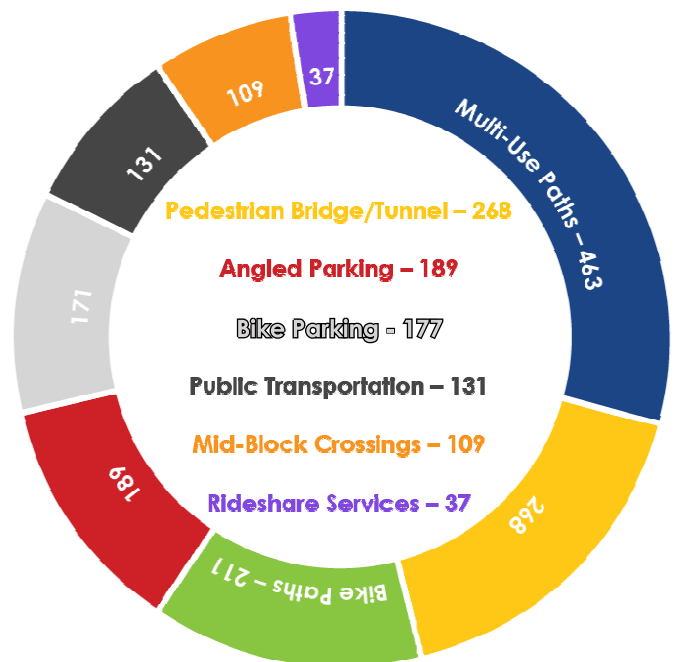
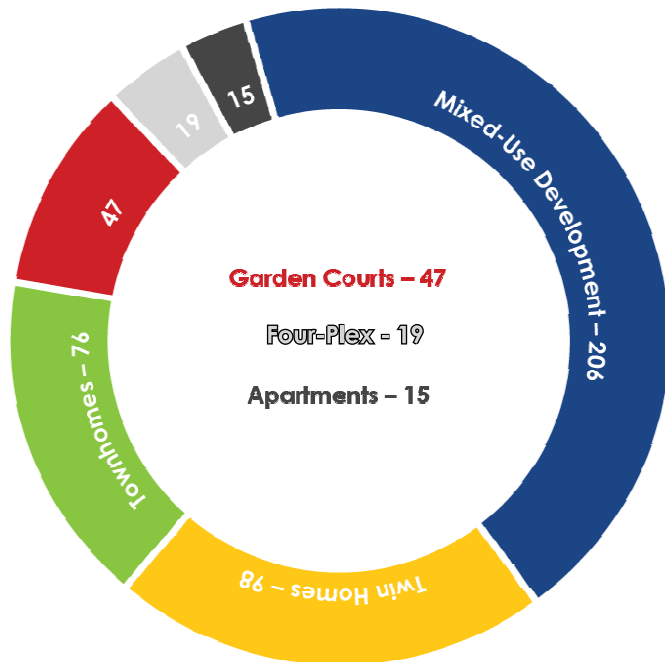
Town Center Master Plan

The results below were based on a visual preference survey. In this visual preference survey, individuals were shown images that represented different options for the Town Center and then selected the images/options they most wanted to see in the Town Center. The data below

represents the number of people who supported each individual option in the survey. Higher numbers indicate options that received more support and lower numbers indicate options that received less support.

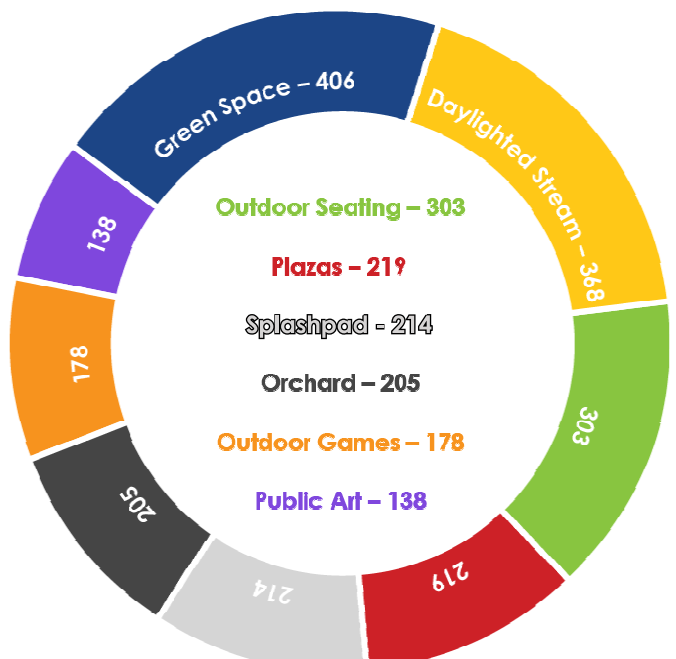
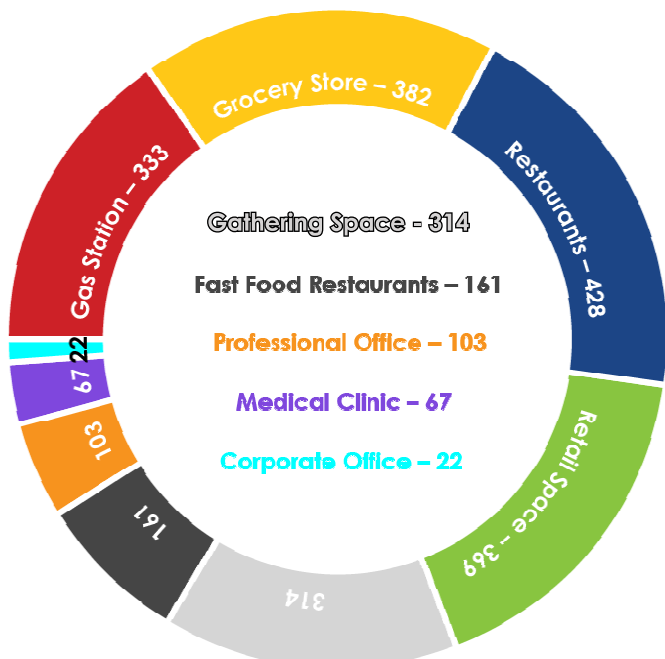
Choose the residential land uses you would MOST like to see within the Town Center

Choose the transportation infrastructure you would MOST like to see within the Town Center



Choose the non-residential land uses you would MOST like to see within the Town Center

Choose the open space / recreation amenities you would MOST like to see within the Town Center



Key Takeaways

The top ten visual preference survey ideas residents wanted to be incorporated within the Town Center were:

- Restaurants
- Mixed-Use Paths
- Retail Space
- Green Space
- Grocery Store
- Daylighted Stream
- Gas Station
- Gathering Space
- Outdoor Seating
- Pedestrian Bridge/Tunnel connecting the north and south sides of the project

Note: A list of the preference survey comments can be found in Appendix A - Public Engagement Summary.



Gordon Avenue and Highway 89

Town Center Master Plan

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Preferred Conceptual Plan

STATISTICAL SUMMARY

Project Total	45.13 ac
Open Space	
Plaza Space	
Roads	
Commercial/Retail Space	180,000 - 220,000 s.f.
Housing Types	
6,000 s.f. Single Family Lots	8 - 12
Duplex	12 - 25
Mansion Home/Triplex	6 - 15
Townhomes	116 - 150
North Density Housing	80 - 105
Total Housing Units	220 - 270
	4.9 - 6.0 (du/ac)

NOTE: This preferred plan is conceptual in nature. Building design, amenities, and road network are all subject to change.

Alternate Conceptual Scenario



Project Total	45.13 ac
Open Space	
Plaza Space	
Roads	
Commercial/Retail Space	190,000 - 230,000 s.f.
Housing Types	
6,000 s.f. Single Family Lots	8 - 12
Duplex	12 - 25
Mansion Home/Triplex	6 - 15
Townhomes	116 - 150
North Density Housing	80 - 130
Total Housing Units	220 - 270
	4.9 - 6.0 (du/ac)
Off-Street Parking	
Required Parking Spaces	730 - 760
Provided Parking Spaces	730 - 760



Development Statistics

BUILDING NUMBER	# OF FLOORS	LAND USE	TOTAL SQ. FT.	COMMERCIAL SQ. FT.	HOUSING UNITS	REQUIRED PARKING	PROVIDED PARKING
1	3	Mixed-Use Retail/ Housing	68,250	22,750	45	170	170
2	3	Mixed-Use Retail/ Housing	68,250	22,750	45	170	211
3	2	General Retail	28,800	28,800	0	115	115
4	2*	General Retail	10,860	10,860	0	44	44
5	2	Grocery Store	44,200	44,200	0	177	193
6	2*	General Retail	22,690	22,690	0	91	91
7	1	General Retail	7,000	7,000	0	28	28
8**	3	Mixed-Use Office/ Housing	13,200	13,200	0***	22	25
9**	3	Mixed-Use Office/ Housing	13,200	13,200	0***	22	25
10**	3	Mixed-Use Office/ Housing	37,950	37,950	0***	64	68

*Functional one story building with facades made to look like a two story building.
**Potential for building to be professional office services. (Ex. Dentist, medical clinic, attorney, etc.)
***In a mixed-use or housing only configuration, these three buildings combined could include from 20-60 housing units

LEGEND

1 STORY

2 STORIES

3 STORIES

Gordon Avenue and Highway 89

Town Center Master Plan

Renderings



Gordon Avenue and Highway 89

Town Center Master Plan



Gordon Avenue and Highway 89

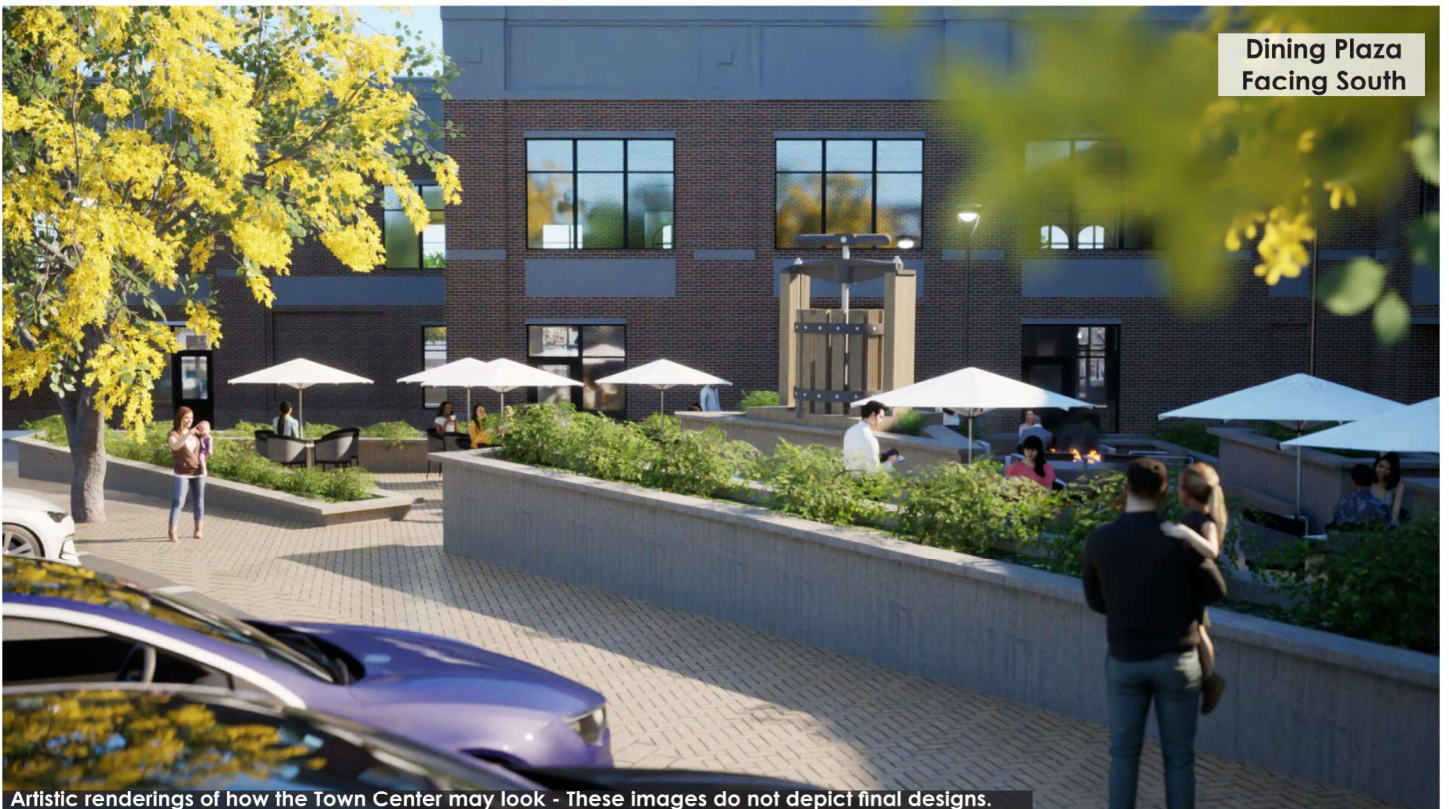
Town Center Master Plan

Town Square
Facing Southeast



Artistic renderings of how the Town Center may look - These images do not depict final designs.

Dining Plaza
Facing South



Artistic renderings of how the Town Center may look - These images do not depict final designs.

Gordon Avenue and Highway 89

Town Center Master Plan



Gordon Avenue and Highway 89

Town Center Master Plan

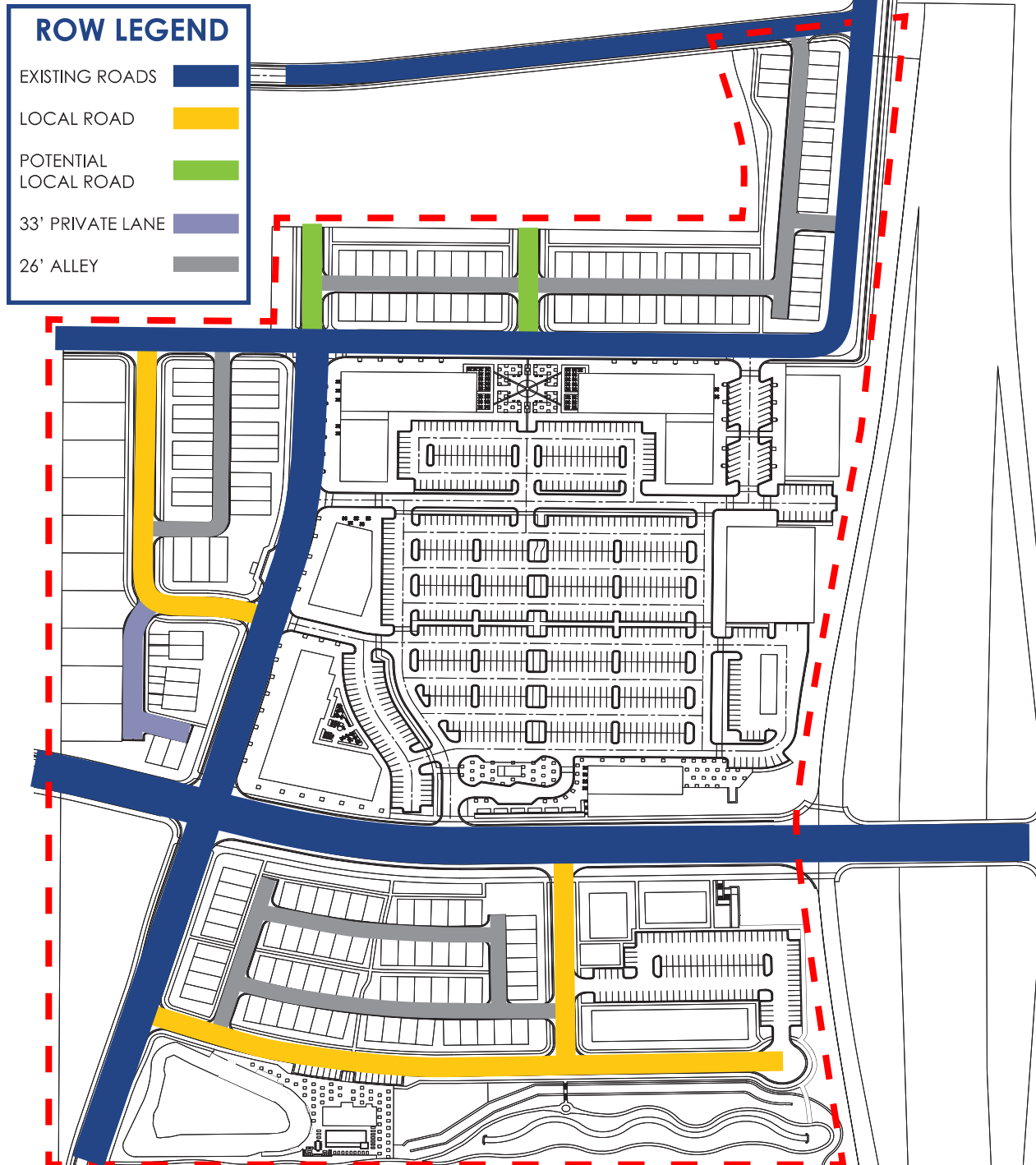


Gordon Avenue and Highway 89

Town Center Master Plan

Potential Thoroughfare Plan

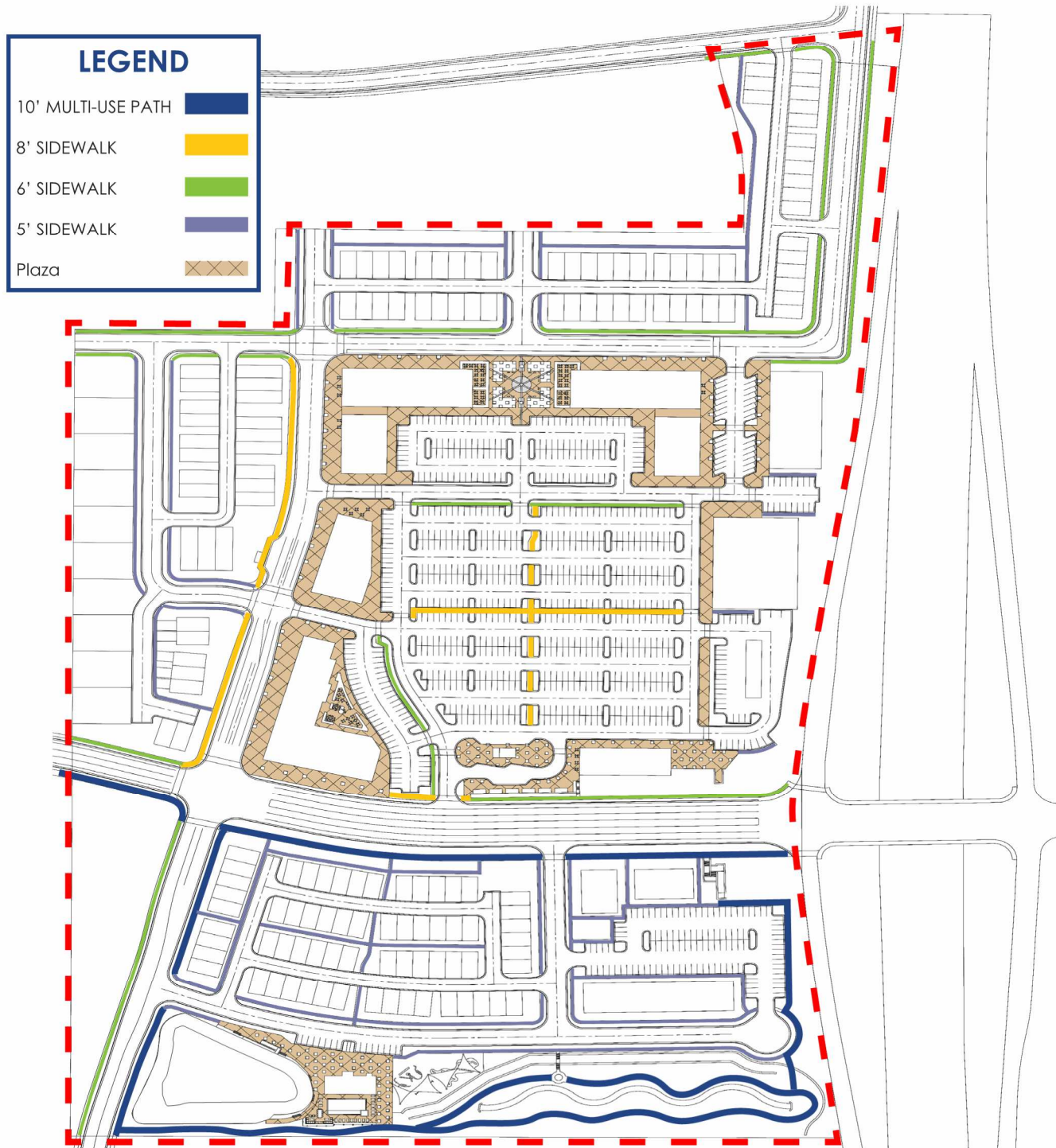
The general street network is delineated below, indicating major street types and their potential general location. Streetscape cross-sections associated with the proposed thoroughfare plan will be built in accordance with Layton City engineering standards for street widths.



Potential Sidewalk Master Plan

The general layout of sidewalks and multi-use paths within the development is delineated below.

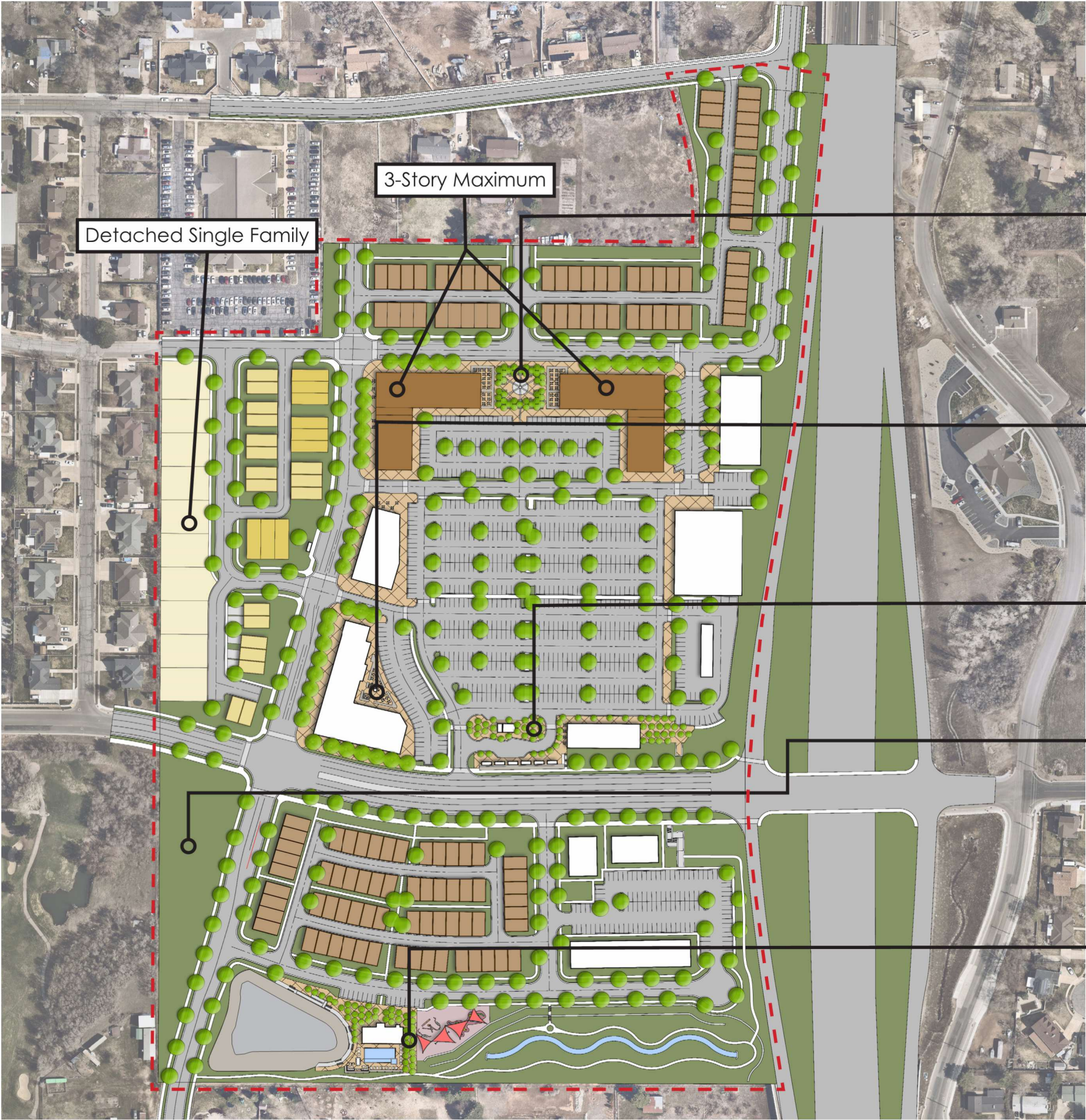
For additional Active Transportation Plan facilities in the area see pages 2 and 5 of this document.



Gordon Avenue and Highway 89

Town Center Master Plan

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Open Space Plan

- Town Square
- Outdoor Dining
 - Raised Planters
 - Orchards
 - Outdoor Seating
 - Lighted Plaza
 - Themed Splash Pad

- Dining Plaza*
- Outdoor Dining
 - Raised Planters
 - Themed Elements
 - Outdoor Seating
 - Lighted Plaza
 - Fire Pits

- Mobility Hub*
- Outdoor Seating
 - Orchards
 - Lighted Plaza
 - Restrooms
 - Transit Station
 - Multi-Use Path
 - Covered Bike Parking

- Trailhead*

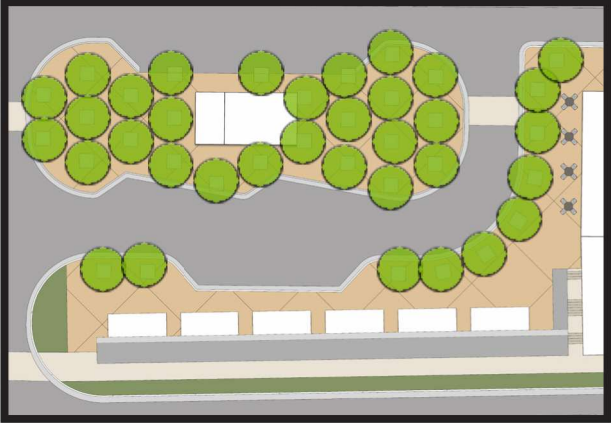
- Amenities*
- Public Amenities
- Large Playground
 - Stream Bed
 - Covered Pavilion & Lighted Plaza
 - Small Activity Lawn
 - Walking Paths
- Private Amenities **
- Clubhouse
 - Pool & Hot Tub
 - Cabanas & Lounge Chairs
 - Fire Pits & Outdoor Seating
 - Barbecue Grills



Town Square



Dining Plaza



Mobility Hub

Footnotes:
* These are proposed potential amenities and are subject to change.
** Proposed amenities would be associated with a private development, these are not public amenities.

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Market Study and Traffic Impact Study Overview

Market Study Synopsis

As stated in the market study for this project, “The supportable commercial analysis reinforces the current concept site plan for the Town Center...”, and “With a high concentration of projected growth, the strong market that has existed in Layton for many years, proximity to a regional transportation network such as Highway 89, and minimal overlap with competitive market sites, the Town Center provides an ideal location for the expansion of neighborhood scale retail. This will be aided by the conversion of the parcels surrounding the Town Center, currently owned by the Utah Department of Transportation (UDOT), to mixed-use for development purposes. The Town Center will primarily serve local demand based on convenience-oriented neighborhood scale retail, with a focus on offering day-to-day goods and services, while providing a gathering place for Layton residents.”

Traffic Impact Study Synopsis

This piece of the Town Center study addresses the traffic impacts associated with the proposed town center development.

Given the project's proposed mixed-use development, the project is anticipated to add an additional 17,562 daily trips to the roadway network, including 737 trips in the morning peak hour and an additional 1,525 trips in the evening peak hour. This was calculated by looking at projected users for the amount of square footage for each use the market study supports, which included approximately 145,000 square feet of commercial space, 350+ units, a gas station, and a mobility hub.

The level of service (LOS) for both morning and evening peak hours was determined for each study intersection under the existing and existing plus project scenarios as outlined in the Traffic Impact Study (TIS). The results of the analysis are summarized in the TIS in the appendix.



Appendix A - Public Engagement Summary

OVERVIEW AND VISION



Public Open House/Online Survey Notes and Comments:

Land Uses

- 12 ▪ Walkable mixed density residential, with local dining, shops, and quality design like Station Park
- 6 ▪ No multi-residential
- 2 ▪ No gas station
- Small police station
- Existing coffee shop has been open during construction. They should be included in commercial space they paid their dues!
- Grants/pricing for local businesses like Grounds for Coffee. No Big Box.
- We need a toy store. How about Teton Toys
- 9 ▪ Outdoor "REI" feel...central gathering space with a vibe for outdoor recreation shops/venues.
- 8 ▪ Open Space and Amenities
- 8 ▪ Green Space!
- 8 ▪ Weekly Farmers' Market
- 8 ▪ Park (like by City Hall), playground, ballfields, gathering space, bike trail, keep the character of the area.
- 4 ▪ Ice Rink!
- 4 ▪ Dog Park
- 3 2 ▪ Plant trees around perimeter to honor orchards that were here before.

Transportation

- 9 ▪ Re-Do the intersection at Gordon Avenue and 2625 East, put in a traffic circle, or place a stoplight here. Individuals can't see towards the East or West. It is an unsafe intersection for pedestrians to cross.
- 9 ▪ Do not complete proposed Road H to Cherry. Does not benefit anyone already connecting north. Make proposed Road I more fluid into road G.
- 5 ▪ Complete road (2625 E/"H") to Cherry Lane
- 4 ▪ Frontage Road From Cherry Lane to Gordon
- 3 ▪ Move stop signs closer to road on 89 entrances and exists
- 2 ▪ Lights at intersections (Pointing to both sides of the interchange)
- 2 ▪ Very dangerous school crossing. Everyone speeds here. Need cross walk/or light. (Pointing to 2550 E and Gordon Ave.)
- 2 ▪ Excessive traffic on 2575 E. no more thru traffic!
- 2 ▪ Excessive Speeds along Gordon Ave.
- No Traffic Lights!!
- Excessive speeds and traffic volume on this residential street (1400 N). Need stop signs (4-way)
- 3 ▪ Trails
- 1 ▪ Amphitheater in park for mini concerts. outdoor movies (Connor Laughlin Creek Reservoir)
- Senior Center

General

- 1 ▪ Increased taxes, traffic, signals, crime. NO!
- 1 ▪ Why isn't this part of the Town Center? (Pointing to the Wall Family properties)
- 3 ▪ Single Developer, good track record, follows master plan
- Should the money be spent elsewhere? Like revitalizing "downtown" Layton instead.
- Clean, efficient, family-oriented
- Nothing
- Would prefer no development here at all.
- A cool "welcome to Layton" sign on 89 instead of a single dopey Layton sign.
- Don't cover mountain view from Homeowners who have lived since 1994!
- Like

Gordon Avenue and Highway 89 Potential Logo Design



Design

- 16 ▪ Add noise/sound wall along south side of Gordon!!
- 6 ▪ Dark Sky
- 3 ▪ No billboard signs
- 2 ▪ Environmentally pleasing to the eyes.
- Please don't cover mountain view by building tall buildings or homes.

- # Like
- # Dislike

TRANSPORTATION

Public Open House/Online Survey Notes and Comments:



Mid-Block Crossings



Multi-Use Paths



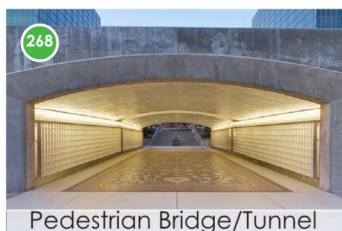
Bike Paths



Bike Parking



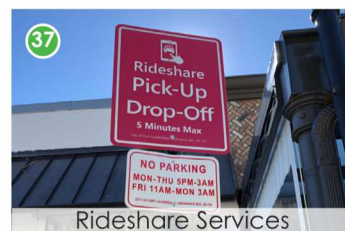
Angled Parking



Pedestrian Bridge/Tunnel



Public Transportation



Rideshare Services

Transportation

- 3
 - Finish road (2625 E) to Cherry Lane
- 4-way stop on on 2575 E and 1400 N
- All-way stop at Gordon Ave and 2575 E
- Maybe a gate closing 2550 E from Gordon
- 9
 - Improve frontage road to divert traffic from using 2575 (and 2550 e) east as a frontage.
- 1
 - Do not extend 2800 E across Cherry Lane.
- 22 2
 - Traffic signal/Traffic Circle at Gordon Ave and 2625 E
 - Vehicle headlight issues south bound off ramp ssw into back yards windows
- 2
 - Need curb cuts off 2575 E for wheelchairs to cross Gordon Street to walk on the other side
 - 2625 needs to go through to cherry lane. 2575 is too narrow to be a primary rd.
 - Please provide concrete fence wall between residential single family homes on 2575 E. And new development. Like you have down Gordon.
 - Crosswalks need to have some sort of alert light and very well marked or people just drive right through.
- 7
 - Traffic circle at 2550 E so children can cross safely
 - Crosswalk on Gordon for kids and walkers or runners south side to get across to elementary school! Needed Now!!
 - Sidewalk on east side Dr.?
- 2
 - Connect trail to park on east of Highway 89
- 5
 - Dedicated bike lanes not sharrows
- 2
 - Circulator shuttle through development
- If rideshare and bus is still on antelope they maybe don't need one here
- Why here? we already have commuter lots nearby.
- We want all of this, especially the bus to Ogden-SLC.
- 2
 - Parking/Bus stop for bus to FrontRunner
- 2
 - East West Route Public Transportation to downtown, especially for those with disabilities
- 1
 - Make adequate parking space and vehicle lanes.
- 1
 - Public EV fast charger 250kw
- 3
 - Good traffic control is paramount! Ample parking and preventing traffic back-ups in the surrounding areas
 - No large parking lots please. This would increase crime in the area.
 - Whatever you do, just don't do another "can't turn left" intersection"
 - Please do not put a road connecting Cherry lane to this area next to the stake center. The parking there is already problematic.
 - Undesirable emissions ordinance enforced.
 - Two tunnels
 - More traffic, more traffic signals, increased property taxes, increased crime. NO!
- 2
 - Van accessible wheelchair parking

Like
Dislike

OPEN SPACE AND RECREATION



Public Open House/Online Survey Notes and Comments:



Daylighted Stream



Outdoor Seating



Orchard



Green Space



Plazas



Public Art



Splashpad



Outdoor Games

Open Space and Recreation

- 6
 - Open Space
- 11
 - Multi Use Biking/Walking Paths - Connect to Bonneville Shoreline
- 15
 - Clean and Family Friendly Park
- 5
 - Plant Trees
- 22
 - We could use a Dog/Pet Park!
- 13
 - Rec Center/Sports Facility- Turf, Soccer, Aquatic Center, Pickleball, etc.
- 3
 - Ice rink!
- 4
 - Community Garden space. Teach kids how to grow food!
- 4
 - Outdoor gathering space that preserves the area's natural beauty.
 - I have a child w/a disability. Please Build an all accessible playground =)
 - Path around Andy Adams Lake
 - We need another open cemetery in Layton.
 - Check out Carmel, Indiana's open space community space incredible <3
- 3
 - More nature and less building development. Parks, bike paths, trees, etc. A giant park would be awesome!
 - You could build a Bike Park!
 - Parkour or ropes course
 - Outdoor amphitheater
 - Farmer's markets
 - A Gym would be great here.
 - It would be great to see a multi use path such as the large sidewalks built around that area. It allows runners like me to get to areas doing what I love!
 - I think it would be great to see a communal area for East Layton. Especially with multi use side walks for any kind of activity.
 - Multi-use paths to connect to other trails – Denver-Rio Grande, Kays Creek
 - Green space. Summer grass, winter sledding hill. Great place for kids to play

LAND USE AND COMMUNITY CHARACTER



Public Open House/Online Survey Notes and Comments:



LAND USE AND COMMUNITY CHARACTER



Public Open House/Online Survey Notes and Comments Continued:

Retail

- 28 ▪ Trader Joe's
- 15 2 ▪ Farmington Station-like space.
- 5 ▪ Maverick Gas Station
- 1 ▪ Car Wash
- 4 ▪ Small Grocery Store
- 11 ▪ Local/Small Businesses
- 5 ▪ Convenience Stores
- 2 ▪ Breakfast Restaurant
- 3 ▪ Ice Cream Parlour
- Movie theater
- I would love for it to be a destination for many facets of life. I'd like to see local interesting restaurants, a spot for exercise classes, a nice grocery store, a walking friendly area with a nice place to hang out and relax, a small concert and arts venue, and a coffee/tea shop.
- 1 ▪ Do not put a strip mall in there that is going to look like crap in five years
- If it is necessary to put in a gas station (we already have enough), at least low-key signage rather than blaring 100 foot poles.
- An outlet type of shopping are kind of like Station Park with some restaurants and retail stores. Trader Joe's and Starbucks and retail like H & M and others.
- 8 ▪ No big box stores! Keep the lovely character of the area!
- "I would love to see some local charm. A unique cafe, desert shop, restaurant, small grocery like Trader Joe's, boutique shopping. Lots of green space, bike parking and very walkable. Easy for the already established neighborhood community
- Please do not create Farmington Station, leave commercial structures in down town Layton.
- 1 ▪ Bountiful town square and main street as pattern so well done, fun, family centered, clean, managed well, etc.
- Serene outdoorsy environment with lots of grass and trees; walkable fitness trail. Please do not put anything that would attract hoodlums. Build the environment to fit the current culture and environment would be great, No Blockbuster style stores/businesses.
- A place with easy walking access, calm outside areas to relax and enjoy some live music. Small personally owned restaurants without all the chains.
- 3 ▪ A space with retail and restaurants and green space for an urban feel with a touch of home!
- City Creek SLC!!! Great model! No highrise apts. Must not block views
- Casual sit down restaurants, indoor outdoor ets. With relaxing atmosphere. We need a nice relaxing area. To get out of the hustle of the day. Clean, trees and flora, if it just crammed with tight buildings it will just be another eyesore on the once beautiful bedroom city. This area is the east gateway.
- Think "Daybreak" here. They have done it so well. This is not a large space but keep it green natural, peaceful, relaxing, residential free, (we have overpopulated our fair city). Yes a few shops and restaurants, but keep it open, green, parks, play areas. An oasis to hang out and enjoy! Thank you for listening.
- Recreation and fun laid back atmosphere. Few restaurants, coffee shop, soda shop, lots of park and paths, dog park (Layton has no dog parks), lots of shade and seating. Fun activities (splash pad, playground). We have enough real estate around. And office space just seems to be out of place. Have it be an area for everyone versus space being taken by people who would live or work there.
- Scenic - shops- fun place to gather for date night like ice cream restaurants- unique food places - entertainment
- I really hope it's a nice place that makes you feel good about going and not just a generic chain shopping plaza
- My biggest request to keep it as "rural" feeling as possible, while still promoting progress. I just don't want it to feel like a bunch of concrete and asphalt. I grew up a couple hundred yards from the area and I want it to feel similar to how it's always felt. A u-pick orchard would be really cool too. Lots of green space and places for families and kids to gather, but not purely shopping like station park. That is way too busy for the area.
- Build an outdoor amphitheater for local talent to use.
- Low profile buildings, 1 or 2 story only, that

Like
Dislike

LAND USE AND COMMUNITY CHARACTER



Public Open House/Online Survey Notes and Comments Continued:

don't block mountain and valley views.
High quality food places like Panera, Corner Bakery Cafe, Costa Vida. Maybe a Soda place like Fizz.

54

1

- Accessible, Retail, Recreation
- NOT Great Clips, or Autozone, or that stuff. We need useful infrastructure.

2

- Like Italian Piazza- more classic than just a plaza

6

- Whole Foods

2

- No car wash or gas station
- Please we don't need commercial here. Keep it in downtown Layton!

Mixed-Use/Walkability

1

- Walkability/ Pedestrian Friendly
- Smart mix of commercial and residential with limited multi family housing and industrial businesses
- Pedestrian outdoor space.
- It would be great if this project was pedestrian-centric. Giant parking lots like the mall and farming station are ugly and not practical for walking around. Street parking would be preferable over parking lots.
- Ground floor retail, professional office, etc. 2nd and 3rd floor residential.
- Beautiful shopping and town center. Hopefully themed with fountains and green space. Not cookie cutter and not with large big box stores or generic buildings. No new homes, unless they are mixed-use with businesses down below them
- A shopping area with easy access from 89 but safe for people to walk from nearby neighborhoods
- Walkable town center with amenities that draw citizens to the area that not only benefits those driving by, but also those who live in the area.
- Combination of small housing apartments, local shopping and green space
- Economically viable high end shops and town homes.
- A pedestrian friendly project where you can feel secure letting your kids bike or walk without fear of getting hit by a car.
- Restaurants, grocery store, shopping, condos incorporated into the lifestyle
- Affordable housing with plenty of parking and a family friendly restaurant and corner

market.

- Small shops with leasable space for mixed-use. Breakfast place, boutiques and such. One or two anchor stores, primarily grocery and household.

Housing

1

- To maintain the existing quiet nature of the area, avoid high density housing, avoid retail, and avoid additional traffic.
- The demographic population voting is established, wealthy, and older. Young people would love more multifamily housing in this location, green and outdoor. Socializing spaces.
- We need single family homes
- Place for families, less cars, no apartments, business you can walk to, small local restaurants
- Local flavor similar to local piazzas in Italy. Variety of restaurants and shops. Outdoor dining options. Walking and visiting friendly. Variety of architectural features and landscaping. No apartments, townhouses, multi-storied structures, etc. Maintain views of mountains and valleys. Provisions for local small entertainment groups - singing groups, instrumental, etc. Warm, cozy, inviting atmosphere.
- My overall vision is to have a serene area open and green space, some single family or duplex homes with limited retail.
- Support for the homes surrounding the area. A few restaurants, gas station convenience store, coffee shop, car wash. No large business stores, warehouses, apartments. Twin homes owned would be ok. Limit unit density to reduce potential cars. Would like to see "dark sky's lighting, all buildings have the same exterior colors and appearance. Landscape buffer 25-30 feet behind curb along all major roads in rock and low water vegetation. Possible business buildings that are single story.
- Beautiful natural greenspace to walk, picnic, relax. Not a bunch of apartments to add to the water usage and traffic problem in Layton. Please use common sense here it is not always about the money. I live down the street from this project. Please keep this a nice place to live, not a noisy traffic jam!

LAND USE AND COMMUNITY CHARACTER



Public Open House/Online Survey Notes and Comments Continued:

- Single family homes and some businesses, such as a grocery store, a couple of restaurants etc. classy places. Please no more town homes, twin homes, apartments or other close quarter housing.
- Single family homes with a grocery store and open park space for families. Do not create another Station Park type development. Leave east layton as a family centered single family home location.
- Low density mix of single family homes (rambler/patio homes) adjacent to existing homes on 2575 E and commercial (Maverik, grocery, McDonald's, car wash, sit-down restaurants) closer to Hwy 89 with green space (park, walking trails) mixed in.
- Nicer than the ones at Heat 293 and Gordon and Fairfield.
- Upscale homes compatible with the others in the area. Unique, Upscale shops. A park. A fountain or other tasteful art.
- Middle to upper class.
- Residential on the West Side
- 4 ▪ I would love to see units set aside for affordable housing given our crazy housing market.
- 1 ▪ Small single family homes so young folks can afford a home
- No High Rises
- Limited Development maybe a few patio homes

Other

- No Billboards
- Something that feels like a small town
- A fun and safe place for everyone to have fun
- A child-friendly spot.
- A beautiful place!!
- A place not overly crowded but useful for the community and in harmony with the area
- A place that is nice to look at and be around. Things to do there.
- Appealing to look at, not overcrowding, not too much traffic for the area
- At the very least put a liquor store in
- Good lighting around the area of it would make it feel really welcoming
- I would like to see the use of plants that are native to Utah on display as part of the

landscape, to encourage people to use our beautiful native plants instead of other, random ornamentals. To attract pollinators for people's education and enjoyment and to save water.

- A high quality, upscale development consistent with the Layton Forward master plan.
- Clean and appealing
- Control night light pollution – keep it dark at night
- 1 ▪ Micro hometown center
- Keep a rural downtown feel
- Quiet area that provides a function for the immediate neighborhood. Not looking for a tourist attraction:
- Safe and sustainable development
- 1 ▪ Senior Center
- Single developer with good track record. Utah based preferred.
- No self storage site!!
- Have ya'll thought about the increase costs for police patrols if you build business centers you bring crime to neighborhood. Just a thought.
- Keep streets lights to minimum and yellow not blue hue

Like
Dislike

Appendix B - Amenities and Theming

Open Space Amenities and Theme

Potential Public / Quasi-Public Amenities



Urban Orchard



Outdoor Dining



Outdoor Seating



Fire Pits



Outdoor Lighting



Themed Plaza Design



Splash Pad



Stream



Walking Paths

Potential Private Amenities



Barbecue Grills



Playground



Community Pool



Poolside Cabanas



Clubhouse

*These are proposed potential amenities and are subject to change.

Gordon Avenue and Highway 89

Town Center Master Plan

General Amenities and Theming

Branding and theming of the development helps establish a sense of place, and encourages neighbors and visitors to form connections and memories with the development. The images on the next two pages are added to create the feeling and style of the development and depict how the architecture

and elements may look. It is not anticipated that the development will look exactly how these images are depicted, only offer guidance for the how the architecture, theme, and elements should be scaled to the pedestrian.

Buildings and Architecture



Lighting



Fencing



Amenities and Theming Continued

Signage

Monument



Monument



Wayfinding



Street



Commercial



Commercial



Commercial



Building



Additional Elements

Trash Cans



Seating



Seating



Character



Planting Design



Planting Design



Outdoor Dining



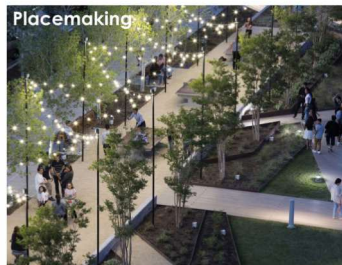
Outdoor Dining



Placemaking



Placemaking



Placemaking



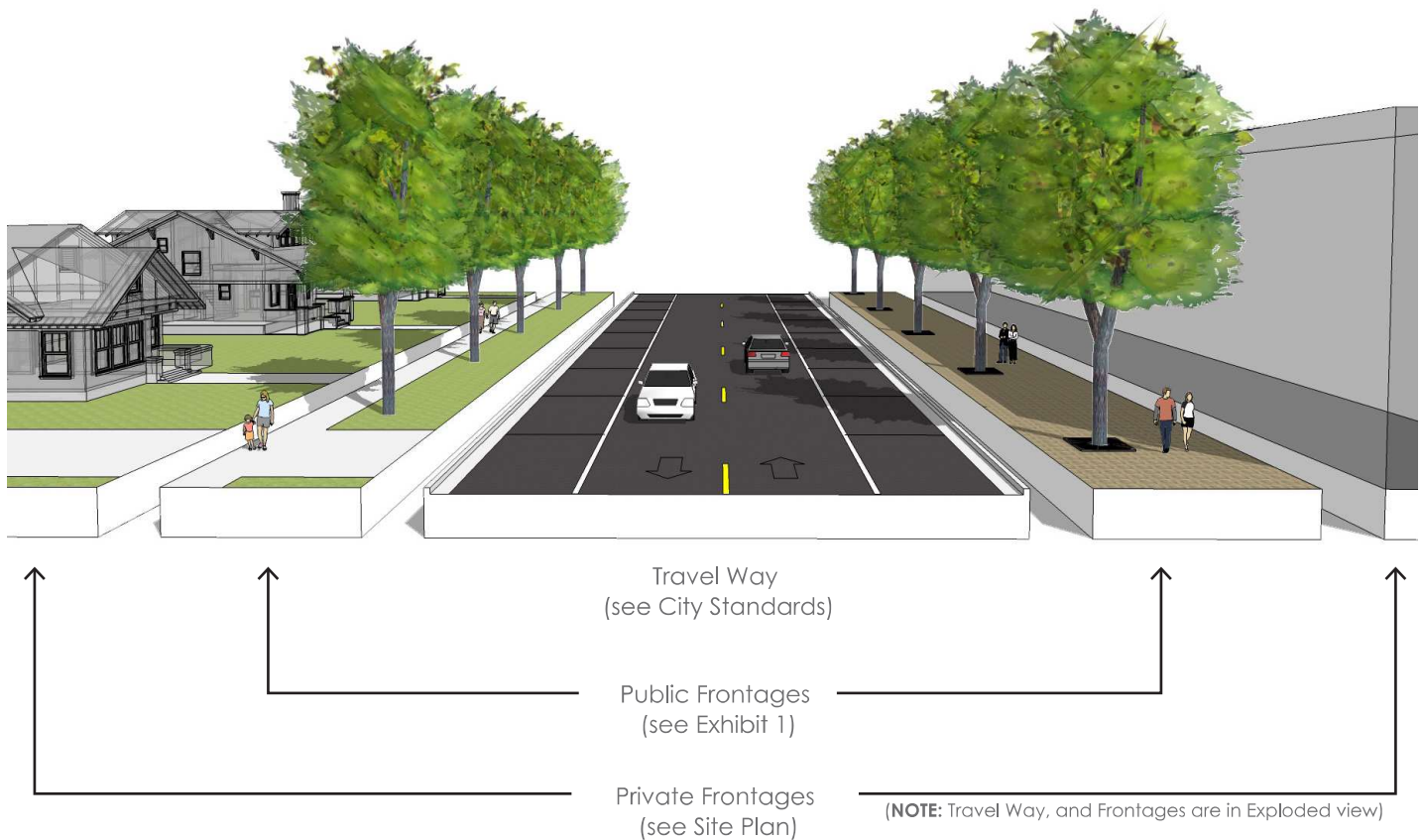
Appendix C - Recommended Street Standards

Street Standards

All streets within the Gordon Avenue and Highway 89 Town Center will comply with City Standards and are measured from back of curb to back of curb. Each street shall include a travel way, a public frontage type (within the right-of-way) on both sides of the travel way, and private frontage types on both sides of a street.

Public frontages shall comply with standards shown in Exhibit 1.

Private frontage types are designed as part of the overall Site Plan based on City approval.



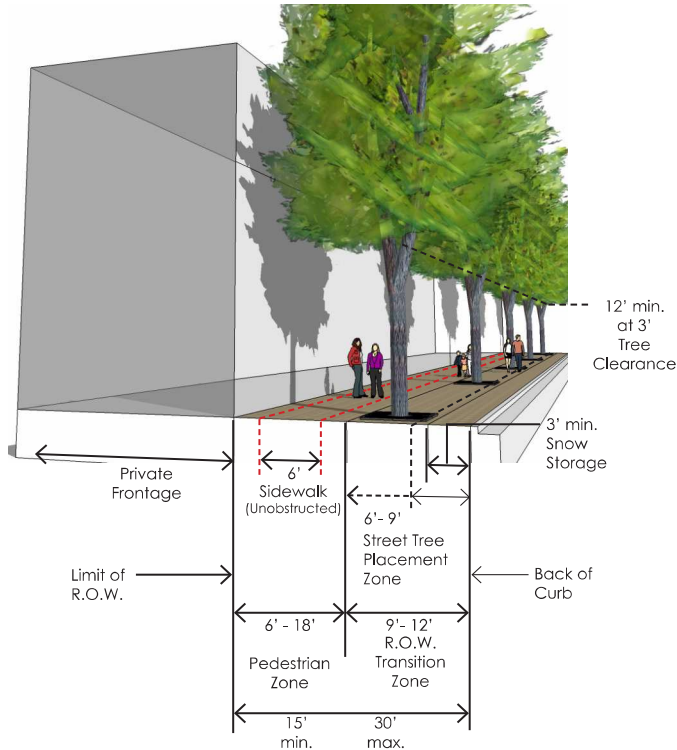
Gordon Avenue and Highway 89

Town Center Master Plan

Exhibit 1 - Public Frontage (PF)*

*Setbacks for non-residential development

Full Range of PF Dimensions



Public Frontage Standards

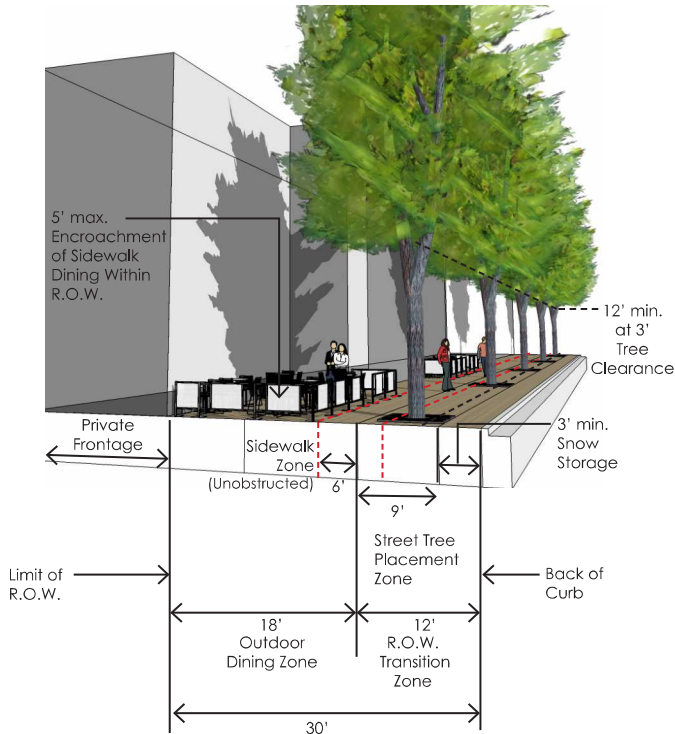
Minimum Width	15 ft.
Permitted Curb Type	Highback
R.O.W*** Transition Zone	9 ft. min.* - 12 ft. max.
R.O.W Transition Zone	Hardscape with Tree Grates (5 ft. min.)
Snow Storage Zone	3 ft. min. (measured from back of curb)
Street Tree Placement**	6 ft. - 9 ft. (measured from back of curb)
Street Tree Spacing	TBD
Pedestrian Zone Width	6 ft. min. - 18 ft. max.
On-Street Dining Sidewalk	Permitted 6 ft. min.*

* Sidewalk minimum must be unobstructed, but may be located within both the parkway transition and pedestrian zones

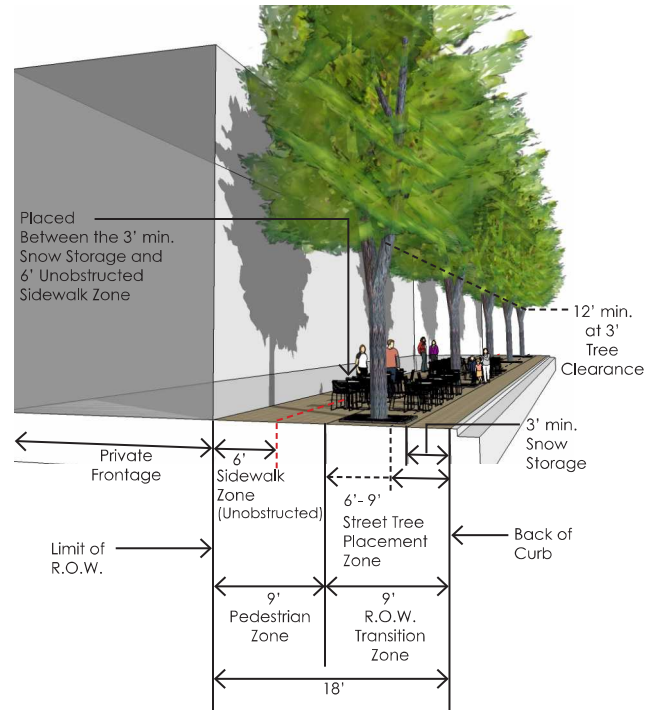
** Refers to trunk placement, not edge of tree grate

***Right of way

PF w/ Fixed Furniture/Dining Option



PF w/ Movable Furniture/Dining Option



Appendix D - Preliminary Concepts

Gordon Avenue and Highway 89

Town Center Master Plan

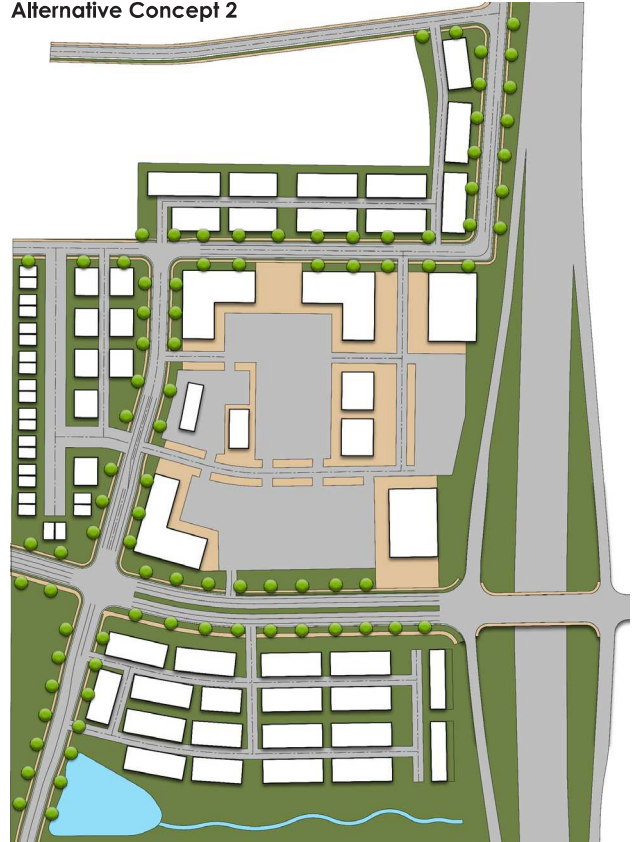
Preliminary Concepts

When crafting plans for any development, decisions in land use, transportation, and economics are highly interconnected. The process to create preliminary concepts is iterative and includes a few rounds of internal tweaking and review. After initial discussions with stakeholders and reviewing the existing conditions of the region, the project team met to discuss and develop three potential scenarios with supportive information on acreage, units, square footage, and transportation network. The purpose of this step in the plan is to get specific feedback on the several alternatives that reflect the community's vision and promote thoughtful, equitable, and accessible distribution of different land uses. This step of the plan serves as a tool that can address issues and concerns specific to the location while creating a unified development that blends with the surrounding community and allows stakeholders to visualize potential possible outcomes. The concepts below reflect public input and previous studies completed:

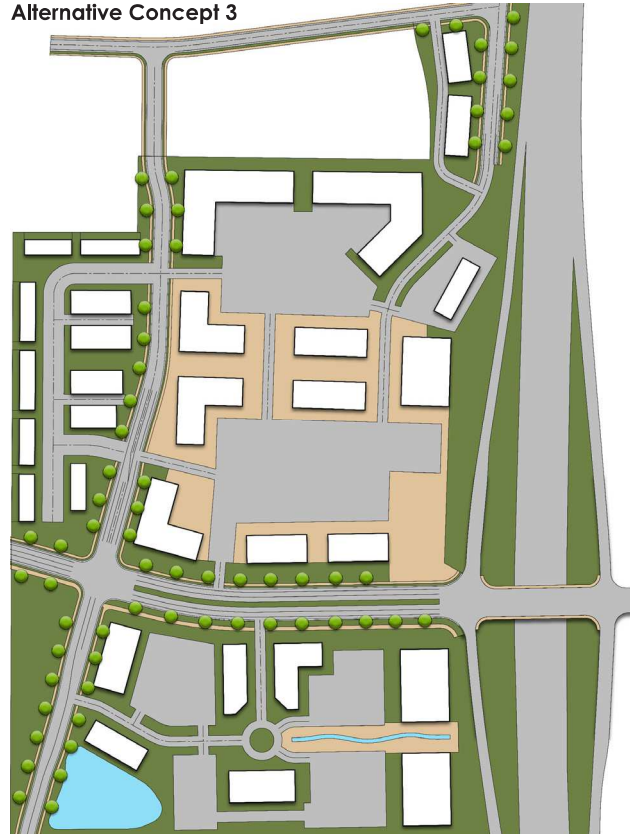
Alternative Concept 1



Alternative Concept 2



Alternative Concept 3



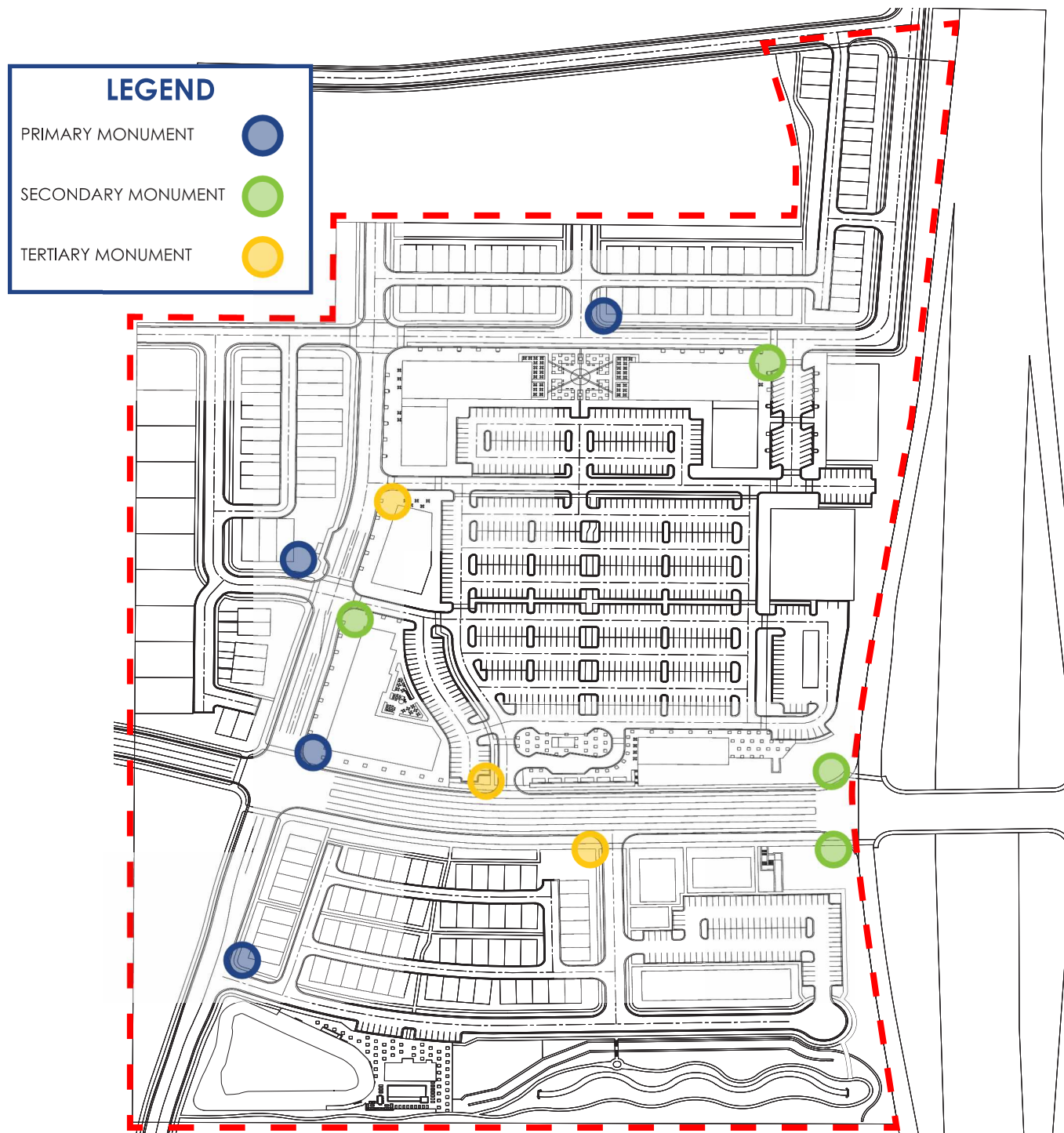
Appendix E - Potential Entry Monumentation Plan

Gordon Avenue and Highway 89

Town Center Master Plan

Potential Entry Monument Plan

This is the proposed monument/signage plan for the town center. The placement of monuments is subject to change as site plans adjust.



Appendix F - Recommended Design Guidelines

Gordon Avenue and Highway 89

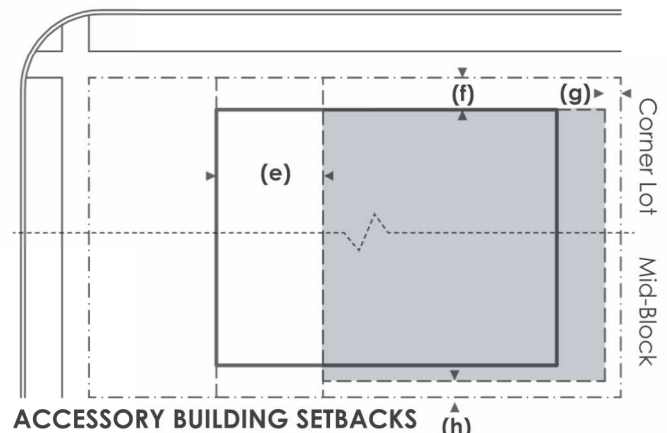
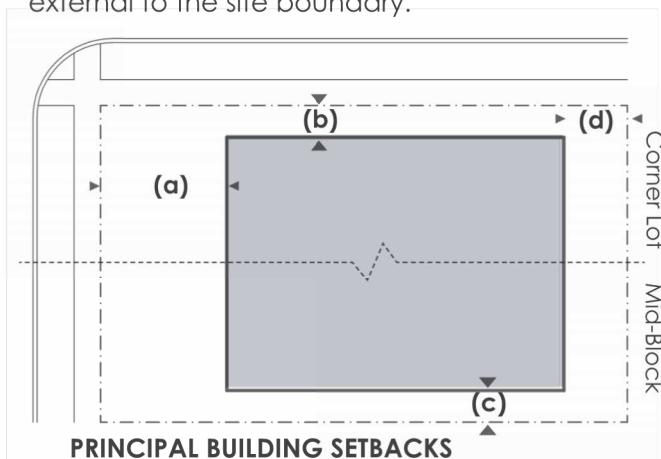
Town Center Master Plan

Project Setback Recommendations

DEVELOPMENT CRITERIA RECOMMENDATIONS

	ATTACHED HOUSING ALLEY-LOAD (AL)	ATTACHED HOUSING FRONT-LOAD (FL)	STACKED HOUSING	AMENITY STRUCTURE
BUILDING CONFIGURATION				
PRINCIPAL BUILDING	3 Stories (35 ft.) max.	2 Stories (30 ft.) max.	3 Stories (35 ft.) max.	2 Stories (30 ft.) max.
ACCESSORY BUILDING	1 Story (12 ft.) max.	1 Story (12 ft.) max.	1 Story (12 ft.) max.	n/a
SETBACKS - PRINCIPAL BUILDING*				
FRONT - PRINCIPAL (a)	12 ft. min.	12 ft. min.	10 ft. min. 30 ft. max.	12 ft. min.
FRONT - SECONDARY (b)	12 ft. min.	12 ft. min.	10 ft. min. 30 ft. max.	12 ft. min.
SIDE (c)	0 ft. min. (12 ft. between buildings)	0 ft. min. (12 ft. between buildings)	0 ft. min. (10 ft. between buildings)	0 ft. min. (12 ft. between buildings)
REAR (d)	N/A	10 ft. min.	10 ft. min.	10 ft. min.
SETBACKS - ACCESSORY BUILDING				
FRONT - PRINCIPAL (e)	12 ft. min.	12 ft. min.	15 ft. min.	n/a
FRONT - SECONDARY (f)	12 ft. min.	12 ft. min.	12 ft. min.	n/a
SIDE (g)	5 ft. min. (6 ft. between buildings)	5 ft. min. (6 ft. between buildings)	0 ft. min.	n/a
REAR (h)	n/a	5 ft. min.	0 ft. min.	n/a
SETBACKS - GARAGE				
DOOR SETBACK	<6 ft or >20 ft.	20 ft.	<6 ft or >20 ft.	<6 ft or >20 ft.

* Buildings over two (2) stories require a 1.5 to 1 ratio of setback to height from existing residential structures external to the site boundary.



* Setbacks are measured to foundation walls

Architectural Design Standards

Community Design Theme

Purity of architectural style is important when seeking to establish high quality developments. The Gordon Avenue and Highway 89 Town Center seeks to maintain its natural agricultural legacy by maintaining an "orchard feel" through the use of craftsman, mountain modern, farmhouse, and prairie architectural styles throughout the community. Architectural details should serve to reinforce the broader community design theme and work together within their broader context.

Materials

Building materials should be consistent with and reinforce

the community architectural style. Quality materials should be used to convey a sense of permanence. Materials should be consistent on the primary and secondary frontages and side adjacent to shared open space.

Colors

Color and texture should reflect a pleasing balance of composition. Architecture should incorporate a diversity of color, but one that fits within the context of the broader community style as well as within the context of each individual street.

Attached - AL



Attached - AL



Attached - FL



Attached- FL



Stacked



Stacked



Gordon Avenue and Highway 89

Town Center Master Plan

Overview

Focusing on architecture is a key piece in creating memorable places, and is one of the most fundamental building blocks of placemaking. The items below outline how this should be approached within the town center area.

ARCHITECTURAL MASSING

The use of a monotonously consistent template or repetitive architecture on a number of residential units in proximity to one another should not be allowed unless approved by the Layton City Planning Commission after a recommendation from the City Planning Staff. As this consideration depends on a degree of aesthetic judgment, the issue must be reviewed carefully – and depends on other factors of the building's quality.

Building heights (as an available maximum) have been recommended for each land use type. The general intent is that the height and bulk of individual buildings should be harmonious with their neighbors. The key characteristic of a building's volume is how clearly it defines the public space of its frontage.

FOUR SIDED ARCHITECTURE

The aesthetic character of a development is crucial in making it successful. Each building has four sides and although the principal (front) elevation often receives the most attention it is imperative that the two sides and rear of the building are not ignored. Side windows create cross breezes and let more natural light in the house. When the sides are treated like part of the overall composition, they elevate the house from a series of walls to a coherent composition. Therefore, the design of all four sides are regulated by facade type in this Code.

ARCHITECTURAL THEME

- The architectural theme for The Gordon Avenue and Highway 89 Town Center is inspired by the historic agricultural tradition in the mountain valley geography. Diversity within the aesthetic structure of the theme is encouraged. Designers are encouraged to find local historic examples to serve as templates for new construction.
- Acceptable styles include craftsman, mountain modern, farmhouse, and prairie.



Craftsman



Mountain Modern



Farmhouse



Prairie

ARCHITECTURAL DESIGN TABLE RECOMMENDATIONS

	CRAFTSMAN	MOUNTAIN MODERN	FARMHOUSE	PRAIRIE
MASSING				
	Side/center gable facing street, dormers, and emphasis on horizontal lines	Simple geometric forms, simple massing, floor height 10 - 14 ft	Front-facing gable without side wings, simple massing with emphasis on vertical elements	Low rectangular forms with emphasis on horizontal elements
ROOFS				
	Low-sloping gable roofs with wide overhangs, shed/pitched dormers, generous eaves	Flat or low-sloping, single pitched roof, often penetrated by chimneys	Gable roofs facing the street, shed/gable end dormers	Low-sloping with hipped secondary roofs of porches and projections
MAIN PITCH	6:12 to 8:12	--	3:12 to 6:12	3:12 to 6:12
OVERHANGS	12 - 30 in	Extending coverage over patios and decks	6 - 12 in	30 in +
PORCH/ENTRY				
COLUMNS	Tapered or double columns with header and base	Exposed wood, stone, and metal siding elements	Square or round 6 in min	Wide with decorative planters
SIZE - FRONT FACADE	Full/Partial	--	Full	Full/Half
WIDTH	10 ft min	10 ft min	10 ft min	10 ft min
DEPTH	7 ft min	7 ft min	7 ft min	7 ft min

Gordon Avenue and Highway 89

Town Center Master Plan

Craftsman Style

ARCHITECTURAL STYLE

- i. The Craftsman style is characterized by simplicity, the expression of certain structural members, and attention to wood joinery, especially at porches. Craftsman homes feature moderate-pitched gable roofs with wide overhangs and large porches with substantial columns and bases.
- ii. The principal features of the Craftsman style are low to moderate-pitched gable roofs with wide overhangs, exposed rafters at porches and, wherever feasible, generous porches with substantial columns and bases. Dormers are typical on 1½-story designs. Symmetry is optional and depends on the orientation of the principal roof.
- iii. Ornamentation is restrained. Details that are characteristic of the style include exposed rafter tails, tapered columns and trim elements, and diagonal knee braces at gable ends. Wall materials may include stone, brick, limited stucco (not to exceed 20% of facade), shingles, and siding (no vinyl).
- iv. The example on the previous page is one interpretation of the Craftsman style. There are many possible successful interpretations. All elements shown here, and described in this style, should not be required on every building. Elements not listed are prohibited.



Craftsman Style (cont.)

CRAFTSMAN

Massing

- A side gable, center gable facing the street, or cross gable with dormers is typical for the primary roof form.
- One-story and 1½-story massing compositions are recommended, although 2-story compositions can also be acceptable.
- Dormers are typical in 1½-story designs.
- Emphasis should be on horizontal rather than vertical lines.

Roofs

- Low-sloping gable roofs with wide overhangs are typical.
- Shed or pitched dormers are common.
- Generously sized eaves with exposed decorative rafters are characteristic of the style, but not required.
- Main roof pitches: 6:12 to 8:12.
- Secondary hip or shed roof pitches: 3:12 to 6:12.
- Roof overhangs: 12 - 30 inches at rakes and eaves.

Windows and Doors

- Individual windows are typically square or vertically oriented.
- Windows are often mulled together in pairs or threes.
- Double-hung windows with divided lites in upper sashes only, usually in a three-over-one configuration, are typical.
- Limited use of small accent windows and angled bays is encouraged.
- A single, rectilinear door is typical.
- Large lites in doors are common and are often divided to match the windows.
- Wide trim (5 to 6-inch) with head trim extending past the jamb is typical for doors and windows. Tapered side trims are typical.

Porch/Entry

- Porches facing the street are common.
- Porch columns typically sit on wider bases or low walls.
- Tapered or double-columns with header and base details are common.

Details

- Expression of structural members and attention to wood joinery is characteristic of the style.
- Beams, knee braces, and brackets are often found at gable ends.
- Extended lintels over door and porch openings are common.
- Tapered elements, including trim work and columns, are common.

Minimum Required Elements

- Front porch that addresses the street, either full or partial-width (10' min. width, 7' min. depth).
- Square columns (tapered or straight).
- Stone or brick plinths.
- Structural or decorative beams or braces under gables.
- Decorative trim (5 to 6-inch wide) around windows with emphasis on top and bottom (no stucco).
- Stone, brick, or wood shingles on all facades (20% min.).
- Lap, wood, or board and batten siding (no vinyl) on all four sides of the structure.
- Main roof pitches 6/12 to 8/12.
- Wide eave overhangs (18" min.).
- Decorative garage door.

Gordon Avenue and Highway 89

Town Center Master Plan

Craftsman Style Examples

Duplex



Townhome



Duplex



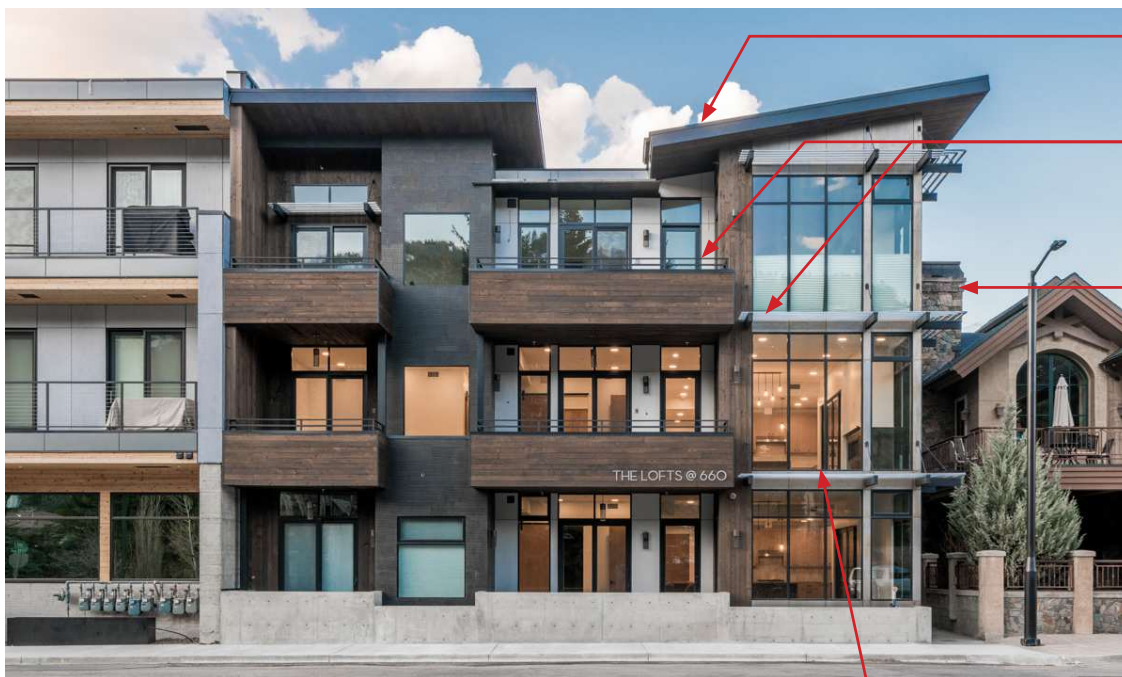
Mansionhome - Triplex



Mountain Modern Style

ARCHITECTURAL STYLE

- i. The Mountain Modern style may be characterized by a low-pitched or flat roof that is capable of withstanding heavy snow loads. The main roof pitch often extends over deck and patio areas to provide additional climatic protection over adjacent decks or patios. Roof planes over the entry patio or courtyard are typical of the front facade. Facade compositions should feature dominant, expanses of glazing with wide roof overhangs providing protection from solar exposure during the warmer months of the year, but capturing the solar exposure during the winter months. Simplified building forms should accentuate horizontal elements with vertical stone or concrete elements penetrating the roof plane.
- ii. Detailing should be simplified. Ornamentation should be employed with restraint at porches, entries and decks. Appropriate wall materials may include concrete, stone or natural wood siding.
- iii. Lighting should be wall sconces with hidden light sources or eave mounted down lighting.



Mountain Modern

Low-sloping, single-pitched roof

Horizontal Building Elements

Penetrating Stone Chimney

Untrimmed Windows and Doors

Gordon Avenue and Highway 89

Town Center Master Plan

Mountain Modern Style (cont.)

MOUNTAIN MODERN

Massing

- Simple geometric forms.
- Overall massing should be simple and emphasize horizontal building elements.
- Low-sloping, single-pitched roofs are common.
- One and two-stories are typical, with a main level floor-to-ceiling height of 10 to 14 feet.

Roofs

- Flat or low-sloping, single-pitched roofs dominate.
- The main roof is often penetrated by stone or concrete chimneys, often represented as oversized massing elements.
- Roof overhangs extending the roof coverage to patios and decks are typical.

Windows and Doors

- Large expanses of floor to ceiling glass are typical.
- Sliding doors that extend the interior space outward onto decks and patios are typical.
- Untrimmed windows and doors that incorporate the glazed element into the exterior wall plane as an extension of the dominant plane are typical.

Porch/Entry

- Street-facing, or side turned one and two-story entry decks are common. Walled entry courtyards are common.
- Porch roofs are typically flat extensions of the roof plane.
- Entries often utilize detailed expressions of exposed wood, stone and metal siding elements.

Detail Elements

- Detailing is simplified and ornamentation is restrained.
- Exposed structural elements are typical.
- Hidden gutters that are integrated into the roof structure are common. Downspouts are often included as "rain-chains" that funnel the runoff into a ground level pond or stream element.
- Detailing is often based on rustic or craftsman elements typical to the local area.

Minimum Required Elements

- Front porch that addresses the street, either full or partial-width (10' min. width, 7' min. depth) that incorporates timber columns with stone plinths and timber trusses that reflect the internal structure of the building.
- Timber beams or braces under gables.
- Stone on public facing facades (20% min).
- Lap siding or batten and board siding (no vinyl) on all four sides of the structure.
- Wide eave overhangs (24" min.).
- Decorative garage door.

Mountain Modern Style Examples

Townhome



Gordon Avenue and Highway 89

Town Center Master Plan

Mountain Modern Style Examples



Farmhouse Style

ARCHITECTURAL STYLE

- i. The Farmhouse style is characterized by a gable roof facing the street. The main gable may be combined with wings on one or two sides or emerge from a larger hip-roofed rectangular volume. A one-story porch should be provided and integrated into the front facade. Gable, hip, shed, or special dormers are employed to provide additional floor area, daylight, and architectural interest. Facade compositions should feature symmetrically placed, vertically proportioned, double or single-hung windows. Symmetry in the overall composition is optional.
- ii. Detailing should be simplified. Ornamentation should be employed with restraint at porches, gable ends, and special features, such as bays. Appropriate wall materials may include horizontal lap siding and board and batten siding.
- iii. The example on the previous page is one interpretation of the Farmhouse style. There are many possible successful interpretations. All elements shown here, and described in this style, should not be required on every building. Elements not listed are prohibited.



Farmhouse

Gordon Avenue and Highway 89

Town Center Master Plan

Farmhouse Style (cont.)

FARMHOUSE

Massing

- A front-facing gable without side wings is typical.
- Overall massing should be simple and emphasize vertical building elements.
- Projecting bays and low-sloping shed roofs are common.
- 1½ to 2-stories are typical, with a main level floor-to-ceiling height of 8 to 10 feet.

Roofs

- Gable roofs facing the street are typical.
- Use of shed or gable-end dormers is encouraged.
- The main gable is often intersected by other roofs.
- Main roof pitches: 6:12 to 12:12.
- Secondary hip or shed roof pitches: 3:12 to 6:12.
- Roof overhangs: 6 to 12 inches.

Windows and Doors

- Vertically proportioned double and single-hung windows are typical.
- Individual or paired window treatments are common.
- Square and angled bay window treatments are common.
- Wide (4 to 6-inch) exterior trim and cap moldings on windows and doors are typical.
- Limited use of multi-pane sashes with divided lites is encouraged and may occur in both sashes in the following configurations: one-over-one, two-over-one, two-over-two, four-square-grid-over-one, and four-square-grid-over-four-square-grid.
- Lites in doors are common and often express ornamentation.

Porch/Entry

- Street-facing, one-story porches are common. Wraparound porches are encouraged at corner lots.
- Porch roofs are typically forward-facing shed or hip.
- Porches may have exposed wood and metal elements.
- Square columns (at least 6x6) or round columns (at least 6 inches) are typical.
- Railings may be turned or square balusters or steel.

Detail Elements

- Detailing is simplified and ornamentation is restrained.
- Exposed structural elements on porches are typical.
- Any exposed concrete foundation must be clad with stone or brick.
- Ogee or half-round gutters are common.
- Board and batten wainscoting (in courser spacing) is recommended.
- Square or more detailed moldings along rakes are common.

Minimum Required Elements

- Full width covered front porch that addresses the street with support columns that have a base, shaft, and capitol.
- Vertically-oriented windows.
- Decorative trim (5 to 6-inch wide) around windows with emphasis on top and bottom (no stucco).
- Decorative garage door.
- Lap siding or board and batten siding (no vinyl) on all four sides of the structure.

Farmhouse Style Examples



Gordon Avenue and Highway 89

Town Center Master Plan

Farmhouse Style Examples



Farmhouse Style Examples



Gordon Avenue and Highway 89

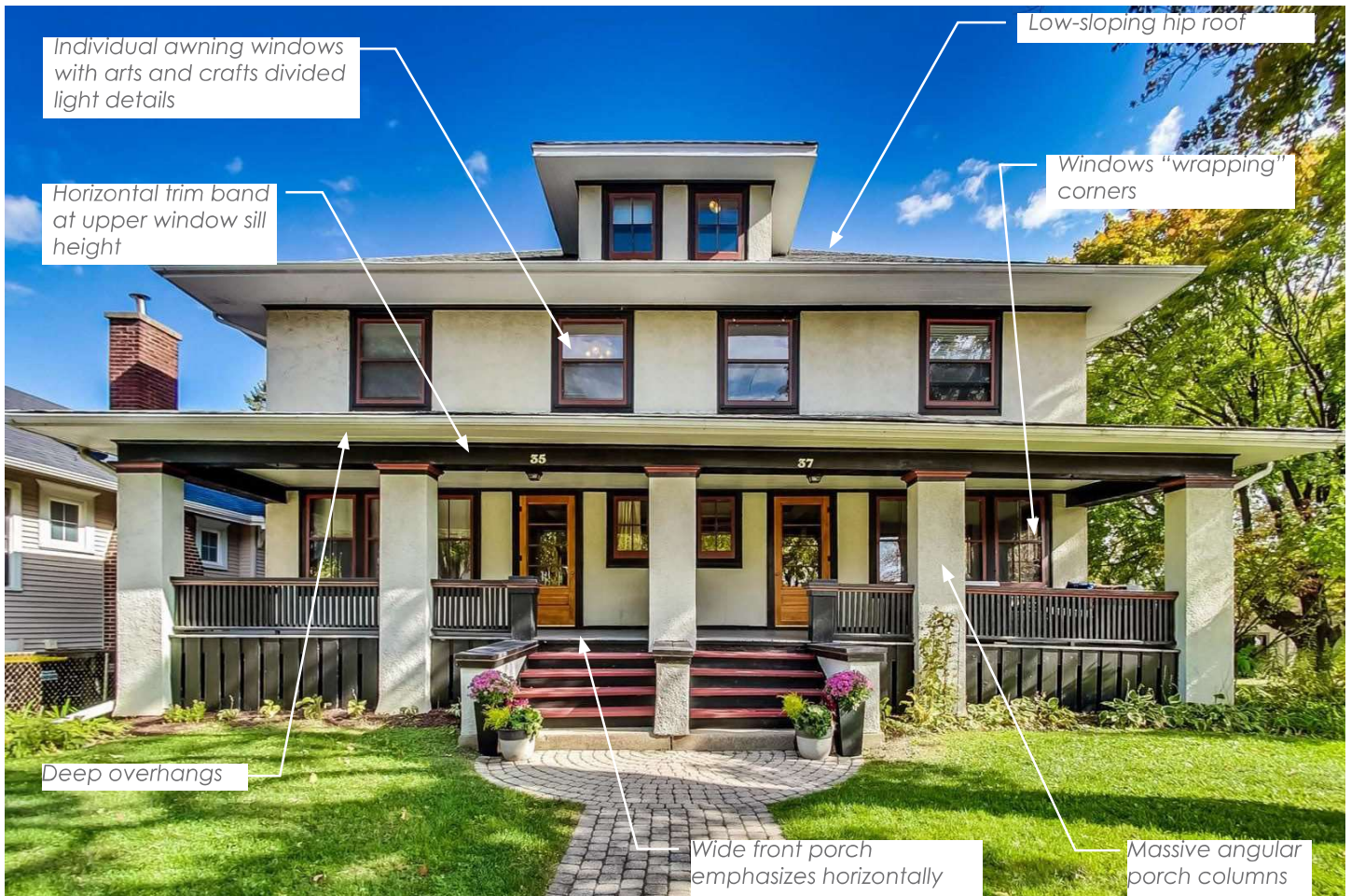
Town Center Master Plan

Prairie Style

ARCHITECTURAL STYLE

The style spread throughout the country due to the use of pattern books and various articles in popular magazines. Various interpretations of the Prairie style have been developed over the years and the style has established its place in urban neighborhoods among other popular American residential architectural styles.

The example below is one interpretation of the Legacy Prairie style. There are many possible successful interpretations. All elements shown here, and described in this style, are not required on every building. Elements not listed are prohibited.



PRAIRIE

Prairie Style (cont.)

PRAIRIE

Massing

- Low, rectangular forms with an emphasis on horizontally are typical.
- Horizontal bands defined by changes in color and/or material emphasize the base, middle and top of the building mass.
- Horizontal bands (usually at sill and header heights) are continuous across building elements, such as secondary roofs and balconies.
- The base band often extends to the bottom of the second-story sill.
- Legacy Prairie should feel lighter than traditional Midwestern Prairie; the use of masonry should be less prominent in Legacy Prairie homes.

Roof

- Low-sloping hip roofs are typical.
- Secondary roofs of porches and projections are also typically hipped.
- Clerestories are common; dormers are rarely used.
- Roofs pitches: 3:12 to 6:12.
- Overhangs: at least 30 inches; larger overhangs are typical.

Porch/Entry

- Porches are often an integral part of the massing, rather than additive components; they sometimes have balconies or shed roofs above.
- Wide porch columns with decorative planters are common.
- Masonry walls are suitable in place of porch railing.

Windows and Doors

- Windows are typically arranged in compositions, rather than individually placed.
- Horizontal bands of windows are common, as well as windows wrapping corners.
- Arts and Crafts windows with divided lites are common.
- Window and door trim is often integrated into horizontal trim bands wrapping building.
- Half-lite and full-lite doors are common and often decorative. Compositions with side lites are common as well.

Details

- Building elements with rectilinear emphasis are encouraged; diagonals or curves are discouraged.
- Large, continuous trim under the eave of the primary roof is typical; trim is often integrated into the headers of second-story windows.
- Horizontal elements like sills, porch roof trim, and balconies, are often aligned to emphasize horizontally.
- Angular brick detailing with two to three colors of brick is often expressed on piers and at the top of the base band.

Minimum Required Elements

- Half to full width covered front porch that addresses the street.
- Vertically-oriented windows that wrap the front and side facades.
- Decorative trim (5 to 6-inch wide) around windows with emphasis on top and bottom (no stucco).
- Low sloping hip roof with 18"+ overhangs.
- Lap siding or board and batten siding (no vinyl) on all four sides of the structure.

Gordon Avenue and Highway 89

Town Center Master Plan

Prairie Style Examples



Images depict interpretive examples of the architectural style rather than specific execution

Prairie Style Examples



Images depict interpretive examples of the architectural style rather than specific execution

Gordon Avenue and Highway 89

Town Center Master Plan

Residential Material Recommendations

MATERIALS

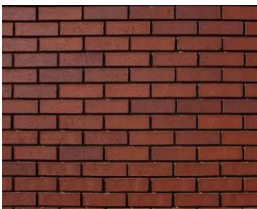
The following are the acceptable predominant materials for residential development



Stone



Lap/Wood Siding
(No Vinyl)



Brick



Shingle Siding
(No Vinyl)



Timber



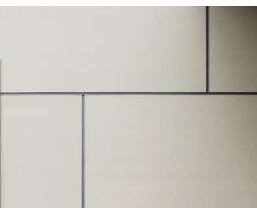
Board and Batten
Siding (No Vinyl)



Metal
(Recommended as
an accent material)



Stucco
(Accent only) Stucco
shall not exceed 20%
of any facade



Fiber Cement Siding



Concrete
(Accent only)
Concrete shall not
exceed 15% of any
facade

Residential Recommended Guidelines

RESIDENTIAL COMPOSITION GUIDELINES

Application of materials on facades should reinforce the logical expression of the materials. In most cases massing of materials should run horizontally. Vertical strips defy structural logic. Horizontal bands make houses feel more structurally sound.



Avoid vertical strips



Use horizontal bands

Gordon Avenue and Highway 89

Town Center Master Plan

Minimize the prominence of front-loaded garages.



Use

In-line or recessed garage door placement to minimize attention being paid to the car. The main entry should be the most prominent feature on the front facade.



Avoid

Making the garage door the most prominent feature of the home with the main entry as the hidden afterthought. Garage forward architecture should be avoided.

Commercial Recommended Guidelines

COMMERCIAL/MIXED-USE ARCHITECTURAL COMPOSITION GUIDELINES

- a. Building Massing
 - i. Two and three-story building masses next to the street create a streetwall that frames the streetscape, improving the pedestrian experience.
 - ii. Building masses are greater at areas with more pedestrians, such as street corners.
 - iii. Building masses frequently defined by a discernible base, shaft and capital.
 - iv. Load-bearing materials should touch the ground.

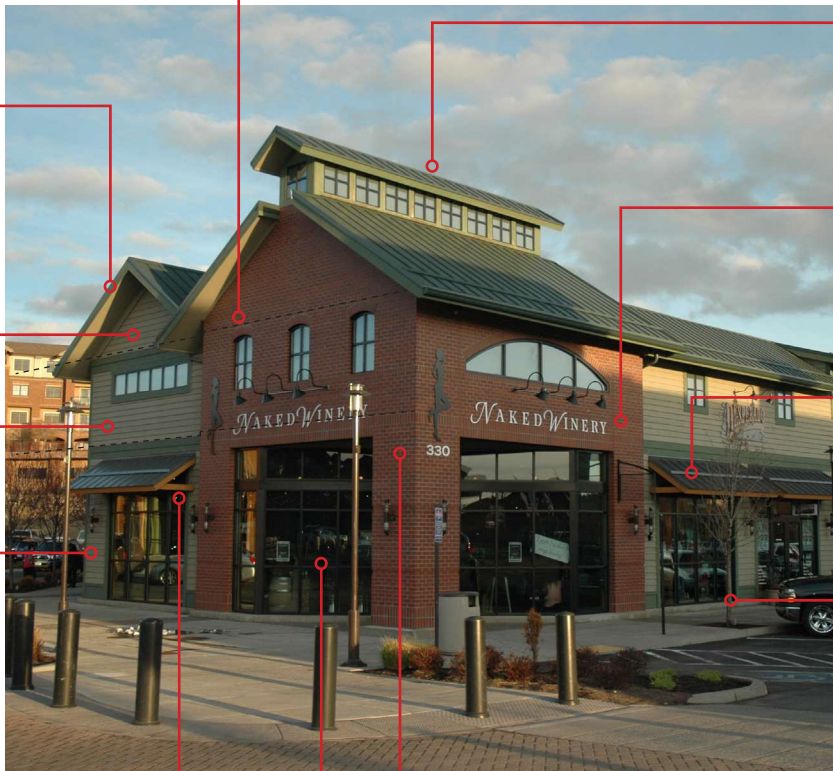
Upper Story Windows.
Vertically oriented
upper-story windows to
allow light into habit-
able spaces

Wide Eaves and Gable
Ends. Wide eaves on
pitched roofs with
gable ends provide
architectural capital
element

Capital

Shaft

Base



Roof Accents.
Specialized roof
accents including
dormers, cupolas,
towers, etc.

Sign Band. Decorative
sign band that identifies
the business and
horizontally divides base
from shaft.

Structural Bays.
Structural bays
composed by individual
storefront windows and
awnings

Street trees. Formal
rows of columnar trees
project an urban image

Building Massing.
Prominent corner accentuated by load bearing
material and slight shift toward foreground

Storefront Windows.
Storefront windows provide ample interior lighting
while displaying merchandise to pedestrians

Building Materials.
A combination of compatible building materials
including lap siding, brick, timber, and metal panels

Gordon Avenue and Highway 89

Town Center Master Plan

Commercial Recommended Guidelines

b. Roof Form

- i. Predominant roof shapes should be slightly pitched with gable ends where appropriate, with limited use of flat roof elements to provide breaks in facade type and/or add visual interest.
- ii. Roof pitches should be typically low (4:12-8:12).
- iii. Moderate overhangs up to 3 feet should be supported by substantial dimensional timber corbels that define the top of the building.



Roof Accents. Specialized roof accents including dormers, cupolas, towers, etc.

On-Street Dining. Take advantage of alcoves and courtyards for on-street dining opportunities.

Commercial Recommended Guidelines

- c. Arcades and Storefront Structural Bays
 - i. Storefront structural bays are commonly orchestrated to appear as phased construction over a long period of time.
 - ii. Substantial structural bays should be composed of vertically oriented piers and horizontal spandrels with intervening storefront windows.
 - iii. Ground-floor storefronts should be very different from upper-story facades with windows scaled and oriented for pedestrian interest.
 - iv. Arcades are encouraged as semi-public spaces between the street and storefront, sheltering pedestrians from the elements.
 - v. Multiple storefronts with separate storefront windows can be massed together within a single facade expanse with the same architectural style. The maximum length of a single facade expanse is 100 feet.



Gordon Avenue and Highway 89

Town Center Master Plan

Commercial Materials Recommendations

NON-RESIDENTIAL ARCHITECTURAL MATERIALS

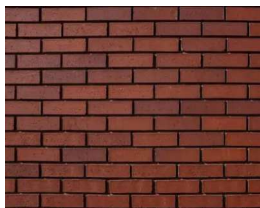
d. The following are the acceptable predominant materials for non-residential development



Stone



Lap/Wood Siding
(No Vinyl)



Brick



Shingle Siding
(No Vinyl)



Timber



Board and Batten
Siding (No Vinyl)



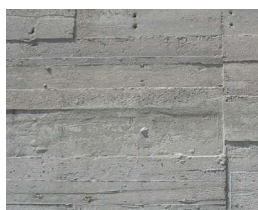
Metal
(Recommended as
an accent material)



Stucco
(Accent only) Stucco
shall not exceed 20%
of any facade

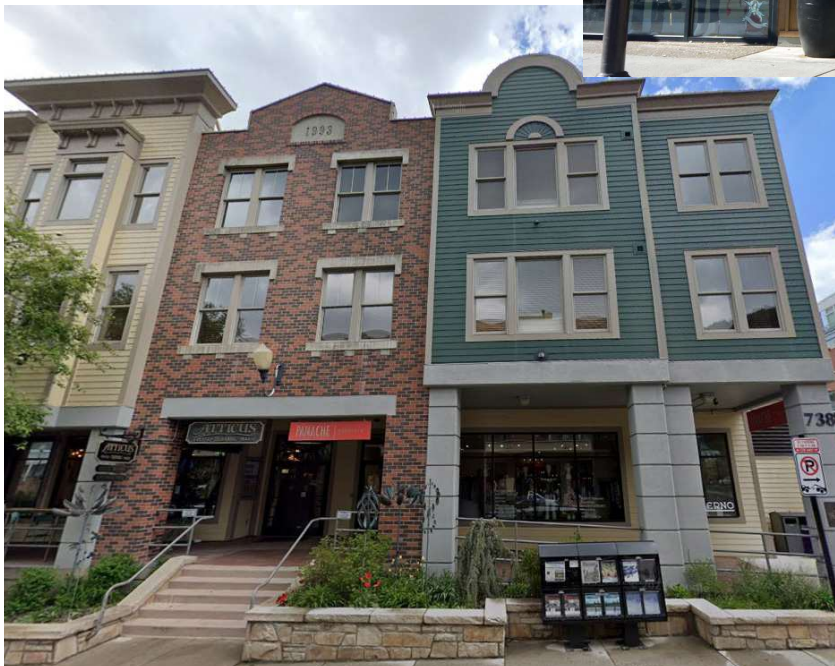


Fiber Cement Siding



Concrete
Concrete shall not
exceed 15% of any
facade.
(Recommended as
an accent material)

COMMERCIAL EXAMPLES



Gordon Avenue and Highway 89

Town Center Master Plan

COMMERCIAL EXAMPLES



COMMERCIAL EXAMPLES



Gordon Avenue and Highway 89

Town Center Master Plan

COMMERCIAL EXAMPLES



Appendix G - Market Study

Incorporated by Reference (Available for review in the Community and Economic Development Department)

Appendix H - Traffic Impact Study

Incorporated by Reference (Available for review in the Community and Economic Development Department)

Appendix I - Transportation Reinvestment Zone

Incorporated by Reference (Available for review in the Community and Economic Development Department)

ADDITIONAL

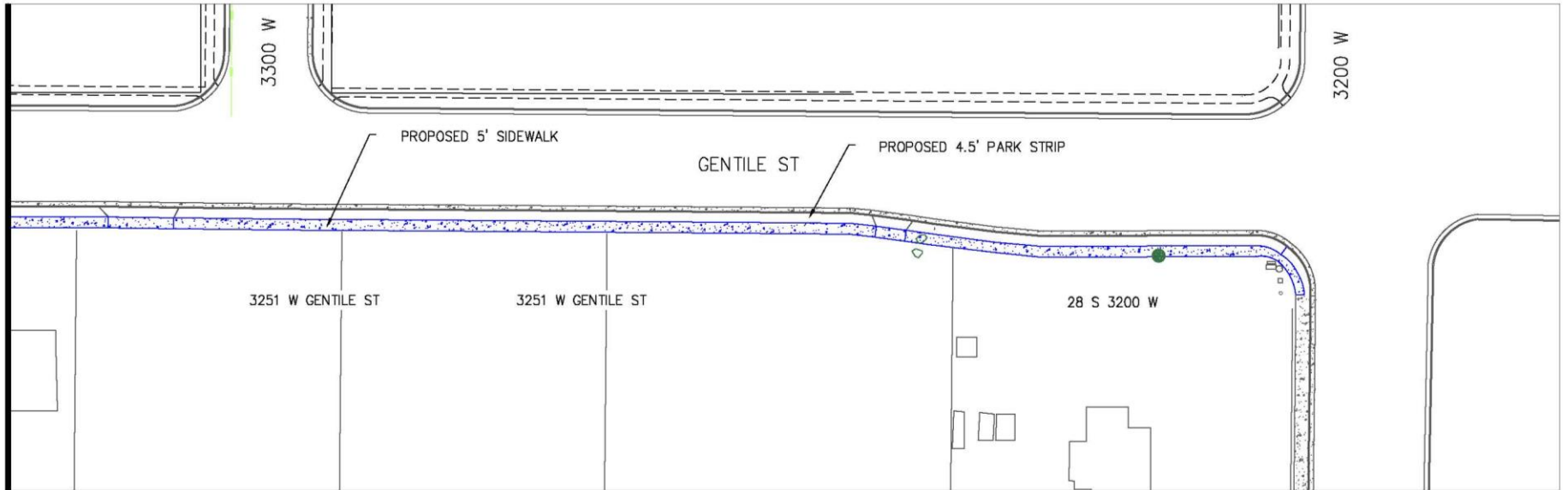
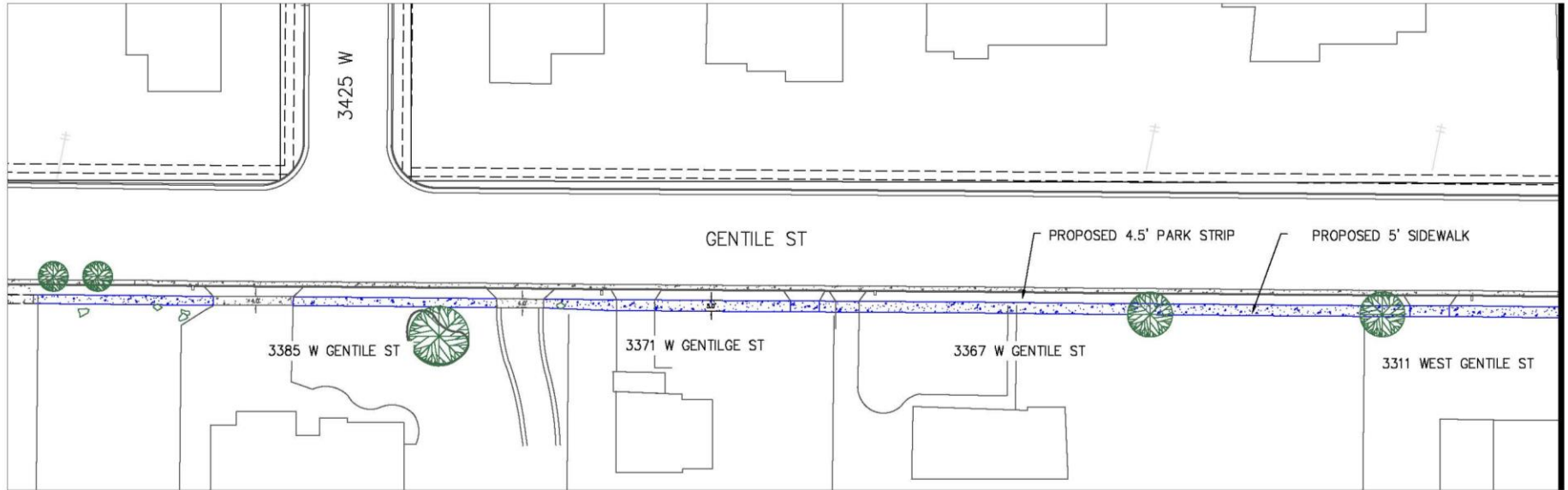
PACKET

ATTACHMENTS

March 21, 2024

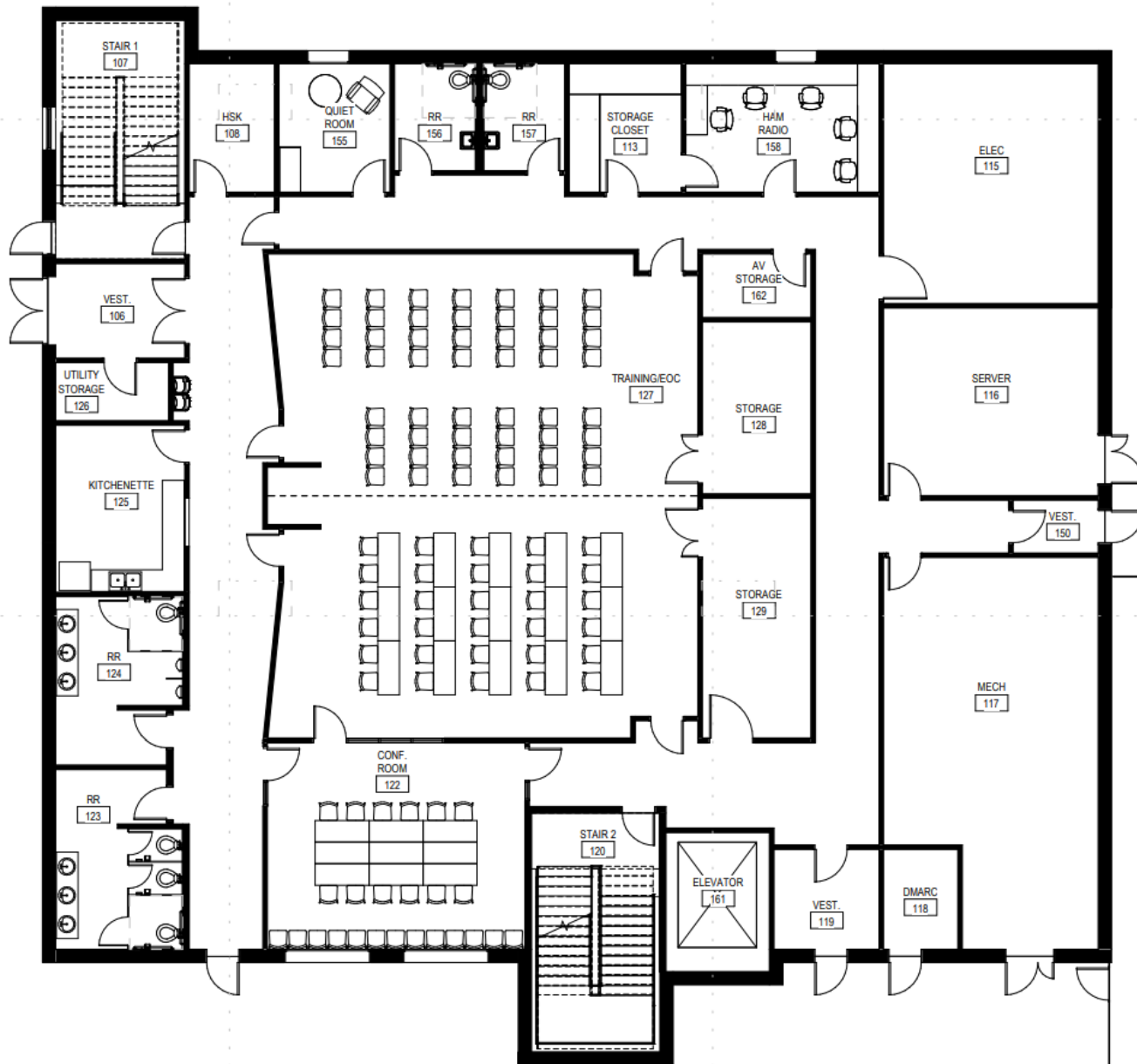
City Council Meeting

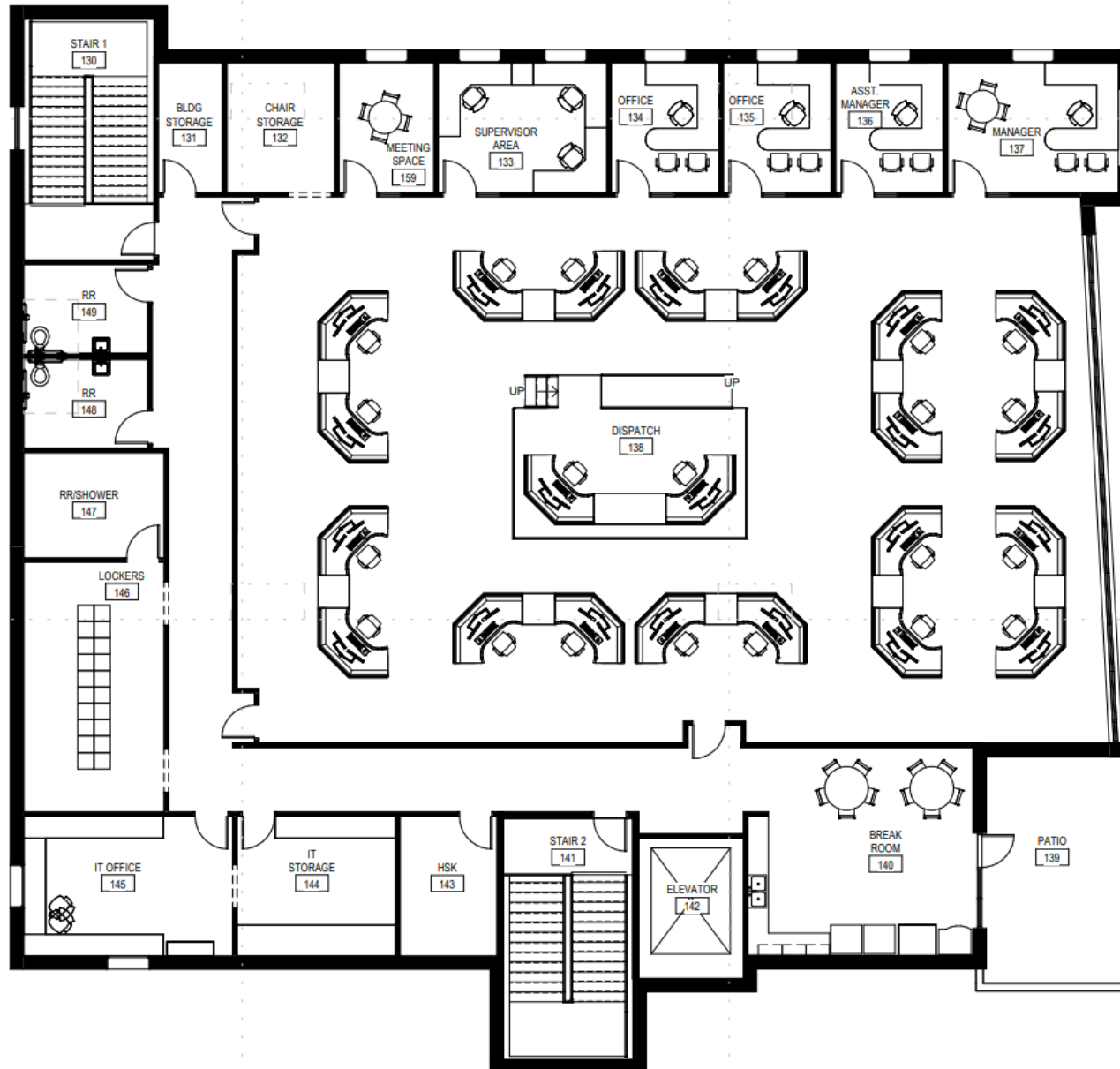
5A. Federal Aid Supplement Agreement Resolution 24-11





5B. Construction Agreement for EOC Resolution 24-10







5C. Annexation Request Resolution 24-02 & 24-06



Annexation Acceptance & Certification

Approx. 3925 N
and 2945 N
Fairfield Rd

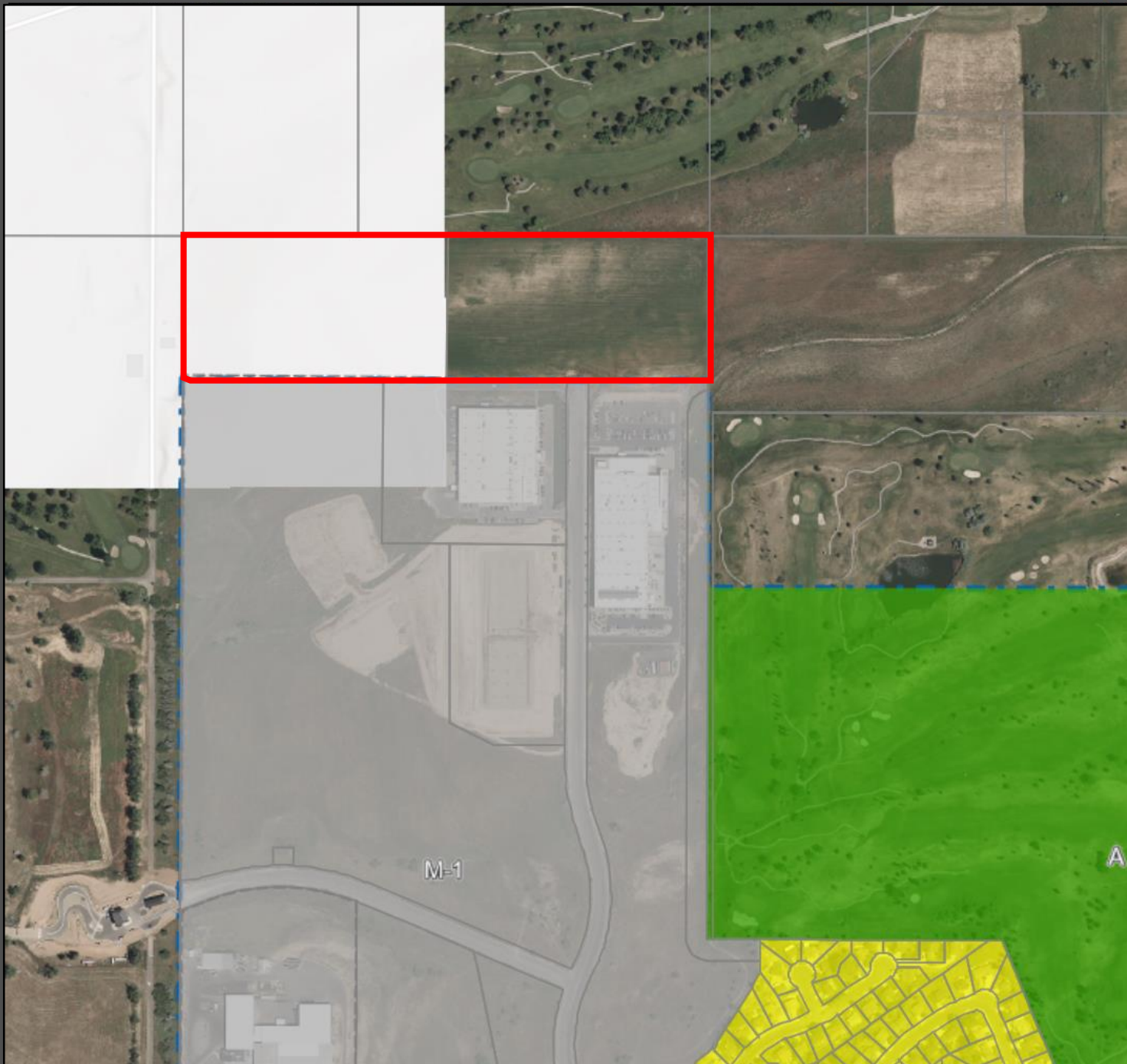
Legend

-  Interstate Highways
-  City Boundary
-  Highways
-  Lakes
-  Streams

 - Project Area



Map 3









5D. Final Plat Adams Oak Hills Subdivision

ADAMS OAK HILLS
SUBDIVISION

2201 EAST
350 NORTH

FINAL PLAT

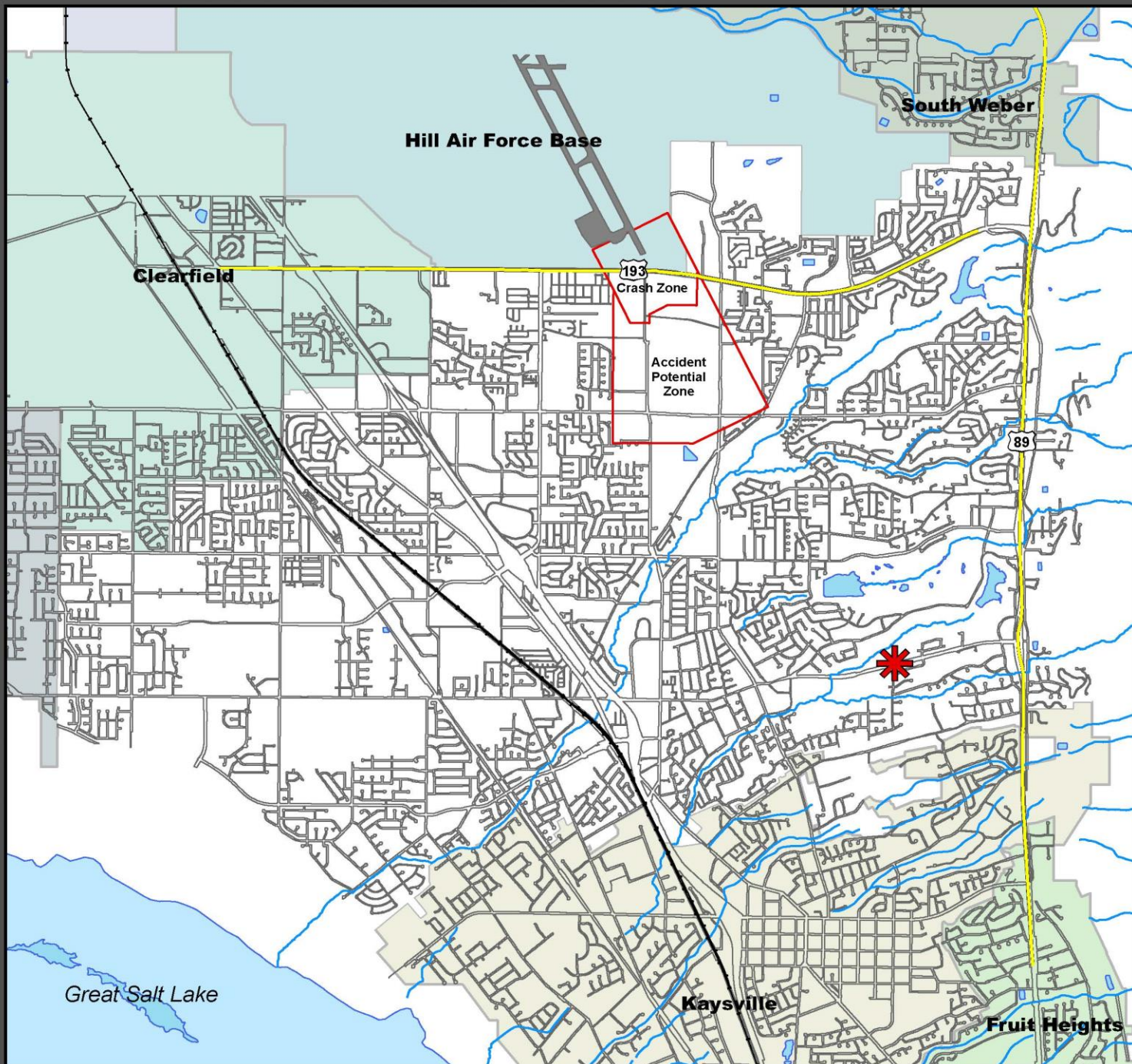
Legend

-  City Boundary
-  Rail Lines
-  APZ
-  Interstate 15
-  Lakes
-  Streams

 - Project Site



Map 1





ADAMS OAK HILLS SUBDIVISION

2201 EAST
350 NORTH

FINAL PLAT

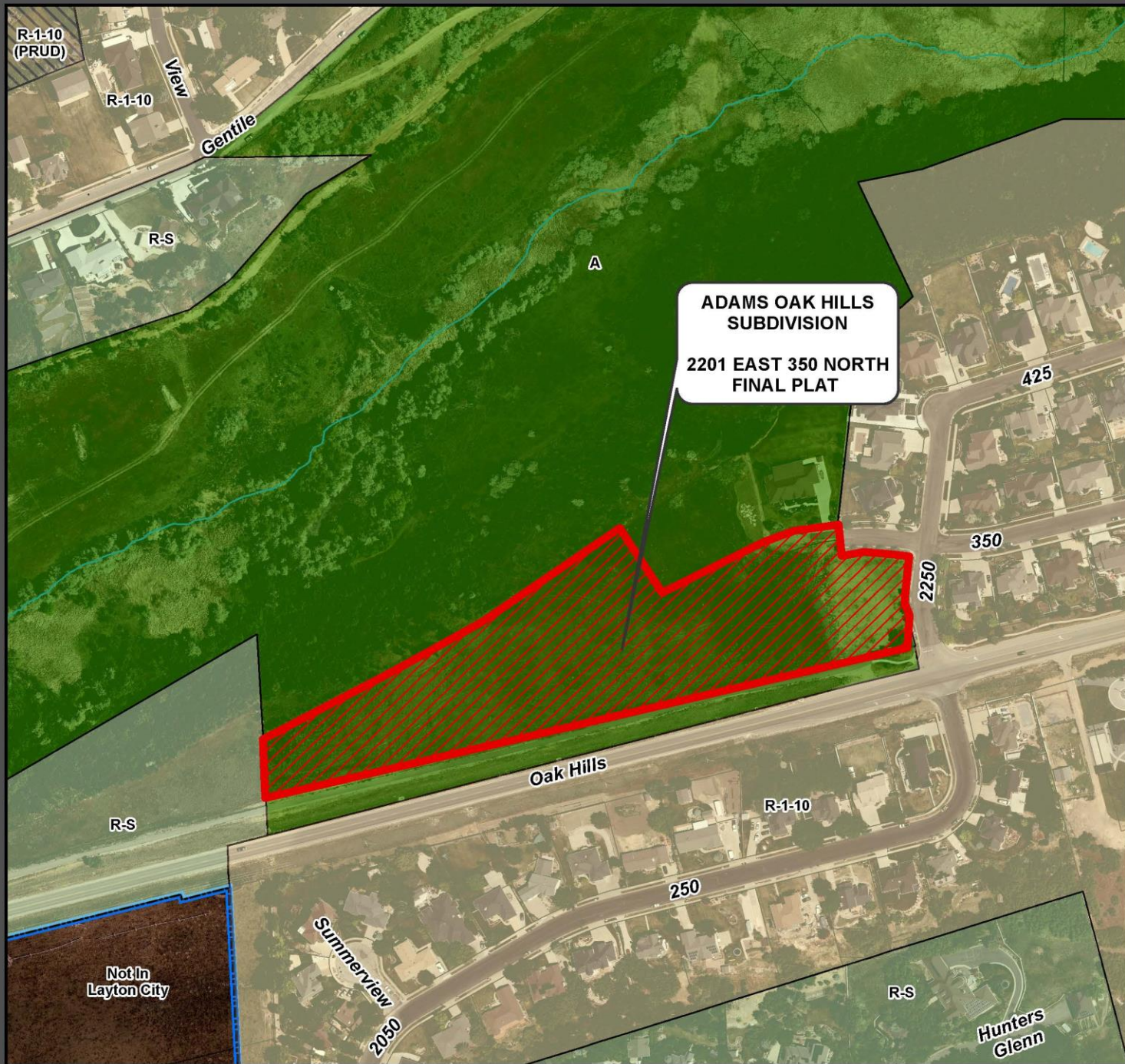
Legend

- Interstate Highways
- City Boundary
- Highways
- Lakes
- Streams

- Project Area



Map 2



ADAMS OAK HILLS
SUBDIVISION

2201 EAST
350 NORTH

FINAL PLAT

Legend

- Interstate Highways
- City Boundary
- Highways
- Lakes
- Streams

 - Project Area



Map 3





6A. Gordon Avenue & Highway 89 Town Center Master Plan Ordinance 24-06

Corner of Gordon Ave. &
2625 E Facing Northwest

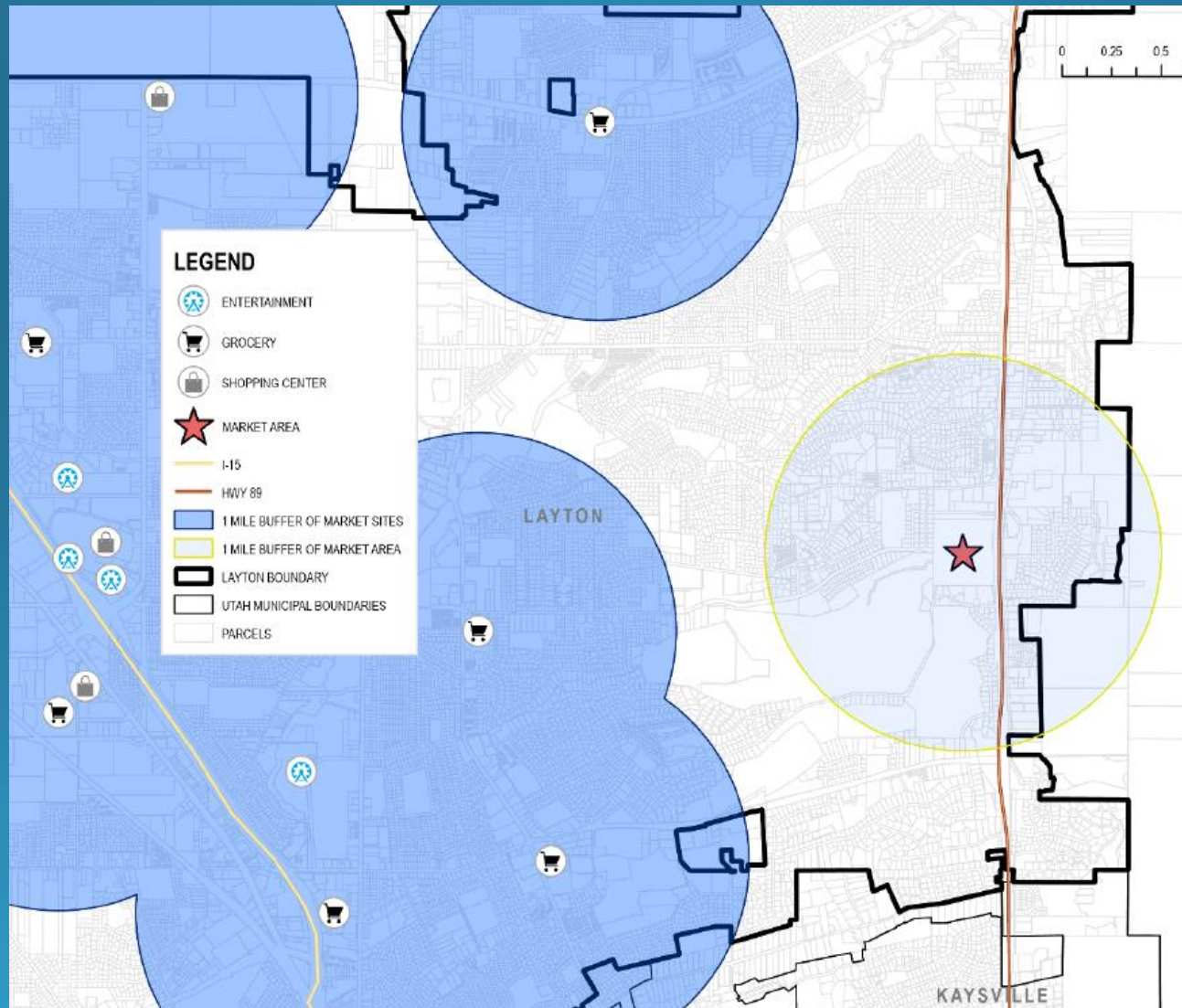


Artistic renderings of how the Town Center may look - These images do not depict final designs.



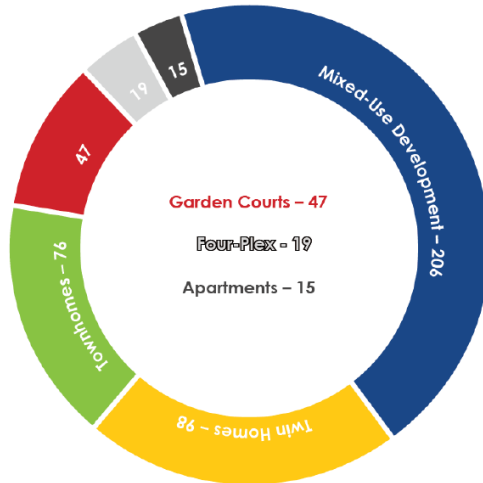
Community & Economic Development Department

Market Study: Neighborhood Scale Retail

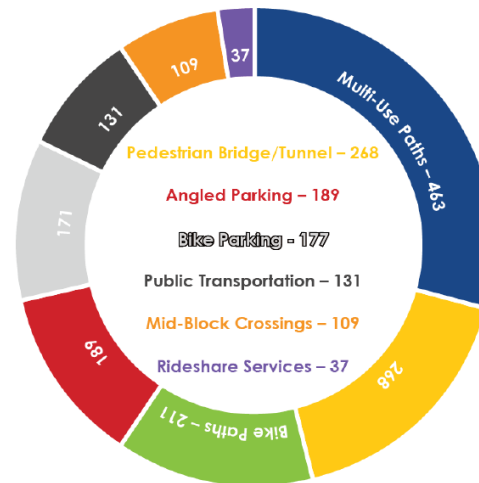


In Person & Online Survey Results

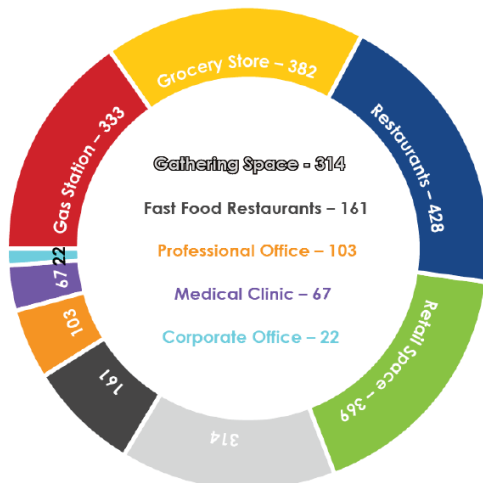
Choose the residential land uses you would MOST like to see within the Town Center



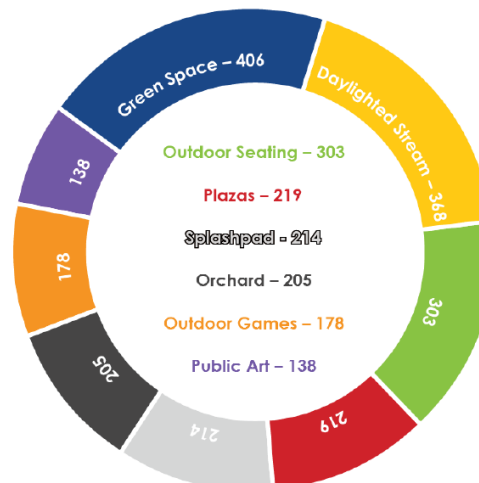
Choose the transportation infrastructure you would MOST like to see within the Town Center



Choose the non-residential land uses you would MOST like to see within the Town Center



Choose the open space / recreation amenities you would MOST like to see within the Town Center



Town Center Concept Plan



Preferred Conceptual Plan STATISTICAL SUMMARY

Project Total	45.13 ac
Open Space	
Plaza Space	
Roads	
Commercial/Retail Space	180,000 - 220,000 s.f.
Housing Types	Unit Range
6,000 s.f. Single Family Lots	8 - 12
Duplex	12 - 25
Mansion Home/Triplex	6 - 15
Townhomes	116 - 150
North Density Housing	80 - 105
Total Housing Units	220 - 270
	4.9 - 6.0 (du/ac)

NOTE: This preferred plan is conceptual in nature. Building design, amenities, and road network are all subject to change.

Alternate Conceptual Scenario



Project Total	45.13 ac
Open Space	
Plaza Space	
Roads	
Commercial/Retail Space	190,000 - 230,000 s.f.
Housing Types	Unit Range
6,000 s.f. Single Family Lots	8 - 12
Duplex	12 - 25
Mansion Home/Triplex	6 - 15
Townhomes	116 - 150
North Density Housing	80 - 130
Total Housing Units	220 - 270
	4.9 - 6.0 (du/ac)
Off-Street Parking	Spaces
Required Parking Spaces	730 - 760
Provided Parking Spaces	730 - 760

LEGEND

1 STORY



2 STORIES

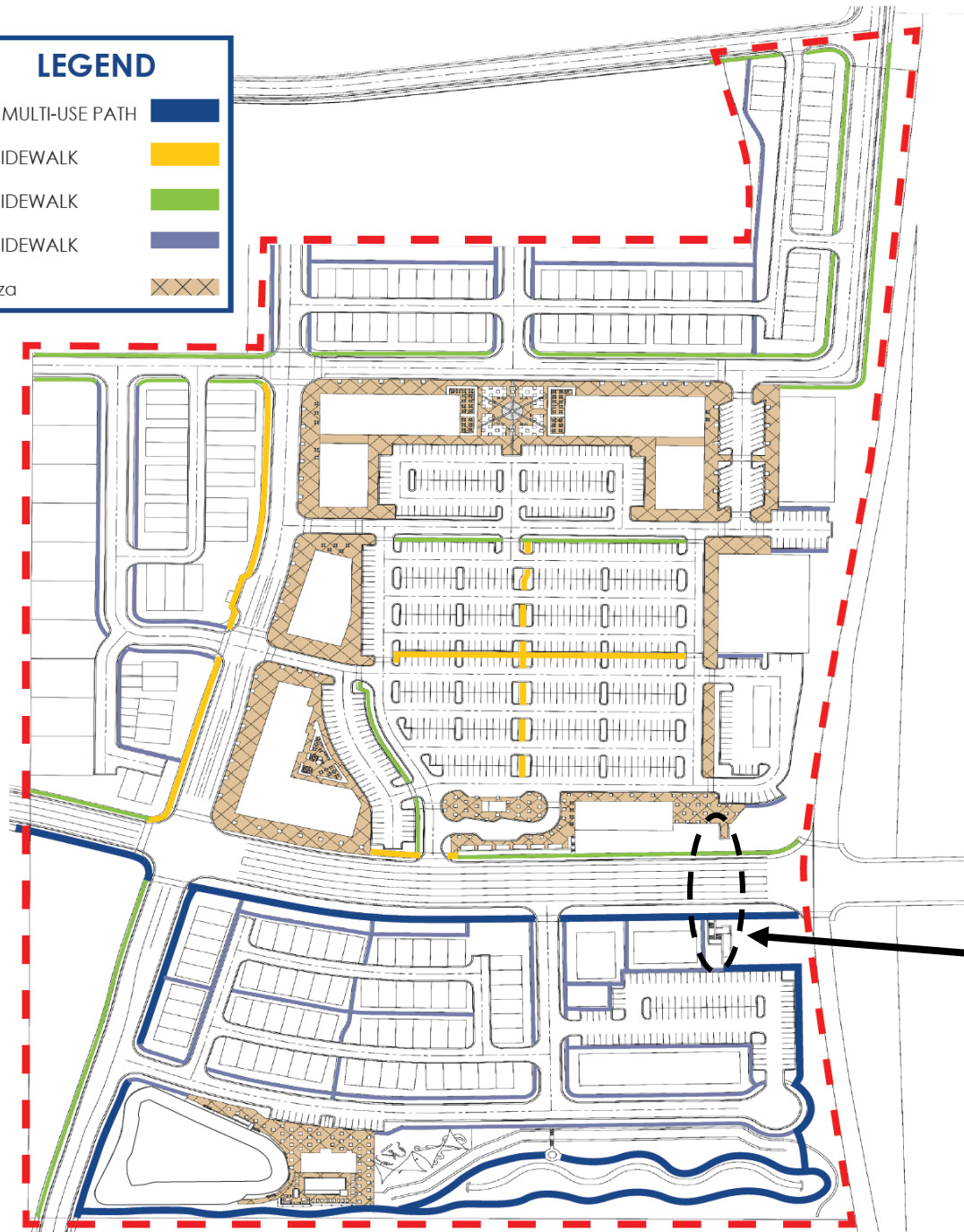


3 STORIES



LEGEND

- 10' MULTI-USE PATH
- 8' SIDEWALK
- 6' SIDEWALK
- 5' SIDEWALK
- Plaza



Pedestrian Underpass

Town Center Core
Facing Northwest



Artistic renderings of how the Town Center may look - These images do not depict final designs.

Corner of Gordon Ave. &
2625 E Facing Northwest



Artistic renderings of how the Town Center may look - These images do not depict final designs.



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Dining Plaza
Facing Northwest



Artistic renderings of how the Town Center may look - These images do not depict final designs.



**Town Square
Facing Southeast**

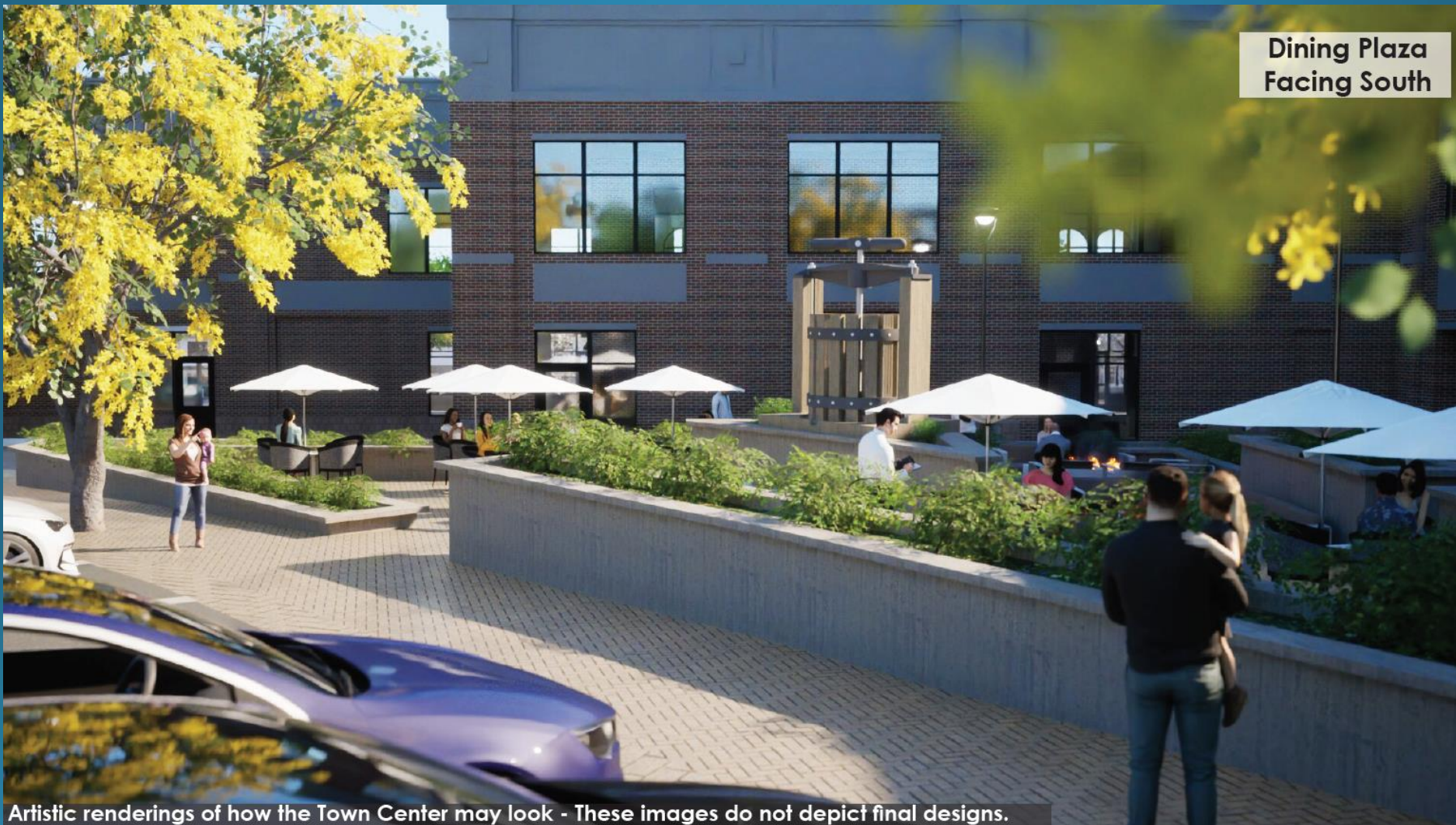


Artistic renderings of how the Town Center may look - These images do not depict final designs.



Community & Economic Development Department

Dining Plaza
Facing South



Artistic renderings of how the Town Center may look - These images do not depict final designs.



Community & Economic Development Department

An aerial artistic rendering of a proposed town center development. The scene shows a large parking lot in the foreground with several cars parked. To the left is a long row of two-story townhouses with dark roofs and light-colored siding. To the right is a modern multi-story apartment building with a mix of brick and light-colored panels. In the background, there's a green field with a winding path and more trees. The lighting suggests late afternoon or early morning.

Area South of Gordon
Ave. Facing Southwest

Artistic renderings of how the Town Center may look - These images do not depict final designs.



Community & Economic Development Department

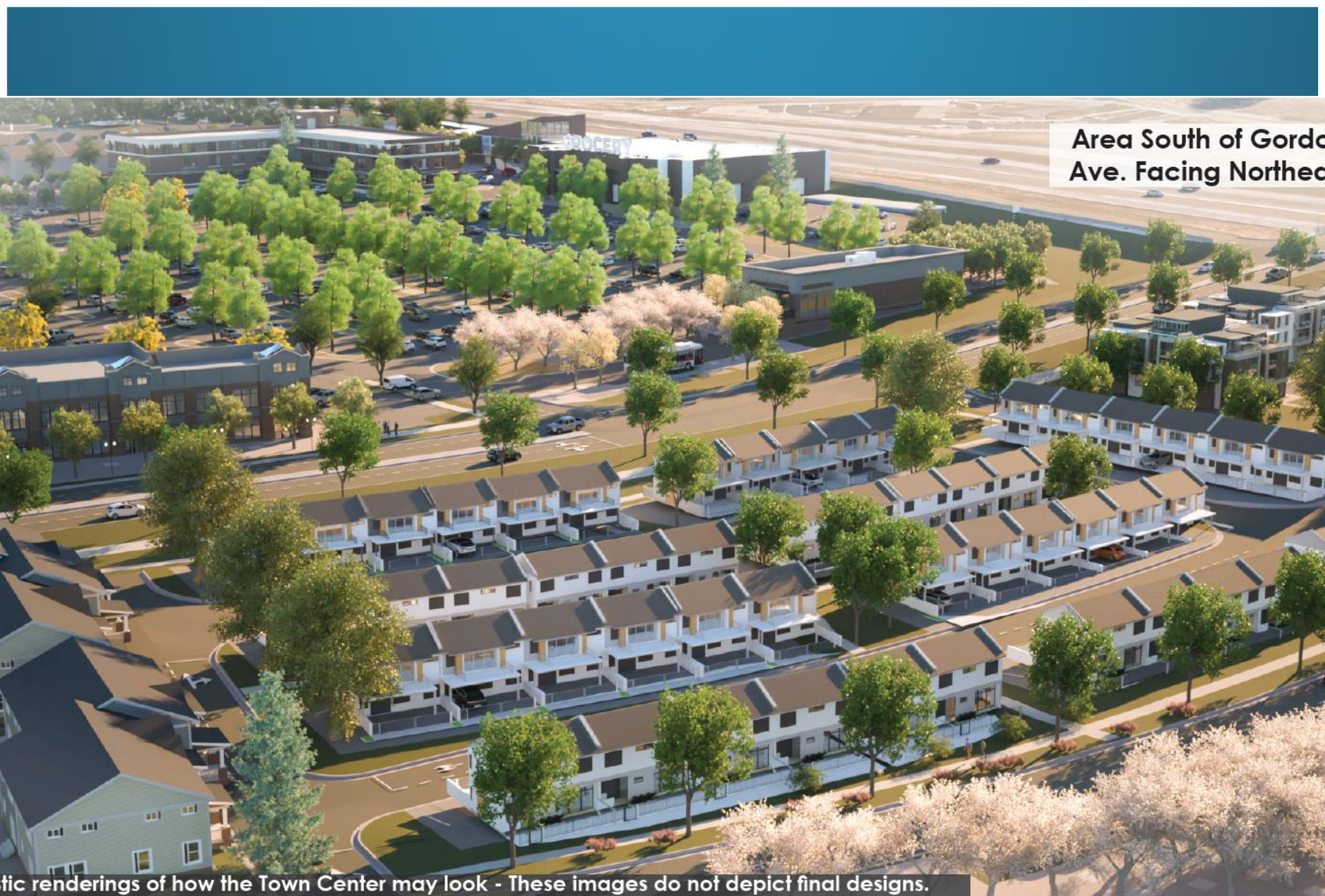
Along 1400 N
Facing East



Artistic renderings of how the Town Center may look - These images do not depict final designs.



Community & Economic Development Department



Area South of Gordon
Ave. Facing Northeast

Artistic renderings of how the Town Center may look - These images do not depict final designs.



Community & Economic Development Department

Residential Design Example



Commercial Design Example



