



**Wednesday, February 7, 2024  
Development Review Committee**

**DEVELOPMENT REVIEW COMMITTEE AGENDA**

**PUBLIC NOTICE is hereby given that the Development Review Committee of Spanish Fork, Utah, will hold a regular meeting at the City Council Chambers at Library Hall, 80 South Main Street, Second Floor, Spanish Fork, Utah, commencing at 10:00 a.m. This meeting is not available to attend virtually.**

**1. Approval of Minutes**

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A. January 24, 2024.

**2. Site Plan**

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A. CHASE BANK. This proposal involves a commercial development located at 1352 North Market Place Drive.

**3. Final Plat**

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A. WYNDSONG SUBDIVISION. This proposal involves the development of 19 residential lots for single family homes located at 349 East 1350 South.

**4. Concept Review**

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- A. HEAPS CONCEPT.
- B. PEREZ CONCEPT.
- C. RIDING PARCELS CONCEPT.
- D. SPANISH SPRINGS PHASES 3 & 4 CONCEPT.

**5. Annexation**

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A. JIM JENSON STEPHENS/HILL ANNEXATION.

**6. Discussion**

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A. CANYON COURT URBAN.

**7. Adjourn**

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**Draft Minutes**  
Spanish Fork City Development Review Committee  
80 South Main Street  
Spanish Fork, Utah  
January 24, 2024

**Staff Members Present:** Chris Thompson, Public Works Director; Seth Perrins, City Manager; Dave Anderson, Community Development Director; Brandon Snyder, Senior Planner; Mary Martin, Associate Planner; Kimberly Brenneman, Development Coordination Manager; Vaughn Pickell, City Attorney; Ana Burgi, Assistant City Attorney; John Little, Chief Building Official; Byron Haslam, Senior Engineer; Marcie Clark, Department Development Secretary; Zach Hendrickson, Outside Plant Manager; Jake Theurer, Power and Light Superintendent; Bart Morrill, Parks Maintenance Supervisor; Jason Turner, Fire Marshall; Kasey Woodard, Division Secretary.

**Citizens Present:** Ryan Phelps, Dave Schoville, Shanice Hone, Ladd Timpson, Cory Andersen, Tate Murphey, Austin Pritchett, Jackie Larson, Mitch Cordner, Ken Cordner, Matt Gertge, Easton Gertge, Dustin McBride, Trevor Dickerson, Tyler Horan, Steven Isaacson

Chris Thompson called the meeting to order at 10:00 a.m.

## **MINUTES**

### **January 10, 2024**

Seth Perrins **moved** to approve the minutes of January 10, 2024.

Jake Theurer **seconded** and the motion **passed** all in favor.

## **PRELIMINARY PLAT and ZONE CHANGE**

### **DB INDUSTRIAL**

Mary Martin gave a brief explanation of the proposed Zone Change from R-R Rural Residential to I-1 Light Industrial and she stated there are a few remaining concerns regarding access to the site, the installation of irrigation and aligning the utilities to the site.

Byron Haslam stated they still need the easement on the North access and there was discussion on proposing an easement from Sunroc. He stated there was never an easement obtained from Sunroc and he stated they are currently on hold because of it.

Dave Anderson **moved** to recommend the approval of the proposed DB Industrial Preliminary Plat to City Council based on the following finding and subject to the following conditions:

Finding:

1. That the application conforms to the City's General Plan Designation and Zoning Map.

Conditions:

1. That the applicant meets the City's Development and Construction standards, zoning requirements, and other applicable City ordinances.
2. That the applicant addresses any red-lines.

John Little **seconded** and the motion **passed** all in favor.

After the motion was made, Seth Perrins asked if this will create a flag lot and he asked if there are any issues or concerns that need to be addressed today?

Chris Thompson stated that it is important that all the frontage of both lots be improved.

Seth Perrins asked if a joint access easement will be required between the flag lot and the driveway?

Dave Anderson stated he understood that this was to be the plan and Byron Haslam agreed this was to be the plan.

Byron Haslam stated two accesses have been proposed, and both lots will be able to use both accesses, including the third property will also have access. He stated this is included in the redlines.

## **SITE PLAN**

### **SKYLINE NORTH AIRPORT**

Brandon Snyder stated these next two items are together. He gave a brief explanation of the proposals. He stated there are no major concerns with either the Site Plan or the Final Plat.

Dave Anderson **moved** to approve the proposed Skyline North Airport Site Plan based on the following finding and subject to the following conditions:

Finding:

1. That the application conforms to the City's General Plan Designation and Zoning Map.

Conditions:

1. That the applicant meets the City's Development and Construction standards, zoning requirements, and other applicable City ordinances.
2. That the applicant addresses any red-lines

Jake Theurer **seconded** and the motion **passed** all in favor.

Dave Anderson asked Byron Haslam for clarification regarding the bonding and it was stated the City will have the applicant bond for the storm drain.

## **FINAL PLAT**

### **SKYLINE NORTH AIRPORT**

It was stated this is subdividing the lot into 14 condominium units.

There were no other concerns.

Vaughn Pickell **moved** to approve the proposed Skyline North Airport Final Plat based on the following finding and subject to the following conditions:

Finding:

1. That the application conforms to the City's General Plan Designation and Zoning Map.

Conditions:

1. That the applicant meets the City's Development and Construction standards, zoning requirements, and other applicable City ordinances.

2. That the applicant addresses any red-lines.
3. That the public infrastructure as required for the plat be bonded before the plat can be recorded.

Dave Anderson **seconded** and the motion **passed** all in favor.

## **CONDITIONAL USE PERMIT**

### **2505 EAST T-MOBILE CELL TOWER MODIFICATION**

Mary Martin stated this is making modifications to an existing cell tower.

Brandon Snyder provided current images of the cell tower. He stated this is fully enclosed in the public works compound that also contains power, water, and other utilities.

Vaughn Pickell asked if this is replacing the antenna and Brandon stated that this is upgrading the equipment at the base of the tower.

Dave Anderson **moved** to recommend the approval of the proposed 2505 East T-Mobile Cell Tower Modification Conditional Use Permit based on the following finding and subject to the following conditions:

Finding:

1. That the application conforms to the City's General Plan Designation and Zoning Map.

Conditions:

1. That the applicant meets the City's Development and Construction standards, zoning requirements, and other applicable City ordinances.
2. That the applicant addresses any red-lines.
3. That the applicant notifies the Power Department prior to work commencing.
4. That work hours be limited to 8 am to 6 pm.
5. Verification with the City that there is a current lease agreement for the area.

Jake Theurer **seconded** and the motion **passed** all in favor.

Jake Theurer reminded the applicant to maintain a distance of 10 feet for safety as there is a lot of power in that area.

## **ANNEXATIONS**

### **WEST MEADOWS ANNEXATION**

Brandon Snyder stated there will be a potential name change as there is an existing annexation with the same name at the County. He stated this may come back to the DRC for acceptance with a different name. He stated this is very similar to another annexation that was proposed to the City a year prior. He gave the location of the proposed area that is to be annexed into the City and stated it is within both our Annexation Policy Plan and the City's Growth Management Boundary Plan. He stated the County will need to review this configuration, and the City will need to review the proposal to ensure this does not create any peninsulas. He stated the current designation is Commercial and Mixed-Use and then Urban Density Residential. He stated that it is being proposed that if it is accepted for further study, that only the C-2 and R-R zoning be considered at this time.

Engineering has no comments.

Jake Theurer stated the dual power lines in the area need to be addressed.

Vaughn Pickell stated this proposal does not show any portion of the interstate right-of-way and it was stated that Travis Warren the City Surveyor caught this oversight and it is being addressed.

There was a brief discussion on the boundary line and where Spanish Fork City is responsible for.

Seth Perrins suggested not annexing the freeway. There was further discussion of the matter.

Dave Anderson stated there were properties within this annexation that were looked at being annexed previously, and he stated he had concerns at the time. He stated he does not have concerns with this proposal as he feels the timing is right and some of the concerns can be addressed. He is comfortable accepting this for further study.

Austin Pritchett stated the properties have already been sold to the County and Seth Perrins stated this meets the requirement.

Dave Anderson the road coming up from the south will be a big deal and will be very similar to Canyon Creek Parkway eventually.

Dave Anderson **moved** to recommend the City Council accept the proposed West Meadows Annexation for further study based on the following findings and subject to the following conditions:

Findings:

1. That the subject property is located within the City's Annexation Policy Boundary and Growth Management Boundary.
2. That the City's General Plan Land Use Designations currently for the annexation area are Commercial, Mixed Use and Urban Density Residential.
3. That the C-2 and R-R zones should be utilized at the time of annexation.
4. That utility and street dedications should be further discussed and addressed with the annexation.
5. That the applicant make any modifications to the boundary that are necessary to the process.

Conditions:

1. That the applicant enters into an Annexation Agreement.

Bart Morrill **seconded** and the motion **passed** all in favor.

## **CONCEPT REVIEW**

### **RUSH FUNPLEX TOWNHOME CONCEPT**

Brandon Snyder stated the applicant has provided an updated plat from what was previously reviewed by staff. He stated modifications were made to the location of the apartments to bring some additional units out to the street. He stated the applicants are looking for feedback on taking the north area of the property, and diverting from the City's General Plan designation of residential use, and seeking to have it zoned commercially to allow for the proposed entertainment use that is being discussed.

Steven Isaacson, the architect representing the concept, gave a brief explanation of the modifications that were made to the previously submitted plans.

Chris Thompson asked the applicant to give more information about the Funplex.

The applicant stated they are an entertainment use that offers bowling, arcades, laser tag and other entertainment opportunities. He stated this would be a single-story building that is roughly 75,000 square feet.

Dave Anderson stated there has been previous discussion held regarding this proposal and it was stated that the General Plan has this area designated exclusively for Residential Development.

Chris Thompson stated there are a few concerns regarding the interchange and the need for additional lanes.

Jake Theurer stated there are large distribution powerlines located underground and that will need to be relocated.

Seth Perrins asked what density is being proposed and Steven Isaacson stated that they looked at the City Code and it stated the density is 18-24 units per acre, which would allow them to build up to 100 townhomes if desired. He stated they are significantly under this density by only utilizing about 70-80% of that allowance.

Seth Perrins stated that with the City's higher density range of 18+ units per acre, it has certain requirements that involve street frontages and if the proposed density is under that number, this may not qualify for an R-4 density.

There was discussion regarding the proposed R-4 zoning due to the number of townhomes proposed. It was stated that the current design does not meet the criteria for the R-4 zoning if their density is higher than 12 units per area.

Seth Perrins stated he is interested in what the Planning Commission would have to say. He asked where the closest Funplex would be to Spanish Fork and it was stated there are locations in the Orem mall and in the Jordan Landing area. Seth Perrins asked what the noise levels would be and the applicant stated there is not much exterior noise. He stated the entertainment noises are enclosed within the building.

There was discussion on placing the Funplex next to the residential area and how the parking could impact the residences and it was suggested that a dividing wall should be considered.

Dave Anderson stated that he would like this to be discussed with the Planning Commission in two weeks and to get their thoughts and opinions.

## **DISCUSSION**

### **RIVER RUN**

Matt Loveland rose to speak briefly about the revisions that are being discussed today. He stated he is seeking feedback from the DRC today before this goes back to the Planning Commission.

There was discussion regarding some of the changes from the previous Preliminary Plat design.

Jake Theurer stated the applicants have been great to work with. He stated there is an existing SUVPS powerline that the applicants plan to build under, and he stated that this powerline is planned to be removed. He stated that until that powerline is relocated, if the developer is ahead of SUVPS, there may be costs incurred.

Dave Anderson gave a brief history of previous discussions that were held regarding this proposal. He feels this configuration is very compactly designed. He stated that he is very appreciative of the applicant working well with the City to come up with a better overall design.

Seth Perrins has questions regarding parking and it was stated that the units to the east have full length driveways and there will be street parking for the units on the other side. He states there are elements of the new design that he likes.

Dave Anderson shared other items of discussion including the road between Volunteer Drive and the City Fairgrounds needs to be wider than what is shown on the design.

Byron Haslam stated there was a meeting held that discussed this proposal but it was decided that it would be better kept at 60 feet as a traffic slowing measure.

Matt Loveland thanked the City for this, and stated they do not want traffic speeding through the neighborhood. He stated that Byron Haslam has given wonderful suggestions and been very accommodating.

There was a discussion regarding the City code and it was brought to the attention of the staff that the way the City code reads currently, requires multifamily developments to follow the Commercial Local Standards as they are not looked at residentially, they are viewed commercially, which require a 70-foot-wide road, not the 60-foot-wide road that is

required in residential areas. It was stated that the City has historically not followed this standard.

Dave Anderson stated this needs to be reviewed by staff, and he feels that there is not a need for all residential areas to have a 70-foot-wide road. He suggested staff can start looking at this next week.

There was a brief discussion regarding the Griffiths property owners.

## **THE ORCHARDS MASTER PLANNED DEVELOPMENT**

Dave Anderson stated that he has a meeting set up with the developer. He stated this is a follow up conversation from the Planning Commission. He stated he wanted to get Vaughn Pickell's feedback on whether the City is under any obligation to provide any financial compensation for the easement. He stated that the applicant has expressed their thoughts that they would be compensated for a portion of that. He stated it was not his understanding that the City would be providing compensation for the dedication.

Vaughn Pickell stated that this surprises him as it was also his understanding that this was a willing dedication and that no financial compensation will be provided.

Seth Perrins stated that without the Master Planned Development Overlay, the development is allowed 14 lots. He stated that if the City must purchase the land, then the development will go back to 14 being only 14 lots. But if the applicant wishes to be approved for 16 lots, then they can have the 16 lots, but they will need to dedicate the ground. But the City will not be purchasing the land and approving 16 lots.

Vaughn Pickell stated the applicant is entitled to a standard subdivision that allows for 14 lots, but states this is not a standard subdivision and the applicant needs to show the City some benefit for allowing additional lots.

It was stated that if this discussion were for approval, it would be approved either way. But it was stated that the applicant can have 14 lots and be compensated for the land, or 16 lots and dedicate the land. But they will not be getting 16 lots and be compensated.

## **MCDONALD ZONE CHANGE**

There have been previous discussions with the DRC regarding this Zone Change and it was stated that this discussion will be regarding the cross-access easement. It was stated this was recommended for approval of all 3 lots with the condition that the applicant provide

necessary access and parking easements to guarantee the different property owners within the development have adequate access and parking.

Brandon Snyder stated that since the meeting the applicant has reached out with concerns regarding the design layout or use of the bottom two lots. He stated it is the preference of the City to have all three lots rezoned at one time.

Dave Anderson suggested the DRC relents on the condition that with the Zone Change, the applicant provides the easement that provides access between the two lots that are south of the driveway that leads to the townhomes, because the driveway provides access to the two northernmost lots.

There was discussion regarding the lots and it was stated it has been common for property owners to record or split the parcel with the County and not going through the City. So, the subdividing of the parcel is not recognized by the City.

Vaughn Pickell stated he has spoken with the property owner and informed them that all three lots need to be platted as a subdivision rather than just created by deeds recorded by the County.

Brandon Snyder asked for clarification that if the applicant came to the City for any Site Plan or development application, that it would be required for the applicant to apply for a Preliminary Plat for the entire area first.

It was discussed that if any development is to occur, the property owner must first apply for a Preliminary Plat. It is stated in the City's Municipal Code that this must go through the City's process to be recognized by the City.

Chris Thompson suggested not allowing the property owner to subdivide the property until they go through the City's process and the suggested zoning be C-2.

Dave Anderson agreed with the suggested C-2 zoning and recommended that the City allow the Zone Change but suggested the City not allow the property owner to subdivide the property until the plat has been recorded and the right of way be dedicated.

## **LELAND MEADOWS PHASING PLAN**

Byron Haslam stated this discussion is stemming from a previous DRC meeting. He stated there were concerns with 1700 West only partially being built. He stated the applicant has provided a temporary solution to the concerns until the full realignment of 1700 West can

be completed. He stated he does not have any major concerns with this proposal as it will be sufficient for the short term.

Brandon Snyder asked if there is a specific time frame when using the verbiage “short term” and it was stated this is regarding the phase lines on 1700 West.

Dean Ingram stated the road will temporarily be asphalt. He stated their plan is to install the curb, gutter, and sidewalk regardless.

Dave Anderson asked for clarification on the order or development.

Dean Ingram gave a brief explanation of their proposed phasing plan.

## **FIRE CODE REGULATIONS**

### **APPENDIX L**

Jason Turner rose to speak about the proposed adoption to the fire code. He stated this proposal would apply to warehouse buildings over 250,000 square feet. He provided a video for the committee to watch that goes over the new system.

Eddie Hales rose to briefly explain what is being proposed. He stated that this adoption could greatly increase fire response times and save firefighters from having to manually carry the air tubes up flights of stairs.

Dave Anderson is in favor of this proposal and would like to see a breakdown of the cost.

Jason Turner stated he has a meeting with The Firefighter Air Coalition the following day and will be getting an estimate that he can provide to the City for review.

Seth Perrins feels this is very beneficial to the high rises as was discussed. He asked how these work with the horizontal warehouses being developed.

Jason Turner **moved** to recommend the approval of the proposed Fire Code Regulations Appendix L to City Council as discussed.

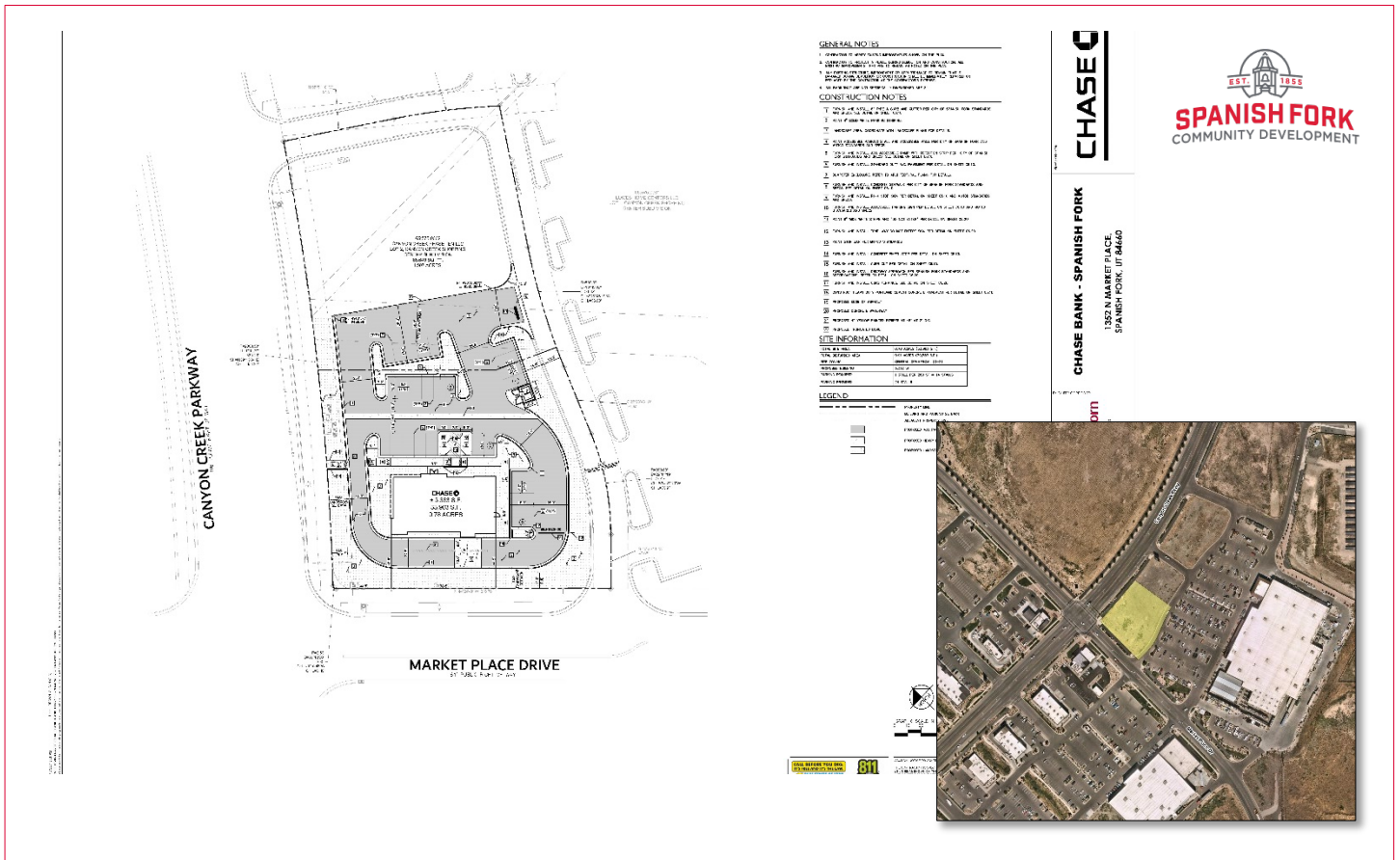
Brandon Snyder **seconded** and the motion **passed** all in favor.

Bart Morrill moved to adjourn the meeting at 12:20 p.m.

Adopted:

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Kasey Woodard  
Community Development Division  
Secretary



**Chase Bank Site Plan Approval Request**

February 7, 2024, Development Review Committee meeting.

Located at 1352 North Market Place Drive, including 0.78 acres.

The subject property is zoned C-2.

The applicant has requested that a Site Plan be approved for the development of a bank.

**Key Issues**

- 1. Improvements.

**Recommendation**

That the proposed Site Plan be approved based on the following finding and subject to the following conditions.

**Finding**

- 1. That the proposal conforms to the City's General Plan Designation and Zoning Map.

**Conditions**

- 1. That the applicant meets the City's Development and Construction standards, zoning requirements, and other applicable City ordinances.
- 2. That the applicant addresses any red-lines.

**Exhibits**

- 1. Site Plan.
- 2. Landscape Plan.
- 3. Building Elevations.



ENGINEERING CONSTRUCTION NOTES

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

ENGINEERING DEMOLITION NOTES

1. ALL DEMOLITION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL DEMOLITION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

ENGINEERING GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

STORM WATER POLLUTION PREVENTION PLAN

MAINTENANCE NOTES
1. ALL MAINTENANCE SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

ENGINEERING SANITARY SEWER NOTES

1. ALL SANITARY SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL SANITARY SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

ENGINEERING PAVING NOTES

1. ALL PAVING CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL PAVING CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

AMERICANS WITH DISABILITIES ACT (ADA) NOTES

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

ENGINEERING DRAINAGE NOTES

1. ALL DRAINAGE CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
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ENGINEERING GRADING NOTES

1. ALL GRADING CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
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STORM WATER POLLUTION PREVENTION PLAN - GENERAL NOTES

1. ALL STORM WATER POLLUTION PREVENTION PLAN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL STORM WATER POLLUTION PREVENTION PLAN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

SPANISH FORK CITY ELECTRICAL NOTES

1. ALL ELECTRICAL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
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SPANISH FORK CITY PERMIT NOTES

1. ALL PERMIT CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
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Table with 2 columns: DATE, TIME SCHEDULE. Includes a header row and several empty rows for scheduling.

CHASE BANK - SPANISH FORK
1352 N MARKET PLACE, SPANISH FORK, UT 84650

Kimley-Horn & Associates, Inc.
Professional Engineers



GENERAL NOTES
1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

Logos for Utah State Office of Public Safety and other agencies.

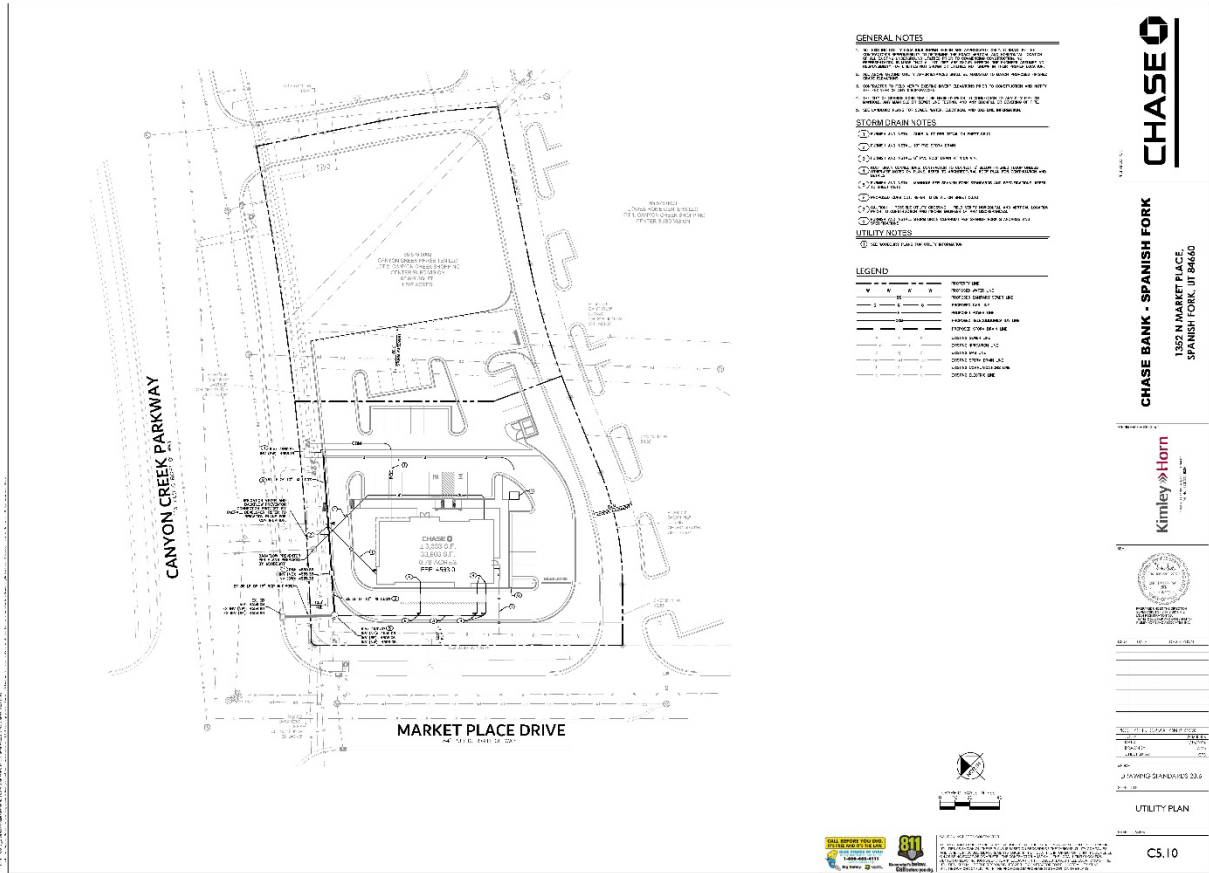


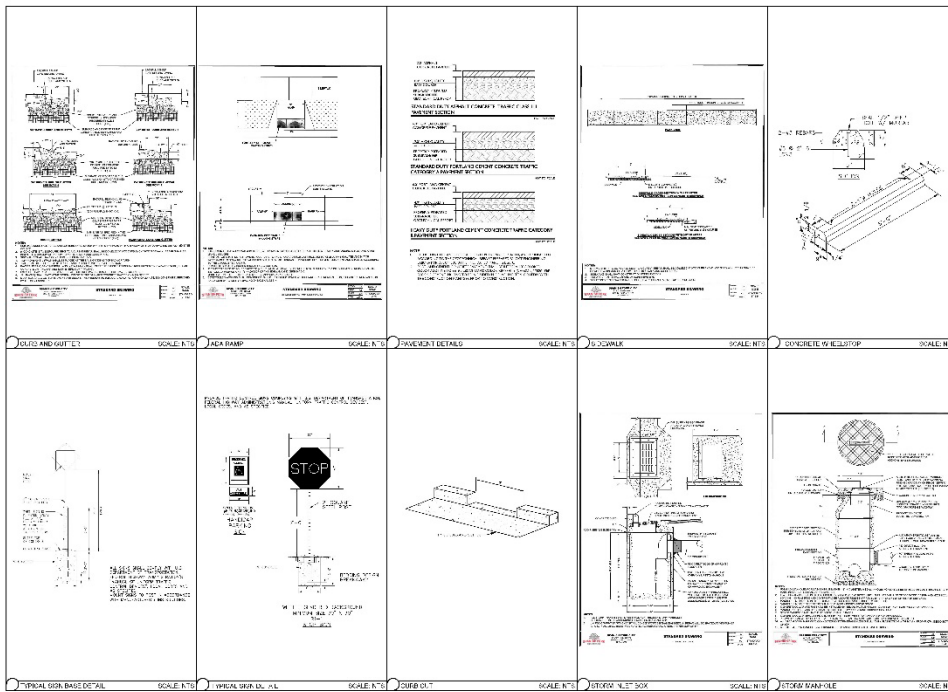












**CHASE**

**CHASE BANK - SPANISH FORK**  
 1352 N MARKET PLACE,  
 SPANISH FORK, UT 84660

**Kimley-Horn**



NO. 001 - 004 - 005 - 006

DATE: 11/20/2023  
 PROJECT: CHASE BANK - SPANISH FORK  
 SHEET: C6.10

CONSTRUCTION  
 DETAILS

C6.10



UTAH STATE OFFICE OF CONSTRUCTION AND SAFETY  
 160 SOUTH STATE STREET, SUITE 200  
 SALT LAKE CITY, UT 84143  
 TEL: 801-538-8800  
 WWW.UTAHGOVT.COM

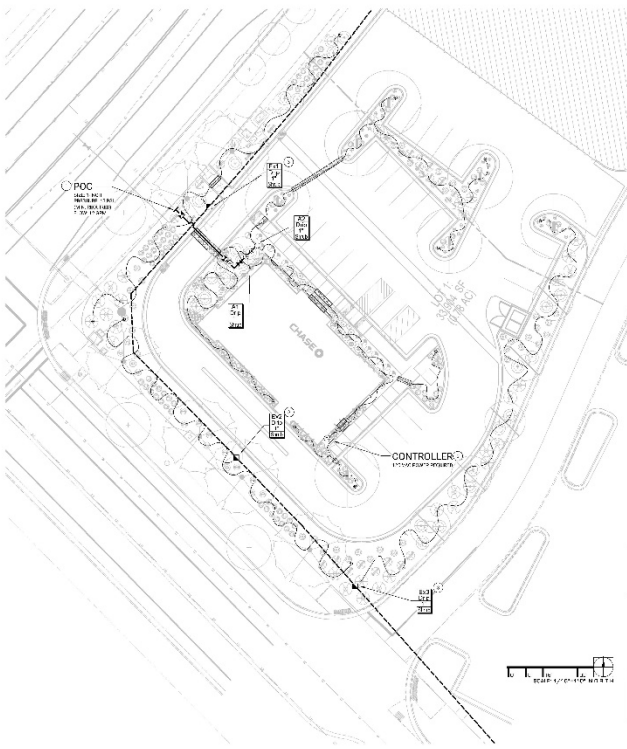
**CHASE BANK - SPANISH FORK**  
1382 N MARKET PLACE,  
SPANISH FORK, UT 84660

**Kimley-Horn**  
CONSULTING ENGINEERS

DATE: 02-07-2024  
PROJECT: CHASE BANK - SPANISH FORK  
DRAWING: CONSTRUCTION DETAILS  
SHEET: C6.20

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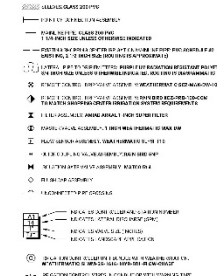




INSTALLATION GENERAL NOTES

1. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITY LINES PRIOR TO ANY CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES.
2. ALL IRRIGATION SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH THE IRRIGATION DESIGN AND THE IRRIGATION CONTRACT DOCUMENTS.
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IRRIGATION LEGEND



CONSTRUCTION NOTES

1. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITY LINES PRIOR TO ANY CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES.
2. ALL IRRIGATION SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH THE IRRIGATION DESIGN AND THE IRRIGATION CONTRACT DOCUMENTS.
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10. ALL IRRIGATION SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH THE IRRIGATION DESIGN AND THE IRRIGATION CONTRACT DOCUMENTS.

**THE WEATHERMATIC IRRIGATION CONTROLLER MEETS CHASE REQUIREMENTS AND WILL PROVIDE UNIQUE CONNECTION TO THE BANK BUILDING MONITORING SYSTEM, LONG-TERM COMMUNICATIONS, AND MANAGED SERVICES. CONTACT THE DISTRIBUTOR FOR ALL IRRIGATION EQUIPMENT ORDERING.**

**REQUIRED: AFTER INSTALLATION AND ONLINE SET UP OF THE SMARTLINK (ONLINE) SYSTEM USING THE PRODUCT INSTRUCTIONS INCLUDED, CONTACT WEATHERMATIC TO PROVIDE THE SITE NAME AND CONTROLLER NAME IN SMARTLINK FOR FINAL ACTIVATION. EMAIL: JFMCSupport@weathermatic.com**

CHASE BANK DESIGN NOTE

THE WEATHERMATIC IRRIGATION CONTROLLER MEETS CHASE REQUIREMENTS AND WILL PROVIDE UNIQUE CONNECTION TO THE BANK BUILDING MONITORING SYSTEM, LONG-TERM COMMUNICATIONS, AND MANAGED SERVICES. CONTACT THE DISTRIBUTOR FOR ALL IRRIGATION EQUIPMENT ORDERING.

NOTE TO CONTRACTOR

THE WEATHERMATIC IRRIGATION CONTROLLER MEETS CHASE REQUIREMENTS AND WILL PROVIDE UNIQUE CONNECTION TO THE BANK BUILDING MONITORING SYSTEM, LONG-TERM COMMUNICATIONS, AND MANAGED SERVICES. CONTACT THE DISTRIBUTOR FOR ALL IRRIGATION EQUIPMENT ORDERING.

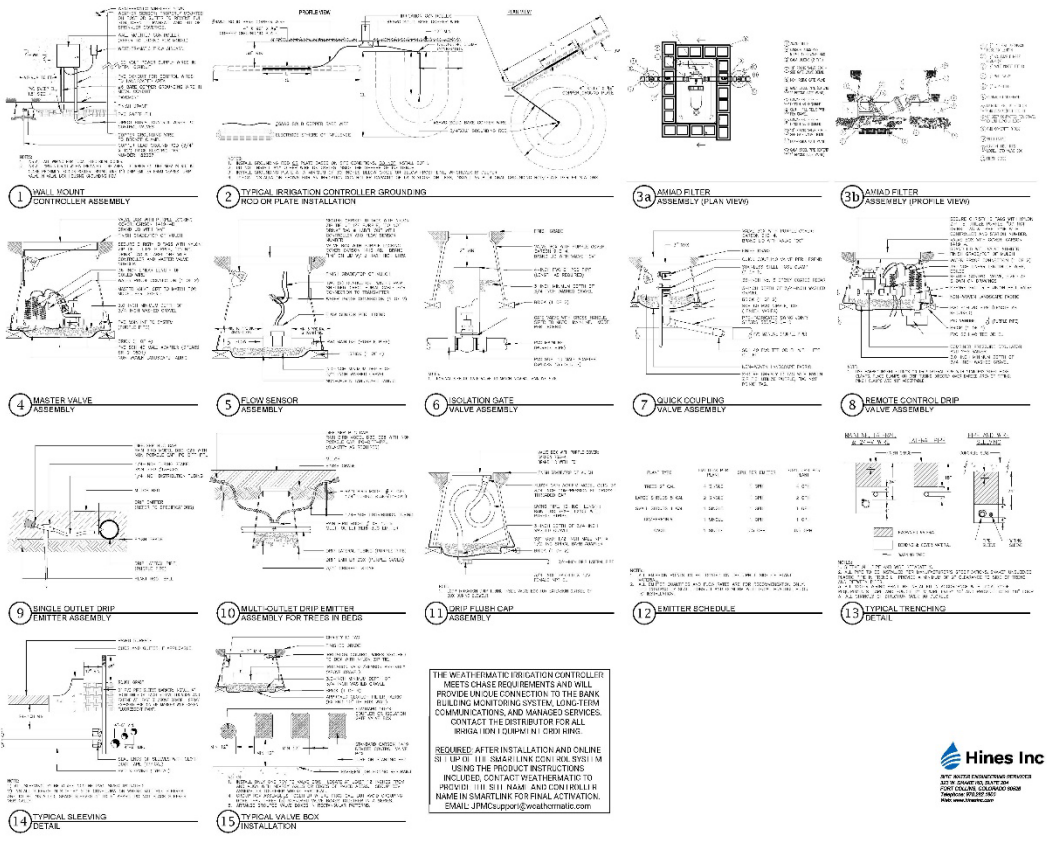


SPANISH FORK  
CHASEO  
ARCHITECTURE

BRIT C. JEFFERY  
COMMERCIAL

DATE: 02/07/2024  
PROJECT: 224720

IR1.00



**THE WEATHERMATIC IRRIGATION CONTROLLER MEETS CHASE REQUIREMENTS AND WILL PROVIDE UNIQUE CONNECTION TO THE BANK BUILDING MONITORING SYSTEM, LONG-TERM COMMUNICATIONS, AND MANAGED SERVICES. CONTACT THE DISTRIBUTOR FOR ALL IRRIGATION EQUIPMENT ORDERING.**

**REQUIRED: AFTER INSTALLATION AND ONLINE SET UP OF THE SMARTLINK (ONLINE) SYSTEM USING THE PRODUCT INSTRUCTIONS INCLUDED, CONTACT WEATHERMATIC TO PROVIDE THE SITE NAME AND CONTROLLER NAME IN SMARTLINK FOR FINAL ACTIVATION. EMAIL: JFMCSupport@weathermatic.com**



SPANISH FORK  
CHASEO  
ARCHITECTURE

BRIT C. JEFFERY  
COMMERCIAL

DATE: 02/07/2024  
PROJECT: 224720

IR2.00

**WEST ELEVATION**  
SP-1  
SCALE: 1/8" = 1'-0"

**NORTH ELEVATION**  
SP-1  
SCALE: 1/8" = 1'-0"

**EXTERIOR FINISH MATERIAL**

- 01.4 SPANISH FORK BRICK: SPANISH FORK BRICK, 2 1/2" x 3 1/2" x 8" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
- 01.5 CEMENT PANEL SIDING: CEMENT PANEL SIDING, 1/2" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
- 01.6 METAL PANEL: METAL PANEL, 1/4" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
- 01.7 ALUMINUM COMPOSITE MATERIAL: ALUMINUM COMPOSITE MATERIAL, 1/4" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
- 01.8 DARK GRANITE: DARK GRANITE, 1/2" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
- 01.9 PAINT COLOR: PAINT COLOR, 1/2" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)

CHASE BANK-SPANISH FORK  
ARCHITECTURAL ELEVATION

**CHASE**

**APMI**  
architecture  
ARCHITECTURE

1336 BTT  
PWS BY: [REDACTED]  
FOR CONSULTATION  
FOR PROVISIONS

DESIGN: [REDACTED] RAK  
DATE: 03/08/2024 NCS  
PROJECT: 23-210

BUILDING  
ELEVATIONS

SP-1

**EAST ELEVATION**  
SP-2  
SCALE: 1/8" = 1'-0"

**SOUTH ELEVATION**  
SP-2  
SCALE: 1/8" = 1'-0"

**EXTERIOR FINISH MATERIAL**

- 01.4 SPANISH FORK BRICK: SPANISH FORK BRICK, 2 1/2" x 3 1/2" x 8" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
- 01.5 CEMENT PANEL SIDING: CEMENT PANEL SIDING, 1/2" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
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- 01.8 DARK GRANITE: DARK GRANITE, 1/2" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)
- 01.9 PAINT COLOR: PAINT COLOR, 1/2" x 12" x 24" (SEE MANUFACTURER'S LITERATURE FOR MORE INFORMATION)

CHASE BANK-SPANISH FORK  
ARCHITECTURAL ELEVATION

**CHASE**

**APMI**  
architecture  
ARCHITECTURE

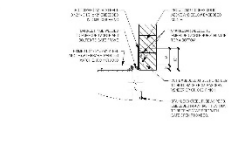
1336 BTT  
PWS BY: [REDACTED]  
FOR CONSULTATION  
FOR PROVISIONS

DESIGN: [REDACTED] RAK  
DATE: 03/08/2024 NCS  
PROJECT: 23-210

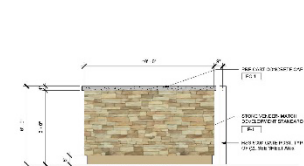
BUILDING  
ELEVATIONS

SP-2

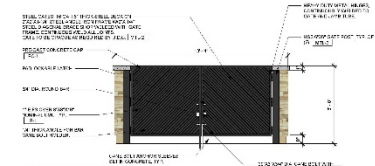
PROVIDE 100% CATCHMENT AREA FOR ALL TRASH ENCLOSURES. PROVIDE 100% CATCHMENT AREA FOR ALL TRASH ENCLOSURES. PROVIDE 100% CATCHMENT AREA FOR ALL TRASH ENCLOSURES.



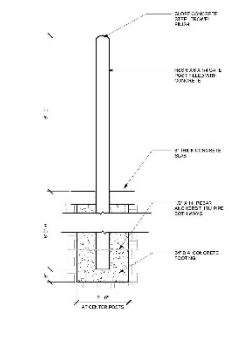
7 TRASH ENCLOSURE - CORNER DETAIL  
SCALE: 1/4\"/>



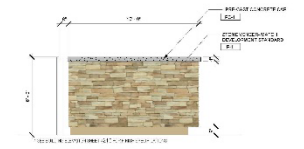
3 TRASH ENCLOSURE ELEV. (SIDE)  
SCALE: 3/4\"/>



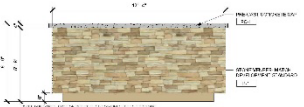
2 TRASH ENCLOSURE ELEV. (FRONT)  
SCALE: 3/4\"/>



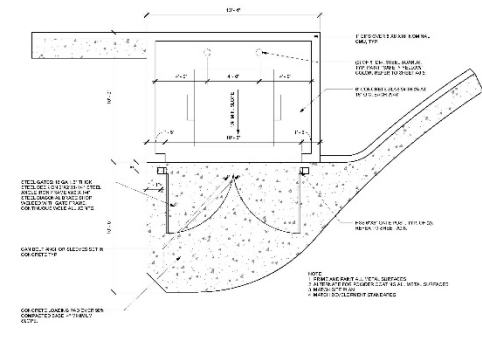
6 GATE POST FOOTING DTL  
SCALE: 1/4\"/>



4 TRASH ENCLOSURE ELEV. (SIDE)  
SCALE: 3/4\"/>



5 TRASH ENCLOSURE ELEV. (REAR)  
SCALE: 3/4\"/>



1 GATE POST FOOTING DTL  
SCALE: 3/4\"/>

CHASE BANK-SPANISH FORK  
1000 GARDNER AVENUE  
MEMPHIS, TN 38103

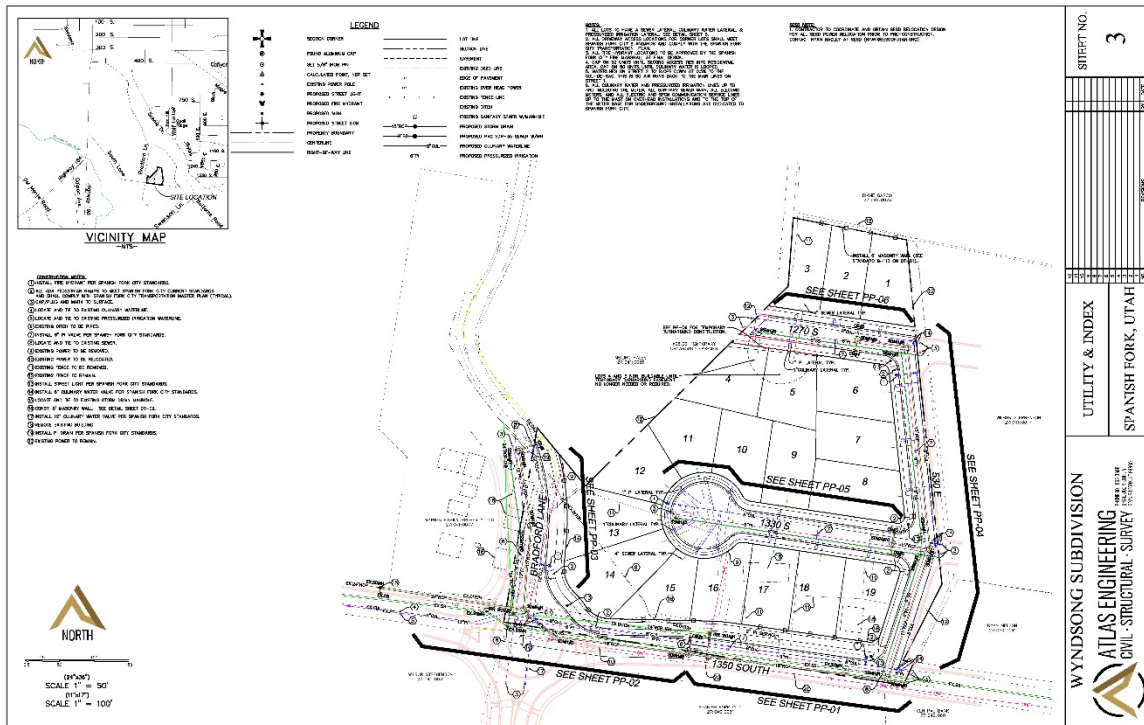
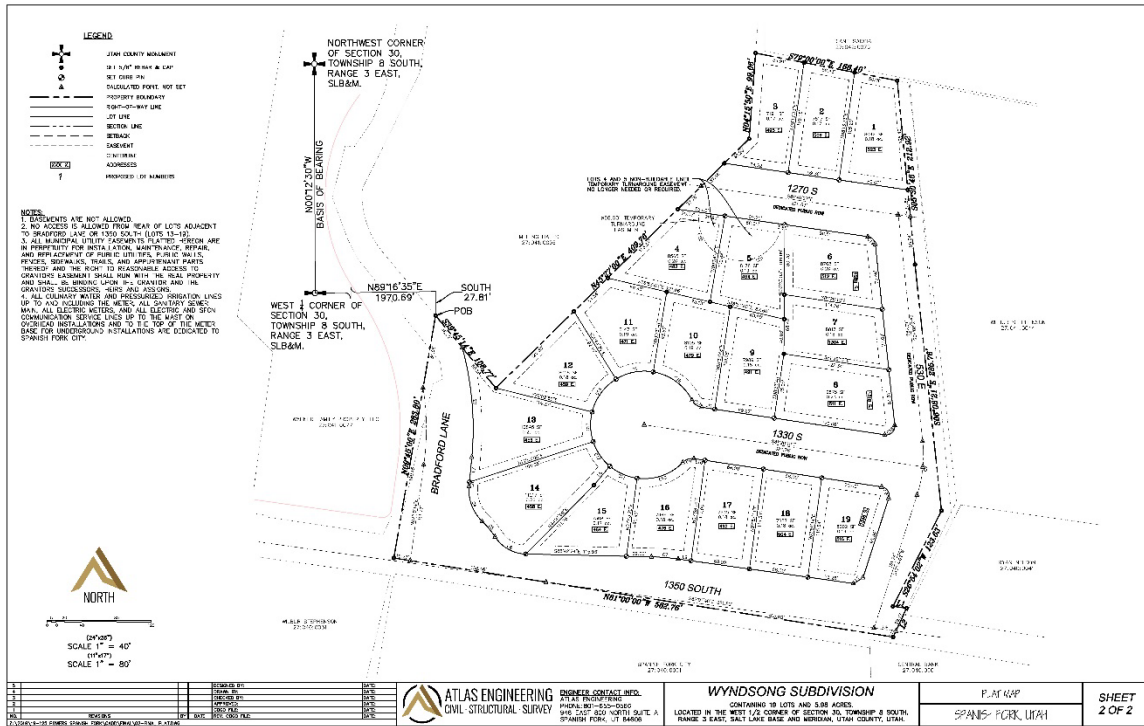
**CHASE**

**APMII**  
architecture  
MEMPHIS, TN 38103

DRAWN BY: RM  
DATE: 01.24.2024  
PROJECT: 23-001  
TRASH ENCLOSURE AND CURB DTLS  
SP-3



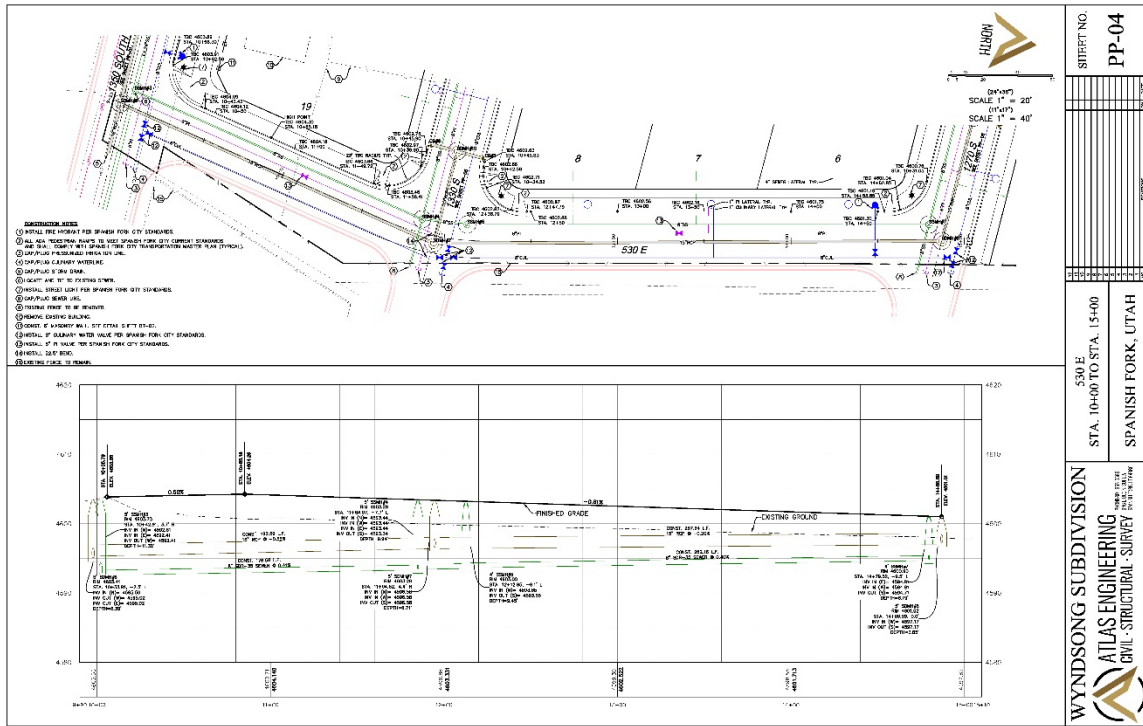








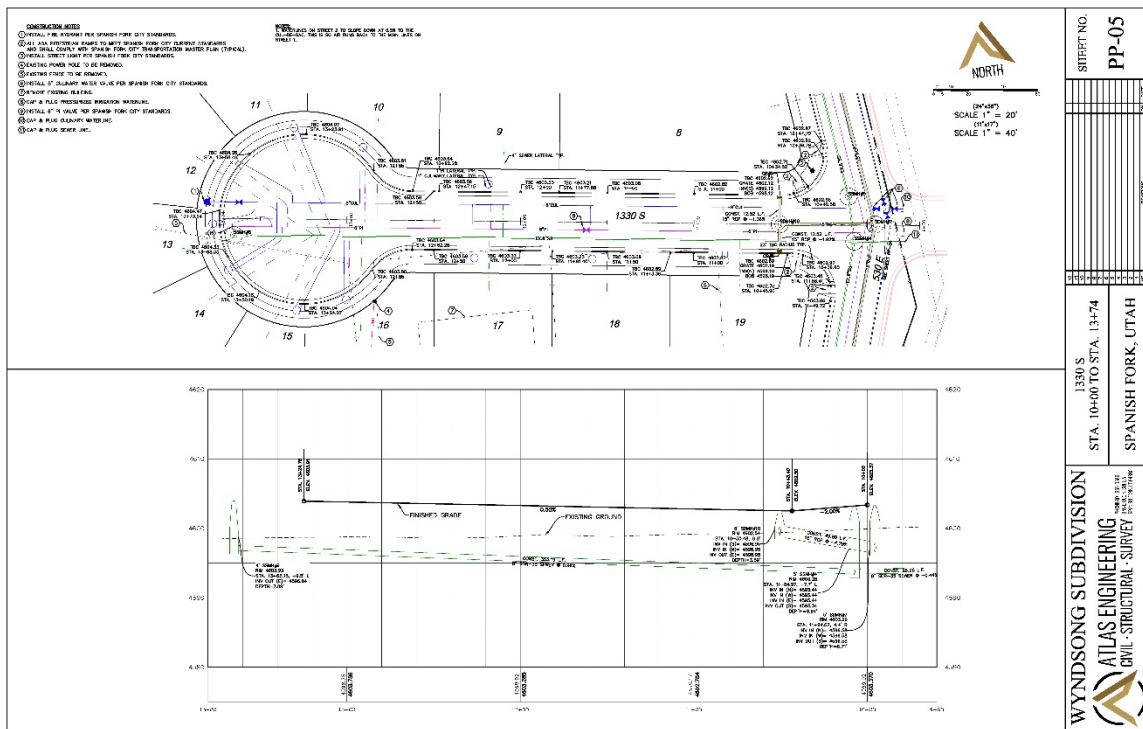




STREET NO. **PP-04**

530 E  
STA. 10+00 TO STA. 15+00  
SPANISH FORK, UTAH

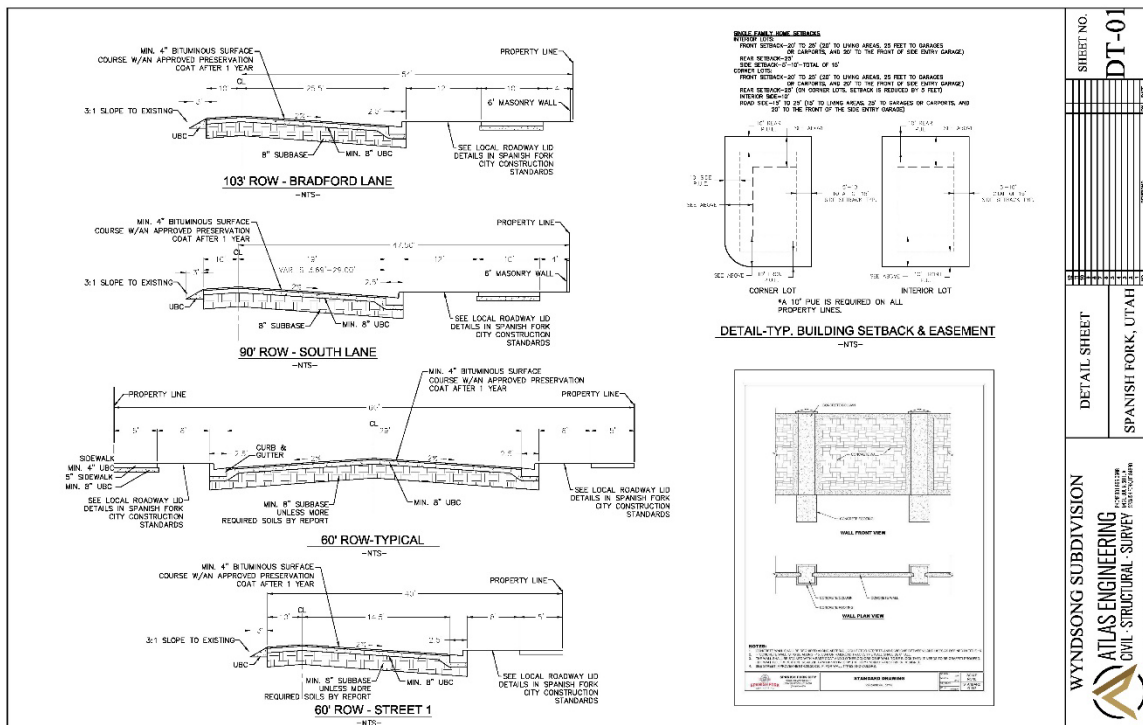
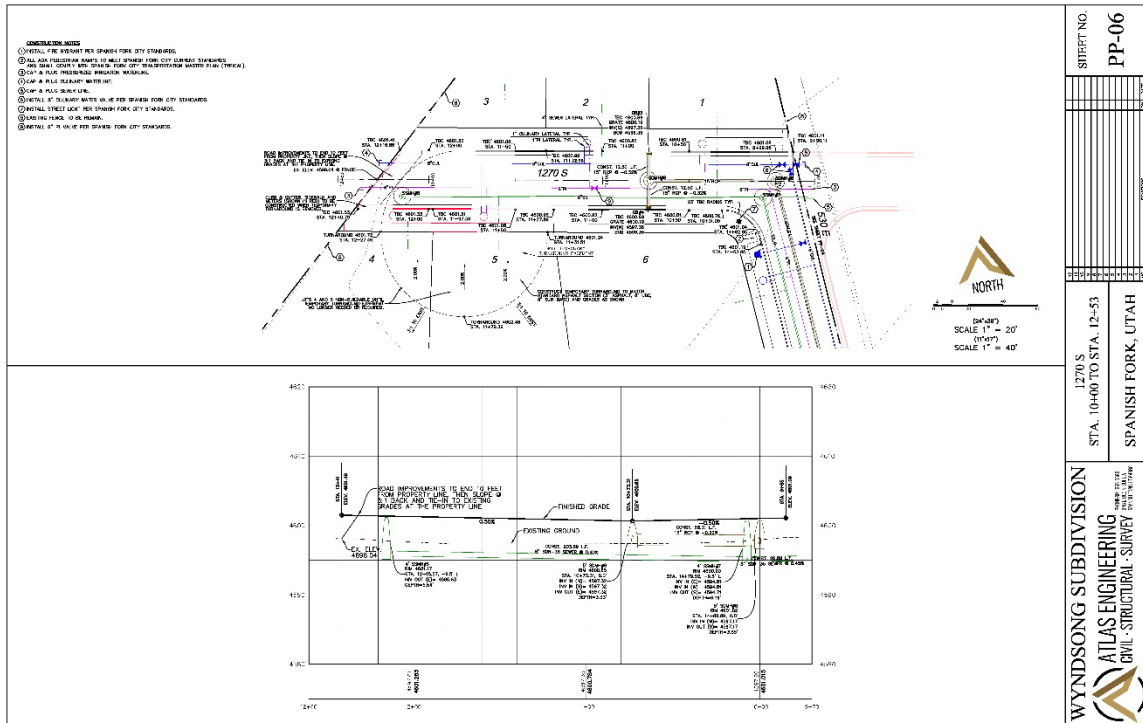
WYNDSONG SUBDIVISION  
ATLAS ENGINEERING  
CIVIL - STRUCTURAL - SURVEY

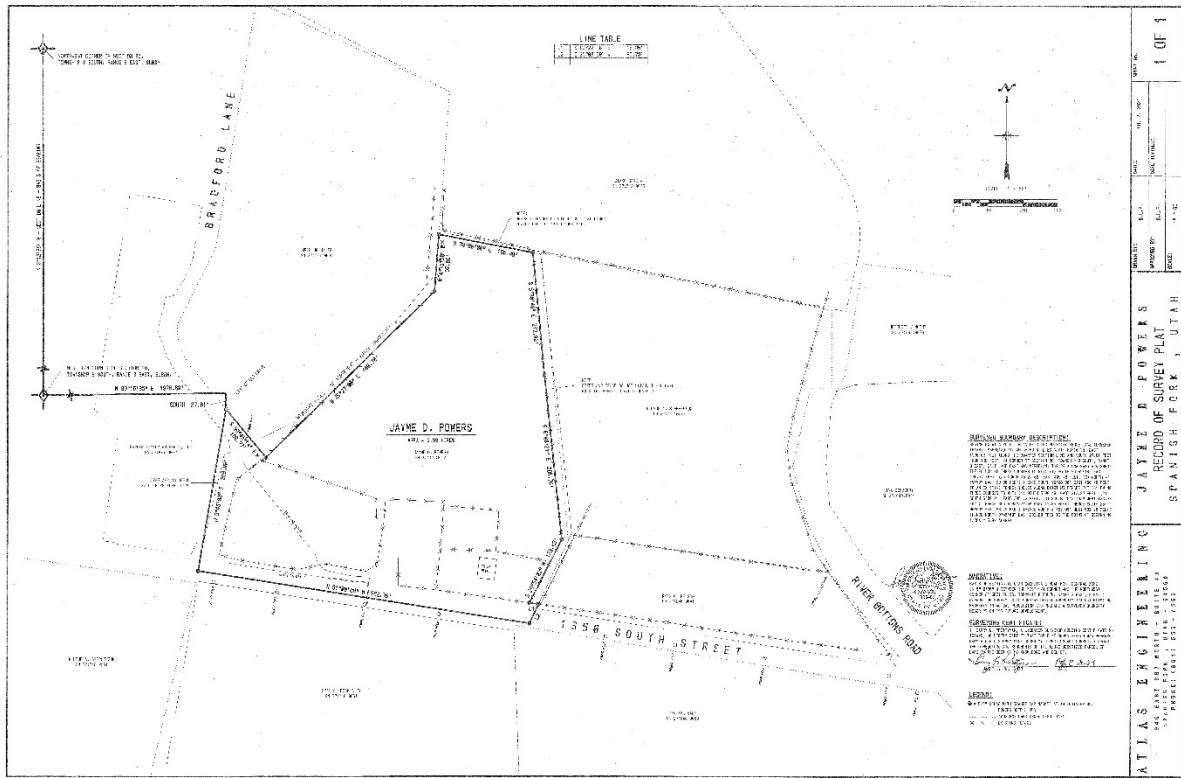


STREET NO. **PP-05**

1330 S  
STA. 10+00 TO STA. 13+74  
SPANISH FORK, UTAH

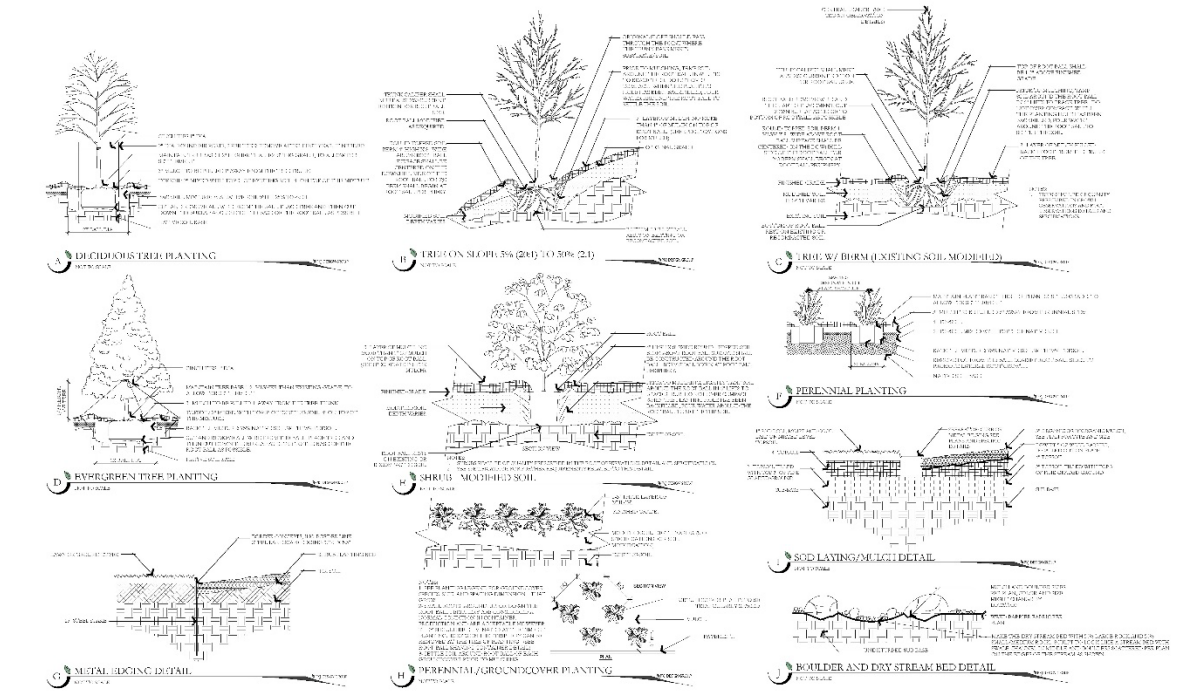
WYNDSONG SUBDIVISION  
ATLAS ENGINEERING  
CIVIL - STRUCTURAL - SURVEY











DATE: 8/23/2023	PROJECT NUMBER: UT230062	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 1	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 2	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 3	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 4	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 5	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 6	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 7	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 8	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 9	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501
NO. 10	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: LP-501



DATE: 8/23/2023	PROJECT NUMBER: UT230062	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 1	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 2	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 3	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 4	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 5	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 6	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 7	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 8	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 9	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100
NO. 10	DATE: 8/23/2023	PROJECT ADDRESS: 811 S. W. 1000 E. SPANISH FORK, UT 84601	CLIENT: JAMES SANDERS	DESIGNER: PKJ DESIGN GROUP	PERMIT NUMBER: IR-100

IRRIGATION PLAN SPECIFICATIONS

1.1. GENERAL

1.2. IRRIGATION SYSTEM

1.3. IRRIGATION SCHEDULE

1.4. IRRIGATION CONTROLLER

1.5. IRRIGATION PIPING

1.6. IRRIGATION VALVES

1.7. IRRIGATION HEADS

1.8. IRRIGATION MAINTENANCE

1.9. IRRIGATION RECORDS

1.10. IRRIGATION INSPECTION

1.11. IRRIGATION ADJUSTMENT

1.12. IRRIGATION REPAIR

1.13. IRRIGATION REPLACEMENT

1.14. IRRIGATION REMOVAL

1.15. IRRIGATION DEMOLITION

1.16. IRRIGATION RESTORATION

1.17. IRRIGATION PROTECTION

1.18. IRRIGATION SAFETY

1.19. IRRIGATION SECURITY

1.20. IRRIGATION LIABILITY

1.21. IRRIGATION WARRANTY

1.22. IRRIGATION GUARANTEE

1.23. IRRIGATION INSURANCE

1.24. IRRIGATION BONDS

1.25. IRRIGATION PERMITS

1.26. IRRIGATION NOTICES

1.27. IRRIGATION ORDINANCES

1.28. IRRIGATION REGULATIONS

1.29. IRRIGATION STANDARDS

1.30. IRRIGATION CODES

1.31. IRRIGATION RULES

1.32. IRRIGATION BYLAWS

1.33. IRRIGATION CHARTERS

1.34. IRRIGATION AGREEMENTS

1.35. IRRIGATION CONTRACTS

1.36. IRRIGATION DEEDS

1.37. IRRIGATION EASEMENTS

1.38. IRRIGATION ENCUMBRANCES

1.39. IRRIGATION LIENS

1.40. IRRIGATION MORTGAGES

1.41. IRRIGATION TRUSTS

1.42. IRRIGATION WILLS

1.43. IRRIGATION PROBATES

1.44. IRRIGATION ESTATES

1.45. IRRIGATION INHERITANCES

1.46. IRRIGATION SUCCESSIONS

1.47. IRRIGATION ADMINISTRATIONS

1.48. IRRIGATION EXECUTORSHIPS

1.49. IRRIGATION FIDUCIARIES

1.50. IRRIGATION TRUSTEES

1.51. IRRIGATION MANAGERS

1.52. IRRIGATION SUPERVISORS

1.53. IRRIGATION ASSISTANTS

1.54. IRRIGATION EMPLOYEES

1.55. IRRIGATION CONTRACTORS

1.56. IRRIGATION SUBCONTRACTORS

1.57. IRRIGATION VENDORS

1.58. IRRIGATION SUPPLIERS

1.59. IRRIGATION DISTRIBUTORS

1.60. IRRIGATION RETAILERS

1.61. IRRIGATION WHOLESALE

1.62. IRRIGATION EXPORTS

1.63. IRRIGATION IMPORTS

1.64. IRRIGATION TRADES

1.65. IRRIGATION INDUSTRIES

1.66. IRRIGATION SECTORS

1.67. IRRIGATION MARKETS

1.68. IRRIGATION EXCHANGES

1.69. IRRIGATION COMMODITIES

1.70. IRRIGATION SERVICES

1.71. IRRIGATION PRODUCTS

1.72. IRRIGATION GOODS

1.73. IRRIGATION MERCHANDISE

1.74. IRRIGATION SUPPLIES

1.75. IRRIGATION EQUIPMENT

1.76. IRRIGATION TOOLS

1.77. IRRIGATION MACHINERY

1.78. IRRIGATION VEHICLES

1.79. IRRIGATION TRANSPORT

1.80. IRRIGATION LOGISTICS

1.81. IRRIGATION DISTRIBUTION

1.82. IRRIGATION SALES

1.83. IRRIGATION MARKETING

1.84. IRRIGATION PROMOTION

1.85. IRRIGATION ADVERTISING

1.86. IRRIGATION PUBLICITY

1.87. IRRIGATION RELATIONS

1.88. IRRIGATION COMMUNICATIONS

1.89. IRRIGATION INFORMATION

1.90. IRRIGATION DATA

1.91. IRRIGATION ANALYTICS

1.92. IRRIGATION RESEARCH

1.93. IRRIGATION DEVELOPMENT

1.94. IRRIGATION INNOVATION

1.95. IRRIGATION TECHNOLOGY

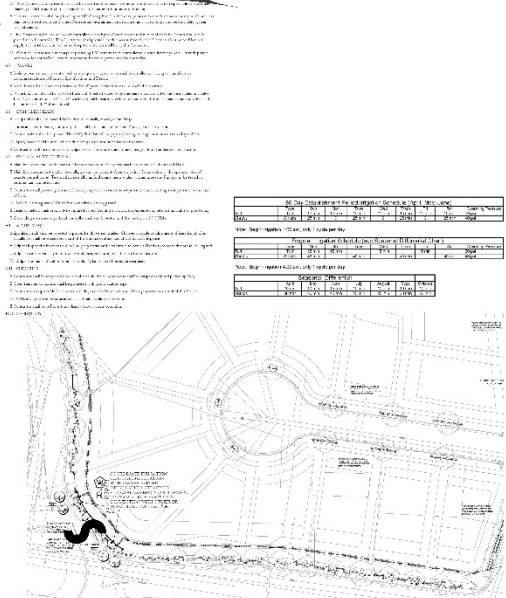
1.96. IRRIGATION SOFTWARE

1.97. IRRIGATION HARDWARE

1.98. IRRIGATION NETWORKS

1.99. IRRIGATION SYSTEMS

1.100. IRRIGATION SOLUTIONS



8" 150' SLOPE PER 100' (SLOPE) SCHEDULE 40		8" 150' SLOPE PER 100' (SLOPE) SCHEDULE 40	
LINE	LENGTH	LINE	LENGTH
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3	100	3	100
4	100	4	100
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8/23/2023 UTZ3952

NO. REGION DATE 8/11/2023

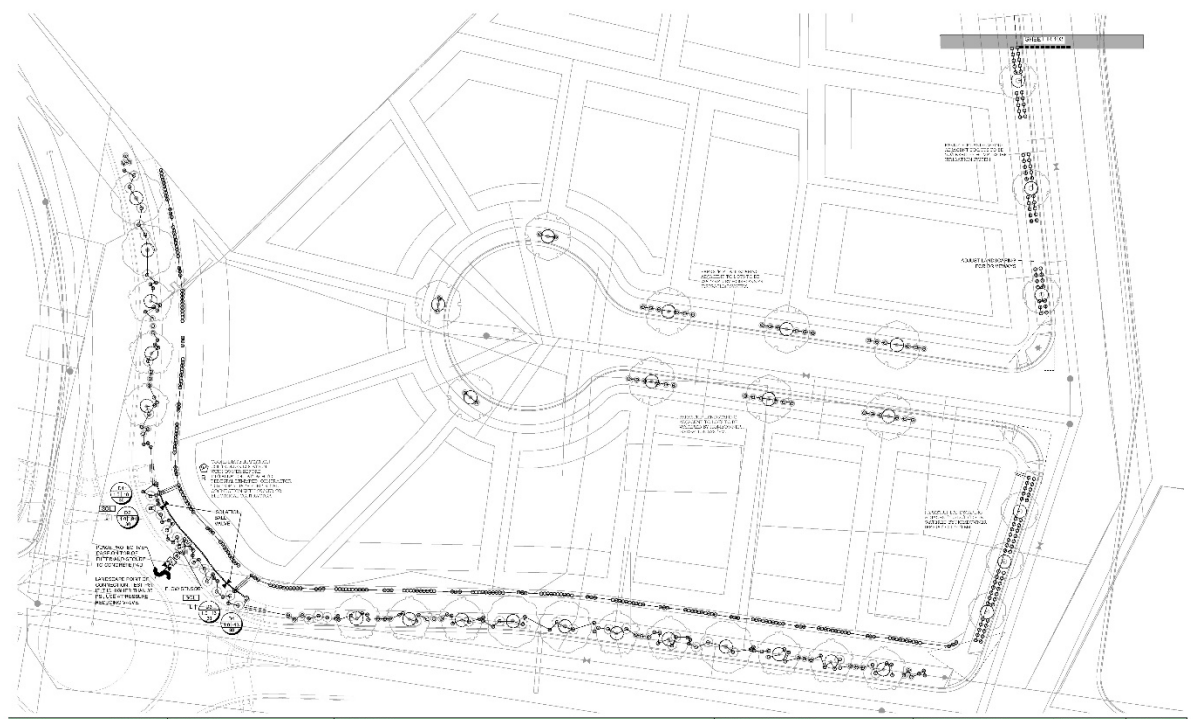
811 UTILITY LOCATIONS

WYNDSONG SUBDIVISION  
BRADFORD I.N. & SOUTH I.N.  
SPANISH FORK, UTAH

JAMIE SANDERS  
JAMESANDERS@PKJ.COM

PKJ DESIGN GROUP  
1500 N. 1000 E. SUITE 100  
SPRINGVILLE, UT 84779  
PH: 435.766.1000  
WWW.PKJDESIGN.COM

IR-101



8/23/2023 UTZ3952

NO. REGION DATE 8/11/2023

811 UTILITY LOCATIONS

WYNDSONG SUBDIVISION  
BRADFORD I.N. & SOUTH I.N.  
SPANISH FORK, UTAH

JAMIE SANDERS  
JAMESANDERS@PKJ.COM

PKJ DESIGN GROUP  
1500 N. 1000 E. SUITE 100  
SPRINGVILLE, UT 84779  
PH: 435.766.1000  
WWW.PKJDESIGN.COM

IR-102





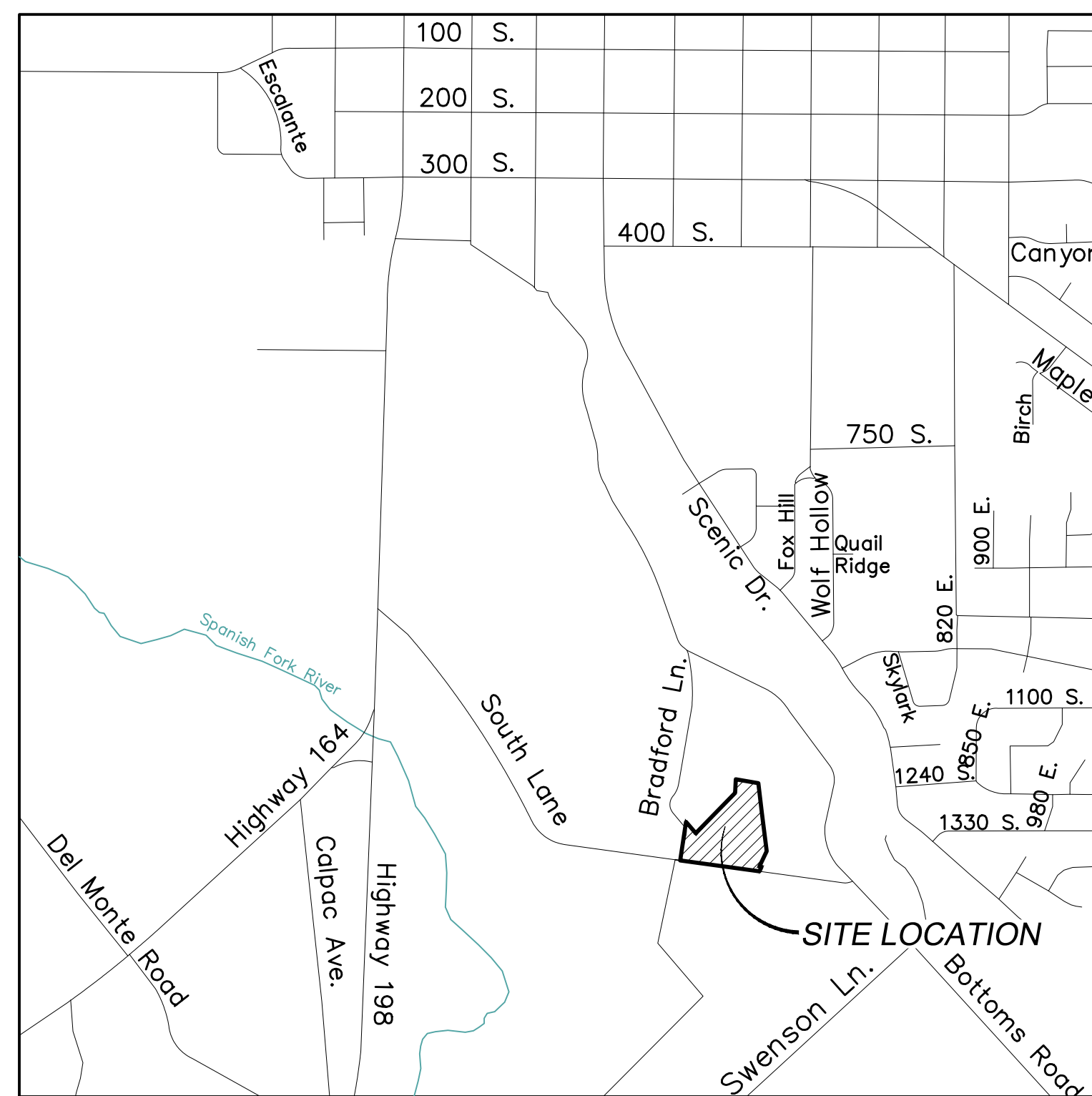
# WYNDSONG SUBDIVISION

A RESIDENTIAL SUBDIVISION

SPANISH FORK, UTAH

FINAL PLAN SET

## -SHEET INDEX-



VICINITY MAP

-NTS-

### SHEET

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- PP-06
- DT-01
- ROS

### SHEET NAME

- COVER
- FINAL PLAT
- UTILITY & INDEX
- EXISTING TOPOGRAPHY
- DRAINAGE PLAN
- ELECTRICAL PLAN
- PLAN & PROFILE - 1350 SOUTH - STA. 10+00 TO 13+50
- PLAN & PROFILE - 1350 SOUTH - STA. 13+50 TO 18+05
- PLAN & PROFILE - BRADFORD LANE - STA. 10+00 TO 13+00
- PLAN & PROFILE - STREET 1 - STA. 10+00 TO 15+00
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- PLAN & PROFILE - STREET 3 - STA. 10+00 TO 12+53
- DETAIL SHEET
- RECORD OF SURVEY

#### GENERAL NOTES:

1. CONTRACTOR TO FIELD VERIFY HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING UTILITIES PRIOR TO COMMENCEMENT OF CONSTRUCTION, AND REPORT ANY DISCREPANCIES TO THE ENGINEER.
2. ANY AND ALL DISCREPANCIES IN THESE PLANS ARE TO BE BROUGHT TO THE ENGINEER'S ATTENTION PRIOR TO COMMENCEMENT OF CONSTRUCTION.
3. BEFORE PROCEEDING WITH THIS WORK, THE CONTRACTOR SHALL CAREFULLY CHECK AND VERIFY ALL CONDITIONS, QUANTITIES, DIMENSIONS, AND GRADE ELEVATIONS, AND SHALL REPORT ALL DISCREPANCIES TO THE ENGINEER.
4. ALL DRINKING WATER AND PRESSURIZED IRRIGATION LINES UP TO AND INCLUDING THE METER, ALL SANITARY SEWER MAINS, ALL ELECTRICAL METERS, AND ALL ELECTRIC AND SFCN COMMUNICATION SERVICE LINES UP TO THE MAST ON OVERHEAD INSTALLATIONS AND TO THE TOP OF THE METER BASE FOR UNDERGROUND INSTALLATIONS ARE DEDICATED TO SPANISH FORK CITY.
5. ALL CONSTRUCTION WILL CONFORM TO SPANISH FORK CITY CONSTRUCTION STANDARDS.

#### CONTRACTOR NOTE:

THE SIZE, ELEVATION, & LOCATIONS OF EXISTING IMPROVEMENTS AND UTILITIES SHOWN HEREON ARE ASSUMED AND APPROXIMATELY SHOWN BASED UPON THE FIELD DATA FROM THE SURVEY. ALL SIZES, LOCATIONS & ELEVATIONS ARE TO BE VERIFIED. IF THERE ARE DIFFERENCES OR DISCREPANCIES, ATLAS ENGINEERING, LLC NEEDS TO BE NOTIFIED BEFORE CONSTRUCTION. ATLAS ENGINEERING, LLC WILL NOT BE LIABLE OR RESPONSIBLE FOR REMOVAL, CONSTRUCTION, OR INSTALLATION OF IMPROVEMENTS THAT ARE NOT IN ACCORDANCE WITH THESE PLANS. ANY AND ALL CHANGES OR VARIATIONS IN THE REMOVAL, CONSTRUCTION OR INSTALLATION OF THE IMPROVEMENTS MADE WITHOUT THE APPROVAL OF THE DESIGNER WILL RESULT IN SOLE LIABILITY TO THE CONTRACTOR. IN ADDITION, ATLAS ENGINEERING, LLC ASSUMES NO RESPONSIBILITY FOR ANY AND ALL EXISTING UTILITIES NOT SHOWN ON THIS PLAN AND ASSUMES NO LIABILITY FOR FAILURE TO EXACTLY LOCATE ALL EXISTING UTILITIES, SHOULD THERE BE INCIDENT.

#### SESD NOTE:

1. CONTRACTOR TO COORDINATE AND OBTAIN SESD RELOCATION DESIGN FOR ALL SESD POWER RELOCATION PRIOR TO PRE-CONSTRUCTION. CONTACT RYAN BAGLEY AT SESD (RYAN@SESDOFUTAH.ORG).



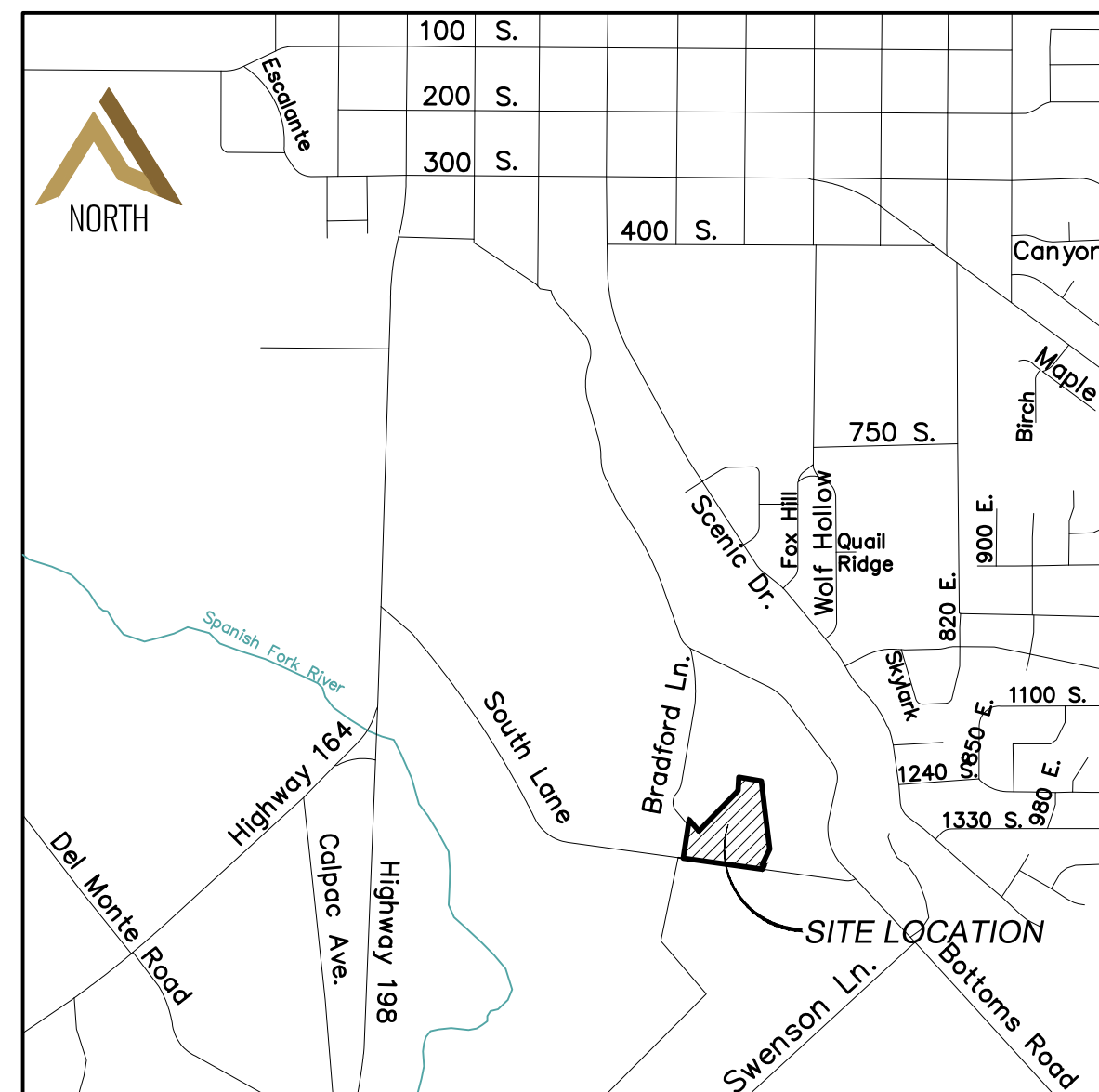
WYNDSONG SUBDIVISION



ATLAS ENGINEERING  
CIVIL · STRUCTURAL · SURVEY

PHONE: 801-655-0566  
946 E. 800 N. SUITE A  
SPANISH FORK, UT 84660

OWNER/DEVELOPER  
MELISSA NELSON / JAMIE SANDERS  
mnelsonutahrealtor@gmail.com  
jamiesanders@kw.com  
801-636-5735



VICINITY MAP  
-NTS-

**NOTES**

1. VERTICAL DATA BASED ON NAVD 29.
2. COORDINATE SYSTEM = NAD83.
3. PROJECT TO BE COMPLETED IN 1 PHASE.
4. DEVELOPMENT IS TO BE A MASTER PLANNED DEVELOPMENT AS DESCRIBED IN CITY CODE 15.3.24.030.
5. HOME ELEVATIONS AND MASTER PLANNED DEVELOPMENT STANDARDS TO BE REVIEWED AT TIME OF BUILDING PERMITS. PLEASE REFER TO REQUIREMENTS IN CITY CODE 15.3.24.030-D. ARCHITECTURE.
6. THIS SUBDIVISION IS TO FOLLOW THE APPROVED TREE PLANTING PLAN SUBMITTED WITH THE PLAT AND ASSOCIATED CITY TREE PLANTING REQUIREMENTS.

**OWNER/DEVELOPER**

MELISSA NELSON / JAMIE SANDERS  
mnelsonutahrealtor@gmail.com  
jamiesanders@kw.com  
801-636-5735

**DATA TABLE**

TOTAL ACREAGE=5.98  
TOTAL # OF LOTS=19  
TOTAL # OF LOTS PER ACRE=3.18  
ACREAGE OF LOTS=3.66 ACRES  
AVERAGE LOT SIZE= 8,394 SQ. FT.  
ACREAGE IN ROADS=2.32 ACRES  
TOTAL ACREAGE OF OPEN SPACE/PONDS=0%  
% OF OPEN SPACE=0%  
ZONING=R-1-8

**SURVEYOR'S CERTIFICATE**

I, DAVID F. HUNT DO HEREBY CERTIFY THAT I AM A PROFESSIONAL LAND SURVEYOR, AND THAT I HOLD CERTIFICATE NO. 5243543 AS PRESCRIBED UNDER THE LAWS OF THE STATE OF UTAH. I FURTHER CERTIFY BY AUTHORITY OF THE OWNERS, I HAVE MADE A SURVEY OF SAID TRACT OF LAND SHOWN ON THIS PLAT AND DESCRIBED BELOW, AND HAVE SUBDIVIDED SAID TRACT OF LAND INTO LOTS, STREETS, AND EASEMENTS AND THAT THE SAME HAS BEEN CORRECTLY SURVEYED AND STAKED ON THE GROUND AS SHOWN ON THIS PLAT AND THAT THIS IS TRUE AND CORRECT.

DATE

**BOUNDARY DESCRIPTION**

BEGINNING AT A POINT WHICH LIES N89°16'35"E 1970.69 FEET ALONG THE QUARTER SECTION LINE AND SOUTH 27.81 FEET FROM THE WEST 1/4 CORNER OF SECTION 30, TOWNSHIP 8 SOUTH, RANGE 3 EAST, SALT LAKE BASE AND MERIDIAN; AND RUNNING THENCE S39°45'14"E 108.77 FEET; THENCE N45°27'00"E 409.76 FEET; THENCE N04°15'50"E 99.06 FEET; THENCE S79°00'00"E 166.40 FEET; THENCE S05°30'48"E 212.92 FEET; THENCE S06°08'21"E 285.76 FEET; THENCE S26°54'02"W 123.63 FEET; THENCE S82°22'16"E 15.85 FEET; THENCE S25°00'00"W 36.78 FEET; THENCE N81°00'00"W 582.76 FEET; THENCE N09°45'00"E 283.80 FEET TO THE POINT OF BEGINNING.  
CONTAINS 5.98 ACRES.

**OWNER'S DEDICATION**

(I)WE, \_\_\_\_\_ BEING THE UNDERSIGNED OWNER(S) OF ALL THE PROPERTY DESCRIBED IN THE SURVEYOR'S CERTIFICATE HEREON AND SHOWN ON THIS MAP, HAVE CAUSED THE SAME TO BE SUBDIVIDED INTO LOTS, STREETS, AND EASEMENTS AND DO HEREBY DEDICATE THE STREETS AND OTHER PUBLIC AREAS AS INDICATED HEREON FOR PERPETUAL USE OF THE PUBLIC.

IN WITNESS WHEREOF WE HAVE HEREUNTO SET OUR HANDS(S) THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D. 2023.

MEMBER: \_\_\_\_\_

MEMBER: \_\_\_\_\_

MEMBER: \_\_\_\_\_

**CORPORATE ACKNOWLEDGMENT**

STATE OF UTAH \_\_\_\_\_ S.S.  
COUNTY OF UTAH \_\_\_\_\_  
ON THE \_\_\_\_\_ DAY OF \_\_\_\_\_, A.D. 2023 PERSONALLY APPEARED BEFORE ME \_\_\_\_\_ WHOSE IDENTITY IS PERSONALLY KNOWN TO ME OR PROVEN IN THE BASIS OF SATISFACTORY EVIDENCE AND WHO BY ME DULY SWORN/AFFIRED, DID SAY THAT THEY ARE THE \_\_\_\_\_ OF \_\_\_\_\_ AND THAT SAID DOCUMENT WAS SIGNED BY THEM IN BEHALF OF SAID \_\_\_\_\_ BY AUTHORITY OF ITS BYLAWS, OR RESOLUTION OF ITS BOARD OF DIRECTORS, AND SAID \_\_\_\_\_ ACKNOWLEDGED TO ME THAT SAID \_\_\_\_\_ EXECUTED THE SAME.

\_\_\_\_\_ A NOTARY PUBLIC COMMISSIONED IN THE STATE OF UTAH

COMMISSION NUMBER / EXPIRES \_\_\_\_\_ PRINTED FULL NAME OF NOTARY \_\_\_\_\_

**ACCEPTANCE BY LEGISLATIVE BODY**

THE \_\_\_\_\_ OF \_\_\_\_\_ COUNTY OF UTAH, APPROVES THIS SUBDIVISION AND HEREBY ACCEPTS THE DEDICATION OF ALL STREETS, EASEMENTS, AND OTHER PARCELS OF LAND INTENDED FOR PUBLIC PURPOSES FOR THE PERPETUAL USE OF THE PUBLIC THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ A.D. 2023.

APPROVED \_\_\_\_\_ APPROVED \_\_\_\_\_

CITY MANAGER CITY ATTORNEY

APPROVED \_\_\_\_\_ ATTEST \_\_\_\_\_

ENGINEER (SEE SEAL) CLERK-RECORDER

APPROVED \_\_\_\_\_

COMMUNITY DEVELOPMENT DIRECTOR

**WYNDSONG SUBDIVISION**

A RESIDENTIAL SUBDIVISION IN  
SPANISH FORK, UTAH COUNTY, UTAH

CONTAINING 19 LOTS AND 5.98 ACRES.  
LOCATED IN THE WEST 1/2 CORNER OF SECTION 30, TOWNSHIP 8 SOUTH, RANGE 3 EAST, SALT LAKE BASE AND MERIDIAN, UTAH COUNTY, UTAH.

**SINGLE FAMILY HOME SETBACKS**

INTERIOR LOTS:  
FRONT SETBACK-20' TO 25' (20' TO LIVING AREAS, 25 FEET TO GARAGES OR CARPORTS, AND 20' TO THE FRONT OF SIDE ENTRY GARAGE)

REAR SETBACK-25'

SIDE SETBACK-5'-10'-TOTAL OF 15'

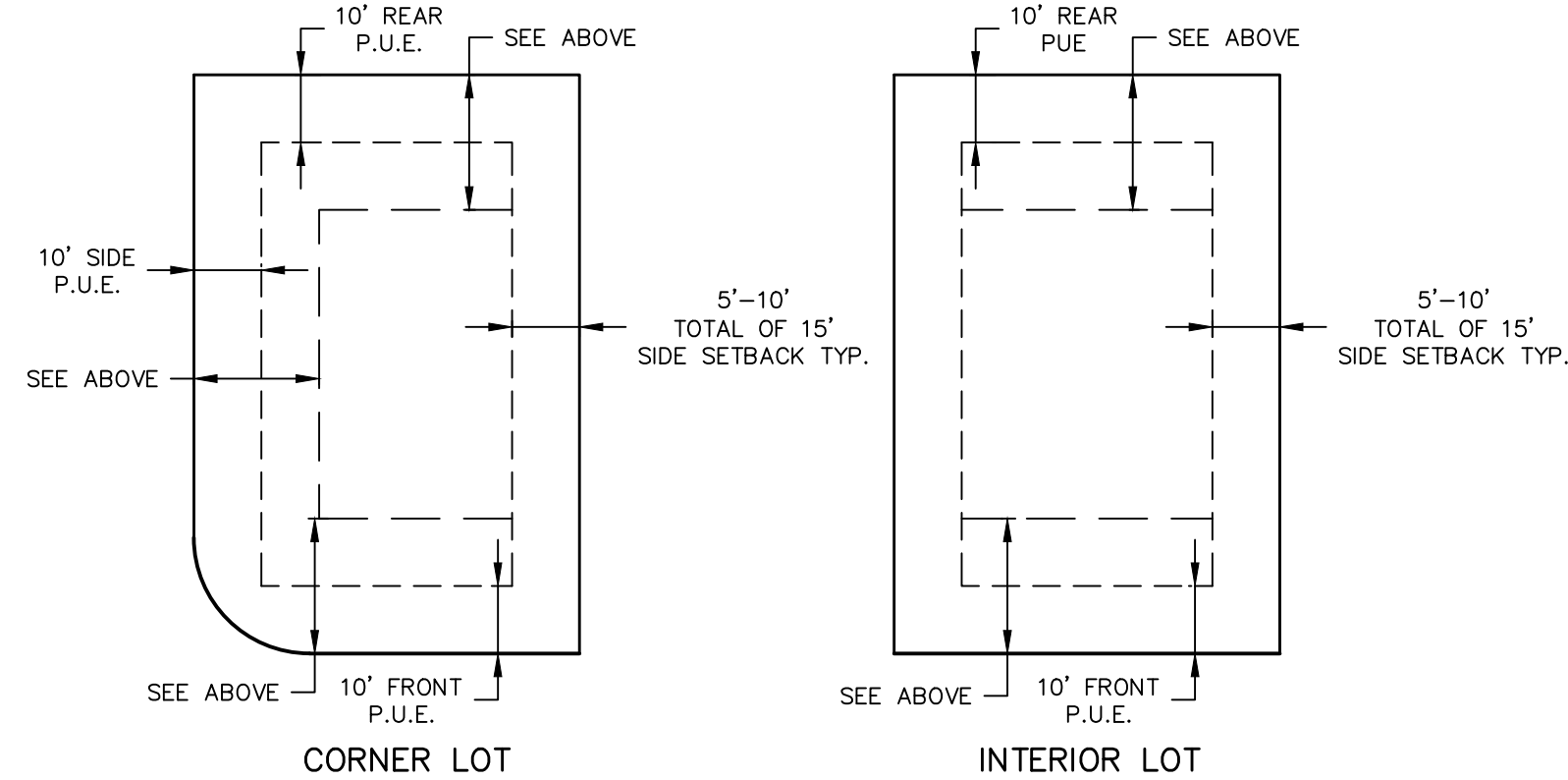
CORNER LOTS:

FRONT SETBACK-20' TO 25' (20' TO LIVING AREAS, 25 FEET TO GARAGES OR CARPORTS, AND 20' TO THE FRONT OF SIDE ENTRY GARAGE)

REAR SETBACK-25' (ON CORNER LOTS, SETBACK IS REDUCED BY 5 FEET)

INTERIOR SIDE-10'

ROAD SIDE-15' TO 25' (15' TO LIVING AREAS, 25' TO GARAGES OR CARPORTS, AND 20' TO THE FRONT OF THE SIDE ENTRY GARAGE)



DETAIL-TYP. BUILDING SETBACK & EASEMENT  
-NTS-

**LEGEND**





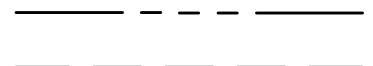
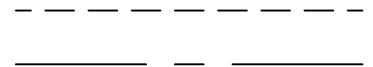
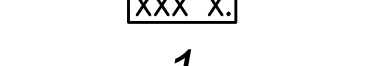






- SECTION CORNER
- FOUND ALUMINUM CAP
- SET 5/8" IRON PIN
- CALCULATED POINT, NOT SET
- EXISTING POWER POLE
- PROPOSED STREET LIGHT
- PROPOSED FIRE HYDRANT
- PROPOSED SIGN
- PROPOSED STREET SIGN
- PROPERTY BOUNDARY
- CENTERLINE
- RIGHT-OF-WAY LINE
- LOT LINE
- SECTION LINE
- EASEMENT
- EXISTING DEED LINE
- EDGE OF PAVEMENT
- EXISTING OVER HEAD POWER
- EXISTING FENCE LINE
- EXISTING DITCH
- EXISTING SANITARY SEWER W/MANHOLE
- PROPOSED STORM DRAIN
- PROPOSED PVC SDR-35 SEWER W/MH
- PROPOSED CULINARY WATERLINE
- PROPOSED PRESSURIZED IRRIGATION
- SETBACK

LINE	DIRECTION	LENGTH
L1	S82°22'16"E	15.85'
L2	S25°00'00"W	36.78'
L3	S81°46'27"E	14.05'
L4	S82°01'46"E	25.37'
L5	N57°56'08"E	12.87'
L6	N39°49'15"E	28.52'
L7	S5°20'38"W	28.79'
L8	S5°20'38"W	4.72'
L9	S82°01'46"E	14.90'
L10	N81°26'12"W	12.88'
L11	S81°26'12"E	15.12'
L12	N25°00'00"E	7.61'

CURVE	LENGTH	RADIUS	CHORD DIST.	CHORD BRG.	DELTA
C1	88.92'	300.00'	88.60'	N0°31'14"W	16°58'57"
C2	62.94'	150.00'	62.48'	N5°52'50"E	24°02'24"
C3	127.49'	354.00'	126.81'	N4°58'25"W	20°38'06"
C4	45.08'	50.00'	43.57'	S20°29'10"E	51°39'35"
C5	23.02'	98.50'	22.97'	N39°37'16"W	13°23'22"
C6	42.35'	43.50'	40.70'	S60°49'00"E	55°46'49"
C7	30.78'	264.00'	30.77'	N85°22'11"W	6°40'51"
C8	17.34'	10.00'	15.25'	N31°46'05"W	99°20'14"
C9	14.20'	15.00'	13.68'	S71°26'39"W	54°14'18"
C10	57.82'	62.00'	55.75'	S71°02'26"W	53°25'52"
C11	34.70'	62.00'	34.25'	N66°12'41"W	32°03'53"
C12	34.70'	62.00'	34.25'	N34°08'48"W	32°03'53"
C13	34.70'	62.00'	34.25'	N2°04'55"W	32°03'53"
C14	47.94'	62.00'	46.76'	N36°06'13"E	44°18'24"
C15	44.70'	62.00'	43.74'	N78°54'36"E	41°18'24"
C16	57.61'	62.00'	55.56'	S53°49'03"E	53°14'18"
C17	14.20'	15.00'	13.68'	S54°19'03"E	54°14'18"
C18	18.27'	10.00'	15.84'	N46°12'43"E	104°42'10"
C19	13.20'	10.00'	12.26'	N43°57'24"W	75°38'06"
C20	56.44'	48.00'	53.25'	N24°16'05"W	67°22'31"
C21	96.35'	48.00'	80.97'	S64°32'18"W	115°00'44"

CLERK-RECORDER SEAL	SURVEYOR'S SEAL	NOTARY PUBLIC SEAL	CITY ENGINEER SEAL	COUNTY RECORDER SEAL
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**LEGEND**

-  UTAH COUNTY MONUMENT
-  SET 5/8" REBAR & CAP
-  SET CURB PIN
-  CALCULATED POINT, NOT SET
-  PROPERTY BOUNDARY
-  RIGHT-OF-WAY LINE
-  LOT LINE
-  SECTION LINE
-  SETBACK
-  EASEMENT
-  CENTERLINE
-  ADDRESSES
-  PROPOSED LOT NUMBERS

**NOTES:**

1. BASEMENTS ARE NOT ALLOWED.
2. NO ACCESS IS ALLOWED FROM REAR OF LOTS ADJACENT TO BRADFORD LANE OR 1350 SOUTH (LOTS 13-19).
3. ALL MUNICIPAL UTILITY EASEMENTS PLATTED HEREON ARE IN PERPETUITY FOR INSTALLATION, MAINTENANCE, REPAIR, AND REPLACEMENT OF PUBLIC UTILITIES, PUBLIC WALLS, FENCES, SIDEWALKS, TRAILS, AND APPURTENANT PARTS THEREOF AND THE RIGHT TO REASONABLE ACCESS TO GRANTORS EASEMENT SHALL RUN WITH THE REAL PROPERTY AND SHALL BE BINDING UPON THE GRANTOR AND THE GRANTORS SUCCESSORS, HEIRS AND ASSIGNS.
4. ALL CULINARY WATER AND PRESSURIZED IRRIGATION LINES UP TO AND INCLUDING THE METER, ALL SANITARY SEWER MAIN, ALL ELECTRIC METERS, AND ALL ELECTRIC AND SFCN COMMUNICATION SERVICE LINES UP TO THE MAST ON OVERHEAD INSTALLATIONS AND TO THE TOP OF THE METER BASE FOR UNDERGROUND INSTALLATIONS ARE DEDICATED TO SPANISH FORK CITY.

NORTHWEST CORNER OF SECTION 30, TOWNSHIP 8 SOUTH, RANGE 3 EAST, SLB&M.

N00°12'30"W  
BASIS OF BEARING

WEST 1/4 CORNER OF SECTION 30, TOWNSHIP 8 SOUTH, RANGE 3 EAST, SLB&M.

SOUTH 27.81' POB

N89°16'35"E 1970.69'

WARNER FAMILY PROPERTY, LLC  
27:041:0077

WILBUR STEPHENSON  
27:040:0031

MEILING HALES  
27:041:0056

DIANE GARCIA  
27:042:0075

WILBUR STEPHENSON  
27:041:0044

RYAN NELSON  
27:040:0041

SPANISH FORK CITY  
27:040:0061

CENTRAL BANK  
27:040:0060



(24"x36")  
SCALE 1" = 40'  
(11"x17")  
SCALE 1" = 80'



5		DESIGNED BY:		DATE:	
4		DRAWN BY:		DATE:	
3		CHECKED BY:		DATE:	
2		APPROVED:		DATE:	
1		COGO FILE:		DATE:	
NO.	REVISIONS	BY	DATE	REV. COGO FILE:	DATE:

**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY

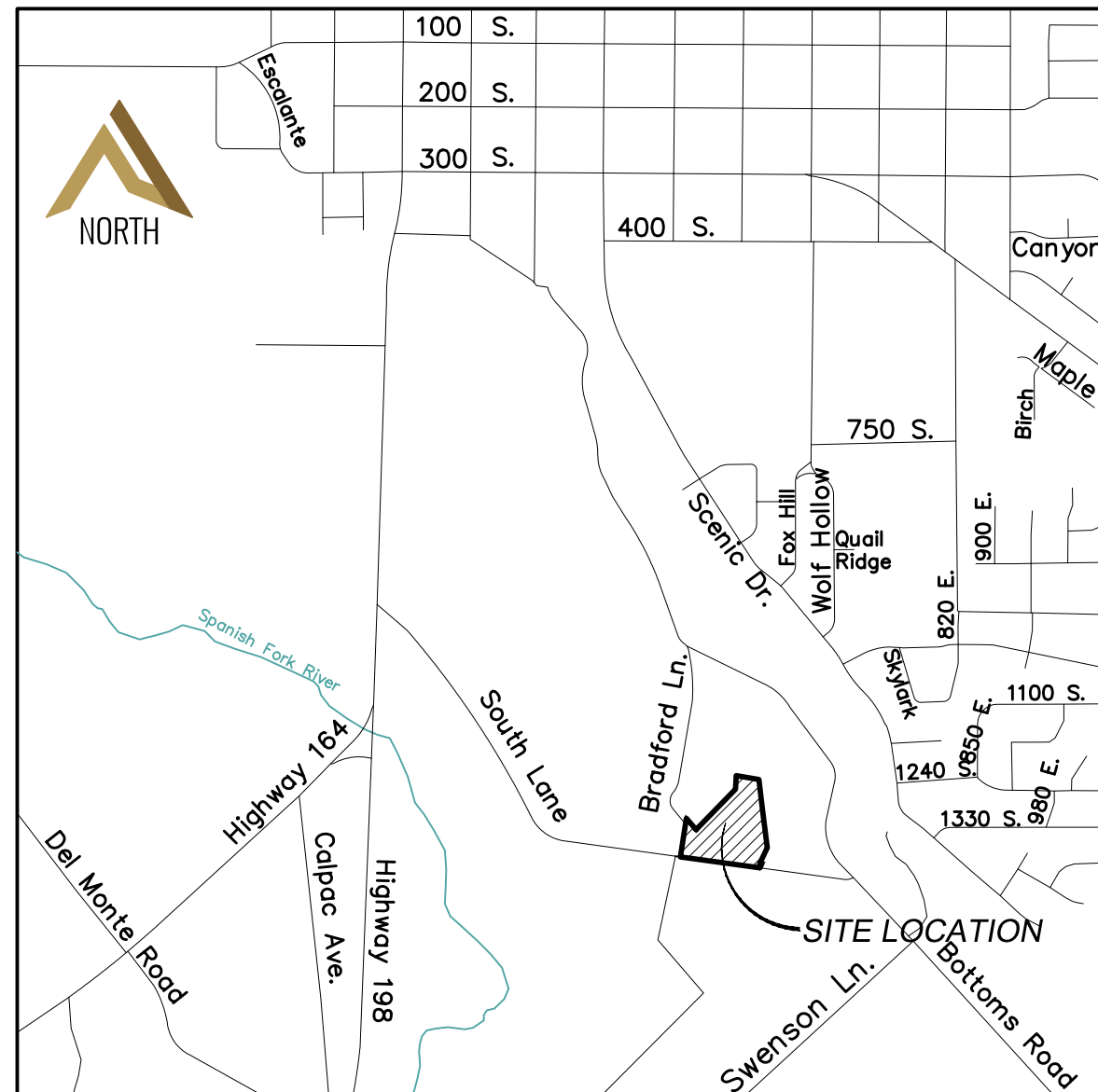
ENGINEER CONTACT INFO:  
ATLAS ENGINEERING  
PHONE: 801-655-0566  
946 EAST 800 NORTH SUITE A  
SPANISH FORK, UT 84606

**WYNDSONG SUBDIVISION**  
CONTAINING 19 LOTS AND 5.98 ACRES.  
LOCATED IN THE WEST 1/2 CORNER OF SECTION 30, TOWNSHIP 8 SOUTH,  
RANGE 3 EAST, SALT LAKE BASE AND MERIDIAN, UTAH COUNTY, UTAH.

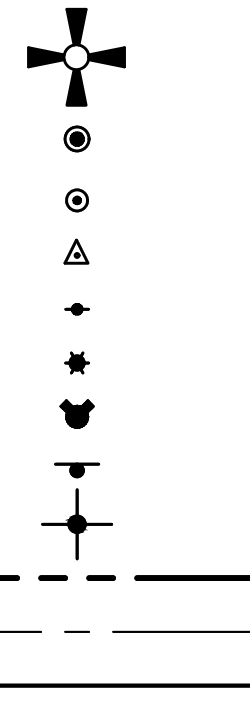
PLAT MAP  
SPANISH FORK, UTAH

SHEET  
2 OF 2

Z:\2019\19-125 POWERS SPANISH FORK\CADD\FINAL\02-FINAL PLAT.DWG



VICINITY MAP  
-NTS-

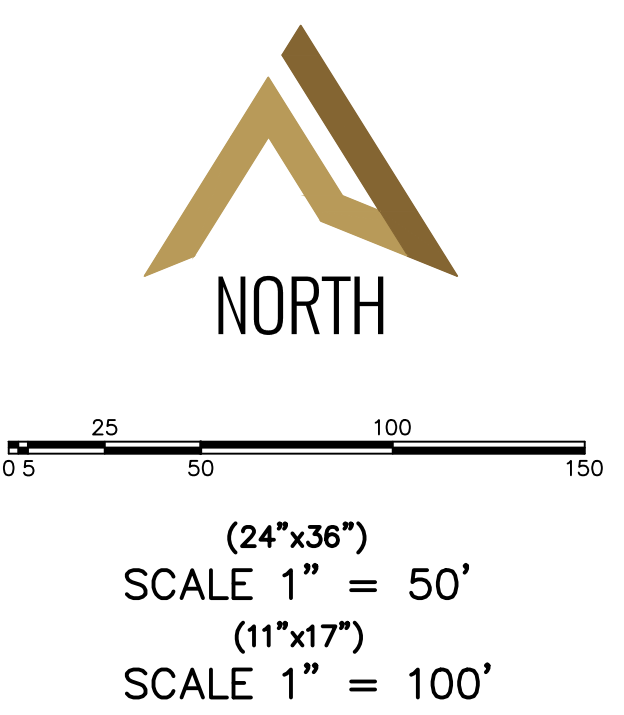
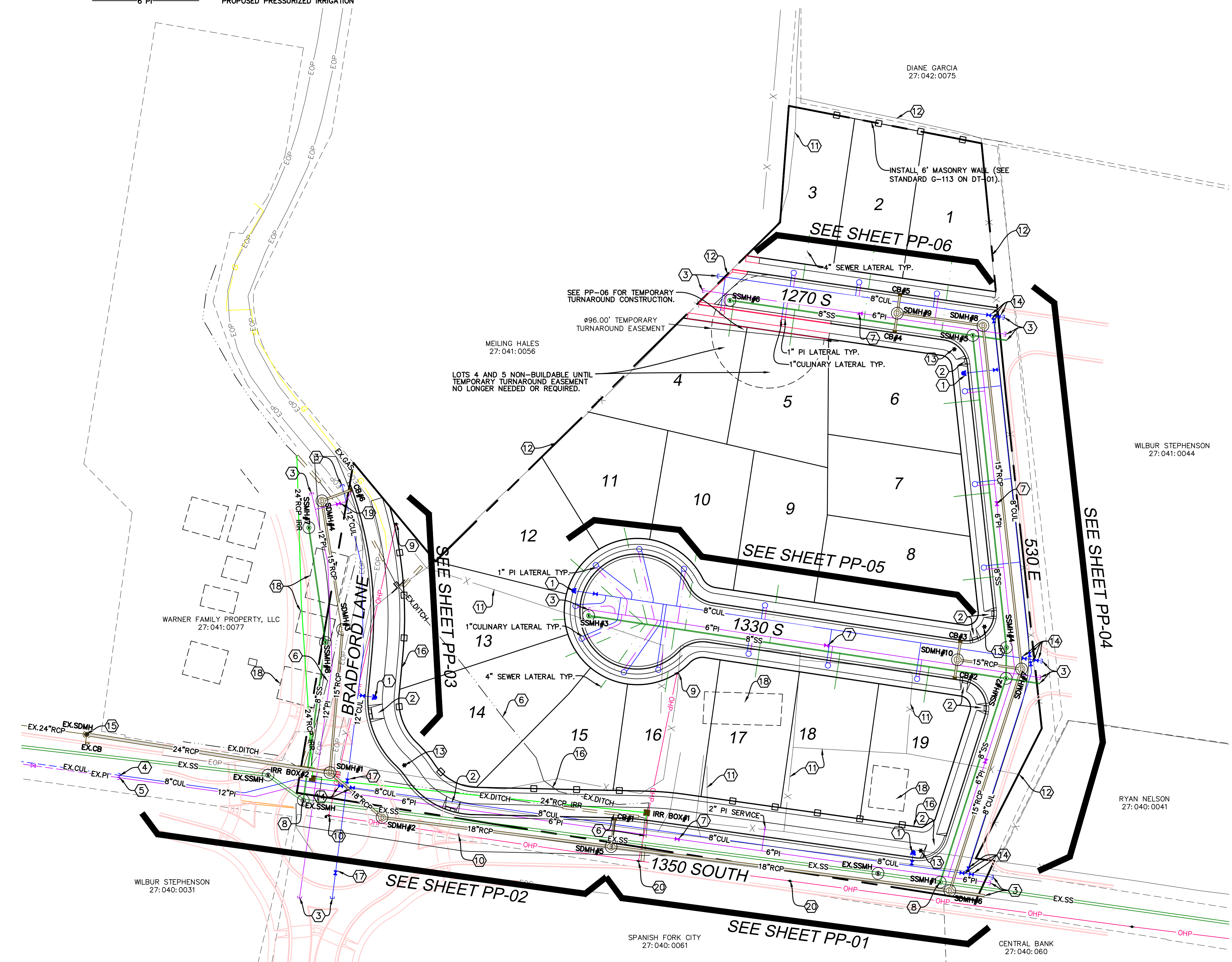


LEGEND	
SECTION CORNER	LOT LINE
FOUND ALUMINUM CAP	SECTION LINE
SET 5/8" IRON PIN	EASEMENT
CALCULATED POINT, NOT SET	EXISTING DEED LINE
EXISTING POWER POLE	EDGE OF PAVEMENT
PROPOSED STREET LIGHT	EXISTING OVER HEAD POWER
PROPOSED FIRE HYDRANT	EXISTING FENCE LINE
PROPOSED SIGN	EXISTING DITCH
PROPOSED STREET SIGN	EXISTING SANITARY SEWER W/MANHOLE
PROPERTY BOUNDARY	PROPOSED STORM DRAIN
CENTERLINE	PROPOSED PVC SDR-35 SEWER W/MH
RIGHT-OF-WAY LINE	PROPOSED CULINARY WATERLINE
	PROPOSED PRESSURIZED IRRIGATION

**NOTES:**  
 1. ALL LOTS TO HAVE A SEWER LATERAL, CULINARY WATER LATERAL, & PRESSURIZED IRRIGATION LATERAL. SEE DETAIL SHEET 5.  
 2. ALL DRIVEWAY ACCESS LOCATIONS FOR CORNER LOTS SHALL MEET SPANISH FORK CITY STANDARDS AND COMPLY WITH THE SPANISH FORK CITY TRANSPORTATION PLAN.  
 3. ALL FIRE HYDRANT LOCATIONS TO BE APPROVED BY THE SPANISH FORK CITY FIRE MARSHALL AT FINAL DESIGN.  
 4. CAP ON 92 UNITS UNTIL SECOND ACCESS TIES INTO RESIDENTIAL AREA. CAP ON 50 UNITS UNTIL CULINARY WATER IS LOOPEL.  
 5. WATERLINES ON STREET 2 TO SLOPE DOWN AT 0.5% TO THE CUL-DE-SAC. THIS IS SO AIR RUNS BACK TO THE MAIN LINES ON STREET 1.  
 6. ALL CULINARY WATER AND PRESSURIZED IRRIGATION LINES UP TO AND INCLUDING THE METER, ALL SANITARY SEWER MAIN, ALL ELECTRIC METERS, AND ALL ELECTRIC AND SFCN COMMUNICATION SERVICE LINES UP TO THE MAST ON OVERHEAD INSTALLATIONS AND TO THE TOP OF THE METER BASE FOR UNDERGROUND INSTALLATIONS ARE DEDICATED TO SPANISH FORK CITY.

**SESD NOTE:**  
 1. CONTRACTOR TO COORDINATE AND OBTAIN SESD RELOCATION DESIGN FOR ALL SESD POWER RELOCATION PRIOR TO PRE-CONSTRUCTION. CONTACT RYAN BAGLEY AT SESD (RYAN@SESDOFUTAH.ORG).

- CONSTRUCTION NOTES:**
- INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
  - ALL ADA PEDESTRIAN RAMPS TO MEET SPANISH FORK CITY CURRENT STANDARDS AND SHALL COMPLY WITH SPANISH FORK CITY TRANSPORTATION MASTER PLAN (TYPICAL).
  - CAP/PLUG AND MARK TO SURFACE.
  - LOCATE AND TIE TO EXISTING CULINARY WATERLINE.
  - LOCATE AND TIE TO EXISTING PRESSURIZED IRRIGATION WATERLINE.
  - EXISTING DITCH TO BE PIPED.
  - INSTALL 6" PI VALVE PER SPANISH FORK CITY STANDARDS.
  - LOCATE AND TIE TO EXISTING SEWER.
  - EXISTING POWER TO BE REMOVED.
  - EXISTING POWER TO BE RELOCATED.
  - EXISTING FENCE TO BE REMOVED.
  - EXISTING FENCE TO REMAIN.
  - INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
  - INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
  - LOCATE AND TIE TO EXISTING STORM DRAIN MANHOLE.
  - CONST. 6' MASONRY WALL. SEE DETAIL SHEET DT-02.
  - INSTALL 12" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
  - REMOVE EXISTING BUILDING.
  - INSTALL PI DRAIN PER SPANISH FORK CITY STANDARDS.
  - EXISTING POWER TO REMAIN.



SHEET NO. **3**

NO.	REVISIONS	BY	DATE
12			
11			
10			
9			
8			
7			
6			
5			
4			
3			
2			
1			

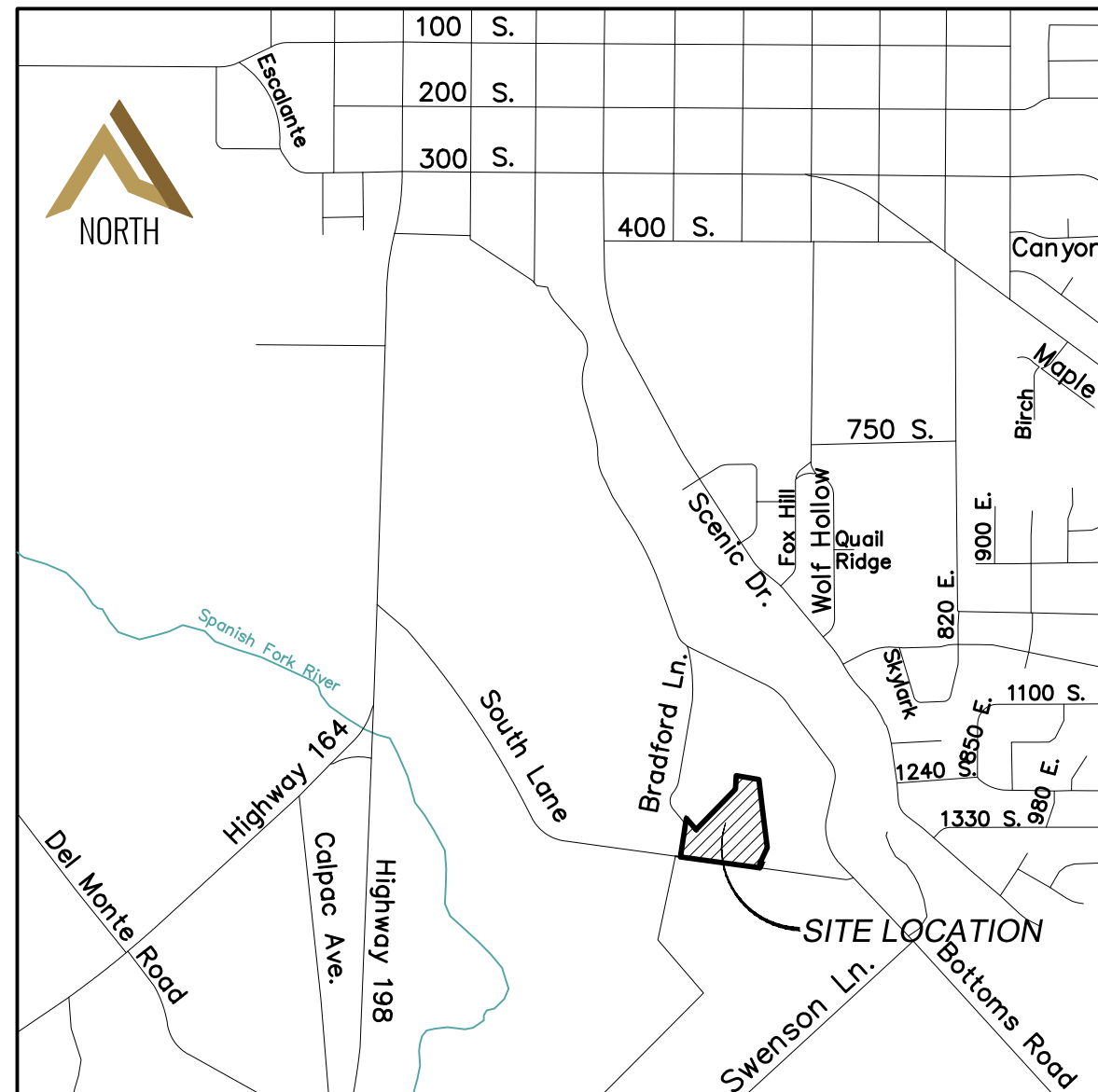
**UTILITY & INDEX**

**SPANISH FORK, UTAH**

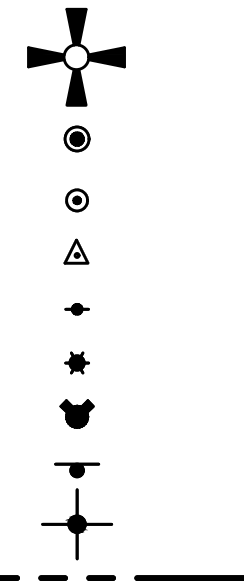
**WYNDSONG SUBDIVISION**

**ATLAS ENGINEERING**  
 CIVIL · STRUCTURAL · SURVEY

PHONE: 801-455-6566  
 946 E. 800 N. SUITE A  
 SPANISH FORK, UT 84660  
 2/1/2023 9:25:40 AM MST  
 2/1/2023 9:25:40 AM MST



VICINITY MAP  
-NTS-



**LEGEND**

SECTION CORNER	— — — — —	LOT LINE
FOUND ALUMINUM CAP	— — — — —	SECTION LINE
SET 5/8" IRON PIN	— — — — —	EASEMENT
CALCULATED POINT, NOT SET	— — — — —	EXISTING DEED LINE
EXISTING POWER POLE	— — — — —	EDGE OF PAVEMENT
PROPOSED STREET LIGHT	— — — — —	EXISTING OVER HEAD POWER
PROPOSED FIRE HYDRANT	— — — — —	EXISTING FENCE LINE
PROPOSED SIGN	— — — — —	EXISTING DITCH
PROPOSED STREET SIGN	— — — — —	EXISTING SANITARY SEWER W/MANHOLE
PROPERTY BOUNDARY	— — — — —	PROPOSED STORM DRAIN
CENTERLINE	— — — — —	PROPOSED PVC SDR-35 SEWER W/MH
RIGHT-OF-WAY LINE	— — — — —	PROPOSED CULINARY WATERLINE
		PROPOSED PRESSURIZED IRRIGATION

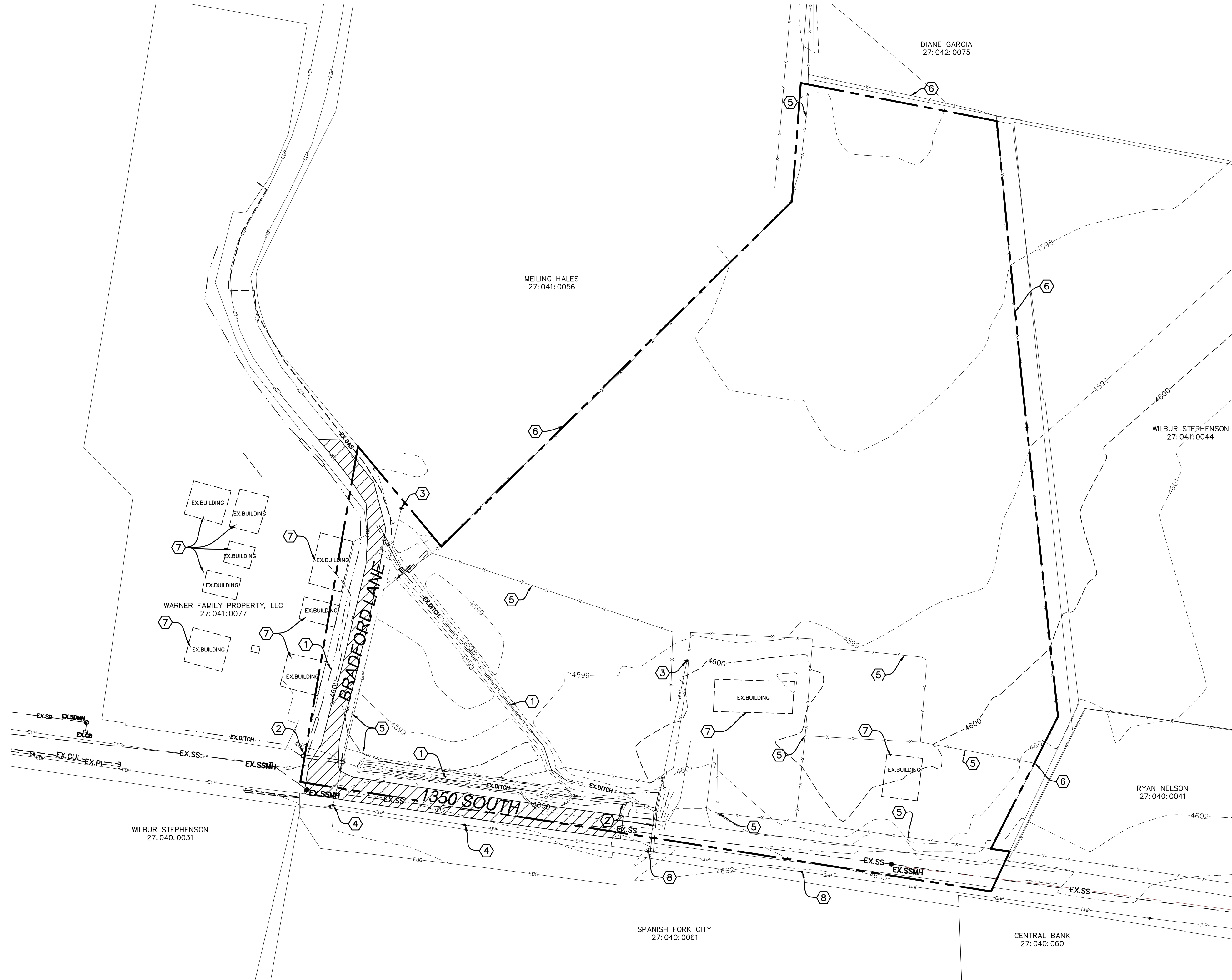
**CONSTRUCTION NOTES:**

- ① EXISTING DITCH TO BE PIPED.
- ② EXISTING IRRIGATION TO REMAIN.
- ③ EXISTING POWER TO BE REMOVED.
- ④ EXISTING POWER TO BE RELOCATED.
- ⑤ EXISTING FENCE TO BE REMOVED.
- ⑥ EXISTING FENCE TO REMAIN.
- ⑦ EXISTING BUILDING TO BE REMOVED.
- ⑧ EXISTING POWER TO REMAIN.

REPLACE EXISTING ASPHALT.



(24"x36")  
SCALE 1" = 50'  
(11"x17")  
SCALE 1" = 100'



SHEET NO.

4

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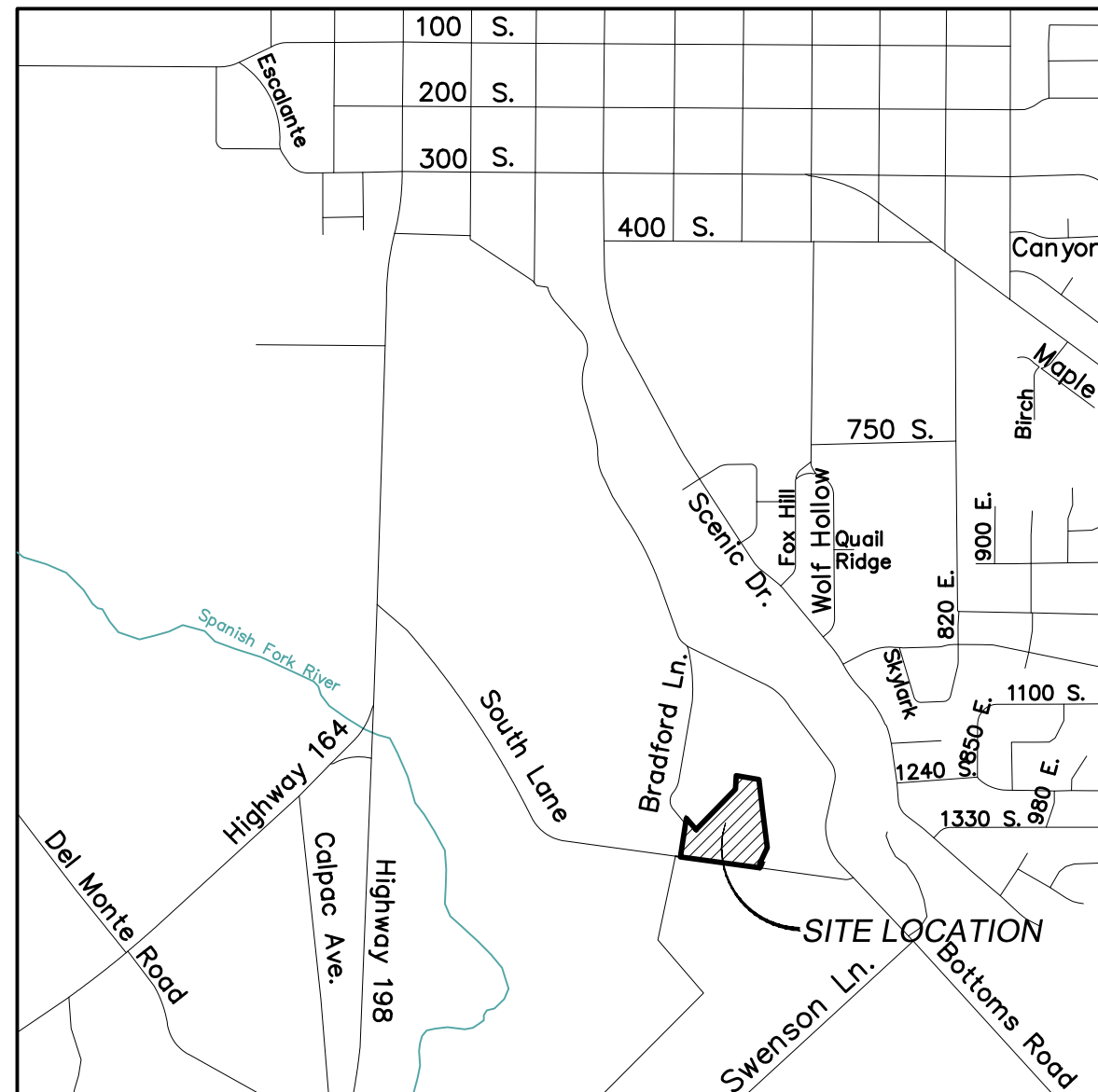
EXISTING TOPOGRAPHY

SPANISH FORK, UTAH

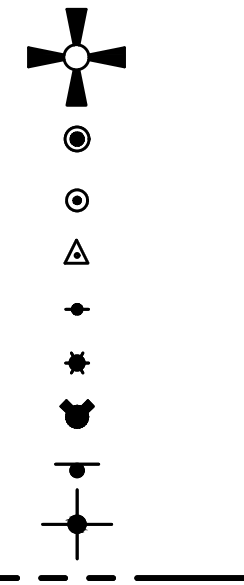
WYNDSONG SUBDIVISION

**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY



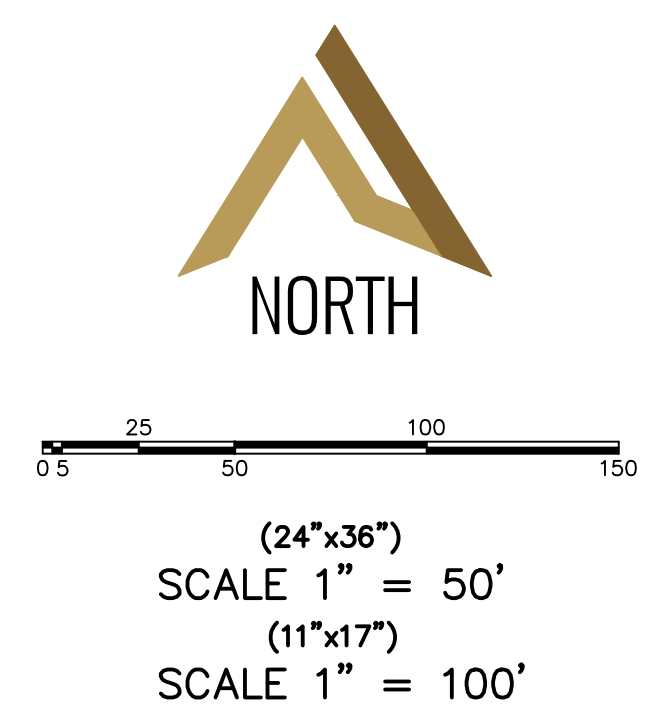
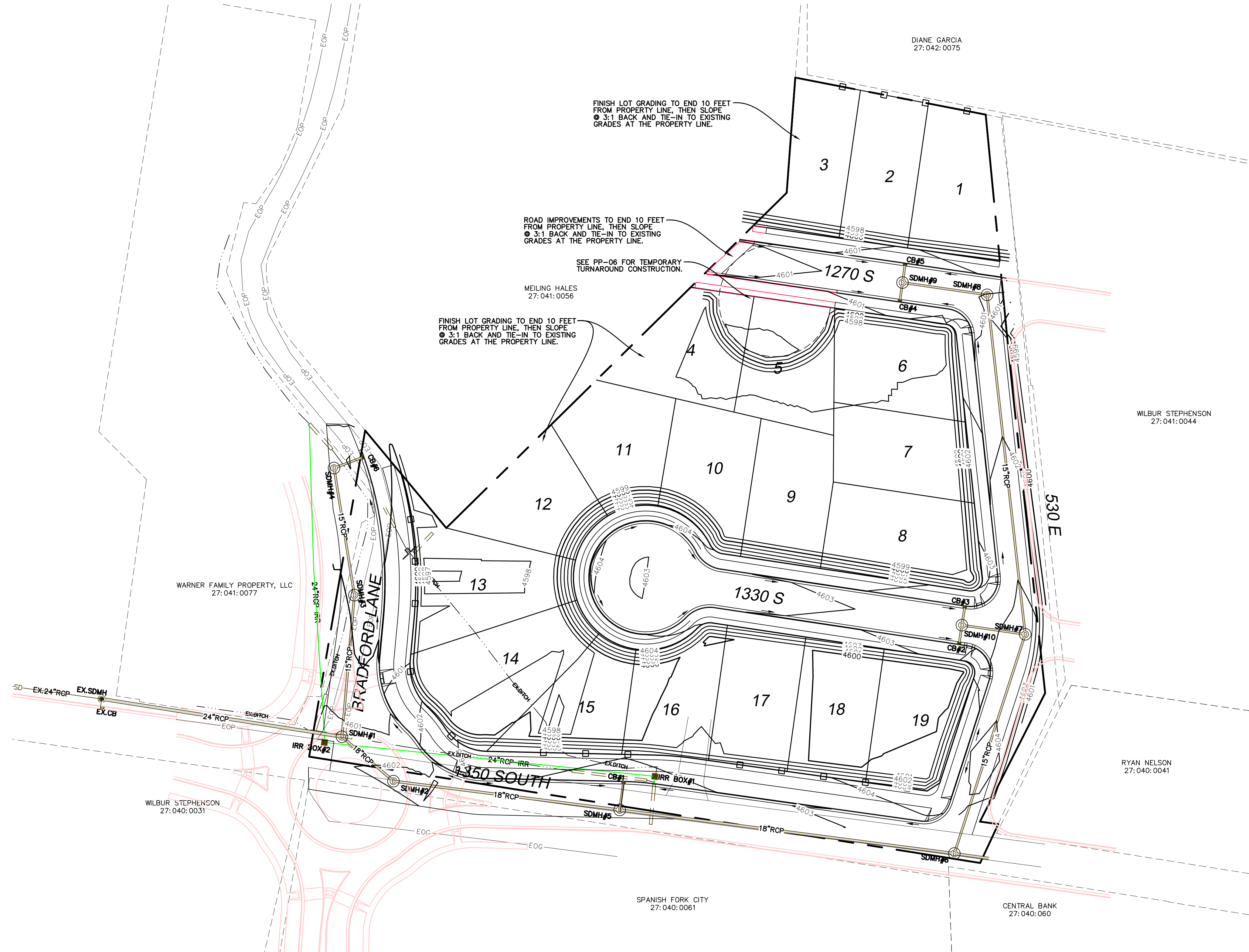


VICINITY MAP  
-NTS-



**LEGEND**

SECTION CORNER	—	LOT LINE
FOUND ALUMINUM CAP	---	SECTION LINE
SET 5/8" IRON PIN	---	EASEMENT
CALCULATED POINT, NOT SET	---	EXISTING DEED LINE
EXISTING POWER POLE	---	EDGE OF PAVEMENT
PROPOSED STREET LIGHT	---	EXISTING OVER HEAD POWER
PROPOSED FIRE HYDRANT	---	EXISTING FENCE LINE
PROPOSED SIGN	---	EXISTING DITCH
PROPOSED STREET SIGN	---	EXISTING SANITARY SEWER W/MANHOLE
PROPERTY BOUNDARY	---	PROPOSED STORM DRAIN
CENTERLINE	---	PROPOSED PVC SDR-35 SEWER W/MH
RIGHT-OF-WAY LINE	---	PROPOSED CULINARY WATERLINE
	---	PROPOSED PRESSURIZED IRRIGATION



SHEET NO. **5**

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GRADING/DRAINAGE PLAN  
SPANISH FORK, UTAH

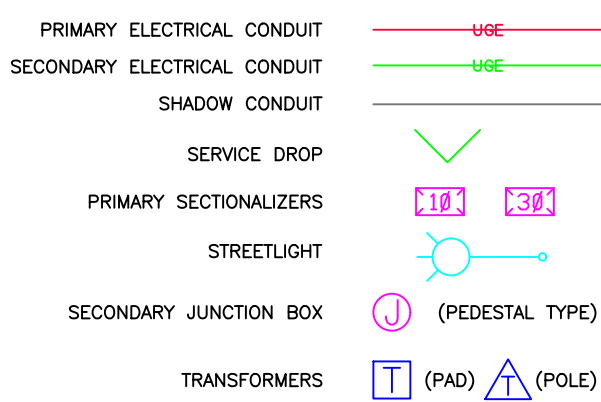
WYNDSONG SUBDIVISION

**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY

PHONE: 801-455-6566  
946 E. 800th SUITE A  
SPANISH FORK, UT 84660

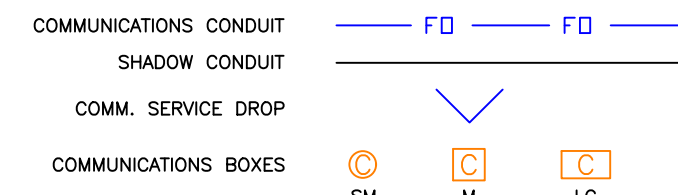
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**ELECTRICAL LEGEND**



- ELECTRIC NOTES:**
1. CONDUIT FOR STREET LIGHTS SHALL BE 1" SCHEDULE 40 RIGID NONMETALLIC CONDUIT (RNC).
  2. CONDUIT FOR SERVICE DROPS SHALL BE 2" (RNC).
  3. CONDUIT FOR 1 PHASE AND SECONDARY CABLE SHALL BE 3" SCHEDULE 40 RIGID NONMETALLIC CONDUIT (RNC).
  4. CONDUIT FOR 3 PHASE SHALL BE 4" SCHEDULE 40 RIGID NONMETALLIC CONDUIT (RNC).
  5. LONG-RADIUS ELBOWS CONSTRUCTED OF EITHER RIGID METAL OR FIBERGLASS ARE REQUIRED FOR ANY ANGLE 45 DEG OR GREATER.
  6. SECONDARY CABLE SHALL BE 4/0 NOMI URD TYPE UNLESS OTHERWISE SPECIFIED.
  7. SECONDARY 1 PHASE AND 3 PHASE TRANSFORMER CONDUITS SHALL BE AT A DEPTH OF 4' TO TOP OF CONDUIT.
  8. 3 PHASE CONDUITS SHALL BE AT A DEPTH OF 4' TO TOP OF CONDUIT, UNLESS OTHERWISE SPECIFIED.
  9. SERVICE DROP CONDUIT SHALL BE AT A DEPTH OF 4' TO TOP OF CONDUIT.
  10. STREET LIGHT CONDUCTORS SHALL BE #10 STR CU TYPE THWN.
  11. STREET LIGHTS SHALL BE LED, DECORATIVE POLE UNLESS OTHERWISE SPECIFIED.
  12. (1) - SHADOW CONDUIT SHALL BE REQUIRED AT EACH POWER ROAD CROSSING. CONDUIT SHALL BE THE SAME SIZE AND TYPE AS LARGEST POWER CONDUIT BEING INSTALLED.

**COMMUNICATIONS LEGEND**



- SFCN NOTES:**
1. CONDUIT FOR SERVICE DROPS SHALL BE 1" SCHEDULE 40 RIGID NONMETALLIC CONDUIT (RNC).
  2. CONDUIT FOR SECONDARY RUNS SHALL BE 2" (RNC).
  3. CONDUIT FOR MAIN RUNS SHALL BE 2" (RNC).
  4. CONDUIT FOR FIBER OPTICS SHALL BE 3" (RNC) UNLESS OTHERWISE SPECIFIED.
  5. LONG-RADIUS ELBOWS CONSTRUCTED OF EITHER RIGID METAL OR FIBERGLASS ARE REQUIRED FOR ANY ANGLE 45 DEG OR GREATER.
  6. COMMUNICATIONS CONDUITS SHALL RUN BEHIND (FARTHEST FROM PROPERTY LINE) ELECTRICAL CONDUITS AND STUB UP BEHIND ELECTRICAL BOXES UNLESS OTHERWISE SPECIFIED.
  7. (1) - SHADOW CONDUIT SHALL BE REQUIRED AT EACH COMMUNICATION ROAD CROSSING. CONDUIT SHALL BE THE SAME SIZE AND TYPE AS LARGEST COMMUNICATIONS CONDUIT BEING INSTALLED.

**GENERAL NOTES:**

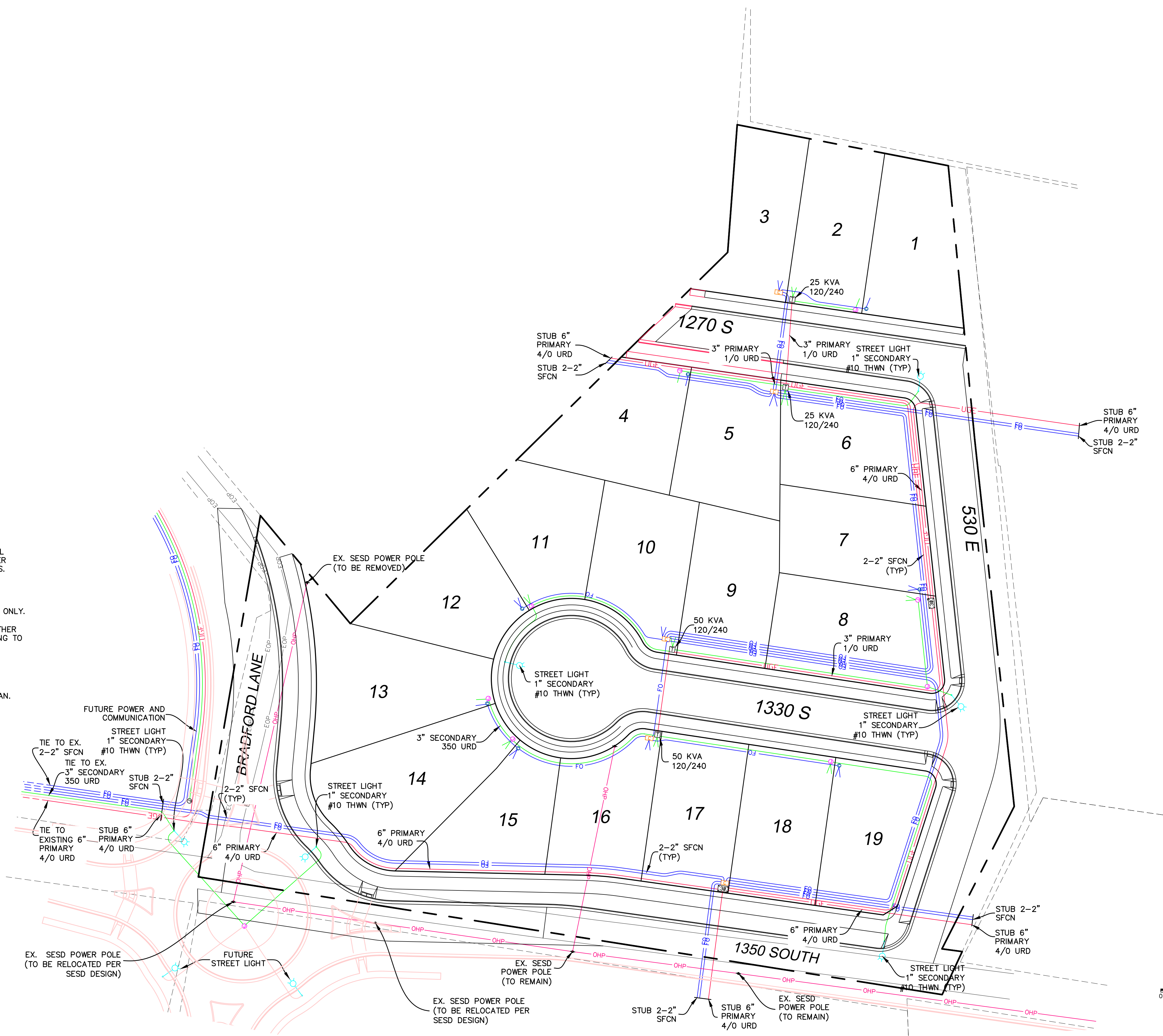
DEVELOPER SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS, EASEMENTS, AND RIGHTS-OF-WAY. DEVELOPER SHALL FOLLOW ALL OF SPANISH FORK CITY STANDARDS.

**GENERAL NOTES:**

THIS MAP IS INTENDED FOR INFORMATIONAL PURPOSES ONLY. THIS MAP DOES NOT SHOW THE ACTUAL, AND EXACT LOCATION(S) OF ANY CONDUITS, BOXES, LIGHTS, OR OTHER APPARATUS. THESE ITEMS SHALL BE PLACED ACCORDING TO SPANISH FORK CITY STANDARDS, AND BY FIELD VERIFICATION.

**GENERAL NOTES:**

ENGINEER STAMP DOES NOT APPLY TO ELECTRICAL PLAN. THIS PLAN MUST BE APPROVED BY SPANISH FORK CITY ELECTRICAL DEPARTMENT PRIOR TO CONSTRUCTION.



5 20 40 80 120  
 (24"x36")  
 SCALE 1" = 40'  
 (11"x17")  
 SCALE 1" = 80'

SHEET NO.

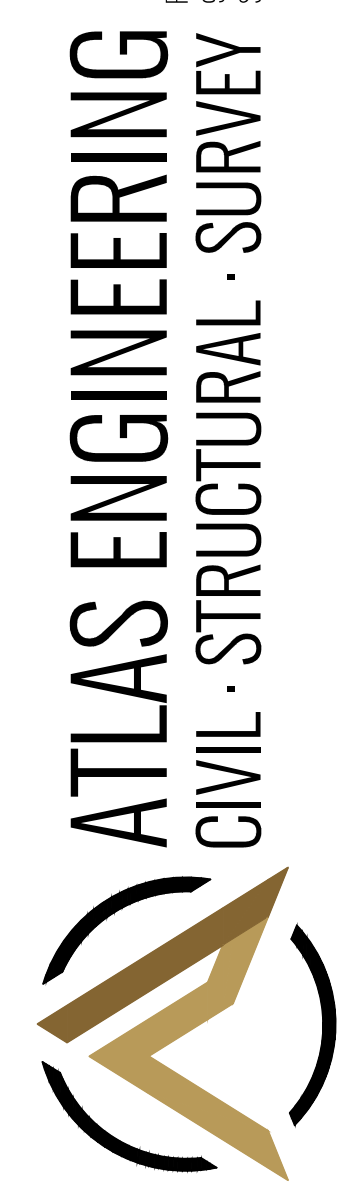
6

NO.	DATE	REVISIONS
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ELECTRICAL PLAN

SPANISH FORK, UTAH

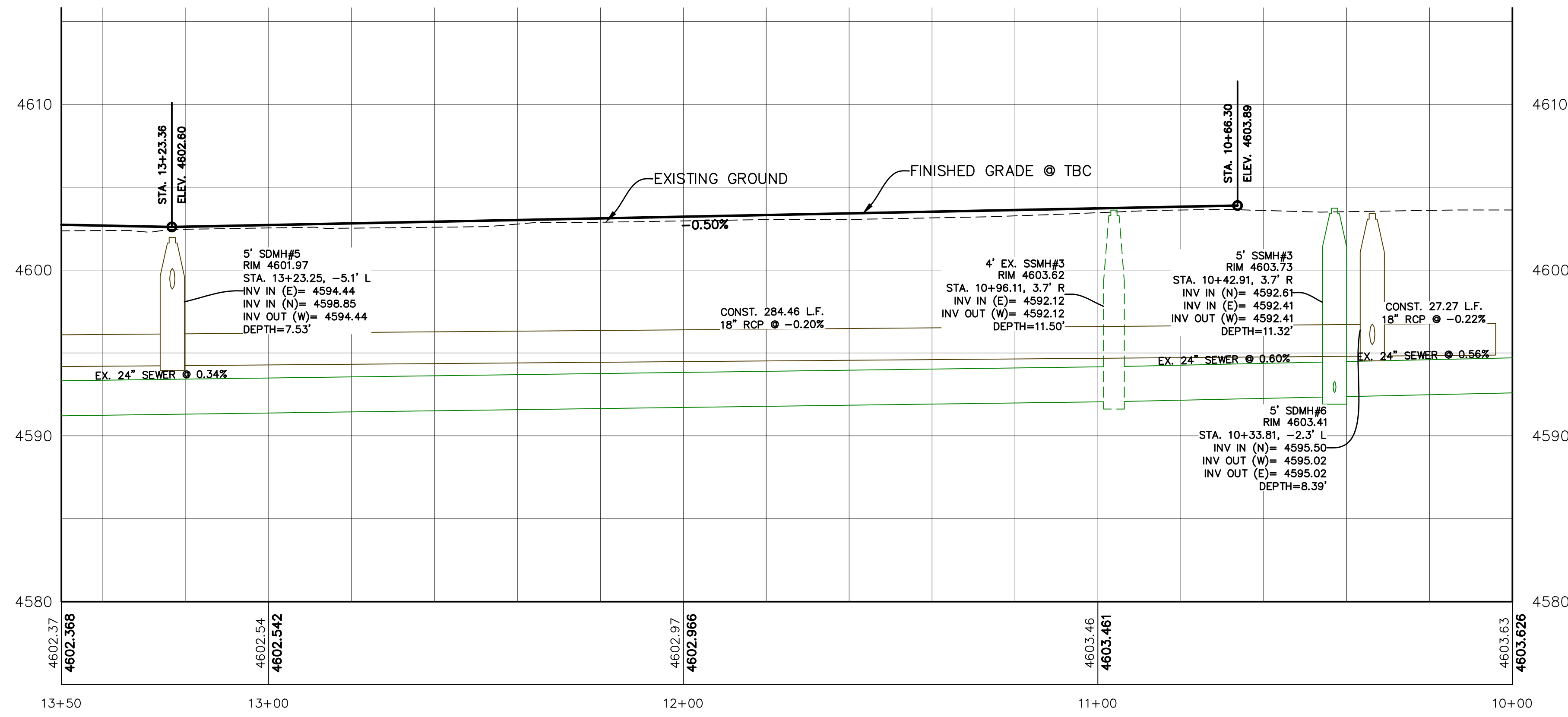
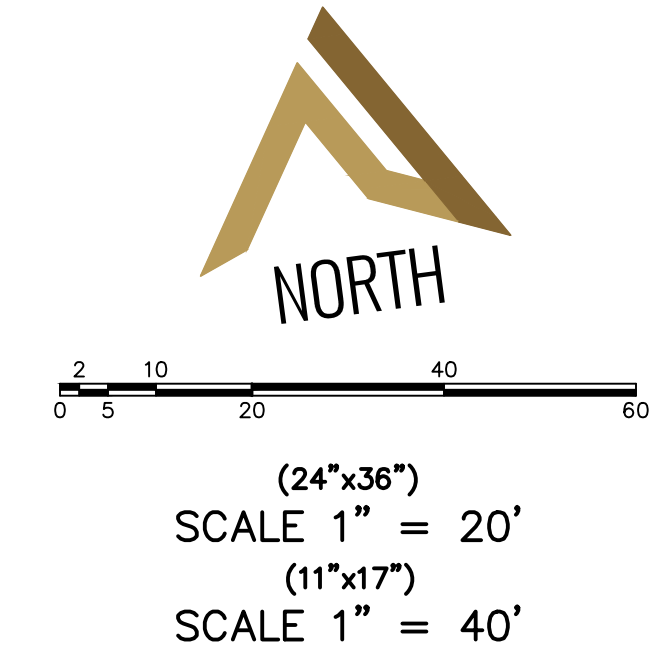
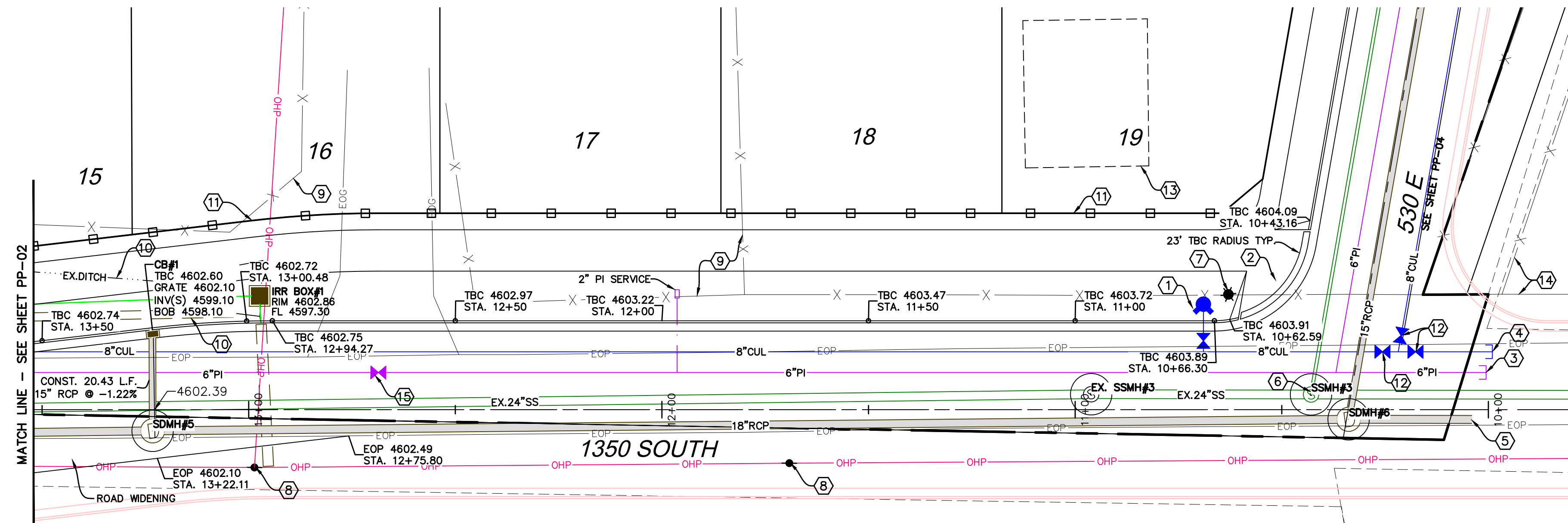
WYNDSONG SUBDIVISION



PHONE: 801-455-6566  
 946 E. 800th, SUITE A  
 SPANISH FORK, UT 84650

**CONSTRUCTION NOTES**

- ① INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
- ② ALL ADA PEDESTRIAN RAMPS TO MEET SPANISH FORK CITY CURRENT STANDARDS AND SHALL COMPLY WITH SPANISH FORK CITY TRANSPORTATION MASTER PLAN (TYPICAL).
- ③ CAP & PLUG PRESSURIZED IRRIGATION WATERLINE.
- ④ CAP & PLUG CULINARY WATERLINE.
- ⑤ CAP & PLUG STORM DRAIN LINE.
- ⑥ LOCATE AND TIE TO EXISTING SEWER.
- ⑦ INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
- ⑧ EXISTING POWER POLE TO REMAIN.
- ⑨ EXISTING FENCE TO BE REMOVED.
- ⑩ EXISTING DITCH TO BE PIPED.
- ⑪ CONST. 6" MASONRY WALL. SEE DETAIL SHEET DT-02.
- ⑫ INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
- ⑬ REMOVE EXISTING BUILDING.
- ⑭ EXISTING FENCE TO REMAIN.
- ⑮ INSTALL 6" PI VALVE PER SPANISH FORK CITY STANDARDS.



SHEET NO.  
**PP-01**

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1350 SOUTH  
STA. 10+00 TO STA. 13+50  
SPANISH FORK, UTAH

**WYNDSONG SUBDIVISION**

**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY

PHONE: 801-555-6565  
946 E. BOON, SUITE A  
SPANISH FORK, UT 84601

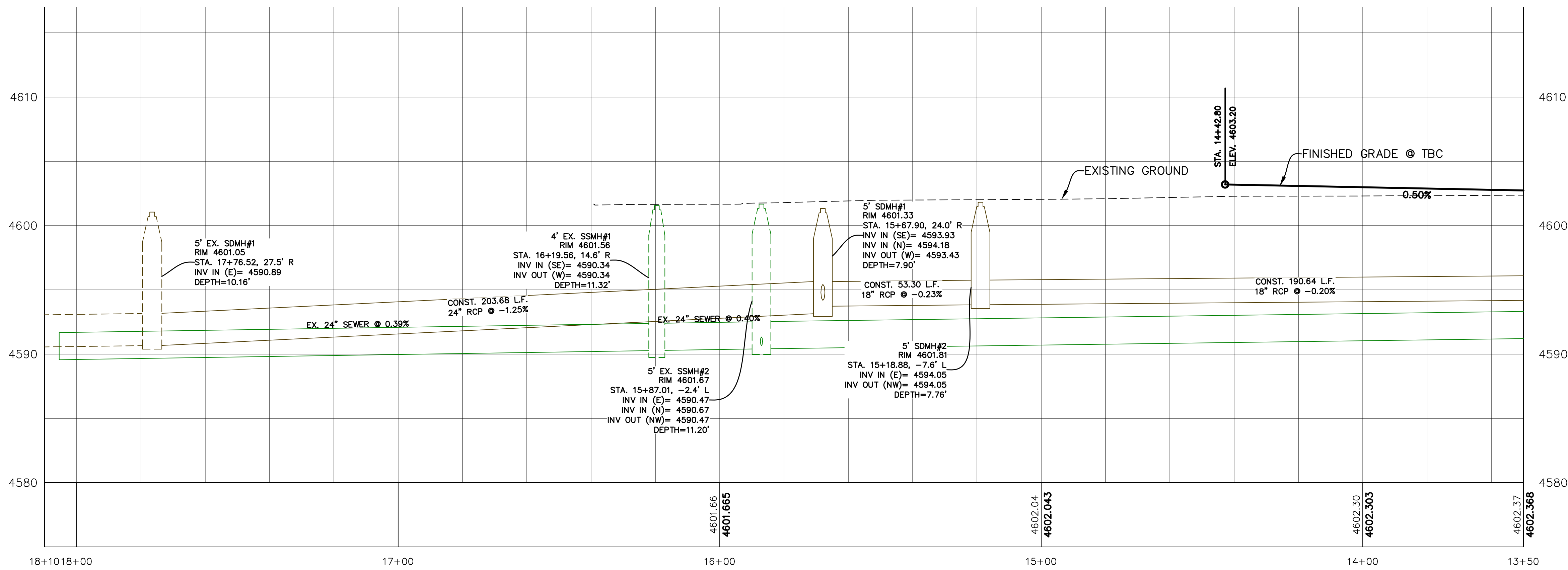
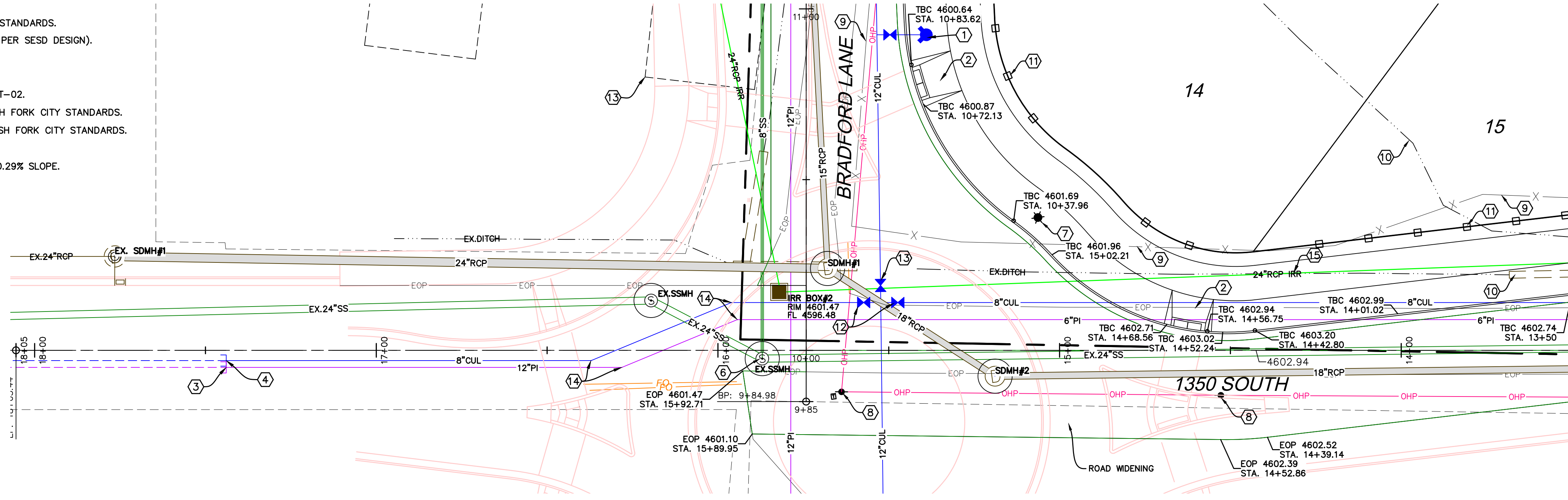
Z:\2019\19-125 POWERS\_SPANISH\_FORK\CADD\FINAL\PP-01.DWG

**CONSTRUCTION NOTES**

- ① INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
- ② ALL ADA PEDESTRIAN RAMPS TO MEET SPANISH FORK CITY CURRENT STANDARDS AND SHALL COMPLY WITH SPANISH FORK CITY TRANSPORTATION MASTER PLAN (TYPICAL).
- ③ LOCATE AND TIE TO EXISTING PRESSURIZED IRRIGATION WATERLINE.
- ④ LOCATE AND TIE TO EXISTING CULINARY WATERLINE.
- ⑤ REMOVE EXISTING BUILDING.
- ⑥ LOCATE AND TIE TO EXISTING SEWER.
- ⑦ INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
- ⑧ EXISTING SESD POWER POLE (TO BE RELOCATED PER SESD DESIGN).
- ⑨ EXISTING FENCE TO BE REMOVED.
- ⑩ EXISTING DITCH TO BE PIPED.
- ⑪ CONST. 6" MASONRY WALL. SEE DETAIL SHEET DT-02.
- ⑫ INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
- ⑬ INSTALL 12" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
- ⑭ INSTALL 22.5" BEND.
- ⑮ CONST. 280.90 L.F. 24" RCP IRRIGATION PIPE @ 0.29% SLOPE.



(24"x36")  
SCALE 1" = 20'  
(11"x17")  
SCALE 1" = 40'



SHEET NO.

PP-02

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1350 SOUTH  
STA. 13+50 TO STA. 18+05

SPANISH FORK, UTAH

WYNDSONG SUBDIVISION

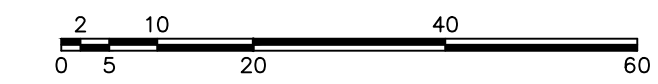
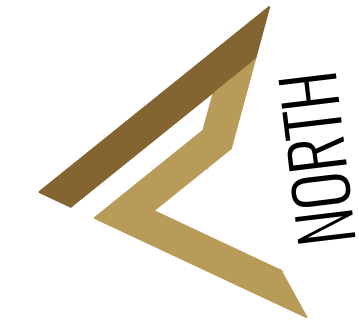
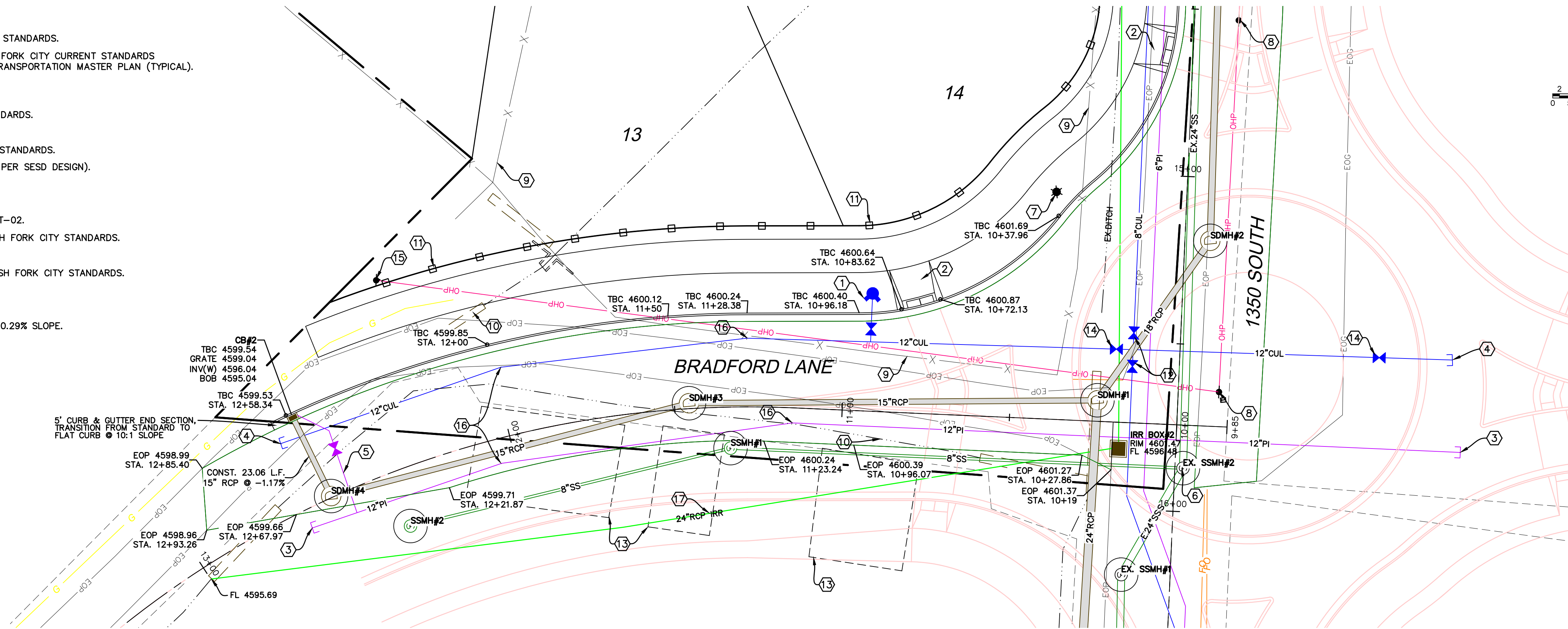
**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY

PHONE: 801-955-6565  
946 E. BOON, SUITE A  
SPANISH FORK, UT 84601

Z:\2019\19-125 POWERS SPANISH FORK\CADD\FINAL\PP-02.DWG

**CONSTRUCTION NOTES**

- ① INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
- ② ALL ADA PEDESTRIAN RAMPS TO MEET SPANISH FORK CITY CURRENT STANDARDS AND SHALL COMPLY WITH SPANISH FORK CITY TRANSPORTATION MASTER PLAN (TYPICAL).
- ③ CAP/PLUG PRESSURIZED IRRIGATION LINE.
- ④ CAP/PLUG CULINARY WATERLINE.
- ⑤ INSTALL PI DRAIN PER SPANISH FORK CITY STANDARDS.
- ⑥ LOCATE AND TIE TO EXISTING SEWER.
- ⑦ INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
- ⑧ EXISTING SESD POWER POLE (TO BE RELOCATED PER SESD DESIGN).
- ⑨ EXISTING FENCE TO BE REMOVED.
- ⑩ EXISTING DITCH TO BE PIPED.
- ⑪ CONST. 6" MASONRY WALL. SEE DETAIL SHEET DT-02.
- ⑫ INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
- ⑬ REMOVE EXISTING BUILDING.
- ⑭ INSTALL 12" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
- ⑮ EXISTING POWER POLE TO BE REMOVED.
- ⑯ INSTALL 11.25' BEND.
- ⑰ CONST. 271.65 L.F. 24" RCP IRRIGATION PIPE @ 0.29% SLOPE.



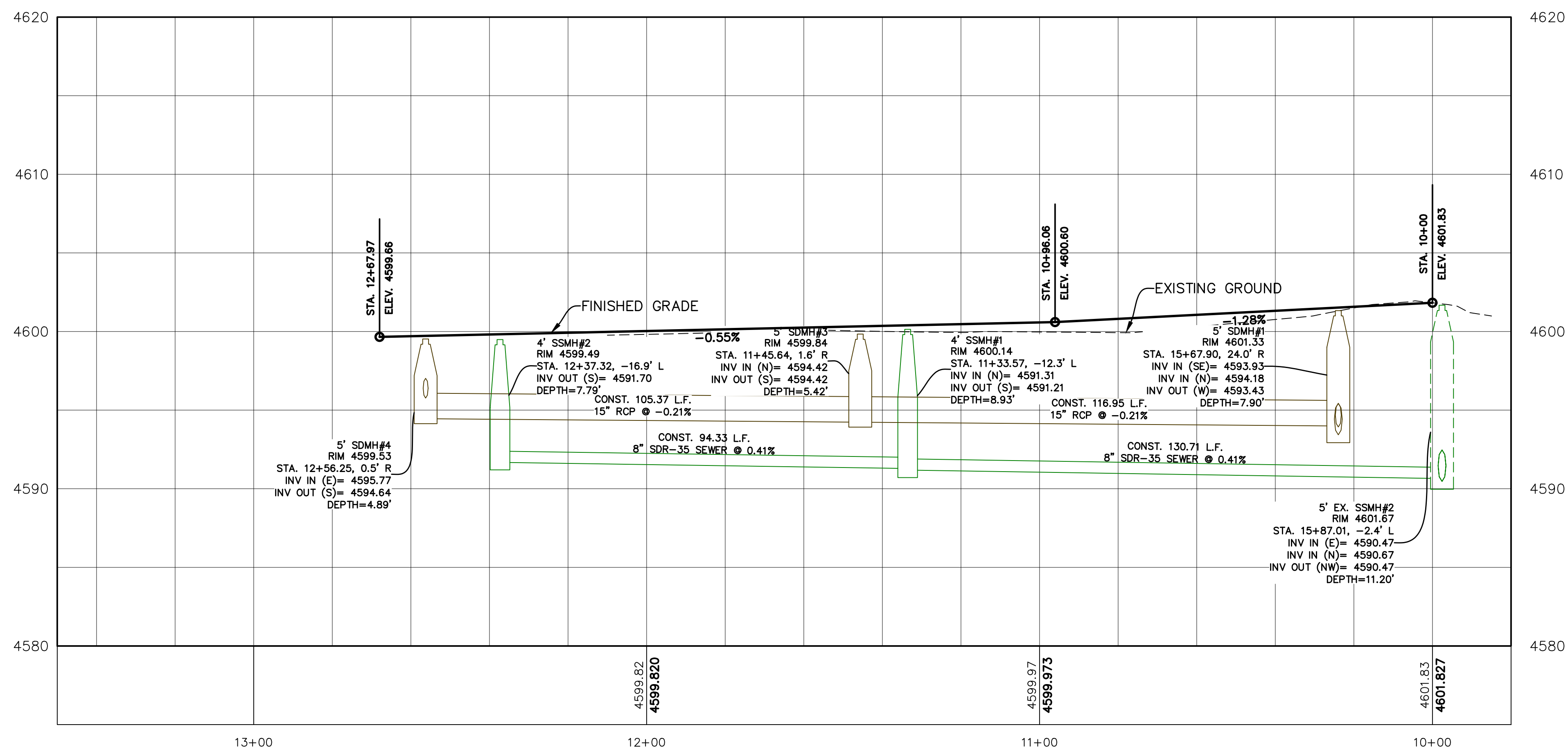
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SCALE 1" = 20'  
(11"x17")  
SCALE 1" = 40'

SHEET NO.  
**PP-03**

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BRADFORD LANE  
STA. 10+00 TO STA. 13+00

SPANISH FORK, UTAH



**WYNDSONG SUBDIVISION**

**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY

PHONE: 801-555-0566  
946 E. BOON, SUITE A  
SPANISH FORK, UT 84601

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(24"x36")  
SCALE 1" = 20'  
(11"x17")  
SCALE 1" = 40'

SHEET NO.  
**PP-04**

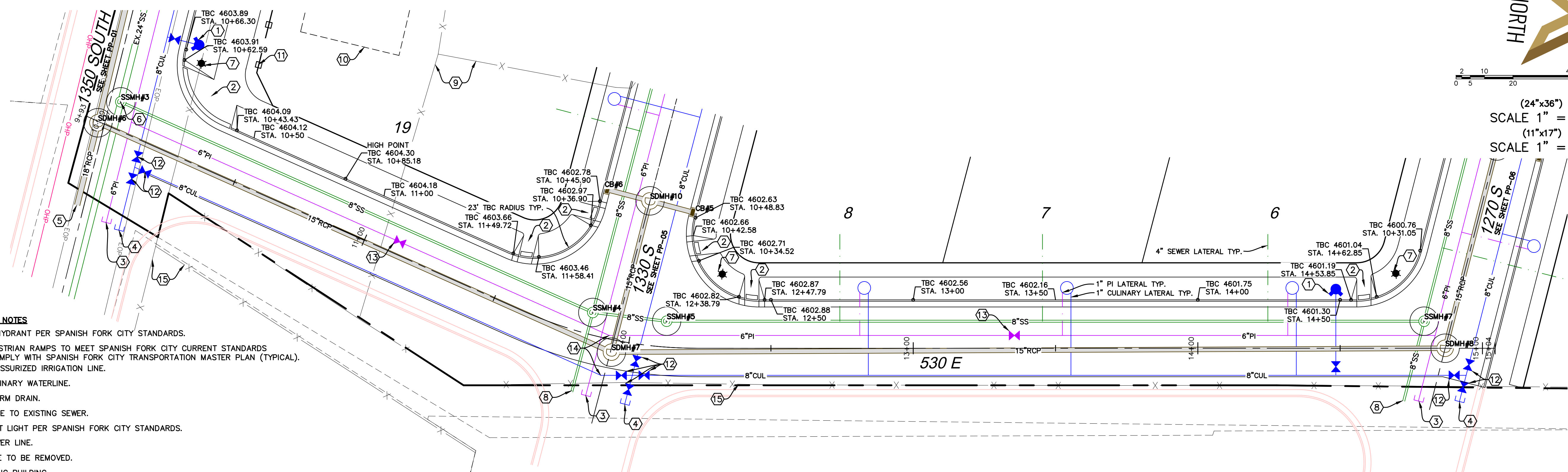
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530 E  
STA. 10+00 TO STA. 15+00  
SPANISH FORK, UTAH

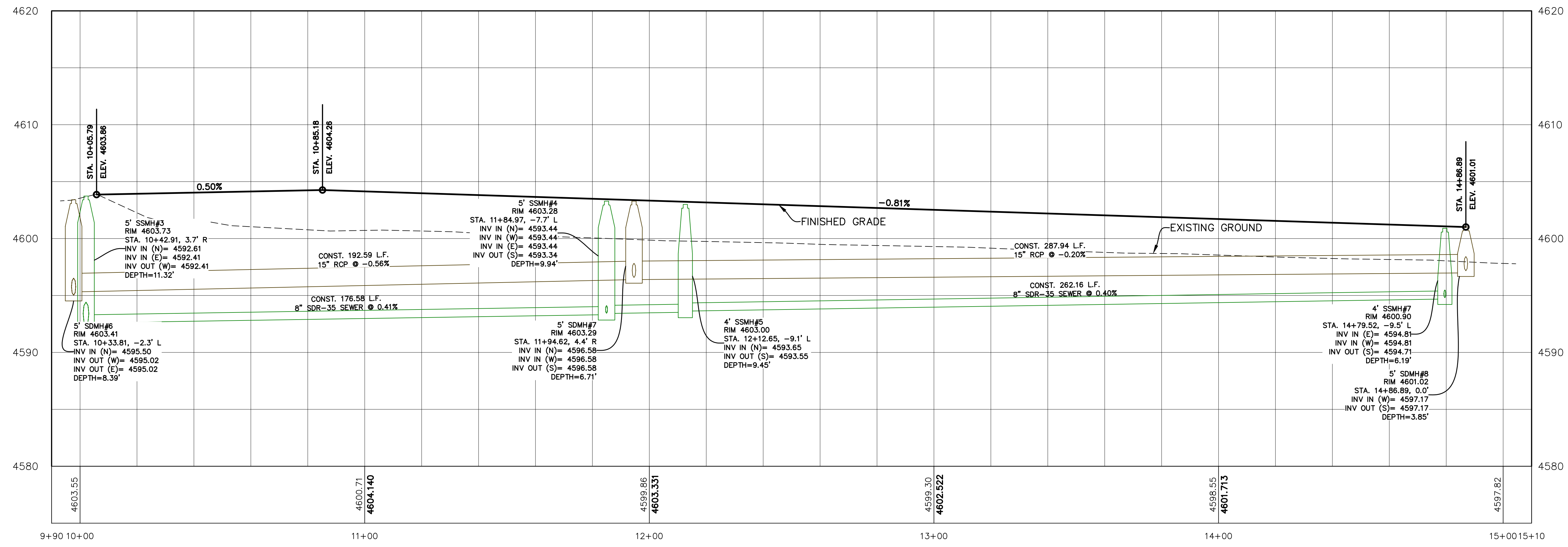
**WYNDSONG SUBDIVISION**  
**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY

PHONE: 801-555-6565  
946 E. BOON, SUITE A  
SPANISH FORK, UT 84601

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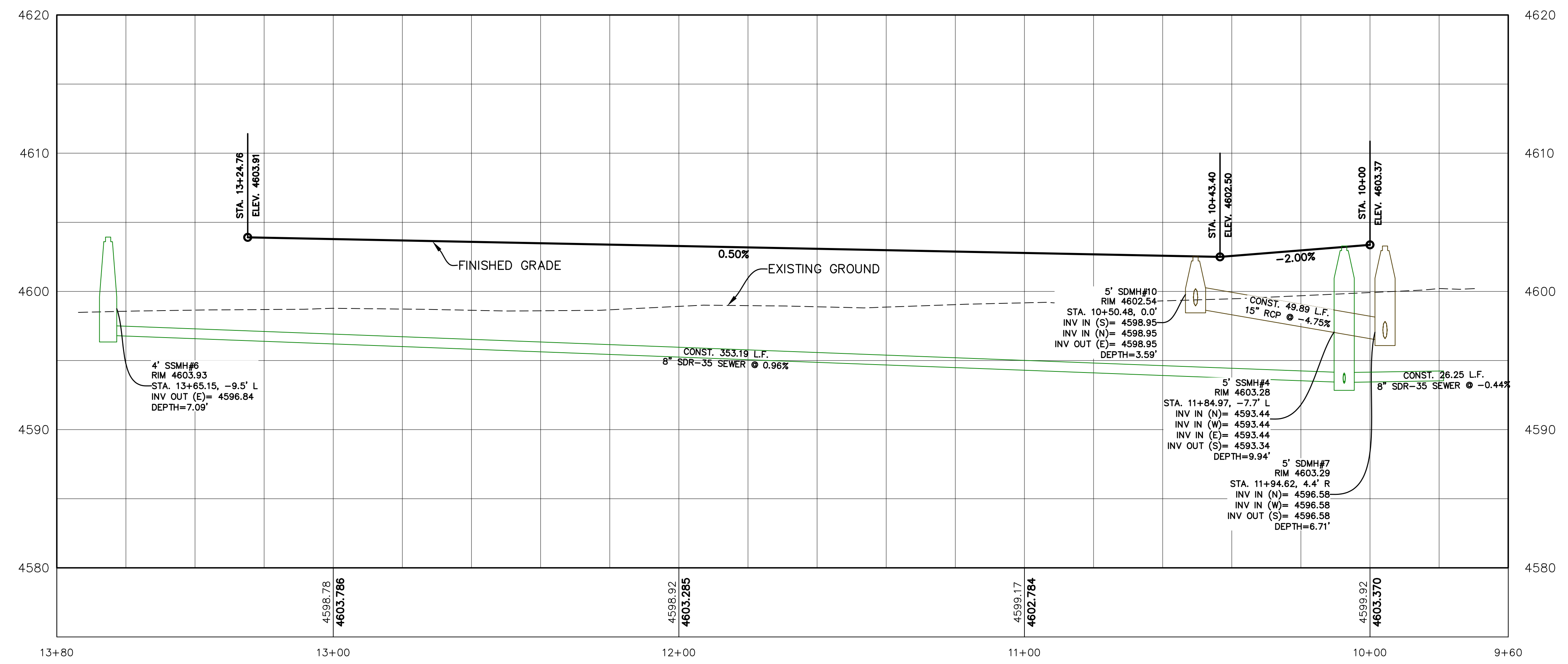
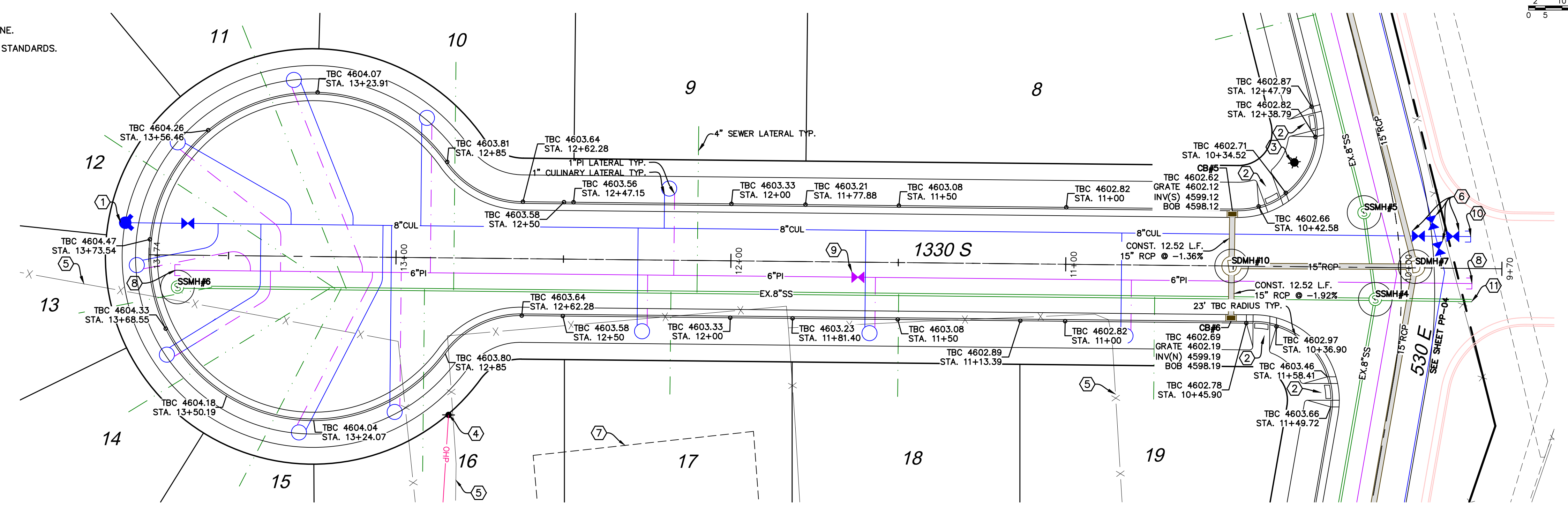
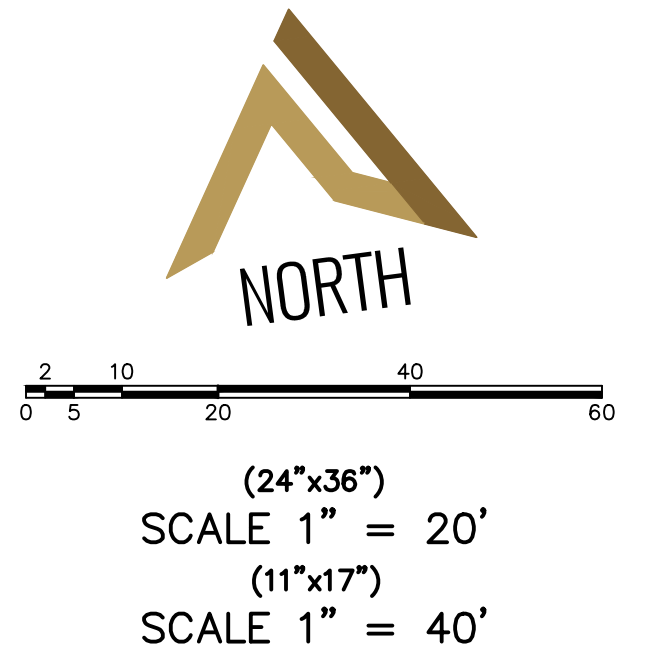


- CONSTRUCTION NOTES**
- ① INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
  - ② ALL ADA PEDESTRIAN RAMPS TO MEET SPANISH FORK CITY CURRENT STANDARDS AND SHALL COMPLY WITH SPANISH FORK CITY TRANSPORTATION MASTER PLAN (TYPICAL).
  - ③ CAP/PLUG PRESSURIZED IRRIGATION LINE.
  - ④ CAP/PLUG CULINARY WATERLINE.
  - ⑤ CAP/PLUG STORM DRAIN.
  - ⑥ LOCATE AND TIE TO EXISTING SEWER.
  - ⑦ INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
  - ⑧ CAP/PLUG SEWER LINE.
  - ⑨ EXISTING FENCE TO BE REMOVED.
  - ⑩ REMOVE EXISTING BUILDING.
  - ⑪ CONST. 6" MASONRY WALL. SEE DETAIL SHEET DT-02.
  - ⑫ INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
  - ⑬ INSTALL 6" PI VALVE PER SPANISH FORK CITY STANDARDS.
  - ⑭ INSTALL 22.5" BEND.
  - ⑮ EXISTING FENCE TO REMAIN.



- CONSTRUCTION NOTES**
- ① INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
  - ② ALL ADA PEDESTRIAN RAMPS TO MEET SPANISH FORK CITY CURRENT STANDARDS AND SHALL COMPLY WITH SPANISH FORK CITY TRANSPORTATION MASTER PLAN (TYPICAL).
  - ③ INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
  - ④ EXISTING POWER POLE TO BE REMOVED.
  - ⑤ EXISTING FENCE TO BE REMOVED.
  - ⑥ INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
  - ⑦ REMOVE EXISTING BUILDING.
  - ⑧ CAP & PLUG PRESSURIZED IRRIGATION WATERLINE.
  - ⑨ INSTALL 6" PI VALVE PER SPANISH FORK CITY STANDARDS.
  - ⑩ CAP & PLUG CULINARY WATERLINE.
  - ⑪ CAP & PLUG SEWER LINE.

**NOTES:**  
 1. WATERLINES ON STREET 2 TO SLOPE DOWN AT 0.5% TO THE CUL-DE-SAC. THIS IS SO AIR RUNS BACK TO THE MAIN LINES ON STREET 1.



SHEET NO. **PP-05**

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1330 S  
 STA. 10+00 TO STA. 13+74

SPANISH FORK, UTAH

**WYNDSONG SUBDIVISION**

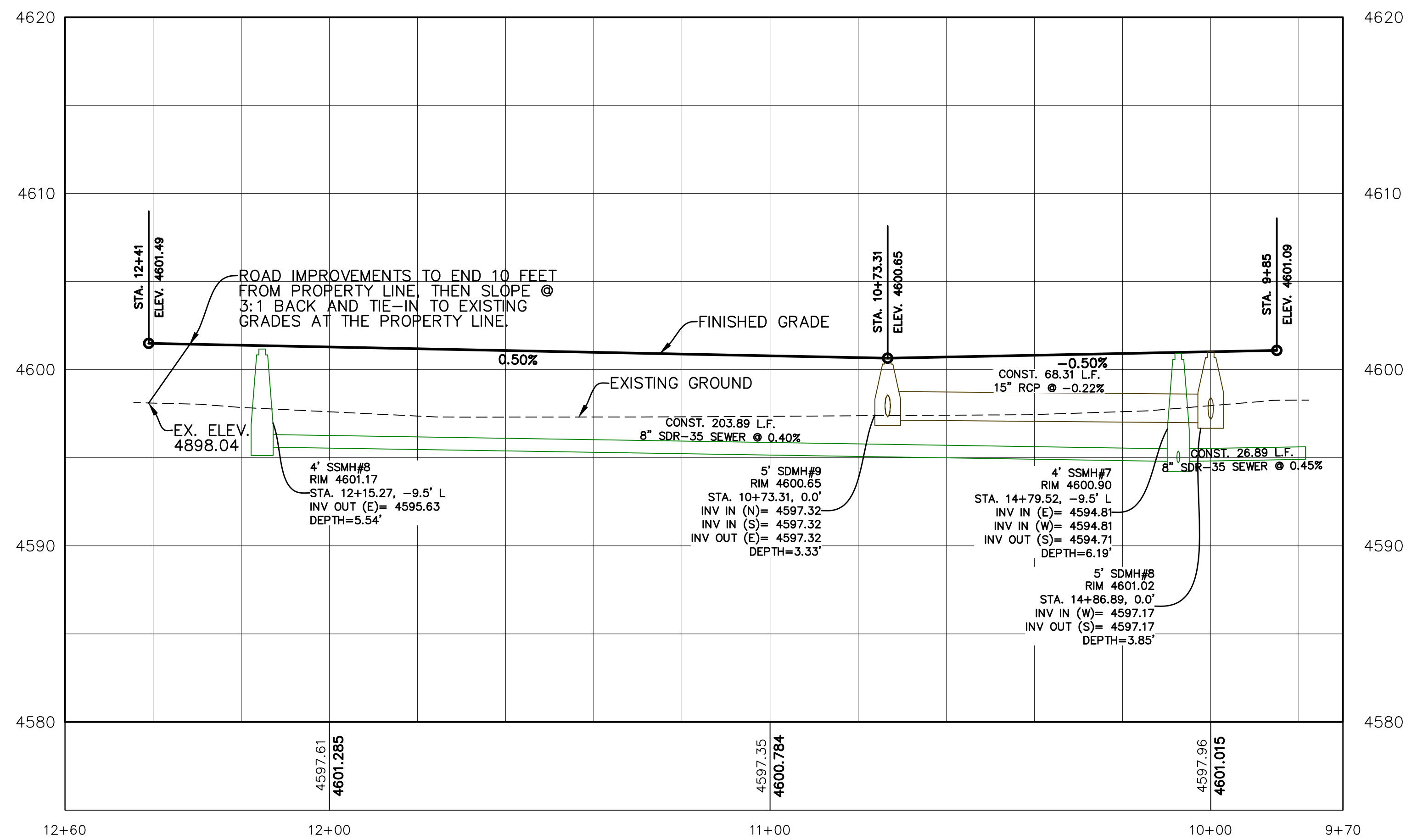
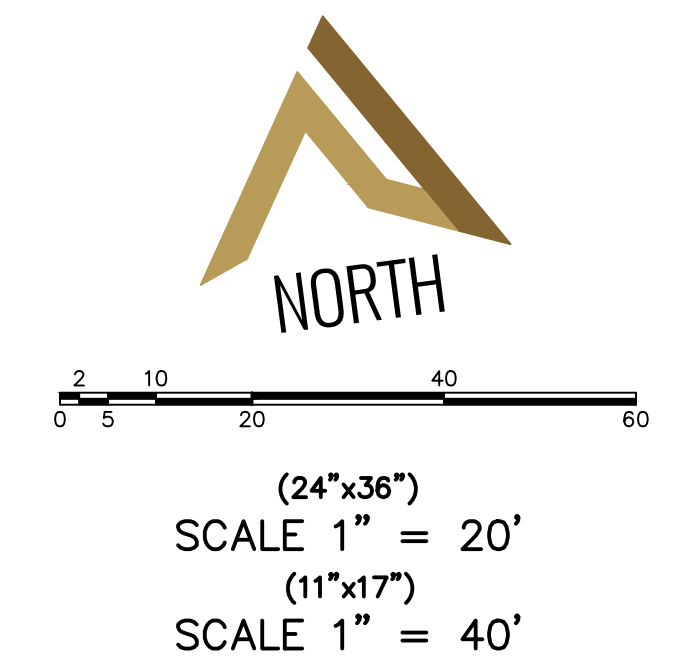
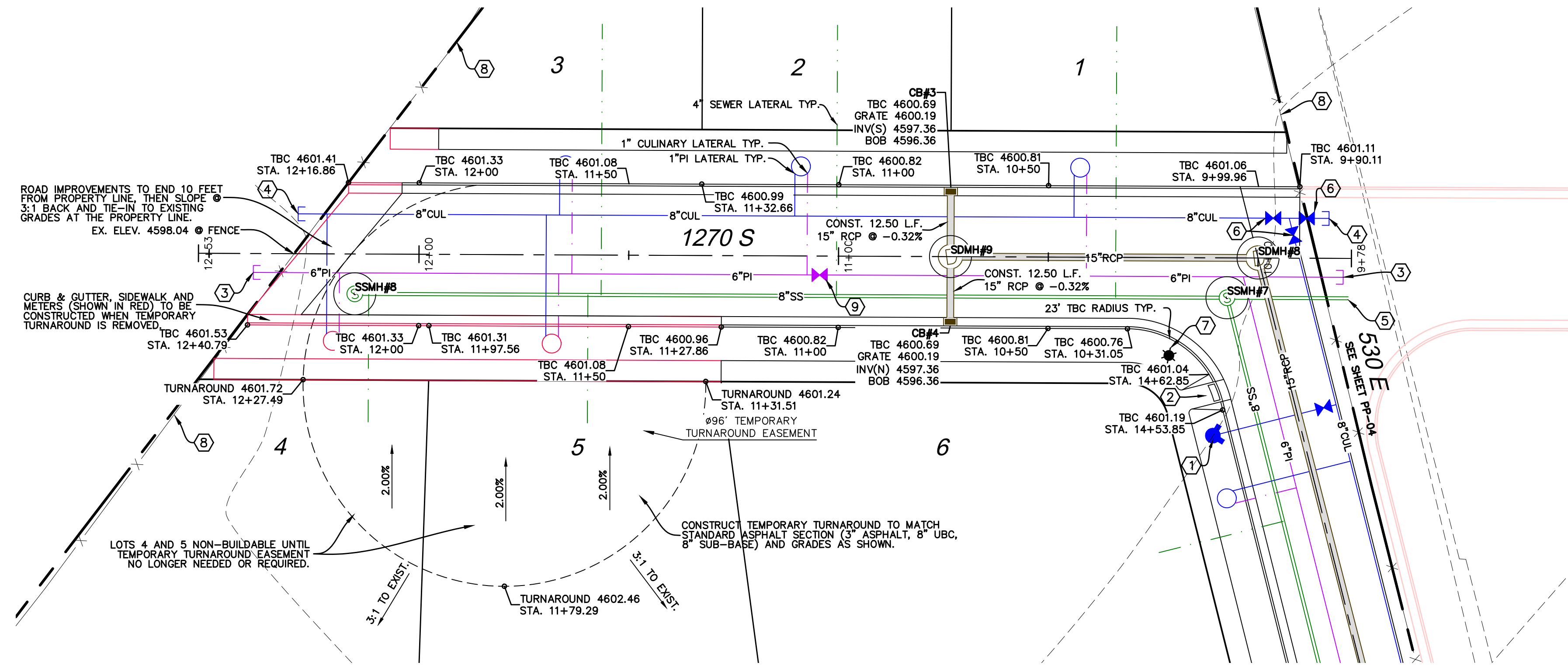
**ATLAS ENGINEERING**  
 CIVIL · STRUCTURAL · SURVEY

PHONE: 801-555-6565  
 946 E. BOON SUITE A  
 SPANISH FORK, UT 84601

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**CONSTRUCTION NOTES**

- ① INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
- ② ALL ADA PEDESTRIAN RAMPS TO MEET SPANISH FORK CITY CURRENT STANDARDS AND SHALL COMPLY WITH SPANISH FORK CITY TRANSPORTATION MASTER PLAN (TYPICAL).
- ③ CAP & PLUG PRESSURIZED IRRIGATION WATERLINE.
- ④ CAP & PLUG CULINARY WATERLINE.
- ⑤ CAP & PLUG SEWER LINE.
- ⑥ INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
- ⑦ INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
- ⑧ EXISTING FENCE TO BE REMAIN.
- ⑨ INSTALL 6" PI VALVE PER SPANISH FORK CITY STANDARDS.



SHEET NO.  
**PP-06**

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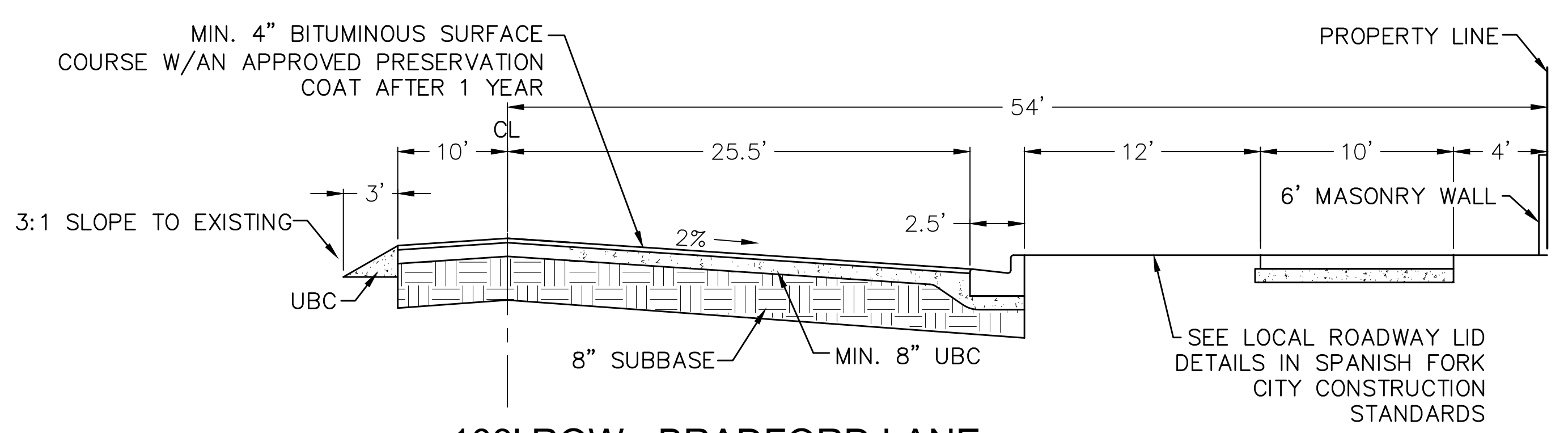
1270 S  
STA. 10+00 TO STA. 12+53  
**SPANISH FORK, UTAH**

**WYNDSONG SUBDIVISION**

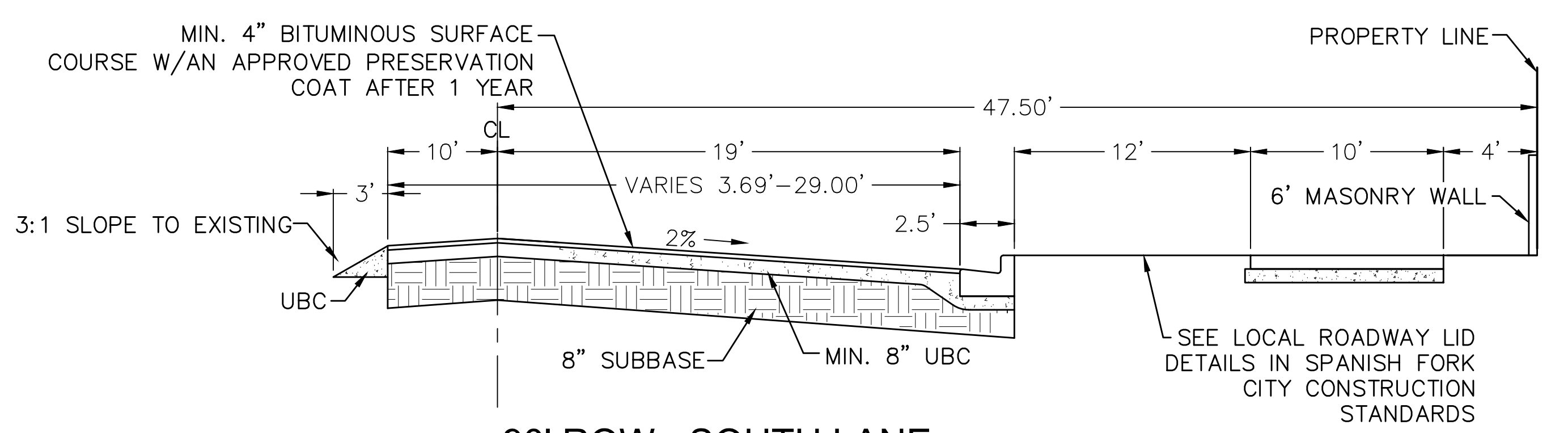
**ATLAS ENGINEERING**  
CIVIL · STRUCTURAL · SURVEY

PHONE: 801-555-0566  
946 E. BOON, SUITE A  
SPANISH FORK, UT 84660

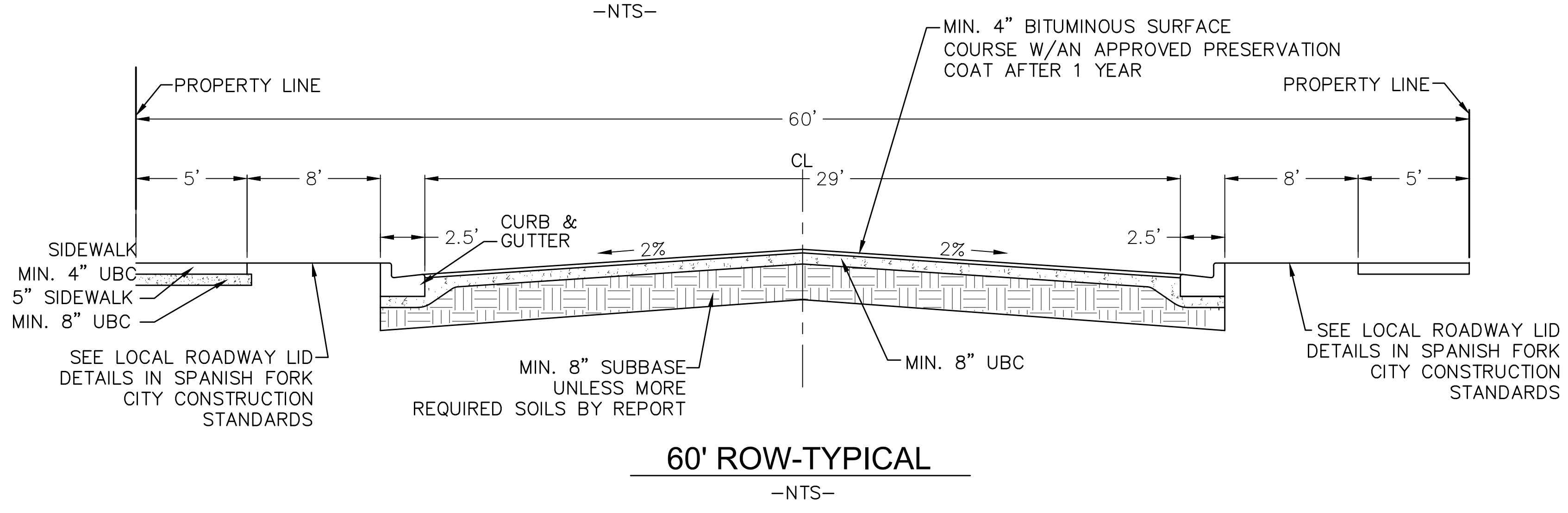
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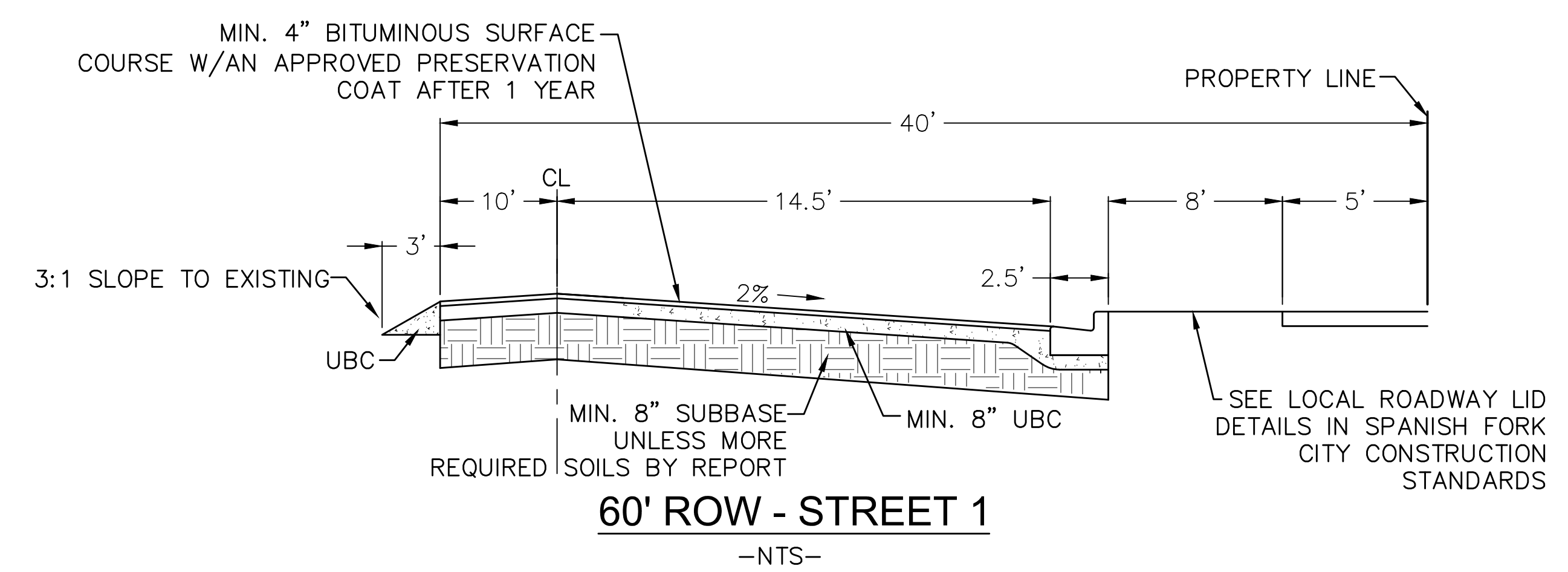
**103' ROW - BRADFORD LANE**  
-NTS-



**90' ROW - SOUTH LANE**  
-NTS-

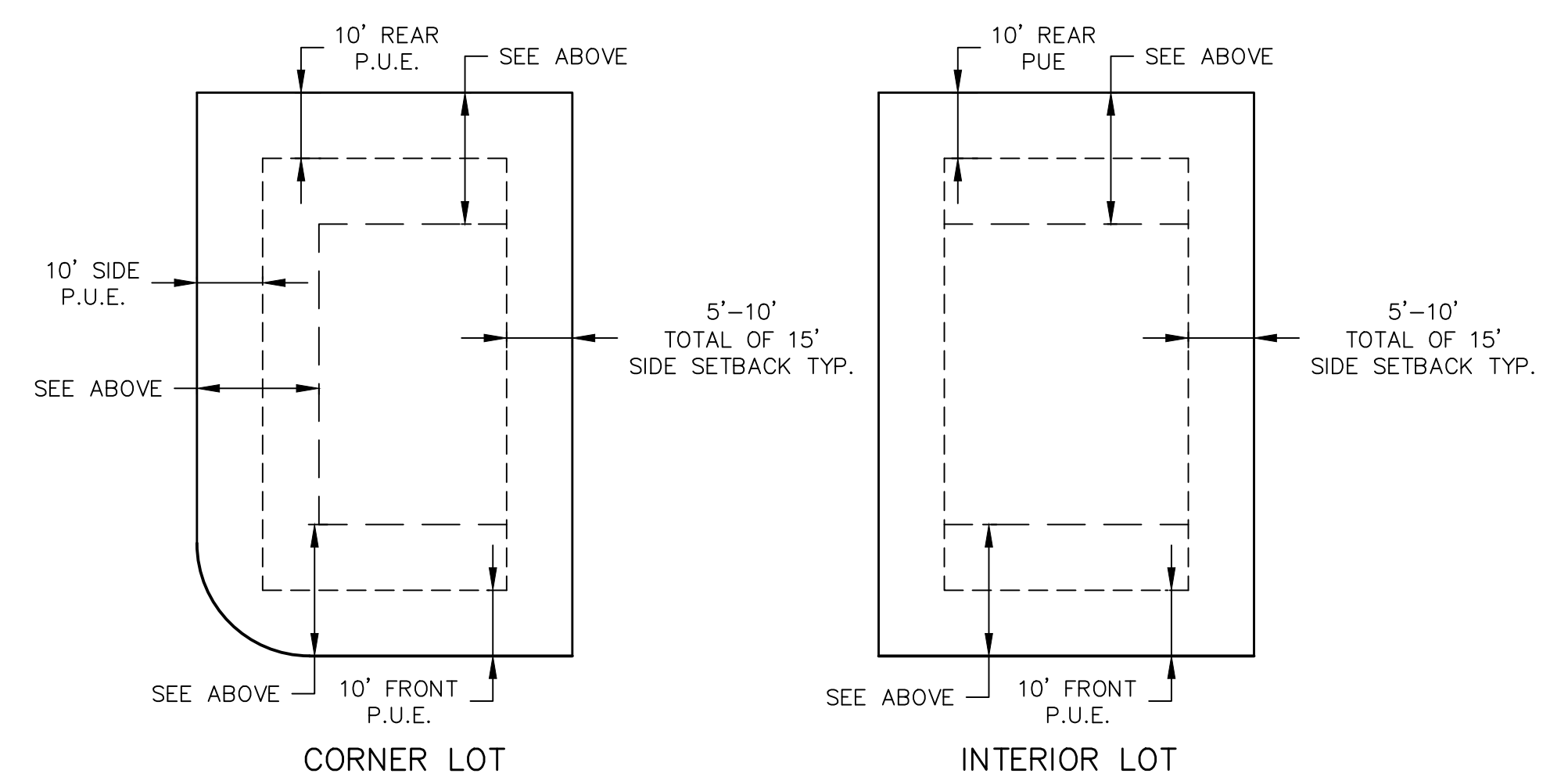


**60' ROW-TYPICAL**  
-NTS-



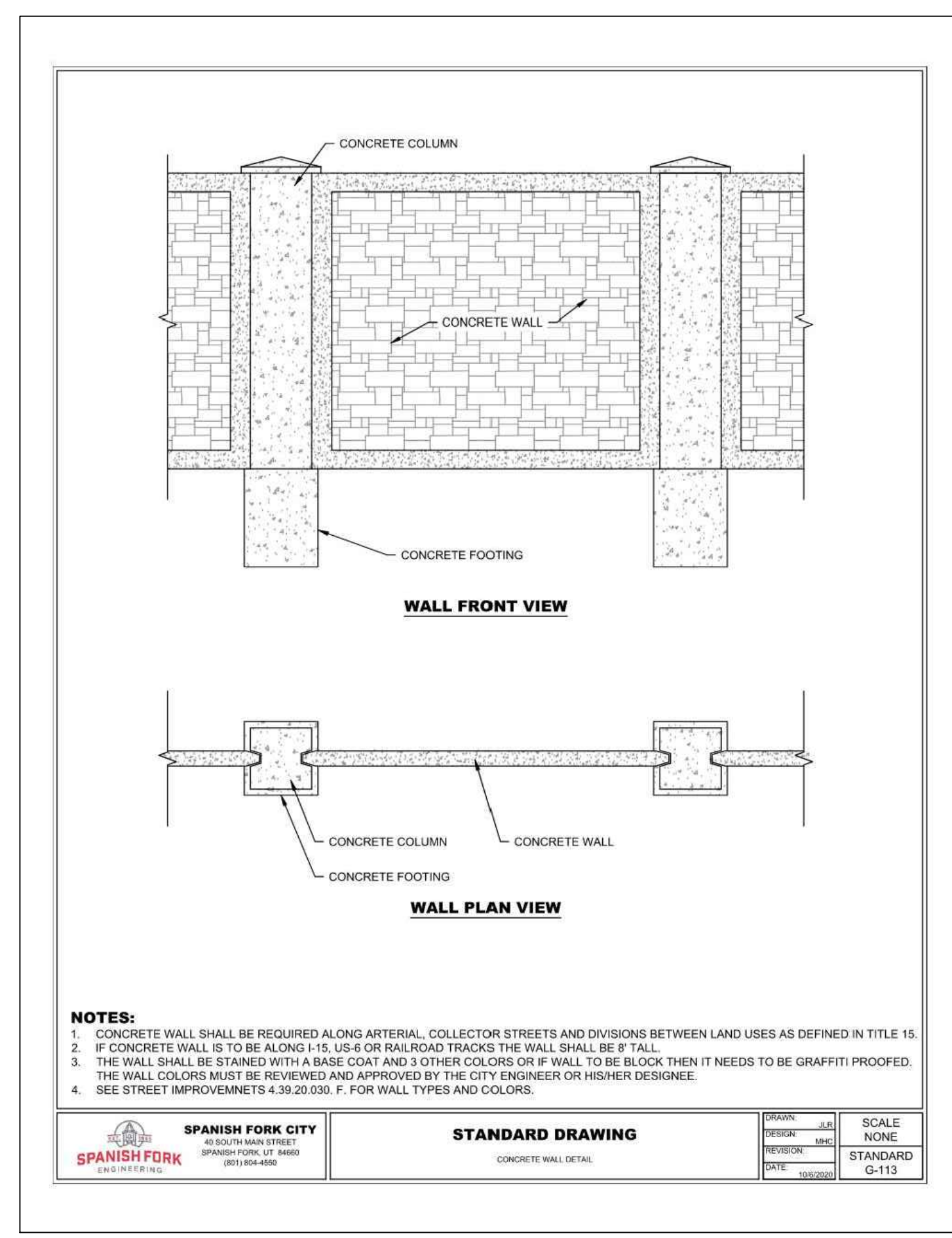
**60' ROW - STREET 1**  
-NTS-

**SINGLE FAMILY HOME SETBACKS**  
**INTERIOR LOTS:**  
 FRONT SETBACK-20' TO 25' (20' TO LIVING AREAS, 25 FEET TO GARAGES OR CARPORTS, AND 20' TO THE FRONT OF SIDE ENTRY GARAGE)  
 REAR SETBACK-25'  
 SIDE SETBACK-5'-10'-TOTAL OF 15'  
**CORNER LOTS:**  
 FRONT SETBACK-20' TO 25' (20' TO LIVING AREAS, 25 FEET TO GARAGES OR CARPORTS, AND 20' TO THE FRONT OF SIDE ENTRY GARAGE)  
 REAR SETBACK-25' (ON CORNER LOTS, SETBACK IS REDUCED BY 5 FEET)  
 INTERIOR SIDE-10'  
 ROAD SIDE-15' TO 25' (15' TO LIVING AREAS, 25' TO GARAGES OR CARPORTS, AND 20' TO THE FRONT OF THE SIDE ENTRY GARAGE)



**CORNER LOT**      **INTERIOR LOT**  
 \*A 10' PUE IS REQUIRED ON ALL PROPERTY LINES.

**DETAIL-TYP. BUILDING SETBACK & EASEMENT**  
-NTS-



<b>SPANISH FORK CITY</b> 40 SOUTH MAIN STREET SPANISH FORK, UT 84650 (801) 854-4500		<b>STANDARD DRAWING</b> CONCRETE WALL DETAIL	PROJECT: JAL SCALE: NONE REVISION: STD DATE: 11/2023 SHEET: Q-113
--	--	---	---

SHEET NO. **DT-01**  
 WYNDSONG SUBDIVISION  
 ATLAS ENGINEERING  
 CIVIL · STRUCTURAL · SURVEY  
 PHONE: 801-455-6566  
 945 E. 800th SUITE A  
 SPANISH FORK, UT 84650  
 2/1/2023 9:56:53 AM MST

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DETAIL SHEET  
 SPANISH FORK, UTAH

NORTHWEST CORNER OF SECTION 30,  
TOWNSHIP 8 SOUTH, RANGE 3 EAST, SLB&M.

N 0°12'30" W - SECTION LINE - BASIS OF BEARING

WEST 1/4 CORNER OF SECTION 30,  
TOWNSHIP 8 SOUTH, RANGE 3 EAST, SLB&M.

N 89°16'35" E 1970.69'

SOUTH 27.81'

WARNER FAMILY PROPERTY, LLC  
SN 27:041:0077

OVERLAPPING DEEDS  
FALL IN PROPOSED ROAD

BRADFORD LANE

POINT OF BEGINNING

POWER POLE

POWER POLE

POWER POLE

POWER POLE

POWER POLE

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POWER POLE

MEILING HALES  
SN 27:041:0056

BOUNDARY FENCE LINE AGREEMENT - ENTRY #30002/2017  
N 45°27'00" E 409.76'

**JAYME D. POWERS**  
AREA = 5.98 ACRES

JAYME D. POWERS  
SN 27:040:0055

EXIST. BLDG.

NOTE:  
DEEDS & FENCES DO NOT MATCH, I FOLLOWED  
POWERS DEED TO AVOID CONFLICT.

S 79°00'00" E 166.40'

N 8°15'50" E 99.06'

S 5°30'40" E 212.92'

S 6°00'21" E 295.76'

S 28°52'18" E 123.63'

N 81°00'00" E 582.76'

S 28°52'18" E 123.63'

S 28°52'18" E 123.63'

S 28°52'18" E 123.63'

S 28°52'18" E 123.63'

S 28°52'18" E 123.63'

S 28°52'18" E 123.63'

S 28°52'18" E 123.63'

S 28°52'18" E 123.63'

LINE TABLE

L1	S 82°22'16" E	15.85'
L2	S 25°00'00" W	36.78'

DIANE GARCIA  
SN 27:042:0075

NOTE:  
DEEDS AND FENCE DO NOT MATCH, I FOLLOWED  
EXISTING FENCE TO AVOID CONFLICT.

WILBUR A. STEPHENSON  
SN 27:041:0044

RYAN G. NELSON  
SN 27:040:0041

JEFFEREY VINCENT  
SN 27:042:0079

NINA DEWBERRY  
SN 27:043:0041

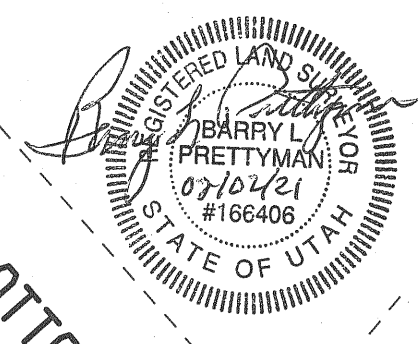
WILBUR A. STEPHENSON  
SN 27:040:0031

SPANISH FORK CITY  
SN 27:040:0061

CENTRAL BANK  
SN 27:040:0060

1350 SOUTH STREET

RIVER BOTTOMS ROAD

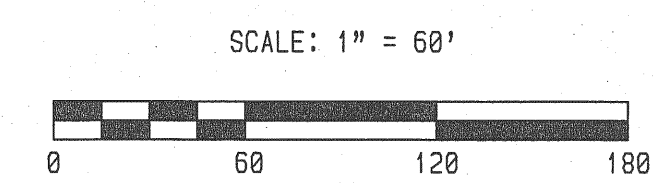
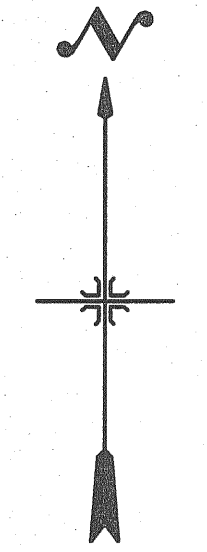


**SURVEYED BOUNDARY DESCRIPTION:**  
BEGINNING AT A POINT IN AN EXISTING BOUNDARY FENCE LINE AGREEMENT (ENTRY #30002/2017), WHICH POINT LIES NORTH 89°16'35" EAST 1970.69 FEET ALONG THE QUARTER SECTION LINE AND SOUTH 27.81 FEET FROM THE WEST 1/4 CORNER OF SECTION 30, TOWNSHIP 8 SOUTH, RANGE 3 EAST, SALT LAKE BASE AND MERIDIAN; THENCE ALONG SAID AGREEMENT THE FOLLOWING THREE COURSES TO WIT: (1) SOUTH 39°45'14" EAST 100.77 FEET, (2) NORTH 45°27'00" EAST 409.76 FEET, (3) NORTH 4°15'50" EAST 99.06 FEET; THENCE SOUTH 79°00'00" EAST 166.40 FEET TO AN EXISTING FENCE; THENCE ALONG EXISTING FENCES THE FOLLOWING THREE COURSES TO WIT: (1) SOUTH 5°30'40" EAST 212.92 FEET, (2) SOUTH 6°00'21" EAST 295.76 FEET, (3) SOUTH 28°54'02" WEST 123.63 FEET; THENCE SOUTH 82°22'16" EAST 15.85 FEET; THENCE SOUTH 25°00'00" WEST 36.78 FEET; THENCE NORTH 81°00'00" WEST 582.76 FEET; THENCE NORTH 9°45'00" EAST 293.00 FEET TO THE POINT OF BEGINNING (AREA = 5.98 ACRES)

**NARRATIVE:**  
BASIS OF BEARING IS UTAH COORDINATE BEARINGS, CENTRAL ZONE. (N 0°12'30" W BETWEEN THE WEST 1/4 CORNER AND THE NORTHWEST CORNER OF SECTION 30, TOWNSHIP 8 SOUTH, RANGE 3 EAST, SLB&M.) PURPOSE OF SURVEY IS TO COMPARE DEEDED PROPERTY AGAINST EXISTING PROPERTY IN ACTUAL POSSESSION AND PREPARE A SURVEYED BOUNDARY DESCRIPTION FOR FUTURE DEVELOPMENT.

**SURVEYORS CERTIFICATE:**  
I, BARRY L. PRETTYMAN, A LICENSED SURVEYOR HOLDING CERTIFICATE NO. 166406, DO HEREBY CERTIFY THAT THE PLAT SHOWN HEREON WAS PREPARED FROM A FIELD SURVEY MADE UNDER MY DIRECTION AND CORRECTLY SHOWS THE DIMENSIONS AND MONUMENTS OF THE ABOVE DESCRIBED PARCEL OF LAND TO THE BEST OF MY KNOWLEDGE AND BELIEF.  
*Barry L. Prettyman* Feb 2, 2021  
BARRY L. PRETTYMAN DATE

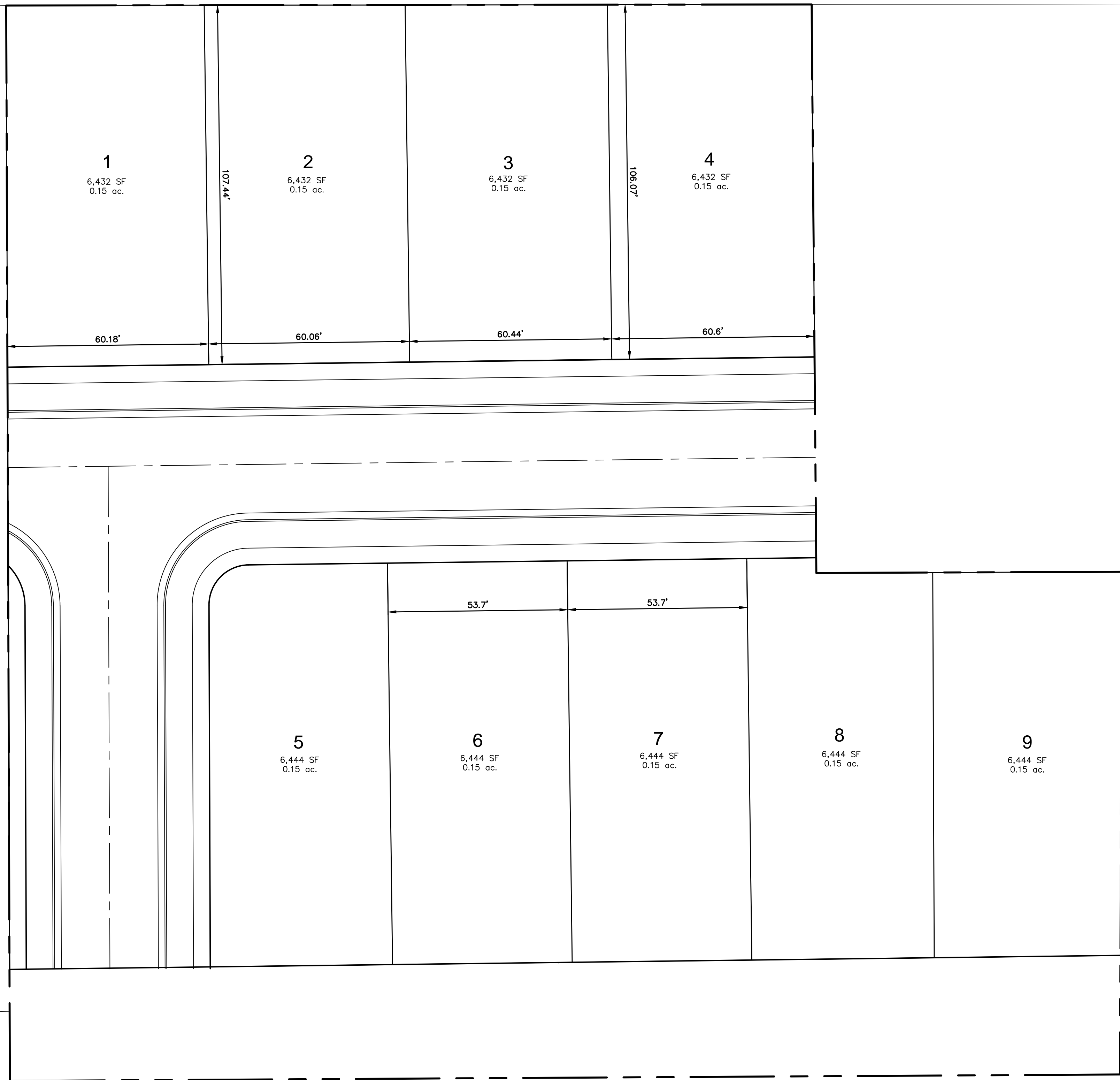
**LEGEND:**  
● = 5/8" REBAR WITH ORANGE CAP MARKED ATLAS ENGINEERING.  
--- POWERS DEED LINES  
--- ADJACENT LAND OWNER DEED LINES  
x x x EXISTING FENCES



**ATLAS ENGINEERING**  
 946 EAST 800 NORTH - SUITE #A  
 SPANISH FORK, UTAH - 84660  
 PHONE: (801) 655-0566

**JAYME D POWERS**  
 RECORD OF SURVEY PLAT  
 SPANISH FORK, UTAH

SHEET No. 1 OF 1  
 DATE: FEB. 2, 2021  
 DRAWN BY: B.L.P.  
 APPROVED BY: B.L.P.  
 SCALE: 1" = 60'



400 NORTH



0 5 10 20 40 60  
 (24"x36")  
 SCALE 1" = 20'  
 (11"x17")  
 SCALE 1" = 40'

SHEET NO.  
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CONCEPTUAL PLAN

SPANISH FORK, UTAH

ZIONS LANDING

ATLAS ENGINEERING  
 CIVIL · STRUCTURAL · SURVEY

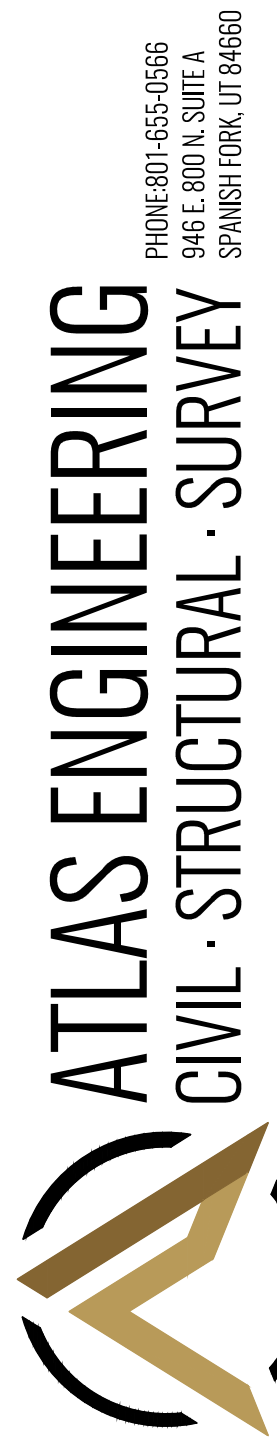
PHONE: 801-555-6566  
 945 E. 800 N. SUITE A  
 SPANISH FORK, UT 84660

Z:\2023\23-110\_HEAPS\_SPANISH\_FORK\CADD\CONCEPT\CONCEPT\_14.DWG



0 25 50 100 150  
 (24"x36")  
 SCALE 1" = 50'  
 (11"x17")  
 SCALE 1" = 100'

**PEREZ SPANISH FORK**



**ATLAS ENGINEERING**  
 CIVIL · STRUCTURAL · SURVEY  
 PHONE: 801-555-6566  
 946 E. 800 N. SUITE A  
 SPANISH FORK, UT 84660

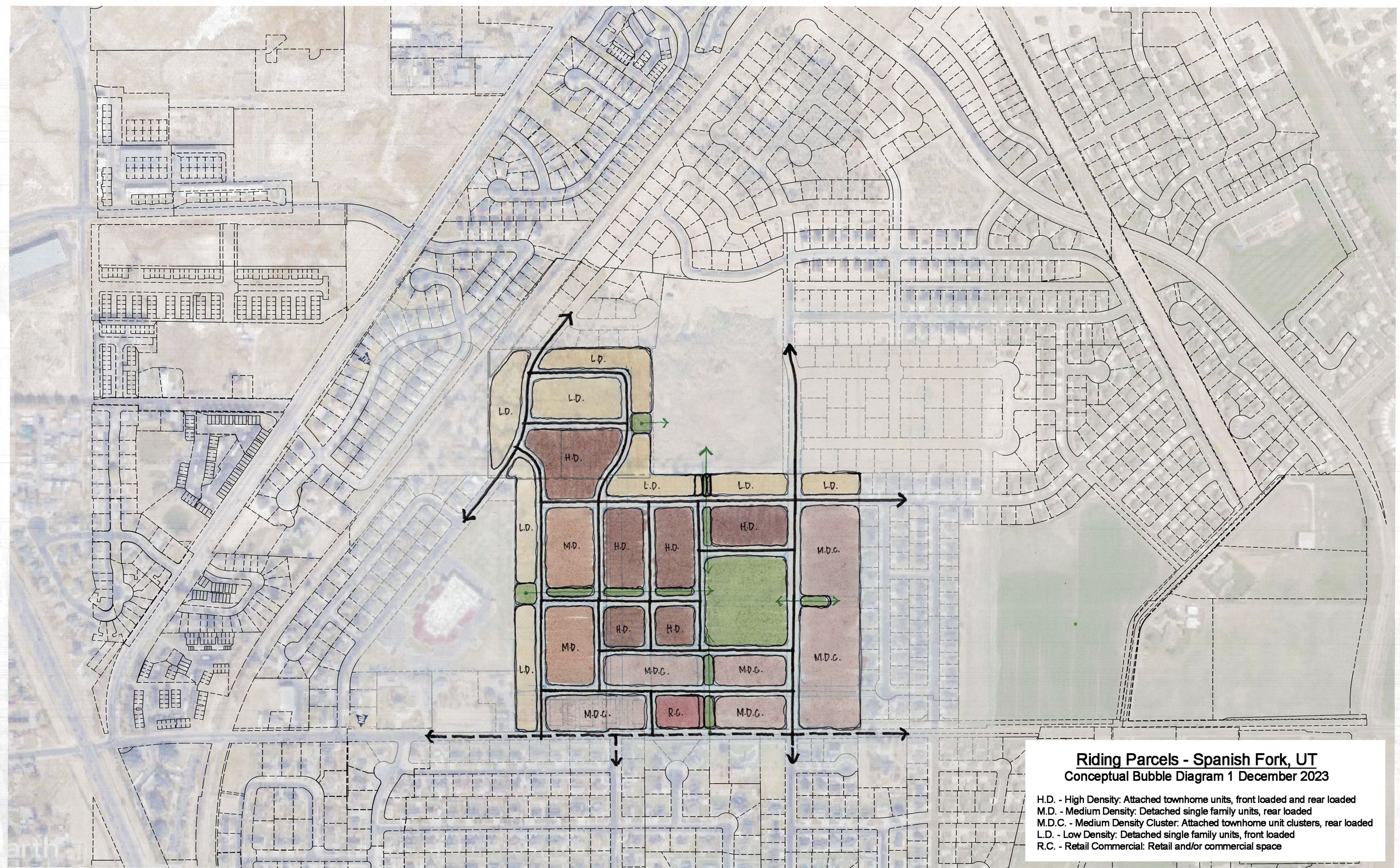
CONCEPT PLAN

SPANISH FORK, UTAH

SHEET NO.

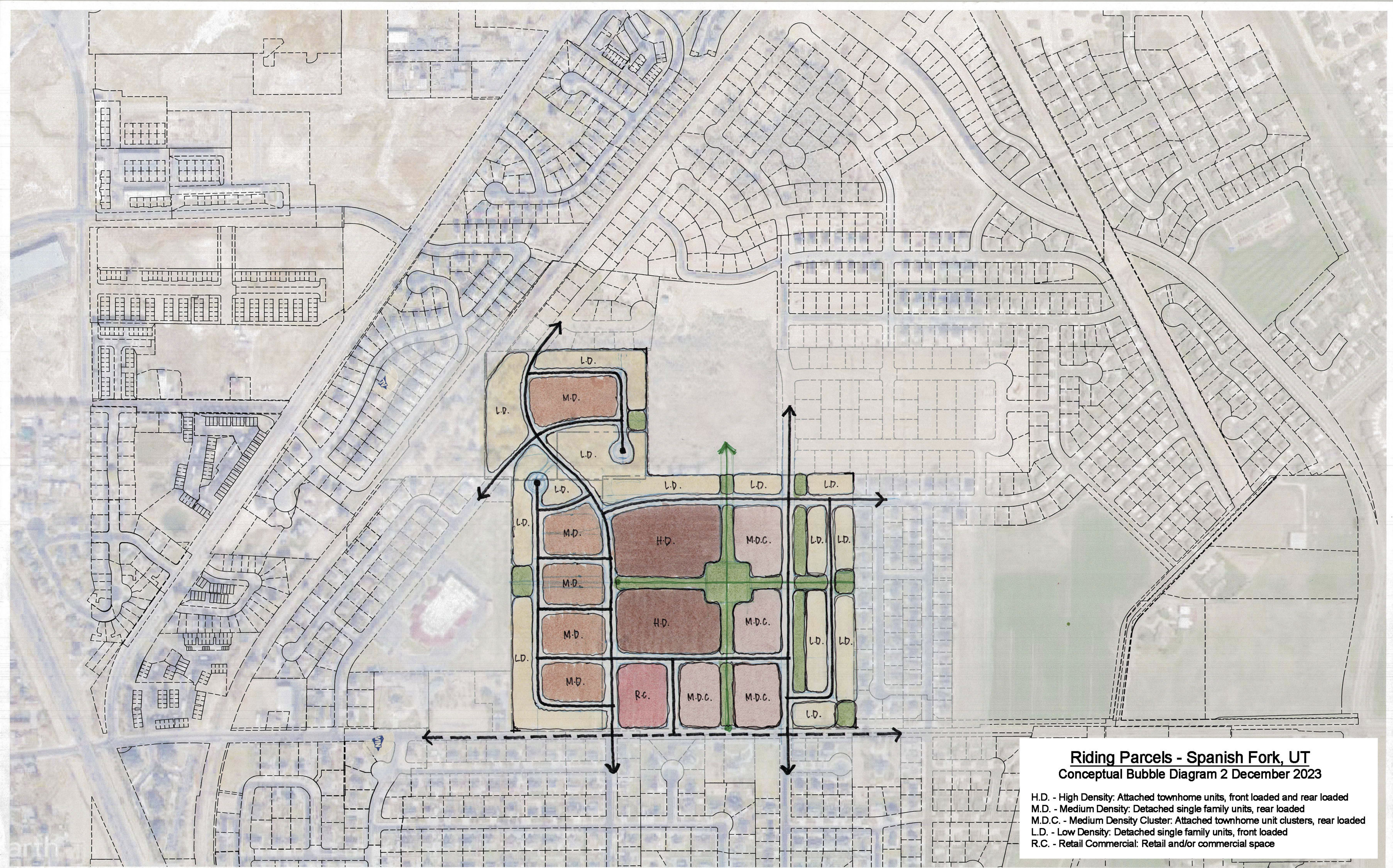
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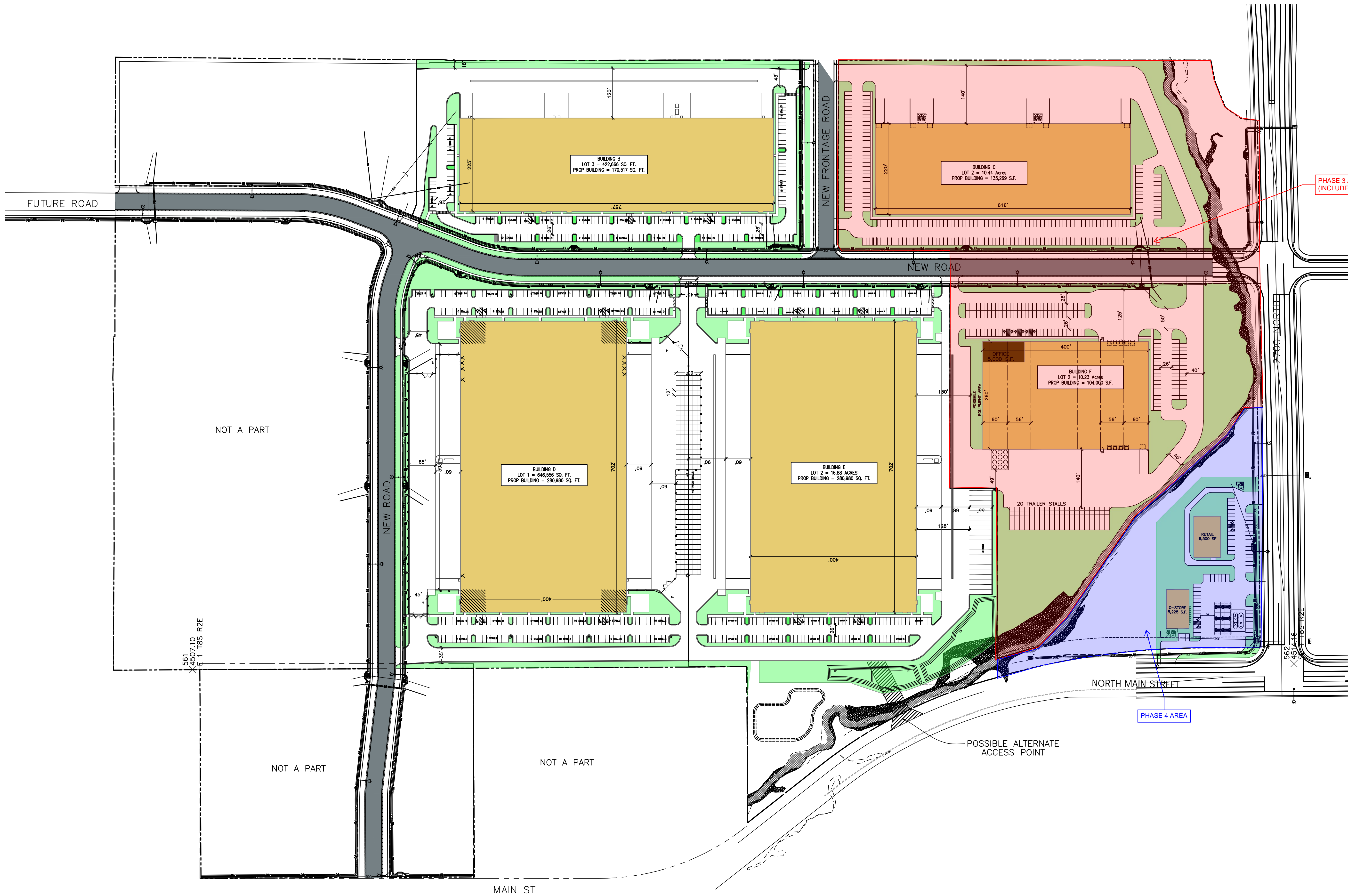
**Riding Parcels - Spanish Fork, UT**  
**Conceptual Bubble Diagram 1 December 2023**

H.D. - High Density: Attached townhome units, front loaded and rear loaded  
 M.D. - Medium Density: Detached single family units, rear loaded  
 M.D.C. - Medium Density Cluster: Attached townhome unit clusters, rear loaded  
 L.D. - Low Density: Detached single family units, front loaded  
 R.C. - Retail Commercial: Retail and/or commercial space



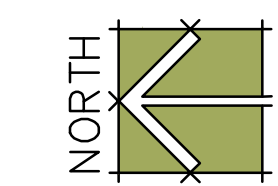
**Riding Parcels - Spanish Fork, UT**  
**Conceptual Bubble Diagram 2 December 2023**

H.D. - High Density: Attached townhome units, front loaded and rear loaded  
 M.D. - Medium Density: Detached single family units, rear loaded  
 M.D.C. - Medium Density Cluster: Attached townhome unit clusters, rear loaded  
 L.D. - Low Density: Detached single family units, front loaded  
 R.C. - Retail Commercial: Retail and/or commercial space



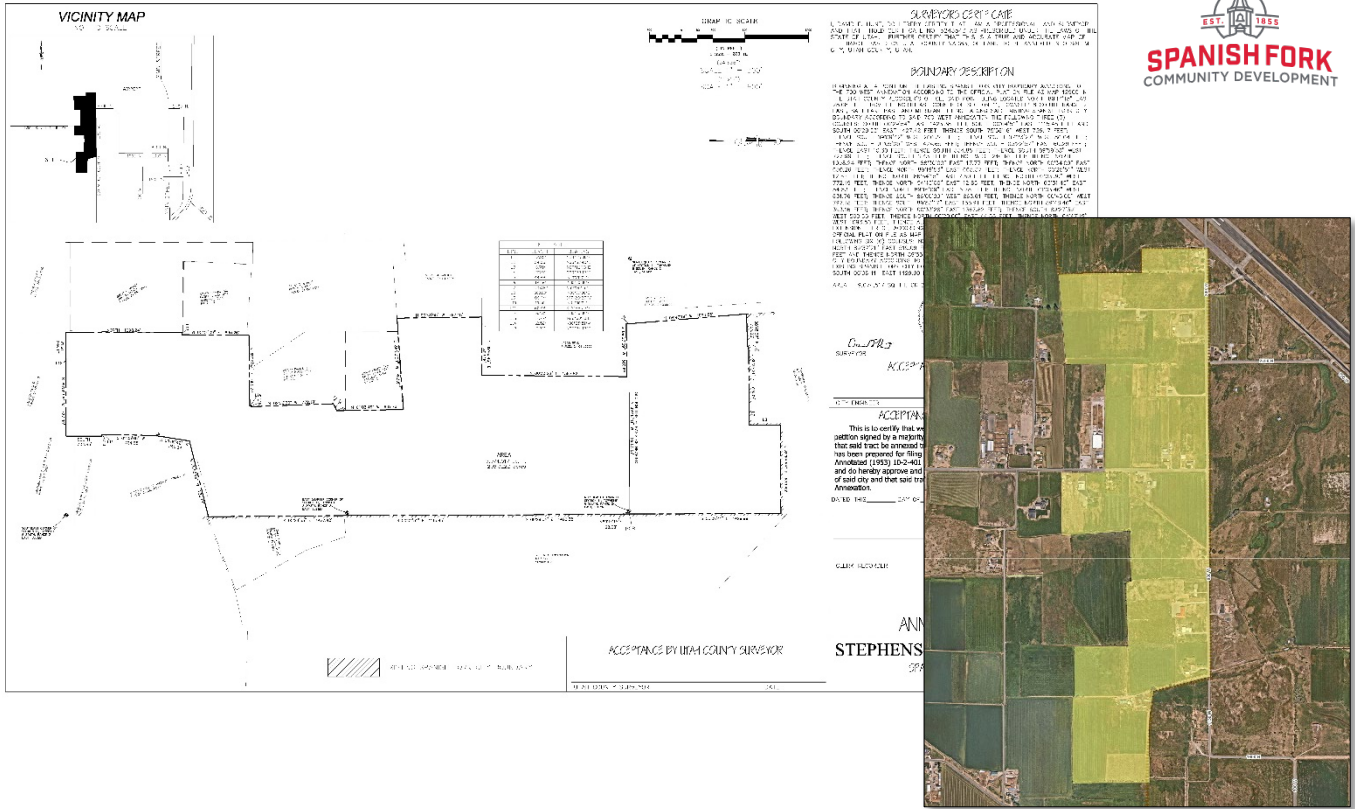
**SITE DATA**

BUILDINGS B	170,517 S.F.
TOTAL PARKING	220 SPACES
RATIO	1.35/1,000 S.F.
LOT ACREAGE	9.99 Ac.
BUILDINGS C	135,269 S.F.
TOTAL PARKING	249 SPACES
RATIO	1.69/1,000 S.F.
LOT ACREAGE	10.44 Ac.
BUILDINGS D	280,980 S.F.
TOTAL PARKING	256 SPACES
RATIO	0.91/1,000 S.F.
LOT ACREAGE	14.84 Ac.
BUILDINGS E	280,980 S.F.
TOTAL PARKING	235 SPACES
RATIO	0.84/1,000 S.F.
LOT ACREAGE	16.88 Ac.
BUILDINGS F	104,000 S.F.
TOTAL PARKING	143 SPACES
RATIO	1.538/1,000 S.F.
LOT ACREAGE	10.23 Ac.





**SPANISH FORK**  
COMMUNITY DEVELOPMENT



**Jim Jensen, Stephens Hill Annexation Approval Request**

February 7, 2024, Development Review Committee meeting.

Located at approximately 2000 North 1150 West, including 208.32 acres.

The subject property is currently outside City boundaries but is located within the City's Annexation Policy Boundary.

**Key Issues**

1. Annexation Configuration.
2. Growth Management Boundary.
3. General Plan.
4. Land Uses.
5. Zoning.
6. Utilities.
7. Power.
8. Access.

**Recommendation**

That the proposed Annexation be recommended for approval with R-R zoning based on the following findings.

**Findings**

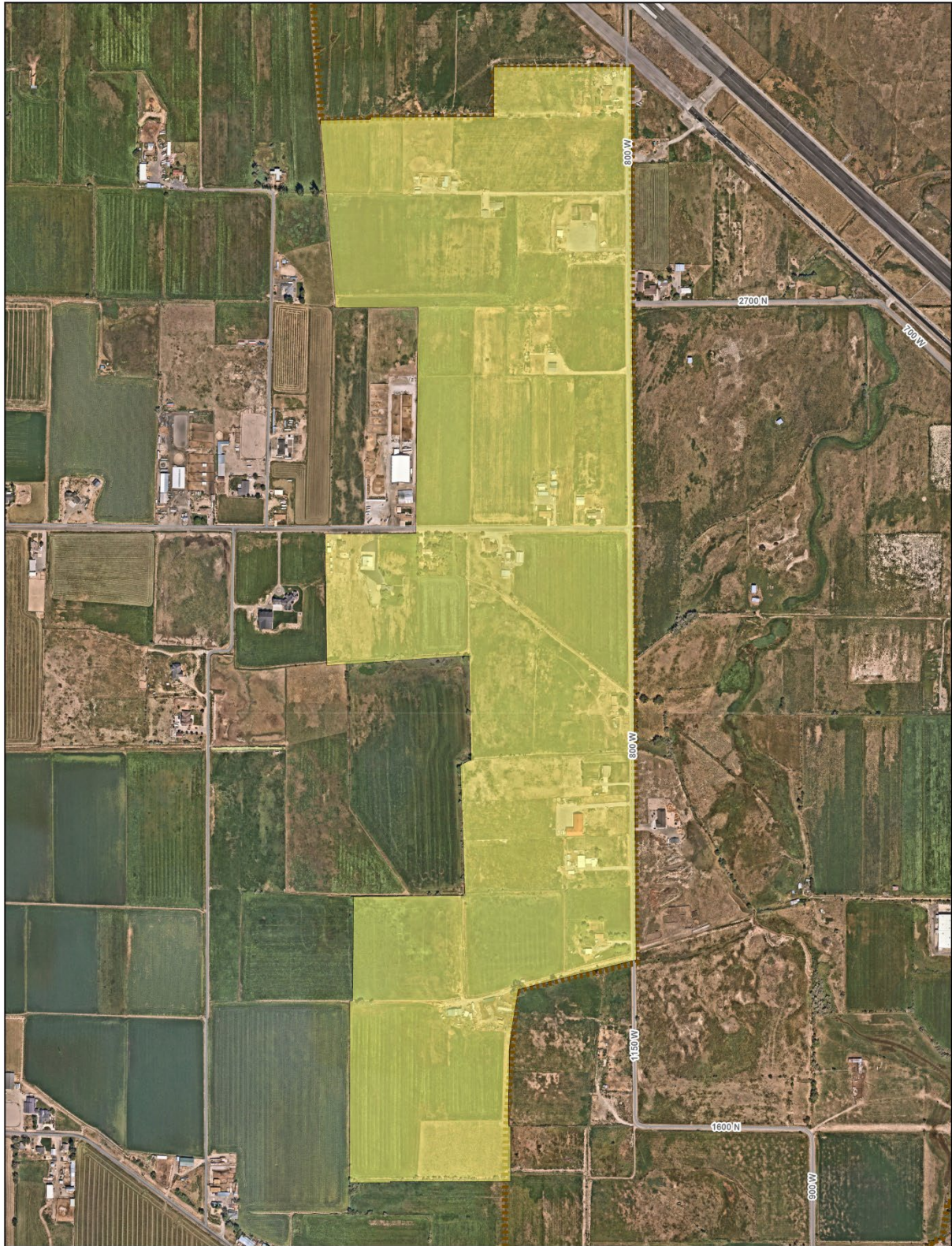
1. That the subject property is located within the City's Annexation Policy Boundary.
2. That the City's General Plan Land Use Designation is Agricultural and Industrial.
3. That the R-R Rural Residential Zone is appropriate for the area at this time.

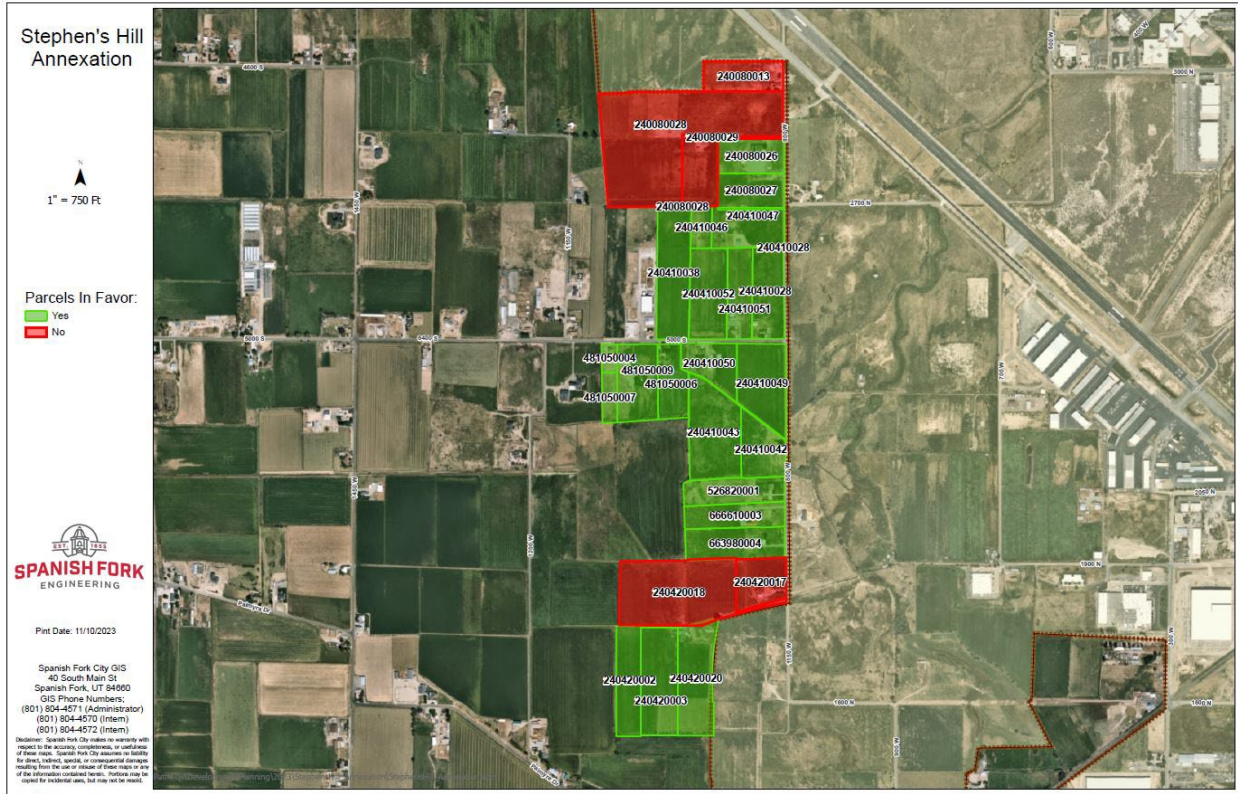
**Exhibits**

1. Annexation Plat.
2. Parcels.
3. Annexation Feasibility Report.









# CANYON COURT COMMERCIAL

A COMMERCIAL SUBDIVISION  
SPANISH FORK, UTAH  
PRELIMINARY PLAN SET

## -SHEET INDEX-

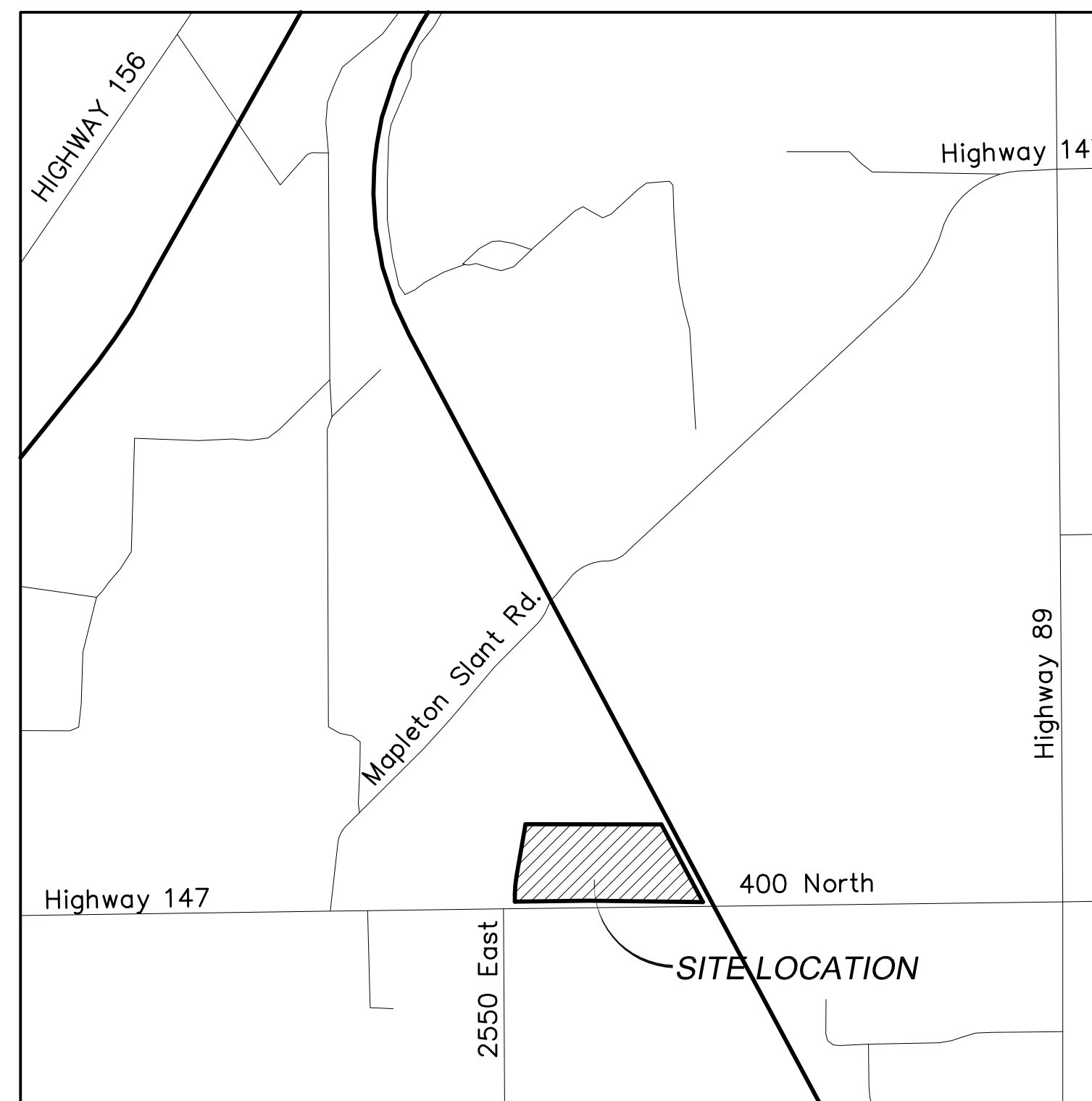
SHEET	SHEET NAME
1	COVER
2	OVERALL BOUNDARY
3	OVERALL SITE PLAN
4	EXISTING TOPOGRAPHY
5	DRAINAGE PLAN
6	STRIPING PLAN
DT-01	DETAIL SHEET
ROS	RECORD OF SURVEY

### GENERAL NOTES:

1. ALL CONSTRUCTION WILL CONFORM TO SPANISH FORK CITY STANDARDS.
2. ELECTRICAL SYSTEM TO BE INSTALLED AS PER SPANISH FORK CITY ELECTRICAL DESIGN AND STANDARDS. ANY ELECTRICAL DESIGNS AS PER ARCHITECT/DEVELOPER DRAWING SHALL BE SUPERSEDED BY CITY DESIGN.
3. ALL ELECTRICAL IMPROVEMENTS SHALL BE INSTALLED TO THE MOST CURRENT VERSION OF THE SPANISH FORK CITY CONSTRUCTION STANDARDS.
4. IF NEWER VERSION OF STANDARDS IS RELEASED DURING PROJECT THE PROJECT SHALL BE COMPLETED USING STANDARDS VERSION THAT WERE ISSUED AT PRE-CONSTRUCTION MEETING.
5. ANY EXISTING FACILITIES AFFECTED BY THE DEVELOPMENT WILL NEED TO BE BROUGHT TO CURRENT NEC, NESC, AND CITY STANDARDS AT DEVELOPERS EXPENSE INCLUDING BUT NOT LIMITED TO UPGRADING OF METER BASE, SERVICE CONDUITS, COMMUNICATIONS SERVICES, AND LANDSCAPING RESTORATION.

### OWNER/DEVELOPER

CORY ANDERSEN  
CORYWANDERSEN@GMAIL.COM  
801-787-3073

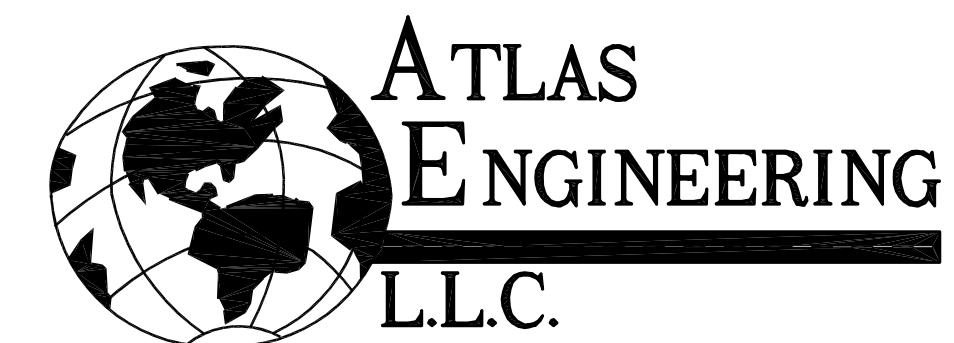


VICINITY MAP

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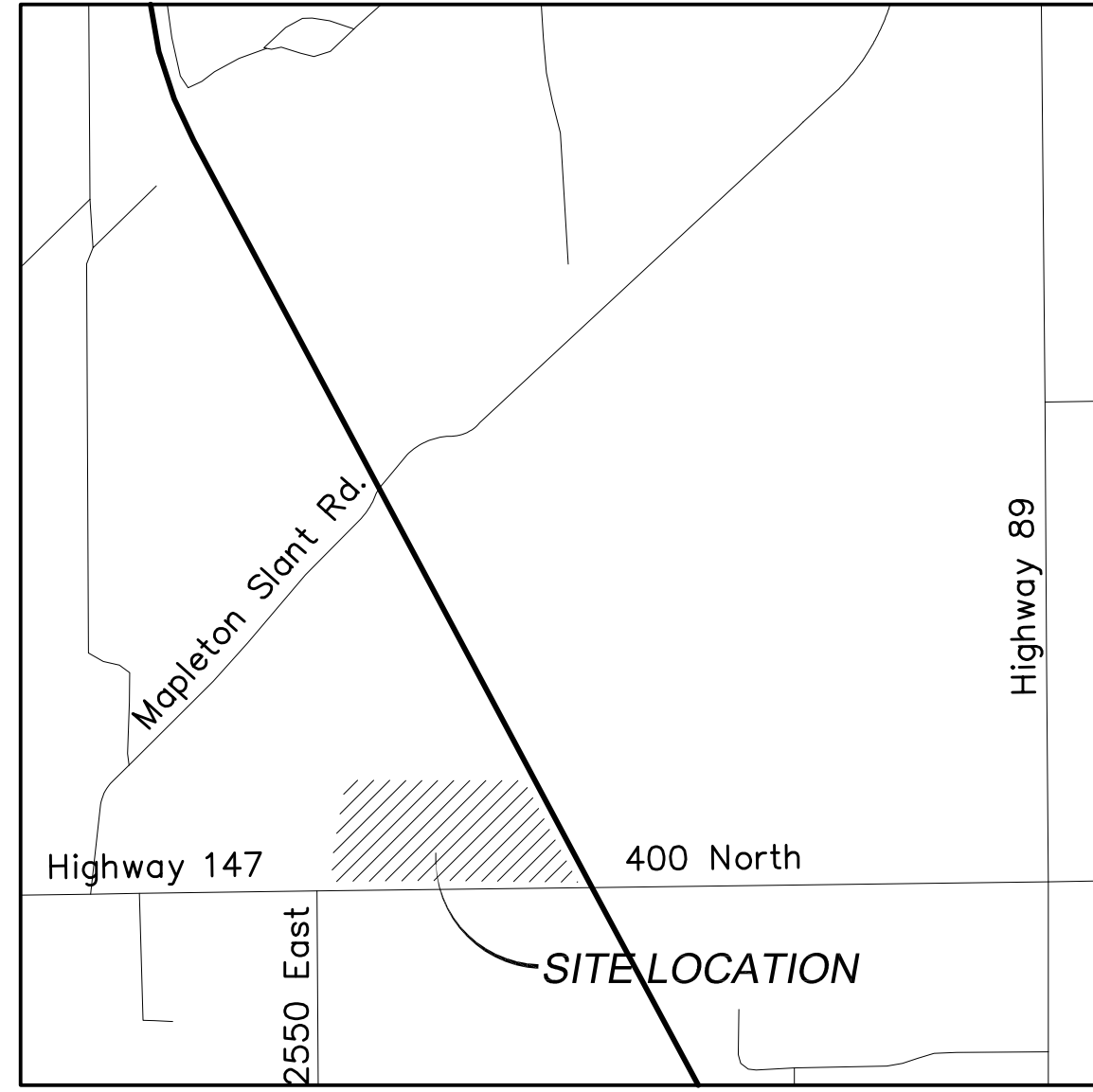


CANYON COURT COMMERCIAL



PHONE: 801-655-0566  
FAX: 801-655-0109  
946 E. 800 N. SUITE A  
SPANISH FORK, UT 84660





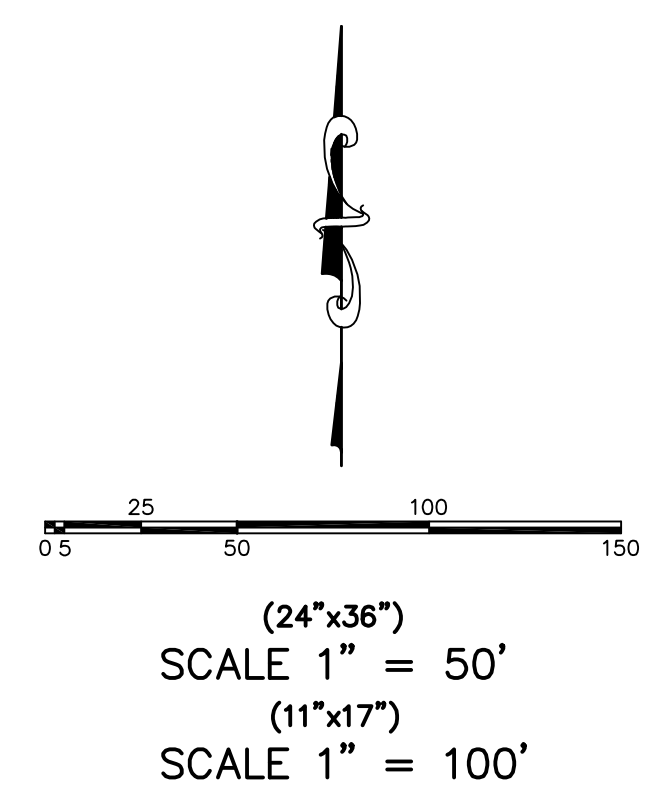
**VICINITY MAP**

**LEGEND**

	SECTION CORNER		LOT LINE
	FOUND ALUMINUM CAP		SECTION LINE
	SET 5/8" IRON PIN		EASEMENT
	CALCULATED POINT, NOT SET		EXISTING DEED LINE
	EXISTING POWER POLE		EDGE OF PAVEMENT
	PROPOSED STREET LIGHT		EXISTING OVER HEAD POWER
	PROPOSED FIRE HYDRANT		EXISTING FENCE LINE
	PROPOSED SIGN		EXISTING DITCH
	PROPOSED STREET SIGN		EXISTING SANITARY SEWER W/MANHOLE
	PROPERTY BOUNDARY		PROPOSED STORM DRAIN
	CENTERLINE		PROPOSED PVC SDR-35 SEWER W/MH
	RIGHT-OF-WAY LINE		PROPOSED CULINARY WATERLINE
			PROPOSED PRESSURIZED IRRIGATION

**NOTES:**  
 1. ALL FIRE HYDRANT LOCATIONS TO BE APPROVED BY THE SPANISH FORK CITY FIRE MARSHALL AT FINAL DESIGN.

- CONSTRUCTION NOTES:**
- 1) INSTALL ADA RAMP PER UDOT STANDARDS.
  - 2) INSTALL STREET LIGHT PER SPANISH FORK CITY STANDARDS.
  - 3) INSTALL 8" CULINARY WATER VALVE PER SPANISH FORK CITY STANDARDS.
  - 4) INSTALL FIRE HYDRANT PER SPANISH FORK CITY STANDARDS.
  - 5) LOCATE AND TIE TO EXISTING CULINARY WATER LINE.
  - 6) LOCATE AND TIE TO EXISTING PRESSURIZED IRRIGATION.
  - 7) CAP/PLUG AND MARK TO SURFACE.
  - 8) EXISTING DITCH TO BE REMOVED.



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OVERALL SITE PLAN

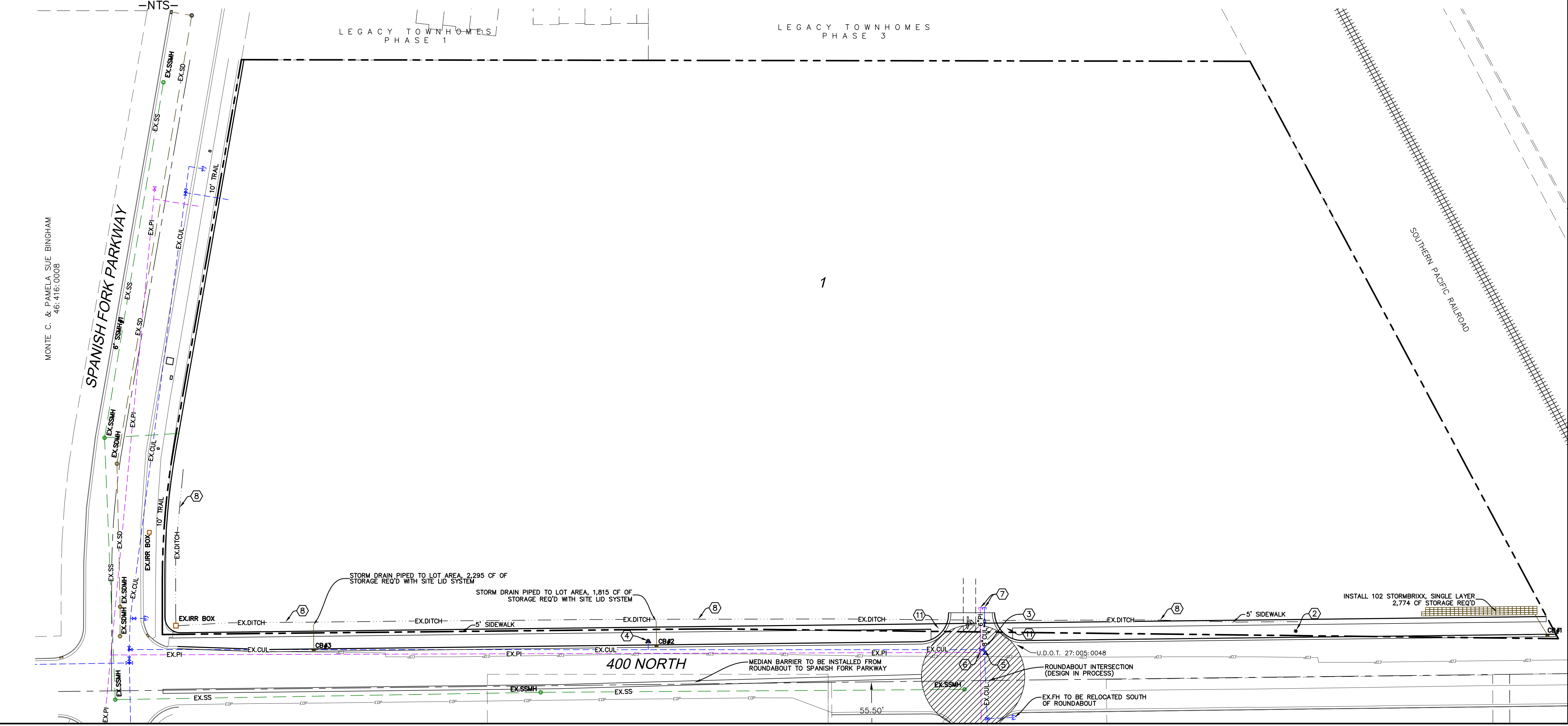
SPANISH FORK, UTAH

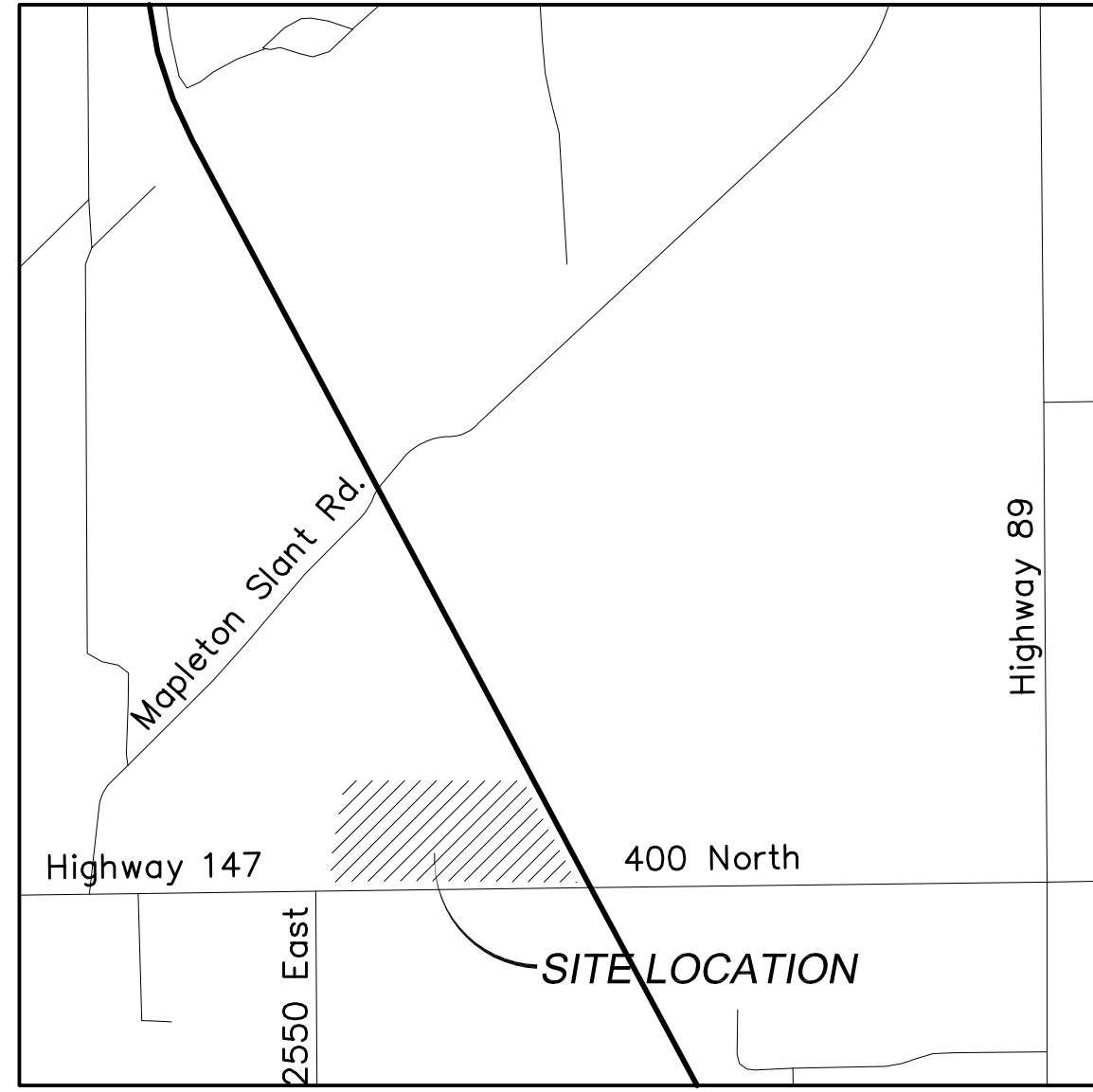
CANYON COURT COMMERCIAL



PHONE: 801-655-0566  
 FAX: 801-655-0109  
 946 E. 800 N. SUITE A  
 SPANISH FORK, UT 84660

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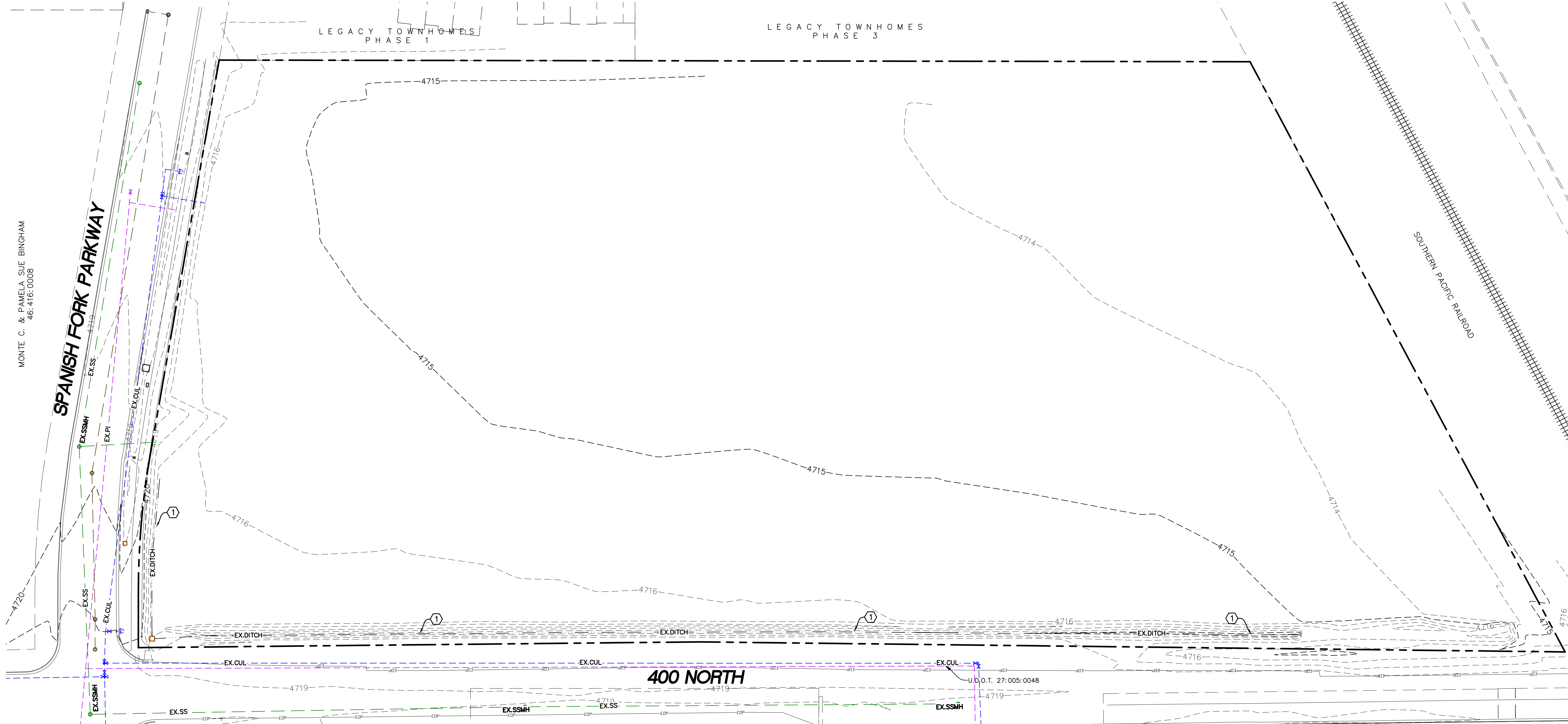
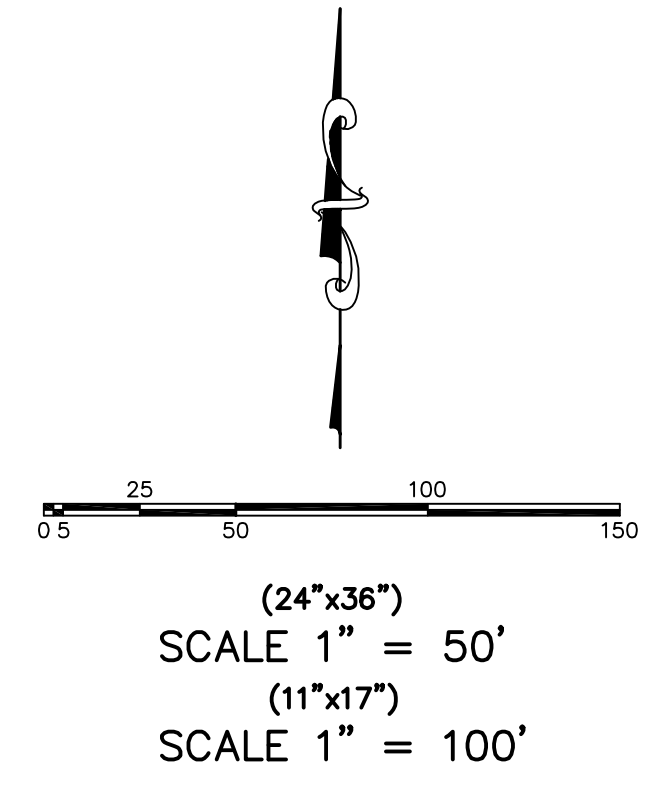




VICINITY MAP  
-NTS-

LEGEND	
	SECTION CORNER
	FOUND ALUMINUM CAP
	SET 5/8" IRON PIN
	CALCULATED POINT, NOT SET
	EXISTING POWER POLE
	PROPOSED STREET LIGHT
	PROPOSED FIRE HYDRANT
	PROPOSED SIGN
	PROPOSED STREET SIGN
	PROPERTY BOUNDARY
	CENTERLINE
	RIGHT-OF-WAY LINE
	LOT LINE
	SECTION LINE
	EASEMENT
	EXISTING DEED LINE
	EDGE OF PAVEMENT
	EXISTING OVER HEAD POWER
	EXISTING FENCE LINE
	EXISTING DITCH
	EXISTING SANITARY SEWER W/MANHOLE
	PROPOSED STORM DRAIN
	PROPOSED PVC SDR-35 SEWER W/MH
	PROPOSED CULINARY WATERLINE
	PROPOSED PRESSURIZED IRRIGATION

CONSTRUCTION NOTES:  
① EXISTING DITCH TO BE REMOVED.



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EXISTING TOPOGRAPHY  
SPANISH FORK, UTAH

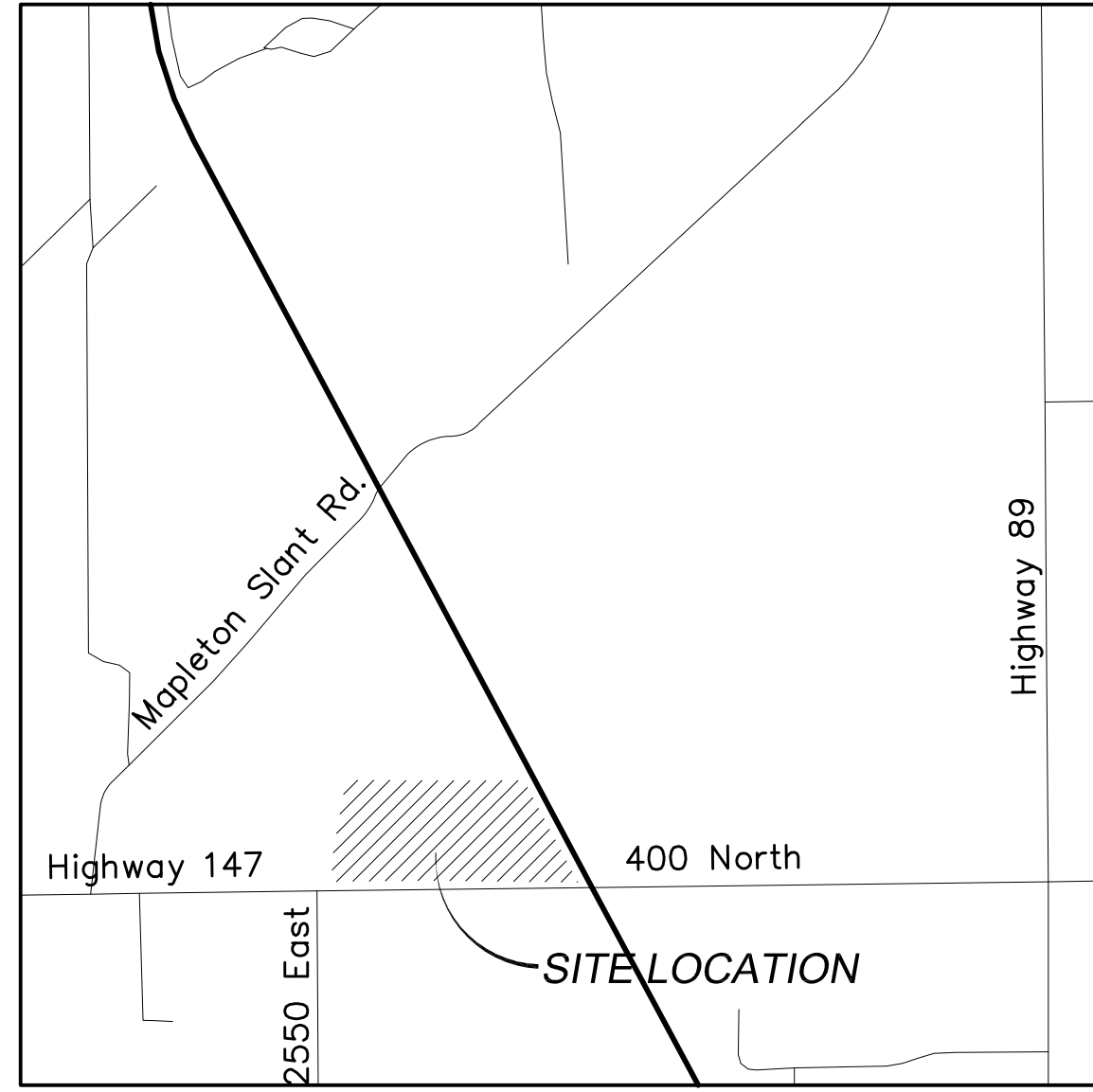
CANYON COURT COMMERCIAL

ATLAS ENGINEERING L.L.C.

PHONE: 801-655-0566  
FAX: 801-655-0109  
946 E. 800 N. SUITE A  
SPANISH FORK, UT 84660

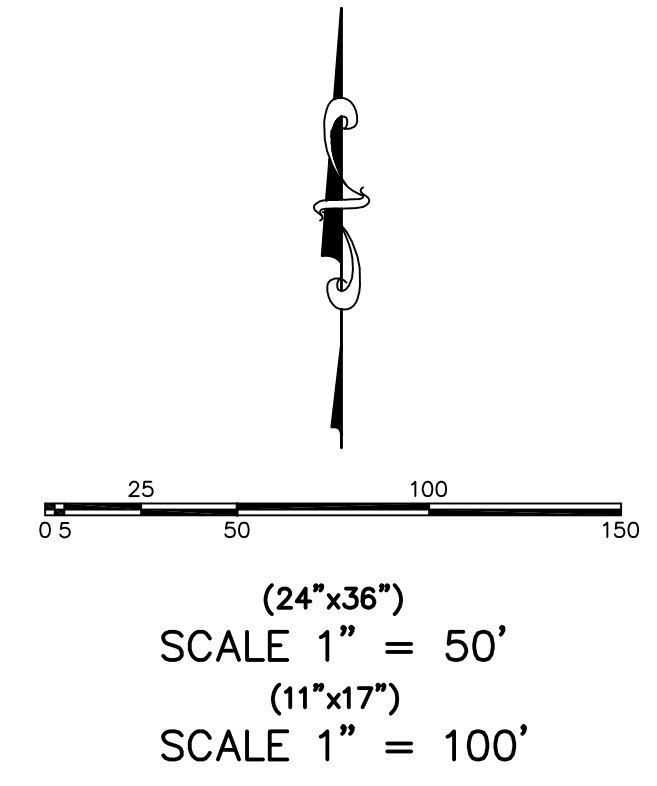
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**VICINITY MAP**

LEGEND			
	SECTION CORNER		LOT LINE
	FOUND ALUMINUM CAP		SECTION LINE
	SET 5/8" IRON PIN		EASEMENT
	CALCULATED POINT, NOT SET		EXISTING DEED LINE
	EXISTING POWER POLE		EDGE OF PAVEMENT
	PROPOSED STREET LIGHT		EXISTING OVER HEAD POWER
	PROPOSED FIRE HYDRANT		EXISTING FENCE LINE
	PROPOSED SIGN		EXISTING DITCH
	PROPOSED STREET SIGN		EXISTING SANITARY SEWER W/MANHOLE
	PROPERTY BOUNDARY		PROPOSED STORM DRAIN
	CENTERLINE		PROPOSED PVC SDR-35 SEWER W/MH
	RIGHT-OF-WAY LINE		PROPOSED CULINARY WATERLINE
			PROPOSED PRESSURIZED IRRIGATION



SHEET NO. 5

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DRAINAGE PLAN

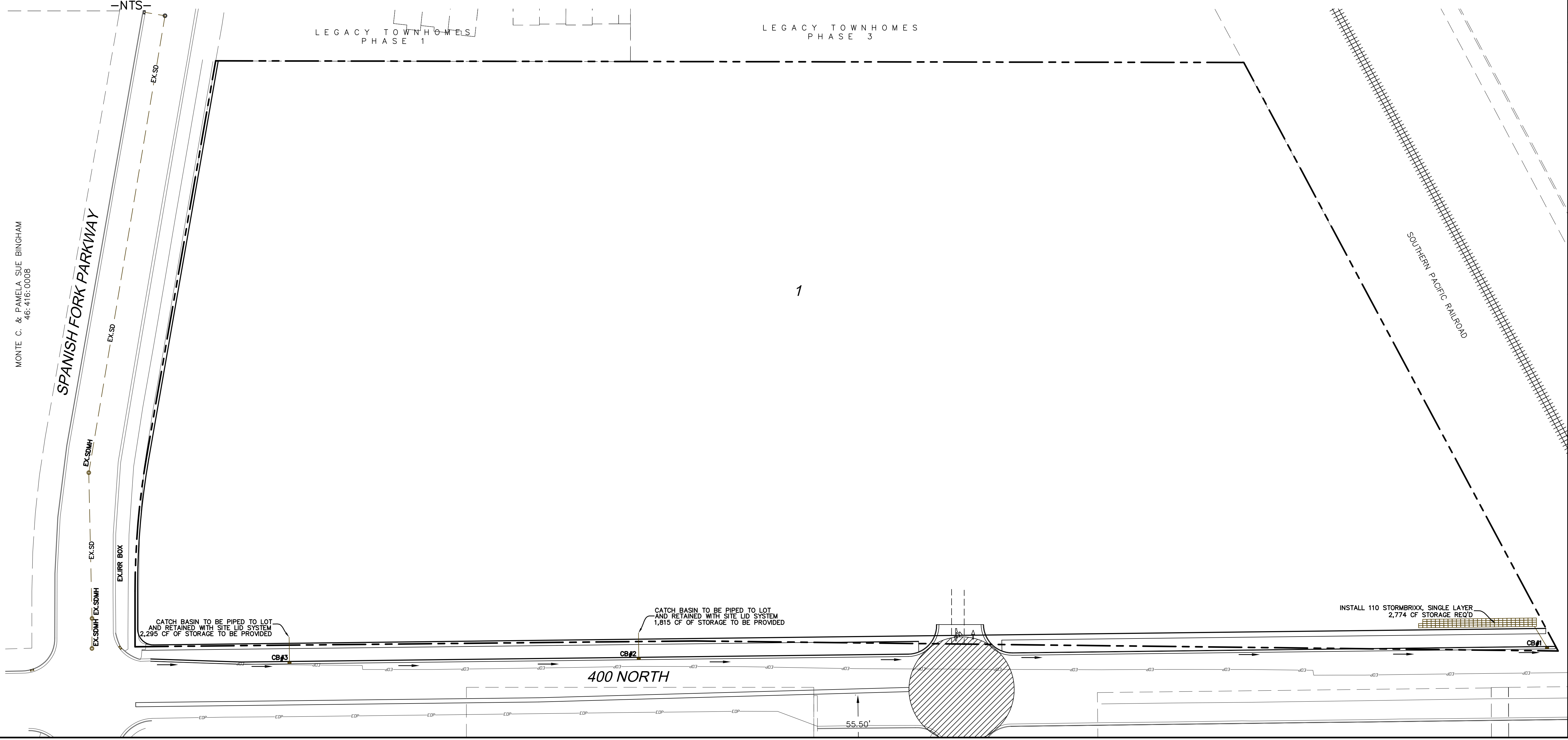
SPANISH FORK, UTAH

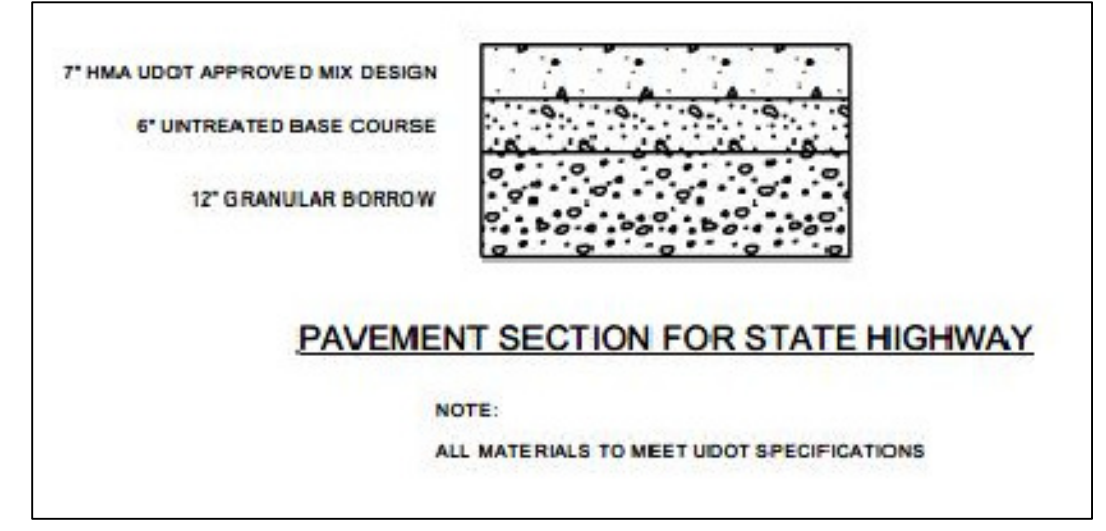
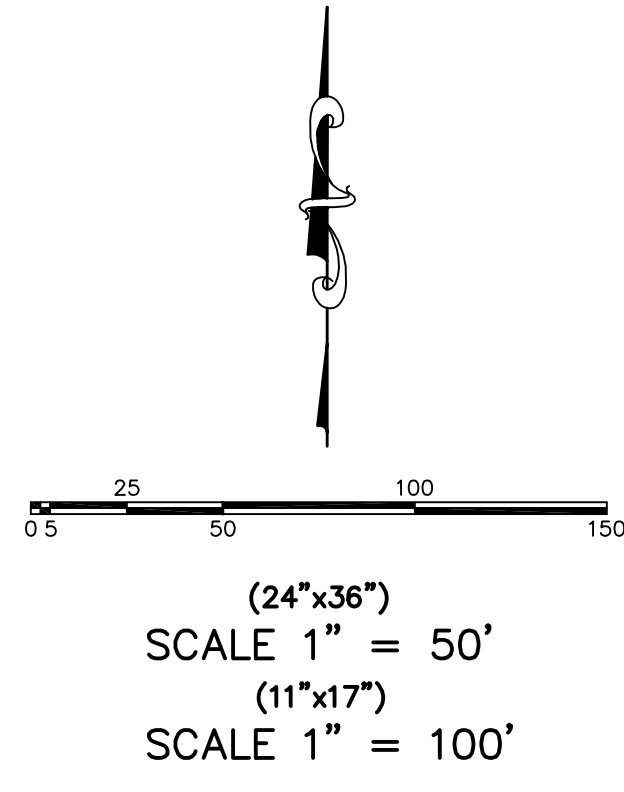
CANYON COURT COMMERCIAL



PHONE: 801-655-0566  
FAX: 801-655-0109  
946 E. 800 N. SUITE A  
SPANISH FORK, UT 84660

DATE: 11/13/2024 PROJECT: CANYON COURT COMMERCIAL SHEET: DRAINAGE PLAN NO. 5





**CANYON COURT COMMERCIAL**

**ATLAS ENGINEERING L.L.C.**

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FAX: 801-655-0109  
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SPANISH FORK, UT 84660

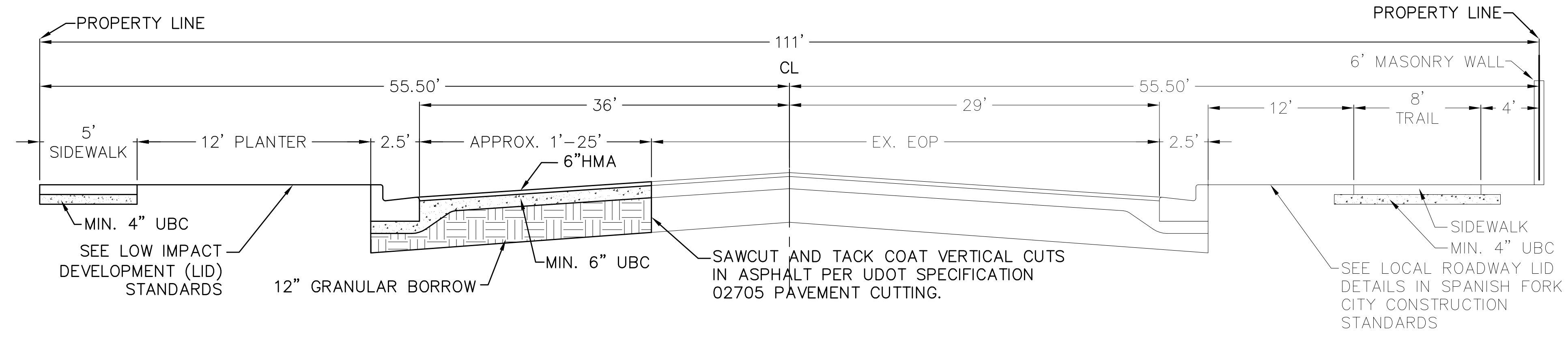
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STRIPING PLAN

SPANISH FORK, UTAH

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SHEET NO. **6**



**MAJOR COLLECTOR STREET - 111' ROW**  
-NTS-

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**DETAIL SHEET**

**SPANISH FORK, UTAH**

**CANYON COURT COMMERCIAL**

**ATLAS ENGINEERING L.L.C.**

PHONE: 801-655-0566  
 FAX: 801-655-0109  
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 SPANISH FORK, UT 84660

1/18/2024 11:16:17 AM MST

NARRATIVE

1. THE PURPOSE OF THIS SURVEY IS SHOW THE RELATIONSHIP BETWEEN THE PHYSICAL POSSESSION AND THE EXISTING DEEDS. THE BOUNDARY OF THE REQUESTED PROPERTY SHOWN IS PROPOSED AND REQUIRES CORRECTIVE ACTION.
2. THE PROPERTY IS LOCATED IN THE SOUTHWEST QUARTER OF SECTION 16, T8S, R3E, SLB&M.
3. FOUND THE SOUTH QUARTER SECTION CORNER AND THE SOUTHWEST SECTION CORNER FOR SAID SECTION 16.
4. THE BASIS OF BEARING IS SOUTH 89° 19' 10" WEST BETWEEN THE SOUTH QUARTER CORNER AND THE SOUTHWEST SECTION CORNER OF SAID SECTION 16.
5. FOUND REBAR & CAP, AS SHOWN HEREON. SET PROPERTY MARKERS, AS SHOWN HEREON. IF CORNER FALLS IN CONCRETE OR FENCE CORNER NOTHING WAS SET.
6. DEED LINES SHOWN, IF GIVEN A BASIS OF BEARING, HAVE BEEN ROTATED TO THE BASIS OF BEARING SHOWN HEREON. IF NONE WAS GIVEN, NONE WAS ASSUMED.

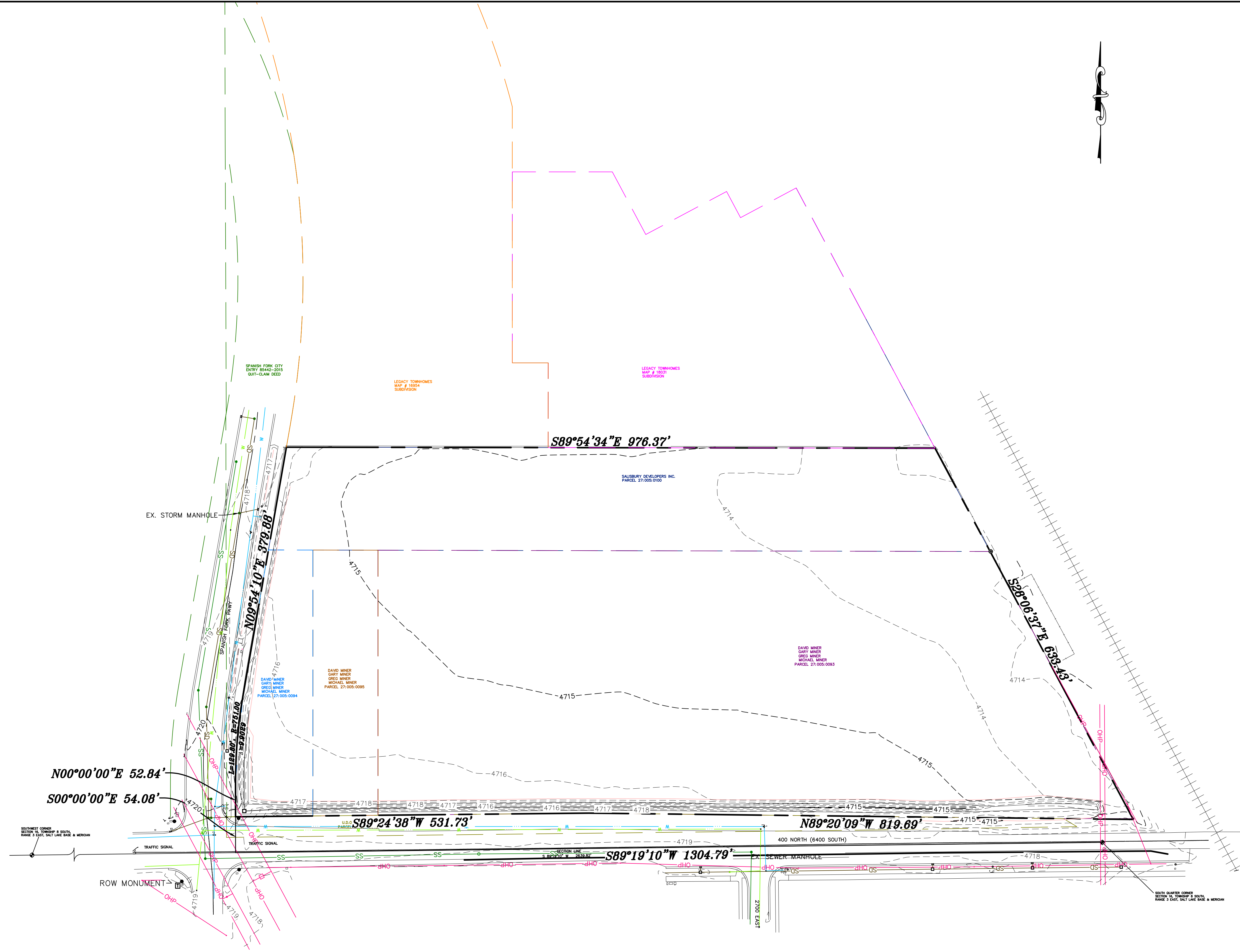
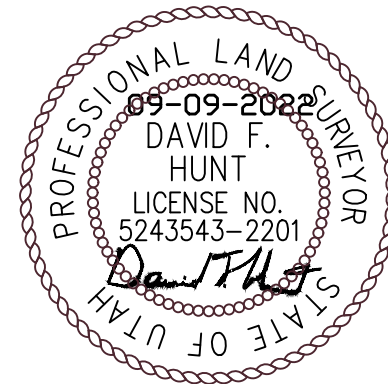
PROPOSED DESCRIPTION

BEGINNING AT A POINT LOCATED SOUTH 89° 19' 10" WEST 1304.79 FEET ALONG THE SECTION LINE AND NORTH 54.08 FEET FROM THE SOUTH QUARTER CORNER SECTION 16, TOWNSHIP 8 SOUTH, RANGE 3 EAST, SALT LAKE BASE AND MERIDIAN; THENCE ALONG THE EAST RIGHT-OF-WAY OF SPANISH FORK PARKWAY THE NEXT 3 CALLS, THENCE NORTH 52.84 FEET, THENCE ALONG A 751 FOOT RADIUS CURVE TO THE RIGHT 129.80 FEET, CORD BEARS NORTH 04° 57' 05" EAST 129.64 FEET, AND THENCE NORTH 09° 54' 10" EAST 379.88 FEET TO THE SOUTH BOUNDARY OF LEGACY TOWNHOMES SUBDIVISION ACCORDING TO THE OFFICIAL PLAT OF RECORD RECORDED AS MAP 16954 IN THE UTAH COUNTY RECORDER'S OFFICE; THENCE SOUTH 89° 54' 34" EAST 976.37 FEET ALONG SAID SUBDIVISION BOUNDARY AND THE SOUTH BOUNDARY OF LEGACY TOWNHOMES SUBDIVISION PHASE 3, ACCORDING TO THE OFFICIAL PLAT OF RECORD RECORDED AS MAP 18031 IN THE UTAH COUNTY RECORDER'S OFFICE; THENCE SOUTH 28° 06' 37" EAST 633.43 FEET; THENCE NORTH 89° 20' 09" WEST 819.69 FEET; THENCE SOUTH 89° 24' 38" WEST 531.73 FEET TO THE POINT OF BEGINNING.

AREA= 648,118.4027 SQ. FEET OR 14.8787 ACRES, MORE OR LESS.

CERTIFICATION

I, DAVID F. HUNT, HEREBY CERTIFY THAT I AM A PROFESSIONAL LAND SURVEYOR IN THE STATE OF UTAH; THAT THIS MAP CORRECTLY REPRESENTS AN BOUNDARY AND TOPOGRAPHIC SURVEY MADE UNDER MY SUPERVISION DURING THE MONTH OF JANUARY, 2022; THAT THE SURVEY IS TRUE AND CORRECT AS SHOWN; THAT THE PROPERTY BOUNDARY SHOWN & DESCRIBED HEREON IS CORRECT AND CAN BE RETRACED.



SHEET NO.  
**1**

RECORD OF SURVEY  
SPANISH FORK, UTAH

ANDERSON PROPERTY  
ATLAS  
ENGINEERING  
L.L.C.

PHONE: 801-655-0566  
846 N. 800 N. SUITE A  
SPANISH FORK, UT 84660

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UPDATED: 08-09-2022  
REVISIONS